

DRAFT CUTLER/OROSI COMMUNITY PLAN 2020 UPDATE

Tulare County Resource Management Agency
Economic Development and Planning Branch



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Cutler/Orosi Community Plan 2020 Update

Adopted: ----

Tulare County Board of Supervisors

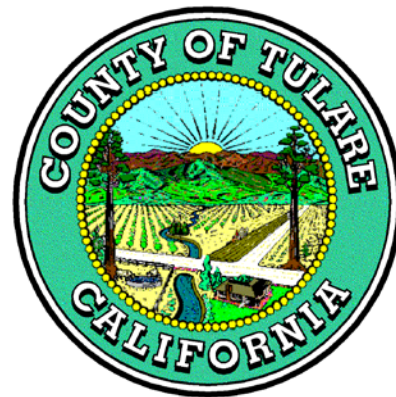
Resolution No. ----

Tulare County Planning Commission

Recommendations:

Resolutions No.

Cutler/Orosi Community Plan Update: GPA 18-003
Section 18.9 Zoning Ordinance (Mixed Use): PZC 18-009
Section 16 Zoning Ordinance (By Right) PZC 18-010
Zoning District Map: PZC 18-011



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Economic Development and Planning Branch
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Executive Summary

Introduction

The Cutler/Orosi Community Plan was adopted by the Tulare County Board of Supervisors (BOS) on August 30, 1988 (by Resolution No. 88-1051). Subsequent to the adoption of the Community Plan a series of General Plan Amendments were adopted as follows: GPA 89-03 adopted by the BOS on November 6, 1990 by Resolution 90-1346; GPA 94-004, GPA 94-005, and GPA 95-001 on November 7, 1995 by Resolution No. 95-1272; GPA 95-005 and GPA 95-006 were adopted by the BOS on April 23, 1996 by Resolution No. 96-0335; GPA 98-004 adopted by the BOS on July 13, 1999 by Resolution No. 99-0480; GPA 00-001 adopted by the BOS on October 10, 2000 by Resolution No. 2000-771; GPA 00-005 adopted by BOS on August 27, 2002 by Resolution No. 2002-0652; and GPA 09-003 adopted by BOS on October 14, 2014 by Resolution No. 2014-0717. The 1988 Cutler/Orosi Community Plan is a collection of goals, objectives, and policies for the physical development of the community. The primary purpose of the plan was to outline community goals regarding physical development and to promote the general welfare of the communities. The plan serves as a general guide for both public and private decisions affecting the community, and provides for the overall direction, density, and type of growth consistent with the needs of the communities.

General Plan Amendments

The proposed 2020 Cutler-Orosi Community Plan is consistent with the Tulare County General Plan 2030 Update (2012) and will include the following primary goals and objectives:

1) Land Use and Environmental Planning - Promote development within planning areas next to the Regional SR 63 Corridor in order to implement the following General Plan goals:

- Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters, such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals;
- Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
- Reduce development pressure on agriculturally designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
- Reduce vehicle miles travelled throughout the County, thereby positively affecting air quality and greenhouse gas reduction; and
- Help to improve the circulation and transit transportation system within this community, including, but not limited to, laying the groundwork for the construction of key projects such as Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths.

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- 2) Improvements for a “disadvantaged community” - It is expected that the community planning areas will be improved for the following reasons:
 - With faster project processing resulting from an updated community plan, increased employment opportunities are more likely to be provided by the private sector as proposed project developments can be approved as expeditiously as possible;
 - Increased housing grant awards are more likely to occur based on updated community plans that are consistent with the policies of the General Plan 2030 Update (August 2012) and the recently adopted Housing Element 2015 Update (November 2015); and
 - With updated community plans, enhanced infrastructure grant awards are more likely, thereby providing access to funding to install or upgrade road, water, wastewater, and storm water facilities.
- 3) Strengthening Relationship with TCAG - An important benefit of this expedited community plan process will be the opportunity for RMA to strengthen the County’s relationship with the Tulare County Association of Governments (TCAG) in that this and other community plans will help to facilitate the funding and implementation of several key transportation programs, such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects.

By pursuing these transportation programs through a heightened collaborative process, the likelihood of getting actual projects in the ground will be realized faster than historically achieved. In doing so, these communities and others can become safer and healthier by providing a more efficient transportation network.

The proposed 2020 Cutler-Orosi Community Plan amends the 1988 Cutler-Orosi Community Plan with this proposed General Plan Amendment and implements the 2012 Tulare County General Plan 2030 Update:

Planning Framework Element (Urban Boundaries). The Planning Framework Element is revised to update the Urban Development Boundary Part 1, of the Tulare County General Plan. The intent is that the County’s UDB is coterminous, as administratively feasible, with the Sphere of Influence (SOI) adopted by Tulare County Local Agency Formation Commission.

Open Space Element. The Environmental Resources Management Element is amended to revise the "Urban Expansion Area" designation on the Open Space Map Part 1, Figure 8-1 of the Tulare County General Plan to reflect the area within the revised Urban Development Boundary of Cutler-Orosi.

Land Use, Transportation and Circulation Elements Part 1. This Plan Amendment incorporates the following: The County’s General Plan land use designations, circulation functional classification, and development policies into the 2020 Cutler-Orosi Community Plan.

Community Plan Updates Part III. The proposed Community Plan for Cutler-Orosi is updated with this proposed amendment.

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Zone Ordinance Amendments

Section 18.9: “MU” Mixed-Use Combining Zone. This amendment will establish the Mixed-Use Combining Zone(s) within the UDB of Cutler-Orosi. Currently, this overlay zone applies to 34 communities including Ivanhoe, Plainview, Woodville, Poplar/Cotton Center, Three Rivers, Goshen, Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Earlimart, Alpaugh, East Orosi, London, Richgrove, Sultana, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, Tooleville, Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem. The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicle miles traveled if residential uses are mixed with uses for employment.

Section 16 H: Additional “By-Right” Uses. This amendment will establish additional by-right uses within the UDB of Cutler-Orosi. The allowance of additional by-right uses applies to 34 communities including Lemon Cove, Ivanhoe, Plainview, Woodville, Poplar/Cotton Center, Three Rivers, Goshen, Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Earlimart, Alpaugh, East Orosi, London, Richgrove, Sultana, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, Tooleville, Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem. The purpose of adding additional by-right uses is to promote sustainability; economic development and prosperity by providing design flexibility, streamline the approval process and reduce vehicles miles traveled by locating residential uses within proximity to employment areas.

Ordinance No. 352: Zoning District Ordinance Map. This amendment to the Zoning District Ordinance Map will rezone properties within the Cutler-Orosi UDB to be consistent with the 2020 Cutler-Orosi Community Plan.

Community Plan Context

The two communities are predominantly rural, agriculturally related service centers. They not only serve as an area where agriculturally oriented enterprises, such as packing houses and cold storage facilities are located, but also as a residential community where many of the areas farm workers reside. Persons residing in smaller surrounding communities, like East Orosi, Sultana, and Yettem, travel to Cutler/Orosi for incidental shopping, school, and banking purposes. For major shopping, leisure services and medical care, persons will generally travel to Dinuba, 5 miles to the west, or Visalia, 15 miles to the south.

The objective in the preparation of the Cutler/Orosi Community Plan is to develop a plan, which can accurately reflect the needs and priorities of the unincorporated communities of Cutler/Orosi. In addition, the County has prepared an Environmental Impact Report. The Environmental Impact Report (EIR) is to assist in fostering future economic development, grants, and economic development opportunities, which can tier off the General Plan and Community Plan EIR's.

Cutler/Orosi are currently designated as Unincorporated Communities in the 2030 Tulare County General Plan (2012). Cutler/Orosi is situated about one-half mile apart along State Route 63, a rural

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highway running north/south through eastern Tulare County. It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding and to stimulate economic development within the community.

As with any community plan, the contents of this document are not intended to be absolute. Planning is a continuous process and, to be effective, requires periodic re-evaluation and revision to reflect changing needs and priorities. This Plan, therefore, should be reviewed on a periodic basis with the assistance and participation of local citizens, groups, and agencies. By doing so, it is envisioned that the Cutler/Orosi Community Plan will continue to provide meaningful and necessary guidance toward the development of the community in the foreseeable future.

California Government Code (Section 65300 et seq.) requires that each local agency, city or county, prepare and adopt comprehensive long-term general plans for the physical development of lands within its jurisdiction. A general plan must function as "a statement of development policies" and must include a diagram and text setting forth goals, policies, standards, and plan proposals. The plan must, on the minimum, include the following elements: land use, circulation, housing, conservation, noise, safety, and open space. State law also provides that a local agency may include one or more several optional elements depending on the needs and characteristics of the jurisdiction.

In Tulare County, the General Plan has historically been developed on a countywide basis or by large geographic sub-areas (such as rural valley, foothill, and mountain), with development policies, emphasizing county-wide and area-wide issues and concerns. In establishing land use planning policies on an area-wide basis, it has been recognized that several unincorporated communities, including Cutler/Orosi, have localized land use needs and issues that should be addressed in a more specific manner particular to its community, geographic features, location of major roadways (such as State Route 63), population characteristics, availability of water, and other issues unique to the community's area. Therefore, the Cutler/Orosi Community Plan 2020 Update has been prepared with an emphasis on these considerations with particular focus on land use and circulation.

Location

Cutler/Orosi are located in California's central San Joaquin Valley, in the easterly Valley floor portion of Tulare County (**see Figure 1**). The two adjacent communities lie in the midst of one of the most productive agricultural regions in the world, and are virtually surrounded by field crops, orchards, and vineyards.

Cutler/Orosi are located in northern Tulare County approximately 16 miles east of State Route (SR) 99 and approximately 15 miles north of Visalia, the county seat. Both communities are located along State Route (SR) 63 about one half mile apart. The Tulare County/Fresno County Line is located approximately 3.3 miles northwest of Cutler. The communities are situated at the base of the Sierra Nevada Mountain foothills.

Cutler is generally bounded by Avenue 402 in the south, Avenue 408 in the north, Road 120 in the west, and the Bowhay Ditch in the east and encompasses 0.8 square miles of land (**see Figure 2**). Cutler is located south of and adjacent to the community of Orosi. Cutler is an agriculturally oriented

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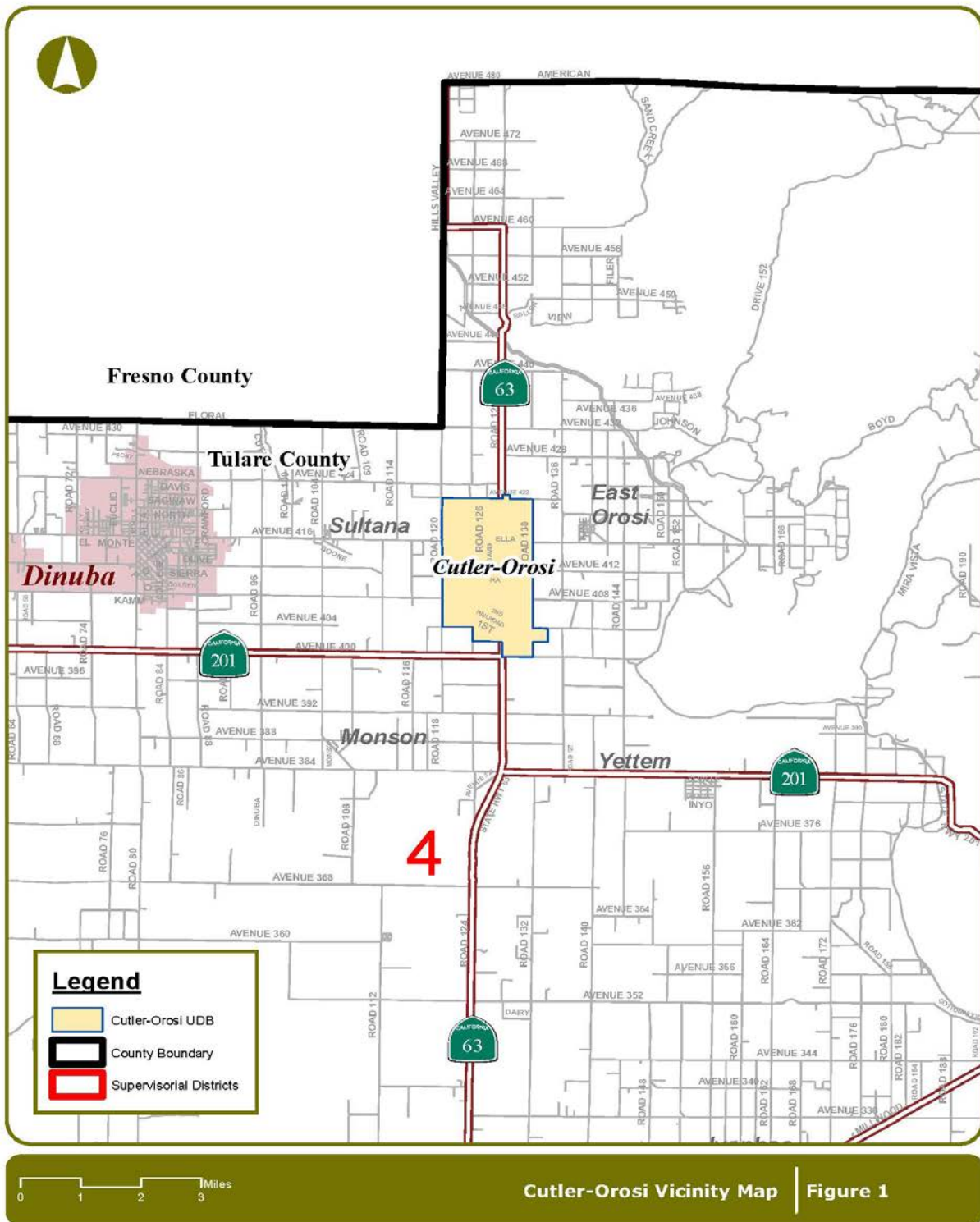
service community surrounded on the south, west and east by lands in agricultural production, vacant lands, and scattered residential homes.

Orosi is generally bounded by Avenue 408 in the south, Avenue 424 in the north, Road 120 in the west, and the Bowhay Ditch and Sand Creek in the east and encompasses 2.4 square miles of land. State Route (SR) 63 directly serves Orosi. Orosi is located north of and adjacent to the community of Cutler. Orosi is an agriculturally oriented service community surrounded on the north, west and east by lands in agricultural production, vacant lands, and scattered residential homes. The community of East Orosi is located to the northeast.

Cutler/Orosi are located in Sections 07, 08, 17, 18, 19, & 20, Township 16 South, Range 25 East; MDB&M, and can be found within the Orange Cove South Quadrant, United States Geological Survey 7.5 minute topographic quadrangle. Comparatively flat and topographically almost featureless, Cutler/Orosi lies at an elevation ranging from 375 feet above mean sea level (msl), near the northeasterly end of Orosi, to 355 feet msl at the west end of Cutler. The community is situated on a very gentle gradient to the southwest. The coordinates of Cutler/Orosi are Latitude: 36° 31' 29N" and Longitude: 119° 17' 20".

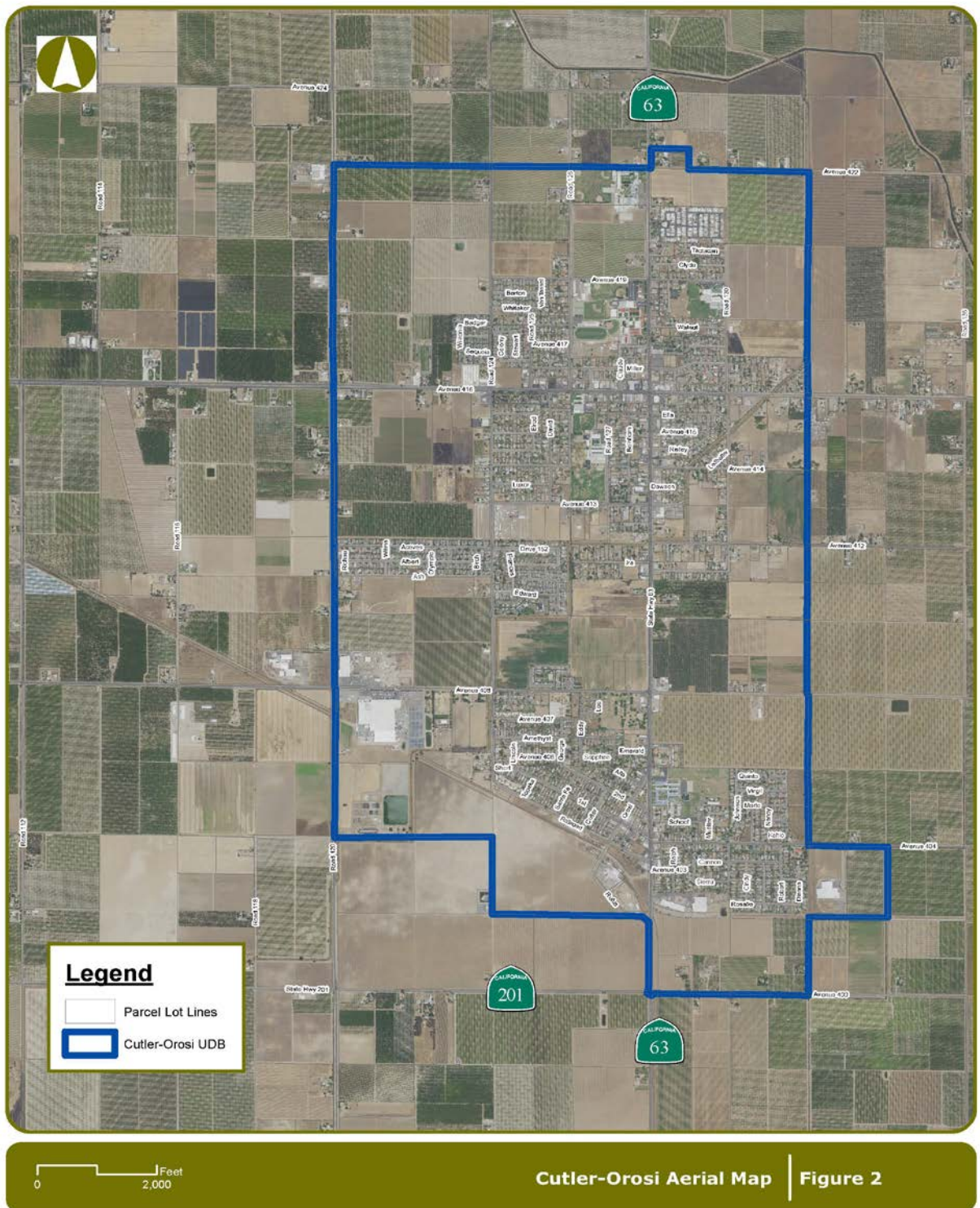
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Figure 1 - Vicinity Map



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Figure 2 - Aerial Map



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Historical Perspective

“Generally, the growth of the two communities has been well planned. Both have developed in a contiguous fashion and have remained compact through infilling. The two communities are separated from each other by distances ranging from one quarter to one half mile. One of the reasons for the open space between the two communities is that Sand Creek was previously subject to periodical floods thereby making some of this land undevelopable prior to current flood control improvements. . . Most of the land that separates Cutler/Orosi is presently used for agriculture.

Cutler/Orosi are surrounded by agricultural lands. Crops grown on these lands include field crops, deciduous fruit orchards and vineyards. Unlike many Valley communities, there is little rural residential development (1 to 5 acre homesites) surrounding either community.

Cutler is bisected north and south by SR 63. It was bounded on the south by the Atchison Topeka Santa Fe Railroad and agricultural land, on the north and east by agricultural land, and on the west by the railroad, the wastewater treatment plant and two major packinghouses. The western half of Cutler is almost fully developed, whereas the eastern half is less than 50 percent urbanized. The Atchison Topeka Santa Fe Railroad tracks that bounded Cutler to the south is now abandoned right-of-way **(see Figure 3)**.¹ The railroad tracks and cross-ties were removed. The cobble and gravel covered railbed and footprint of the former railroad are still visible. Low-lying grass and vegetation have grown in and along the railbed.

Residential development has occurred on the east side of Cutler. Development to the south and southeast has been restricted by a number of features, including the railbed footprint, the treatment plant, industry, and lands under the Williamson Act. To the north, development was historically restricted by flooding from Sand Creek prior to existing flood control improvements. Commercial development is concentrated along both sides of SR 63, while industrial uses are situated along the railbed footprint. Cutler Elementary School and two parks are also located on SR 63.

State Route 63 and Avenue 416 divide Orosi into four neighborhood quadrants. Each quadrant supports a mix of single family, mobile home and rural residential development. Almost all the multifamily development is located in the southwest quadrant, except the southeast, support a school. Residential development has occurred in the northeast and southwest quadrants. Development to the south and east has historically been restricted by flooding and irregularly shaped parcels, which are difficult to develop.

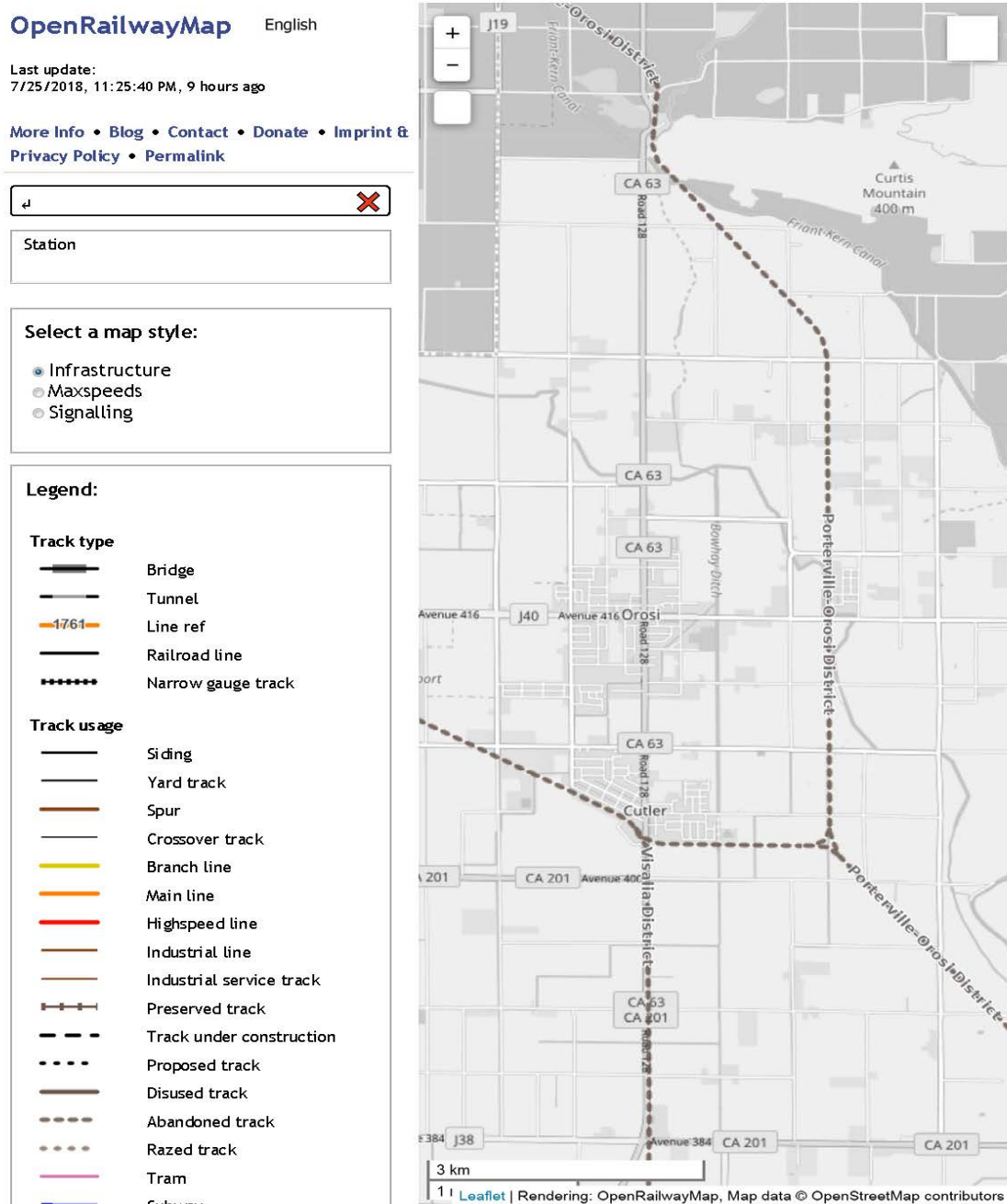
Orosi’s commercial district is concentrated along SR 63 and the west side of Avenue 416. New Commercial development has been absent in Orosi in recent years. The proximity of Dinuba and Visalia make commercial development in Orosi somewhat risky due to competition from these neighboring communities. Orosi has little industrial development, and what little there is dispersed in the southern part of the community.”²

¹ Open Railway Map <https://www.openrailwaymap.org>.

² Cutler/Orosi Community Plan, adopted August 30, 1988 by Tulare County Board of Supervisors, Resolution No. 88-1051.

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Figure 3 - Abandoned Railroad Track



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Cutler/Orosi Charrette, November 2001

“The Cutler/Orosi Community Design Charrette was conducted from November 1 – 5, 2001. Its goal was to create a conceptual plan for improvements for all roadway users along State Route 63 and the intersecting main streets of Cutler/Orosi. It was funded through a Caltrans Community-Based Transportation Planning Grant, and grew out of community visioning work initiated through a Great Valley Center Legaci grant. The design team included two transportation planners, a traffic engineer, a landscape architect, and an architect. All events were conducted in both English and Spanish.

Community participation during the charrette was exceptional. Over 130 people attended the opening event, and approximately 300 people, including County and Caltrans District 6 staff took part during the five-day event. A multi-phased plan with several low-cost implementation steps to improve safety and help stimulate investment in Cutler/Orosi was developed.

Recommendations that were recommended included a series of community-based actions that identified little cost but anticipated delivering a high impact. Recommendations that will require more time include interim safety and beautification measures such as sidewalk completion, curb extensions at critical crossings, the narrowing of State Route 63 north of Avenue 416, and street tree plantings.

In the longer term vision, commuter traffic to and from Dinuba and heavy trucks were recommended to be rerouted off SR 63 through the construction of a truck bypass on County Road 120. By moving a significant portion of through traffic off SR 63, it allows for significant enhancements and traffic calming which are appropriate for a pedestrian-friendly town center that the community envisions on the vacant land between the two towns. A median, on street parking, bicycle lanes, and the removal of two traffic lanes were recommended for nearly the entire corridor in this later phase.

This report is one of three undertaken by the County for the previously established Cutler/Orosi Redevelopment Project Area. Along with the concurrent water and sewer needs assessment study, the charrette report included recommendations of the Cutler/Orosi Commercial Development Plan.”³

³ Cutler/Orosi Charrette Report, November 2001, by Walkable Communities, Inc. and the Local Government Commission prepared for County of Tulare Redevelopment Agency and Local Government Commission. Page 5.

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Cutler/Orosi Background Report

Community Profile

Planning Area

Cutler/Orosi are two unincorporated communities located in northern Tulare County. Both communities are located along State Route 63 about one half mile apart. The population for Cutler/Orosi is 5,850 and 7,760 persons in 2017. Cutler/Orosi are surrounded by agricultural lands. The Cutler/Orosi Urban Development Boundary (UDB) area consists of approximately 2,441.9 acres (**see Figure 4**). Cutler is bisected north and south by State Route (SR) 63. SR 63 and Avenue 416 divides Orosi into four neighborhood quadrants.

Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2017, Cutler's median household income was \$31,939 and Orosi's median household income was \$35,798, whereas the State of California's median household income was \$67,169. Median household income for Cutler was 47.5% and Orosi median household income was 53.3% of the State of California's median household income, and therefore considered severely disadvantaged communities.

Climate

The southern San Joaquin Valley climate is influenced to a great extent by the Coast Ranges to the west which prevent the cool, moisture-laden maritime air from reaching the valley. It is generally characterized as a Mediterranean climate (one of three similar zones in the world). The area in general has a climate that tends to be clear, sunny, warm and dry. The mean temperatures range from a low of 34° F. (1.1° C) in January to a high of 100° F. (37.7° C) in July. Because of the Coast Ranges, the average rainfall for the area is very low, ranging from three to nine inches per year, with 90% of the yearly precipitation between November and April. There are periods in winter when the valley floor is covered with dense wet ground fogs with winds typically light and from the north.

Topography

The communities are situated on relatively level terrain with a slight fall towards the southwest. The major natural features in the area include the Sierran foothills located two miles to the east, and Sand Creek, which flows between the two communities. Average elevation above sea level for Cutler is 374 and 361 feet for Orosi.

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Existing Urban Development Boundary

The existing Cutler/Orosi Urban Development Boundary (UDB) area (**see Figure 4**) consists of approximately 2,441.9-acres (including rights-of-way). Within the existing Cutler/Orosi UDB, approximately 1,245.4-acres are currently zoned for urban uses and approximately 956.9 acres are zoned for agricultural uses. Cutler/Orosi are surrounded by agricultural lands, crops grown on these lands include field crops, deciduous fruit orchards, and vineyards. Unlike many Valley communities, there is little rural residential development (1 to 5 acre homesites) surrounding either community. The UDB includes areas within the Cutler Public Utility District (CPUD) and the Orosi Public Utility District (OPUD) in order to provide service area consistency between these two boundaries.

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Existing Land Use Plan

Land use patterns in Cutler/Orosi are typical of other Valley communities. Commercial development and apartments are situated on the more heavily traveled streets, industrial development is located along railroads, schools, and parks are integrated with residential districts, and the entire community is surrounded by agriculture.

The purpose of the Cutler/Orosi Community Plan is to establish land use policies to guide existing and future development to the year 2030. The general intent of these policies is to protect the health, safety, and welfare of persons living in Cutler/Orosi. In more specific terms, the policies serve to identify the most appropriate locations and arrangement of different types of land uses based upon environmental, circulation, infrastructure/services, and planning concerns.

The County of Tulare, through existing policies, has encouraged both incorporated and unincorporated communities to establish urban development and land use patterns, which are compact and contiguous. This policy position has reduced so-called “leap frog” development throughout the County, helping preserve agricultural lands, and minimize land use conflicts between urban and agricultural areas.

Residential

To provide for a variety of living environments and opportunities for affordable housing, the 1988 Cutler/Orosi Community Plan establishes three residential densities: low, medium, and high. Low density residential allows six units or less per acre; medium density allows 4 to 14 units per acre and high density allows 15 to 29 units per acre.

Low Density Residential The Low Density areas are planned to accommodate single-family homes on individual lots where urban services (i.e. community water and sewer) are provided. Properties designated low density residential generally lack adequate infrastructure to warrant higher densities, or serve as a transitional use between urban and agricultural uses. This residential designation promotes a rural environment where livestock and small farming operations are allowed.

Medium Density Residential Medium Density Residential areas are planned to accommodate single-family homes on individual lots where urban services (i.e. community water and sewer) are provided, at higher densities than the area designated for Low Density Residential Development. Medium Density Residential is applied to many areas of the residential land in Cutler/Orosi. Properties with this designation are, or will become, the single-family neighborhoods of each community. This designation is generally applied to properties that are free of excessive noise and through traffic, are in close proximity to parks and schools, are provided with off-site sewer and water, and are within the immediate service area of fire and police services.

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High Density Residential High Density Residential designation provides for residential development with a wide range of densities and housing types. High density residential is the designation reserved for multiple family units or apartments. This Plan has attempted to insure that no one quadrant of either community is overburdened with apartments. In addition, multiple family development presents many more design options that can be used to help mitigate noise situations. Therefore, this Plan recommends that most of the high-density residential development be located along arterial or collector streets, which can handle greater amounts of traffic and where noise levels are usually greater than most single-family subdivisions can tolerate.

In addition, this designation has also been applied to areas of Orosi, which contain irregular parcels in terms of size and shape. It is the strategy of this Plan that a higher level of land use may encourage property owners to privately redevelop their land. This redevelopment could lead to removal of dilapidated residential units, a better utilization of the land for residential development, and reduce the residential demands for outlying agricultural properties, thereby preserving agricultural land.

Residential Reserve

Land designated for future residential use, should remain in accordance with Policy 5.1. It should be noted that a general plan amendment is not agricultural use until it is determined that conditions warrant conversion to residential use, needed to develop land in a reserve classification.

General Commercial

Commercial development first appeared near the intersection of SR 63 and Avenue 416, and have since spread in strip fashion along these routes.

Service Commercial

Orosi contains one area approximately 12 acres of service commercial, located south of Avenue 416. Cutler contains two areas (approximately 68 acres and approximately 11 acres of Service Commercial) along the railbed footprint.

Professional Office

Professional Office contains approximately 16.6 acres. In Orosi, Land Use Designation Professional Office is located along Avenue 416 and SR 63. Family Healthcare Network is located in Cutler on Avenue 408.

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Industrial

Currently, industry in the Cutler/Orosi area is concentrated along the railbed. Included in this area are packinghouses, cold storage facilities, a box manufacturing plant, and an agricultural chemical company. Orosi has a five (5) acre parcel south of Avenue 416. Cutler is along the railbed and on east side of SR 63

Industrial Reserve

Land within the Plan Area which is recognized as suitable for industrial uses or agriculturally-related industries and is designated for eventual conversion to commercial use, but which is expected to be left in exclusive agricultural zoning until it is determined that conditions warrant conversion to industrial use, in accordance with Policy 5.1.

Agriculture

Agriculture is the foundation of Tulare County's economy. For this reason, it is important that agricultural lands be preserved and that agricultural operations remain free of adjacent incompatible land uses, which may hamper the operation. The Cutler/Orosi Community Plan takes into consideration surrounding agricultural operations and their needs to be free of intruding urban uses. Where possible, the UDB follows a road, railroad, or creek so that there is some spatial distance between future urban uses and agriculture.

Park

Ledbetter Park is approximately 11 acres in size and is located one mile northwest of Cutler on Road 124/SR 63.

Adopted Land Use Plan

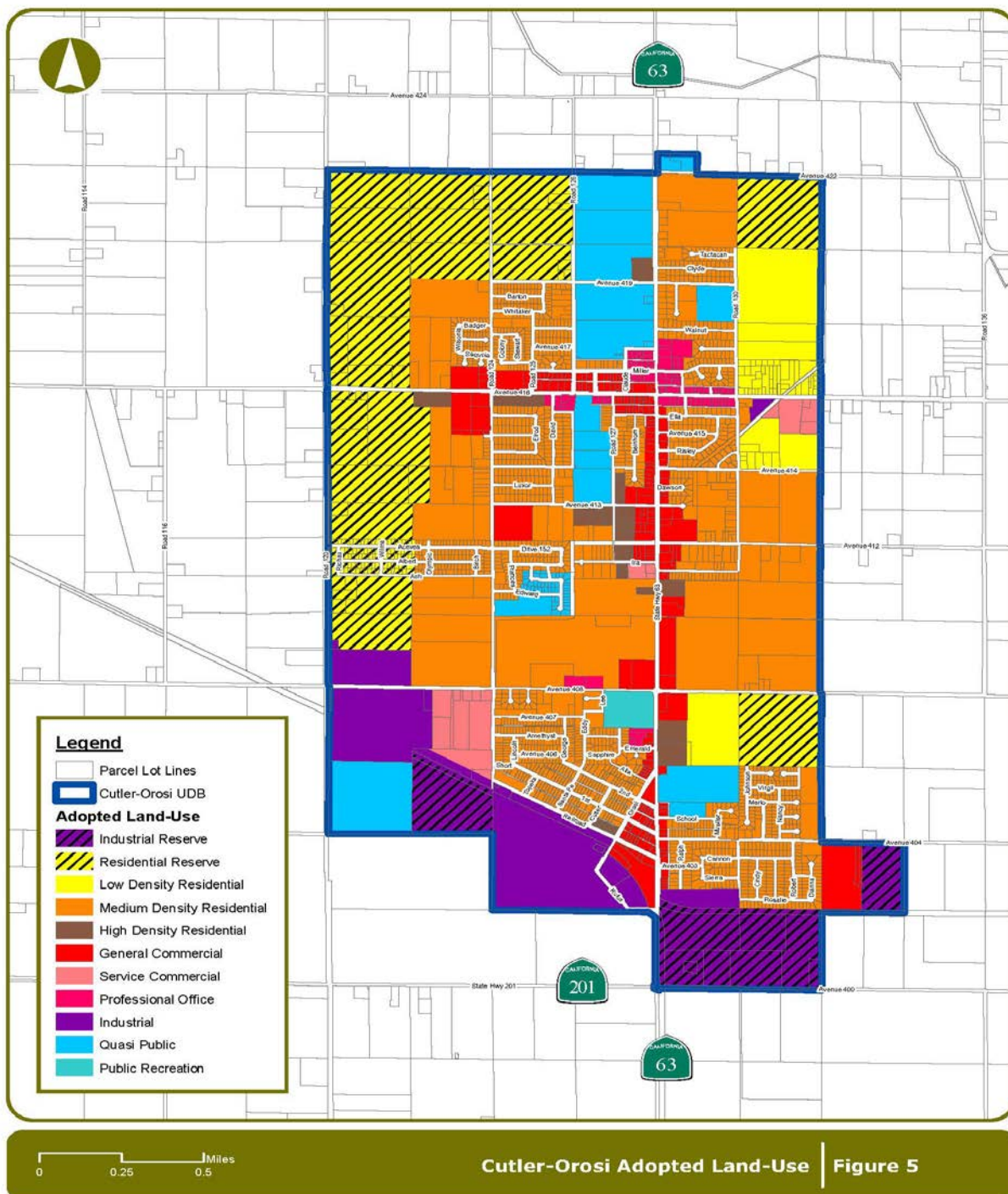
Table 1 shows that a majority of the land in the 1988 adopted Cutler/Orosi Community Plan (As Amended) area is designated Residential (1,014 acres). In total, there is about 2,412.3 acres of designated lands in the Cutler/Orosi Community Planning Area (see **Figure 5**) and approximately 231.3 acres within the plan area is dedicated to rights-of-way.

Table 1 Cutler/Orosi Adopted Land Use Plan 1988		
Designation	Total Acreage	Percentage
General Commercial	140.4	5.74
High Density Residential	38.7	1.58
Industrial	168.3	6.89
Industrial Reserve	135.0	5.53
Low Density Residential	125.1	5.12
Medium Density Residential	850.2	34.82
Professional Office	28.6	1.17
Public Recreation	11.9	0.49
Quasi-Public	201.1	8.23
Residential Reserve	459.9	18.83
Service Commercial	42.8	1.75
(blank)	8.3	0.34
Unclassified (Right-of-Way)	231.3	9.47
Total	2,441.9	100

Source: Tulare County GIS

Draft Cutler/Orosi Community Plan 2020 Update

Figure 5 – Cutler/Orosi Adopted Land Use Plan Map



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Existing Zoning Districts

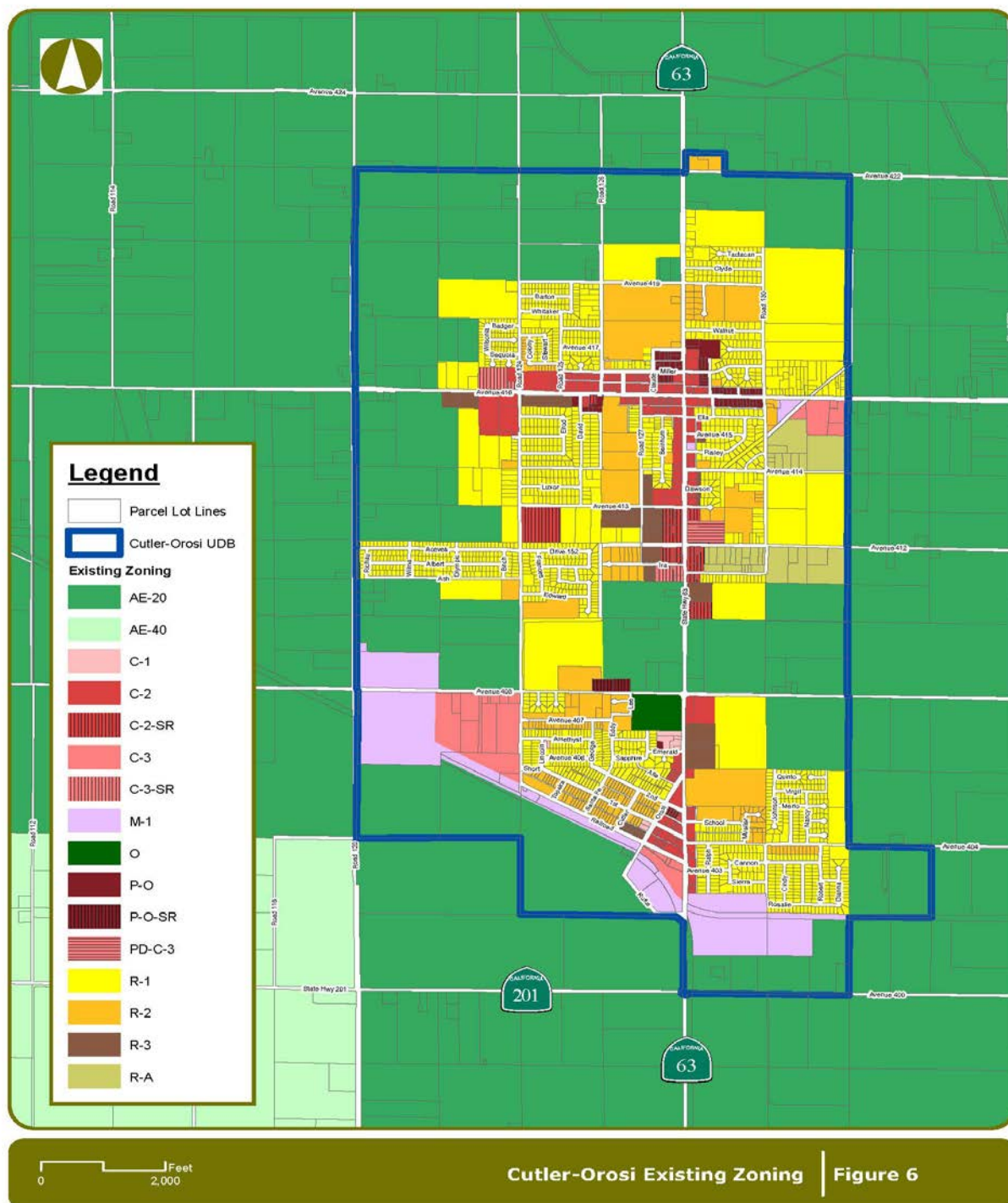
The Zoning designations within the existing 1988 Cutler/Orosi Community Plan Update are shown in **Figure 6**, as demonstrated in **Table 2**.

Table 2 Existing Zoning Districts		
Zoning Designations	Existing Acres	Percent
AE-20	956.9	39.1
C-1	3.5	0.1
C-2	69.8	2.8
C-2-SR	23.2	0.9
C-3	55.1	2.2
C-3-SR	7.5	0.3
M-1	130.8	5.3
O	11.9	0.4
PD-C-3	5.4	0.2
P-O	16.6	0.6
P-O-SR	3.1	0.1
R-1	644.3	26.3
R-2	189.1	7.7
R-3	36.3	1.4
R-A	50.3	2.0
Z	6.8	0.2
Unclassified (Right-of-Way)	231.3	9.4
Total	2,441.9	100

Source: Tulare County GIS

Draft Cutler/Orosi Community Plan 2020 Update

Figure 6 Cutler/Orosi Existing Zoning Districts



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Demographics

Introduction

An important part of planning is having information that describes the characteristics of a Community's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in the Community plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Community allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Community. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of a smaller community such as Cutler/Orosi.

Historic Population Growth

The rate of population growth over a 20-year period, 1960 - 1980, in the unincorporated County and Cutler/Orosi grew from 3239 to 7225 persons, an average annual growth rate of 6.1 persons.

Recent Population Growth

In 2000, Cutler/Orosi's population was 11,809. The population increased to 13,610 by 2017. The male population increased from 6,371 in 2000 to 7,313 in 2017. The female population increased from 5,438 in 2000 to 6,297 in 2017 (see Table 3).

Table 3 Population (2000 and 2017)						
Geography	2000			2017		
	Total Population (2000)	Male (2000)	Female (2000)	Total Population (2017)	Male (2017)	Female (2017)
California	33,871,648	16,874,892	16,996,756	39,982,847	19,366,579	19,616,268
Tulare County	368,021	184,010	184,011	458,809	229,488	229,321
Cutler CDP	4,491	2,508	1,983	5,850	3,291	2,559
Orosi CDP	7,318	3,863	3,455	7,760	4,022	3,738
Cutler Percentage	-	55.8	44.2	-	51.8	48.2
Orosi Percentage	-	52.8	47.2	-	56.3	43.7
Source: California Department of Finance						

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Projected Population

“The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County (see Table 4) is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population.”⁴

Table 4 Projected Annual Growth Rates		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate can be applied to Cutler/Orosi.

Median Age

The median age in Cutler went up from 23.5 in 2000 to 24.5 in 2017 and in Orosi from 24.6 in 2000 to 28.8 in 2017. Cutler/Orosi’s median age is lower than the median age of Tulare County and of the State of California (see Table 5).

Table 5 Median Age (2000 and 2017)		
	2000	2017
Geography	Median age (years)	Median age (years)
California	33.3	36.1
Tulare County	29.2	30.6
Cutler CDP	23.5	24.5
Orosi CDP	24.6	28.8

Source: California Department of Finance

Cutler/Orosi has a higher percentage of persons age 18 and under at 38.6% and 32.4% than Tulare County (31.4%) and the State of California (23.4%). Cutler/Orosi also has a lower elderly population. Persons 60 years old and older made up 2.0% in Cutler and 4.1% in Orosi, comparatively, persons 60 years and older in Tulare County was 13.2% and in the State of California was 16.4% (see Table 6).

Table 6 Age Percentage 2017					
Geography	Persons Under 5 years	Persons Under 18 years	Persons Age 21+	Persons Age 60+	Persons Age 65+
California	6.4%	23.4%	72.4%	16.4%	13.2%
Tulare County	8.6%	31.4%	63.9%	13.2%	10.7%
Cutler CDP	8.1%	38.6%	57.4%	2.0%	5.4%
Orosi CDP	6.4%	32.4%	63.0%	4.1%	8.0%

Source: California Department of Finance

⁴ Tulare County Regional Blueprint, page 7

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Ethnicity and Race

In 2000 (see **Table 7**), 34.5 % of Cutler's and 48.4% of Orosi's population were white, 0.4% for both Cutler/Orosi were African American, 1.2% for Cutler's and 0.5% for Orosi were Native American, 0.8% for Cutler and 10.2% for Orosi were Asian, and 4.5% for Cutler and 5.5% were Orosi were two races or more. Approximately 92.1% for Cutler and 77.6% for Orosi were Hispanic (of any race).

Table 7 Race and Ethnicity (2000)							
	Total Populatio n	White	Hispanic or Latino (of any race)	Black or African American	American Indian and Alaska Native	Asian	Total Population of Two or More Races
California	33,871,648	20,170,059	10,966,556	2,263,882	333,346	3,697,513	1,607,646
Tulare County	368,021	213,751	186,846	5,852	5,737	12,018	16,938
Cutler CDP	4,491	1,547	4,136	17	53	37	204
Orosi CDP	7,318	2,153	5,679	26	39	747	406
Cutler % Total;	-	34.5	92.1	0.4	1.2	0.8	4.5
Orosi % of Total	-	48.4	77.6	0.4	0.5	10.2	5.5

Source: California Department of Finance

In 2017 (see **Table 8**), 1.6% of Cutler's and 3.3% of Orosi's population were white. Approximately 98.8% of Cutler and 85.58% of Orosi were Hispanic (of any race). Between 2000 and 2017, the proportion of the White population declined in both Cutler/Orosi from 34.5% to 1.6% and in Cutler and from 48.4% to 3.3% in Orosi. During this time, the African American population moved away from both Cutler and Orosi. The Asian population percentage increased in Orosi from 0.8% to 10.6% and declined in Cutler from 0.8% to 0.0%. The two or more race demographic declined in both Cutler and Orosi from 4.5% to 0.0% in Cutler and 5.5% to 0.6% in Orosi. The Hispanic (of any race) increased from 92.1% to 98.8% in Cutler and increased from 77.6% to 85.5% in Orosi.

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Table 8 Race and Ethnicity (2017)

	Total Population	White	Hispanic or Latino (of any race)	Black or African American	American Indian and Alaska Native	Asian	Total Population of Two or More Races
California	38,982,847	14,777,594	15,105,806	2,161,459	117,813	5,427,928	1,140,164
Tulare County	458,809	135,372	291,867	5,973	3,029	14,622	6,709
Cutler CDP	5,850	94	5,756	0	0	0	0
Orosi CDP	7,760	255	6,632	0	0	826	47
Cutler % of Total	-	1.6%	98.8%	0.0%	0.0%	0.0%	0.0%
Orosi % of Total	-	3.3%	85.5%	0.0%	0.0%	10.6%	0.6%

Source: California Department of Finance

Economic Conditions

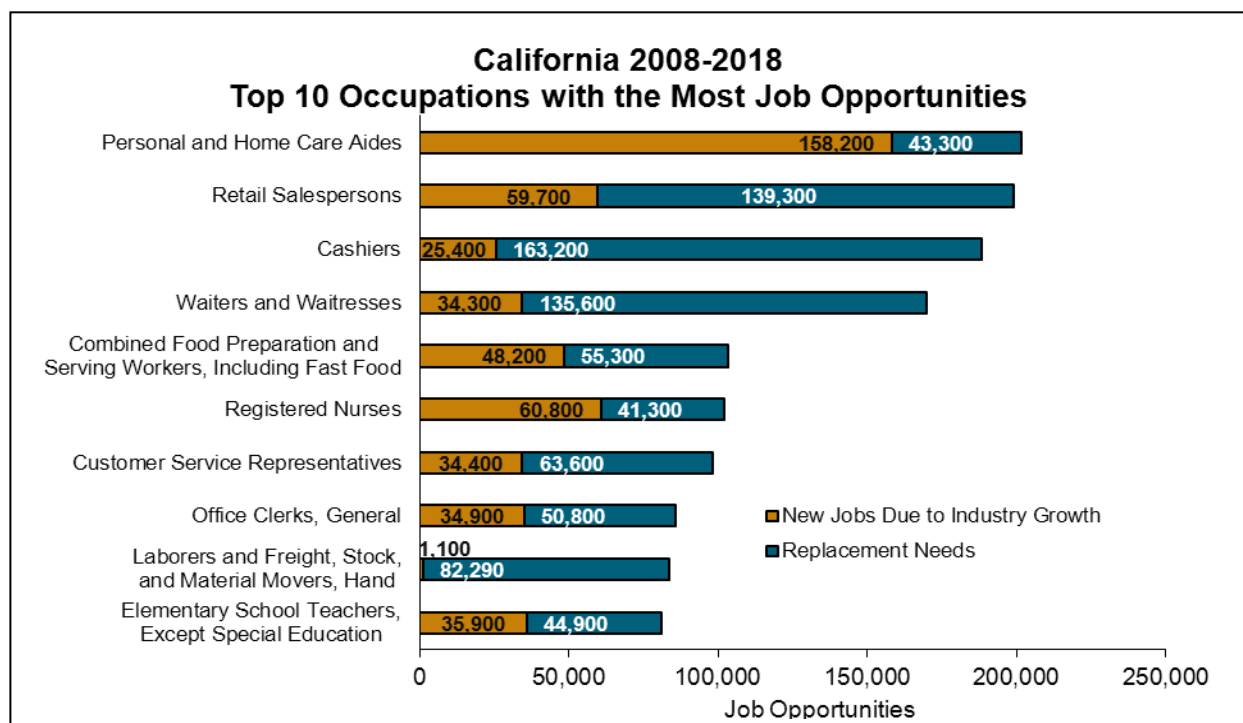
Employment Projections California

“By the end of the 2008-2018 projection period, total nonfarm employment in California is projected to grow to nearly 16.5 million jobs. This exceeds peak job level of just over 15.2 million jobs reached before the Great Recession by over 1.2 million jobs. From June 2007 to June 2009, 1.1 million jobs were lost (not seasonally adjusted). Over the 2008-to-2018 projections period, nonfarm employment is expected to rebound by 1,511,100 jobs as the economy recovers from these recessionary job losses. More than 50 percent of all projected nonfarm job growth is in education services (private), health care, social assistance, and professional and business services. The largest number of new jobs is expected in education services, health care, and social assistance, with a gain of more than 421,000 jobs.

Factors fueling the economic recovery in California include the state’s population growth and a rise in foreign imports and exports... The state’s population increased by more than 3.3 million from 2000 to 2010 and the California Department of Finance projects the population will increase by another 4.3 million from 2010 to 2020. A steady increase in foreign imports and exports has strengthened the wholesale, retail, and transportation industry sectors.”⁵

⁵ California Labor and Market and Economic Analysis, 2012, page 27

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Source: California Employment Development Dept., California Labor and Market and Economic Analysis,

Tulare County's Local Economy

Similar to the broader Central Valley area, Tulare County's economy has been largely based on agriculture, food processing, and manufacturing, while professional services jobs have been limited. Tulare is the second most productive agricultural county in a State that itself is by far the most productive in the nation. Overall, agribusinesses produced \$6 billion in commodities in 2016 with the County considered one of the largest milk producers in the United States.”⁶

Tulare County is also a major distribution hub because of its central location in the State, 200 miles north of Los Angeles and 225 miles south of San Francisco. The County's employment base has been significantly impacted by the recent downturn with unemployment increasing to 18.3 percent in January 2010, significantly above the historic range of between 8.5 and 18.2 since 1990. In 2008, the median household income was approximately \$44,000.

The county's major employers are Tulare County government, Porterville Development Center, both (Kaweah Delta Healthcare, and Ruiz Food Products). The top 20 employers combine for about 19,300 jobs, or 11 percent of the overall county employment. The major distributors include Jo-Ann Fabrics, VF Distribution, Wal-Mart, and Best Buy Electronics that combine for nearly 3.5 million

⁶ 2017 Tulare County annual Crop and Livestock Report

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square feet of distribution space. The county's overall industrial market includes about 23 million square feet of building space.”⁷

Unemployment in Tulare County

According to the 2030 Update of the Tulare County General Plan, Tulare County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Nearly 20% of the employment in Cutler/Orosi is agriculturally related according to the Tulare County Housing Element. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production.

According to the California Department of Finance, the 2013-2017 American Community Survey (see **Table 9**) indicated that the unemployment rate in Cutler was 13.1% and Orosi had an unemployment rate of 20.0% while Tulare County's unemployment rate was 10.0%. The State of California's unemployment rate was 7.7%.

Table 9 American Community Survey: Unemployment 2013-2017

Geography	Population	Total Civilian Labor Force	Unemployment
California	38,982,847	19,485,061	7.7%
Tulare County	458,809	193,225	10.0%
Cutler CDP	5,850	2,208	13.1%
Orosi CDP	7,760	3,334	20.0%

Source: California Department of Finance

Income

Mean and Median income(see **Table 10**) in Cutler/Orosi is very low compared to Tulare County and the State of California. Average median household income for Cutler was \$31,939 and Orosi was \$35,798 compared to \$44,871 for Tulare County and \$67,169 for the State of California.

Table 10 - 2013-2017 American Community Survey: Income

Geography	Median household income (dollars)	Mean household income (dollars)	Median family income (dollars)	Mean family income (dollars)	Per capita income (dollars)
California	\$67,169	\$96,104	\$76,975	\$106,970	\$33,128
Tulare County	\$44,871	\$62,325	\$47,280	\$65,927	\$18,927
Cutler CDP	\$31,939	\$36,990	\$30,760	\$32,501	\$8,436
Orosi CDP	\$35,798	\$46,444	\$41,379	\$40,839	\$12,163

Source: California Department of Finance

⁷ Visalia General Plan Update: Existing Conditions Report, page 3-16

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The mean family income for Cutler was \$32,501 and Orosi was \$40,839 compared to \$65,927 for Tulare County and \$106,970 for the State of California. Average per capita income for Cutler was also low at \$8,436 and Orosi was \$12,163 compared to \$18,927 for Tulare County and \$33,128 for the State of California.

Poverty

According to the California Department of Finance, the 2013-2017 American Community Survey (see Table 11) indicated that 39.7% of all families living in Cutler lived below the poverty line and 24.3% of all families in Orosi lived below the poverty line. For all people Cutler (47.5%) and Orosi

Table 11 - 2013-2017 American Community Survey: Poverty					
Geography	All families	Married couple families	Families with female householder, no husband present	All people	Persons under 18 years
California	11.1%	6.6%	26.0%	15.1%	20.8%
Tulare County	23.0%	15.4%	42.2%	27.1%	36.2%
Cutler CDP	39.7%	27.7%	52.0%	47.5%	61.6%
Orosi CDP	24.3%	21.3%	29.9%	25.7%	46.0%

Source: California Department of Finance

(25.7%) had a higher level of poverty compared to Tulare County at 27.1% and the State of California at 15.1%. The highest differential was the poverty rate of persons under 18 years. Poverty rate for persons under 18 years for Cutler was 61.6% and Orosi was 46.0% compared to 36.2% for Tulare County and 20.8% for the State of California.

Housing Characteristics

Housing Units

During the decade between 2000 and 2017 (see Table 12), the number of housing units in Cutler increased from 973 to 1,293 and in Orosi increased from 1,741 to 2,076, which represents an increase of 32.9% for Cutler and 19.2% for Orosi.

Table 12 - Housing Units (2000 & 2017)			
Geography	2010	2017	Percent Increase
	Total housing units	Total housing units	
California	13,680,081	13,996,299	2.3%
Tulare County	141,696	146,712	3.5%
Cutler CDP	973	1,293	32.9%
Orosi CDP	1,741	2,076	19.2%

Source: California Department of Finance

Housing Types

According to the California Department of Finance, the 2013-2017 American Community Survey (see Table 13) indicated that 69.6% of the Cutler housing units and 80.0 of Orosi were 1-unit detached. In Tulare County 45.4% of the housing units were 1-unit detached, and in California 0.6% of housing units were 1-unit detached.

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Table 13 - 2013-2017 American Community Survey: Unit Types				
	California	Tulare County	Cutler CDP	Orosi CDP
Total housing units	13,996,299	146,712	1,293	2,076
1-unit detached	8,131,716	110,555	900	1,661
%	0.6%	45.4%	69.6%	80.0%
1-unit attached	978,110	3,866	26	28
%	1.5%	2.6%	2.0%	1.3%
2 units	343,548	4,084	153	106
%	11.5%	2.8%	11.8%	5.1%
3 or 4 units	775,541	8,342	146	68
%	10.9%	5.7%	11.3%	3.3%
5 to 9 units	857,711	4,084	47	58
%	6.1%	2.8%	3.6%	2.8%
10 to 19 units	728,840	1,667	21	55
%	5.2%	1.1%	1.6%	2.6%
20 or more units	1,647,167	4,027	0	15
%	11.8%	2.7%	0.0%	0.7%
Mobile home	518,818	9,931	0	85
%	3.7%	6.8%	0.0%	4.1%

During the decade between 2010 and 2017 (see Table 14), the home ownership percentage in California went down by approximately 2.67%. In Tulare County, that percentage went down by approximately 4.74%. In Cutler, the homeownership percentage went down by approximately 8.0% and Orosi went up to 15.6%. While the average household size went up in the State of California and Orosi, Tulare County and Cutler's average household size went up.

Table 14 - Ownership and Household Size (2010 & 2017)						
Geography	2010			2017		
	Percent Ownership	Average household size of owner-occupied units	Average household size of renter-occupied units	Percent Ownership	Average household size of owner-occupied units	Average household size of renter-occupied units
California	56%	2.95	2.83	54.5%	3.00	2.91
Tulare County	59%	3.24	3.52	56.2%	3.27	3.46
Cutler CDP	43.7%	4.51	4.69	40.2%	3.68	5.34
Orosi CDP	56.3%	4.41	4.43	65.1%	3.91	3.82

Source: California Department of Finance

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Housing Conditions

According to the 2015 Cutler Community Housing Condition Survey, approximately 15% of the housing stock in Cutler (**see Table 15**) was sound and 75% of the housing stock were deteriorated and in need of replacing. Conversely, 87% of Orosi's housing stock (**see Table 16**) is sound, and only 15% is deteriorated or dilapidated.

Table 15 - Housing Conditions Survey (Cutler)											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Cutler	43	15	35	12	162	57	18	6	24	9	282

Source: Tulare County 2015 Housing Condition Survey, Tulare County 2015 Housing Element

Table 16 - Housing Conditions Survey (Orosi)											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Orosi	482	87	17	3	14	3	9	2	31	6	553

Source: Tulare County 2015 Housing Condition Survey, Tulare County 2015 Housing Element

The percentage of substandard housing in Cutler/Orosi increased between 1992 and 2015. The percentage was 30% in 1992, 14% in 2003, 17% in 2009, and 76% in 2015 (**see Table 17**).

Table 17 - Percentages of Substandard Housing Units in Tulare County Unincorporated Community 1992-2015				
	1992 Survey Results	2003 Survey Results	2009 Survey Results	2015 Survey Results
Cutler/Orosi	30	14	17	76

Source: 1992, 2003, 2009, 2015 Tulare County Housing Survey of Unincorporated Communities, 2015 Housing Element

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Age of Structures

According to the US Census, the 2013-2017 Community Survey (see **Tables 18 and 19**) noted that 24.9% of the housing structures in Cutler were built between 1980 and 1989 and in Orosi 26.4% of housing structures were built between 1960 and 1969.

Table 19 - 2013-2017 American Community Survey: Age of Structures in Orosi		
Age of Structures	Number	Percentage
Built 2014 or later	0	0.0%
Built 2010 to 2013	0	0.0%
Built 2000 to 2009	277	13.3%
Built 1990 to 1999	402	19.4%
Built 1980 to 1989	95	4.6%
Built 1970 to 1979	342	16.5%
Built 1960 to 1969	548	26.4%
Built 1950 to 1959	172	8.3%
Built 1940 to 1949	59	2.8%
Built 1939 or earlier	181	8.7%
Total:	2,076	-

Source: US Census

Table 18 - 2013-2017 American Community Survey: Age of Structures in Cutler		
Age of Structures	Number	Percentage
Built 2014 or later	0	0.0%
Built 2010 to 2013	25	1.9%
Built 2000 to 2009	214	16.6%
Built 1990 to 1999	121	9.4%
Built 1980 to 1989	322	24.9%
Built 1970 to 1979	244	18.9%
Built 1960 to 1969	141	10.9%
Built 1950 to 1959	74	5.7%
Built 1940 to 1949	83	6.4%
Built 1939 or earlier	69	5.3%
Total:	1,293	-

Source: US Census

Household Size (Overcrowding)

In 2017 the average owner occupied household size in Cutler was 3.68 and Orosi was 3.91 persons per household (see **Table 20**) and the average renter household size in Cutler was 5.34 and in Orosi was 3.82. By definition, the most common measure of overcrowding is persons per room in a dwelling unit.⁸ More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

⁸ U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf

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Table 20 Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.27	3.46
Cutler	3.68	5.34
Orosi	3.91	3.82

2013-2017 American Community Survey 5-Year Estimates

Vacancy Rate

In 2000, the vacancy rate (see Table 21) in Cutler was 6.6% and Orosi was 3.6%, which was lower than Tulare County at 7.7% and higher than the State of California at 5.8%. In 2010, the vacancy rate in Cutler was 4.5% and Orosi was 4.1%, which is lower than Tulare County at 8.0% and the State of

Table 21 - Vacancy Rate (2000 & 2010)						
Geography	2000			2010		
	Vacancy rate	Homeowner vacancy rate (1)	Rental vacancy rate (1)	Vacancy rate	Homeowner vacancy rate (1)	Rental vacancy rate (1)
California	5.8%	1.4%	10.7%	8.1%	2.1%	6.3%
Tulare County	7.7%	1.8%	5.8%	8.0%	2.4%	5.8%
Cutler CDP	6.6%	0.6%	2.4%	4.5%	1.0%	4.0%
Orosi CDP	3.6%	0.3%	5.5%	4.1%	2.6%	3.7%

Source: California Department of Finance

California at 8.1%. While the State of California's rental vacancy rate decreased from 10.7% to 6.3%, the rental vacancy rate in Cutler increased from 2.4% to 4.0% and Orosi decreased from 5.5% to 3.7% between 2000 and 2010. While Tulare County's rental vacancy rate remained at 5.8% during this decade.

Regional Housing Needs Assessment (RHNA)

State housing element law assigns the responsibility for preparing the Regional Housing Needs Assessment (RHNA) for the Tulare County region to the Tulare County Association of Governments (TCAG). The RHNA is updated prior to each housing element cycle. The current RHNA, adopted on June 30, 2014, is for the fifth housing element cycle and covers a 9.75-year projection period (January 1, 2014 – September 30, 2023).

The growth projections applied in the Tulare County Housing Element Update are based upon growth projections developed by the State of California. A "Regional Housing Needs Assessment Plan" provides a general measure of each local jurisdiction's responsibility in the provision of housing to

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meet those needs. The TCAG was responsible for allocating the State's projections to each local jurisdiction within Tulare County including the County unincorporated area, which is reflected in the Housing Element.

"The Sustainable Communities and Climate Protection Act of 2008 (SB 375) was passed to support the State's climate action goals...to reduce greenhouse gas (GHG) emissions through coordinated transportation and land use planning. The bill mandates each of California's Metropolitan Planning Organizations (MPO) to prepare a sustainable communities strategy as part of its regional transportation plan (RTP). The SCS contains land use, housing and transportation strategies that, if implemented, would allow the region to meet its GHG reduction targets."⁹

"In the past, the RHNA was undertaken independently from the RTP. SB 375 requires that the RHNA and RTP/SCS processes be undertaken together to better integrate housing, land use, and transportation planning. The law recognizes the importance of planning for housing and land use in creating sustainable communities where residents of all income levels have access to jobs, services, and housing using transit, or by walking and bicycling."¹⁰

"In addition to the RHNA requirements, SB 375 requires that TCAG address the region's housing needs in the SCS of the RTP, to include sections on state housing goals (Government Code Section 65080(b)(2)(B)(vi)); identify areas within the region sufficient to house all the population of the region (including all economic segments of the population) over the course of the planning period for the RTP (out to 2040 for the 2040 RTP/SCS); and identify areas within the region sufficient to meet the regional housing needs."¹¹

The RHNA housing results are summarized in **Figure 7**. The Tulare County RHNA Plan recommends that the County provide land use and zoning for approximately 7,081 units per year in the unincorporated portions of the County. The County administratively agreed to a housing share of 7,081 units (726 units per year over the 9.75-year RHNA planning period). The RTP allocates 30% of population to the County. The RHNA bases the housing needs assessment on this percentage.

⁹ TCAG, Final Regional Housing Needs Plan for Tulare County 2014-2023, Page 5, <http://www.tularecog.org/wp-content/uploads/2015/07/Final-Regional-Housing-Needs-Plan-for-Tulare-County-2014-2023.pdf>

¹⁰ Ibid.

¹¹ Op. Cit.

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Figure 7 - RHNA 2014-2023

Tulare County Region January 1, 2014 to September 30, 2023								
Jurisdiction	1/1/2014 Housing Unit Control Totals	2023 Housing Unit Control Totals	Estimated 9/30/2023 Housing Units		2024 Housing Unit Control Totals	Housing Units 9/30/2023 (Based on Allocation of 26,910 units)		
			Housing Units	Percent of Total		Total Housing Units	Percent of Total	Net New Housing Units 1/1/2014- 9/30/2023
	A	B	C	D	E	F	G	H
Dinuba	6,223	7,106	7,186	4.05%	7,212	7,188	4.05%	965
Exeter	3,803	4,305	4,426	2.50%	4,365	4,428	2.50%	625
Farmersville	2,878	3,253	3,343	1.89%	3,298	3,344	1.89%	466
Lindsay	3,384	3,858	3,972	2.24%	3,914	3,974	2.24%	590
Porterville	17,764	20,331	20,952	11.82%	20,639	20,960	11.82%	3,196
Tulare	20,022	22,908	23,606	13.32%	23,255	23,616	13.32%	3,594
Visalia	47,380	55,411	57,379	32.37%	56,386	57,401	32.37%	10,021
Woodlake	2,187	2,486	2,558	1.44%	2,521	2,559	1.44%	372
Unincorporated County	46,774	52,477	53,834	30.37%	53,151	53,855	30.37%	7,081
Total	150,415	172,134	177,255	100.00%	174,741	177,325	100.00%	26,910

Sources

Columns A, B, and E: TCAG 2040 Demographic Forecast (2013)

Column C: Estimated using trendline growth between Columns B and E

Column D: Column C divided by countywide total from Column C (177,225)

Column F: Proportionally scaled up from Column D to 9/30/2023 countywide total (177,325)

Column G: Column F divided by countywide total from Column F (177,325)

Column H: Column A subtracted from Column F

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Natural and Cultural Resources

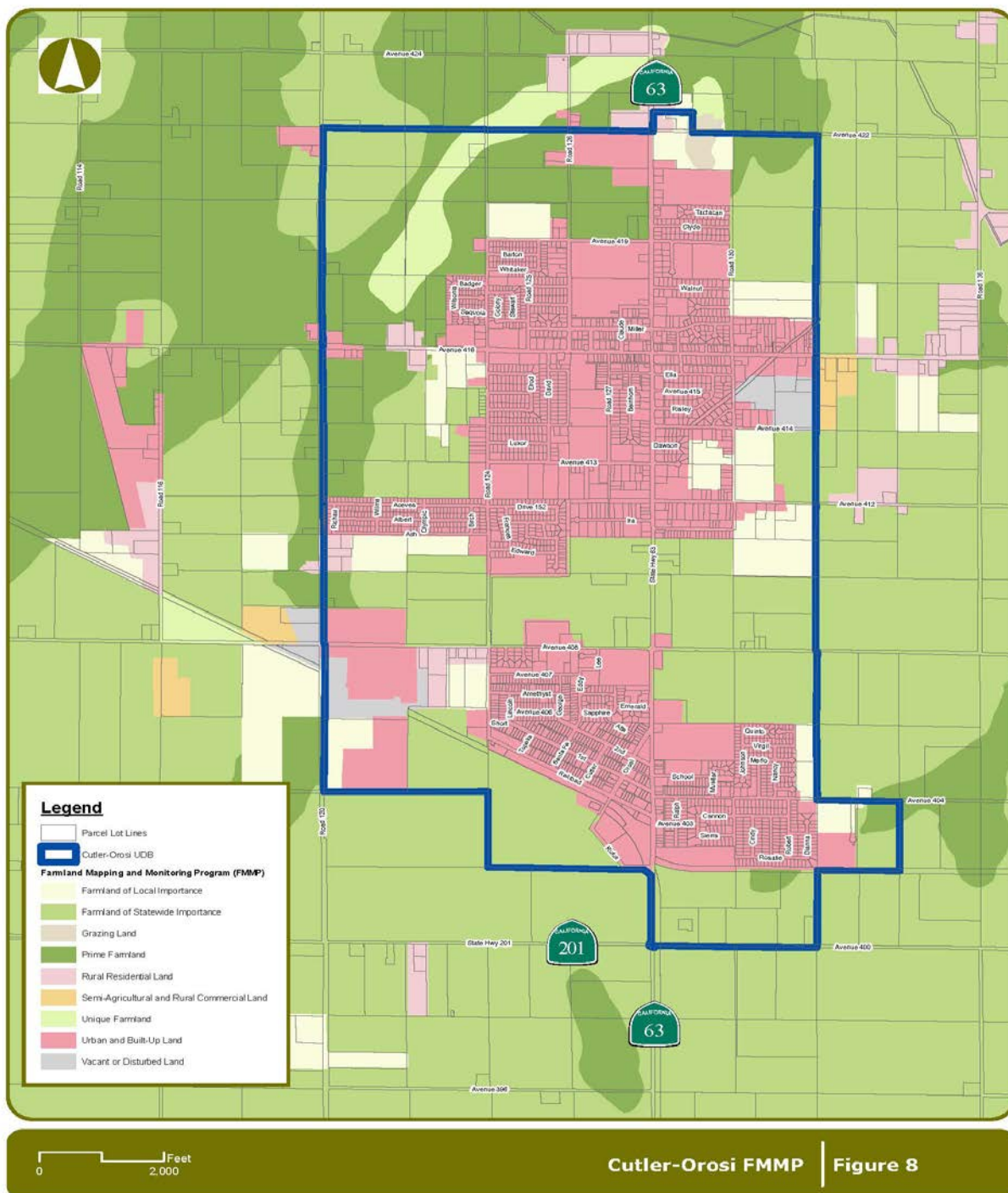
Agriculture

The California Department of Conservation, Division of Land Resource Protection, maintains the Farmland Mapping and Monitoring Program (FMMP), which monitors the conversion of the state's farmland to and from agricultural use. The program monitors a wide variety of farmland types: Prime Farmland is farmland with the best combination of physical and chemical soil features to sustain long-term agricultural production; Farmland of Statewide Importance is Prime Farmland but has minor shortcomings, such as greater slopes or less ability to store soil moisture; and Unique Farmland has lesser quality soils used for the production of the state's leading agricultural crops.

The area within the existing 2,441.9-acre UDB is designated in the 2017 FMMP maps (**see Figure 8**). Of these, approximately 1,246.9 acres are designated Urban and Built-up Land, approximately 956.9 acres are designated Prime Farmland.

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Figure 8 FMMP Map



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Williamson Act Lands

Agricultural land is a resource that must be conserved just like air and water resources. It is also economically important and provides other benefits such as wildlife habitat, groundwater recharge, and open space, which contributes to the rural character of the area.

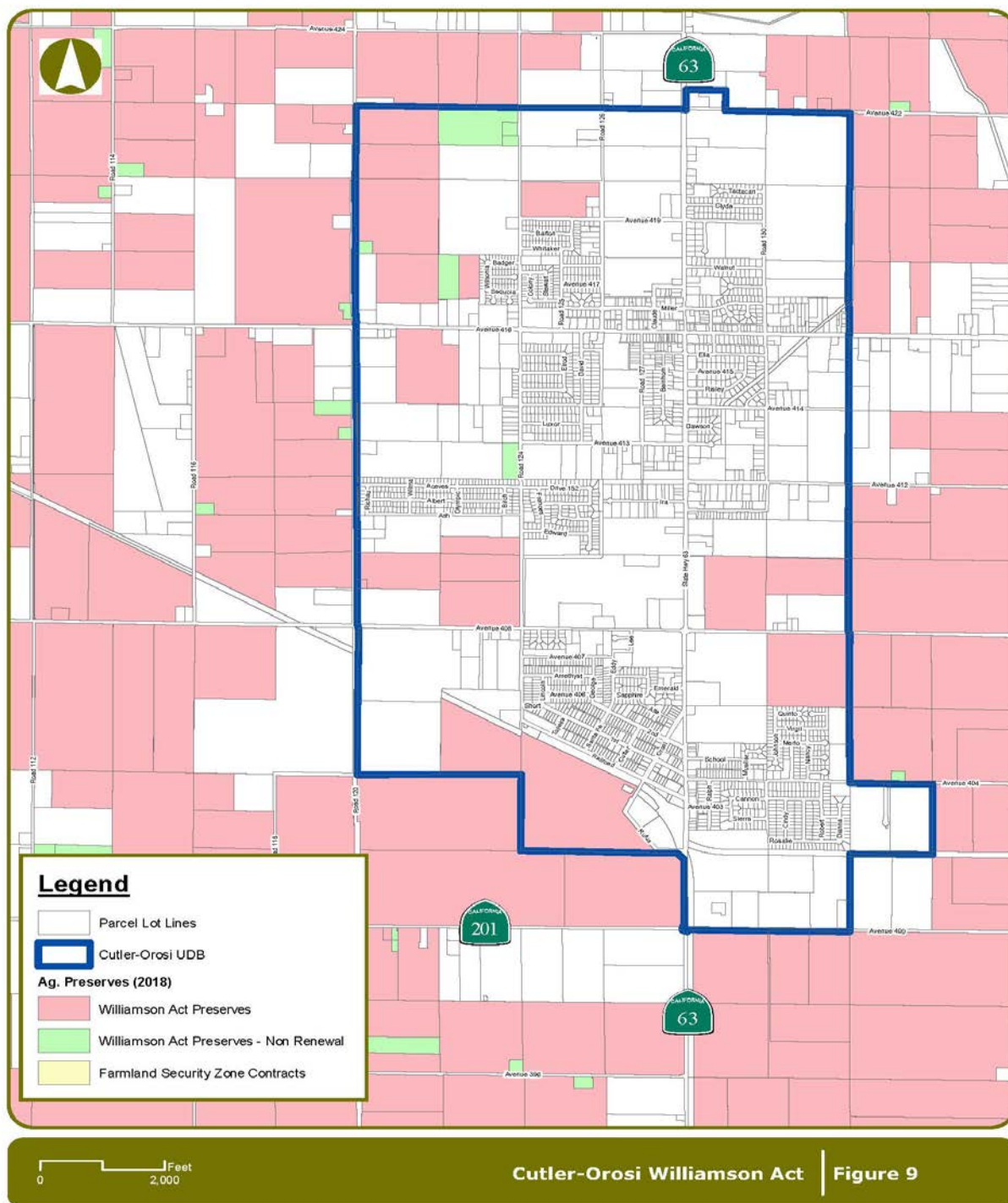
The importance of agricultural land is underscored by the level of attention state planning law has placed on it. Three mandatory elements of the general plan: 1) land use; 2) open space; and 3) conservation, all require local governments to include a discussion of agricultural lands in their general plans. The County's planning policies also underscore agricultural land importance to the local economy and environment as well.

Within the area, there are several farms in the Williamson Act. The Williamson Act is designed to keep productive farmland of a contract, that the land will not be converted to an urban type use. Land in the Williamson Act is required to remain in agriculture for a 10-year period. Unless a notice of non-renewal is filed, the contract is automatically self-renewing every year for an additional 10-year period.

Fifteen (15) parcels in Cutler/Orosi have entered into land conservation contracts and **(see Figure 9)** are subject to the Williamson Act and within the planning area (proposed UDB)

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Figure 9 - Williamson Act Map



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Air Quality

The Cutler/Orosi Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State O₃ 1-hour standard, non-attainment for the State O₃ 8-hour standard, non-attainment for the State PM₁₀ standard, non-attainment for the federal and State PM_{2.5} standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NO_x emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

The Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies
- Adopt Local Air Quality Mitigation Fee Program
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps
- Adopt ordinance limiting wood burning appliances/fireplace installations
- Fugitive dust regulation enforcement coordinated with SJVUAPCD
- Energy efficiency incentive programs
- Local alternative fuels programs
- Coordinate location of land uses to separate odor generators and sensitive receptors

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Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Cutler/Orosi is located near the southern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

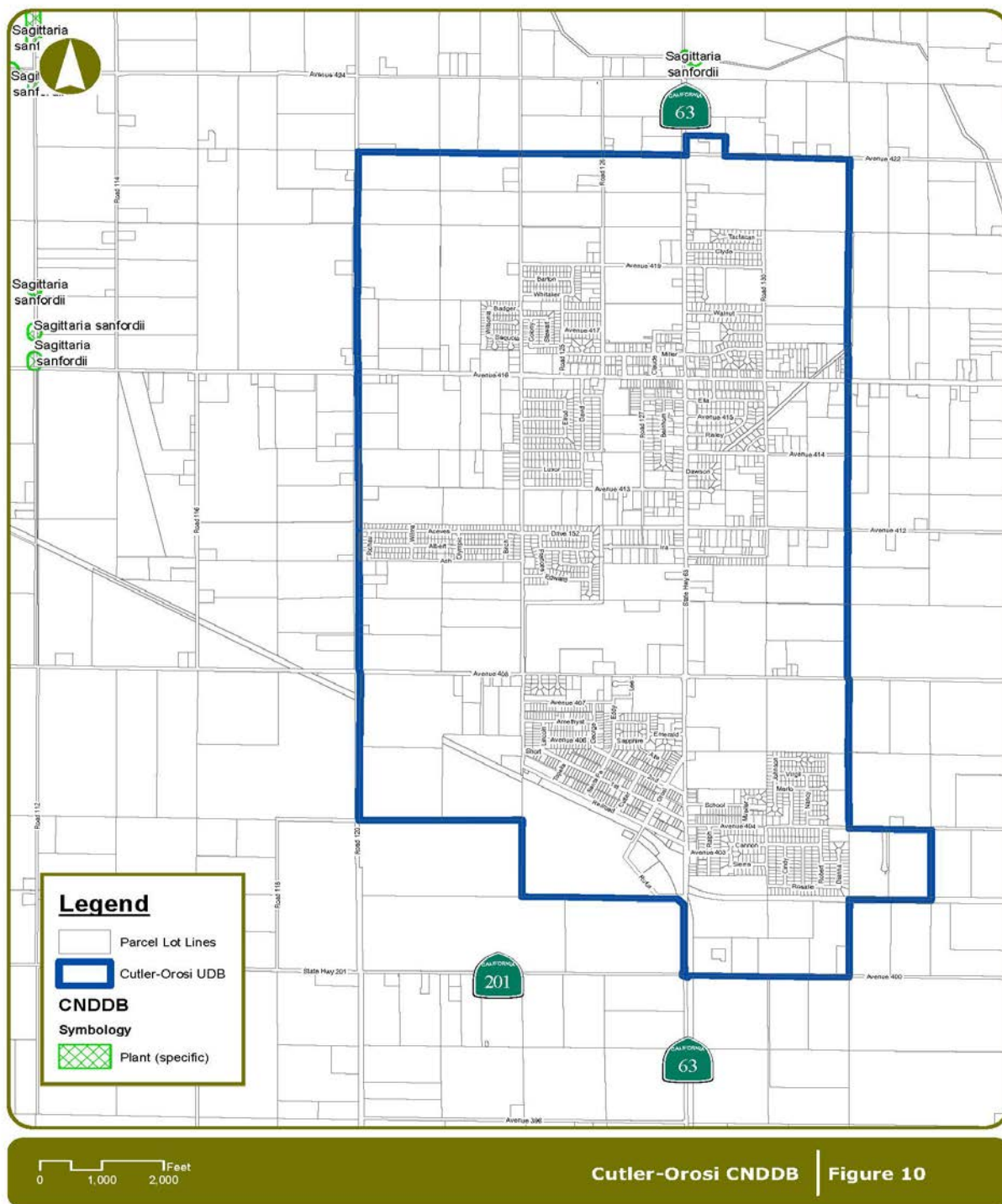
Local air pollution sources within the general vicinity of Cutler/Orosi and within the community itself include SR 63, approaching and departing jet aircraft, and industrial firms emitting dust and odors, and agricultural activities. Dust and odors are concerns of residents within the area, particularly from nearby agricultural operations.

Biological Resources

Cutler/Orosi is situated within a matrix of agricultural lands, industrial complexes, and residential/commercial development. A California Natural Diversity Database (CNDDDB) search conducted on July 25, 2018, (**see Figure 10**) indicated there are special status species within the Orange Cove South Quadrant Species List (which includes the Cutler/Orosi Planning Area) consisting of three animal species and one plant species: California tiger salamander (*Ambystoma californiense*, Federal and State threatened); vernal pool fairy shrimp (*Branchinecta lynchi*, Federal Threatened); and vernal pool tadpole shrimp (*Lepidurus packardii* Federally endangered); and San Joaquin Valley adobe sunburst (*Pseudobahia peirsonii*, Federal Threatened and State Endangered).

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Figure 10 - Cutler/Orosi CNDDB Map



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Cultural Resources

“Tulare County lies within a culturally rich province of the San Joaquin Valley. Studies of the prehistory of the area show inhabitants of the San Joaquin Valley maintained fairly dense populations situated along the banks of major waterways, wetlands, and streams. Tulare County was inhabited by aboriginal California Native American groups consisting of the Southern Valley Yokuts, Foothill Yokuts, Monache, and Tubatulabal. Of the main groups inhabiting the Tulare County area, the Southern Valley Yokuts occupied the largest territory.”¹²

“California’s coast was initially explored by Spanish (and a few Russian) military expeditions during the late 1500s. However, European settlement did not occur until the arrival into southern California of land-based expeditions originating from Spanish Mexico starting in the 1760s. Early settlement in the Tulare County area focused on ranching. In 1872, the Southern Pacific Railroad entered Tulare County, connecting the San Joaquin Valley with markets in the north and east. About the same time, valley settlers constructed a series of water conveyance systems (canals, dams, and ditches) across the valley. With ample water supplies and the assurance of rail transport for commodities such as grain, row crops, and fruit, a number of farming colonies soon appeared throughout the region.”¹³

“The colonies grew to become cities such as Tulare, Visalia, Porterville, and Hanford. Visalia, the County seat, became the service, processing, and distribution center for the growing number of farms, dairies, and cattle ranches. By 1900, Tulare County boasted a population of about 18,000. New transportation links such as SR 99 (completed during the 1950s), affordable housing, light industry, and agricultural commerce brought steady growth to the valley. The California Department of Finance estimated the 2007 Tulare County population to be 430,167.”¹⁴

On October 23, 2018, the Southern San Joaquin Valley Information Center, Bakersfield (SSJVIC) conducted a cultural resources records search at the request of RMA Planning Branch staff. According to the information provided by the SSJVIC, there have been 17 previous cultural resource study conducted within the project area and no additional studies conducted within the one-half mile radius. However, until the specific location of a development proposal occurs, the locations and nature of the resources will remain confidential and will only be shared with an applicant and remain confidential until otherwise determined by the courts.

There are two (2) recorded cultural resources within the project area. There is one recorded resource within the one-half mile resource. These resources consist of two historic era buildings and one historic era canal. The Orosi Branch Library has been given a National Register status code of 1S, indicating the individual property has been listed in the National Register of Historic Places by the Keeper. It is also listed in the California Register of Historical Resources. There are no recorded cultural resources within the project area or radius that are listed in the National Register of Historic Places, the California Register of Historical Resources, the California Points of Historical Interest, California Inventory of Historic Resources, or the California State Historic Landmarks.

¹² Tulare County 2030 General Plan. Page 8-5.

¹³ Ibid. Page 8-5.

¹⁴ Op. Cit. Page 8-6.

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Orosi Branch Library located at El Monte Way/Avenue 416 across from Eddy Road, just west of SR 63, was built in 1921 with a \$3,000 grant from the Carnegie Foundation; while the foundation had issued the grant in 1917, construction was held up for four years by World War I. The wood frame library is a California bungalow, a plain style. The Orosi Branch Library was added to the National Register of Historic Places on August 25, 1983.



Native American Consultation

The Native American Heritage Commission (NAHC) was contacted on October 8, 2018 with a request that they conduct a sacred lands files (SLF) search. The NAHC provided the results of its SLF search dated October 18, 2018 indicating “negative results” (that is, no sacred lands are known to be located in the Lemon Cove Planning area). The following Native American tribes were contacted on October 24, 2018 in order to solicit their interest regarding tribal consultation: Kern Valley Indian Community, Kern Valley Indian Community, Santa Rosa Rancheria Tachi Yokut Tribe, Tubatulabals of Kern Valley, Tule River Indian Tribe, and Wukusache Indian Tribe/Eshom Valley Band.

Geology & Seismic Hazards

The southern San Joaquin Valley is a broad arid plain, essentially level underlain by about 28,000 feet of marine and continental strata with the sediments derived from areas now occupied by mountain ranges. “Seismicity varies greatly between the two major geologic provinces represented in Tulare County. The Central Valley is an area of relatively low tectonic activity bordered by mountain ranges on either side. The Sierra Nevada Mountains, partially located within Tulare County, are the result of movement of tectonic plates, which resulted in the creation of the mountain range. The Coast Range on the west side of the Central Valley is also a result of these forces, and the continued uplifting of Pacific and North American tectonic plates continues to elevate these ranges. The remaining seismic hazards in Tulare County generally result from movement along faults associated with the creation of these ranges.”¹⁵

“Groundshaking is the primary seismic hazard in Tulare County because of the county’s seismic setting and its record of historical activity. Thus, emphasis focuses on the analysis of expected levels of groundshaking, which is directly related to the magnitude of a quake and the distance from a quake’s epicenter. Magnitude is a measure of the amount of energy released in an earthquake, with higher

¹⁵ General Plan Background Report, page 8-5

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magnitudes causing increased groundshaking over longer periods of time, thereby affecting a larger area. Groundshaking intensity, which is often a more useful measure of earthquake effects than magnitude, is a qualitative measure of the effects felt by population. The valley portion of Tulare County is located on alluvial deposits, which tend to experience greater groundshaking intensities than areas located on hard rock. Therefore, structures located in the valley will tend to suffer greater damage from groundshaking than those located in the foothill and mountain areas. However, existing alluvium valleys and weathered or decomposed zones are scattered throughout the mountainous portions of the county, which could also experience stronger intensities than the surrounding solid rock areas. The geologic characteristics of an area can therefore be a greater hazard than its distance to the epicenter of the quake.”¹⁶

“There are three faults within the region that have been, and will be, principal sources of potential seismic activity within Tulare County. These faults are described below:

- **San Andreas Fault.** The San Andreas Fault is located approximately 40 miles west of the Tulare County boundary. This fault has a long history of activity, and is thus the primary focus in determining seismic activity within the county. Seismic activity along the fault varies along its span from the Gulf of California to Cape Mendocino. Just west to Tulare County lies the “Central California Active Area,” where many earthquakes have originated.
- **Owens Valley Fault Group.** The Owens Valley Fault Group is a complex system containing both active and potentially active faults, located on the eastern base of the Sierra Nevada Mountains. The Group is located within Tulare and Inyo Counties and has historically been the source of seismic activity within Tulare County.
- **Clovis Fault.** The Clovis Fault is considered to be active within the Quaternary Period (within the past two million years), although there is no historic evidence of its activity, and is therefore classified as “potentially active.” This fault lies approximately six miles south of the Madera County boundary in Fresno County. Activity along this fault could potentially generate more seismic activity in Tulare County than the San Andreas or Owens Valley fault systems. In particular, a strong earthquake on the Fault could affect northern Tulare County. However, because of the lack of historic activity along the Clovis Fault, inadequate evidence exists for assessing maximum earthquake impacts.”¹⁷

According to the five County Seismic Safety Element¹⁸ and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the General Plan Health and Safety (GPHSE)¹⁹ Cutler / Orosi area is located in the “V1 zone: an area of “low” seismic risk. The San Andreas Fault is the nearest active seismic area, located approximately 60 miles to the west. The Element states that active faults do not exist in Tulare County.

¹⁶ General Plan Background Report, page 8-7

¹⁷ General Plan Background Report, pages 8-6 and 8-7

¹⁸ Tulare County Association of Governments. Five County Seismic Safety Element, 1974. Page 15

¹⁹ Faults identified in Tulare County 2030 General Plan, Figure 10-1 and on the California Geological Survey website <http://www.quake.ca.gov/gmaps/FAM/faultactivitymap.html>

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Soils Characteristics

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Cutler/Orosi (see Figure 11). The following soil types for Cutler/Orosi are provided below.

Calgro/Calgro, consists of moderately deep to a duripan, moderately well drained soils formed in alluvium derived from granitic rock source.

Crosscreek/Kai Association, is formed by the chemical and mechanical alteration of the Kai soil that originally formed in alluvium derived from granitic rock sources. The soil is well drained, and the permeability is moderately slow above the duripan due to sodicity and very slow in the duripan. The available water capacity is moderate to high and the shrink-swell potential is moderate.

Exeter loam, 0 to 2 percent slopes, consists of moderately deep to a duripan, moderately well drained soils that formed in alluvium mainly from granitic sources.

Flamen Loam is an alluvium derived mainly from granitic rock sources and is found on stream terraces. The soil has moderate shrink-swell capacity, is deep to duripan and is moderately well drained. Flamen loam is classified as prime farmland when it is irrigated and has a Class II agricultural rating.

Greenfield sandy loam, 0 to 9 percent slopes, consists of deep, well drained soils that formed in moderately coarse and coarse textured alluvium derived from granitic and mixed rock sources. Greenfield sandy loam is located in the northwest quadrant of Orosi. It is a class I agricultural soil which is well-suited for urbanization, including buildings, streets and roads, and septic tanks.

Hanford Sandy loam, 0 to 2 percent slopes, consists of very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains and alluvial fans.

Honcut sandy loam, 0 to 2 percent slopes, consists of very deep, well drained soils that formed in moderately coarse textured alluvium from basic igneous and granitic rocks. Honcut soils are on floodplains and moderately sloping alluvial fans.

Porterville Clay, 0 to 2 percent slopes, consists of 32± inches of brown and dark reddish brown clay overlying dark reddish gray clay and sandy clay with a high shrink swell potential.

San Joaquin Loam, is moderately deep to a hardpan, well drained and nearly level. This soil is suitable for orchards, vineyards, and cultivated crops but is somewhat limited by the presence of hardpan which restricts root growth. This problem can be alleviated by ripping and shattering the hardpan. The soil is poorly suited to urban uses because of a high clay content, very slow permeability and a cemented hardpan. Septic tank filter fields are severely limited for these reasons.

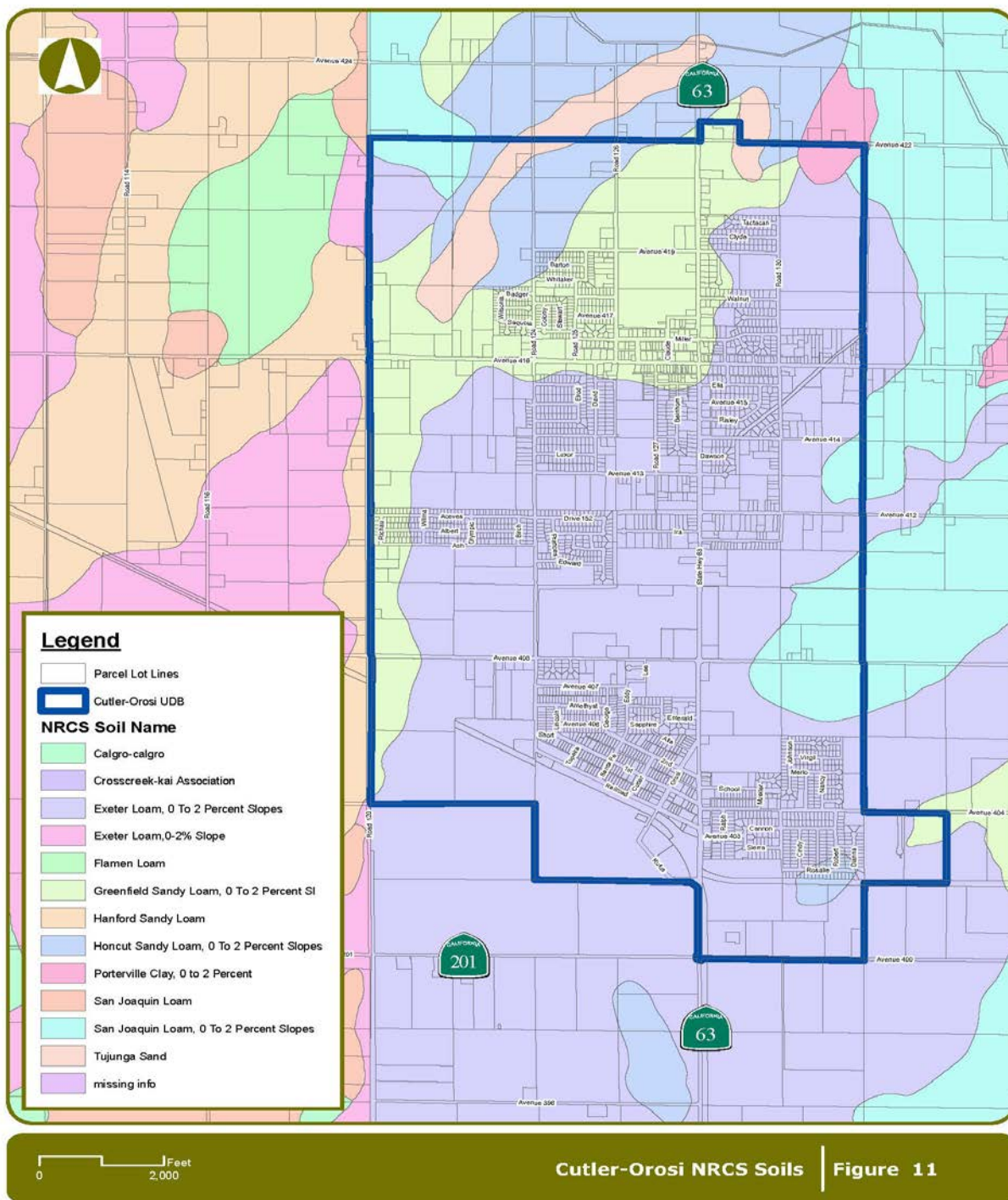
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San Joaquin loam, 0 to 2 percent slopes, consists of moderately deep to a duripan, well and moderately well drained soils that formed in alluvium derived from mixed but dominantly granitic rock sources.

Tujunga sand, consists of very deep, somewhat excessively drained soils that formed in alluvium from granitic sources. Tujunga soils are on alluvial fans and floodplains, including urban areas.

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Figure 11 - NRCS Soils Map



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Greenhouse Gases

“Executive Order S-3-05 was signed by Governor Schwarzenegger on June 1, 2005. This executive order established [GHG] emission reduction targets for California. Specifically, the executive order established the following targets:

- By 2010, reduce GHG emissions to 2000 levels.
- By 2020, reduce GHG emissions to 1990 levels.
- By 2050, reduce GHG emissions to 80 percent below 1990 levels.

The executive order additionally ordered that the Secretary of the California Environmental Protection Agency (Cal EPA) would coordinate oversight of the efforts among state agencies made to meet the targets and report to the Governor and the State Legislature biannually on progress made toward meeting the GHG emission targets. Cal EPA was also directed to report biannually on the impacts to California of global warming, including impacts to water supply, public health, and agriculture, the coastline, and forestry, and prepare and report on mitigation and adaptation plans to combat these impacts.

In response to the EO [executive order], the Secretary of Cal EPA created the Climate Action Team (CAT), composed of representatives from the Air Resources Board; Business, Transportation, & Housing; Department of Food and Agriculture; Energy Commission; California Integrated Waste Management Board (CIWMB); Resources Agency; and the Public Utilities Commission (PUC). The CAT prepared a recommended list of strategies for the state to pursue to reduce climate change emission in the state...”²⁰

In 2006, California passed the California Global Warming Solutions Act of 2006 (Assembly Bill 32; California Health and Safety Code Division 25.5, Sections 38500, et seq.), which requires the CARB to design and implement emission limits, regulations, and other measures, such that feasible and cost-effective statewide GHG emissions are reduced to 1990 levels by 2020.

The Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The San Joaquin Valley Air Pollution Control District (SJVAPCD) adopted the *Climate Change Action Plan* (CCAP) in August 2008. “The (CCAP) directed the District Air Pollution Control Officer to develop guidance to assist Lead Agencies, project proponents, permit applicants, and interested parties in assessing and reducing the impacts of project specific greenhouse gas (GHG) emissions on global climate change.

²⁰ Tulare County General Plan 2030 Update RDEIR, pages 3.4-4 to 3.4-5

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On December 17, 2009, the San Joaquin Valley Air Pollution Control District (SJVAPCD) adopted the guidance: *Guidance for Valley Land-use Agencies in Addressing GHG Emission Impacts for New Projects* under CEQA, and the policy: District Policy – Addressing GHG Emission Impacts for Stationary Source Projects Under CEQA When Serving as the Lead Agency. The guidance and policy rely on the use of performance based standards, otherwise known as Best Performance Standards (BPS), to assess significance of project specific greenhouse gas emissions on global climate change during the environmental review process, as required by CEQA.

“The Tulare County Climate Action Plan (CAP) (last updated in 2016), serves as a guiding document for County of Tulare (“County”) actions to reduce greenhouse gas emissions and adapt to the potential effects of climate change. The CAP is an implementation measure of the 2030 General Plan Update. The General Plan provides the supporting framework for development in the County to produce fewer greenhouse gas emissions during Plan buildout. The CAP builds on the General Plan’s framework with more specific actions that will be applied to achieve emission reduction targets consistent with California legislation.”²¹

Federal Emergency Management Agency (FEMA)

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0345E, Panel No. 345 dated June 16, 2009, (**see Figure 12**) shows the majority of the Cutler/Orosi footprint is within Flood Zone AO, AE, AH, X (shaded), and X (unshaded). A substantial portion of Cutler/Orosi are subject to 100 and 500 year flood hazard. FEMA requires development in Flood Zones AE to be constructed so that a building’s ground floor elevation is above the flood contour line existing in the flood are.

“Flooding is a natural occurrence in the Central Valley because it is a natural drainage basin for thousands of watershed acres of Sierra Nevada and Coast Range foothills and mountains. Two kinds of flooding can occur in the Central Valley: general rainfall floods occurring in the late fall and winter in the foothills and on the valley floor; and snowmelt floods occurring in the late spring and early summer. Most floods are produced by extended periods of precipitation during the winter months. Floods can also occur when large amounts of water (due to snowmelt) enter storage reservoirs, causing an increase in the amount of water that is released.”²² “Flood events in the Tulare Lake region are caused by rainfall, snowmelt, and the resultant rising of normally dry lakes. Although significant progress has been made to contain floodwaters in the region, improvements to the flood control system are still needed to lessen the flood risk to life and property.”²³

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). “Floodplain” or “flood-prone area” means any land area susceptible to being inundated by water from any source. “Base Flood” is the flood having a one percent chance of being equaled or exceeded in any given year. “One-hundred-year flood” or “100 year flood” has the same meaning as “base flood.” “Special flood hazard area” is the land in the floodplain subject to a one percent or greater chance of

²¹ Tulare County Climate Action Plan, page 1

²² General Plan Background Report, page 8-13

²³ California Water Plan Update 2009, Tulare Lake, page TL-28 to TL-29

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flooding in any given year. "Floodway" means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms."²⁴ Although some areas of Tulare County have experienced major flooding along its major rivers, the Cutler/Orosi Plan Area has not. There are portions of Cutler/Orosi, however, that are within and adjacent to the FEMA 500 and 100 year flood zones. According to the Tulare County General Plan Update, substantial flooding could occur in Tulare County if the two (2) major dams were to experience failure. The primary source of floodwaters comes from Sand Creek. Sand Creek runs in a southwesterly direction through the northwestern part of Tulare County.

Sand Creek lies within the Sand Creek Dam inundation zone due to potential flood concerns if there was a dam failure. Sand Creek in Tulare County, California and displayed on the Monson USGS quad topo map. Sand Creek Dam is used for drinking water, fish and wildlife protection and flood control, among other things. Construction of the dam was completed in 1980. At normal levels, it has a surface area of 55 acres. The dam is owned by Tulare County Resource Management Agency. Sand Creek is rock fill. Its height is 60 feet with a length of 933 feet. Normal storage is 1,050 acre. It drains an area of 26.3 square miles. [FEMA Special Flood Hazard Areas and that the inundation zone must be included on hazard disclosures pertaining to real estate contracts].

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by actively adopting minimum regulatory standards as set forth by the Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: www.fema.gov. As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <https://tularecounty.ca.gov/rma/index.cfm/public-works/flood-information/>.

²⁴ General Plan Background Report, page 8-14

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On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

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Cutler-Orosi FEMA Flood Hazard Zones **Figure 12**

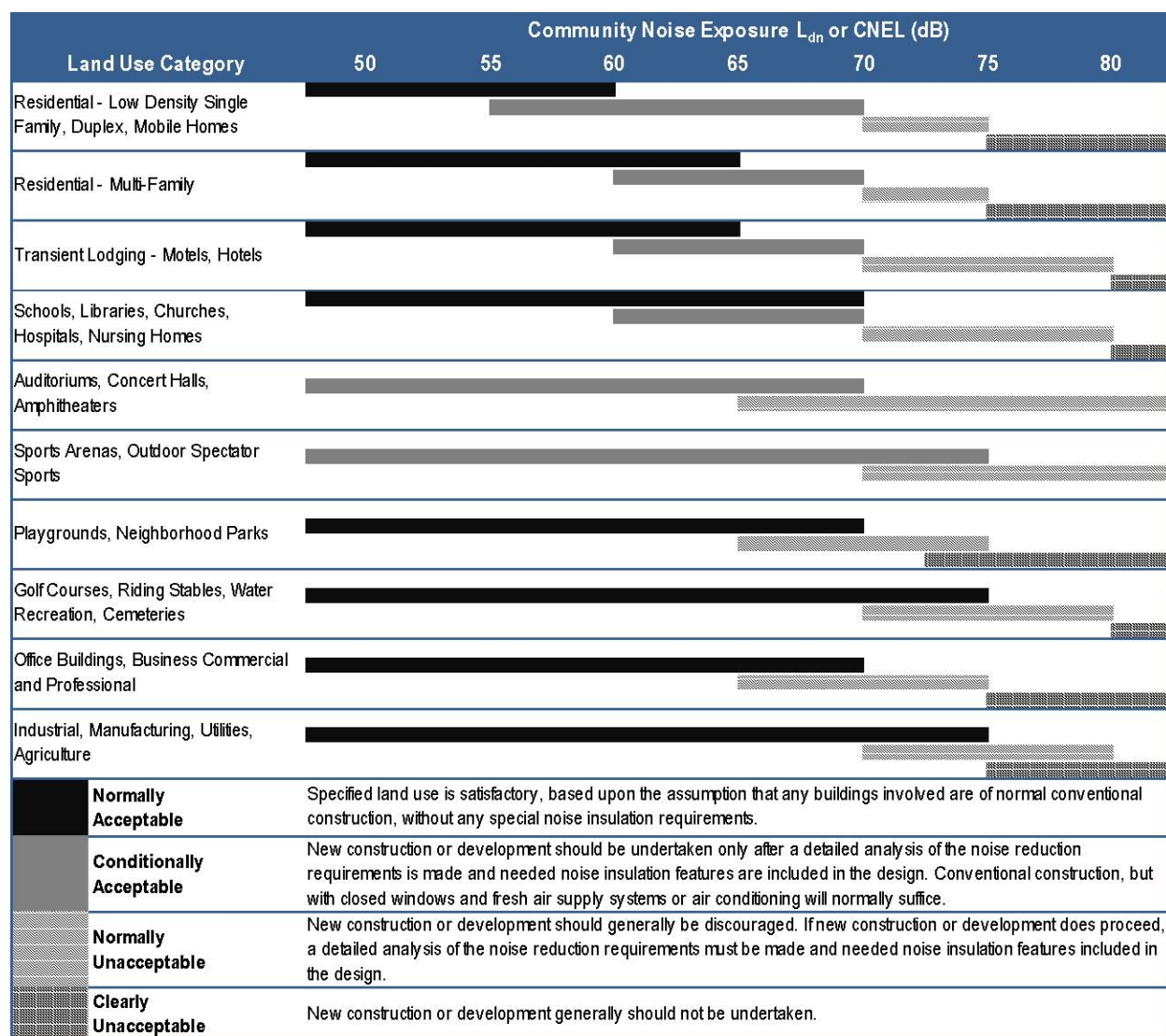


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Noise

State of California General Plan Guidelines (California Governor's Office of Planning and Research, 2003) identifies guidelines for the Noise Elements of city and county General Plans, including a sound level/land-use compatibility chart that categorized, by land use, outdoor L_{dn} ranges in up to four categories (normally acceptable, conditionally acceptable, normally unacceptable, and clearly unacceptable). These guidelines provide the State's recommendations for city and county General Plan Noise Elements (see Figure 13).

Figure 13 - Community Noise Exposure



Source: Tulare County General Plan

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The 2010 Recirculated Environmental Impact Report (RDEIR) prepared for the Tulare County General Plan Update included the following information regarding freeway and railroad noise. Baseline traffic noise contours for major roads in the County were developed using Sound32 (Caltrans' computer implementation of the FHWA Traffic Noise Prediction Model). Table 3.5-3 in the RDEIR summarized the daily traffic volumes, the predicted Ldn noise level at 100 feet from the roadway centerline is approximately 79 feet, and the distance from the roadway centerline to the 60-, 65-, and 70-dB-Ldn contours are 82 feet, 1,813 feet, and 3,907 feet respectively.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands, which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered the maximum normally acceptable noise level for single-family residential areas. The two primary noise sources are SR 63 and Avenue 416. Together, these noise sources place a portion of the urbanized portion of Cutler/Orosi within the 60 dB Ldn noise contour. Roadways and traffic noise are the dominant source of ambient noise in the County.

The Health and Safety Element of the Tulare County General Plan adopted two countywide goals regarding noise in 2012. They are: 1) Protect the citizens of Tulare County from the harmful effects of exposure to excessive noise; and 2) Protect the economic base of Tulare County by preventing the encroachment of incompatible land uses near known noise-producing industries, railroads, airports and other sources. The Tulare County General Plan 2030 models noise contours for lands adjacent to freeways, airports, and local industries for the base year (1986) and provides projected contours for the year 2010. The noise contours were prepared in terms of either the community noise equivalent level (CNEL) or day-night average decibel level (Ldn), which is descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise. **Table 22** summarizes the daily traffic volumes on SR 63 from Avenue 400 to Emerald Drive, Emerald Drive to Avenue 416, Avenue 416 to Avenue 422 and on Avenue 416 from Road 120 to SR 63 and SR 63 to Boyd Drive.

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Table 22 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
SR 63 Ave 400 to Emerald Dr.	8,300	34	74	159	343
SR 63 Emerald Dr. to Ave 416	13,000	43	92	198	426
SR 63 Ave 416 to Ave 422	7,200	29	62	133	287
Ave 416 Road 120 to SR 63	8,000	37	79	171	368
Ave 416 SR 63 to Boyd Dr	850	8	18	38	83

Source: 2010 General Plan Background Report

Infrastructure

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Cutler/Orosi, this Community Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, wells, water distribution piping and storage tanks, curbs, gutters, streets, sidewalks, etc.

Sustainable Groundwater Management Act SGMA

On September 16, 2014, governor Jerry Brown signed into law a three-bill legislative package, composed of AB 1739 (Dickinson), SB 1168 (Pavley), and SB 1319 (Pavley), collectively known as the Sustainable Groundwater Management Act (SGMA). The SGMA provides local governments and stakeholders the time needed to implement the complex law. Completion of plans in critically over drafted basins timeframe is January 31, 2020, and high- and medium-priority basins achieve sustainability 20-years after adoption of their plan (2040).

Groundwater Sustainability Agency (GSA)

The Cutler Public Utility District (CPUD) and the Orosi Public Utility District (OPUD) are located within the Kings River East Groundwater Sustainability Agency (GSA).

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Surface Water

“There is no natural surface water supply in the vicinity of the CPUD or the OPUD. A surface water supply for domestic purposes will have to be transported to the area through Alta Irrigation District’s open channels, the Friant-Kern Canal, a dedicated pipeline or a combination of all three. The Alta Irrigation District surface water supply originates in the Kings River watershed, with their headgate on the Kings River being located downstream of Piedra. Storage of their water supply is provided by Pine Flat Dam.”²⁵

Ground Water

“The CPUD and the OPUD presently rely entirely on groundwater for domestic water supply purposes.”²⁶

Domestic Water and Wastewater

May 3, 2006 by Resolution 06-021 Tulare County Local Agency Formation Commission (LAFCO) adopted the Cutler Public Utility District (CPUD) and Orosi Public Utility District (OPUD) Municipal Service Review (MSR). The agencies in Tulare County were divided into three (3) categories: agencies subject to a full comprehensive study; agencies subject to a questionnaire study; and agencies exempt from a MSR study. The Cutler Public Utility District (CPUD) and Orosi Public Utility District (OPUD) was subject to a full comprehensive study.

“The Orosi Public Utility District (OPUD) and the Cutler Public Utility District (CPUD) provide domestic water to the residents of the unincorporated communities of Orosi and Cutler, respectively. Each district relies solely on groundwater to meet the water demands of its customers. OPUD presently utilizes four wells and CPUD utilizes two active wells.”²⁷

“Currently, each district has sufficient water supply to meet existing water demands.”

Table 23 - Description of Existing Infrastructure						
Community	Drinking Water			Waste Water*		
	No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
Cutler	1,032	1,032	0	1,255	1,255	0
Orosi	1,788	3,788	2,000	2,162	2,162	0

* Tulare County Housing Element Action Program 9, Data current as of May 2012

²⁵ Water Supply Study Cutler-Orosi Area, Dennis R. Keller/James H. Wegley, Consulting Civil Engineers, February 2007, page 4-9.

²⁶ Ibid page 4-1

²⁷ Water Supply Study Cutler – Orosi Area, February 2007, Dennis R. Keller/James H. Wegly, Consulting Civil Engineers.

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Cutler Public Utility District (CPUD)

“CPUD has a total of four developed wells. Two of the wells are active and two of the wells are inactive at this time (**see Figure 14**). The two inactive wells (Well Nos. 3 and 4) were taken out of service because water test results exceeded the maximum Contaminant Level (LCL) limit of nitrates. Well Nos. 5 and 6 are the two active wells that supply water for the community.”²⁸

“There is a well within CPUD (Well No. 7) that is not owned by CPUD. The well is owned by the Tulare County Redevelopment Agency and is used for fire flow at a local industry. Well No. 8 was completed in April 2006. Water quality testing; however, has revealed high nitrate concentrations approaching the MCL. Future use of Wells No. 8 is uncertain. Well No. 9 was drilled on the site for a proposed blending tank facility for CPUD. The well facility, when completed, will allow for water from Well Nos. 3 and 4 to be used in combination with flows from Well No. 5 and Well No. 9. The availability of sufficient quantities of low nitrate concentration water from CPUDE’s wells is uncertain.”²⁹

“The CPID utilizes one elevated water storage tank for water system storage and pressure. The tank holds 50,000 gallons. The tank is connected to the distribution system by a common fill inlet and outlet configuration.”³⁰

“The CPUD’s water supply is derived from four existing deep underground wells that have a total maximum production efficiency of 2,930 GPM, or 4.23 MGD.”³¹

The CPUD water system (**see Table 23**) supports 1,032 total connections including three industry-packing houses, and one box plant. Based upon a calculation performed in accordance with General Order 103, published by the California Public Utilities Commission, it is concluded that the District’s water system is currently operating at or near its capacity, and cannot support additional connections at this time.

The amount of developable land available, including the availability of infrastructure, are two factors that have limited community growth from occurring, including affordable housing objectives, and commercial enterprise.

Currently, the District charges a flat rate for water service in the community. The District should consider installing water meters on all connections to their water system.

“Lovell High School, which is operated by the Cutler-Orosi Joint Unified School District, has requested water capacity from the Cutler PUD. The PUD plans to provide the school with water service pending the approval and implementation of the blending tank project. The school is located at the northwest quadrant of Avenue 392 and State Route 63, which is currently outside of the Cutler

²⁸ Water Supply Study Cutler – Orosi Area, February 2007, Dennis R. Keller/James H. Wegly, Consulting Civil Engineers page 2-2.

²⁹ Ibid. page 2-2

³⁰ Ibid. page 2-3.

³¹ Tulare County Housing Element – Action Program 9, page 2-2.

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PUD boundary and sphere of influence (SOI). It is anticipated that the PUD would provide water service to the school on a contractual basis”.³²

Orosi Public Utility District (OPUD)

The Orosi PUD’s water supply is derived from four existing deep underground wells that have a total maximum production efficiency of approximately 2,930 GPM, or 4.22 MGD. The District also has a water storage tank with a capacity of approximately 750,000 gallons (**see Table 23**).

“OPUD has a total of six developed wells. Four of the wells are active and two of the wells are inactive at this time. Well No. 6 is inactive and was taken out of service because water test results exceeded the MCL limit for nitrates. Well NO. 9 is also considered inactive due to high nitrates and is not connected to the system because of a development dispute. Wells Nos. 4, 5a, 7, and 8 are the four active wells that supply water for the community.”³³

“OPUD has one ground level water storage tank and four hydropneumatic tanks that also provide some limited water storage. The ground level tank has a capacity of 750,000 gallons and delivers water to the system through two booster pumps located at the site of Well No. 5A. There is a 10,000 gallon hydropneumatic tank at each of the active wells.”³⁴

“The Orosi PUD water system supports 1,788 total connections to their water system including 1,639 residential connections, 132 commercial connections, 3 agricultural connections, and 14 connections, which are inactive.”³⁵

³² Tulare County LAFCO Group 2 Municipal Service Reviews, page 3-2.

³³ Water Supply Study Cutler – Orosi Area, February 2007, Dennis R. Keller/James H. Wegly, Consulting Civil Engineers, page 2-4.

³⁴ Ibid. 2-5

³⁵ Tulare County LAFCO Group 2 Municipal Service Reviews, page 4-1

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Sanitary Sewer

In March 1980, the Cutler Public Utility District entered into the Joint Wastewater Treatment and Disposal Facilities Agreement with the Orosi Public Utility District, forming the Cutler-Orosi Joint Power Wastewater Authority for the purpose of operating a wastewater treatment and disposal facility. Under the terms of the Agreement, which expires July 1, 2022, the Cutler Public Utility District owns 50 percent of the property and 40 percent of the plant and equipment of Authority. The Orosi Public Utility District owns 50 percent of the property and 60 percent of the plant and equipment of the Authority.

Figure 15 “graphically displays the approximate location of the sewer system and wastewater treatment plant. The Cutler PUD is currently allocated 1,255 equivalent dwelling units of capacity at the Cutler/Orosi Wastewater Treatment Facility (WWTF). The Orosi PUD is currently allocated 2,162 equivalent dwelling units of capacity at the WWTF. The Cutler and Orosi PUDs are currently under a building moratorium, and have waiting lists for additional sewer connections.

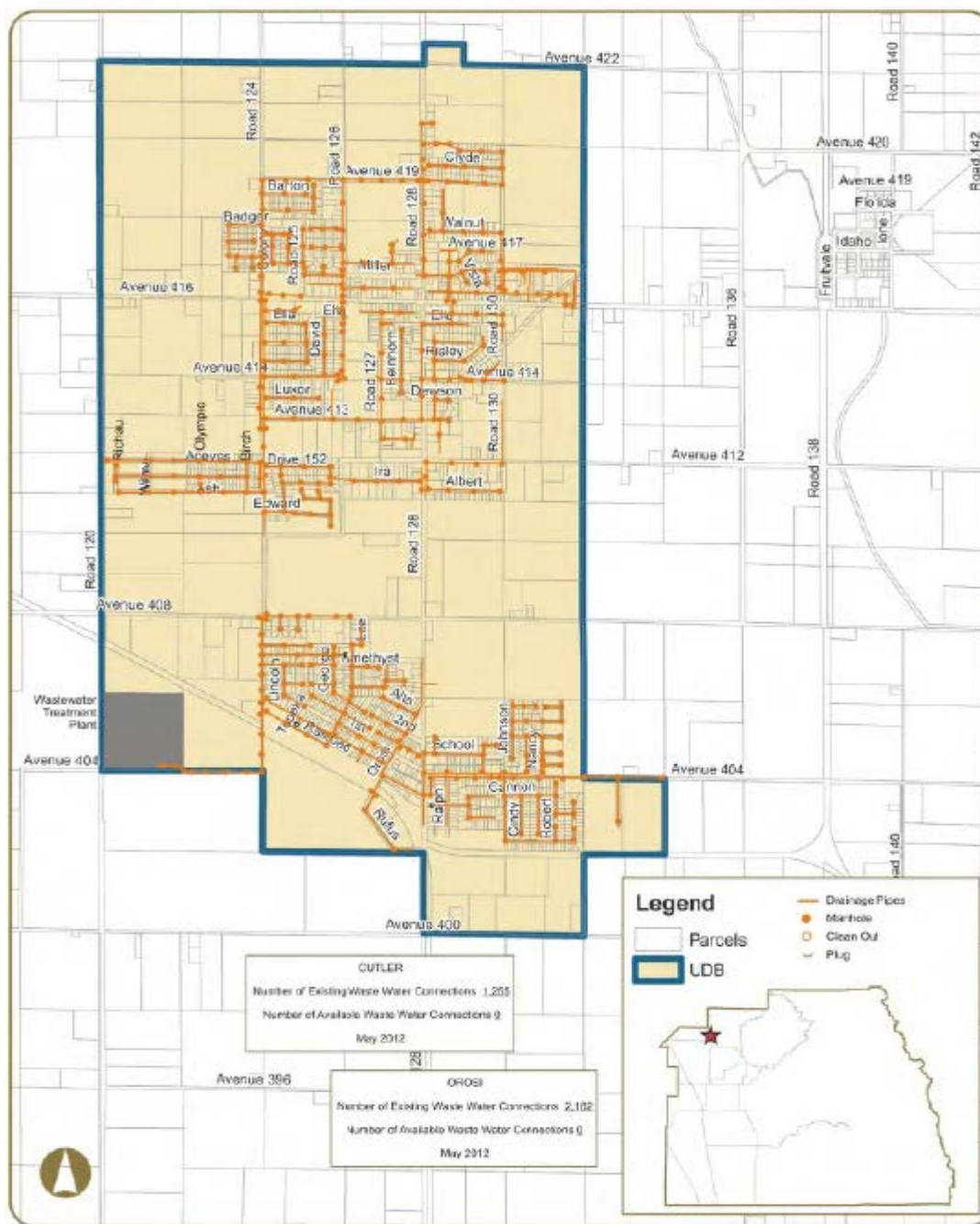
According to Cutler/Orosi PUD staff, the sanitary sewer collection system is very old and pipe leaks and breaks cause significant problems including groundwater inflow/infiltration and cross contamination with groundwater. The Orosi PUD is implementing a phased sewer collection system rehabilitation/replacement project, and has awarded a contract for the construction of the phase 1 improvements.

Treatment and disposal of the collected effluent is provided at the Cutler-Orosi WWTF, jointly owned and operated by the Cutler PUD and Orosi PUD. The Cutler-Orosi WWTF serves the communities of Cutler, Orosi, East Orosi, Yettem, Seville, and Sultana. It operates under the provisions of Waste Discharge Requirements (WDR) Order No. 97-106, issued by the California Regional Water Quality Control Board (RWQCB). The average dry weather flow at the WWTF is approximately 1.40 MGD, with a historical high flow of 1.89 MGD. Flow at the WWTF is greater during winter months than in summer months due to inflow/infiltration of storm water into the collection system during winter months, and ex-filtration during dry summer months. The PUDs will be able to more accurately predict the remaining capacity at the WWTF once repairs are made to leaking pipes throughout the collection system.

The Cutler PUD and Orosi PUD are working with Tulare County to secure funding that will be used to correct deficiencies that would increase the capacity of the WWTF. Proposed improvements will modernize the facility and add capacity to bring the serviceable operational limits to 2.4 MGD.

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Figure 15 - Inventory of Sewer Service for Cutler/Orosi



Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

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Storm Drainage

A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drainpipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration

Table 24 identifies the location of drainage inlets and sumps in Cutler/Orosi.

Table 24 - Location of Existing Storm Drainage Facilities			
No.	East-West Roadway	North-South Roadway	Type
1	1st Drive	Road 124	Inlet
2	1st Drive	Topeka Drive	Inlet
3	1st Drive	Santa Fe Drive	Inlet
4	1st Drive	Cutler Drive	Inlet
5	1st Drive	Orosi Drive	Inlet
6	1st Drive	Road 128	Inlet
7	2nd Drive	Eddy Avenue	Inlet
8	2nd Drive	Road 128	Inlet
9	Amethyst Avenue	Lincoln Road	Inlet
10	Amethyst Avenue	George Road	Inlet
11	Amethyst Avenue	Eddy Avenue	Inlet
12	Avenue 404	Road 128	Inlet
13	Avenue 404	Mueller Road	Inlet
14	Avenue 404	Road 130	Inlet
15	Avenue 406	Eddy Avenue	Inlet
16	Avenue 406	Alta Drive	Inlet
17	Avenue 407	Road 124	Inlet
18	Avenue 413	David Road	Sump
19	Avenue 413	Road 127	Inlet
20	Avenue	East 413of Road 128	Inlet

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Table 24 - Location of Existing Storm Drainage Facilities			
No.	East-West Roadway	North-South Roadway	Type
21	Avenue 414	David Road	Sump
22	Avenue 414	Road 127	Sump
23	Avenue 414	East of Road 128	Inlet
24	Avenue 414	Road 128	Inlet
25	Avenue 414	Sand Creek	Inlet
26	Avenue 414	Road 130	Inlet
27	Avenue 415	East of Road 128	Inlet
28	Avenue 416	Road 124	Inlet
29	Avenue 416	Road 125	Inlet
30	Avenue 416	David Road	Inlet
31	Avenue 416	Road 126	Inlet
32	Avenue 416	Eddy Road	Inlet
33	Avenue 416	Claude Road	Inlet
34	Avenue 416	Road 130	Inlet
35	Avenue 417	Claude Road	Sump
36	Avenue 419	Between Ralph Rd and Road 130	Sump
37	Cannon Avenue	East of Road 130	Inlet
38	Dawson Avenue	East of Road 128	Inlet
39	Ella Avenue	David Road	Sump
40	Ella Avenue	East of Road 128	Inlet
41	Ella Avenue	Road 130	Inlet
42	Emerald Avenue	Road 127	Inlet
43	Ira Avenue	West end	Sump
44	Luxor Avenue	Road 124	Inlet
45	Miller Avenue	Road 125	Sump
46	Miller Avenue	Eddy Road	Sump
47	Miller Avenue	Claude Road	Sump
48	Railroad Drive	Road 124	Inlet
49	Railroad Drive	Topeka Drive	Sump
50	Railroad Drive	Santa Fe Drive	Sump
51	Railroad Drive	Between Santa Fe Drive and Cutler Drive	Inlet
52	Railroad Drive	Cutler Drive	Sump
53	Railroad Drive	Orosi Drive	Sump
54	Risley Avenue	Road 124	Inlet
55	Risley Avenue	East of Road 128	Inlet
56	Rosalie Avenue	Road 130	Inlet
57	Rosalie Avenue	Nancy Road	Inlet
58	Sierra Avenue	Road 128	Inlet
59	South of Avenue 408	Lincoln Road	Inlet
60	South of Avenue 408	Topeka Road	Inlet
61	Walnut Avenue	Road 128	Sump

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

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Solid Waste

Pena's Disposal, a private company, provides solid waste disposal services for the Community of Cutler/Orosi. Solid waste generated in Cutler/Orosi can be disposed of at the Visalia Landfill, located at 22466 Road 80, Visalia, California.

Energy Natural Gas/Electricity

Pacific Gas & Electric (PGE) provides electrical and gas power in Cutler/Orosi.

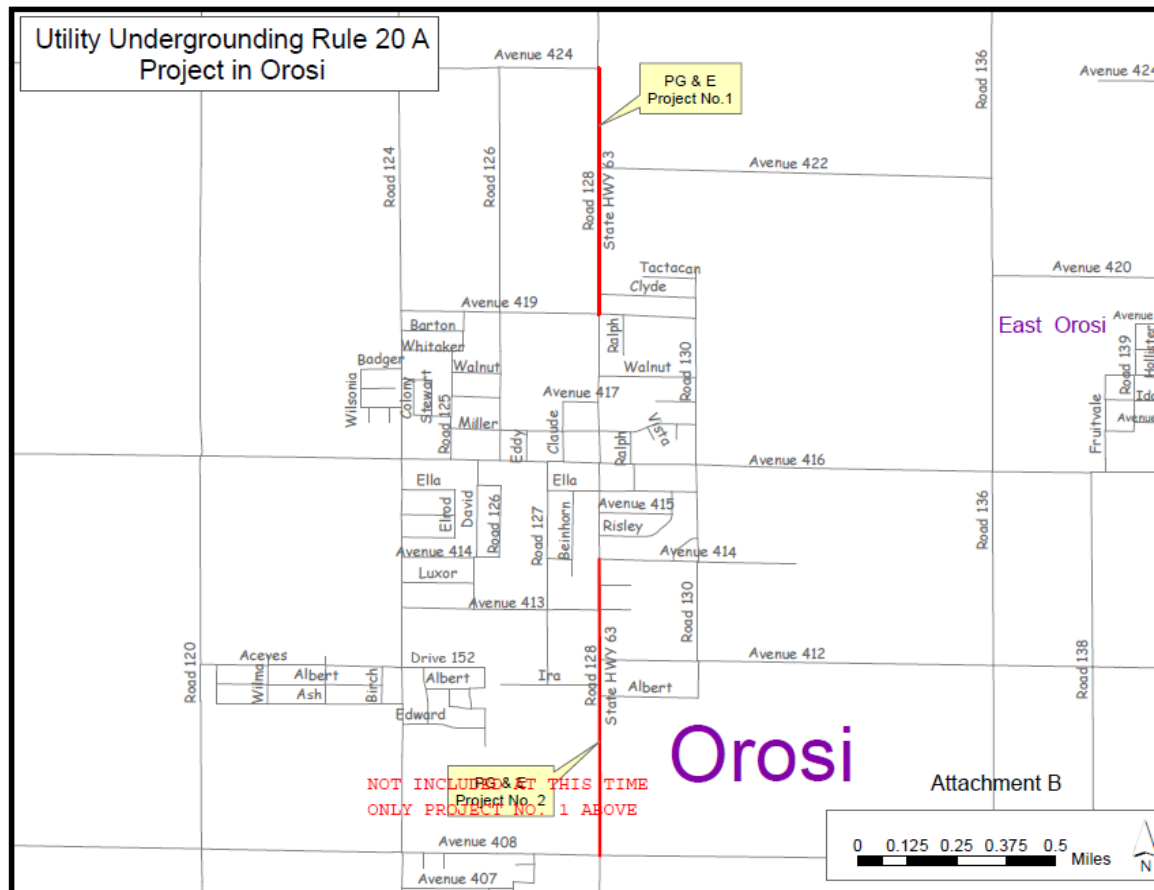
Undergrounding Electricity (Rule 20A)

The California Public Utilities Commission (CPUC) Rule 20 includes policies and procedures for the undergrounding of overhead power lines. Utility ratepayers for projects of public benefit fund rule 20A projects.

Southern California Edison has one proposed Rule 20A project in Orosi (**see Figure 16**): State Route 63 (Road 128) from Avenue 419 to Avenue 424. The Board of Supervisors will consider the formation of an underground utility district for this project.

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Figure 16: Rule 20A Utility Underground Districts in Orosi



Public Services

Tulare County Sheriff

Police protection services are provided in Cutler/Orosi by the Tulare County Sheriff's Department sub-station, located at 12800 Avenue 416, in Orosi. The Substation covers approximately 289 square miles serving a rural population to include the unincorporated communities of Cutler, East Orosi, Orosi, Seville, Sultana, Traver and Yettem. The Substation runs a four-shift operation, which includes 23 deputies, four (4) sergeants and one (1) lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. In addition, general shift staffing the communities of Cutler/Orosi are assigned a Community Based Officer assigned specifically to those areas. The substation is open for walk-ins from 8:00 am to 5:00 pm Monday thru Friday. After hours and weekends there is a phone provided outside the substation that calls directly into the dispatch center. The substation provides patrol services 24-hours per day, 365 days per year. Additional Sheriff Resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

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Tulare County Fire Department

The Tulare County Fire Department provides fire protection and emergency medical services for Cutler/Orosi. The Tulare County Fire Station #4 is located at 40779 Road 128, Cutler/Orosi Fire Station. Cutler/Orosi Fire Station has Patrol 4, Engine 4, and Engine 204 assigned to this location. Paid On-Call Fire Fighters are assigned to this station and they respond when called out to an incident.

There are 267 fire hydrants found within Cutler/Orosi (**see Table 25**). These fire hydrants are located within the County rights-of-way. **Figure 17** display Existing Fire Hydrants in Cutler/Orosi

Table 25: Existing Fire Hydrants Cutler/Orosi			
No.	Location		
1	Avenue 422 west of Road 128	134	Avenue 413 east of Road 128
2	Road 128 south of Avenue 422	135	Dawson Avenue east of Road 128
3	Road 128 south of Avenue 422	136	Dawson Avenue east of Road 128
4	Orosi Mobile Home Estates east of Road 128	137	Beinhorn Road south of Avenue 414
5	Orosi Mobile Home Estates east of Road 128	138	Beinhorn Road south of Avenue 414
6	Orosi Mobile Home Estates east of Road 128	139	Road 127 north of Avenue 413
7	Orosi Mobile Home Estates east of Road 128	140	Avenue 413 and Road 127
8	Orosi Mobile Home Estates east of Road 128	141	Avenue 413 east of Road 127
9	Road 128 north of Clyde Avenue	142	Avenue 413 east of Road 127
10	Tactacan Avenue west of Road 130	143	Avenue 414 east of Road 124
11	Tactacan Avenue and Road 130	144	Road 124 south of Avenue 414
12	Tactacan Avenue west of Road 130	145	David Road south of Avenue 414
13	Clyde Avenue and Road 130	146	Road 124 south of Luxor Avenue
14	Road 130 south of Clyde Avenue	147	Avenue 413 east of Road 124
15	Clyde Avenue west of Road 130	148	David Road south of Luxor Avenue
16	Clyde Avenue east of Road 128	149	Avenue 413 east of David Road
17	Avenue 419 east of Ralph Road	150	Avenue 413 east of David Road
18	Road 128 south of Avenue 419	151	Avenue 413 east of David Road
19	Road 126 north of Avenue 419	152	Ira Avenue and Road 127
20	Avenue 419 west of Road 126	153	Ira Avenue west of Road 127
21	Avenue 419 west of Van Tassel Road	154	David Road and Aceves avenue
22	Avenue 419 west of Van Tassel Road	155	Edward Avenue east of Road 124
23	Avenue 419 east of Road 124	156	Edward Avenue east of Road 124
24	Road 124 north of Barton Avenue	157	Edward Avenue east of Road 124
25	Road 124 and Barton Avenue	158	Road 124 north of Edward Avenue
26	Road 124 south of Barton Avenue	160	Aceves Avenue west of David Road

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Table 25: Existing Fire Hydrants Cutler/Orosi

27	Barton Avenue east of Road 124	161	Albert Avenue east of Frances Road
28	Van Tassel Road south of Barton Avenue	162	Frances Road south of Aceves Avenue
29	Road 126 south of Avenue 419	163	Aceves Avenue and Road 124
30	Road 126 south of Avenue 419	164	Aceves Avenue and Birch Road
31	Road 126 north of Walnut Avenue	165	Birch Road south of Aceves Avenue
32	Road 126 north of Walnut Avenue	166	Ash Avenue and Birch Road
33	Road 126 north of Walnut Avenue	167	Albert Avenue west of Birch Road
34	Ralph Road south of Avenue 419	168	Ash Avenue east of Olympic Street
35	Road 130 north of Walnut Avenue	169	Olympic Street south of Aceves Avenue
36	Road 130 north of Walnut Avenue	170	Albert Avenue and Olympic Street
37	Road 130 north of Walnut Avenue	171	Albert Avenue west of Olympic Street
38	Walnut Avenue east of Road 128	172	Albert Avenue west of Olympic Street
39	Walnut Avenue and Road 128	173	Albert Avenue and Wilma Road
40	Road 128 south of Walnut Avenue	174	Aceves Avenue and Wilma Road
41	Road 125 south of Walnut Avenue	175	Ash Avenue west of Wilma Road
42	Dennison Drive west of Stewart Street	176	Ash Avenue west of Wilma Road
43	Colony Street north of Buenna Vista Avenue	177	Ash Avenue east of Richau Street
44	Colony Street north of Buenna Vista Avenue	178	Ash Avenue east of Richau Street
45	Badger Avenue and Road 124	179	Aceves Avenue east of Richau Street
46	Badger Avenue west of Road 124	180	Avenue 408 and Road 124
47	Road 124 south of Badger Avenue	181	Avenue 408 east of Lincoln Road
48	Sequoia Avenue and Granite Court	182	Avenue 408 east of Topeka Road
49	Sequoia Avenue and Wilsonia Avenue	183	Avenue 408 east of Topeka Road
50	Avenue 417 east of Road 125	184	Avenue 408 east of Topeka Road
51	Avenue 417 west of Road 126	185	Road 124 south of Avenue 408
52	Avenue 417 and Road 126	186	Avenue 407 and Eddy Road
53	Road 126 south of Avenue 417	187	Avenue 407 east of Road 124
54	Miller Avenue east of Road 126	188	Avenue 407 east of Road 124
55	Road 128 south of Walnut Avenue	189	George Road south of Avenue 407
56	Road 128 south of Walnut Avenue	190	George Road south of Amethyst Avenue
57	Road 130 and Avenue 417	191	Amethyst Avenue west of Lincoln Road
58	Avenue 417 west of Road 130	192	Road 124 and Amethyst Avenue
59	Avenue 417 west of Road 130	193	Amethyst Avenue and Eddy Road
60	Avenue 417 west of Road 130	194	Amethyst Avenue and Road 127
61	Avenue 416 east of Road 130	195	Road 128 south of Avenue 408
62	Avenue 416 east of Road 130	196	Emerald Avenue and Road 128
63	Road 130 north of Avenue 416	197	Emerald Avenue east of Road 127

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Table 25: Existing Fire Hydrants Cutler/Orosi

64	Miller Road east of Road 130	198	Avenue 406 and Road 127
65	Miller Road east of Road 130	199	Road 128 north of Emerald Avenue
66	Miller Road and Pacifica Court	200	Road 128 south of Emerald Avenue
67	Miller Road and Vista Court	201	Road 128 south of Emerald Avenue
68	Lincoln Road north of Avenue 416	202	Avenue 406 and Eddy Road
69	Miller Avenue and Ralph Road	203	Avenue 406 west of George Road
70	Miller Avenue and Ralph Road	204	Avenue 406 west of George Road
71	Road 128 north of Avenue 416	205	George Road south of Avenue 406
72	Miller Avenue west of Road 128	206	Topeka Drive south of 1st Drive
73	Miller Avenue and Claude Road	207	Road 124 south of Railroad Drive
74	Eddy Road south of Miller Avenue	208	Road 128 north of Alta Drive
75	Miller Avenue east of Road 125	209	Orosi Drive and Road 128
76	Avenue 416 and Road 124	210	Orosi Drive south of Road 128
77	Avenue 416 east of Road 124	211	Road 128 north of 2nd Drive
78	Avenue 416 east of Road 124	212	Alta Drive west of Orosi Drive
79	Avenue 416 east of Road 124	213	2nd Drive west of Cutler Drive
80	Avenue 416 east of Road 124	214	Eddy Road north of 2nd Drive
81	Road 124 south of Avenue 416	215	Santa Fe Drive south of 2nd Drive
82	Avenue 416 east of Road 125	216	Santa Fe Drive south of 2nd Drive
83	Avenue 416 east of Road 125	217	Santa Fe Drive south of 1st Drive
84	Avenue 416 east of Road 125	218	Santa Fe Drive south of 1st Drive
85	Road 126 south of Ella Avenue	219	Railroad Drive east of Santa Fe Drive
86	Ella Avenue and Road 124	220	Road 124 south of Railroad Drive
87	Ella Avenue east of Road 124	221	Cutler Drive south of 1st Drive
88	Ella Avenue west of Elrod Road	222	Railroad Drive east of Santa Fe Drive
89	Avenue 415 west of Elrod Road	223	Orosi Drive south of Railroad Drive
90	Avenue 415 west of Elrod Road	224	Orosi Drive south of Railroad Drive
91	Elrod Road south of Avenue 415	225	Avenue 402 west of Road 128
92	Risley Avenue west of Elrod Road	226	2nd Drive and Road 128
93	Avenue 415 and Road 124	227	Road 128 south of 1st Drive
94	Road 124 north of Risley Avenue	228	School Avenue east of Road 128
95	Road 124 south of Risley Avenue	229	Quinto Court and Johnston Road
96	Road 124 south of Avenue 415	230	Virgil Avenue and Johnston Road
97	Avenue 416 east of Eddy Road	231	Virgil Avenue and Johnston Road
98	Road 127 and Ella Avenue	232	Virgil Avenue west of Nancy Road
99	Road 127 and Ella Avenue	233	Robert Road south of Rivera Court
100	Avenue 416 and Claude Road	234	Robert Road south of Rivera Court
101	Avenue 416 east of Claude Road	235	Merlo Court east of Nancy Road
102	Avenue 416 east of Claude Road	236	Merlo Avenue and Cindy Road
103	Avenue 416 east of Claude Road	237	Johnston Road south of Merlo Avenue

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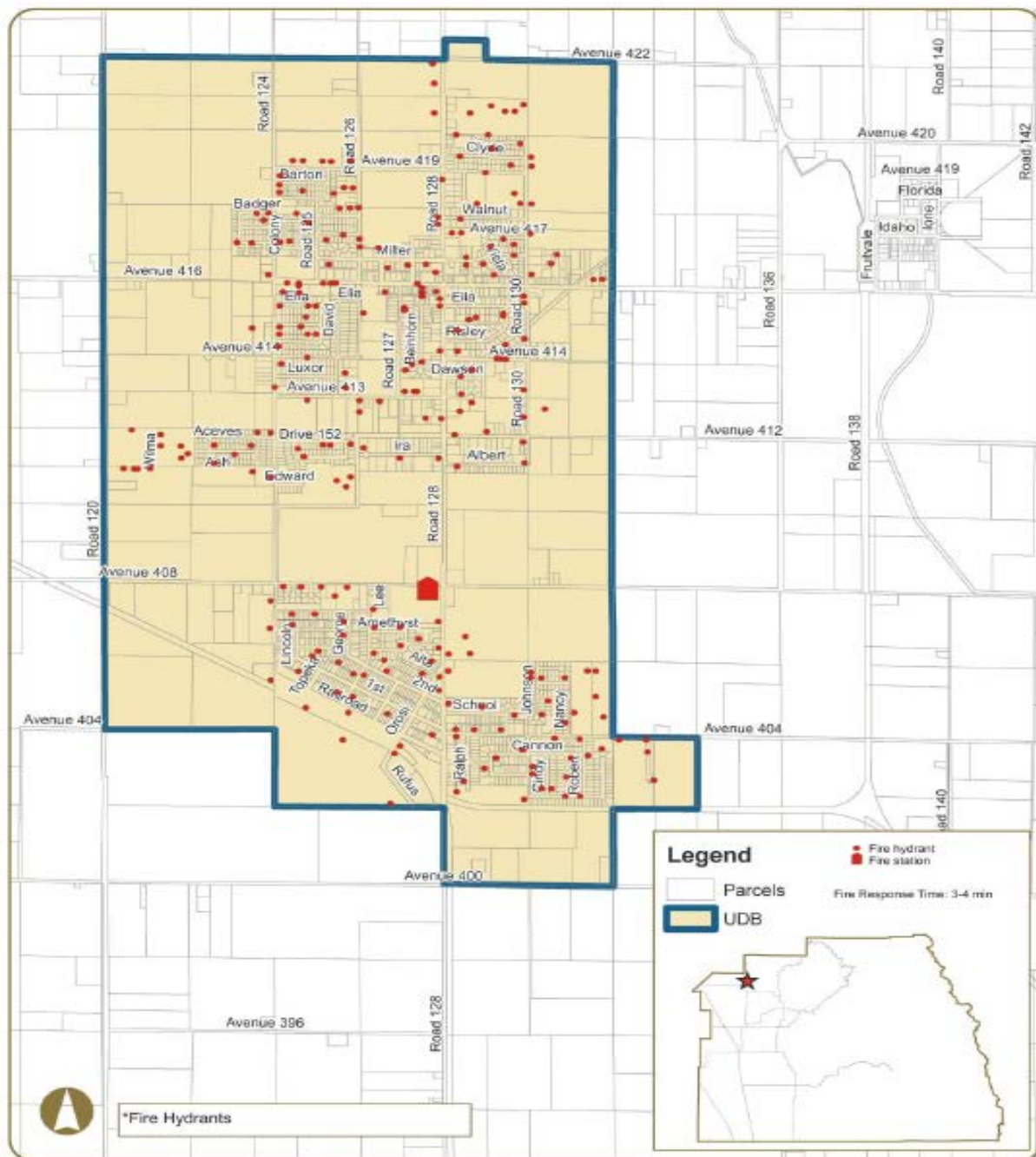
Table 25: Existing Fire Hydrants Cutler/Orosi

104	Road 128 south of Avenue 416	238	School Avenue and Mueller Road
105	Road 128 south of Avenue 416	239	Antonia Avenue and Nancy Road
106	Ella Avenue at Road 128	240	Kahlo Court and Nancy Road
107	Road 130 south of Avenue 416	241	Kahlo Court east of Nancy Road
108	Ella Avenue and Road 130	242	Avenue 404 east of Road 128
109	Mueller Road south of Ella Avenue	243	Avenue 404 east of Road 128
110	Mueller Road south of Ella Avenue	244	Avenue 404 and Ralph Road
111	Avenue 415 east of Road 128	245	Avenue 404 and Mueller Road
112	Avenue 415 east of Road 128	246	Avenue 404 and Cindy Road
113	Avenue 415 east of Road 128	247	Avenue 404 and Robert Road
114	Ledbetter Drive and Road 130	248	Avenue 404 east of Robert Road
115	Ledbetter Drive west of Road 130	249	Avenue 404 east of Robert Road
116	Avenue 414 west of Road 130	250	Private Dwy east of Robert Road south of Avenue 404
117	Avenue 415 and Ledbetter Drive	251	Private Dwy east of Robert Road south of Avenue 404
118	Avenue 415 west of Ledbetter Drive	252	Dianna Road north of Sierra Avenue
119	Avenue 414 east of Road 128	253	Cannon Avenue and Robert Road
120	Road 128 north of Avenue 414	254	Cannon Avenue and Nancy Road
121	Road 130 south of Avenue 414	255	Nancy Road south of Cannon Avenue
122	Road 130 north of Avenue 412	256	Rosalie Avenue and Nancy Road
123	Road 130 north of Avenue 412	257	Rosalie Avenue and Robert Road
124	Avenue 412 and Road 130	258	Cindy Road north of Rosalie Avenue
125	Albert Avenue and Road 130	259	Cindy Road north of Rosalie Avenue
126	Avenue 412 west of Road 130	260	Rosalie Avenue and Road 130
127	Albert Avenue east of Road 128	261	Sierra Avenue and Road 130
128	Albert Avenue and Road 128	262	Road 130 north of Sierra Avenue
129	Avenue 412 east of Road 128	263	Cannon Avenue and Road 130
130	Road 128 north of Avenue 412	264	Cannon Avenue north of Sierra Avenue
131	Road 128 north of Avenue 412	265	Sierra Avenue and Cannon Avenue
132	Road 128 south of Avenue 413	266	Ralph Road south of Avenue 403
133	Avenue 413 east of Road 128	267	Ralph Road south of Avenue 403

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

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Figure 17 : Inventory of Fire Infrastructure in Cutler/Orosi



Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

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Schools

The Cutler/Orosi Community Plan Area is within the Cutler/Orosi Joint Unified School District located within its boundaries. It offers pre-school through 12th grade education and has a 2019-2020 enrollment of 4,123 students (see **Table 26**). School enrollment has been variable since 2000. According to records from the California Department of Education, enrollment in the Cutler/Orosi Joint Unified School District in 2019-20³⁶, of these students approximately 3,936 are Hispanic, 9 are Asian, 73 are White, 48 are African American, 2 are Two or More Races, 5 are American Indian or Alaska Native, 119 are Filipino, and 2 are Pacific Islander. One hundred percent of the students participate in the Free or Reduced Price Meal (FRPM).

Table 26 - Cutler/Orosi Joint Unified - Student Enrollment 2000-2017		
Year	Enrollment	Change
2000-2001	3,844	
2001-2001	3,914	+70
2002-2003	3,981	+7
2003-2004	4,017	+36
2004-2005	4,026	+9
2005-2006	4,058	+32
2006-2007	4,023	-35
2007-2008	4,062	+39
2008-2009	4,128	+66
2009-2010	4,162	+34
2010-2011	4,178	+16
2011-2012	4,133	-45
2012-2013	4,128	-5
2013-2014	4,114	-14
2014-2015	4,083	-14
2015-2016	4,095	+12
2016-2017	4,126	+31
2017-2018	4,125	-1
2018-2019	4,151	+26
2019-2020	4,123	-28

California Department of Education

³⁶ California Department of Education, Data enrollment,
<https://dq.cde.ca.gov/dataquest/dqcenus/EnrEthGrd.aspx?cds=5471860&aggllevel=district&year=2019-20>

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Family Education Center

The Family Resource Center (FRC) is located at the Family Education Center, 40802 Road 128 in Orosi, California, on the northeast corner of SR 63 and Avenue 408. This location places the FRC halfway between Cutler/Orosi, with a Tulare County Area Transit stop directly in front of the FRC. Anyone living within the Cutler/Orosi school district boundaries may be eligible to receive resource and referral services.

Their ultimate goals are to support families, meet individual needs, build upon strengths, share responsibility, and engage the community in efforts to create safe, nurturing environments for Cutler/Orosi community children to grow and learn.

Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”³⁷

The closest library is located in Orosi (see Table 27).

Table 27 - Library Location & Hours		
Branch	Address	Service Hours (2003)
Orosi	Orosi Branch 12646 Avenue 416 Orosi, CA 93647	Wednesday, Thursday, and Friday 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 6:00 p.m.

Library hours current as of April 2019

Parks

Cutler/Orosi’s nearest park is Ledbetter Park located at 40779 Road 124 in Cutler, California.

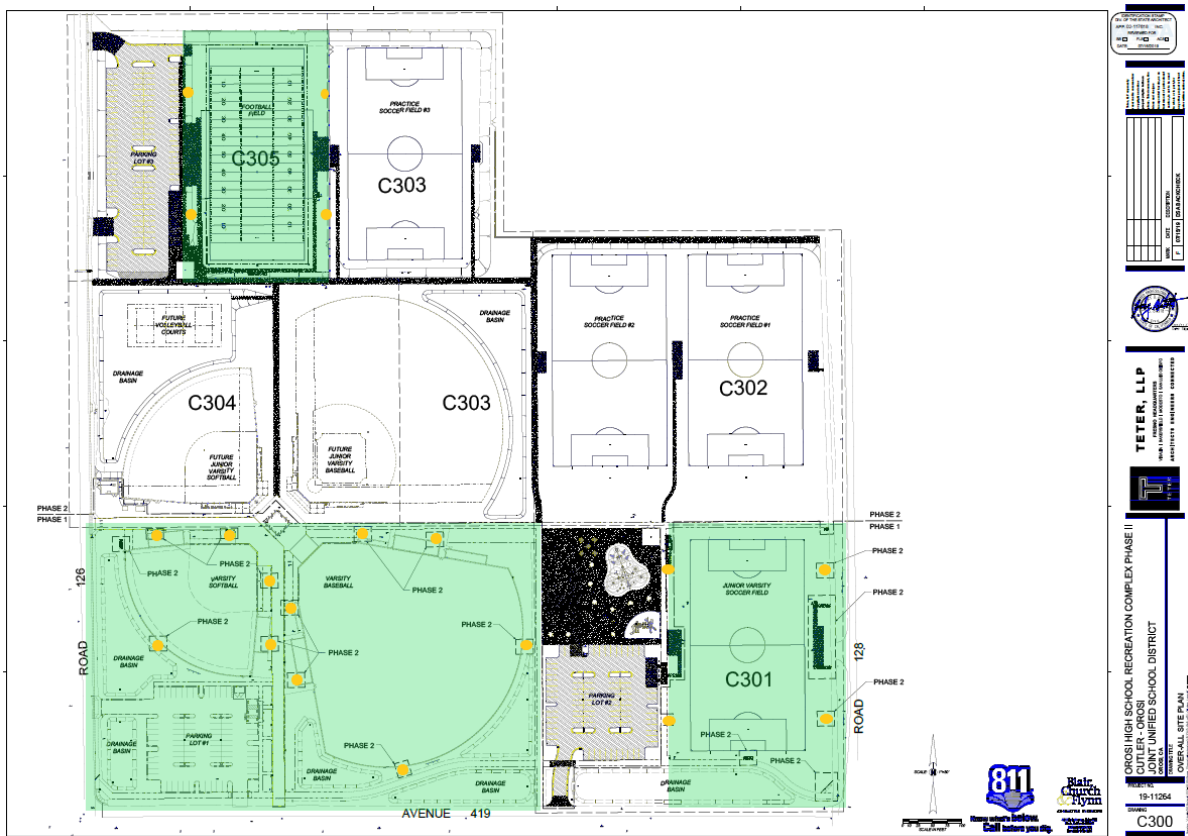
The Cutler/Orosi Joint Unified School District has been in the process of developing the Orosi High School Recreation Complex (approximately 32-acres) located at 41815 Road 128 in Orosi, California. Phase I currently contains football, soccer, baseball, and softball fields that were constructed in the spring/summer of 2019.

The Orosi High School Recreation Complex Phase II proposes the installation of 19 light standards within the previously developed recreational complex area in Orosi, California. Eight 70-foot light standards would be installed around the existing football field and junior varsity soccer field, six 60- to 80-foot light standards would be installed around the existing baseball field, and five 60- to 70 foot light standards would be installed around the existing softball field (see Figure 18).

³⁷ General Plan Background Report, page 7-96

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Figure 18 Orosi High School Recreation Sports Park



Roads

Road Conditions

There are various roadways in Cutler/Orosi that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt, then immediately covered with aggregate, and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

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Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.

Table 28 lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 19** graphically displays this information on a map.

No.	Roadway	Limits	Repair Code
1	Aceves Avenue	Road 124 to David Road	OLAY
2	Albert Avenue	Road 128 to Road 130	CHIP
3	Albert Avenue	Frances Road to David Road	CHIP
4	Alta Drive	Orosi Drive to Avenue 406	CHIP
5	Amethyst Avenue	Road 124 to George Road	CHIP
6	Amethyst Avenue	Eddy Road to Road 127	CHIP
7	Avenue 403	Robert Road to Dianna Road	OLAY
8	Avenue 404	Road 128 to Cindy Road	GRX
9	Avenue 404	Cindy Road to Nancy Road	OLAY
10	Avenue 404	Nancy Road to Robert Road	GRX
11	Avenue 406	Lincoln Road to George Road	CHIP
12	Avenue 406	Eddy Road to Road 127	CHIP
13	Avenue 408	Topeka Road to Lee Road	GRX
14	Avenue 413	Road 124 to Road 128	CHIP
15	Avenue 413	Road 128 to East end	OLAY
16	Avenue 414	Road 127 to Beinhorn Road	CHIP
17	Avenue 415	Road 124 to Elrod Road	OLAY
18	Avenue 415	Road 128 to Mueller Road	CHIP
19	Avenue 417	Road 130 to West end	CHIP
20	Avenue 419	Road 128 to Road 130	CHIP
21	Badger Avenue	Wilsonia Avenue to Road 124	CHIP
22	Barton Avenue	Road 124 to Van Tassel Road	CHIP
23	Beinhorn Road	Avenue 414 to South end	OLAY
24	Beinhorn Road	Avenue 414 to Ella Avenue	CHIP
25	Buena Vista Avenue	Colony Street to Road 125	CHIP
26	Cindy Road	Rosalie Avenue to Cannon Avenue	OLAY
27	Cindy Road	Avenue 404 to Merlo Avenue	CHIP
28	Clyde Avenue	Road 128 to Road 130	OLAY

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Table 28 - Road Maintenance Strategies

29	David Road	Albert Avenue to Aceves Avenue	CHIP
30	Dawson Avenue	Road 128 to East end	CHIP
31	Dennison Avenue	Road 124 to Stewart Street	CHIP
32	Dianna Road	Rosalie Avenue to North end	OLAY
33	Eddy Road	Santa Fe Drive to Avenue 407	CHIP
34	Edward Avenue	Road 124 to David Road	OLAY
35	El Monte Way	Road 128 to Road 130	CHIP
36	El Monte Way	Road 130 to Road 136	OLAY
37	El Monte Way	Elrod Road to Road 126	GRX
38	Ella Avenue	Road 124 to Elrod Road	OLAY
39	Ella Avenue	David Road to George Road	CHIP
40	Ella Avenue	Road 127 to Road 128	GRX
41	Ella Avenue	Road 128 to Road 130	CHIP
42	Elrod Road	Risley Avenue to Ella Avenue	CHIP
43	First Drive	Road 128 to Santa Fe Drive	CHIP
44	George Road	Second Street to Avenue 407	CHIP
45	Johnston Road	South end (Merlo Avenue) to North end (Quinto Court)	CHIP
46	Lincoln Road	First Drive to Amethyst Avenue	CHIP
47	Merlo Avenue	Johnston Road to Nancy Road	CHIP
48	Miller Avenue	Road 126 to Road 128	CHIP
49	Miller Avenue	Ralph Road to Road 130	CHIP
50	Mueller Road	School Avenue to North end	CHIP
51	Mueller Road	Avenue 415 to Ella Avenue	CHIP
52	Nancy Road	Rosalie Avenue to Cannon Avenue	OLAY
53	Nancy Road	Avenue 404 to Virgil Avenue	CHIP
54	Orosi Drive	Railroad Drive to Road 128	CHIP
55	Pacifica Court	Miller Avenue to South end	CHIP
56	Quinto Court	Johnston Road to East end	CHIP
57	Railroad Drive	Road 124 to Road 128	CHIP
58	Ralph Road	Avenue 404 to South end	CHIP
59	Ralph Road	Ella Avenue to El Monte Way	CHIP
60	Ralph Road	Avenue 419 to South end	OLAY
61	Risley Road	Road 124 to Elrod Road	CHIP
62	Road 124	Edward Avenue to Aceves Avenue	CHIP
63	Road 124	Luxor Avenue to El Monte Way	CHIP
64	Road 126	Avenue 414 to Ella Avenue	GRX
65	Road 127	Avenue 406 to North end	CHIP
66	Road 127	Avenue 413 to Avenue 414	CHIP
67	Road 130	Albert Avenue to Avenue 414	CHIP
68	Road 130	Avenue 414 to El Monte Way	GRX
69	Road 130	Walnut Avenue to North end	CHIP
70	Robert Road	Rosalie Avenue to Avenue 404	OLAY
71	Rosalie Avenue	Road 130 to Dianna Road	CHIP
72	Rufus Drive	Road 128 to Orosi Drive	CHIP

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Table 28 - Road Maintenance Strategies

73	Santa Fe Drive	Railroad Drive to Second Drive	CHIP
74	School Avenue	Road 128 to Mueller Road	GRX
75	Sequoia Avenue	Wilsonia Avenue to Road 124	CHIP
76	Short Avenue	Road 124 to Lincoln Road	GRX
77	Sierra Avenue	Road 128 to Road 129	CHIP
78	Stewart Street	Buena Vista Avenue to Dennison Avenue	CHIP
79	Tactacan Avenue	Road 130 to West end	CHIP
80	Topeka Drive	Railroad Drive to First Drive	CHIP
81	Twin Peaks Court	Wilsonia Avenue to East end	CHIP
82	Van Tassel Road	Whitaker Avenue to Avenue 419	CHIP
83	Virgil Avenue	Johnston Road to Nancy Road	CHIP
84	Walnut Avenue	Elrod Road to Road 126	GRX
85	Whitaker Avenue	Road 124 to Van Tassel Road	CHIP
86	Wilma Street	Ash Avenue to North end	CHIP
87	Wilsonia Avenue	Sequoia Avenue to Badger Avenue	CHIP

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

OLAY = Overlay resurfacing operation

CHIP = Chip Seal

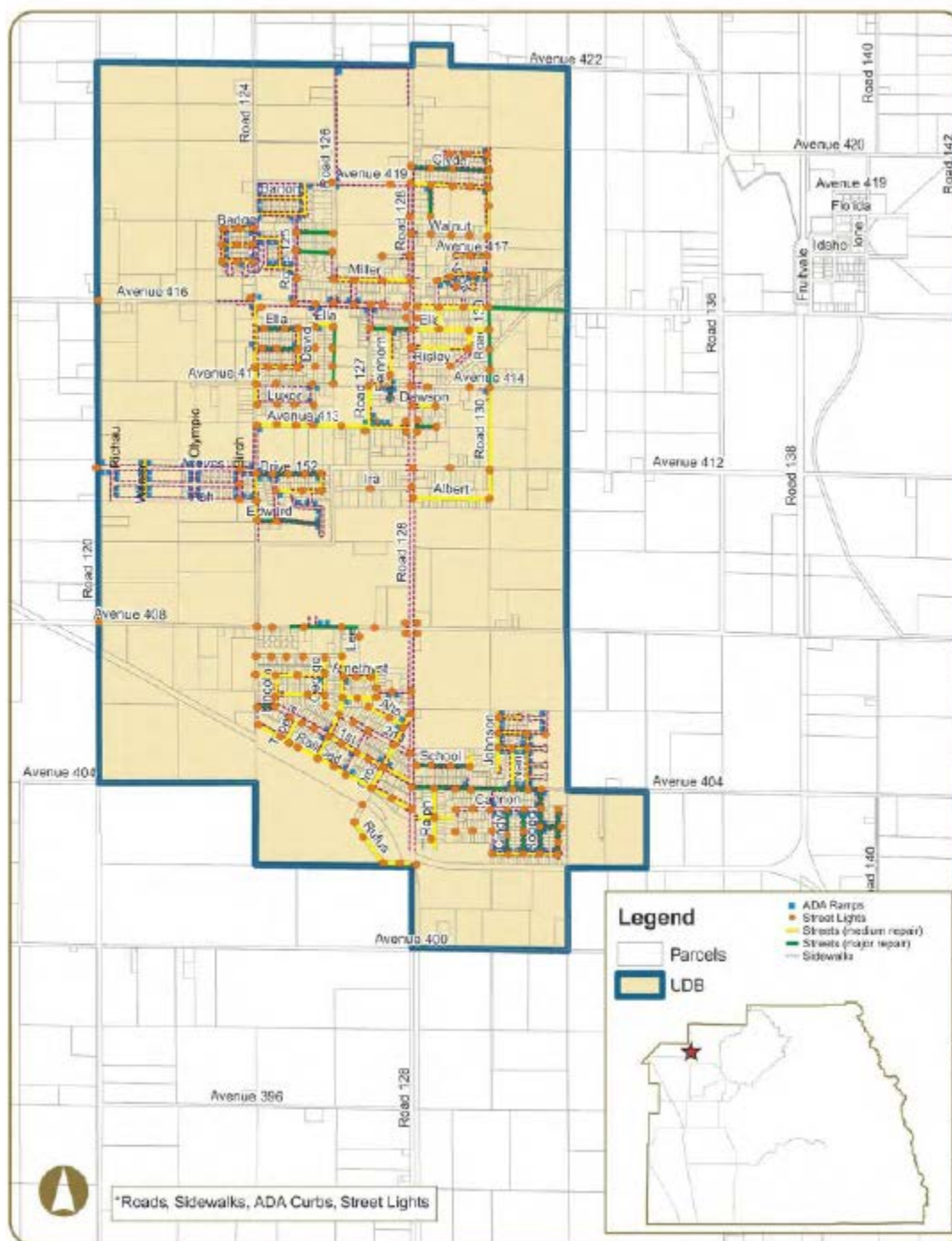
GRXx = Grind and remix

ACST = asphalt reconstruction

RCST = cold mix reconstruction

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Figure 19 - Inventory of Roadway Facilities in Cutler/Orosi



Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

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Sidewalks

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

The County and VRPA Technologies surveyed existing sidewalks within the Community. **Table 29** identifies the location of existing sidewalks in Cutler/Orosi. **Figure 19** also displays this information graphically. The sidewalks represented in **Table 29** and **Figure 19** do not distinguish between ADA compliant sidewalks and noncompliant sidewalks. The majority of sidewalks represented below were constructed prior to current ADA guidelines and would be considered non-ADA compliant facilities. Such noncompliant facilities would require complete reconstruction to be considered ADA compliant.

Table 29 - Location of Existing Sidewalks			
No.	Roadway	Limits	Location
1	1st Drive	Lincoln Road to Cutler Drive	North side
2	1st Drive	150' east of Cutler Drive to 150' east of Orosi Drive	North side
3	1st Drive	Lincoln Road to Topeka Drive	South side
4	1st Drive	175' east of Topeka Drive to Santa Fe Drive	South side
5	1st Drive	75' east of Santa Fe Drive to Cutler Drive	South side
6	1st Drive	150' east of Cutler Drive to Road 128	South side
7	2nd Drive	175' east of Santa Fe Drive to Road 128	North side
8	2nd Drive	225' east of Santa Fe Drive to 150' east of Cutler Drive	South side
9	2nd Drive	Road 128 to 250' west	South side
10	Aceves Avenue	Road 120 to David Road	North side
11	Aceves Avenue	Road 120 to David Road	South side
12	Albert Avenue	Richau Street to Birch Road	North side
13	Albert Avenue	Richau Street to Birch Road	South side
14	Albert Avenue	Rancho Court to David Road	North side
15	Albert Avenue	Central Drive to David Road	South side
16	Alta Drive	250' west of Orosi Drive to 250' west	North side
17	Amethyst Avenue	150' west of Lincoln Road to 300' east of Lincoln Road	North side

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Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
18	Amethyst Avenue	George Road to 375' west	South side
19	Amethyst Avenue	Eddy Avenue to 475' east	North side
20	Antonia Avenue	Nancy Road to east end	North side
21	Antonia Avenue	Nancy Road to east end	South side
22	Ash Avenue	Richau Street to Road 124	North side
23	Ash Avenue	Rancho Court to David Road	North side
24	Ash Avenue	Rancho Court to David Road	South side
25	Avenue 403	Ralph Road to 175' west	North side
26	Avenue 404	Road 128 to Robert Road	North side
27	Avenue 404	175' east of Ralph Road to Mueller Road	South side
28	Avenue 404	Road 130 to Robert Road	South side
29	Avenue 406	George Road to 275' west	North side
30	Avenue 408	Topeka Road to 450' east of Villa de Guadalupe	North side
31	Avenue 413	Road 127 to Road 128	North side
32	Avenue 413	Road 128 to east end	South side
33	Avenue 414	Road 124 to David Road	South side
34	Avenue 414	Road 127 to Beinhorn Road	North side
35	Avenue 414	Road 127 to Beinhorn Road	South side
36	Avenue 415	Road 124 to Elrod Road	North side
37	Avenue 415	Road 124 to Elrod Road	South side
38	Avenue 415	Mueller Road to 300' west	South side
39	Avenue 416	650' west of Road 124 to Road 124	North side
40	Avenue 416	Road 125 to Ella Avenue	North side
41	Avenue 416	Road 126 to Road 130	North side
42	Avenue 416	225' west of Road 124 to Road 128	South side
43	Avenue 417	Road 130 to west end	North side
44	Avenue 417	Road 130 to west end	South side
45	Avenue 419	Road 124 to Road 130	South side
46	Avenue 419	Ralph Road to Road 130	North side
47	Avenue 422	Road 126 to Road 128	South side
48	Badger Avenue	Wilsonia Avenue to Road 124	North side
49	Badger Avenue	Wilsonia Avenue to Road 124	South side
50	Barton Avenue	Road 124 to Van Tassel Road	North side
51	Barton Avenue	Road 124 to Van Tassel Road	South side
52	Beinhorn Road	South end to 575' north of Avenue 414	West side
53	Beinhorn Road	South end to 125' north of Avenue 414	East side
54	Beinhorn Road	Ella Avenue to 600' south	East side
55	Birch Road	Ash Avenue to Aceves Avenue	East side
56	Birch Road	Ash Avenue to Aceves Avenue	West side
57	Buenna Vista Avenue	Road 124 to Road 125	North side

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Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
58	Buenna Vista Avenue	Road 124 to Road 125	South side
59	Cannon Avenue	Sierra Avenue to Robert Road	North side
60	Cannon Avenue	Sierra Avenue to Robert Road	South side
61	Central Drive	Albert Avenue to Ash Avenue	East side
62	Central Drive	Albert Avenue to Ash Avenue	West side
63	Cindy Road	Cannon Avenue to Rosalie Avenue	East side
64	Cindy Road	Cannon Avenue to Rosalie Avenue	West side
65	Cindy Road	Avenue 404 to Merlo Avenue	West side
66	Clyde Avenue	Road 128 to Road 130	North side
67	Clyde Avenue	Road 128 to Road 130	South side
68	Colony Street	Dennison Drive to Buenna Vista Avenue	East side
69	Colony Street	Dennison Drive to Buenna Vista Avenue	West side
70	Cutler Drive	Railroad Drive to 2nd Drive	West side
71	Cutler Drive	2nd Drive to 200' south	East side
72	David Road	Aceves Avenue to Albert Avenue	East side
73	David Road	Ash Avenue to south end	West side
74	David Road	Avenue 414 to Luxor Avenue	West side
75	David Road	Avenue 416 to 200' south	East side
76	David Road	Avenue 416 to 200' south	West side
77	Dennison Drive	Road 124 to Stewart Street	North side
78	Dennison Drive	Road 124 to Stewart Street	South side
79	Dianna Road	Rosalie Avenue to north end	East side
80	Dianna Road	Rosalie Avenue to north end	West side
81	Eddy Avenue	Amethyst Avenue to 200' south	East side
82	Eddy Avenue	Amethyst Avenue to 175' north	West side
83	Eddy Road	Miller Avenue to Avenue 416	East side
84	Eddy Road	Miller Avenue to Avenue 416	West side
85	Edward Avenue	Road 124 to David Road	South side
86	Edward Avenue	Frances Road to David Road	North side
87	Ella Avenue	Road 124 to Elrod Road	North side
88	Ella Avenue	Road 124 to Elrod Road	South side
89	Ella Avenue	Beinhorn Road to Road 128	South side
90	Elrod Road	Ella Avenue to Risley Avenue	East side
91	Elrod Road	Ella Avenue to Risley Avenue	West side
92	Emerald Avenue	Road 127 to Road 128	South side
93	Frances Road	Aceves Avenue to Edward Avenue	East side
94	Frances Road	Aceves Avenue to Edward Avenue	West side
95	George Road	Amethyst Avenue to Avenue 406	West side
96	Granite Court	Sequoia Avenue to south end	East side
97	Granite Court	Sequoia Avenue to south end	West side

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Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
98	Johnston Road	North of Quinto Court to south end	East side
99	Kahlo Court	Nancy Road to east end	North side
100	Kahlo Court	Nancy Road to east end	South side
101	Lincoln Road	Short Avenue to 400' north	West side
102	Lincoln Road	Short Avenue to 300' north	East side
103	Luxor Avenue	Road 124 to David Road	North side
104	Merlo Avenue	Johnston Road to east end	North side
105	Merlo Avenue	Johnston Road to east end	South side
106	Miller Avenue	Road 126 to Claude Road	North side
107	Miller Avenue	Road 126 to Road 128	South side
108	Miller Avenue	Ralph Road to Road 130	North side
109	Miller Avenue	Ralph Road to Road 130	South side
110	Mueller Road	Avenue 404 to 175' north	West side
111	Nancy Road	Cannon Avenue to Rosalie Avenue	East side
112	Nancy Road	Cannon Avenue to Rosalie Avenue	West side
113	Nancy Road	Avenue 404 to Virgil Avenue	West side
114	Nancy Road	Avenue 404 to Virgil Avenue	East side
115	David Road	Avenue 416 to 200' south	East side
116	Olympic Street	Ash Avenue to north of Aceves Avenue	East side
117	Olympic Street	Ash Avenue to north of Aceves Avenue	West side
118	Orosi Drive	2nd Drive to Road 128	East side
119	Orosi Drive	2nd Drive to Road 128	West side
120	Orosi Drive	1st Drive to 200' north	West side
121	Orosi Drive	1st Drive to 200' north	East side
122	Orosi Drive	Railroad Drive to 200' north	East side
123	Pacifica Court	Miller Avenue to south end	East side
124	Pacifica Court	Miller Avenue to south end	West side
125	Paradise Court	Sequoia Avenue to south end	East side
126	Paradise Court	Sequoia Avenue to south end	West side
127	Quinto Court	Johnston Road to east end	North side
128	Quinto Court	Johnston Road to east end	South side
129	Railroad Drive	Road 124 to Topeka Drive	North side
130	Railroad Drive	225' east of Santa Fe Drive to Cutler Drive	North side
131	Railroad Drive	Orosi Drive to Road 128	North side
132	Ralph Road	Avenue 419 to 300' south	East side
133	Ralph Road	Avenue 419 to 300' south	West side
134	Ralph Road	Miller Avenue to Avenue 416	East side
135	Rancho Court	Albert Avenue to Ash Avenue	East side
136	Rancho Court	Albert Avenue to Ash Avenue	West side
137	Richau Street	Aceves Avenue to Ash Avenue	East side

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Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
138	Richau Street	Aceves Avenue to Ash Avenue	West side
139	Risley Avenue	Road 124 to Elrod Road	North side
140	Risley Avenue	Road 124 to Elrod Road	South side
141	Risley Avenue	Bend to Mueller Road	North side
142	Risley Avenue	Bend to Mueller Road	South side
143	Rivera Court	Robert Road to west end	North side
144	Rivera Court	Robert Road to west end	South side
145	Road 124	Railroad Drive to Short Avenue	East side
146	Road 124	Avenue 413 to Aceves Avenue	East side
147	Road 124	Avenue 413 to Ash Avenue	West side
148	Road 124	400' south of Edward Avenue to 150' north of Ash Avenue	East side
149	Road 124	Avenue 414 to Luxor Avenue	East side
150	Road 124	Ella Avenue to Avenue 415	West side
151	Road 124	Ella Avenue to Risley Avenue	East side
152	Road 124	Avenue 419 to Buenna Vista Avenue	East side
153	Road 124	Badger Avenue to Buenna Vista Avenue	West side
154	Road 125	Whittaker Avenue to Avenue 419	East side
155	Road 125	Whittaker Avenue to Avenue 419	West side
156	Road 126	Avenue 422 to Avenue 419	East side
157	Road 126	Avenue 417 to Miller Avenue	West side
158	Road 127	Avenue 413 to 450' north	East side
159	Road 127	Avenue 416 to 500' south	West side
160	Road 127	Avenue 416 to Ella Avenue	East side
161	Road 128	Avenue 422 to 750' south	West side
162	Road 128	Avenue 419 to 550' south of Avenue 403	West side
163	Road 128	Clyde Avenue to 550' south of Avenue 403	East side
164	Road 130	Rosalie Avenue to Avenue 404	East side
165	Road 130	North end to 175' north of Walnut Avenue	West side
166	Road 130	North end to Avenue 419	East side
167	Road 130	Walnut Avenue to Avenue 416	East side
168	Robert Road	Avenue 404 to Rosalie Avenue	East side
169	Robert Road	Avenue 404 to Rosalie Avenue	West side
170	Robert Road	Virgil Avenue to north end	East side
171	Robert Road	Virgil Avenue to north end	West side
172	Rosalie Avenue	Road 130 to Dianna Road	North side
173	Rosalie Avenue	Road 130 to Dianna Road	South side
174	Santa Fe Drive	Railroad Drive to 125' north of 1st Drive	West side
175	Santa Fe Drive	1st Drive to 2nd Drive	East side
176	School Avenue	Road 128 to Mueller Road	North side

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Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
177	School Avenue	400' east of Road 128 to Mueller Road	South side
178	Sequoia Avenue	Wilsonia Avenue to Road 124	North side
179	Sequoia Avenue	Wilsonia Avenue to Road 124	South side
180	Short Avenue	Road 124 to Lincoln Road	North side
181	Short Avenue	Road 124 to Lincoln Road	South side
182	Sierra Avenue	Robert Road to Dianna Road	North side
183	Sierra Avenue	Robert Road to Dianna Road	South side
184	Stewart Street	Dennison Drive to Buenna Vista Avenue	East side
185	Stewart Street	Dennison Drive to Buenna Vista Avenue	West side
186	Tactacan Avenue	Road 130 to west end	North side
187	Tactacan Avenue	Road 130 to west end	South side
188	Topeka Drive	Railroad Drive to 1st Drive	West side
189	Twin Peaks Avenue	Wilsonia Avenue to east end	North side
190	Twin Peaks Avenue	Wilsonia Avenue to east end	South side
191	Van Tassel Road	Avenue 419 to Whittaker Avenue	East side
192	Van Tassel Road	Avenue 419 to Whittaker Avenue	West side
193	Virgil Avenue	Johnston Road to Robert Road	North side
194	Virgil Avenue	Johnston Road to Robert Road	South side
195	Vista Court	Miller Avenue to south end	East side
196	Vista Court	Miller Avenue to south end	West side
197	Whittaker Avenue	Road 124 to Van Tassel Road	North side
198	Whittaker Avenue	Road 124 to Van Tassel Road	South side
199	Wilma Road	Ash Avenue to north of Aceves Avenue	East side
200	Wilma Road	Ash Avenue to north of Aceves Avenue	West side
201	Wilsonia Avenue	Badger Avenue to Sequoia Avenue	East side
202	Wilsonia Avenue	Badger Avenue to Sequoia Avenue	West side

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

ADA Curb Ramps

The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself, which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

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The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are various ADA compliant curb ramps located within Cutler/Orosi and are listed in **Table 30** and displayed in **Figure 19**.

Table 30 - Location of Existing ADA Ramps			
No.	East-West Roadway	North-South Roadways	Location
1	1st Drive	Santa Fe Drive	NW Corner
2	1st Drive	Santa Fe Drive	SW Corner
3	1st Drive	Cutler Drive	SW Corner
4	1st Drive	Orosi Drive	NE Corner
5	1st Drive	Orosi Drive	NW Corner
6	Aceves Avenue	Road 120	NE Corner
7	Aceves Avenue	Road 120	SE Corner
8	Aceves Avenue	Richau Street	SE Corner
9	Aceves Avenue	Richau Street	SW Corner
10	Aceves Avenue	Wilma Road	NE Corner
11	Aceves Avenue	Wilma Road	NW Corner
12	Aceves Avenue	Wilma Road	SE Corner
13	Aceves Avenue	Wilma Road	SW Corner
14	Aceves Avenue	Olympic Street	NE Corner
15	Aceves Avenue	Olympic Street	NW Corner
16	Aceves Avenue	Olympic Street	SE Corner
17	Aceves Avenue	Olympic Street	SW Corner
18	Aceves Avenue	Birch Road	SE Corner
19	Aceves Avenue	Birch Road	SW Corner
20	Aceves Avenue	Road 124	NW Corner
21	Aceves Avenue	Road 124	SW Corner
22	Aceves Avenue	Road 124	NE Corner
23	Aceves Avenue	Road 124	SE Corner
24	Aceves Avenue	Frances Road	SE Corner
25	Aceves Avenue	Frances Road	SW Corner
26	Aceves Avenue	David Road	SW Corner
27	Albert Avenue	Richau Street	NE Corner
28	Albert Avenue	Richau Street	SE Corner
29	Albert Avenue	Wilma Road	SW Corner
30	Albert Avenue	Wilma Road	NE Corner
31	Albert Avenue	Wilma Road	SE Corner
32	Albert Avenue	Olympic Street	NE Corner
33	Albert Avenue	Olympic Street	NW Corner
34	Albert Avenue	Olympic Street	SE Corner
35	Albert Avenue	Olympic Street	SW Corner
36	Albert Avenue	Birch Road	NW Corner

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Table 30 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
37	Albert Avenue	Birch Road	SW Corner
38	Albert Avenue	Frances Road	SE Corner
39	Alta Drive	Orosi Drive	NW Corner
40	Alta Drive	Orosi Drive	SW Corner
41	Amethyst Avenue	Eddy Avenue	SE Corner
42	Amethyst Avenue	George Road	SW Corner
43	Antonia Avenue	Nancy Road	NE Corner
44	Antonia Avenue	Nancy Road	SE Corner
45	Ash Avenue	Richau Street	NE Corner
46	Ash Avenue	Wilma Road	NE Corner
47	Ash Avenue	Wilma Road	NW Corner
48	Ash Avenue	Olympic Street	NE Corner
49	Ash Avenue	Olympic Street	NW Corner
50	Ash Avenue	Birch Road	NE Corner
51	Ash Avenue	Birch Road	NW Corner
52	Ash Avenue	Road 124	NW Corner
53	Ash Avenue	Rancho Court	NE Corner
54	Ash Avenue	Central Drive	NE Corner
55	Ash Avenue	Central Drive	NW Corner
56	Ash Avenue	David Road	SW Corner
57	Avenue 404	Mueller Road	NW Corner
58	Avenue 404	Cindy Road	NE Corner
59	Avenue 404	Cindy Road	NW Corner
60	Avenue 404	Nancy Road	NE Corner
61	Avenue 404	Nancy Road	NW Corner
62	Avenue 404	Robert Road	SE Corner
63	Avenue 404	Robert Road	SW Corner
64	Avenue 406	Eddy Avenue	SE Corner
65	Avenue 408	Villa de Guadalupe	NE Corner
66	Avenue 408	Villa de Guadalupe	NW Corner
67	Avenue 413	Road 124	SE Corner
68	Avenue 413	David Road	NE Corner
69	Avenue 413	Road 127	NE Corner
70	Avenue 413	Road 127	NW Corner
71	Avenue 413	Sequoia View Apts.	NE Corner
72	Avenue 413	Sequoia View Apts.	NW Corner
73	Avenue 414	David Road	SE Corner
74	Avenue 414	David Road	SW Corner
75	Avenue 414	Beinhorn Road	SW Corner
76	Avenue 414	Beinhorn Road	NE Corner

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Table 30 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
77	Avenue 414	Beinhorn Road	SE Corner
78	Avenue 414	Road 130	SW Corner
79	Avenue 415	Road 124	NW Corner
80	Avenue 415	Elrod Road	NW Corner
81	Avenue 416	Road 124	NE Corner
82	Avenue 416	Road 124	NW Corner
83	Avenue 416	Road 124	SE Corner
84	Avenue 416	Road 124	SW Corner
85	Avenue 416	Road 125	NW Corner
86	Avenue 416	David Road	SE Corner
87	Avenue 416	David Road	SW Corner
88	Avenue 416	Road 126	NE Corner
89	Avenue 416	Road 126	NW Corner
90	Avenue 416	Eddy Road	NE Corner
91	Avenue 416	Eddy Road	NW Corner
92	Avenue 416	Road 127	SE Corner
93	Avenue 416	Road 127	SW Corner
94	Avenue 419	Road 124	SE Corner
95	Avenue 419	Van Tassel Road	SW Corner
96	Avenue 419	Van Tassel Road	SE Corner
97	Avenue 419	Road 126	NE Corner
98	Avenue 419	Road 126	SE Corner
99	Avenue 419	Ralph Road	SW Corner
100	Avenue 419	Road 130	SW Corner
101	Avenue 422	Road 126	SE Corner
102	Badger Avenue	Wilsonia Avenue	SE Corner
103	Badger Avenue	Road 124	NW Corner
104	Badger Avenue	Road 124	SW Corner
105	Barton Avenue	Road 124	NE Corner
106	Barton Avenue	Road 124	SE Corner
107	Barton Avenue	Van Tassel Road	NW Corner
108	Barton Avenue	Van Tassel Road	SW Corner
109	Buenna Vista Avenue	Colony Street	NE Corner
110	Buenna Vista Avenue	Stewart Street	NE Corner
111	Buenna Vista Avenue	Stewart Street	NW Corner
112	Buenna Vista Avenue	Road 125	NW Corner
113	Buenna Vista Avenue	Road 125	SW Corner
114	Cannon Avenue	Road 130	NE Corner
115	Cannon Avenue	Road 130	SE Corner
116	Cannon Avenue	Cindy Road	SE Corner

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Table 30 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
117	Cannon Avenue	Cindy Road	SW Corner
118	Cannon Avenue	Nancy Road	SE Corner
119	Cannon Avenue	Nancy Road	SW Corner
120	Cannon Avenue	Robert Road	NW Corner
121	Cannon Avenue	Robert Road	SW Corner
122	Dennison Drive	Road 124	NE Corner
123	Dennison Drive	Road 124	SE Corner
124	Dennison Drive	Colony Street	SE Corner
125	Dennison Drive	Colony Street	SW Corner
126	Dennison Drive	Stewart Street	SW Corner
127	Edward Avenue	David Road	NW Corner
127	Edward Avenue	David Road	SW Corner
128	Ella Avenue	David Road	SE Corner
130	Ella Avenue	Beinhorn Road	SE Corner
131	Emerald Avenue	Pearl Road	SE Corner
132	Emerald Avenue	Pearl Road	SW Corner
133	Kahlo Court	Nancy Road	NE Corner
134	Kahlo Court	Nancy Road	SE Corner
135	Luxor Avenue	David Road	NW Corner
136	Merlo Avenue	Nancy Road	NE Corner
137	Merlo Avenue	Nancy Road	SE Corner
138	Merlo Avenue	Johnston Road	NE Corner
139	Merlo Avenue	Johnston Road	SE Corner
140	Merlo Avenue	Cindy Road	SE Corner
141	Merlo Avenue	Cindy Road	SW Corner
142	Merlo Avenue	Nancy Road	NW Corner
143	Merlo Avenue	Nancy Road	SW Corner
144	Miller Avenue	Road 130	NW Corner
145	Miller Avenue	Road 130	SW Corner
146	Miller Avenue	Pacifica Court	SW Corner
147	Miller Avenue	Pacifica Court	SE Corner
148	Miller Avenue	Vista Court	SE Corner
149	Miller Avenue	Vista Court	SW Corner
150	Miller Avenue	Ralph Road	SE Corner
151	Quinto Court	Johnston Road	NE Corner
152	Quinto Court	Johnston Road	SE Corner
153	Railroad Drive	Cutler Drive	NE Corner
154	Railroad Drive	Cutler Drive	NW Corner
155	Rivera Court	Robert Road	NW Corner
156	Rivera Court	Robert Road	SW Corner

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Table 30 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
157	Rosalie Avenue	Road 130	NE Corner
158	Rosalie Avenue	Cindy Road	NE Corner
159	Rosalie Avenue	Cindy Road	NW Corner
160	Rosalie Avenue	Nancy Road	NE Corner
161	Rosalie Avenue	Nancy Road	NW Corner
162	Rosalie Avenue	Robert Road	NE Corner
163	Rosalie Avenue	Robert Road	NW Corner
164	Rosalie Avenue	Dianna Road	NW Corner
165	Sequoia Avenue	Granite Court	SW Corner
166	Sequoia Avenue	Paradise Court	SW Corner
167	Sequoia Avenue	Wilsonia Avenue	NE Corner
168	Sequoia Avenue	Road 124	NW Corner
169	Sierra Avenue	Robert Road	NE Corner
170	Sierra Avenue	Robert Road	NW Corner
171	Sierra Avenue	Robert Road	SE Corner
172	Sierra Avenue	Robert Road	SW Corner
173	Tactacan Avenue	Road 130	NW Corner
174	Tactacan Avenue	Road 130	SW Corner
175	Twin Peaks Avenue	Wilsonia Avenue	NE Corner
176	Twin Peaks Avenue	Wilsonia Avenue	SE Corner
177	Virgil Avenue	Johnston Road	NE Corner
178	Virgil Avenue	Johnston Road	SE Corner
179	Virgil Avenue	Nancy Road	SE Corner
180	Virgil Avenue	Nancy Road	SW Corner
181	Virgil Avenue	Robert Road	NW Corner
182	Whittaker Avenue	Road 124	NE Corner
183	Whittaker Avenue	Road 124	SE Corner
184	Whittaker Avenue	Road 125	SE Corner
185	Whittaker Avenue	Road 125	SW Corner
186	Whittaker Avenue	Van Tassel Road	NW Corner

Source: County of Tulare Public Works, August 2013

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Street Lights

Streetlights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

Table 31 identifies the location of existing streetlights that are maintained by Tulare County, in Cutler/Orosi, as well as their specifications. **Figure 19** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type "W" represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete.

No	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	1st Drive	Lincoln Road	NE Corner	N/A	5800	W	S	PG&E
2	1st Drive	Topeka Drive	NE Corner	1526	5800	W	S	PG&E
3	1st Drive	Santa Fe Drive	NW Corner	1528	5800	W	S	PG&E
4	1st Drive	Cutler Drive	SW Corner	1582	5800	W	S	PG&E
5	1st Drive	Orosi Drive	SE Corner	1523	5800	W	W	PG&E
6	1st Drive	Between Topeka Drive	North Side	1526	5800	W	S	PG&E
7	1st Drive	Between Santa Fe Drive	South Side	1560	5800	W	N	PG&E
8	1st Drive	Between Cutler Drive	South Side	1523	5800	W	W	PG&E
9	2nd Drive	George Road	West Side	N/A	5800	W	NE	PG&E
10	2nd Drive	Orosi Drive	NE Corner	1524	5800	W	S	PG&E
11	2nd Drive	Road 128	East Side	1606	5800	M	E	PG&E
12	2nd Drive	Santa Fe Drive	SW Corner	1586	5800	W	N	PG&E
13	2nd Drive	Between Santa Fe Drive and Cutler Drive	South Side	N/A	5800	W	N	PG&E
14	2nd Drive	Cutler Drive	SE Corner	1562	5800	W	N	PG&E
15	Aceves Avenue	Road 124	NE Corner	2025	N/A	M	E	PG&E
16	Aceves Avenue	Frances Road	North Side	2026	N/A	M	S	PG&E
17	Aceves Avenue	Between Frances Road and David Road	North Side	2027	N/A	N/A	S	PG&E
18	Aceves Avenue	Between Frances Road and David Road	North Side	N/A	N/A	N/A	S	PG&E
19	Aceves Avenue	David Road	East Side	2029	N/A	W	W	PG&E
20	Aceves Avenue	Birch Road	North Side	2642	5800	N/A	S	PG&E
21	Aceves Avenue	Road 120	NE Corner	N/A	5800	S	W	PG&E
22	Albert	Frances Road	West Side	N/A	N/A	N/A	E	PG&E

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Table 31 - Existing Street Lights

	Avenue							
23	Albert Avenue	Between Rancho Court	North Side	2251	N/A	N/A	S	PG&E
24	Albert Avenue	David Road	East Side	N/A	N/A	N/A	W	PG&E
25	Albert Avenue	Birch Road	East Side	2643	5800	N/A	W	PG&E
26	Albert Avenue	Road 128	SE Corner	2449	5800	N/A	W	PG&E
27	Albert Avenue	Road 130	South Side	2280	5800	N/A	N	PG&E
28	Alta Drive	South of Avenue 406	West Side	1600	5800	W	S	PG&E
29	Alta Drive	Between Avenue 406 and Orosi Drive	South Side	N/A	5800	N/A	N	PG&E
30	Alta Drive	Orosi Drive	SW Corner	1602	5800	W	E	PG&E
31	Amethyst Avenue	Road 124	SE Corner	1544	5800	W	E	PG&E
32	Amethyst Avenue	Lincoln Road	NE Corner	N/A	5800	W	S	PG&E
33	Amethyst Avenue	Road 125	North Side	1579	5800	W	S	PG&E
34	Amethyst Avenue	George Road	East Side	1580	5800	W	W	PG&E
35	Amethyst Avenue	Eddy Avenue	West Side	N/A	5800	W	E	PG&E
36	Amethyst Avenue	East of Eddy Avenue	South Side	1580	5800	W	N	PG&E
37	Amethyst Avenue	Road 127	East Side	1595	5800	W	W	PG&E
38	Ash Avenue	Birch Road	NW Corner	2644	5800	N/A	S	PG&E
39	Ash Avenue	Road 124	NW Corner	N/A	N/A	N/A	E	PG&E
40	At south end	Ralph Road	East Side	1534	5800	W	S	PG&E
41	Avenue 404	Road 128	NE Corner	1520	5800	W	W	PG&E
42	Avenue 404	Ralph Road	SE Corner	1525	5800	W	N	PG&E
43	Avenue 404	Mueller Road	South Side	1505	5800	W	N	PG&E
44	Avenue 404	Nancy Road	NW Corner	2758	5800	M	S	PG&E
45	Avenue 404	Between Ralph Road and Mueller Road	South Side	1554	5800	W	N	PG&E
46	Avenue 404	Cindy Road	South Side	2390	5800	S	N	PG&E
47	Avenue 404	Robert Road	SE Corner	2022	5800	S	N	PG&E
48	Avenue 406	Lincoln Road	West Side	1582	5800	W	E	PG&E
49	Avenue 406	Eddy Avenue	West Side	N/A	N/A	N/A	E	PG&E
50	Avenue 406	Between Eddy Avenue and Alta Drive	North Side	N/A	N/A	N/A	S	PG&E
51	Avenue 406	Between Alta Drive and Road 127	North Side	N/A	N/A	N/A	S	PG&E
52	Avenue 406	Between Lincoln Road and George Road	North Side	N/A	N/A	N/A	S	PG&E
53	Avenue 406	George Road	East Side	N/A	N/A	N/A	W	PG&E
54	Avenue 407	George Road	SE Corner	1538	5800	W	N	PG&E
55	Avenue 407	Eddy Avenue	SE Corner	1539	5800	W	W	PG&E
56	Avenue 407	Road 124	West Side	1575	5800	W	E	PG&E
57	Avenue 407	Lincoln Road	South Side	1576	5800	W	N	PG&E

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Table 31 - Existing Street Lights

58	Avenue 407	Topeka Drive	South Side	1578	5800	W	N	PG&E
59	Avenue 407	Between Topeka Drive	South Side	N/A	N/A	N/A	N	PG&E
60	Avenue 408	Road 120	NW Corner	2762	9500	W	S	PG&E
61	Avenue 408	Road 124	NW Corner	1483	5800	N/A	S	PG&E
62	Avenue 408	Lincoln Road	NW Corner	1586	5800	W	S	PG&E
63	Avenue 408	Villa De Guadalupe Apts.	NW Corner	1569	5800	N/A	S	PG&E
64	Avenue 408	West of Lee Road	North Side	1593	5800	N/A	S	PG&E
65	Avenue 408	SR 63	NW Corner	N/A	5800	N/A	S	PG&E
66	Avenue 408	SR 63	NE Corner	N/A	5800	N/A	W	PG&E
67	Avenue 408	SR 63	SE Corner	N/A	5800	N/A	N	PG&E
68	Avenue 408	SR 63	SW Corner	N/A	5800	N/A	E	PG&E
69	Avenue 408	West of SR 63	North Side	N/A	N/A	N/A	S	PG&E
70	Avenue 412	SR 63	SE Corner	N/A	5800	N/A	N	PG&E
71	Avenue 412	Between Road 128 and Road 130	South Side	N/A	5800	N/A	N	PG&E
72	Avenue 412	Road 130	SE Corner	2279	N/A	W	N	PG&E
73	Avenue 413	Road 124	SE Corner	1668	N/A	N/A	W	PG&E
74	Avenue 413	Between Road 124 and David Road	North Side	1678	N/A	N/A	S	PG&E
75	Avenue 413	Between Road 124 and	North Side	1680	N/A	N/A	S	PG&E
76	Avenue 413	David Road	NW Corner	1682	N/A	N/A	SE	PG&E
77	Avenue 413	Between David Road and Road 127	South Side	1683	N/A	N/A	N	PG&E
78	Avenue 413	Road 127	NE Corner	N/A	N/A	N/A	S	PG&E
79	Avenue 413	Road 127	South Side	N/A	N/A	N/A	N	PG&E
80	Avenue 413	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
81	Avenue 413	SR 63	NW Corner	N/A	N/A	N/A	S	PG&E
82	Avenue 413	SR 63	SE Corner	N/A	N/A	N/A	N	PG&E
83	Avenue 413	SR 63	SW Corner	N/A	N/A	N/A	E	PG&E
84	Avenue 413	East end	North Side	2639	5800	N/A	S	PG&E
85	Avenue 414	David Road	SE Corner	1651	5800	W	W	PG&E
86	Avenue 414	Ledbetter Drive	NW Corner	1641	5800	W	SE	PG&E
87	Avenue 414	Road 127	SE Corner	1636	5800	N/A	W	PG&E
88	Avenue 414	Road 126	East Side	1676	N/A	N/A	W	PG&E
89	Avenue 414	Road 124	West Side	1667	N/A	N/A	E	PG&E
90	Avenue 414	East of Road 124	North Side	1669	N/A	N/A	S	PG&E
91	Avenue 414	Road 130	NE Corner	1662	5800	N/A	W	PG&E
92	Avenue 414	SR 63	NE Corner	N/A	5800	N/A	W	PG&E
93	Avenue 414	East of Road 128	North Side	1660	N/A	N/A	S	PG&E
94	Avenue 415	SR 63	NE Corner	1715	5800	W	W	PG&E
95	Avenue 415	Mueller Road	SE Corner	1643	5800	W	W	PG&E
96	Avenue 415	Road 128	NE Corner	1666	5800	W	N	PG&E
97	Avenue 415	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
98	Avenue 415	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
99	Avenue 415	Elrod Road	NW Corner	2033	N/A	S	S	PG&E
100	Avenue 416	Road 124	NW Corner	3338	9500	W	S	PG&E
101	Avenue 416	Road 124	SE Corner	3341	9500	W	N	PG&E
102	Avenue 416	Road 125	NW Corner	1648	5800	O	S	PG&E
103	Avenue 416	David Road	NW Corner	1647	5800	O	S	PG&E
104	Avenue 416	Road 126	NW Corner	1611	5800	O	S	PG&E
105	Avenue 416	Eddy Road	NW Corner	2187	5800	O	S	PG&E

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Table 31 - Existing Street Lights

106	Avenue 416	Road 127	NW Corner	1632	5800	M	S	PG&E
107	Avenue 416	Claude Road	NE Corner	1613	5800	O	S	PG&E
108	Avenue 416	Ralph Road	NE Corner	2188	5800	W	S	PG&E
109	Avenue 416	Road 130	NE Corner	1649	5800	W	S	PG&E
110	Avenue 416	Road 120	NW Corner	3259	16000	W	S	PG&E
111	Avenue 416	Lincoln Road	NE Corner	2188	5800	N/A	S	PG&E
112	Avenue 416	SR 63	NE Corner	2358	N/A	N/A	W	PG&E
113	Avenue 416	SR 63	NW Corner	2357	N/A	N/A	S	PG&E
114	Avenue 416	SR 63	SE Corner	2356	N/A	N/A	N	PG&E
115	Avenue 416	SR 63	SW Corner	2355	16000	N/A	E	PG&E
116	Avenue 417	Road 125	NE Corner	1639	5800	W	W	PG&E
117	Avenue 417	Road 126	SW Corner	1634	5800	W	E	PG&E
118	Avenue 417	SR 63	SW Corner	2189	5800	M	E	PG&E
119	Avenue 417	Claude Road	SW Corner	1631	5800	W	E	PG&E
120	Avenue 417	Road 130	SW Corner	2199	5800	S	E	PG&E
121	Avenue 417	West of Road 130	South Side	2198	5800	S	N	PG&E
122	Avenue 418	SR 63	West Side	1637	5800	N/A	E	PG&E
123	Avenue 419	Ralph Road	NW Corner	1655	5800	W	S	PG&E
124	Avenue 419	Road 126	SW Corner	1689	N/A	W	E	PG&E
125	Avenue 419	Claude Road	South Side	1690	N/A	W	N	PG&E
126	Avenue 419	SR 63	SW Corner	1716	N/A	W	W	PG&E
127	Avenue 419	Between Road 129 and Road 130	North Side	1696	5800	W	S	PG&E
127	Avenue 419	Between Road 129 and	North Side	1690	5800	W	S	PG&E
128	Avenue 419	Road 130	East Side	1698	5800	W	W	PG&E
130	Badger Avenue	Wilsonia Avenue	West Side	2920	N/A	N/A	E	PG&E
131	Badger Avenue	Between Wilsonia	North Side	2921	N/A	N/A	S	PG&E
132	Badger Avenue	Road 124	SW Corner	2922	N/A	N/A	E	PG&E
133	Between Avenue 414 and Ella Avenue	David Road	East Side	1671	N/A	W	W	PG&E
134	Between Avenue 414	David Road	East Side	1672	N/A	N/A	W	PG&E
135	Between Avenue 414	Road 126	West Side	1674	N/A	N/A	E	PG&E
136	Between Avenue 414 and Ella Avenue	Road 126	West Side	1675	N/A	N/A	E	PG&E
137	Cannon Avenue	Between Sierra Avenue and Road 130	North Side	1549	5800	W	W	PG&E
138	Cannon Avenue	Road 130	NE Corner	1550	5800	W	W	PG&E
139	Cannon Avenue	Robert Road	East Side	2023	5800	S	W	PG&E
140	Cannon Avenue	Nancy Road	North Side	2021	5800	S	S	PG&E
141	Cannon Avenue	Cindy Road	North Side	2020	5800	S	S	PG&E
142	Cannon Avenue	Road 130	NE Corner	N/A	N/A	N/A	W	PG&E
143	Clyde Avenue	SR 63	NE Corner	1695	16000	W	W	PG&E

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Table 31 - Existing Street Lights

144	Clyde Avenue	East of SR 63	North Side	1707	5800	W	S	PG&E
145	Clyde Avenue	Between SR 63 and Road 130	North Side	N/A	5800	N/A	S	PG&E
146	Clyde Avenue	Road 130	East Side	1710	5800	W	W	PG&E
147	Dawson Avenue	SR 63	NE Corner	1652	5800	M	W	PG&E
148	Dawson Avenue	East end	East Side	1652	5800	N/A	W	PG&E
149	Edward Avenue	Road 124	SE Corner	2249	N/A	N/A	N	PG&E
150	Edward Avenue	Frances Road	South Side	2249	N/A	M	S	PG&E
151	Ella Avenue	Beinhorn Road	SW Corner	1654	5800	W	N	PG&E
152	Ella Avenue	David Road	NE Corner	1650	5800	W	W	PG&E
153	Ella Avenue	Road 127	NW Corner	1629	5800	W	E	PG&E
154	Ella Avenue	Ralph Road	SE Corner	1645	5800	W	N	PG&E
155	Ella Avenue	Mueller Road	NW Corner	1646	5800	W	S	PG&E
156	Ella Avenue	SR 63	SW Corner	1615	5800	W	E	PG&E
157	Ella Avenue	Elrod Road	NE Corner	2037	N/A	S	SW	PG&E
158	Ella Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
159	Ella Avenue	Between Road 124 and Elrod Road	North Side	2035	N/A	S	S	PG&E
160	Ella Avenue	Road 126	North Side	1673	N/A	N/A	S	PG&E
161	Ella Avenue	Road 130	East Side	1665	9500	W	W	PG&E
162	Emerald Avenue	Road 127	West Side	1598	5800	W	E	PG&E
163	Emerald Avenue	Pearl Road	North Side	1590	5800	W	S	PG&E
164	Emerald Avenue	SR 63	East Side	1604	9500	S	E	PG&E
165	Hazel Avenue	Lee Road	NW Corner	1540	5800	W	E	PG&E
166	Ira Avenue	SR 63	East Side	N/A	5800	N/A	W	PG&E
167	Ira Avenue	Road 127	North Side	3036	N/A	W	S	PG&E
168	Ledbetter Drive	Road 130	East Side	1663	5800	W	W	PG&E
169	Luxor Avenue	Road 124	SW Corner	1653	5800	N/A	E	PG&E
170	Luxor Avenue	David Road	SW Corner	1681	N/A	N/A	E	PG&E
171	Luxor Avenue	Between Road 124 and	South Side	1677	N/A	N/A	N	PG&E
172	Luxor Avenue	Between Road 124 and David Road	South Side	N/A	N/A	N/A	N	PG&E
173	Merlo Avenue	Johnston Road	SW Corner	2759	5800	W	E	PG&E
174	Merlo Avenue	Cindy Road	SE Corner	2756	5800	M	N	PG&E
175	Merlo Avenue	Nancy Road	SW Corner	2757	5800	M	E	PG&E
176	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E
177	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
178	Miller	Eddy Road	NW Corner	1618	5800	W	S	PG&E

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Table 31 - Existing Street Lights

	Avenue							
179	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
180	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
181	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
182	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
183	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
184	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
185	North of Avenue 414	Beinhorn Road	West Side	1687	N/A	N/A	E	PG&E
186	North of Rosalie Avenue	Dianna Road	East Side	2269	5800	S	W	PG&E
100	North of Rosalie Avenue	Nancy Road	East Side	2077	5800	S	W	PG&E
101	North of Rosalie Avenue	Cindy Road	East Side	2076	5800	S	W	PG&E
102	North of Sierra Avenue	Dianna Road	North Side	2079	5800	S	S	PG&E
103	Orosi Drive	Road 128	SW Corner	1607	5800	W	E	PG&E
104	Quinto Court	Johnston Road	West Side	2651	5800	M	E	PG&E
105	Railroad Drive	Orosi Drive	SE Corner	1522	5800	W	N	PG&E
106	Railroad Drive	Santa Fe Drive	NW Corner	1527	5800	W	S	PG&E
107	Railroad Drive	Road 128	East Side	N/A	5800	W	W	PG&E
108	Railroad Drive	Road 124	East Side	1543	5800	N/A	SW	PG&E
109	Railroad Drive	Lincoln Road	North Side	N/A	5800	W	S	PG&E
110	Railroad Drive	Topeka Drive	NE Corner	1591	5800	W	S	PG&E
111	Railroad Drive	Between Topeka Drive and Santa Fe Drive	North Side	1592	5800	N/A	S	PG&E
112	Railroad Drive	Between Santa Fe Drive and Cutler Drive	North Side	N/A	5800	W	S	PG&E
113	Railroad Drive	Cutler Drive	South Side	N/A	5800	W	N	PG&E
114	Railroad Drive	Between Orosi Drive and Road 128	South Side	1522	5800	W	N	PG&E
115	Risley Avenue	Between Road 128 and Avenue 415	South Side	1642	5800	W	N	PG&E
116	Risley Avenue	Road 124	NE Corner	2044	N/A	N/A	W	PG&E
117	Risley Avenue	Between Road 124 and Elrod Road	North Side	2045	N/A	S	S	PG&E
118	Risley	Elrod Road	NW Corner	2047	N/A	W	W	PG&E

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Table 31 - Existing Street Lights

	Avenue							
119	Risley Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
120	Risley Avenue	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
121	Rosalie Avenue	Road 130	NE Corner	453	5800	S	W	PG&E
122	Rosalie Avenue	Nancy Road	South Side	2180	5800	S	N	PG&E
123	Rosalie Avenue	Robert Road	South Side	2267	5800	S	N	PG&E
124	Rosalie Avenue	Dianna Road	South Side	2268	5800	S	NW	PG&E
125	Rosalie Avenue	Cindy Road	South Side	N/A	N/A	N/A	N	PG&E
126	Rufus Drive	Orosi Drive	North Side	1566	5800	W	S	PG&E
127	Rufus Drive	Orosi Drive	East Side	1533	5800	W	W	PG&E
127	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E
128	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
130	Miller Avenue	Eddy Road	NW Corner	1618	5800	W	S	PG&E
131	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
132	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
133	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
134	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
135	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
136	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
137	North of Avenue 414	Beinhorn Road	West Side	1687	N/A	N/A	E	PG&E
138	North of Rosalie Avenue	Dianna Road	East Side	2269	5800	S	W	PG&E
139	North of Rosalie Avenue	Nancy Road	East Side	2077	5800	S	W	PG&E
140	North of Rosalie Avenue	Cindy Road	East Side	2076	5800	S	W	PG&E
141	North of Sierra Avenue	Dianna Road	North Side	2079	5800	S	S	PG&E
142	Orosi Drive	Road 128	SW Corner	1607	5800	W	E	PG&E
143	Quinto Court	Johnston Road	West Side	2651	5800	M	E	PG&E
144	Railroad Drive	Orosi Drive	SE Corner	1522	5800	W	N	PG&E
145	Railroad Drive	Santa Fe Drive	NW Corner	1527	5800	W	S	PG&E
146	Railroad Drive	Road 128	East Side	N/A	5800	W	W	PG&E
147	Railroad	Road 124	East Side	1543	5800	N/A	SW	PG&E

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Table 31 - Existing Street Lights

	Drive							
148	Railroad Drive	Lincoln Road	North Side	N/A	5800	W	S	PG&E
149	Railroad Drive	Topeka Drive	NE Corner	1591	5800	W	S	PG&E
150	Railroad Drive	Between Topeka Drive and Santa Fe Drive	North Side	1592	5800	N/A	S	PG&E
151	Railroad Drive	Between Santa Fe Drive and Cutler Drive	North Side	N/A	5800	W	S	PG&E
152	Railroad Drive	Cutler Drive	South Side	N/A	5800	W	N	PG&E
153	Railroad Drive	Between Orosi Drive and Road 128	South Side	1522	5800	W	N	PG&E
154	Risley Avenue	Between Road 128 and Avenue 415	South Side	1642	5800	W	N	PG&E
155	Risley Avenue	Road 124	NE Corner	2044	N/A	N/A	W	PG&E
156	Risley Avenue	Between Road 124 and Elrod Road	North Side	2045	N/A	S	S	PG&E
157	Risley Avenue	Elrod Road	NW Corner	2047	N/A	W	W	PG&E
158	Risley Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
159	Risley Avenue	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
160	Rosalie Avenue	Road 130	NE Corner	453	5800	S	W	PG&E
161	Rosalie Avenue	Nancy Road	South Side	2180	5800	S	N	PG&E
162	Rosalie Avenue	Robert Road	South Side	2267	5800	S	N	PG&E
163	Rosalie Avenue	Dianna Road	South Side	2268	5800	S	NW	PG&E
164	Rosalie Avenue	Cindy Road	South Side	N/A	N/A	N/A	N	PG&E
165	Rufus Drive	Orosi Drive	North Side	1566	5800	W	S	PG&E
166	Rufus Drive	Orosi Drive	East Side	1533	5800	W	W	PG&E
167	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E
168	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
169	Miller Avenue	Eddy Road	NW Corner	1618	5800	W	S	PG&E
170	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
171	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
172	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
173	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
174	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
175	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
176	North of	Beinhorn Road	West Side	1687	N/A	N/A	E	PG&E

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Table 31 - Existing Street Lights

	Avenue 414							
177	North of Rosalie Avenue	Dianna Road	East Side	2269	5800	S	W	PG&E
178	North of Rosalie Avenue	Nancy Road	East Side	2077	5800	S	W	PG&E
179	North of Rosalie Avenue	Cindy Road	East Side	2076	5800	S	W	PG&E
180	North of Sierra Avenue	Dianna Road	North Side	2079	5800	S	S	PG&E
181	Orosi Drive	Road 128	SW Corner	1607	5800	W	E	PG&E
182	Quinto Court	Johnston Road	West Side	2651	5800	M	E	PG&E
183	Railroad Drive	Orosi Drive	SE Corner	1522	5800	W	N	PG&E
184	Railroad Drive	Santa Fe Drive	NW Corner	1527	5800	W	S	PG&E
185	Railroad Drive	Road 128	East Side	N/A	5800	W	W	PG&E
186	Railroad Drive	Road 124	East Side	1543	5800	N/A	SW	PG&E
187	Railroad Drive	Lincoln Road	North Side	N/A	5800	W	S	PG&E
188	Railroad Drive	Topeka Drive	NE Corner	1591	5800	W	S	PG&E
189	Railroad Drive	Between Topeka Drive and Santa Fe Drive	North Side	1592	5800	N/A	S	PG&E
190	Railroad Drive	Between Santa Fe Drive and Cutler Drive	North Side	N/A	5800	W	S	PG&E
191	Railroad Drive	Cutler Drive	South Side	N/A	5800	W	N	PG&E
192	Railroad Drive	Between Orosi Drive and Road 128	South Side	1522	5800	W	N	PG&E
193	Risley Avenue	Between Road 128 and Avenue 415	South Side	1642	5800	W	N	PG&E
194	Risley Avenue	Road 124	NE Corner	2044	N/A	N/A	W	PG&E
195	Risley Avenue	Between Road 124 and Elrod Road	North Side	2045	N/A	S	S	PG&E
196	Risley Avenue	Elrod Road	NW Corner	2047	N/A	W	W	PG&E
197	Risley Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
198	Risley Avenue	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
199	Rosalie Avenue	Road 130	NE Corner	453	5800	S	W	PG&E
200	Rosalie Avenue	Nancy Road	South Side	2180	5800	S	N	PG&E
201	Rosalie Avenue	Robert Road	South Side	2267	5800	S	N	PG&E
202	Rosalie Avenue	Dianna Road	South Side	2268	5800	S	NW	PG&E
203	Rosalie	Cindy Road	South Side	N/A	N/A	N/A	N	PG&E

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Table 31 - Existing Street Lights								
	Avenue							
204	Rufus Drive	Orosi Drive	North Side	1566	5800	W	S	PG&E
205	Rufus Drive	Orosi Drive	East Side	1533	5800	W	W	PG&E
206	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E
207	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
208	Miller Avenue	Eddy Road	NW Corner	1618	5800	W	S	PG&E
209	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
210	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
211	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
212	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
213	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
214	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
215	Rufus Drive	Road 127	North Side	1566	5800	W	S	PG&E

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure,

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Cutler/Orosi Community Plan Update

The Need for a Community Plan

Purpose

The Cutler/Orosi Community Plan was adopted in 1988. Conditions in Cutler/Orosi have changed and policies and implementation strategies should be updated to address existing conditions. This community plan update will be used to foster economic development by identifying opportunities for development. This community plan is also a part of the implementation of the San Joaquin Valley Regional Blueprint, Tulare County Regional Blueprint, Transportation Concept Report State Route 63-District, and the Tulare County 2030 General Plan.

Relationship to the General Plan

Tulare County's General Plan provides a comprehensive statement of the objectives, themes and policies, which the community is seeking to achieve in the areas of land use, growth management, community design, transportation, open space, parks and public facilities, environmental conservation, health and safety, noise, and housing. This Community Plan, as an instrument which promulgates and is an extension of the General Plan, incorporates, by definition, the stated general objectives, themes and policies and, where more specific objectives and policies are stated, makes reference to such objectives and policies and provides further elaboration on the ways in which the Community Plan is responsive to this guidance. Relevant General Plan goals, policies, and programs that provide direction and input to this Community Plan are provided in this document. In addition, this plan has specific policies for the Community of Cutler/Orosi.

Planning Framework

San Joaquin Valley Regional Blueprint

"The San Joaquin Valley Blueprint is the result of an unprecedented effort of the eight Valley Regional Planning Agencies (RPA), that include the Fresno Council of Governments, the Kern Council of Governments, the Kings County Association of Governments, the Madera County Transportation Commission, the Merced County Association of Governments, the San Joaquin Council of Governments, the Stanislaus Council of Governments, and the Tulare County Association of Governments, to develop a long-term regional growth strategy for the future of the San Joaquin Valley. Following three-years of visioning and outreach by the eight Valley RPAs, the Regional Policy Council (RPC), the decision-making body for the Valleywide process, adopted the Valley Blueprint in April 2009.

The Blueprint is a long-range vision for a more efficient, sustainable, and livable future for the Valley. The Valleywide Blueprint is made up three elements: a 2050 growth scenario diagram that identifies areas of existing development, new development, and future regional transit and highway improvements; a Valleywide average target density of 6.8 units per acre for new residential growth to

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the year 2050; and a set of 12 Smart Growth Principles. Importantly, the Blueprint recognizes and incorporates by reference the visioning and outreach efforts undertaken by the eight Valley Regional Planning Agencies.”³⁸

Tulare County Regional Blueprint

“TCAG and its member agencies felt that it was important to prepare a Tulare County Regional Blueprint that clarified Tulare County’s role in the Blueprint process. The Tulare County Regional Blueprint is a stand-alone policy document that is consistent with the San Joaquin Valley Regional Blueprint. This document represents Tulare County’s local vision and goals as a participant in the San Joaquin Valley Regional Blueprint process.”³⁹ Key elements of the preferred growth scenario outlined in the Tulare County Regional Blueprint include 25% increase in overall density and focused growth in urban areas.

TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Cutler/Orosi. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describes bicycle circulation patterns and pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

Senate Bill 244, Housing Element Implementation

Senate Bill (SB) 244, passed by the California Legislature and signed into law in 2011, requires California municipalities analyze the inequality and infrastructure deficits within disadvantaged unincorporated communities (DUCs), which lack basic community infrastructure like sidewalks, safe drinking water, and adequate waste processing. As a part of this process and the implementation of the Housing Element the County continues to identify housing related infrastructure needs, such as; water, sewer, natural gas or streetlights, using community needs assessments, housing condition surveys, public comments at community meetings, redevelopment implementation plans and amendments, community plans and other relevant information from the Health & Human Services Agency (HHSA) Environmental Health Services, Regional Water Quality Control Board, public utility districts, community services districts and other agencies. The County of Tulare prepared a 2016 Disadvantage Unincorporated Communities Assessment (infrastructure needs assessment) of the County in conjunction with SB 244 and Action Program 9 as part of the 2015 Tulare County Housing Element Update.

Transportation Concept Report (TRC)-State Route (SR) 63

The purpose of the TCR is to evaluate current, project conditions along the route, and communicate the vision for the development of each route in each Caltrans District during a 20-25 year planning

³⁸ San Joaquin Valley Blueprint Roadmap Guidance Framework, page i

³⁹ Tulare County Regional Blueprint, page 3

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horizon. The TRC is developed with the goals of increasing safety, improving mobility, providing excellent stewardship, and meeting community and environmental needs along the corridor through integrated management of the transportation network, including the highway, transit, pedestrian, bicycle, freight, operational improvements, and travel demand management components of the corridor.

Transportation Concept Report (TRC)-State Route (SR) 201

The TRC is a planning document that identifies the existing and future route conditions as well as future needs for each route on the State Highway System. The TRC is developed with the goals of increasing safety, improving mobility, providing excellent stewardship, and meeting community and environmental needs along the corridor through integrated management of the transportation network, including the highway, transit, pedestrian, bicycle, freight, operational improvements, and travel demand management components of the corridor.

Tulare County 2030 General Plan Implementation

This Community Plan is intended to implement the Tulare County 2030 General Plan. The General Plan Policies relevant to this Community Plan are outlined in General Plan Policies Section below. In addition to the General Plan Policies, this Community Plan outlines policies specific to Cutler/Orosi. Following are the ways in which this Community Plan implements the General Plan:

- Update Zoning Map to match the Community Plan Land Use Map.
- Addition of Design Standards to replace use permit standards.
- Update Zoning text to outline allowed uses in this Community Plan.
- Introduction of a Mixed Use Overlay Zoning District
- Provides a Market Analysis of the Cutler/Orosi Area.
- Provides an updated analysis of Cutler/Orosi's population and housing characteristics.
- Defines an economic development strategy.

Tulare County 2030 General Plan Implementation

This Community Plan is intended to implement the Tulare County 2030 General Plan. The General Plan Policies relevant to this Community Plan are outlined in General Plan Policies Section below. In addition to the General Plan Policies, this Community Plan outlines policies specific to Cutler/Orosi. Following are the ways in which this Community Plan implements the General Plan:

- Update Zoning Map to match the Community Plan Land Use Map.
- Addition of Design Standards to replace use permit standards.
- Update Zoning text to outline allowed uses in this Community Plan.
- Introduction of a Mixed Use Overlay Zoning District
- Provides a Market Analysis of the Cutler/Orosi Area.
- Provides an updated analysis of Cutler/Orosi's population and housing characteristics.
- Defines an economic development strategy.

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Community Plan Update Project Description and Objectives

On September 30, 2014, the Tulare County Board of Supervisors (BOS) approved General Plan Implementation (GPI 14-004) by Resolution 2104-0688 for the Planning Branch proposal to update the Cutler/Orosi Community Plan. The project Study Area Boundary will assess the potential project impacts from the proposed land use changes, for the areas south of Avenue 422 and north of Avenue 400, east of Road 116 and west of Road 134. The project EIR is based on a projected annual population growth rate of 1.3%. Additional growth beyond the 1.3% annual growth rate will require further growth analysis pursuant to CEQA. The Cutler/Orosi Community Plan 2020 Update components are described later in this section will become consistent with the General Plan 2030 Update, and will include the following primary goals and objectives.

- 1) Land Use and Environmental Planning - Promote development within planning areas next to the Regional State Route 63 Corridor in order to implement the following General Plan goals:
 - a) Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals;
 - b) Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
 - c) Reduce development pressure on agriculturally designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
 - d) Reduce vehicle miles travelled throughout the County, thereby positively affecting air quality and greenhouse gas reduction; and
 - e) Help to improve the circulation, transit and railroad transportation system within this community, including, but not limited to, laying the groundwork for the construction of key projects such as Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths.
- 2) Improvements for a “disadvantaged community” - It is expected that the community planning areas will be improved for the following reasons:
 - a) With faster project processing resulting from an updated community plan, increased employment opportunities are more likely to be provided by the private sector as proposed project developments can be approved as expeditiously as possible;
 - b) Increased housing grant awards are more likely to occur based on updated community plans that are consistent with the policies of the recently adopted (August 2013) General Plan Update and Housing Element; and
 - c) With updated community plans, enhanced infrastructure grant awards are more likely, thereby providing access to funding to install or upgrade road, water, wastewater, and storm water facilities.
- 3) Strengthening Relationship with TCAG - An important benefit of this expedited community plan process will be the opportunity for RMA to strengthen the County’s relationship with the Tulare County Association of Governments (TCAG) in that this and other community plans will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects.

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By pursuing these transportation programs through a heightened collaborative process, the likelihood of getting actual projects in the ground will be realized faster than historically achieved. In doing so, these communities and others can become safer and healthier by providing a more efficient transportation network.

Community Outreach Process

Community outreach is vital to the success of the community plan update, because it provided the opportunity for residents to share their ideas and concerns in Cutler/Orosi. This also helps guide decisions on priorities for the community and identify potential challenges. The update process included extensive community and policymaker engagements that were conducted in English and Spanish.

Cutler/Orosi Public Meetings

In order to ensure that the Cutler/Orosi Community Plan 2020 Update was a community-driven, the County conducted public outreach meetings from 2015 through 2020. Broad public input was obtained through a series of workshops where residents, employees, property owners, as well as representatives from the school district and the public utility district, and the surrounding neighborhoods, weighed in on issues and provided recommendations (**see Attachment A-7**).

Complete Streets

- Complete Streets Meeting February 2, 2015
- Complete streets Meeting March 3, 2015

Cutler/Orosi Joint Unified School Board District meeting

- March 14, 2019



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Cutler School meetings

- May 2, 2019
- May 7, 2019



2019 LCAP Community Input Forums

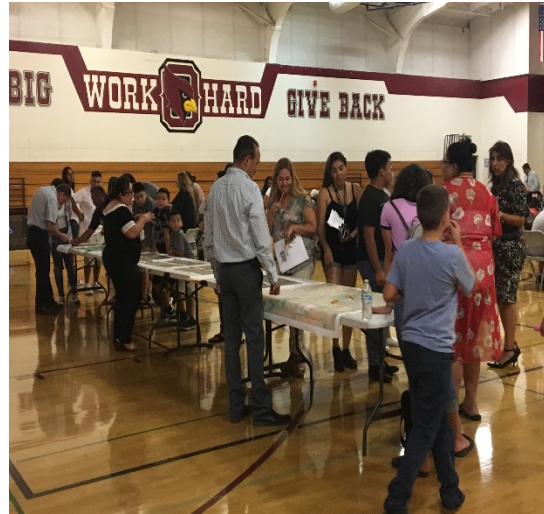
County of Tulare Resource Management Agency (RMA) staff joined Yolanda Valdez, Superintendent, Cutler/Orosi Joint Unified School District, for Back to School Nights at the following schools in Cutler/Orosi. The focus of these workshops were to understand the community's vision and priorities for Cutler/Orosi.

- Cutler Elementary School August 20, 2019
- Orosi High School, August 21, 2019
- Golden Valley Elementary School August 22, 2019
- Palm Elementary School August 27, 2019
- Lovell High School August 28, 2019
- El Monte Middle School September 3, 2019
- Family Education Center September 4, 2019
- Family Education Center September 10, 2019

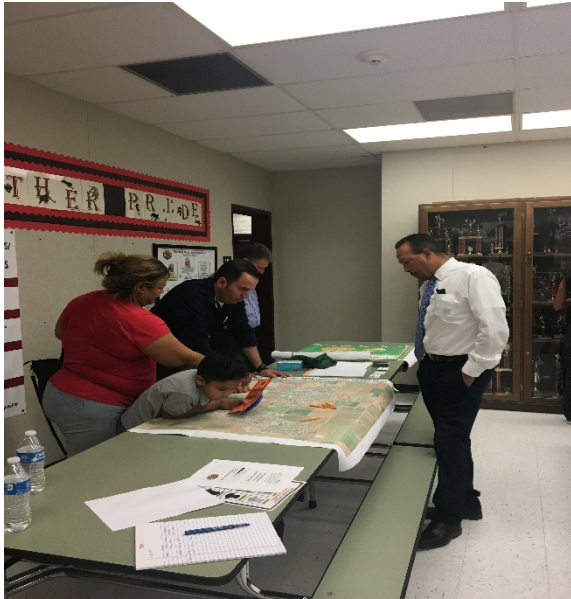
To reach a larger selection of community members the County of Tulare worked with Yolanda Valdez, Superintendent, of the Cutler/Orosi Joint Unified School District to attend Back to School Nights at the below schools. Tables were set up for County staff to lay out maps of the Cutler/Orosi area to give the participants hands on opportunity discuss with the parents living in Cutler/Orosi School District and opportunity for them to show us their vision. Participants identified a number of public safety concerns, largely relating to infrastructure. These concerns included a need for new lights in neighborhoods, flashing lights, crosswalks, widening of dangerous sidewalks, and pedestrian related

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concerns along various roadways. Feedback from participants has been combined into a vision map and spreadsheet addressing the Public Safety Improvement Concerns (see **Figure 20**).

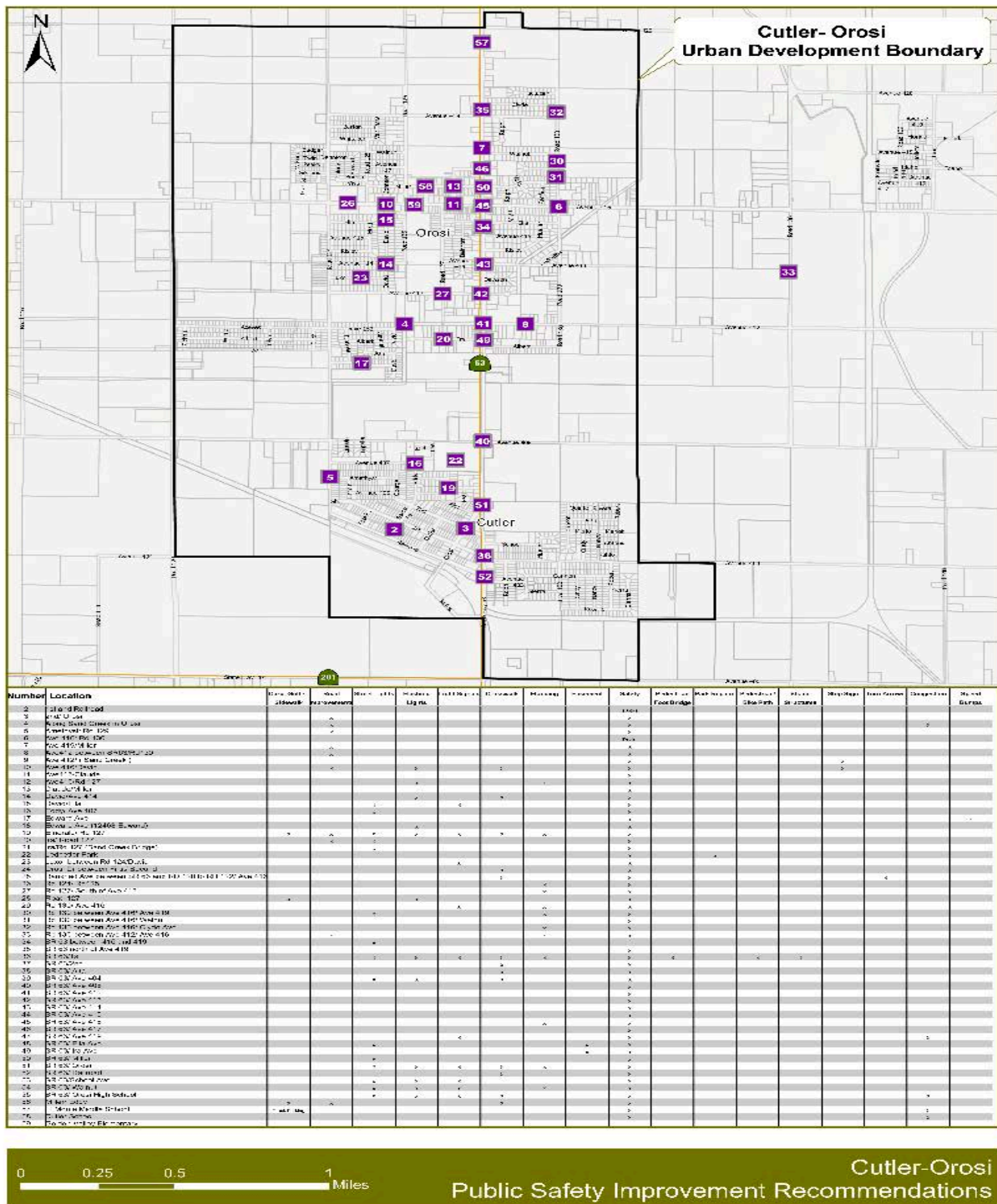


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Figure 20 Cutler/Orosi Community Concerns



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Cutler-Orosi/East Orosi Bus Tour October 1, 2019

County of Tulare RMA staff joined Superintendent Yolanda Valdez and Supervisor Eddie Valero for a guided bus tour around Cutler, Orosi, and East Orosi informing them of infrastructure needs within these communities. The tour included the Orosi Recreation Sports Complex Phase 1 and explanation of the proposed Phase 2 prior to the scheduled Cutler/Orosi Town Hall meeting.

Cutler Orosi Town Hall Meeting October 1, 2019

Tulare County Supervisor Eddie Valero and Superintendent Yolanda Valdez held a Town Hall at the Cutler/Orosi Joint Union School District Board Chambers. At the Town Hall meeting Supervisor Valero welcomed the public and made introductions. Presentations were made by the Cutler/Orosi after School Program. Tulare County staff presented updates and answered questions for the different County Departments as follows:

- Tulare County Water Resource Program
- Tulare County Fire Department
- Tulare County Sheriff's Department
- Tulare County Planning Department
- Resource Management Agency – Code Enforcement
- Resource Management Agency – Roads
- Resource Management Agency – Transit
- Tulare County Animal Services
- Tulare County Parks and Recreation

Cutler Public Utility District

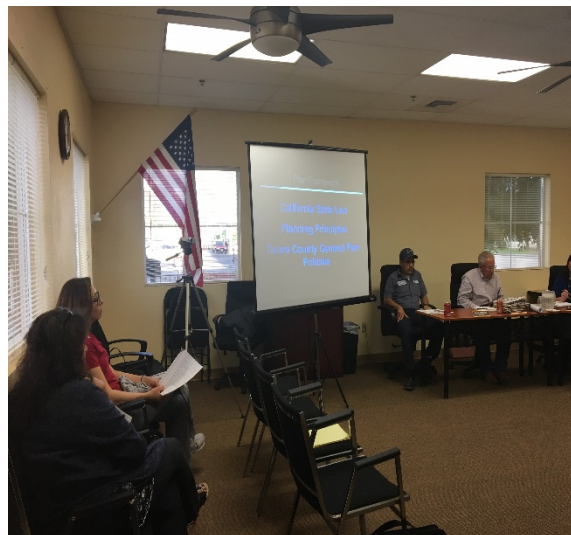
- March 19, 2019
- April 19, 2019



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Orosi Public Utility District

- April 9, 2019



2019/2020 County Transportation Improvement Program 19/20 CTIP

Tulare County receives funding for transportation projects from a variety of sources. These sources can be divided into three categories: Local, State, and Federal. Local sources consist primarily of Measure R (1/2 cent sales tax specifically for transportation and some Maintenance Assessment district revenues. State sources include Local Transportation Funds (from sales tax), and SB1 and Highway User Tax Account (Gas taxes). Federal sources include a variety of competitive grant funds as well as allocations from transportation spending legislation, which can vary from year to year. A road fund balance does exist for transportation projects, emergencies, and other necessary reserves. No property taxes or County general funds are applied to road maintenance and construction within Tulare County.

The CTIP includes all new projects that will be started in the FY 2019/2020. Two projects for the community of Cutler and one project for the community of Orosi, was approved by the Tulare County Board of Supervisors on June 18, 2019, by Resolution No. 2019-0542.

2017 CTIP:

- Avenue 416 Crosswalk and ADA Ramps budgeted at \$200,000 (Construction summer 2019).

2018 CTIP:

- Orosi- ADA Improvement & Blade Patching budgeted at \$500,000 (Construction in Summer 2019)
- Cutler- ADA Improvement & Blade Patching budgeted at \$300,000 (Construction in Summer 2019)

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- Cutler- Road 144 and Avenue 384 to Avenue 416 budgeted at \$1,600,000 (Construction in Progress)

2019 CTIP (Proposed projects):

- ☐ Orosi- ADA Improvement & Blade Patching budgeted at \$200,000 (Construction in Summer 2020)
- Cutler- ADA Improvement & Blade Patching budgeted at \$200,000 (Construction in Progress)

Summer 2020

- George Road and 2nd Drive in Cutler. Pedestrian improvements (sidewalk and drainage improvements to be specific) with an estimated cost of \$2,500,000.

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Sustainability

Climate Change/Adaptation/Resiliency/Sustainability

According to the 2017 Tulare County Multi Jurisdiction Local Hazard Mitigation Plan (LHMP), Climate change has occurred throughout the history of the planet. Due to variations in the earth's inclination to the sun, volcanic activity and other factors such as asteroids impacts, the amount of solar radiation reaching the earth's surface. The temperature of the planet correlates to the amount of solar radiation arriving at the surface and with it the climate.

In relatively recent history, the last glacial period, popularly known as the Ice Age, occurred from c. 110,000 to 12,000 years ago. This most recent glacial period is part of a larger pattern of glacial and interglacial periods known as the Quaternary glaciation (c. 2,588,000 years ago to present). From this point of view, scientists consider this "ice age" to be merely the latest glaciation event in a much larger ice age, one that dates back over two million years and is still ongoing. During this last glacial period, there were several changes between glacier advance and retreat. The Last Glacial Maximum, the maximum extent of glaciation within the last glacial period, was approximately 22,000 years ago. While the general pattern of global cooling and glacier advance was similar, local differences in the development of glacier advance and retreat make it difficult to compare the details from continent to continent. Generally, the pattern of temperature variation and glaciation has lagged atmospheric carbon dioxide (CO₂) content.

The Tulare County (County) has prepared the 2017 Multi-Jurisdictional Local Hazard Mitigation Plan (MJLHMP) to assess the natural, technological, and human-caused risks to County communities, to reduce the potential impact of the hazards by creating mitigation strategies. The 2017 MJLHMP represents the County's commitment to create a safer, more resilient community by taking actions to reduce risk and by committing resources to lessen the effects of hazards on the people and property of the County.

The plan complies with The Federal Disaster Mitigation Act (DMA 2000), Federal Register 44 CFR Parts 201 and 206, which modified the Robert T. Stafford Disaster Relief and Emergency Assistance Act (Stafford Act) by adding a new section, 322 - Mitigation Planning. This law, as of November 1, 2004, requires local governments to develop and submit hazard mitigation plans as a condition of receiving Federal Emergency Management Agency (FEMA) Hazard Mitigation Grant Program (HMGP) and other mitigation project grants. The County; the Cities of Dinuba, Exeter, Farmersville, Lindsay, Porterville, Tulare, Visalia, and Woodlake; the Tule River Tribe; and Special District staffs have coordinated preparation of the MJLHMP in cooperation with stakeholders, partner agencies and members of the public.

Tulare County Climate Action Plan

Tulare County adopted a Climate Action Plan (CAP) on August 28, 2012. The CAP is an implementation measure of the 2030 General Plan Update. The policies, regulations, and programs considered in the CAP include those by federal, state, and local governments.

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“The Tulare County Climate Action Plan (CAP) serves as a guiding document for County of Tulare (“County”) actions to reduce greenhouse gas emissions and adapt to the potential effects of climate change. The CAP is an implementation measure of the 2030 General Plan Update. The General Plan provides the supporting framework for development in the County to produce fewer greenhouse gas emissions during Plan buildout. The CAP builds on the General Plan’s framework with more specific actions that will be applied to achieve emission reduction targets consistent with California legislation.”⁴⁰

Tulare County General Plan Policies (Sustainability)

The Tulare County General Plan has a number of policies that apply to projects within County of Tulare. General Plan policies that relate to Sustainability include the following.

LU-1.1 Smart Growth and Healthy Communities

The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including:

1. Creating walkable neighborhoods,
2. Providing a mix of residential densities,
3. Creating a strong sense of place,
4. Mixing land uses,
5. Directing growth toward existing communities,
6. Building compactly,
7. Discouraging sprawl,
8. Encouraging infill,
9. Preserving open space,
10. Creating a range of housing opportunities and choices,
11. Utilizing planned community zoning to provide for the orderly pre-planning and long term development of large tracks of land which may contain a variety of land uses, but are under unified ownership or development control, and
12. Encouraging connectivity between new and existing development.

LU-1.8 Encourage Infill Development

The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

LU-7.15 Energy Conservation

The County shall encourage the use of solar power and energy conservation building techniques in all new development.

⁴⁰ Tulare County Climate Action Plan, page 1

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LU-7.16 Water Conservation

The County shall encourage the inclusion of “extra-ordinary” water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development.

LU-7.17 Shared Parking Facilities

The County shall encourage, where feasible, the use of shared parking facilities. Such areas could include developments with different day/night uses.

AQ-3.3 Street Design

The County shall promote street design that provides an environment, which encourages transit use, biking, and pedestrian movements.

AQ-3.5 Alternative Energy Design

The County shall encourage all new development, including rehabilitation, renovation, and redevelopment, to incorporate energy conservation and green building practices to maximum extent feasible. Such practices include, but are not limited to building orientation and shading, landscaping, and the use of active and passive solar heating and water systems.

AQ-3.6 Mixed Land Uses

The County shall encourage the clustering of land uses that generate high trip volumes, especially when such uses can be mixed with support services and where they can be served by public transportation.

TCAG Sustainable Communities Strategy (2014 Regional Transportation Plan)

AB 32 set emission targets for the State of California. SB 375 requires the California Air Resources Board to set greenhouse gas emission targets for different regions in California. Under SB 375 Metropolitan Planning Organizations like TCAG are required to create a Sustainable Communities Strategy. TCAG included this strategy in the 2014 Regional Transportation Plan. Highlights of the implementation strategies include:

- Encourage jurisdictions in Tulare County to consider bicycle lanes, public transit, transit-oriented and mixed-use development, pedestrian networks, train and other complete streets development during updates of general plan or other local plans.
- Implement a Complete Streets Program whereby agencies will prepare plans to accommodate all transportation users, including pedestrians, bicyclists, transit riders, and motor vehicle operators and riders, and implement those plans as aggressively as feasible.
- Provide for continued coordination and evaluation of the planned circulation system among cities and the county.
- Fund the development of capital improvement programs for complete streets and active transportation-type plans, as funds are available.
- Evaluate intersections, bridges, interchanges, and rail grade crossings for needed safety improvements.

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- Develop funding strategies for safety projects in cooperation with Caltrans and member agencies.
- Examine alternative funding sources for streets, roads, state highways, rail systems, transit, bicycle, pedestrian, and other transportation mode improvements.
- Utilize Cap and Trade funds available for transit, if available, for projects in Tulare County.
- Encourage local agencies to support implementation of bicycle support facilities such as bike racks, showers, and other facilities during the project review process.
- Utilize Cap and Trade funds available for bicycle and pedestrian projects, if available, for projects in Tulare County.
- Encourage mixed-use developments in urbanized areas.
- Encourage provision of an adequate supply of housing for the region's workforce and adequate sites to accommodate business expansion to minimize interregional trips and long-distance commuting.
- Support and participate in efforts and coalitions promoting use of Cap and Trade funding for projects that help reduce greenhouse gas emissions in Tulare County.
- Support investment in bicycle and pedestrian systems, giving attention to projects and networks that will allow residents to walk and bicycle to frequented destinations, including schools, parks, healthcare institutions and transit stops.
- Provide environmental justice communities opportunities for input into transportation plans, programs, and projects in a manner consistent with Title VI of the 1964 Civil Rights Act and Executive Order 12898 on Environmental Justice, including the prohibition of intentional discrimination and adverse disparate impact with regard to race, ethnicity or national origin.

These implementation strategies are compatible with the Tulare County General Plan policies.

Urban Development Boundary

Urban Development Boundaries (UDBs) are officially adopted and mapped County lines delineating the area expected for urban growth in cities and unincorporated communities over a 20-year period. Within UDB boundaries, the County and cities will coordinate plans, policies and standards related to building construction, subdivision development, land use and zoning regulations, street and highway construction, public utility systems, environmental studies, and other closely related matters affecting the orderly development of urban fringe areas. These boundaries provide an official definition of the interface between future urban and agricultural land uses.⁴¹

“For unincorporated communities, the UDB is a County adopted line dividing land to be developed from land to be protected for agricultural, natural, open space, or rural uses. It serves as the official planning area for communities over a 20-year period. Land within an unincorporated UDB is assumed appropriate for development and is not subject to the Rural Valley Lands Plan or Foothill Growth Management Plan (RVLP Policy 1-1)”⁴²

⁴¹ TCGPU Part 1, Goals and Policies Report, Planning Framework, page 2-3

⁴² TCGPU Part 1, Goals and Policies Report, Planning Framework, page 2-3

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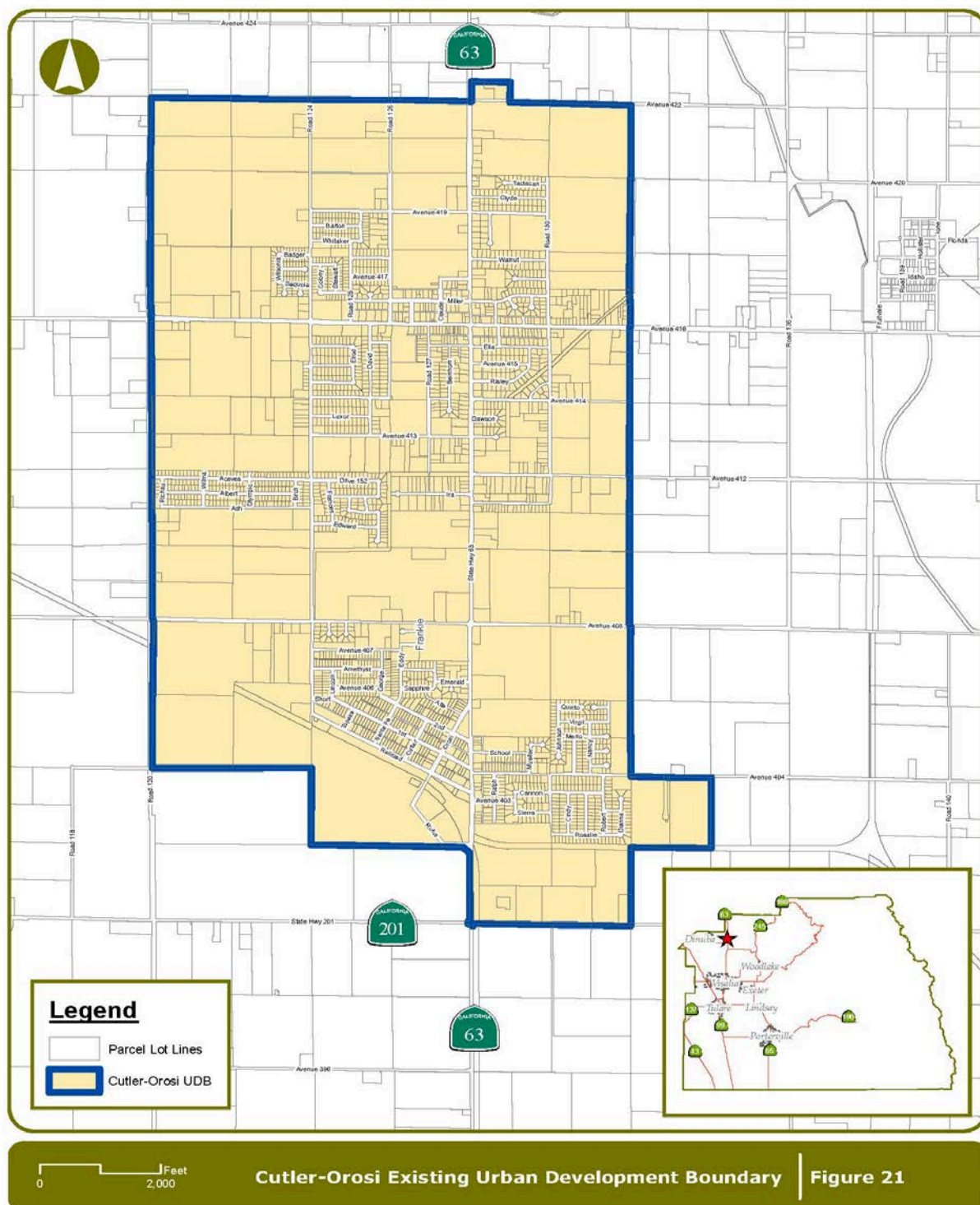
The purpose of this chapter is to review the adequacy of the adopted Urban Development Boundary (UDB) and determine through analysis contained in this chapter whether modifications may be required. The proposed UDB functions as the planning area boundary of the Cutler/Orosi Community Plan (**see Figure 21**). The UDB line establishes a twenty-year growth boundary for the community of Cutler/Orosi. Over the years, services will be extended into this area, which will allow new growth to occur. Since the UDB line defines the area where growth will occur, it is logical that it also serves as the planning area boundary for this plan.

In addition to defining there in which future development of the community of Cutler/Orosi will occur, designating an urban boundary can provide local agencies and citizens with other benefits, including:

1. Encouraging coordination between land use planning and the provision of governmental services.
2. Identifying and resolving potential interagency conflicts regarding service areas.
3. Encouraging efficient, economical and effective delivery of public services.
4. Allowing property owners to identify the type and level of service their lands presently receive or may receive in the future.
5. Assisting in the County's efforts to preserve open space and productive agricultural land.

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Figure 21 - Existing Urban Development Boundary



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Policy Framework

In determining a UDB and planning area for the Cutler/Orosi Community Plan, it is important to not only fulfill the requirements of State planning law, but to the greatest degree possible, fulfill the local goals and policies that regulate land uses in the area. As discussed earlier, there are many County policies that guide development in Cutler/Orosi area. However, those which have direct effect on the establishment of the community's urban boundary include the following policies in the Tulare County General Plan Planning Framework Element:

PF-2.1 Urban Development Boundaries – Communities

The County shall limit urban development to the area within the designated UDB for each community. Each community's UDB is defined as shown on Figures 2.2-2 thru 2.2-22.

PF-2.2 Modification of Community UDB

1. The County may consider modification to a community UDB under any of, but not limited to the following circumstances:
 - a. The location of the UDB shall be evaluated during preparation or update of a community plan.
 - b. All community UDBs should be reviewed on a five-year cycle to reflect changes in growth and development patterns.
 - c. A request for expansion of the UDB boundary can be applied for as part of a General Plan Amendment to the Land Use Diagram.
 - d. At the request of a special district or the community.
 - e. A UDB should be considered for expansion at such time as land for infill becomes limited. This condition is considered satisfied when 80 percent of the non-Williamson Act land within the UDB is developed for urban uses.
 - f. UDBs should not be expanded onto Prime Farmland if Farmland of Statewide Importance or of lesser quality is available and suitable for expansion.
2. Prior to approval of a UDB boundary expansion, the County shall ensure that infrastructure can be provided to serve the new areas added to the UDB and that sufficient water supplies are also available. This may require preparation of an infrastructure master plan that includes methods of financing of improvements and maintenance, as well as representation/documentation of availability and sufficiency of long-term water supplies.
3. Preservation of productive agricultural lands shall be the highest priority when considering modifications. Expansion of a UDB to include additional agricultural land shall only be allowed when other non-agricultural lands are not reasonably available to the community or are not suitable for expansion.

PF-2.3 UDB and Other Boundaries

The County shall provide notice and opportunity for special districts, school districts, and other service providers to comment when evaluating the expansion of a Community's UDB.

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PF-2.8 Inappropriate Land Use

Areas within UDBs are hereby set aside for those types of urban land uses, which benefit from urban services. Permanent uses, which do not benefit from such urban services, shall be discouraged within the UDBs. This is not intended to apply to agricultural or agricultural supported uses, including the cultivation of land or other uses accessory to the cultivation of land, provided that such accessory uses are time-limited through special use permit procedures.

Existing Adopted Land Use Plan

Existing Land Use Plan

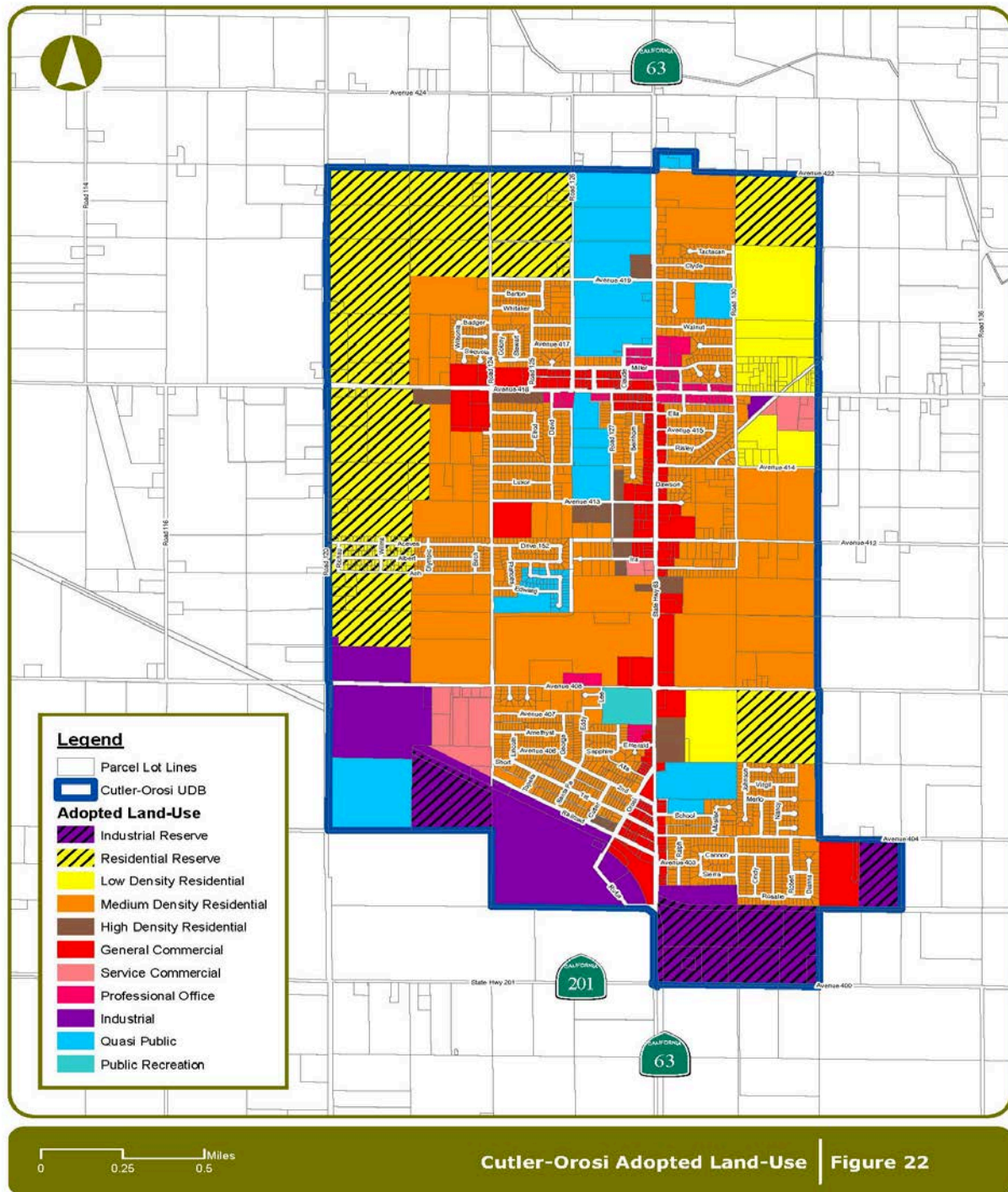
Table 32 land in the adopted Cutler/Orosi Community Planning area is designated Residential (1,014 acres). In total, there is about 2,412.3 acres of designated lands in the Cutler/Orosi Community Planning Area (see **Figure 22**). Approximately 231.3 acres within the plan area is dedicated to rights-of-way.

Table 32 – Adopted Land Use		
Designation	Total Acreage	Percentage
General Commercial	140.4	5.74
High Density Residential	38.7	1.58
Industrial	168.3	6.89
Industrial Reserve	135.0	5.53
Low Density Residential	125.1	5.12
Medium Density Residential	850.2	34.82
Professional Office	28.6	1.17
Public Recreation	11.9	0.49
Quasi-Public	201.1	8.23
Residential Reserve	459.9	18.83
Service Commercial	42.8	1.75
(blank)	8.3	0.34
Unclassified (Right-of-Way)	231.3	9.47
Total	2,441.9	100

Source: Tulare County GIS

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Figure 22 – Adopted Land Use Plan Map as Amended



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Existing Adopted Zoning Districts

Existing Zoning Districts

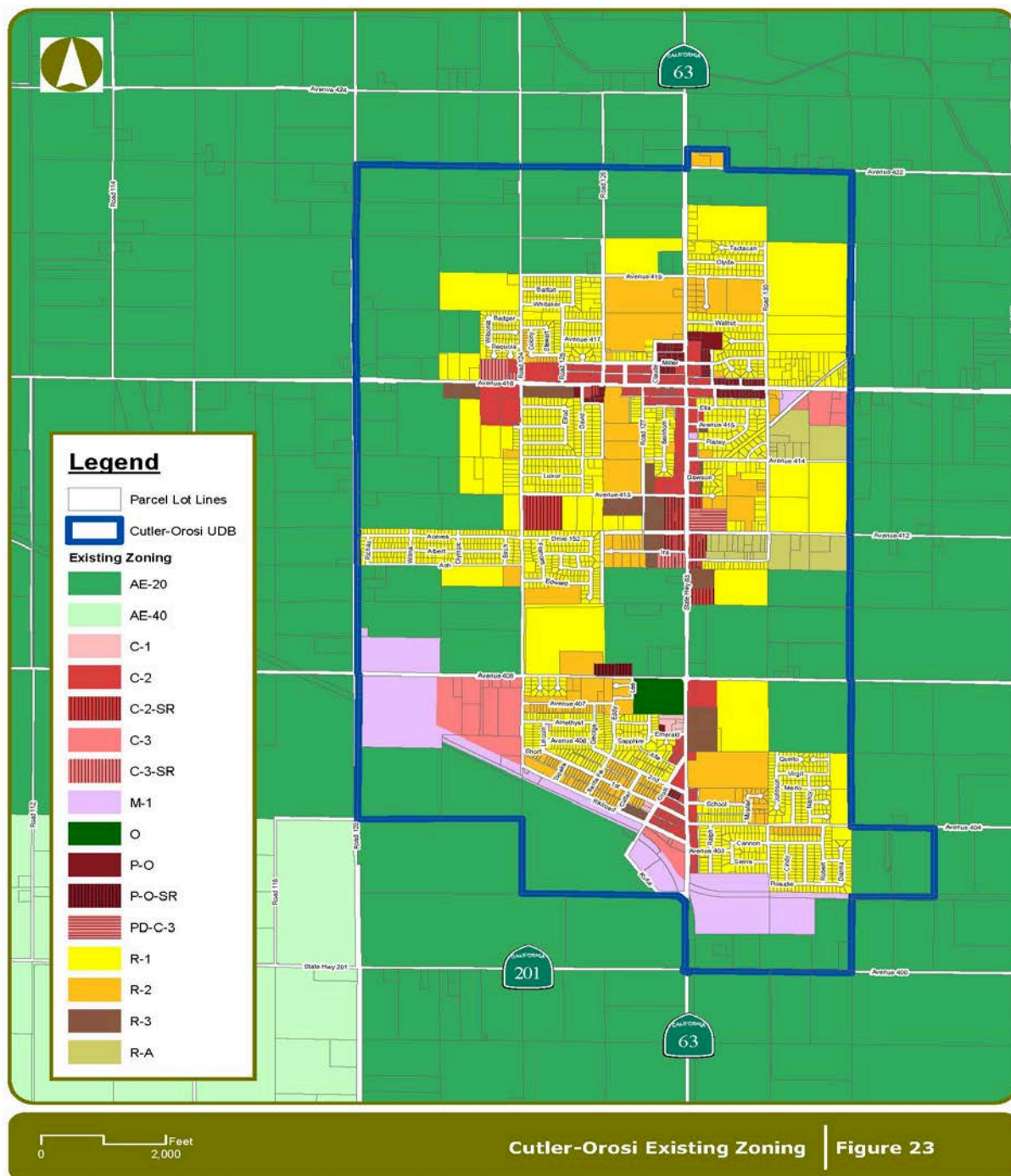
The zoning designations within the existing Cutler/Orosi Community Plan UDB are shown in **Figure 21** and demonstrated in **Table 33**.

Table 33 - Existing Zoning Districts		
Zoning Designations	Existing Acres	Percent
AE-20	956.9	39.1
C-1	3.5	0.1
C-2	69.8	2.8
C-2-SR	23.2	0.9
C-3	55.1	2.2
C-3-SR	7.5	0.3
M-1	130.8	5.3
O	11.9	0.4
PD-C-3	5.4	0.2
P-O	16.6	0.6
P-O-SR	3.1	0.1
R-1	644.3	26.3
R-2	189.1	7.7
R-3	36.3	1.4
R-A	50.3	2.0
Z	6.8	0.2
Unclassified (Right-of-Way)	231.3	9.4
Total	2,441.9	100

Source: Tulare County GIS

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Figure 23 - Existing Zoning Districts Map



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General Plan Policies

The following adopted policies from the Tulare County General Plan are applicable to the Cutler/Orosi Community Plan. Policies throughout the General Plan use the terminology “shall” and “should.” For the purposes of interpreting the policies in the General Plan, the term “shall” indicates a mandatory or required action or a duty to undertake an action unless the context indicates otherwise, in which case the term is synonymous with “should.” The term “should,” indicates a directive subject to discretion and requires at least review or consideration and, in that context, substantial compliance with the spirit or purpose of these General Plan policies. The term “may” indicates at the sole discretion of the County.

Land Use Policies

PF-1.3 Land Uses in UDBs/HDBs

The County shall encourage those types of urban land uses that benefit from urban services to develop within UDBs and HDBs. Permanent uses, which do not benefit from urban services shall be discouraged within these areas. This shall not apply to agricultural or agricultural support uses, including the cultivation of land or other uses accessory to the cultivation of land provided that such accessory uses are time-limited through Special Use Permit procedures.

PF-2.6 Land Use Consistency

The County shall require all community plans, when updated, to use the same land use designations as used in this Countywide General Plan (See Chapter 4-Land Use). All community plans shall also utilize a similar format and content. The content may change due to the new requirements such as Global Climate Change and Livable Community Concepts, as described on the table provided (Table 2.1: Community Plan Content). Changes to this format may be considered for unique and special circumstances as determined appropriate by the County. Until such time as a Community Plan is adopted for those communities without existing Community Plans, the land use designation

shall be Mixed Use, which promotes the integration of a compatible mix of residential types and densities, commercial uses, public facilities, services and employment opportunities.

PF-2.8 Inappropriate Land Use

Areas within UDBs are hereby set aside for those types of urban land uses, which benefit from urban services. Permanent uses, which do not benefit from such urban services shall be discouraged within the UDBs. This is not intended to apply to agricultural or agricultural supported uses, including the cultivation of land or other uses accessory to the cultivation of land, provided that such accessory uses are time-limited through special use permit procedures.

PF-4.7 Avoiding Isolating Unincorporated Areas

The County may oppose any annexation proposal that creates an island, peninsula, corridor, or irregular boundary. The County will also encourage the inclusion of unincorporated islands or peninsulas adjacent to proposed annexations.

LU-1.2 Innovative Development

The County shall promote flexibility and innovation through the use of planned unit developments, development agreements,

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specific plans, Mixed Use projects, and other innovative development and planning techniques.

LU-1.3 Prevent Incompatible Uses

The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.

LU-1.4 Compact Development

The County shall actively support the development of compact mixed-use projects that reduce travel distances.

LU-3.1 Residential Developments

The County shall encourage new major residential development to locate near existing infrastructure for employment centers, services, and recreation.

LU-3.2 Cluster Development

The County shall encourage proposed residential development to be clustered onto portions of the site that are more suitable to accommodating the development, and shall require access either directly onto a public road or via a privately maintained road designed to meet County road standards.

LU-3.3 High-Density Residential Locations

The County shall encourage high-density residential development (greater than 14 dwelling units per gross acre) to locate along collector roadways and transit routes, and near public facilities (e.g., schools, parks), shopping, recreation, and entertainment.

LU-3.4 Mountain, Rural, and Low-Density Residential

The Mountain, Rural, and Low-Density Residential development located outside of a

UDB shall be subject to the following requirements:

1. Able to meet the Rural Valley Lands Plan policies, Foothill Growth Management Plan policies, or Mountain Framework Plan policies and requirements,
2. Areas which qualify for minimum densities greater than 1 unit per 10 acres must meet the following characteristics (unless clustering is used):
 - a. Average slopes must be below a 30 percent grade,
 - b. Not identified as a moderate-to-high landslide hazard area, and
 - c. Access to new development is provided via an existing publicly maintained road or via a new road improved consistent with adopted County standards.

LU-1.8 Encourage Infill Development

The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

ED-2.11 Industrial Parks

As part of new or updated community plans, the County shall designate sites for industrial development to meet projected demand.

Housing Policy 3.24

When locating agricultural industry in rural areas, a determination should be made that there are transit opportunities and an adequate employment base living within a reasonable distance to the site.

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AQ-3.2 Infill near Employment

The County shall identify opportunities for infill development projects near employment areas within all unincorporated communities and hamlets to reduce vehicle trips.

AQ-3.6 Mixed Land Uses

The County shall encourage the clustering of land uses that generate high trip volumes, especially when such uses can be mixed with support services and where they can be served by public transportation.

PFS-8.4 Library Facilities and Services

The County shall encourage expansion of library facilities and services as necessary to meet the needs (e.g., internet access, meeting rooms, etc.) of future population growth.

Circulation Policies

Q-3.3 Street Design

The County shall promote street design that provides an environment, which encourages transit use, biking, and pedestrian movements.

LU-7.3 Friendly Streets

The County shall encourage new streets within UDBs to be designed and constructed to not only accommodate traffic, but also serve as comfortable pedestrian and cyclist environments. These should include, but not be limited to:

1. Street tree planting adjacent to curbs and between the street and sidewalk to provide a buffer between pedestrians and automobiles, where appropriate,
2. Minimize curb cuts along streets,
3. Sidewalks on both sides of streets, where feasible,
4. Bike lanes and walking paths, where feasible on collectors and arterials, and
5. Traffic calming devices such as roundabouts, bulb-outs at intersections,

traffic tables, and other comparable techniques.

LU-7.4 Streetscape Continuity

The County shall ensure that streetscape elements (e.g., street signs, trees, and furniture) maintain visual continuity and follow a common image for each community.

LU-1.10 Roadway Access

The County shall require access to public roadways for all new development.

SL-2.1 Designated Scenic Routes and Highways

The County shall protect views of natural and working landscapes along the County's highways and roads by maintaining a designated system of County scenic routes and State scenic highways by:

1. Requiring development within existing eligible State scenic highway corridors to adhere to land use and design standards and guidelines required by the State Scenic Highway Program,
2. Supporting and encouraging citizen initiatives working for formal designation of eligible segments of State Highway 198 and State Highway 190 as State scenic highways,
3. Formalizing a system of County scenic routes throughout the County (see Figure 7-1), and
4. Requiring development located within County scenic route corridors to adhere to local design guidelines and standards.

SL-4.1 Design of Highways

The County shall work with Caltrans and Tulare County Association of Governments (TCAG) to ensure that the design of SR 99 and other State Highways protects scenic resources and provide access to vistas of working and natural landscapes by:

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1. Limiting the construction of sound walls that block views of the County's landscapes (incorporate setbacks to sensitive land uses to avoid noise impacts whenever feasible),
2. Using regionally-appropriate trees and landscaping and incorporating existing landmark trees,
3. Preserving historic and cultural places and vistas,
4. Avoiding excessive cut and fill for roadways along State scenic highways and County scenic routes, and along areas exposed to a large viewing area, and
5. Promote highway safety by identifying appropriate areas for traffic pullouts and rest areas.

SL-4.2 Design of County Roads

The County's reinvestment in rural County roads outside urban areas should, in addition to meeting functional needs and safety needs, preserve the experience of traveling on the County's "country roads" by:

1. Maintaining narrow as possible rights-of-ways,
2. Limiting the amount of curbs, paved shoulders, and other "urban" edge improvements,
3. Preserving historic bridges and signage, and
4. Promote County road safety by identifying appropriate areas for traffic pullout.

TC-1.1 Provision of an Adequate Public Road Network

The County shall establish and maintain a public road network comprised of the major facilities illustrated on the Tulare County Road Systems to accommodate projected growth in traffic volume.

TC-1.2 County Improvement Standards

The County's public roadway system shall be built and maintained consistent with adopted County Improvement Standards, and the need and function of each roadway, within constraints of funding capacity.

TC-1.6 Intermodal Connectivity

The County shall ensure that, whenever possible, roadway, highway, and public transit systems will interconnect with other modes of transportation. Specifically, the County shall encourage the interaction of truck, rail, and air-freight/passenger movements.

TC-1.7 Intermodal Freight Villages

The County shall consider the appropriate placement of intermodal freight villages in locations within the Regional Growth Corridors.

TC-1.8 Promoting Operational Efficiency

The County shall give consideration to transportation programs that improve the operational efficiency of goods movement, especially those that enhance farm-to-market connectivity

TC-1.9 Highway Completion

The County shall support State and Federal capacity improvement programs for critical segments of the State Highway System. Priority shall be given to improvements to State Routes 65, 99, and 198, including widening and interchange projects in the County.

TC-1.10 Urban Interchanges

The County shall work with TCAG to upgrade State highway interchanges from rural to urban standards within UDBs.

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TC-1.11 Regionally Significant Intersections

To enhance safety and efficiency, the County shall work to limit the frequency of intersections along regionally significant corridors.

TC-1.12 Scenic Highways and Roads

The County shall work with appropriate agencies to support the designation of scenic highways and roads in the County.

TC-1.16 County Level of Service (LOS) Standards

The County shall strive to develop and manage its roadway system (both segments and intersections) to meet a LOS of “D” or better in accordance with the LOS definitions established by the Highway Capacity Manual.

TC-1.18 Balanced System

The County shall strive to meet transportation needs and maintain LOS standards through a balanced Multimodal Transportation Network that provides alternatives to the automobile.

TC-2.3 Amtrak Service

The County shall encourage Amtrak to add passenger service to the Union Pacific corridor in the County.

TC-4.7 Transit Ready Development

The County shall promote the reservation of transit stops in conjunction with development projects in likely or potential locations for future transit facilities.

TC-5.1 Bicycle/Pedestrian Trail System

The County shall coordinate with TCAG and other agencies to develop a Countywide integrated multi-purpose trail system that provides a linked network with access to recreational, cultural, and employment facilities, as well as offering a recreational

experience apart from that available at neighborhood and community parks.

TC-5.2 Consider Non-Motorized Modes in Planning and Development

The County shall consider incorporating facilities for non-motorized users, such as bike routes, sidewalks, and trails when constructing or improving transportation facilities and when reviewing new development proposals. For developments with 50 or more dwelling units or non-residential projects with an equivalent travel demand, the feasibility of such facilities shall be evaluated.

TC-5.3 Provisions for Bicycle Use

The County shall work with TCAG to encourage local government agencies and businesses to consider including bicycle access and provide safe bicycle parking facilities at office buildings, schools, shopping centers, and parks.

TC-5.4 Design Standards for Bicycle Routes

The County shall utilize the design standards adopted by Caltrans and as required by the Streets and Highway Code for the development, maintenance, and improvement of bicycle routes.

TC-5.5 Facilities

The County shall require the inclusion of bicycle support facilities, such as bike racks, for new major commercial or employment locations.

TC-5.7 Designated Bike Paths

The County shall support the creation and development of designated bike paths adjacent to or separate from commute corridors.

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TC-5.8 Multi-Use Trails

The County shall encourage the development of multi-use corridors (such as hiking, equestrian, and mountain biking) in open space areas, along power line transmission corridors, utility easements, rivers, creeks, abandoned railways, and irrigation canals.

TC-5.9 Existing Facilities

The County shall support the maintenance of existing bicycle and pedestrian facilities.

Housing Policies

LU-3.5 Rural Residential Designations

The County shall not re-zone any new areas for residential development in the RVLP area, unless it can be shown that other objectives, such as buffers and the relationship of the development to surrounding uses, can be achieved.

Housing Guiding Principle 1.1

Endeavor to improve opportunities for affordable housing in a wide range of housing types in the communities throughout the unincorporated area of the County.

Housing Policy 1.11

Encourage the development of a broad range of housing types to provide an opportunity of choice in the local housing market.

Housing Policy 1.13

Encourage the utilization of modular units, prefabricated units, and manufactured homes.

Housing Policy 1.14

Pursue an equitable distribution of future regional housing needs allocations, thereby providing a greater likelihood of assuring a balance between housing development and the location of employment opportunities.

Housing Policy 1.15

Encourage housing counseling programs for low-income homebuyers and homeowners.

Housing Policy 1.16

Review community plans and zoning to ensure they provide for adequate affordable residential development.

Housing Guiding Principle 1.2

Promote equal housing opportunities for all persons regardless of race, religion, sex, marital status, ancestry, national origin, color, family status, disability, or any other arbitrary basis.

Housing Guiding Principle 1.3

Strive to meet the housing needs of migrant and non-migrant farmworkers and their families with a suitable, affordable and satisfactory living environment.

Housing Policy 1.31

Encourage the provision of farmworker housing opportunities in conformance with the Employee Housing Act.

Housing Guiding Principle 1.4

Enhance and support emergency shelters and transitional and supportive housing programs that assist the homeless and others in need.

Housing Policy 1.51

Encourage the construction of new housing units for “special needs” groups, including senior citizens, large families, single heads of households, households of persons with physical and/or mental disabilities, minorities, farmworkers, and the homeless in close proximity to transit, services, and jobs.

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Housing Policy 1.52

Support and encourage the development and improvement of senior citizen group housing, convalescent homes and other continuous care facilities.

Housing Policy 1.55

Encourage development of rental housing for large families, as well as providing for other housing needs and types.

Housing Guiding Principle 1.6

Assess and amend County ordinances, standards, practices and procedures considered necessary to carry out the County's essential housing goal of the attainment of a suitable, affordable and satisfactory living environment for every present and future resident in unincorporated areas.

Housing Policy 2.14

Create and maintain a matrix of Infrastructure Development Priorities for Disadvantaged Unincorporated Communities in Tulare County thorough analysis and investigation of public infrastructure needs and deficits, pursuant to Action Program 9.

Housing Guiding Principle 2.2

Require proposed new housing developments located within the development boundaries of unincorporated communities to have the necessary infrastructure and capacity to support the development.

Housing Policy 2.21

Require all proposed housing within the development boundaries of unincorporated communities is either (1) served by community water and sewer, or (2) that physical conditions permit safe treatment of liquid waste by septic tank systems and the use of private wells.

Housing Guiding Principle 3.1

Encourage "smart growth" designed development that serves the unincorporated communities, the environment, and the economy of Tulare County.

Housing Policy 3.11

Support and coordinate with local economic development programs to encourage a "jobs to housing balance" throughout the unincorporated area.

Housing Policy 3.23

Prepare new and/or updated community plans that provide adequate sites for a variety of types of housing within the development boundaries of community.

Conservation Policies

AG-1.1 Primary Land Use

The County shall maintain agriculture as the primary land use in the valley region of the County, not only in recognition of the economic importance of agriculture, but also in terms of agriculture's real contribution to the conservation of open space and natural resources.

AG-1.4 Williamson Act in UDBs and HDBs

The County shall support non-renewal or cancellation processes that meet State law for lands within UDBs and HDBs.

AG-1.5 Substandard Williamson Act Parcels

The County may work to remove parcels that are less than 10 acres in Prime Farmland and less than 40 Acres in Non-Prime Farmland from Williamson Act Contracts (Williamson Act key term for Prime/Non-Prime).

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AG-1.6 Conservation Easements

The County shall consider developing an Agricultural Conservation Easement Program (ACEP) to help protect and preserve agricultural lands (including “Important Farmlands”), as defined in this Element. This program may require payment of an in-lieu fee sufficient to purchase a farmland conservation easement, farmland deed restriction, or other farmland conservation mechanism as a condition of approval for conservation of important agricultural land to non-agricultural use. If available, the ACEP shall be used for replacement lands determined to be of statewide significance (Prime or other Important Farmlands), or sensitive and necessary for the preservation of agricultural land, including land that may be a part of a community separator as part of a comprehensive program to establish community separators. The in-lieu fee or other conservation mechanism shall recognize the importance of land value and shall require equivalent mitigation.

AG-1.7 Preservation of Agricultural Lands

The County shall promote the preservation of its agricultural economic base and open space resources through the implementation of resource management programs such as the Williamson Act, Rural Valley Lands Plan, Foothill Growth Management Plan or similar types of strategies and the identification of growth boundaries for all urban areas located in the County.

AG-1.8 Agriculture within Urban Boundaries

The County shall not approve applications for preserves or regular Williamson Act contracts on lands located within a UDB and/or HDB unless it is demonstrated that the restriction of such land will not detrimentally affect the growth of the community involved for the

succeeding 10 years, that the property in question has special public values for open space, conservation, other comparable uses, or that the contract is consistent with the publicly desirable future use and control of the land in question. If proposed within a UDB of an incorporated city, the County shall give written notice to the affected city pursuant to Government Code §51233.

AG-1.10 Extension of Infrastructure into Agricultural Areas

The County shall oppose extension of urban services, such as sewer lines, water lines, or other urban infrastructure, into areas designated for agriculture use unless necessary to resolve a public health situation. Where necessary to address a public health issue, services should be located in public rights-of-way in order to prevent interference with agricultural operations and to provide ease of access for operation and maintenance. Service capacity and length of lines should be designed to prevent the conversion of agricultural lands into urban/suburban uses.

AG-1.11 Agricultural Buffers

The County shall examine the feasibility of employing agricultural buffers between agricultural and non-agricultural uses, and along the edges of UDBs and HDBs. Considering factors include the type of operation and chemicals used for spraying, building orientation, planting of trees for screening, location of existing and future rights-of-way (roads, railroads, canals, power lines, etc.), and unique site conditions.

AG-1.13 Agricultural Related Uses

The County shall allow agriculturally related uses, including value-added processing facilities by discretionary approvals in areas designated Valley or Foothill Agriculture, subject to the following criteria:

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1. The use shall provide a needed service to the surrounding agricultural area which cannot be provided more efficiently within urban areas or which requires location in a non-urban area because of unusual site requirements or operational characteristics;
2. The use shall not be sited on productive agricultural lands if less productive land is available in the vicinity;
3. The operational or physical characteristics of the use shall not have a significant adverse impact on water resources or the use or management of surrounding agricultural properties within at least one-quarter (1/4) mile radius;
4. A probable workforce should be located nearby or be readily available; and
5. For proposed value-added agricultural processing facilities, the evaluation under criterion “1” above shall consider the service requirements of the use and the capability and capacity of cities and unincorporated communities to provide the required services.

AG-1.16 Schools in Agricultural Zones

The County shall discourage the location of new schools in areas designated for agriculture, unless the School District agrees to the construction and maintenance of all necessary infrastructure impacted by the project.

AG-2.6 Biotechnology and Biofuels

The County shall encourage the location of industrial and research oriented businesses specializing in biotechnologies and biofuels that can enhance agricultural productivity, enhance food-processing activities in the County, provide for new agriculturally related products and markets, or otherwise enhance the agricultural sector in the County.

LU-7.12 Historic Buildings and Areas

The County shall encourage preservation of buildings and areas with special and recognized historic, architectural, or aesthetic value. New development should respect architecturally and historically significant buildings and areas. Landscaping, original roadways, sidewalks, and other public realm features of historic buildings or neighborhoods shall be restored or repaired wherever feasible.

LU-7.13 Preservation of Historical Buildings

The County shall encourage and support efforts by local preservation groups to identify and rehabilitate historically significant buildings.

LU-7.14 Contextual and Compatible Design

The County shall ensure that new development respects Tulare County’s heritage by requiring that development respond to its context, be compatible with the traditions and character of each community, and develop in an orderly fashion, which is compatible with the scale of surrounding structures.

LU-7.15 Energy Conservation

The County shall encourage the use of solar power and energy conservation building techniques in all new development.

LU-7.16 Water Conservation

The County shall encourage the inclusion of “extra-ordinary” water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development

Housing Guiding Principle 4.1

Support and encourage County ordinances, standards, practices and procedures that promote residential energy conservation.

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Housing Policy 4.13

Promote energy efficiency and water conservation.

Housing Policy 4.21

Promote energy conservation opportunities in new residential development.

Housing Policy 5.24

Encourage the development of suitable replacement housing when occupied housing units are demolished due to public action.

ERM-1.1 Protection of Rare and Endangered Species

The County shall ensure the protection of environmentally sensitive wildlife and plant life, including those species designated as rare, threatened, and/or endangered by State and/or Federal government, through compatible land use development.

ERM-1.2 Development in Environmentally Sensitive Areas

The County shall limit or modify proposed development within areas that contain sensitive habitat for special status species and direct development into less significant habitat areas. Development in natural habitats shall be controlled so as to minimize erosion and maximize beneficial vegetative growth.

ERM-1.3 Encourage Cluster Development

When reviewing development proposals, the County shall encourage cluster development in areas with moderate to high potential for sensitive habitat.

ERM-1.4 Protect Riparian Areas

The County shall protect riparian areas through habitat preservation, designation as

open space or recreational land uses, bank stabilization, and development controls.

ERM-1.5 Riparian Management Plans and Mining Reclamation Plans

The County shall require mining reclamation plans and other management plans to include measures that protect, maintain, and restore riparian resources and habitats.

ERM-1.6 Management of Wetlands

The County shall support the preservation and management of wetland and riparian plant communities for passive recreation, groundwater recharge, and wildlife habitats.

ERM-1.8 Open Space Buffers

The County shall require buffer areas between development projects and significant watercourses, riparian vegetation, wetlands, and other sensitive habitats and natural communities. These buffers should be sufficient to assure the continued existence of the waterways and riparian habitat in their natural state.

ERM-1.12 Management of Oak Woodland Communities

The County shall support the conservation and management of oak woodland communities and their habitats.

ERM-2.1 Conserve Mineral Deposits

The County will encourage the conservation of identified and/or potential mineral deposits, recognizing the need for identifying, permitting, and maintaining a 50-year supply of locally available PCC grade aggregate.

ERM-2.2 Recognize Mineral Deposits

The County will recognize as a part of the General Plan those areas of identified and/or potential mineral deposits.

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ERM-3.2 Limited Mining in Urban Areas

Within the County UDBs and HDBs, new commercial mining operations should be limited due to environmental and compatibility concerns.

ERM-3.3 Small-Scale Oil and Gas Extraction

The County shall allow by Special Use Permit small-scale oil and gas extraction activities and facilities that can be demonstrated to not have a significant adverse effect on surrounding or adjacent land and are within an established oil and gas field outside of a UDB.

ERM-3.4 Oil and Gas Extraction

Facilities related to oil and gas extraction and processing in the County may be allowed in identified oil and gas fields subject to a special use permit. The extraction shall demonstrate that it will be compatible with surrounding land uses and land use designations.

ERM-4.1 Energy Conservation and Efficiency Measures

The County shall encourage the use of solar energy, solar hot water panels, and other energy conservation and efficiency features in new construction and renovation of existing structures in accordance with State law.

ERM-4.2 Streetscape and Parking Area Improvements for Energy Conservation

The County shall promote the planting and maintenance of shade trees along streets and within parking areas of new urban development to reduce radiation heating.

ERM-5.20 Allowable Uses on Timber Production Lands

The County shall allow uses (not related to forest production) on lands designated

Resource Conservation in forestry production areas, provided it is demonstrated that:

1. They are compatible with forestry uses,
2. Will not interfere with forest practices,
3. Consider forest site productivity and minimize the loss of productive forest lands,
4. Will meet standards relating to the availability of fire protection, water supply, and waste disposal, and
5. Will not degrade the watershed and/or water quality due to increased erosion.

ERM-7.1 Soil Conservation

The County of Tulare shall establish the proper controls and ordinances for soil conservation.

WR-1.4 Conversion of Agricultural Water Resources

For new urban development, the County shall discourage the transfer of water used for agricultural purposes (within the prior ten years) for domestic consumption except in the following circumstances:

1. The water remaining for the agricultural operation is sufficient to maintain the land as an economically viable agricultural use,
2. The reduction in infiltration from agricultural activities as a source of groundwater recharge will not significantly impact the groundwater basin.

WR-1.5 Expand Use of Reclaimed Wastewater

To augment groundwater supplies and to conserve potable water for domestic purposes, the County shall seek opportunities to expand groundwater recharge efforts

WR-1.6 Expand Use of Reclaimed Water

The County shall encourage the use of tertiary treated wastewater and household gray water for irrigation of agricultural lands, recreation

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and open space areas, and large landscaped areas as a means of reducing demand for groundwater resources.

WR-3.3 Adequate Water Availability

The County shall review new development proposals to ensure the intensity and timing of growth will be consistent with the availability of adequate water supplies. Projects must submit a Will-Serve letter as part of the application process, and provide evidence of adequate and sustainable water availability prior to approval of the tentative map or other urban development entitlement.

HS-9.2 Walkable Communities

The County shall require where feasible, the development of parks, open space, sidewalks and walking and biking paths that promote physical activity and discourage automobile dependency in all future communities.

PF-1.4 Available Infrastructure

The County shall encourage urban development to locate in existing UDBs and HDBs where infrastructure is available or may be established in conjunction with development. The County shall ensure that development does not occur unless adequate infrastructure is available, that sufficient water supplies are available or can be made available, and that there are adequate provisions for long-term management and maintenance of infrastructure and identified water supplies.

Open-Space Policies

LU-2.3 Open Space Character

The County shall require that all new development requiring a County discretionary approval, including parcel and subdivision maps, be planned and designed to maintain the scenic open space character of open space resources including, but not limited to,

agricultural areas, rangeland, riparian areas, etc., within the view corridors of highways. New development shall utilize natural landforms and vegetation in the least visually disruptive way possible and use design, construction and maintenance techniques that minimize the visibility of structures on hilltops, hillsides, ridgelines, steep slopes, and canyons.

SL-1.3 Watercourses

The County shall protect visual access to, and the character of, Tulare County's scenic rivers, lakes, and irrigation canals by:

1. Locating and designing new development to minimize visual impacts and obstruction of views of scenic watercourses from public lands and right-of-ways, and
2. Maintaining the rural and natural character of landscape viewed from trails and watercourses used for public recreation.

ERM-5.1 Parks as Community Focal Points

The County shall strengthen the role of County parks as community focal points by providing community center/recreation buildings to new and existing parks, where feasible.

ERM-5.2 Park Amenities

The County shall provide a broad range of active and passive recreational opportunities within community parks. When possible, this should include active sports fields and facilities, community center/recreation buildings, children's play areas, multi-use areas and trails, sitting areas, and other specialized uses as appropriate.

ERM-5.3 Park Dedication Requirements

The County shall require the dedication of land and/or payment of fees, in accordance with local authority and State law (for example the Quimby Act), to ensure funding for the

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acquisition and development of public recreation facilities.

ERM-5.5 Collocated Facilities

The County shall encourage the development of parks near public facilities such as schools, community halls, libraries, museums, prehistoric sites, and open space areas and shall encourage joint-use agreements whenever possible.

ERM-5.6 Location and Size Criteria for Parks

Park types used in Tulare County are defined as follows:

- **Neighborhood Play Lots (Pocket Parks).** The smallest park type, these are typically included as part of a new development to serve the neighborhood in which they are contained. Typical size is one acre or less. If a park of this type is not accessible to the general public, it cannot be counted towards the park dedication requirements of the County. Pocket Parks can be found in communities, hamlets, and other unincorporated areas.
- **Neighborhood Parks.** Neighborhood parks typically contain a tot lot and playground for 2-5 year olds and 5-12 year olds, respectively, one basketball court or two half-courts, baseball field(s), an open grassy area for informal sports activities (for example, soccer), and meandering concrete paths that contain low-level lighting for walking or jogging. In addition, neighborhood parks typically have picnic tables and a small group picnic shelter. These park types are typically in the range of 2 to 15 acres and serve an area within a 1/2-mile radius. Neighborhood parks can be found in communities, hamlets, and other unincorporated areas.
- **Community Parks.** Community parks are designed to serve the needs of the

community as a whole. These facilities can contain the same facilities as the neighborhood park. In addition, these parks can contain sports facilities with night lighting, community centers, swimming pools, and facilities of special interest to the community. These parks are typically 15 to 40 acres in size and serve an area within a 2-mile radius. Community parks can be found in communities, planned community areas, and large hamlets.

- **Regional Parks.** Regional parks are facilities designed to address the needs of the County as a whole. These facilities may have an active recreation component (play area, group picnic area, etc.), but the majority of their area is maintained for passive recreation (such as hiking or horseback riding), and natural resource enjoyment. Regional parks are typically over 200 acres in size, but smaller facilities may be appropriate for specific sites of regional interest.

The following guidelines should be observed in creating and locating County parks:

1. The County shall strive to maintain an overall standard of five or more acres of County-owned improved parkland per 1,000 population in the unincorporated portions of the County,
2. Neighborhood play lots (pocket parks) are encouraged as part of new subdivision applications as a project amenity, but are not included in the calculation of dedication requirements for the project,
3. Neighborhood parks at three acres per 1,000 population, if adjoining an elementary school and six acres per 1,000 population if separate [ERME IV-C; Open Space; Policy 3; Pg. 101],
4. Community parks at one-acre per 1,000 population if adjoining a high school and two acres per 1,000 population if separate

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[ERME IV-C; Open Space; Policy 4; Pg. 101],

5. Regional parks at one-acre per 1,000 population,
- 6.
7. Only public park facilities shall be counted toward Countywide parkland standards, and
8. A quarter mile walking radius is the goal for neighborhood parks.

ERM-5.12 Meet Changing Recreational Needs

The County shall promote the continued and expanded use of national and State forests, parks, and other recreational areas to meet the recreational needs of County residents.

ERM-5.13 Funding for Recreational Areas and Facilities

The County shall support the continued maintenance and improvement of existing recreational facilities and expansion of new recreational facilities opportunities for County, State, and Federal lands. The County shall strive to obtain adequate funding to improve and maintain existing parks, as well as construct new facilities.

ERM-5.15 Open Space Preservation

The County shall preserve natural open space resources through the concentration of development in existing communities, use of cluster development techniques, maintaining large lot sizes in agricultural areas, discouraging conversion of lands currently used for agricultural production, limiting development in areas constrained by natural hazards, and encouraging agricultural and ranching interests to maintain natural habitat in open space areas where the terrain or soil is not conducive to agricultural production.

HS-9.1 Healthy Communities

To the maximum extent feasible, the County shall strive through its land use decisions to promote community health and safety for all neighborhoods in the County by encouraging patterns of development that are safe and influence crime prevention, promote a high-quality physical environment and encourage physical activity by means such as sidewalks and walking and biking paths that discourage automobile dependency in existing communities.

Noise Policies

HS-8.7 Inside Noise

The County shall ensure that in instances where the windows and doors must remain closed to achieve the required inside acoustical isolation, mechanical ventilation or air conditioning is provided.

HS-8.8 Adjacent Uses

The County shall not permit development of new industrial, commercial, or other noise-generating land uses if resulting noise levels will exceed 60 dB Ldn (or CNEL) at the boundary of areas designated and zoned for residential or other noise-sensitive uses, unless it is determined to be necessary to promote the public health, safety and welfare of the County.

HS-8.9 County Equipment

The County shall strive to purchase equipment that complies with noise level performance standards set forth in the Health and Safety Element.

HS-8.10 Automobile Noise Enforcement

The County shall encourage the CHP, Sheriff's office, and local police departments to actively enforce existing sections of the California Vehicle Code relating to adequate vehicle

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mufflers, modified exhaust systems, and other amplified noise.

HS-8.11 Peak Noise Generators

The County shall limit noise generating activities, such as construction, to hours of normal business operation (7 a.m. to 7 p.m.). No peak noise generating activities shall be allowed to occur outside of normal business hours without County approval.

HS-8.14 Sound Attenuation Features

The County shall require sound attenuation features such as walls, berming, heavy landscaping, between commercial, industrial, and residential uses to reduce noise and vibration impacts.

HS-8.15 Noise Buffering

The County shall require noise buffering or insulation in new development along major streets, highways, and railroad tracks.

Safety Policies

HS-1.1 Maintain Emergency Public Services

The County shall ensure that during natural catastrophes and emergency situations, the County can continue to provide essential emergency services.

HS-1.9 Emergency Access

The County shall require, where feasible, road networks (public and private) to provide for safe and ready access for emergency equipment and provide alternate routes for evacuation.

HS-1.10 Emergency Services Near Assisted Living Housing

In approving new facilities, such as nursing homes, housing for the elderly and other housing for the mentally and physically infirm, to the extent possible, the County shall ensure that such facilities are located within

reasonable distance of fire and law enforcement stations.

HS-5.2 Development in Floodplain Zones

The County shall regulate development in the 100-year floodplain zones as designated on maps prepared by FEMA in accordance with the following:

1. Critical facilities (those facilities which should be open and accessible during emergencies) shall not be permitted.
2. Passive recreational activities (those requiring non-intensive development, such as hiking, horseback riding, picnicking) are permissible.
3. New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-5.8 Road Location

The County shall plan and site new roads to minimize disturbances to banks and existing channels and avoid excessive cuts and accumulations of waste soil and vegetative debris near natural drainage ways.

HS-5.9 Floodplain Development Restrictions

The County shall ensure that riparian areas and drainage areas within 100-year floodplains are free from development that may adversely impact floodway capacity or characteristics of natural/riparian areas or natural groundwater recharge areas.

HS-5.10 Flood Control Design

The County shall evaluate flood control projects involving further channeling, straightening, or lining of waterways until alternative multipurpose modes of treatment,

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such as wider berms and landscaped levees, in combination with recreation amenities, are studied.

HS-5.11 Natural Design

The County shall encourage flood control designs that respect natural curves and vegetation of natural waterways while retaining dynamic flow and functional integrity.

HS-7.4 Upgrading for Streets and Highways

The County shall evaluate and upgrade vital streets and highways to an acceptable level for emergency services.

PFS-7.1 Fire Protection

The County shall strive to expand fire protection service in areas that experience growth in order to maintain adequate levels of service.

PFS-7.6 Provision of Station Facilities and Equipment

The County shall strive to provide sheriff and fire station facilities, equipment (engines and other apparatus), and staffing necessary to maintain the County's service goals. The County shall continue to cooperate with mutual aid providers to provide coverage throughout the County.

PFS-7.11 Locations of Fire and Sheriff Stations/Sub-stations

The County shall strive to locate fire and sheriff sub-stations in areas that ensure the minimum response times to service calls.

PFS-7.12 Design Features for Crime Prevention and Reduction

The County shall promote the use of building and site design features as means for crime prevention and reduction.

PFS-8.3 Location of School Sites

The County shall work with school districts and land developers to locate school sites consistent with current and future land uses. The County shall also encourage siting new schools near the residential areas that they serve and with access to safe pedestrian and bike routes to school.

Other

PF-2.7 Improvement Standards in Communities

The County shall require development within the designated UDBs to meet an urban standard for improvements. Typical improvements shall include curbs, gutters, sidewalks, and community sewer and water systems.

ERM-6.3 Alteration of Sites with Identified Cultural Resources

When planning any development or alteration of a site with identified cultural or archaeological resources, consideration should be given to ways of protecting the resources. Development should be permitted in these areas only after a site specific investigation has been conducted pursuant to CEQA to define the extent and value of resource, and mitigation measures proposed for any impacts the development may have on the resource.

HS-8.5 State Noise Standards

The County shall enforce the State Noise Insulation Standards (California Administrative Code, Title 24) and Chapter 35 of the Uniform Building Code (UBC). Title 24 requires that interior noise levels not exceed 45 dB Ldn (or CNEL) with the windows and doors closed within new developments of multi-family dwellings, condominiums, hotels, or motels. Where it is not possible to reduce exterior noise levels within an acceptable range

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the County shall require the application of noise reduction technology to reduce interior noise levels to an acceptable level.

HS-8.6 Noise Level Criteria

The County shall ensure noise level criteria applied to land uses other than residential or other noise-sensitive uses are consistent with the recommendations of the California Office of Noise Control (CONC).

WR-3.9 Establish Critical Water Supply Areas

The County shall designate Critical Water Supply Areas to include the specific areas used by a municipality or community for its water supply system, areas critical to groundwater recharge, and other areas possessing a vital role in the management of the water resources in the County.

PFS-6.1 Telecommunications Services

The County shall work with telecommunication providers to ensure that all residents and businesses have access to telecommunications services, including broadband internet service. To maximize access to inexpensive telecommunications services, the County shall encourage marketplace competition from multiple service providers.

PFS-8.4 Library Facilities and Services

The County shall encourage expansion of library facilities and services as necessary to meet the needs (e.g., internet access, meeting rooms, etc.) of future population growth.

PFS-8.5 Government Facilities in Community Centers

The County shall actively support development and expansion of federal, State, County, districts, and other governmental offices and facilities where infrastructure exists within community core areas.

S-9.1 Expansion of Gas and Electricity Facilities

The County shall coordinate with gas and electricity service providers to plan the expansion of gas and electrical facilities to meet the future needs of County residents.

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General Plan Policies that Relate to Health

PF-2.7 Improvement Standards in Communities

The County shall require development within the designated UDBs to meet an urban standard for improvements. Typical improvements shall include curbs, gutters, sidewalks, and community sewer and water systems.

Land Use Element

LU-1.1 Smart Growth and Healthy Communities

The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including:

1. Creating walkable neighborhoods,
2. Providing a mix of residential densities,
3. Creating a strong sense of place,
4. Mixing land uses,
5. Directing growth toward existing communities,
6. Building compactly,
7. Discouraging sprawl,
8. Encouraging infill,
9. Preserving open space,
10. Creating a range of housing opportunities and choices,
11. Utilizing planned community zoning to provide for the orderly pre-planning and long term development of large tracks of land which may contain a variety of land uses, but are under unified ownership or development control, and
12. Encouraging connectivity between new and existing development.

LU-1.2 Innovative Development

The County shall promote flexibility and innovation through the use of planned unit developments, development agreements, specific plans, Mixed Use projects, and other innovative development and planning techniques.

LU-1.3 Prevent Incompatible Uses

The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.

LU-1.4 Compact Development

The County shall actively support the development of compact mixed-use projects that reduce travel distances.

LU- 1.5 Paper Subdivision Consolidations

The County shall encourage consolidation of paper parcels/subdivisions, especially those lots that are designated Valley Agriculture (VA), Foothill Agriculture (FA), or Resource Conservation (RC), are irregular in shape, inadequate in size for proper use, or lack infrastructure.

LU-1.6 Permitting Procedures and Regulations

The County shall continue to ensure that its permitting procedures and regulations are consistent and efficient.

LU-1.7 Development on Slopes

The County shall require a preliminary soils report for development projects in areas with shallow or unstable soils or slopes in

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excess of 15 percent. If the preliminary soil report indicates soil conditions could be unstable, a detailed geologic/hydrologic report by a registered geologist, civil engineer, or engineering geologist shall be required demonstrating the suitability of any proposed or additional development.

LU-1.8 Encourage Infill Development

The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

LU-1.10 Roadway Access

The County shall require access to public roadways for all new development.

LU-3.1 Residential Developments

The County shall encourage new major residential development to locate near existing infrastructure for employment centers, services, and recreation.

LU-3.3 High-Density Residential Locations

The County shall encourage high-density residential development (greater than 14 dwelling units per gross acre) to locate along collector roadways and transit routes, and near public facilities (e.g., schools, parks), shopping, recreation, and entertainment.

LU-3.6 Project Design

The County shall require residential project design to consider natural features, noise exposure of residents, visibility of structures, circulation, access, and the relationship of the project to surrounding

uses. Residential densities and lot patterns will be determined by these and other factors. As a result, the maximum density specified by General Plan designations or zoning for a given parcel of land may not be attained.

LU-7.3 Friendly Streets

The County shall encourage new streets within UDBs to be designed and constructed to not only accommodate traffic, but also serve as comfortable pedestrian and cyclist environments. These should include, but not be limited to:

1. Street tree planting adjacent to curbs and between the street and sidewalk to provide a buffer between pedestrians and automobiles, where appropriate,
2. Minimize curb cuts along streets,
3. Sidewalks on both sides of streets, where feasible,
4. Bike lanes and walking paths, where feasible on collectors and arterials, and
5. Traffic calming devices such as roundabouts, bulb-outs at intersections, traffic tables, and other comparable techniques.

LU-7.5 Crime Prevention through Design

The County shall encourage design of open space areas, bicycle and pedestrian systems, and housing projects so that there is as much informal surveillance by people as possible to deter crime.

LU-7.15 Energy Conservation

The County shall encourage the use of solar power and energy conservation building techniques in all new development.

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LU-7.16 Water Conservation

The County shall encourage the inclusion of “extra-ordinary” water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development.

Housing Element

Housing Policy 1.51

Encourage the construction of new housing units for “special needs” groups, including senior citizens, large families, single heads of households, households of persons with physical and/or mental disabilities, minorities, farmworkers, and the homeless in close proximity to transit, services, and jobs.

Housing Policy 3.12

Support locally initiated programs to provide neighborhood parks and recreational facilities for residential areas within unincorporated communities.

Housing Policy 3.13

Encourage subdivision and housing unit design, which provides for a reasonable level of safety and security.

Housing Policy 3.16

Actively seek federal, state, and private foundation grant funds for park and recreation facilities in unincorporated areas, including dual-use storm drainage ponding basins/recreation parks.

Housing Policy 4.21

Promote energy conservation opportunities in new residential development.

Housing Policy 4.22

Enforce provisions of the Subdivision Map Act regulating energy-efficient subdivision design.

Housing Policy 5.21

Administer and enforce the relevant portions of the Health and Safety Code.

Housing Policy 5.26

Prohibit concentrations of dwelling units near potentially incompatible agricultural uses as defined in the Animal Confinement Facilities Plan.

Environmental Resources Element

ERM-5.2 Park Amenities

The County shall provide a broad range of active and passive recreational opportunities within community parks. When possible, this should include active sports fields and facilities, community center/recreation buildings, children’s play areas, multi-use areas and trails, sitting areas, and other specialized uses as appropriate.

Air Quality Element

AQ-1.3 Cumulative Air Quality Impacts

The County shall require development to be located, designed, and constructed in a manner that would minimize cumulative air quality impacts. Applicants shall be required to propose alternatives as part of the State CEQA process that reduce air emissions and enhance, rather than harm, the environment.

AQ-1.4 Air Quality Land Use Compatibility

The County shall evaluate the compatibility of industrial or other developments, which are likely to cause undesirable air pollution

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with regard to proximity to sensitive land uses, and wind direction and circulation in an effort to alleviate effects upon sensitive receptors.

AQ-1.7 Support Statewide Climate Change Solutions

The County shall monitor and support the efforts of Cal/EPA, CARB, and the SJVAPCD, under AB 32 (Health and Safety Code §38501 et seq.), to develop a recommended list of emission reduction strategies. As appropriate, the County will evaluate each new project under the updated General Plan to determine its consistency with the emission reduction strategies.

AQ-1.8 Greenhouse Gas Emissions Reduction Plan/Climate Action Plan

The County will develop a Greenhouse Gas Emissions Reduction Plan (Plan) that identifies greenhouse gas emissions within the County as well as ways to reduce those emissions. The Plan will incorporate the requirements adopted by the California Air Resources Board specific to this issue. In addition, the County will work with the Tulare County Association of Governments and other applicable agencies to include the following key items in the regional planning efforts.

1. Inventory all known, or reasonably discoverable, sources of greenhouse gases in the County,
2. Inventory the greenhouse gas emissions in the most current year available, and those projected for year 2020, and
3. Set a target for the reduction of emissions attributable to the County's discretionary land use decisions and its own internal government operations.

AQ-2.2 Indirect Source Review

The County shall require major development projects, as defined by the SJVAPCD, to reasonably mitigate air quality impacts associated with the project. The County shall notify developers of SJVAPCD Rule 9510 – Indirect Source Review requirements and work with SJVAPCD to determine mitigations, as feasible, that may include, but are not limited to the following:

1. Providing bicycle access and parking facilities,
2. Increasing density,
3. Encouraging mixed use developments,
4. Providing walkable and pedestrian-oriented neighborhoods,
5. Providing increased access to public transportation,
6. Providing preferential parking for high-occupancy vehicles, car pools, or alternative fuels vehicles, and
7. Establishing telecommuting programs or satellite work centers.

AQ-2.3 Transportation and Air Quality

When developing the regional transportation system, the County shall work with TCAG to comprehensively study methods of transportation, which may contribute to a reduction in air pollution in Tulare County. Some possible alternatives that should be studied are:

1. Commuter trains (Light Rail, Amtrak, or High Speed Rail) connecting with Sacramento, Los Angeles, and San Francisco, with attractive services scheduled up and down the Valley,
2. Public transportation such as buses and light rail, to serve between communities of the Valley, publicly subsidized if feasible,
3. Intermodal public transit such as buses provided with bicycle racks, bicycle

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parking at bus stations, bus service to train stations and airports, and park and ride facilities, and

4. Community transportation systems supportive of alternative transportation modes, such as cycling or walking trails, with particular attention to high-density areas.

AQ-3.2 Infill near Employment

The County shall identify opportunities for infill development projects near employment areas within all unincorporated communities and hamlets to reduce vehicle trips.

AQ-3.3 Street Design

The County shall promote street design that provides an environment, which encourages transit use, biking, and pedestrian movements.

AQ-3.4 Landscape

The County shall encourage the use of ecologically based landscape design principles that can improve local air quality by absorbing CO₂, producing oxygen, providing shade that reduces energy required for cooling, and filtering particulates. These principles include, but are not limited to, the incorporation of parks, landscaped medians, and landscaping within development.

AQ-3.5 Alternative Energy Design

The County shall encourage all new development, including rehabilitation, renovation, and redevelopment, to incorporate energy conservation and green building practices to maximum extent feasible. Such practices include, but are not limited to building orientation and shading, landscaping, and the use of active and passive solar heating and water systems.

AQ-3.6 Mixed Land Uses

The County shall encourage the clustering of land uses that generate high trip volumes, especially when such uses can be mixed with support services and where they can be served by public transportation.

Health and Safety Element

HS-1.4 Building and Codes

Except as otherwise allowed by State law, the County shall ensure that all new buildings intended for human habitation are designed in compliance with the latest edition of the California Building Code, California Fire Code, and other adopted standards based on risk (e.g., seismic hazards, flooding), type of occupancy, and location (e.g., floodplain, fault).

HS-1.5 Hazard Awareness and Public Education

The County shall continue to promote awareness and education among residents regarding possible natural hazards, including soil conditions, earthquakes, flooding, fire hazards, and emergency procedures.

HS-1.6 Public Safety Programs

The County shall promote public safety programs, including neighborhood watch programs, child identification and fingerprinting, public awareness and prevention of fire hazards, and other public education efforts.

HS-1.7 Safe Housing and Structures

The County shall continue to seek grant funding for the rehabilitation of deteriorated and dilapidated structures and provide available information regarding housing programs and other public services.

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HS-1.9 Emergency Access

The County shall require, where feasible, road networks (public and private) to provide for safe and ready access for emergency equipment and provide alternate routes for evacuation.

HS-1.10 Emergency Services near Assisted Living Housing

In approving new facilities, such as nursing homes, housing for the elderly and other housing for the mentally and physically infirm, to the extent possible, the County shall ensure that such facilities are located within reasonable distance of fire and law enforcement stations.

HS-4.3 Incompatible Land Uses

The County shall prevent incompatible land uses near properties that produce or store hazardous waste.

HS-4.4 Contamination Prevention

The County shall review new development proposals to protect soils, air quality, surface water, and groundwater from hazardous materials contamination.

HS-4.5 Increase Public Awareness

The County shall work to educate the public about household hazardous waste and the proper method of disposal.

HS-4.6 Pesticide Control

The County shall monitor studies of pesticide use and the effects of pesticide on residents and wildlife and require mitigation of the effects wherever feasible and appropriate.

HS-4.8 Hazardous Materials Studies

The County shall ensure that the proponents of new development projects address hazardous materials concerns

through the preparation of Phase I or Phase II hazardous materials studies for each identified site as part of the design phase for each project. Recommendations required to satisfy federal or State cleanup standards outlined in the studies will be implemented as part of the construction phase for each project.

HS-5.1 Development Compliance with Federal, State, and Local Regulations

The County shall ensure that all development within the designated floodway or floodplain zones conforms with FEMA regulations and the Tulare County Flood Damage Prevention Ordinance.

New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-5.2 Development in Floodplain Zones

The County shall regulate development in the 100-year floodplain zones as designated on maps prepared by FEMA in accordance with the following:

4. Critical facilities (those facilities, which should be open and accessible during emergencies) shall not be permitted.
5. Passive recreational activities (those requiring non-intensive development, such as hiking, horseback riding, picnicking) are permissible.

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6. New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-8.5 State Noise Standards

The County shall enforce the State Noise Insulation Standards (California Administrative Code, Title 24) and Chapter 35 of the Uniform Building Code (UBC). Title 24 requires that interior noise levels not exceed 45 dB Ldn (or CNEL) with the windows and doors closed within new developments of multi-family dwellings, condominiums, hotels, or motels. Where it is not possible to reduce exterior noise levels within an acceptable range, the County shall require the application of noise reduction technology to reduce interior noise levels to an acceptable level.

HS-9.1 Healthy Communities

To the maximum extent feasible, the County shall strive through its land use decisions to promote community health and safety for all neighborhoods in the County by encouraging patterns of development that are safe and influence crime prevention, promote a high-quality physical environment and encourage physical activity by means such as sidewalks and walking and biking paths that discourage automobile dependency in existing communities.

HS-9.2 Walkable Communities

The County shall require where feasible, the development of parks, open space, sidewalks and walking and biking paths that promote physical activity and discourage automobile dependency in all future communities.

Water Resource Element

WR-2.1 Protect Water Quality

All major land use and development plans shall be evaluated as to their potential to create surface and groundwater contamination hazards from point and non-point sources. The County shall confer with other appropriate agencies, as necessary, to assure adequate water quality review to prevent soil erosion; direct discharge of potentially harmful substances; ground leaching from storage of raw materials, petroleum products, or wastes; floating debris; and runoff from the site.

WR-2.2 National Pollutant Discharge Elimination System (NPDES) Enforcement

The County shall continue to support the State in monitoring and enforcing provisions to control non-point source water pollution contained in the U.S. EPA NPDES program as implemented by the Water Quality Control Board.

WR-2.3 Best Management Practices (BMPs)

The County shall continue to require the use of feasible BMPs and other mitigation measures designed to protect surface water and groundwater from the adverse effects of construction activities, agricultural operations requiring a County Permit and urban runoff in coordination with the Water Quality Control Board.

Transportation and Circulation Element

TC-1.18 Balanced System

The County shall strive to meet transportation needs and maintain LOS standards through a balanced Multimodal

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Transportation Network that provides alternatives to the automobile.

TC-1.19 Balanced Funding

The County shall promote a balanced approach to the allocation of transportation funds to optimize the overall County transportation system.

TC-4.1 Transportation Programs

The County shall support the continued coordination of transportation programs provided by social service agencies, particularly those serving elderly and/or handicapped.

TC-4.2 Determine Transit Needs

The County will continue to work with TCAG, cities, and communities in the County to evaluate and respond to public transportation needs.

TC-4.3 Support Tulare County Area Transit

The County shall request the support of TCAG for development of transit services outlined in the County's Transit Development Plan (TDP). Efforts to expand Tulare County Area Transit should be directed towards:

1. Encouraging new and improving existing transportation services for the elderly and disabled, and
2. Providing intercommunity services between unincorporated communities and cities.

TC-4.4 Nodal Land Use Patterns that Support Public Transit

The County shall encourage land uses that generate higher ridership including; high density residential, employment centers, schools, personal services, administrative and professional offices, and

social/recreational centers, to be clustered within a convenient walking distance of one another.

TC-5.1 Bicycle/Pedestrian Trail System

The County shall coordinate with TCAG and other agencies to develop a Countywide integrated multi-purpose trail system that provides a linked network with access to recreational, cultural, and employment facilities, as well as offering a recreational experience apart from that available at neighborhood and community parks.

TC-5.2 Consider Non-Motorized Modes in Planning and Development

The County shall consider incorporating facilities for non-motorized users, such as bike routes, sidewalks, and trails when constructing or improving transportation facilities and when reviewing new development proposals. For developments with 50 or more dwelling units or non-residential projects with an equivalent travel demand, the feasibility of such facilities shall be evaluated.

TC-5.3 Provisions for Bicycle Use

The County shall work with TCAG to encourage local government agencies and businesses to consider including bicycle access and provide safe bicycle parking facilities at office buildings, schools, shopping centers, and parks.

Public Facilities and Services Element

PFS-1.1 Existing Development

The County shall generally give priority for the maintenance and upgrading of County-owned and operated facilities and services to existing development in order to prevent

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the deterioration of existing levels-of-service.

PFS-1.3 Impact Mitigation

The County shall review development proposals for their impacts on infrastructure (for example, sewer, water, fire stations, libraries, streets, etc). New development shall be required to pay its proportionate share of the costs of infrastructure improvements required to serve the project to the extent permitted by State law. The lack of available public or private services or adequate infrastructure to serve a project, which cannot be satisfactorily mitigated by the project, may be grounds for denial of a project or cause for the modification of size, density, and/or intensity of the project.

PFS-1.4 Standards of Approval

The County should not approve any development unless the following conditions are met:

1. The applicant can demonstrate all necessary infrastructure will be installed and adequately financed,
2. Infrastructure improvements are consistent with adopted County infrastructure plans and standards, and
3. Funding mechanisms are provided to maintain, operate, and upgrade the facilities throughout the life of the project.

PFS-1.5 Funding for Public Facilities

The County shall implement programs and/or procedures to ensure that funding mechanisms necessary to adequately cover the costs related to planning, capital improvements, maintenance, and operations of necessary public facilities and services are in place, whether provided by the County or another entity.

PFS-1.6 Funding Mechanisms

The County shall use a wide range of funding mechanisms, such as the following, to adequately fund capital improvements, maintenance, and on-going operations for publicly owned and/or operated facilities:

1. Establishing appropriate development impact fees,
2. Establishing assessment districts, and
3. Pursuing grant funding.

PFS-1.7 Coordination with Service Providers

The County shall work with special districts, community service districts, public utility districts, mutual water companies, private water purveyors, sanitary districts, and sewer maintenance districts to provide adequate public facilities and to plan/coordinate, as appropriate, future utility corridors in an effort to minimize future land use conflicts.

PFS-1.8 Funding for Service Providers

The County shall encourage special districts, including community service districts and public utility districts to:

1. Institute impact fees and assessment districts to finance improvements,
2. Take on additional responsibilities for services and facilities within their jurisdictional boundaries up to the full extent allowed under State law, and
3. Investigate feasibility of consolidating services with other districts and annexing systems in proximity to promote economies of scale, such as annexation to city systems and regional wastewater treatment systems.

PFS-1.9 New Special Districts

When feasible, the County shall support the establishment of new special districts, including community service districts and

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public utility districts, to assume responsibility for public facilities and services.

PFS-1.10 Homeowner Associations

The County shall support the creation of homeowner associations, condominium associations, or other equivalent organizations to assume responsibility for specific public facilities and services.

PFS-1.11 Facility Sizing

The County shall ensure that publicly-owned and operated facilities are designed to meet the projected capacity needed in their service area to avoid the need for future replacement to achieve upsizing. For facilities subject to incremental sizing, the initial design shall include adequate land area and any other elements to easily expand in the future.

PFS-1.12 Security

The County shall seek to minimize vulnerability of public facilities to natural and man-made hazards and threats.

PFS-2.1 Water Supply

The County shall work with agencies providing water service to ensure that there is an adequate quantity and quality of water for all uses, including water for fire protection, by, at a minimum, requiring a demonstration by the agency providing water service of sufficient and reliable water supplies and water management measures for proposed urban development.

PFS-2.2 Adequate Systems

The County shall review new development proposals to ensure that the intensity and timing of growth will be consistent with the availability of adequate production and delivery systems. Projects must provide

evidence of adequate system capacity prior to approval.

PFS-2.3 Well Testing

The County shall require new development that includes the use of water wells to be accompanied by evidence that the site can produce the required volume of water without impacting the ability of existing wells to meet their needs.

PFS-2.4 Water Connections

The County shall require all new development in UDBs, UABs, Community Plans, Hamlet Plans, Planned Communities, Corridor Areas, Area Plans, existing water district service areas, or zones of benefit, to connect to the community water system, where such system exists. The County may grant exceptions in extraordinary circumstances, but in these cases, the new development shall be required to connect to the water system when service becomes readily available.

PFS-2.5 New Systems or Individual Wells

Where connection to a community water system is not feasible per PFS-2.4: Water Connections, service by individual wells or new community systems may be allowed if the water source meets standards for quality and quantity.

PFS-3.1 Private Sewage Disposal Standards

The County shall maintain adequate standards for private sewage disposal systems (e.g., septic tanks) to protect water quality and public health.

PFS-3.2 Adequate Capacity

The County shall require development proposals to ensure the intensity and timing

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of growth is consistent with the availability of adequate wastewater treatment and disposal capacity.

PFS-3.3 New Development Requirements

The County shall require all new development, within UDBs, UABs, Community Plans, Hamlet Plans, Planned Communities, Corridor Areas, Area Plans, existing wastewater district service areas, or zones of benefit, to connect to the wastewater system, where such systems exist. The County may grant exceptions in extraordinary circumstances, but in these cases, the new development shall be required to connect to the wastewater system when service becomes readily available.

PFS-3.4 Alternative Rural Wastewater Systems

The County shall consider alternative rural wastewater systems for areas outside of community UDBs and HDBs that do not have current systems or system capacity. For individual users, such systems include elevated leach fields, sand filtration systems, evapotranspiration beds, osmosis units, and holding tanks. For larger generators or groups of users, alternative systems, including communal septic tank/leach field systems, package treatment plants, lagoon systems, and land treatment, can be considered.

PFS-3.7 Financing

The County shall cooperate with special districts when applying for State and federal funding for major wastewater related expansions/upgrades when such plans promote the efficient solution to wastewater treatment needs for the area and County.

PFS-4.2 Site Improvements

The County shall ensure that new development in UDBs, UABs, Community Plans, Hamlet Plans, Planned Communities, Corridor Areas, and Area Plans includes adequate stormwater drainage systems. This includes adequate capture, transport, and detention/retention of stormwater.

PFS-4.3 Development Requirements

The County shall encourage project designs that minimize drainage concentrations and impervious coverage, avoid floodplain areas, and where feasible, provide a natural watercourse appearance.

PFS-4.4 Stormwater Retention Facilities

The County shall require on-site detention/retention facilities and velocity reducers when necessary to maintain existing (pre-development) storm flows and velocities in natural drainage systems. The County shall encourage the multi-purpose design of these facilities to aid in active groundwater recharge.

PFS-4.5 Detention/Retention Basins Design

The County shall require that stormwater detention/retention basins be visually unobtrusive and provide a secondary use, such as recreation, when feasible.

PFS-4.6 Agency Coordination

The County shall work with the Army Corps of Engineers and other appropriate agencies to develop stormwater detention/retention facilities and recharge facilities that enhance flood protection and improve groundwater recharge.

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PFS-4.7 NPDES Enforcement

Fire Staffing and Responses Time Standards			
	Demographics	Staffing/ Response Time	% of Calls
Urban	>1,000 people/sq. mi.	15 FF/9 min.	90
Suburban	500-100 people/sq. mi.	10 FF/10 min.	80
Rural	<500 people/sq. mi.	6 FF/14 min.	80
Remote*	Travel Dist.>8 min.	4 FF/no specific response time	90

*Upon assembling the necessary resources at the emergency scene, the fire department should have the capacity to safety commence an initial attack within 2 minutes, 90% of the time.

FF: fire fighters

The County shall continue to monitor and enforce provisions to control non-point source water pollution contained in the U.S. Environmental Protection Agency National Pollution Discharge Elimination System (NPDES) program.

PFS-5.1 Land Use Compatibility with Solid Waste Facilities

The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.

PFS-5.8 Hazardous Waste Disposal Capabilities

The County shall require the proper disposal and recycling of hazardous materials in accordance with the County's Hazardous Waste Management Plan.

PFS-7.2 Fire Protection Standards

The County shall require all new development to be adequately served by water supplies, storage, and conveyance facilities supplying adequate volume, pressure, and capacity for fire protection.

PFS-7.5 Fire Staffing and Response Time Standards

The County shall strive to maintain fire department staffing and response time goals consistent with National Fire Protection Association (NFPA) standards.

PFS-7.12 Design Features for Crime Prevention and Reduction

The County shall promote the use of building and site design features as means for crime prevention and reduction.

PFS-8.2 Joint Use Facilities and Programs

The County shall encourage the development of joint school facilities, recreation facilities, and educational and service programs between school districts and other public agencies.

PFS-8.3 Location of School Sites

The County shall work with school districts and land developers to locate school sites consistent with current and future land uses. The County shall also encourage siting new schools near the residential areas that they serve and with access to safe pedestrian and bike routes to school.

PFS-9.1 Expansion of Gas and Electricity Facilities

The County shall coordinate with gas and electricity service providers to plan the expansion of gas and electrical facilities to meet the future needs of County residents.

PFS-9.2 Appropriate Siting of Natural Gas and Electric Systems

The County shall coordinate with natural gas and electricity service providers to locate and design gas and electric systems that minimize impacts to existing and future residents.

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PFS-9.3 Transmission Corridors

The County shall work with the Public Utilities Commission and power utilities so that transmission corridors meet the following minimum requirements:

1. Transmission corridors shall be located to avoid health impacts on residential lands and sensitive receptors, and
2. Transmission corridors shall not impact the economic use of adjacent properties.

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Goals, Objectives, and Policies specific to Cutler/Orosi

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements, which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives.

Taken as a whole, goals, objectives and policies provide the guidelines as to how the community is to grow in terms of type, quantity and quality of development. The goals, objectives and policies are an integral part of the plan itself and the final land use map and plan description must reflect the goals, objectives and policies of the community. The plan map then is a graphic portrayal of what goals, objectives and policies are intended to accomplish.

The goals, objectives and policies of this community plan are divided into four categories. The four areas are: Community Development, Housing, Economic Base, and Environmental Quality.

****** Draft Ground Water Management Act Policy ******

Goal: That the County and Kings River East Groundwater Sustainability Agency work collaboratively under the Tulare County General Plan to assist the Cutler PUD and Orosi PUD in establishing conservation measures and credits in order to sustainably grow water and sewer infrastructure consistent with the Projected Growth Rates considered in the General Plan of Tulare County.

Objective: To not inhibit the Cutler/Orosi Community Plan projected growth rates due to the Groundwater Management Act. Instead to use thoughtful localized conservation measures, funding and credits for storm water retention / groundwater reclamation that can be utilized by the School District and PUD's in order meet the demands of the Kings River East GSA and the Management Area.

Policy 1: The County as a member of the Kings River East GSA will use its role as a GSA member to assist the Cutler PUD, Orosi PUD, and Cutler/Orosi Joint Unified School District to enhance and establish conservation measures that reduce the demand requirements consistent with previous drought measures (2014-15) that are still the law of California, and under the precepts of SGMA.

Policy 2: The Cutler PUD, the Orosi PUD, and Cutler/Orosi Joint Unified School District should work with the Kings River East GSA to establish credits for storm water retention / ground water reclamation consistent with the County General Plan.

Policy 3: The Cutler PUD and the Orosi PUD seek and be given credits consistent with the Kings River East GSA's ultimate definitions of available reclamation credits for recycled wastewater effluent land applications.

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Policy 4: That the Cutler/Orosi Joint Unified School District be given credits for any ground water reclamation they can supply through utilization of their existing storm water detention basins.

Community Development

GOAL I: Foster a cohesive community with easy access to necessary services and support facilities

Objective: Prevent premature urban-type development on agriculturally productive lands.

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.
2. The County shall carefully coordinate the extension of water and sewer services in the Plan Area with the Cutler PUD and the Orosi PUD to promote orderly and efficient development patterns.

GOAL II: Avoid land use conflicts through planning separation of uses.

Objective: Promote concentrations of similar or compatible uses.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.
3. The County shall ensure that new development respects Tulare County's heritage by requiring that development

respond to its context, be compatible with the traditions and character of each community, and develop in an orderly fashion, which is compatible with the scale of surrounding structures.

4. The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.
5. Land well suited for industrial development because of access, availability of infrastructure and proximity to similar land uses should be designated for industry and protected from the encroachment of incompatible uses.
6. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.
7. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
8. Locate high-density residential uses in close proximity to planned shopping areas.
9. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
10. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
11. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.

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12. The County shall work with the Cutler/Orosi Unified School District in facilitating the location and establishment of new school sites, or expansion of existing sites, as needed.
13. The County shall work with the Cutler/Orosi School District to provide safe routes to school.
14. The County will solicit recommendations from all interested public agencies on matters regarding the Cutler/Orosi Community Plan.
15. The Cutler/Orosi Community Plan should be reviewed every five years to determine if amendments are appropriate.
16. When considering any land use proposal, capital expenditure or other matters of community importance, the County will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

Objective: Encourage land uses adjacent to State Route 63 and Avenue 416 which are consistent with noise impacts.

Policies:

1. Encourage commercial and/or industrial development to locate adjacent to SR 216 where access is appropriate for such development.
2. Require installation of walls, berms or heavy planting along SR 63 in conjunction with any new residential development.

3. Discourage new residential development on vacant lots within areas proposed for commercial and industrial development.
4. Encourage the eventual conversion of existing residential uses within areas proposed for commercial and industrial development to nonresidential uses; except living quarters used in conjunction with a business.
5. Phase-out existing non-conforming commercial and industrial uses within planned residential areas by zoning such areas residential, and by enforcement of local zoning regulations pertaining to illegal buildings and uses.
6. The County shall designate and zone sufficient amounts of land to accommodate existing and projected industrial, commercial, residential, and public (e.g., parks and recreational) needs of the community.
7. Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

GOAL III: Achieve development densities consistent with levels of available service.

Objective: Urbanization in the planning areas should be contiguous and compact.

Policies:

1. The County shall encourage high-density residential development (greater than 14 dwelling units per gross acre) to locate along collector roadways and transit routes, and near public facilities (e.g., schools, parks), shopping, recreation, and entertainment.
2. The County shall review development proposals for their impacts on infrastructure (for example, sewer, water, fire stations, libraries, streets, etc). New development shall be required to pay its proportionate

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share of the costs of infrastructure improvements required to serve the project to the extent permitted by State law. The lack of available public or private services or adequate infrastructure to serve a project, which cannot be satisfactorily mitigated by the project, may be grounds for denial of a project or cause for the modification of size, density, and/or intensity of the project.

3. The extension of water and sewer facilities into the planning area shall be coordinated with the policies of this Plan and the goals and policies of the Tulare County General Plan. Development in the planning area shall pay their fair share for services.

Objective: Encourage merger of existing vacant substandard lots within the townsite of Cutler/Orosi.

Policies:

1. Conduct a study of the Cutler/Orosi townsite area to determine the impact of a comprehensive vacant lot merger action and undertake such a merger, if feasible.

GOAL IV: Coordinate Community Development Decisions with the Cutler PUD and Orosi PUD.

Objective: Ensure that all development can be served by the Cutler Public Utility District (PUD) and Orosi PUD during the planning period.

Policies:

1. Coordinate zoning with availability of utilities and community services.
2. Promote commercial and industrial development with wastewater discharge characteristics, which can be accommodated by the Cutler PUD and Orosi PUD.
3. Encourage industries with excessive effluent to pre-treat Cutler/Orosi wastewater system.

4. Encourage coordination between developers and the Cutler/Orosi throughout the application and development process to prevent time delays and to assure that the Cutler/Orosi can accommodate the needs of any proposed development.
5. Before the issuance of any land use permit, the Tulare County Resource Management Agency must receive confirmation from the Cutler/Orosi Wastewater Treatment Plant that water and sewer service requirements can be accommodated.
6. Assist the Cutler/Orosi Wastewater Treatment Plant in applications for grant funds to carry out their capital improvement program for providing, maintaining and improving their sewer and water systems to serve new and existing developments, which implement the goals and objectives of this Plan and of the Tulare County General Plan.
7. Prohibit to the extent allowed by law all development from holding, diverting and/or disposing of storm water run-off at locations, or in such a manner, as to cause groundwater recharge contributable to raising the groundwater to an unsafe level in the vicinity of the Cutler/ Orosi wastewater treatment facilities.
8. Investigate the necessity of preparing a drainage plan, within five years of adoption of the Community Plan, for diverting and disposing of storm water runoff and excess irrigation water at a location, or locations, where the retention or disposition of such water will not contribute to raising the groundwater level in the vicinity of the Cutler/Orosi wastewater treatment facilities.
9. Before the issuance of any land use permit, the Tulare County Economic and Planning Department will require all project applications for new development or redevelopment to include storm water disposal plans in accordance with the recommendations of the Tulare County

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Public Works Department and Caltrans to prevent runoff flows into the State highway rights-of-way.

Housing

GOAL V: Provide safer and adequate housing for all citizens within the community.

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Through the Housing Element process, Tulare County shall strive to minimize or eliminate blight in Cutler/Orosi.
2. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
3. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
4. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
5. The County will strive to ensure that there is an adequate amount of planned residential land to meet the housing needs of Cutler/Orosi.
6. The County will ensure that there are adequate sites and will work with the Cutler PUD and Orosi PUD and other agencies to ensure that there are adequate public facilities to support future housing needs in Cutler/Orosi.
7. The County will work diligently towards the rehabilitation of the housing stock in Cutler/Orosi.
8. The County will attempt to maintain a balance between owner and renter-occupied housing stock in Cutler/Orosi.

9. Sites for multi-family development shall be identified which do not overburden any one area of the community or neighborhood. Large developments should be located on collector or arterial streets.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Cutler/Orosi.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.

Objective: Provide a role for mobile homes in satisfying community-housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.

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4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.

Economic Base

GOAL VI: Develop a strong and diversified economy.

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. New service commercial uses should be located away from existing or planned residential areas or mitigation measures should be incorporated into the design of the project that will eliminate any undesirable conditions.
4. The County shall encourage industrialization in Cutler/Orosi, especially industries that provide non-seasonal employment, and which require close freeway access.
5. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

Policies:

1. Encourage the Cutler PUD and Orosi PUD to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

Environmental Quality and Public Safety

GOAL VII:

Preserve and enhance the quality of life for present and future generation of Cutler/Orosi citizens.

Objective: Upgrade the level of community health, sanitation and safety.

Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas, which will upgrade the community image and improve safety.
2. The County shall, within its authority, protect the public from danger to life and property caused by fire.
3. The County shall, within its authority, protect the public against crime against people and property.

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Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Protect Agricultural Lands:

1. Land within the respective Urban Development Boundary of Cutler/Orosi, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.
2. The following criteria shall be used to determine when conversion to urban use is appropriate:
 - a. The property is not subject to an agricultural preserve contract;
 - b. Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
 - c. At least 30% the property boundaries are contiguous on at least one side to existing urban development.
3. Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.
4. Agricultural uses outside the UDB shall be protected from conflicting urban uses by aligning the UDB along streets, canals or other man-made or natural features in order to buffer the two uses to the extent possible.
5. The County (and developers) shall carefully coordinate the extension of public water and sewer services in the planning area with Cutler Public Utility District (PUD) and Orosi PUD, to promote logical and orderly development patterns.
6. New agricultural preserves and contracts shall not be approved for properties within Cutler PUD and Orosi PUD.
7. Commercial and residential uses will be required to connect to public services provided by the Cutler PUD and Orosi PUD.
8. Large lot agricultural zoning such as AE-20 shall be applied as a holding zone to properties, which do not meet the criteria set forth in policy Agriculture Policy 2 above.
9. Promote growth along SR 63 for industrial and commercial uses to preserve adjacent agricultural lands consistent with the Corridor Framework Policies in the Tulare County General Plan.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Cutler/Orosi.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Cutler/Orosi.

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|---|---|
| 4. Carefully evaluate proposed heavy industrial uses to be located east of SR 63 to assure that | such uses will not have an adverse impact on the community. |
|---|---|
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General Plan Framework

Value Statements

1. The beauty of the County and the health and safety of its residents will be protected and enhanced.
2. The County will create and facilitate opportunities to improve the lives of all County residents.
3. The County will protect its agricultural economy while diversifying employment opportunities.
4. Every community will have the opportunity to prosper from economic growth.
5. Growth will pay its own way providing sustainable, high quality infrastructure and services.

Framework Concepts

- ***Concept 1: Agriculture***
One of the most identified assets in Tulare County is the rich agricultural land on the valley floor and in the foothills. The General Plan identifies agriculture not only as an economic asset to the County but also as a cultural, scenic, and environmental element to be protected and to insure that the utilization of these resources may continue to economically succeed.
- ***Concept 2: Land Use***
Tulare County has a number of unincorporated communities that will grow and develop and natural resource lands (agriculture, mineral extraction, and open space) that will be preserved and permitted to expand. It is anticipated that much of the projected population growth will require a range of housing choices, neighborhood support services, and employment producing uses that are centrally located in cities and unincorporated communities. The County will also utilize its goals and policies to guide the conversion of agricultural and natural resource lands to urban uses.
- ***Concept 3: Scenic Landscapes***
The scenic landscapes in Tulare County will continue to be one of its most visible assets. The Tulare County General Plan emphasizes the enhancement and preservation of these resources as critical to the future of the County. The County will continue to assess the recreational, tourism, quality of life, and economic benefits that scenic landscapes provide and implement programs that preserve and use this resource to the fullest extent.
- ***Concept 4: Natural and Cultural Resources***
As Tulare County develops its unincorporated communities, the County will ensure that development occurs in a manner that limits impacts to natural and cultural resources through the implementation of its Goals and Policies and through proper site planning and design techniques.

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Guiding Principles

- ***Principle 1: Opportunities***
Provide opportunities for small unincorporated communities to grow or improve quality of life and their economic viability.
- ***Principle 2: Reinvestment***
Promote reinvestment in existing unincorporated communities in a way that enhances the quality of life and their economic viability in these locations.
- ***Principle 3: Protection of Resources***
Protect the County's important agricultural resources and scenic natural lands from urban encroachment through the implementation of Goals and Policies of the General Plan.
- ***Principle 4: Limit Rural Residential Development***
Strictly limit rural residential development potential in important agricultural areas outside of unincorporated communities, hamlets, and city UDBs, UDBs (i.e., avoid rural residential sprawl).
- ***Principle 5: Agricultural Facilities***
Allow existing and outdated agricultural facilities in rural areas to be retrofitted and used for new agricultural related businesses (including non-agricultural uses) if they provide employment.
- ***Principle 6: Planning Coordination and Cooperation***
Enhance planning coordination and cooperation with the agencies and organizations with land management responsibilities in and adjacent to Tulare County.

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Preliminary Assessment of Land Needs

Market Analysis

Renter Affordability

According to the US Census Bureau, the 2013-2017 American Community Survey (see **Table 34**) data indicated that in 2017 the cost of rent in Cutler/Orosi was lower than in Tulare County and the State of California, but that rent constituted a larger percentage of household income. The median rent was \$755 in Cutler and \$873 in Orosi, whereas the median rent was \$877 in Tulare County and \$1,358 in the State of California, respectively. In Cutler, the percentage of households paying 35% or more of income on housing was 63.9% and in Orosi, it was 68.2%, while the percentage of households paying 35% or more of income on housing was 47.2% in Tulare County and 47.0% in the State of California.

Table 34 - 2013-2017 American Survey: Renter Cost

Geography	Median Rent	Gross Rent as a % of Household Income					
		Less than 15.0 %	15.0% to 19.9%	20.0% to 24.9%	25.0% to 29.9%	30.0% to 34.9%	35.0% or more
California	\$1,358	9.6%	10.9%	12.1%	11.5%	9.6%	46.4%
Tulare County	\$877	10.6%	10.5%	12.1%	10.7%	8.7%	47.2%
Cutler CDP	\$755	12.0%	13.1%	5.7%	1.1%	4.0%	63.9%
Orosi CDP	\$873	11.5%	14.1%	6.2%	0.0%	0.0%	68.2%

Source: 2013-2017 American Community Survey 5-Year Estimates

Owner Affordability

According to the US Census Bureau, the 2013-2017 American Community Survey data indicated that in 2017 the cost of a mortgage in Cutler/Orosi was lower in Tulare County and the State of California. The mortgage constituted a smaller percentage of household income compared to Tulare County and the State of California. The median owner cost (with mortgage) was \$927.00 in Cutler and \$1,132.00 in Orosi, whereas the median owner cost was \$1,345 in Tulare County and \$2,206 in the State of California, respectively. In Cutler, the percentage of households paying 35% or more of income on housing was 29.9% and in Orosi 53.0%. The percentage of households paying 35% or more of income on housing was 31.7% in Tulare County and 30.7% in the State of California (see **Table 35**).

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Table 35 - 2013-2017 American Community Survey: Owner Cost						
Geography	Median Owner Cost (with mortgage)	Mortgage as a % of Household Income				
		Less than 20.0%	20.0% to 24.9%	25.0% to 29.9%	30.0% to 34.9%	35.0% or more
California	\$2,206	32.5%	19.6%	12.5%	9.0%	30.7%
Tulare County	\$1,345	36.2%	14.5%	10.4%	7.1%	31.7%
Cutler CDP	\$927	62.9%	0.0%	7.0%	14.8%	29.9%
Orosi CDP	\$1,132	52.4%	14.5%	0.0%	0.0%	53.0%
Source: 2013-2017 American Community Survey 5-Year Estimates						

Existing Businesses

According to the US Business Directory, there were 90 existing businesses in the Cutler area in 2018, <http://us-business.info/directory/cutler-ca/> (see Table 36) and 186 existing businesses in the Orosi area <http://us-business.info/directory/orosi-ca/> (see Table 37).

Table 36: Existing Businesses in Cutler

99 Cents Plus Store	Cutler Orosi Waste Water	Ledbetter Park	St. Mary's Religious Education CCD
Abby's Video & Deli	E R Holden & Sons	Lovell Continuation School	T Rod INC
Aden Market	El Progreso	M I Salon	Tulare County Child Care
Arnold Trucking	El Ranchero Perez	Magnolia Market	Tulare County Fire Department
Avila's Tires	Family Healthcare Network	Martinez Accounting	Tulare County Sheriff's Office
Awasthi, Sarvamitra, MD	First Southern Baptist Church	Mini Fashions Outlet	Twin Girls Farms
Baba, Steven, DDS	Fresco Market Place	Monterey Water Company	US Post Office
Barsamian Farms	George Brothers Ranch Shop	Mulholland Citrus	Valero
Big Discount Store	Golden Sierra Cold Storage	Mundi Diesel	Warren & Baerg Manufacturing INC
Birreria	Golden Star Citrus	Nacho Auto Repair	Wawona Garage & Machine Shop
Birrieria Apatzingan	Golden State Vintners	O&R Trucking	Wawona Packing
C & E Ananian	Green Luck Landscape & Maintenance	Pacific Trellis Fruit	Western Farm Service INC
Carniceria Moyahua	Guerrero Martinez Tax Service	Paramount Citrus Association	Wileman Brothers & Elliott INC
Christian Worship Center	HB Gills INC	Pena's Auto Sales	
Comp Tek	J & L Tree Service	Pena's Recycling Center	

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Crop Production Service	J V Farm Labor Service	Phils Lock & Key	
Cutler Liquor	Jaime Lisa A (MA)	Panaderia Esesarte	
Cutler Child Care Center	Junior's #2 Fashion	Rene G Ortega Concrete	
Cutler Market		Rosewood Villas	
Cutler Bakery	Karon's	Rubalcaba Grocery	
Cutler Orosi Senior Center	Kathy Ruvalcaba	Ruvalcaba Meat Market	
Cutler School	Kathy's Style Shoppe	Saint Mary's Parish Hall	
Cutler Supermarket	Kaweah Container INC	Shell	
Cutler Orosi Unified School District	LA Esperanza	St. Mary's Catholoc Church	
Cutler PUD	LA Fiesta Food		

Table 37: Existing Businesses in Orosi

1 st Baptist Church	Cevallos Bakery	Gil's Auto Wrecking	McDonalds
99 Cents & More	Citricove Orchards	Golden Valley School	McPhaill Citrus Ranch
A & Engraving	Cutler Orosi Unified SCHL Dist	Golden Villa Mini-Mart	Mickie's Hair Salon
A S Oriental Seafood Market	CSET	Glenn's Orosi Mini Storage	Monterey Water Company
Abe-EL Produce	Cutler Orosi Special Education	Golden West Labor	Midway Auto Parts
Abel Sahagun Insurance	Cutler Rexall Pharmacy	GSF Nut Company	Mountain View AG Services INC
Academy West Insurance Services INC	Cuts N Styles	Gonzalez Serge	N & R Transportation
Agrape Christian Superstore	Davila Memorials & Granite Works	Good Choice Insurance Service	Nakatsuchi Hirofumi DDS
Allied Insurance	Diana's Daycare	Green Valley Medical Clinic	Navarro, Isaac R
Aleman 99 Cents Y Mas	Dollar Store & More	Gonzalez, Anna M. MD	Nava Smog
Aguilar, Valerie (MA)	Doneright Electricians	Green Medical Caregiver	Novedandes Nana
Alejandra Flowers	East Orosi Community Service District	Guevara's PM Roofing Co.	Orlopp Turkey Breeding Farms
Aldaz Pedro	East Orosi Market	H & R Block	Orosi Auto Repair & Tire Service
Alta Vista Apartments	Ecology Sound Farms	Happy Apple Co	Orosi Barber Shopo
Amigos Transmission & Auto Repair	El Cesar	Hong Kong Chop Suey	Orosi Branch Library
Aleman Clothing	El Lago	Iglesia Ni Christo-Church	Orosi Center
Angie's Beauty Salon Alta Vista Apartments	El Mexicano-Soccer Accessories	Ikard & Ikard	Orosi Food Mart
Arturo's Portable Toilet Service	El Monte School Office	JL Recycle Center	Orosi Family Medical Care
Augies Farm Labor Service	El Pio Pio Restaurant	K & K Market	Orosi Flowers N More

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Auto Title Experts	El Progreso Tortilleria	Kaleka, Virender S. MD	Orosi Donuts
Bank of the West	El Rincon Market	Kaspian's Liquor	Orosi ER Dental Center
Bay Area P O S	Faith Bible Church of God	Kay Bee Farm	Orosi MH Estates
Bayardo, Carlos, MD	Family Dollar	Kwick Korner	Orosi Mart & Deli
Beneje's Drive In	Family Health Care Network	La Bonita Supermercado	Orosi Medical Supply
Big O Gas & Deli	Fancher Creek Packing	La Mexicana	Orosi High School
BHK Nut Corp	Farm Labor Contractor	Lara's Meat Market	Orosi Urgent Care CTR Medical
Butler Ranches	First Baptist Church	Lawson Packing	Orosi Swap Meet
Boss Concrete	Freedom Transport	Leon Service Station	Orosi Public Utility District
Calvary Temple	Gabriel's Auto Mechanic	Little Caesars Pizza	Orosi Mini Mart
Camilo Galacgac	Galindo Electric	Lopez, Aileen J, MD	Orosi Pizza House
Cely's Beauty	Garcia Window Screens	M & G Farms	Orosi Wireless
Centro Cristiano Vida ABundante	Giannandrea Rita	Madrid George	P Marquez Enterprises
Palm Elementary School	Papich Construction Co.	Paramount Citrus	Pop's Propane SVC
R-N Market	RBM Industries	Rising C Ranches	Rose City Transportation
S & J Ranch INC	Saint Germain	Sal's Tabacco & More	Salon Expressions
Sanchez, Alma R. DDS	Sequois Presbyterian Church	Sand Creek Apartments	Secera Vending
Sequoia View Apartments	Seventh-Day Adventist Church	Smart Money	Singh, Gurteg, DDS
Sierra Pacific Materials	St Germain Botanicals	Suarez, Boris M	Super 7
Subway	Taqueria Santa Fe	Templo De Jesus	Templo La Paz Mennonite Brithrn
The Car Wash	The Saul-on	Tulare County Child Care	Trvino Construction
Trevino & Son Farm Labor	Town & Country Christian School	Tulare 2010 Community LP	U-Haul
United Pentecostal Church	US Post Office	VM Logistics	Valero Cristina MD
Valley Smog	Velozz Communication	Video Castle	Villa De Guadalupe Apts
Vidrio, Maria	W & E Electric Service	Wawona Garage & Machine Shop	YR Pizza Planet
Z's Communication			

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Market Feasibility

The community of Cutler (**see Table 37**) already has seven (7) grocery stores including Aden Market, Cutler Market, Cutler Supermarket, Fresco Market Place, La Fiesta Food, Magnolia Market, and Rubalcaba Grocery and two (2) convenience stores: Cutler Liquor and Shell Mini Mart. Cutler also has five (5) restaurants including: Abby's Video & Deli, Birrieria Apatzingan, El Progreso, El Ranchero Perez, La Esperanza. There is also Avila's Tires, Pena's Auto Sales, Western Farm Service, 99 Cent Plus Store, and Big Discount Store located in Cutler. Without a substantial increase in households, the discretionary income of Cutler residents would not support a new large commercial area.

The community of Orosi (**see Table 37**) already has 10 grocery stores including A S Oriental Seafood Market, East Orosi Market, El Progreso Tortilleria, El Rincon Market, K & K Market, Orosi Food Mart, R-N Market, Saint Germain, and Super 7, six (6) convenience stores: Kaspian's Liquor, Kwick Korner, Golden Villa Mini-Mart, Orosi Mini Mart, Orosi Mart & Deli, Orosi Food Mart, and has 12 restaurants including: Benje's Drive In, El Cesar, El Lago, El Pio Pio Restaurant, Hong Kong Chop Suey, La Mexicana, Little Caesars Pizza, McDonalds, Orosi Pizza House, Subway, Taqueria Santa Fe, and YR Pizza Planet. There is also Alejandra Flowers, Aleman Clothing, Family Dollar, Orosi Auto Repair & Tire Service, and Bank of the West, located in Orosi. Without a substantial increase in households, the discretionary income of Orosi residents would not support a new large commercial area.

Assessment of Land Needs

Population Growth Forecast

The estimated 2030 population is 15,440 and the forecasted increase in population from 2017 to 2030 is 1,830 persons. (**see Table 38, 39, and 40**).

Population Growth Forecast

Table 38 - Cutler/Orosi Population Projections			
Growth Rate	2017	2020	2030
Cutler	5,850	6,081	6,920
Orosi	7,780	8,067	9,179

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Demand Forecast

With the existing 2,441.9 acre Cutler/Orosi Urban Development Boundary, approximately 1,246 acres are urbanized. By dividing the estimated 2030 population of 15,440 by 1,246 urbanized acres, a ratio of 12.39 persons per urbanized acres is calculated. The forecasted increase in population from 2017 to 2030 is 1,830 persons. Projecting this ratio into the future (1,830 persons divided by 12.39) suggests that an additional 148 acres will be needed by the year 2030.

Based on the data and analysis contained above and forecasted population and housing estimates below, the following table includes the year 2030 square footage and residential unit demand forecast for the Cutler/Orosi planning area.

Population and Housing Units

Table 39 Population Projection (Cutler)		
Year	Population	Growth (%)
2017	5,850	0.013
2018	5,926	0.013
2019	6,003	0.013
2020	6,081	0.013
2021	6,160	0.013
2022	6,240	0.013
2023	6,321	0.013
2024	6,404	0.013
2025	6,487	0.013
2026	6,571	0.013
2027	6,657	0.013
2028	6,743	0.013
2029	6,831	0.013
2030	6,920	0.013

Table 40 Population Projection (Orosi)		
Year	Population	Growth (%)
2017	7,760	0.013
2018	7,861	0.013
2019	7,963	0.013
2020	8,067	0.013
2021	8,171	0.013
2022	8,278	0.013
2023	8,385	0.013
2024	8,494	0.013
2025	8,605	0.013
2026	8,717	0.013
2027	8,830	0.013
2028	8,945	0.013
2029	9,061	0.013
2030	9,179	0.013

Population and Housing Units

The year 2017 baseline population was determined by projecting the 2017 American Community Survey data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2017 the community of Cutler had 1,293 dwelling units (including vacant dwellings) with a population of 5,850. At an annual growth rate of 1.3%, the projected housing units are 1,344 and 1,529 in years 2020 and 2030, respectively, and projected population is 6,081 and 6,920 in Years 2020 and 2030, respectively. In the community of Orosi had 2,076 dwelling units (including vacant dwellings) with a

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population of 7,760. At an annual growth rate of 1.3%, the projected housing units are 2,158 and 2,456 in years 2020 and 2030, respectively, and projected population is 8,067 and 9,179 in Years 2020 and 2030, respectively.

Table 41 Housing Projection (Cutler)		
Year	Housing	Growth (%)
2017	1,293	0.013
2018	1,310	0.013
2019	1,327	0.013
2020	1,344	0.013
2021	1,362	0.013
2022	1,379	0.013
2023	1,397	0.013
2024	1,415	0.013
2025	1,434	0.013
2026	1,452	0.013
2027	1,471	0.013
2028	1,490	0.013
2029	1,510	0.013
2030	1,529	0.013

Table 42 Housing Projection (Orosi)		
Year	Housing	Growth (%)
2017	2,076	0.013
2018	2,103	0.013
2019	2,130	0.013
2020	2,158	0.013
2021	2,186	0.013
2022	2,214	0.013
2023	2,243	0.013
2024	2,272	0.013
2025	2,302	0.013
2026	2,332	0.013
2027	2,362	0.013
2028	2,393	0.013
2029	2,424	0.013
2030	2,456	0.013

Opportunities & Constraints

Opportunities

Complete Streets

The Complete Streets Act of 2007 (Assembly Bill 1358) requires counties when updating General Plans, to identify how the jurisdiction will provide for the routine accommodation of all users of the roadway including motorists, pedestrians, bicyclists, individuals with disabilities, seniors, and users of public transportation.

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Affordable Housing

The community of Cutler has a median income of \$31,939 and Orosi medium income is \$35,798, which is less than 80% of the State median income of \$67,169. Approximately 55% of the households in Cutler and 47.0% of the households in Orosi spend 52.69% or more of their income on housing. As such, there is a high need for affordable housing.

Moreover, 63.9% of Cutler's renters and 68.2% of Orosi's renters spent over 35% or more of their income on rent. Average household size of renters was 5.34 for Cutler and 3.82 for Orosi's. In addition, 29.9% of Cutler's owner-occupied units and was 53.0% for Orosi spent over 35% or more of their income on mortgages. Average household size of owner-occupied units was 3.68 for Cutler and was 3.91 for Orosi.

It is very likely that many children in Cutler/Orosi share bedrooms. As there are no bus lines in Cutler/Orosi, reduced parking is not a realistic strategy to reduce development costs. Affordable housing will require more land in Cutler/Orosi than would typically be required in an area where public transit is available. In terms of siting, medium to high-density housing should be located along collector streets and/or arterials.

Urban Development Boundary

Although State planning law does not define specific requirements for establishing planning area boundaries, it is generally agreed that the planning boundaries should include the territory within a community's probable ultimate physical boundaries and service area. Urban Development Boundaries provide a planning framework that promotes the viability of communities, hamlets, and cities while protecting the agricultural, open space, scenic, cultural, historic, and natural resource heritage of the County. In the past, the County used three key planning tools to guide urban development in all unincorporated areas of the County. The first was the Urban Boundaries Element; the second are the Area Plans; the third are the General Plans for identified incorporated cities and Community Plans for unincorporated communities. In 1974, Tulare County added an Urban Boundaries Element to its General Plan. The element required the designation of an urban boundary for every "viable" unincorporated community in the county. The Urban Boundaries Element also established Urban Improvement Areas (20-year planning boundaries) for certain communities. The 1974 Urban Boundaries Element designated both an Urban Area Boundary and an Urban Improvement Area for Cutler/Orosi.

In 1983, the Urban Boundaries Element was amended to create Urban Development Boundaries (UDBs, which are also to function as 20-year planning boundaries) and to Change the function of the Urban Area Boundary to simply a "comment line" around incorporated cities. Under the 1983 amendment, Urban Area Boundaries are no longer established around unincorporated communities - and Urban Improvement Areas are to be phased out over time (replaced with UDBs) as each community's boundaries are updated.

For unincorporated communities as per the Planning Framework Element of the General Plan, the UDB is a County adopted line dividing land to be developed from land to be protected for agricultural, natural, open space, or rural uses. It serves as the official planning area for communities over a 20-

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year period. Land within an unincorporated UDB is assumed appropriate for development and is not subject to the Rural Valley Lands Plan or Foothill Growth Management Plan.

Constraints

There are several constraints or restrictions which will impact the nature and location of future development within the community. In particular, these constraints pertain to existing problems of public health and safety; acceptable noise levels impacts of deteriorating housing, lack of a full range of community services. Following are constraints that were recognized in the preparation of this plan.

Noise Contours

There are a variety of sources that produce noise in the Cutler/Orosi Plan Area and include traffic, railroad operations, airport operations, and agricultural operations. Traffic noise is the most dominant source of ambient noise in the County, according to the Tulare County General Plan EIR (see Table 43). SR 63 and Avenue 416 run through the Cutler/Orosi Plan Area and would be the largest source of traffic noise in the area due to the high volumes of traffic. Noise from SR 63 adversely impacts an area through central Cutler/Orosi making properties in close proximity to the highway less desirable for new housing construction.

Table 43 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
SR 63 Ave 400 to Emerald Dr.	8,300	34	74	159	343
SR 63 Emerald Dr. to Ave 416	13,000	43	92	198	426
SR 63 Ave 416 to Ave 422	7,200	29	62	133	287
Ave 416 Road 120 to SR 63	8,000	37	79	171	368
Ave 416 SR 63 to Boyd Dr	850	8	18	38	83

Source: 2010 General Plan Background Report

Sand Creek

Sand Creek, an ephemeral stream, which carries local storm water runoff southerly to Cottonwood Creek. Cottonwood Creek flows into Cross Creek, and flows to the Tule River. Sand Creek lies between the communities of Cutler/Orosi. Periodic flooding of Sand Creek has previously precluded the quarter to half-mile separating the communities from developing to urban uses prior to existing flood control improvements. Sand Creek usually is dry during the summer.

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Sand Creek is located within the FEMA Special Flood Hazard Areas and that the inundation zone must be included on hazard disclosures pertaining to real estate contracts.

Sand Creek lies within the Sand Creek Dam inundation zone due to dam failure. Sand Creek is located in Tulare County, California and displayed on the Monson USGS quad topo map. Sand Creek Dam is used for drinking water, fish and wildlife protection and flood control, among other things. Construction of the dam was completed in 1980. At normal levels it has a surface area of 55 acres. The dam is owned by Tulare County Resource Management Agency. Sand Creek is rock fill and its height is 60 feet with a length of 933 feet. Normal storage is 1,050 acre, and it drains an area of 26.3 square miles.

Tulare County Economic Development Strategy

Tulare County's current Economic Development Strategy focuses on tourism, the agricultural industry and pursuing grants.

Agriculture

Tulare County has a booth at the World Agricultural Exposition (Ag Expo) every year. The Economic Development Office uses the event to promote Tulare County tourism and business opportunities. Partnering with the County's Purchasing Department the Ag Expo provides an excellent method to market directly to the global agriculture related businesses attending the Ag Expo and sell surplus county equipment.

Grants

- State Water Resources Control Board – State Revolving Fund: \$500,000 for Traver Community Wastewater System Improvements Planning Study and Design, once plans are near complete we will apply for construction funding between \$8 and 10 million.
- State Water Resources Control Board: have applied and received \$5 million in construction funding for Phase 1 of the Yettem Seville Water System.
- County Measure R funding \$575k for sidewalks and ADA improvements in Goshen.
- ATP Active Transportation Program- Statewide competitive \$2 million funding is anticipated for Safe Routes to School and ADA improvements in and around three (3) Goshen.
- Low Carbon Transit Program funding \$147,474.00.
- Prop 84 – Goshen Neighborhood Improvement Program funding \$2,153,900.00.
- Yettem & Seville Project Phase 1 funding \$4,300,200.00
- Navigation Aids at Sequoia Field Airport funding \$340,200.00
- Transit Operations & Maintenance Facility (TOMF) funding \$10,800,000.00

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Solar Projects

In Tulare County, there have been 13 Utility Scale Solar Projects that have a capacity of 198 MW. There are nine (9) projects in /under Construction with a capacity of 260 MW. In terms of total solar projects (including Utility Scale, Solar on Dairies, Commercial Solar, and Residential Solar) there have been 1570 projects built that accounts for a capacity of 227.5 MW. The Corridor offers realistic potential to locate solar projects closer to the urban areas and outside of the direct line-of-sight viewshed of the Highway 99 Corridor. **Figure 24** provides a summary of solar development in Tulare County.

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Figure 24 - Solar Development in Tulare County

Utility Scale Solar Projects		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	2	740
Pre-construction/Under Construction	2	90
Constructed	24	388
Total	28	1218
Solar Projects on Dairies		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	2	2.16
Pre-construction/Under Construction	9	8.30
Constructed	61	47.63
Total	72	58.09
Other Commercial Solar Projects		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	1	0.06
Pre-construction/Under Construction	16	6.46
Constructed	265	73.56
Total	282	80.08
Anaerobic Digesters		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	2	
Pre-construction/Under Construction	52	
Constructed	12	
Total	66	
Residential Solar Projects (based on 7 kw/sfd average)		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	16	0.26
Pre-construction/Under Construction	147	2.24

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Constructed	3174	24.14
Total	3337	26.64
Solar Project Totals		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	23	742.48
Pre-construction/Under Construction	226	124.00
Constructed	3536	533.33
Total	3785	1399.81

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SWOT ANALYSIS	
Strengths	Weaknesses
<ul style="list-style-type: none"> ▪ Tulare County has a strong agricultural economy. ▪ There is an elementary school ▪ There is a High School in Cutler/Orosi ▪ There a number of highway commercial businesses. ▪ Cutler/Orosi is located on SR 63 and Avenue 416. ▪ There is a variety of commercial and industrial uses. ▪ The cost of living is low in Tulare County. ▪ Water supply has reserve of 70,000 gpd. ▪ The sewer system is operating at approximately 65% of its capacity. ▪ Recreational sports complex and a community park 	<ul style="list-style-type: none"> ▪ Cutler (\$31,939) and Orosi (\$35,798) are considered severely disadvantaged communities. ▪ Sidewalks are needed. ▪ The Storm drain system is inadequate improvements are required.
Opportunities	Threats
<ul style="list-style-type: none"> ▪ Vacant land is available. ▪ Complete Streets ▪ Cutler/Orosi is located next to State Route 63 which has 17,500,000 annual trips (or about 48K AADT) ▪ A Mixed Use Overlay Zone would allow for additional development. ▪ There is growth in distribution jobs in California. ▪ Tulare County Transit Agency (TCaT) has bus routes that link Cutler/Orosi with Visalia and Dinuba. 	<ul style="list-style-type: none"> ▪ There has been a gradual decline in the number businesses and jobs. ▪ Cutler/Orosi competes with Visalia, Tulare, Dinuba, and other parts of the County. Cutler/Orosi also competes with the City of Kingsburg and Selma in Fresno County. ▪ Tulare County has a limited budget for road improvements. Budgeting for roadway maintenance could be reduced depending upon the financial health of Public Works branch. ▪ Small businesses are sensitive to changes in tax rates, minimum wage, requirement of health care provisions and other nationally established policies and requirements. ▪ Farms are getting larger and relying more on automation. This can reduce jobs and incomes. ▪ National and Statewide economic trends can impact Cutler/Orosi's economy.

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Reducing Barriers to Economic Development

California Competes

“The California Competes Tax Credit is an income tax credit available to businesses that want to locate in California or stay and grow in California. Tax credit agreements will be negotiated by GO-Biz and approved by a newly created “California Competes Tax Credit Committee,” consisting of the State Treasurer, the Director of the Department of Finance, the Director of GO-Biz, one appointee from the Senate, and one appointee from the Assembly.”⁴³

“The California Competes Tax Credit only applies to state income tax owed to the Franchise Tax Board. The credit is non-refundable, and in the case where the credit allowed exceeds tax owed, the excess may be carried over to reduce the tax in the following year, and the succeeding five years if necessary, until exhausted.”⁴⁴

“The value of the credit will be based on the following factors:

- The number of jobs the business will create or retain in this state.
- The compensation paid or proposed to be paid by the business to its employees, including wages and fringe benefits.
- The amount of investment in this state by the business.
- The extent of unemployment or poverty where the business is located.
- The incentives available to the business in this state, including incentives from the state, local government, and other entities.
- The incentives available to the business in other states.
- The duration of the business’ proposed project and the duration the business commits to remain in this state.
- The overall economic impact in this state of the business.
- The strategic importance of the business to the state, region, or locality.
- The opportunity for future growth and expansion in this state by the business.
- The extent to which the anticipated benefit to the state exceeds the projected benefit to the business from the tax credit.”⁴⁵

“The tentative amount of credits that GO-Biz can allocate is as follows:

- \$30 million in fiscal year 2013/14.
- \$150 million in fiscal year 2014/15.
- \$200 million in each fiscal year 2015/16 through 2017-18.”⁴⁶

“...25 percent of the total credits available each year is expressly reserved for small businesses (gross receipts of less than \$2 million during the previous taxable year).”⁴⁷

⁴³ California Competes Tax Credit FAQ

⁴⁴ Ibid.

⁴⁵ Ibid.

⁴⁶ Ibid.

⁴⁷ Ibid.

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Tulare County Strategy

In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

Infrastructure

The water system is at capacity and the wastewater system is near limiting capacity. In order for more development to occur, service levels for water and wastewater need to be expanded. Grant funding is needed to increase service levels.”

Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

Education

Tulare County has five satellite campuses for four-year universities: California State University-Fresno, University of California-Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Table 44 - Estimated Education Attainment of Cutler/Orosi Population				
	Percent Less than 9th grade	Percent 9th to 12th grade, no diploma	Percent High School graduate (includes equivalency)	Percent bachelor's degree or higher
California	9.9%	8.0%	20.6%	20.1%
Tulare County	20.5%	11.4%	25.4%	9.4%
Cutler	43.9%	17.1%	24.9%	0.2%
Orosi	34.1%	14.7%	21.8%	6.2%

Source: 2013-2017 American Community Survey, 5-year

Based on the 2013-2017 American Community Survey (see **Table 44**), the educational barrier in Cutler/Orosi begins in grade school. Of the adults age 25 and older, Cutler 43.9% and Orosi 34.1% had an educational level of less than 9th grade. This limits the types of jobs that these adults are qualified for. Improving educational attainment needs to begin in elementary school. As part of the safe routes to schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This should make it easier for children to get to school.

Home Occupation Regulations

Encouraging small-scale entrepreneurship is one way to foster economic development. Although some home occupations are allowed by right, these home occupations have regulations which limit the type and scope of allowed businesses. By reducing some of these regulations, we can eliminate a few barriers to small-scale entrepreneurship. The following are proposed changes to the Zoning Ordinance.

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Eliminate:

Section 15.A.7.a.6 “No one other than residents of the dwelling shall be employed in the conduct of a home occupation.”

Section 15.A.7.b.6 “No one other than residents of the dwelling shall be employed in the conduct of a home occupation.”

Section 15.A.7.c.5 “A rural home occupation shall be limited in employment to residents of the property and not more than one (1) additional person.”

Replace with:

“Employment in a rural occupation shall be limited to residents of the property and not more than three (3) additional non-resident persons.”

Add:

All home occupations located within the Cutler/Orosi UDB will not require a use permit.

Health Care

Health care is important for economic development, as businesses need healthy employees. Cutler Orosi Medical Clinic in Cutler, Adventist Health Medical Office in Orosi, and Family HealthCare Network provides healthcare services for the Cutler/Orosi area. Other medical offices are located in the communities of Visalia, Kingsburg, and Dinuba.

Marketing Strategy

Place

In terms of real estate, location is a major factor (influence) in development and expansion of businesses. There are locational advantages in Cutler/Orosi, access to SR 63 and Avenue 416. These locational advantages are discussed below.

- *Access to State Route (SR) 63:* There is freeway entry and exit points, which provides convenient access and/or egress to SR 63.
- *General Commercial.* One of the goals of this Community Plan is to strengthen the current businesses located in Cutler/Orosi by taking advantage of existing industrial and commercial uses. With the establishment of industrial clusters, existing businesses can attract other type of similar business. Along SR 63 and Avenue 416 are commercial clusters. This cluster includes grocery stores, meat market, bakery, discount stores, fashion shops, beauty salons, and auto maintenance.
- *Quality of Life:* It is possible to increase the desirability of visiting and living in Cutler/Orosi. With physical improvements, the quality of life can be improved. There are a number of potential projects that can improve the quality of life in Cutler/Orosi. New recreational facilities and aesthetic improvements have most the visible effect on the quality of life as the perception of these improvements project change and image enhancements. Physical improvements could include the following:
 - Orosi- ADA Improvement & Blade Patching,

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- Cutler- ADA Improvement & Blade,
- George Road and 2nd Drive in Cutler. Pedestrian improvements (sidewalk and drainage improvements to be specific).

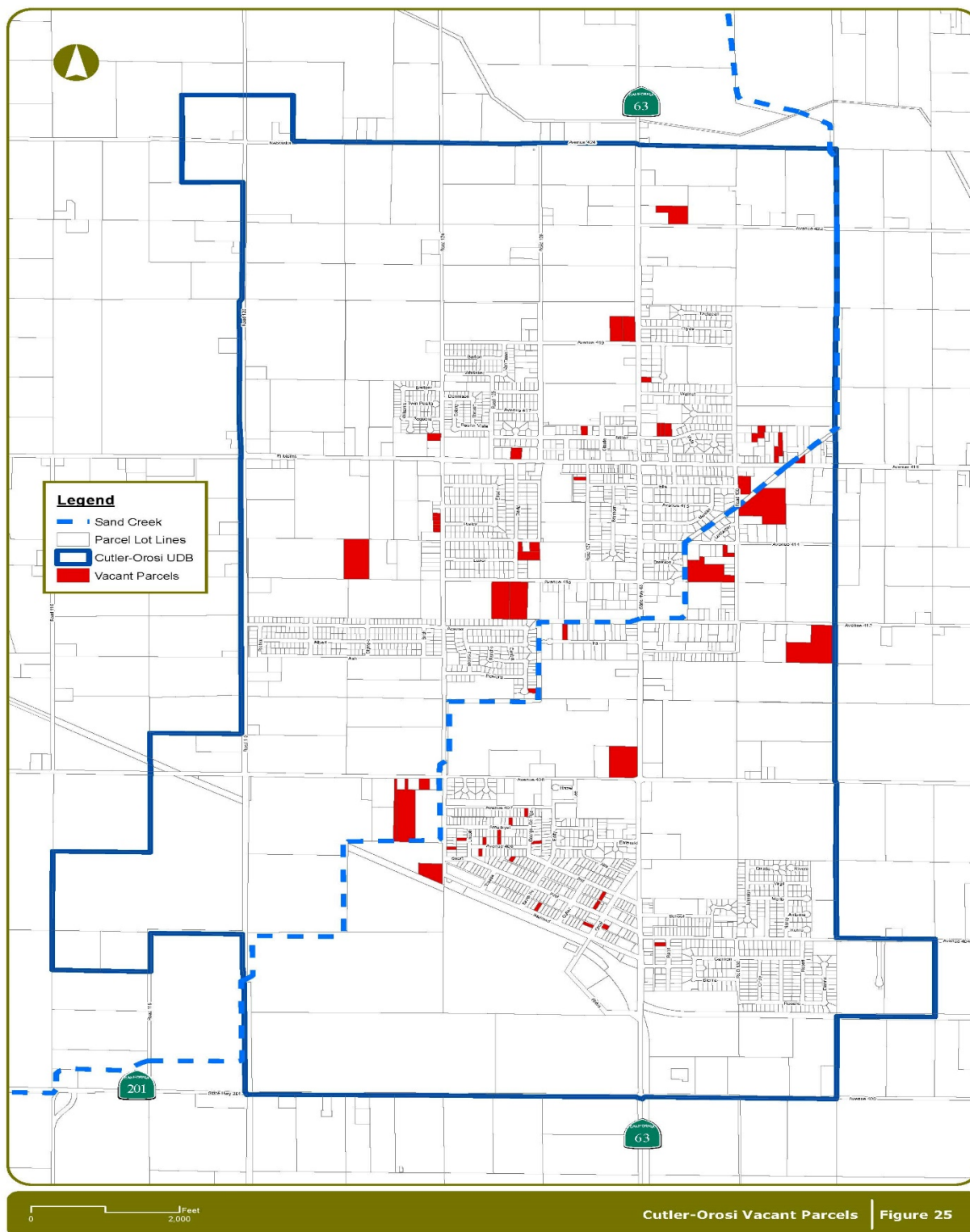
Product

The products to be marketed in Cutler/Orosi are vacant land and available buildings.

- *Vacant Land:* Staff has prepared a vacant parcel inventory, which identifies vacant parcels of various sizes, and various zoning districts **(see Figure 25 and Table 45)**.
- *Available Buildings:* There are limited opportunities to use buildings for commercial or industrial uses. When individual spaces become available, it would behoove a property owner to coordinate with a real estate agent to lease out each space.
- *Inventory:* The Tulare County Economic Development Department is reaching out to local real estate brokers to develop a parcels availability database. In addition, the County will be providing links from its Economic Development Webpage to individual broker webpages, which can serve to advertise the availability, location, size, etc., of parcels.

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Figure 25 - Vacant Parcels Map



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Table 45 - List of Vacant Parcels

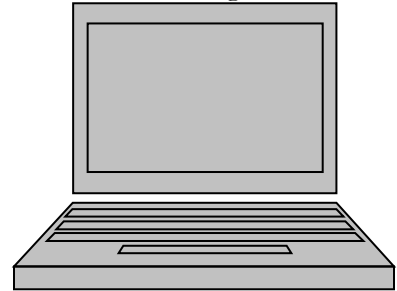
APN #	Acres	Zone	APN #	Acres	Zone
21100011	0.52	C-2	23061004	0.27	P-O
23033007	0.64	C-2	23052006	0.31	R-1
21370003	0.15	R-1	23080048	0.16	R-1
21260055	4.64	AE-20	23090022	1.19	R-2
21303017	0.20	R-1	23061006	0.48	P-O, Z
21370004	0.15	R-1	23170033	3.48	R-1
21360014	5.14	AE-20	25150035	7.36	R-A
23010030	1.66	AE-20	25050041	2.30	R-2
21370002	0.16	R-1	32173004	0.03	R-1
21370005	0.15	R-1	32070007	0.16	R-2
21370006	0.15	R-1	32083025	0.15	R-1
21270037	0.44	R-2	32083021	0.15	R-1
23010029	1.60	AE-20	32050025	0.58	C-3
23211017	0.30	R-2	32085014	0.16	R-1
23170032	3.50	R-1	32081016	0.15	R-1
23222012	0.25	R-1	32142017	0.17	C-2
23222013	0.18	R-1	32083030	0.15	R-1
23080065	0.27	R-1	32082007	0.16	R-1
23090026	6.52	R-A	32133012	0.17	R-3
23193003	1.21	R-1	32132006	0.17	R-2
23193001	0.51	R-1	32050035	1.07	C-3
23080072	0.55	R-1	32085018	0.16	R-1
23080003	0.51	R-1	32132010	0.17	R-2
23080030	0.23	R-1	32113009	0.13	R-1
23080004	0.26	R-1	32084002	0.15	R-1
23136009	0.20	R-1	32050037	5.07	C-3
23222007	4.19	R-2	32171004	0.21	R-1
23061005	0.27	P-O	32134020	0.17	R-2
			32260002	1.92	C-3, M-1

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Promotion

Promotion of Cutler/Orosi and properties in Cutler/Orosi is a continual need for effective economic development. The following tasks can be used to promote the community of Cutler/Orosi.

- *Companies to Target:* As Cutler/Orosi is a small community along the SR 63 corridor, economic development should be focused on enhancing existing assets. Based on the existing businesses, the County of Tulare should target the following types of businesses:
 1. Agricultural Food Packing
 2. Cold Storage
 3. Food Processing (Dairy-related products such as milk, ice cream, yogurt, butter, etc.)
 4. Distribution Centers
- *Online Presence:* Content is one of the most important aspects of an online presence. The amount of content provides relevancy and usefulness. The following could be done to improve the County of Tulare's online presence:
 1. Constantly improve (and keep current) the Economic Development website
 2. Additional information about Tulare County could be added to this website. Pictures from Tech Fair Photo Contest are available for use by Tulare County Departments.
 3. Write and submit articles for trade magazines and bloggers.
 4. Develop a Tulare County Economic Development Blog.
 5. In addition to social media sites Facebook and Twitter, Tulare County's Economic Development Department could utilize other Social Media sites such as Instagram, Youtube, Flickr, etc.
- *Brochures:* Brochures provide specific and relevant information. Brochures should be designed for the following.
 1. Specific Site Handouts brochures with the following information: APN, address, size, price, contact information, pictures of site, and pictures of the neighborhood, zoning designation, and a location map.
 2. A Food Production Industry Cluster brochure with the following information: a list of available properties, pictures of the area, prices of land, highlights of the Agricultural Industry in Tulare County.
 3. A Solar/Alternative Energy Industry Cluster brochure with the following information: a list of available properties, pictures of the area, prices of land, highlights of the Solar/Alternative Energy Industry in Tulare County.
 4. A brochure that lists the real estate brokers in Tulare County. This brochure should include contact information such as name, address, phone and email.
- *Advertising:* Tulare County's Economic Development Department and the community of Cutler/Orosi can be advertised in targeted trade journals. These trade journals could include:
 1. California Leagues of Food Processors
 2. Food Manufacturing Magazine
 3. Food Processing Magazine



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4. Food Business News Magazine
5. Logistics Business Magazine
6. Food and Beverage Packing Magazine
7. Packing World
8. Packing Digest

- *Events:* Conferences and other events are excellent venues for networking. Cutler/Orosi could be promoted at the following conferences.
 1. World Ag Expo
 2. Food Shows
- *Videos:* Promotional videos could be available online and displayed on conferences.
 1. Powerpoint presentation video of pictures and statistics.
 2. Videos of movies filmed in Tulare County.

Development Suitability Analysis

The purpose of a development suitability analysis is to determine the areas of the community and surrounding vicinity, which could most appropriately accommodate new growth. It is a means of identifying areas free of development constraints and areas in which improvements must be made before urban only to determine the location of the Cutler/Orosi Urban Development Boundary, but development can be allowed. This development suitability analysis will be used not also to help establish land use patterns for the community's future growth.

To determine development suitability, factors that either encourage or constrain development were selected and mapped. Each factor was assigned a suitability rating to show the level of influence it will have on potential new development Suitability ratings used in this study include:

- | | |
|-------------------|---|
| <i>Very High:</i> | <i>These areas are "infill lands", or are surrounded on three sides by existing urbanized land and are, or can be easily be, serviced with urban services.</i> |
| <i>High:</i> | <i>These areas are lands free of development constraints and which are either adjacent to existing development and are, or can be, served with community sewer and water service. Wastewater service areas but which are near existing urban development.</i> |
| <i>Moderate:</i> | <i>These lands are outside existing boundaries of local water and these lands also exhibit other qualities that make them suitable for future development.</i> |
| <i>Low:</i> | <i>These lands should be precluded from intensive development until certain constraining conditions can be changed or corrected. Examples of such constraints include flooding and Williamson Act Lands.</i> |

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From the process of mapping of the various factors potentially influencing the develop ability of the community and surrounding environs, a composite analysis of the application of these factors to each segment of the planning area is possible. The patterns, which emerged from this composite analysis, have enabled the overall developed suitability of the planning area to be mapped, in conformance with the ratings described above.

It is recommended, based on the Market Analysis, Opportunities and Constraints analysis as contained above the existing configuration of the Urban Development Boundary, as adopted is appropriate and reflective of the development suitability criteria contained above, except to include the Cutler PUD and Orosi PUD plan and surrounding areas consistent with the Sphere of Influence for the CPUD and OPUD.

Proposed Planning Areas

Policy Plan

This chapter of the Cutler/Orosi Community Plan prescribes the policy framework, which will govern the development of the community over the term of the planning period (through the year 2030). It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place.

The plan, although long range in scope, is to be used on a day to day basis to guide the decisions of County staff, the Planning Commission, and the Board of Supervisors as they affect community development. Further, it will provide residents and property owners in the community with direction and guidelines regarding the evolution and growth of their town and its resources. In addition, importantly, this plan will aid other public agencies and entities, such as the school district and the water company, in their own long-range planning and capital expenditure programming. Each subsequent section of this chapter addresses a topical aspect of the community-planning environment. For each aspect, background discussion of relevant issues is included, policies are stated, and implementation programs and activities are outlined.

Proposed Urban Development Boundary

The Community Plan Update encompasses the Cutler Public Utility District and Orosi Public Utility District including the Cutler/Orosi Wastewater Treatment Plan. The Community Plan Update proposes approximately 712.1-acre expansion to the existing Urban Development Boundary (UDB), and amendments to land use and zoning designations. As such, the proposed Community Plan Update will expand the existing 2,441.9-acre UDB (**see Figure 26**) by approximately 29.2%, for a total UDB area of approximately 3,154.0-acres.

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General Plan Consistency

Policy Relationship to the General Plan

The Cutler/Orosi Community Plan is a component in Part III of the Tulare County General Plan and, as such, has the same force and effect as any other adopted element of the general plan. Structurally, the Cutler/Orosi Community Plan is part of the Land Use and Circulation Element of the overall general plan. The principal emphasis of the community plan is on establishing local land use and circulation system patterns and prescribing associated standards and policies. In addition to the specific prescriptions of the community plan, the broader policies and standards of the overall Land Use and Circulation Element apply to Cutler/Orosi.

Also applicable to Cutler/Orosi, and governing all future development in the community, are the other elements (e.g. Planning Framework, Environmental Resources Management, Air Quality, Health and Safety, Transportation and Circulation, etc.) of the Tulare County General Plan. In instances where the policies and/or standards of the Cutler/Orosi Community Plan are more specific or more restrictive than those in other elements of the general plan, the community plan shall take precedence and prevail.

Proposed Land Use Plan and Zoning Districts

Land Use Plan

One of the most important purposes of the Cutler/Orosi Community Plan is to establish land use patterns and development policies and standards for the community for the planning period, through the year 2030. The general intent of the land use plan for Cutler/Orosi is to identify the most appropriate types and distribution of land uses for the community, based on environmental, circulation, infrastructure, services, opportunities and constraints, urban development boundary suitability analysis and other economic capacities and concerns discussed in the previous chapters of the plan.

The County of Tulare, through existing policies, has encouraged both incorporated and unincorporated communities to establish urban development and land use patterns, which are compact and contiguous. This policy position has reduced so-called “leap frog” development County-wide, has helped preserve agricultural lands, and has minimized land use conflicts between urban and agricultural areas. The Cutler/Orosi Community Plan 2020 Update analysis supporting the establishment of an Urban Development Boundary (UDB) for Cutler/Orosi is described, and the UDB is delineated. This boundary is sensitive to and consistent with the Countywide policy of encouraging compact and contiguous urban growth, and at the same time enables the community to accommodate any foreseen demand for economic and population expansion. **Figure 27** depicts and prescribes the proposed land use pattern development recommended for Cutler/Orosi.

As illustrated in this proposed land use diagram, future urban development of the community will be concentrated on SR 63 and Avenue 416. The land use plan for the community provides for substantial expansion of other commercial development along SR 63 and Avenue 416; provides for the

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development of additional commercial uses potentially serving community-oriented demand; and enables the development of additional residential uses to meet demand generated by forecasted population growth.

Proposed Land Use Plan

As suggested above and based on the forecasted growth and the recommended Urban Development boundary, on the economic Development/Market Analysis and Opportunities and Constraints analysis, the proposed land use plan (see **Table 46 and Figure 27**)

Table 46 - Proposed Land Use Plan	
Land Use	Sum Acres
General Commercial	237.2
High Density Residential	70.0
Light Industrial	236.4
Low-Medium Density Residential	817.8
Medium Density Residential	274.4
Mixed Use	7.3
Neighborhood Commercial	0.8
Public Recreation	11.9
Public/Quasi-Public	343.7
Service Commercial	108.1
Urban Reserve-Light Industrial	98.0
Urban Reserve-Medium Density Residential	566.0
Urban Reserve-Mixed Use	77.1
Unclassified (Right-of-Way)	19.9
Total	3,154.0

Proposed Land Use Designations

The following land use designations along with descriptions including density and intensity are recommended for Cutler/Orosi to address land demand needs through the 2030 planning horizon year.

Mixed Use (MU)

This designation establishes areas appropriate for the planned integration of some combination of retail; office; single and multi-family residential; hotel; recreation; limited industrial; public facilities or other compatible use. Mixed Use areas allow for higher density and intensity development, redevelopment, or a broad spectrum of compatible land uses ranging from a single use on one parcel to a cluster of uses. These areas are intended to provide flexibility in design and use for contiguous parcels having multiple owners, to protect and enhance the character of the area. The consideration of development proposals in Mixed Use areas should focus on compatibility between land uses, and

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the development potential of a given area compared to the existing and proposed mix of land uses and their development impacts. Density bonuses for residential units of 25 % to 35% may be granted, according to the Density Bonus Ordinance or State law, to Mixed Use areas to encourage the development of affordable housing units, compact development in the implementation of development strategies that support the use of mass transit, reduction of air impacts, and policies.

Maximum Density: 1-30.0 Dwelling Units/Acre

Maximum Intensity: 0.5 FAR

Urban Reserve (UR)

This designation establishes a holding zone whereby properties shall remain zoned for agriculture or open space use until such a time as conversion to urban uses is deemed appropriate. The UR designation shall be appended by the intended future land use designation, for example, Urban Reserve Commercial (UR) and Urban Reserve Residential (UR). When a rezoning occurs without a General Plan amendment, the UR designation shall be removed from the parcel. This designation applies primarily within UDBs.

Minimum Parcel Size: 1 Dwelling Unit per 10 Acres

Maximum Intensity: 0.02 FAR

Low Density Residential (LDR)

This designation establishes areas for single-family residences with individual homes on lots generally ranging from 12,500 square feet to one acre. Uses typically allowed include detached single-family homes; secondary dwellings; and residential support uses such as churches, schools, and other necessary public utility and safety facilities.

This designation is typically found inside communities or on the outside edge of UDBs.

Maximum Density: 1-4 Dwelling Units/Acre

Low-Medium Density Residential (LMDR)

This designation establishes areas suitable for single-family neighborhoods at relatively low densities on lots ranging from generally 5,000 to 12,500 square feet in urbanized areas. Uses typically allowed include detached single-family homes; secondary dwellings; and residential support uses such as churches, schools, parks, medical facilities, and other necessary public utility and safety facilities. This designation is used only within UDBs.

Medium Density Residential (MDR)

This land designation establishes areas for single-family and low-density multi-family dwellings. Uses typically allowed include single-family dwellings, second units, townhomes, duplexes, triplexes, and mobile home parks. This designation is used only within UDBs.

Maximum Density: 4-14 Dwelling Units/Acre

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High Density Residential (HDR)

This designation established areas for multi-family dwellings in urbanized areas. Uses typically allowed include duplexes, townhouses, and apartments located near schools, parks, and other public services. This designation is used only within UDBs. Dwelling Units are based on Gross Acreage and development shall be no less than that identified as the intensity per gross acreage High Density Residential designated lands.

Maximum Density: 14-30 Dwelling Units/Acre

Neighborhood Commercial (NC)

This designation establishes areas for small-scale, general retail, and service businesses that provide goods to the immediate surrounding area. Uses typically allowed include food and beverage retail sales; limited personal, medical, professional, and repair services; and retail sales. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

General Commercial (GC)

This designation establishes areas for small, localized retail, recreational, and service businesses that provide goods and services to the surrounding community. Uses typically allowed include eating and drinking establishments; food and beverage retail sales; limited personal, medical, professional services; repair services; and retail sales. Such facilities may range from a single use to a cluster of uses such as a shopping center. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

Service Commercial (SC)

This designation establishes areas for service commercial uses in urbanizing areas. Uses typically allowed include automotive-related or heavy equipment sales and services; building maintenance services; construction sales and services; and warehousing. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

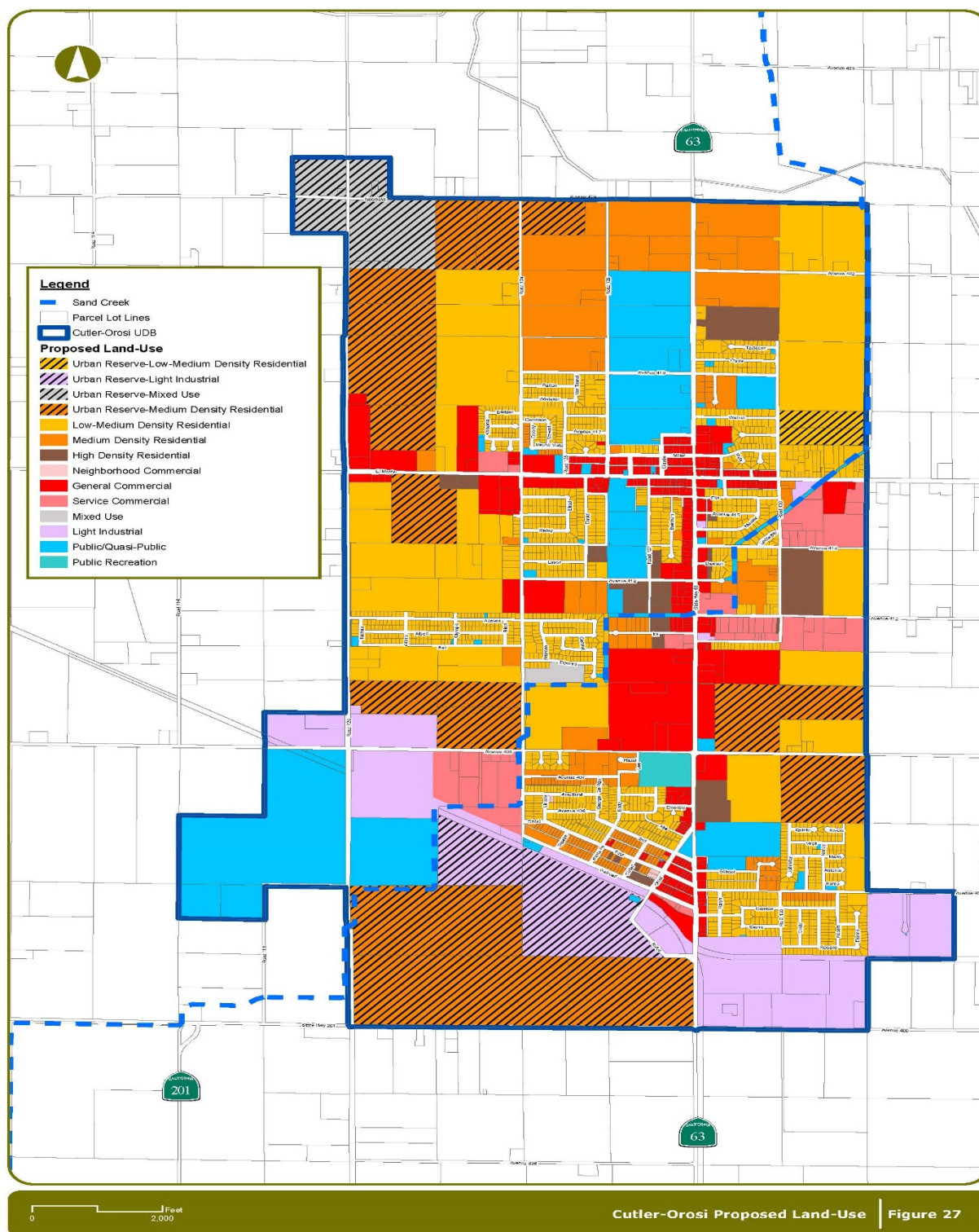
Light Industrial (LI)

This designation establishes areas for a range of non-intensive business park, industrial park, and storage uses that do not have detrimental noise or odor impacts on surrounding urban uses. Uses typically allowed include: warehousing; welding, and fabrication shops; manufacturing and processing; and business support uses such as retail or eating establishments that serve adjacent light industrial uses and employees. This designation is found primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

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Figure 27 - Proposed Land Use Plan - Cutler/Orosi



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Heavy Industrial (HI)

This designation establishes areas for the full range of industrial uses, which may cause noise or odor impacts on surrounding urban uses. Uses typically allowed include manufacturing; processing; fabrication; ethanol plants; warehouses; asphalt batch plants; mills; wood processing yards; and support uses such as retail or eating establishments that support adjacent heavy industrial uses and employees. This designation is found both primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

Proposed Zoning Districts

The proposed Zoning Districts Map (see **Figure 28**) for Cutler/Orosi is compatible to the Land Use Map outlined in the General Plan. Zoning changes that need to occur to allow the General Plan and Zoning Ordinance to be in conformity with each other (see **Table 47**).

Table 47 - Proposed Zoning Districts	
Zoning Districts	Acres
AE-10	893.8
C-1	0.3
C-1-MU	0.5
C-2-MU	253.3
C-3-MU	108.1
M-1	110.3
M-1-MU	127.5
M-2	39.9
R-1	805.9
R-1-MU	23.9
R-2	405.1
R-2-MU	15.3
R-3	30.1
R-3-MU	54.4
Unclassified (Right-of-Way)	285.6
Total	3,154.0
Source: Tulare County GIS	

Proposed Zoning Districts

The **MU (Mixed Use) Overlay Combining** Zone allows a mix of uses that promotes flexibility in the types of entitlements that can be issued. All uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 uses are allowed.

The **R-1 (One Family)** Zone allows one-family dwellings units of a permanent character placed in permanent locations and one-family manufactured homes installed on a foundation system pursuant to Section 18551 of the California Health and Safety Code which comply with Subsection G of this Section. Private garages to accommodate not more than three (3) cars.

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The **R-2 (Two Family)** Zone allows any use permitted in the R-1 Zone, two-family dwellings, multiple dwellings subject to site review, and incidental and accessory uses to the aforesaid.

The **R-3 (Multiple-Family)** Zone is any use permitted in the R-2 Two-Family Zone. Multiple dwellings; provided, however, that if more than four (4) dwelling units are proposed to be constructed on one (1) lot, the construction of such units shall be subject to approval of a site plan pursuant to the procedure set forth in Paragraph 1 of Subsection G of Section 16.2 of this Ordinance.

The **C-1 (Neighborhood Commercial)** Zone is intended for retail stores and personal service businesses, which are appropriately located in close proximity to residential areas, while minimizing the undesirable impact of such uses in the neighborhoods, which they serve.

The **C-2 (General Commercial)** Zone is intended for retail stores and businesses, which do not involve manufacturing and/or processing.

The **C-3 (Service Commercial)** Zone is intended for wholesale establishments and establishments engaged in repairing and servicing equipment, materials and products, but which do not involve the manufacturing, assembling, packaging or processing of articles of merchandise for distribution and retail sales.

The **M-1 (Light Manufacturing)** Zone is intended for establishments engaged in the manufacturing, assembling, packaging, treatment and processing of products other than those which may be obnoxious or offensive by reason of emission of odor, dust, smoke, gas, noise, or other similar causes.

The **M-2 (Heavy Manufacturing)** Zone is intended for establishments engaged in the M-1 Zone, gas, boiler works, ovens, mills, canning, plastics, machining, quarry, wood processing and other similar causes.

Mixed Use - Any combination of retail/commercial, service, office, residential, hotel, or other use in the same building or on the same site typically configured in one (1) of the following ways:

- **Vertical Mixed Use.** A single structure with the above floors used for residential or office use and a portion of the ground floor for retail/commercial or service uses.
- **Horizontal Mixed Use – Attached.** A single structure which provides retail/commercial or service use in the portion fronting the public or private street with attached residential or office uses behind.
- **Horizontal Mixed Use – Detached.** Two (2) or more structures on one (1) site which provide retail/commercial or service uses in the structure(s) fronting the public or private street, and residential or office uses in separate structure(s) behind or to the side.

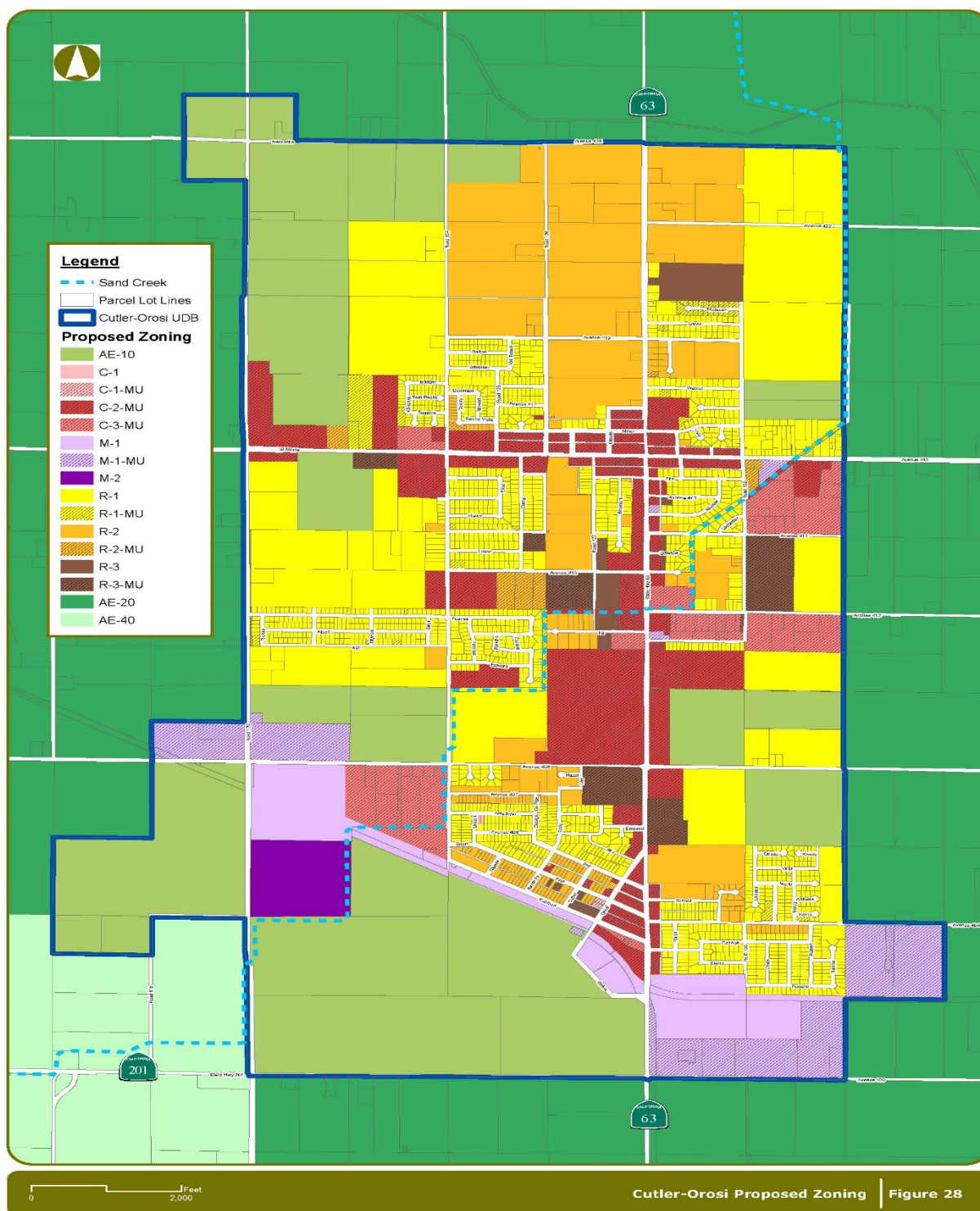
Mixed Use allows for a variety of development projects. By allowing the community of Cutler/Orosi to respond to market forces, more opportunities are created for economic development and job development.

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The **AE-10 (Exclusive Agricultural Zone 10 Acre Minimum)** The AE-10 Zone is an exclusive zone for intensive and extensive agricultural uses and for those uses, which are a necessary and integral part of intensive and extensive agricultural operations.

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Figure 28 - Proposed Zoning Districts Map



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Circulation Element

The purpose of this Circulation Element Update for the community of Cutler/Orosi is to provide for a safe, convenient and efficient transportation system. The Circulation Element has been designed to accommodate anticipated transportation needs based on the land use element. In compliance with state law, all city and county general plans must contain a circulation element that designates future road improvements and extensions, addresses non-motorized transportation alternatives, and identifies funding options. The intent of this Circulation Element is to:

- Identify transportation needs and issues within Cutler/Orosi, as well as regional relationships that affect the transportation system;
- Consider alternatives to the single-occupant vehicle as means of providing services and access to facilities; and
- Establish policies that coordinate the Cutler/Orosi transportation and circulation system with General Plan and area plan land use maps and provide direction for future decision-making.

Figure 29 shows Cutler/Orosi in the context of its region. The transportation system within the Cutler/Orosi planning area includes SR 63, SR 201 and Avenue 416 as well as several County routes and local streets as shown on **Figure 30**. This figure also shows key intersections that were selected for detailed analysis.

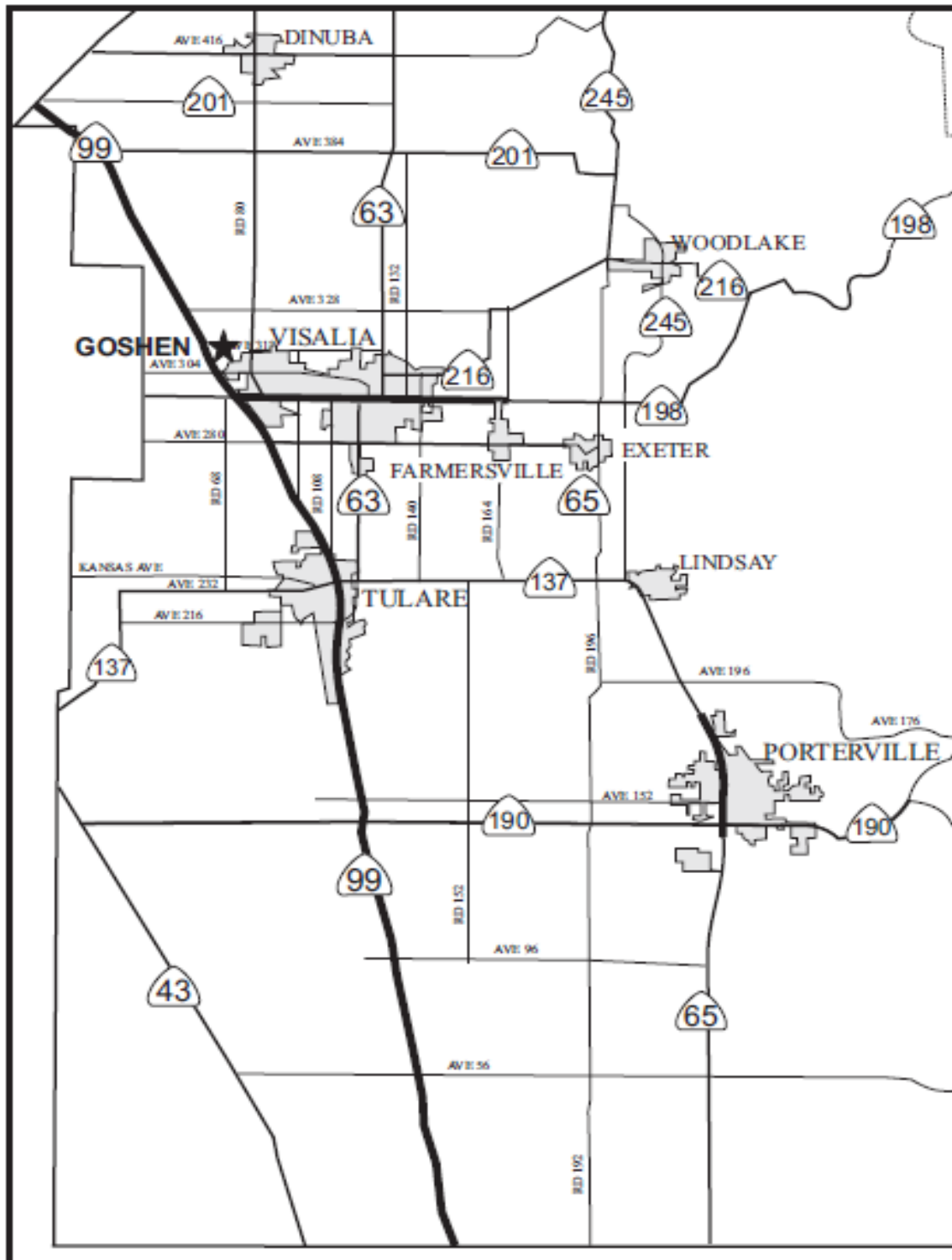
Existing Transportation Conditions

Cutler/Orosi have an excellent circulation system in terms of access to other parts of the County. SR 201 and several County roads provide access to SR 99, 20 miles to the west, and the foothill region to the east. SR 99 is an important route used by industry to move goods to the southern and northern parts of the State. It also provides commuters with access to the Central Valley's larger metropolitan areas. SR 63 is another major north/south route in this region. It connects Cutler/Orosi with Visalia and it is the prime route between the two communities.

The communities also have a local circulation system, that when completed, will tie them together and create an excellent framework from which minor neighborhood streets can be built. Roads 120, 124, 128 SR 63, 130, and 136 traverse the planning area in a north/south fashion and Avenues 425, 416, 408, and 400 (SR 201) are laid out on an east-west axis. Several roadways must either be built or widened in order to create an efficient circulation system. Most notably, there is an absence of a north-south road east of SR 63 which would connect Cutler to Orosi, and a road system southwest of Cutler which would "open up" land for industrial development.

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Figure 29 - Roadway Network of County



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The internal circulation system of Cutler/Orosi is composed of paved streets with a curb-to-curb width of 40 feet. These “minor” streets provide circulation within each neighborhood of the community. Although they all have ample capacity for additional traffic, the condition of some of these streets is poor because they lack curbs and/or gutters or their pavement condition is deteriorating. Further, there are numerous areas in Orosi, which are composed of large-lot rural residential development and are not “linked” to adjacent neighborhoods.

Street and Highway System

Functional classification is the process by which streets and highways are grouped into classes according to the type of service they provide. Streets and highways are classified according to their primary function and may be assigned into several basic classifications:

- State Highways (which may be freeways, expressways or conventional highways)
- Arterials and Collectors
- Local Streets

State Highways connect regional destinations and generally pass through several jurisdictions. Traffic carrying capacity is maintained through access control at two-mile or more intervals, with shorter intervals between access points permitted in large urban areas. There are two designated State Routes within the Planning Area.

- State Route 63
- State Route 201

Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the community area and connect with important county roads and state highways. They also provide for the distribution and collection of through traffic to and from collector and local streets. There is one designated “Arterial” street within the Planning Area.

- Avenue 416

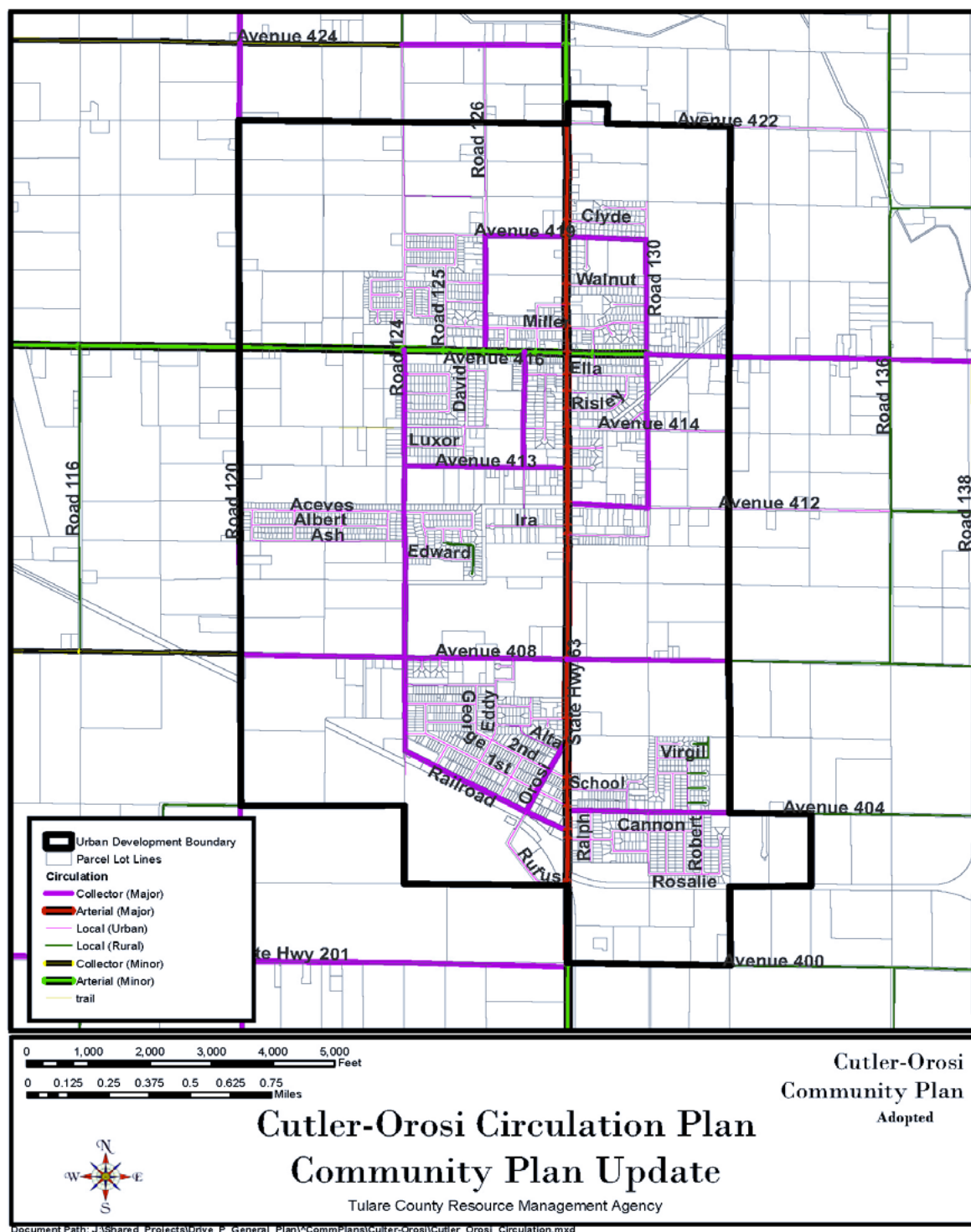
Collectors provide for traffic movement between arterial and local streets, traffic movement within and between neighborhoods and major activity centers, and limited direct access to abutting properties.

Local streets provide for direct access to abutting properties and for localized traffic movements within residential, commercial and industrial areas.

- All streets in the Circulation network are classified as local streets.

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Figure 30 - Community of Cutler/Orosi Circulation Plan



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Regional Transportation Planning

Tulare County Association of Governments (TCAG) Regional Transportation Plan

The Regional Transportation Plan (RTP) is a multi-modal, long-range planning document prepared by the Tulare County Association of Governments (TCAG). The RTP includes programs and policies for congestion management, transit, bicycles and pedestrians, roadways, freight, and finances for Tulare County. The RTP is prepared every four years and contains a listing of projects considered to be financially feasible within a 25-year planning time frame. All federally funded transportation projects must be consistent with the RTP.

The RTP for Tulare has just been updated and was adopted in July 2014. The new RTP is the first to respond to state legislation (SB 375) that requires that the RTP show reductions in greenhouse gas emissions from passenger vehicles. Thus, there is a new emphasis in the RTP on promoting ridesharing (transit, van and carpools) and active transportation (walking and bicycling). To this end, the RTP now includes a Sustainable Communities Strategy (SCS), a blueprint for land use patterns and transportation facilities and services that will facilitate fewer vehicle trips and vehicle miles traveled.

San Joaquin Valley Air Quality Management Plan

The San Joaquin Valley Air Pollution Control District (SJVAPCD) has prepared the Air Quality Management Plan (AQMP) and various other regulations to reduce air emissions. Both the plan and several regulations aim to reduce emissions from mobile sources – automobiles and trucks, as well as other modes of transportation.

Measure R ½ Percent Sales Tax for Transportation

Measure R is the half-percent sales tax measure for transportation improvements passed by the voters of Tulare County in 2006 and managed by the Tulare County Transportation Authority (TCTA). The Measure provides funding for transportation projects (highway, transit, and ridesharing) over the 20-year duration of the Measure. Measure R funds are used by the County in Cutler/Orosi to repair streets, and to improve the existing and planned transportation system.

Public Transit and Active Transport Systems

While the private automobile is the dominant mode of travel within Cutler/Orosi, as it is throughout Tulare County, other modes of transportation are important. The latest available Census survey data for Cutler/Orosi indicates that about two-third of commuters drive alone to work, while one-third use other means: 14 percent carpool or vanpool, 9 percent walked, 6 percent used public transportation and 5 percent worked at home. The Census bureau does not collect data on non-work trips, which represent a greater share of travel than work trips, but tend to be less concentrated in peak traffic periods. Off-peak trips also tend to have a greater proportion of shared ride and active (walk and bike) trips. While congestion is not a major issue in Cutler/Orosi, overreliance on automobiles creates other costs for both society and households, and means that many in the community who cannot drive (the young, the old, the disabled, the poor) must rely on those who can drive for their mobility. For this reason, it is important to encourage public transit systems and

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increased use of active modes of transportation, including bicycles and walking. The public transit system alternatives for Cutler/Orosi include fixed route public transit systems, common bus carriers, and other local agency transit and paratransit services.

Transportation Demand Management

Transportation Demand Management (TDM) strategies reduce dependence on the single-occupant vehicle, increase the ability of the existing transportation system to carry more people, and enhance mobility. Examples of TDM strategies include telecommuting, flexible work hours, and electronic commerce that enable people to work and shop from home. According to CalVans, the major vanpool broker in the Valley, vanpools are becoming more prevalent for short-to-medium range commute trips, as well as for traditional long-distance usage: Key vanpool users include agricultural workers, and employees at large firms and government agencies. Park-n-ride facilities and carpooling will also continue to be a significant link between highway and transit modes. For the remainder of the study area, an overall rate of traffic growth of one percent per year was determined to be a reasonable forecast assumption. It is not likely that TDM strategies would be very effective due to Popular/Cotton Center's current and projected small population and lab

Road Capacity and Level of Service (LOS)

LOS is categorized by two parameters, uninterrupted flow and interrupted flow. Uninterrupted flow facilities have no fixed elements, such as traffic signals, that cause interruptions in traffic flow (e.g., freeways, highways, and controlled access, some rural roads). Interrupted flow facilities have fixed elements that cause an interruption in the flow of traffic such as stop signs and signalized intersections. The definitions and measurements used for determining level of service in interrupted and uninterrupted conditions are shown in **Tables 48 and 49**.

In Tulare County, General Plan Policy **TC – 1.16 County Level of Service (LOS) Standards** states; “The County shall strive to develop and manage its roadway system (both segments and intersections) to meet a LOS of “D” or better in accordance with the LOS definitions established by the Highway Capacity Manual.”⁴⁸

⁴⁸ Tulare County General Plan 2030 Update, Part 1 – Goals and Policies Report. Page 13-7.

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Table 48 - Uninterrupted Traffic Flow Facilities	
LEVEL OF SERVICE	DEFINITION
A	Describes free-flow operations. Free-Flow Speed (FFS) prevails on the freeway, and vehicles are almost completely unimpeded in their ability to maneuver within the traffic stream. The effects of incidents or point breakdowns are easily absorbed.
B	Represents reasonably free-flow operations, and FFS on the freeway is maintained. The ability to maneuver within the traffic stream is only slightly restricted, and the general level of physical and psychological comfort provided to drivers is still high. The effects of minor incidents and point breakdowns are still easily absorbed.
C	Provides for flow with speeds near the FFS of the freeway. Freedom to maneuver within the traffic stream is noticeably restricted, and lane changes require more care and vigilance on the part of the driver. Minor incidents may still be absorbed, but the local deterioration in service quality will be significant. Queues may be expected to form behind any significant blockages.
D	At this level speeds begin to decline with increasing flows, with density increasing more quickly. Freedom to maneuver within the traffic stream is seriously limited and drivers experience reduced physical and psychological comfort levels. Even minor incidents can be expected to create queuing, because the traffic stream has little space to absorb disruptions.
E	Describes operation at capacity. Operations on the freeway at this level are highly volatile because there are virtually no useable gaps within the traffic stream, leaving little room to maneuver within the traffic stream. Any disruption to the traffic stream, such as vehicles entering from a ramp or changing lanes, can establish a disruption wave that propagates throughout the upstream traffic flow. At capacity, the traffic stream has no ability to dissipate even the most minor disruption, and any incident can be expected to produce a serious breakdown and substantial queuing, the physical and psychological comfort afforded to drivers is poor.
F	Describes breakdown, or unstable flow. Such conditions exist within queues forming behind bottlenecks. Breakdowns occur for a number of reasons: <ul style="list-style-type: none"> • Traffic incidents can temporarily reduce the capacity of a short segment, so that the number of vehicles arriving at a point is greater than the number of vehicles that can move through it. • Points of recurring congestion, such as merge or weaving segments and lane drops, experience very high demand in which the number of vehicles arriving is greater than the number of vehicles that can be discharged. • In analyses using forecast volumes, the projected flow rate can exceed the estimated capacity of a given location.

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Table 49 - Interrupted Traffic Flow Facilities

LEVEL OF SERVICE	DEFINITION
A	Describes operations with a control delay of 10 s/veh or less and a volume-to- capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is low and either progression is exceptionally favorable or the cycle length is very short. If it is due to favorable progression, most vehicles arrive during the green indication and travel through the intersection without stopping.
B	Describes operations with a control delay between 10 and 20 s/veh and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is low and either progression is highly favorable or the cycle length is short. More vehicles stop than with LOS A, with reasonably unimpeded travel between intersections.
C	Describes operations with control delay between 20 and 35 s/veh and a volume-to- capacity ratio no greater than 1.0. This level is typically assigned when progression is favorable or the cycle length is moderate. Individual cycle failures (i.e.one or more queued vehicles are not able to depart as a result of the insufficient capacity during the cycle) may begin to appear at this level. The number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping. May be longer queues and operations between locations may be more restricted.
D	Describes operations with control delay between 35 and 55 s/veh and a volume-to- capacity ratio no greater than 1.0. Travel speeds are about 40 percent below free flow speeds. This level is typically assigned when the volume-to-capacity ratio is high and either progression is ineffective or the cycle length is long. Many vehicles stop and individual cycle failures are noticeable.
E	Describes operations with control delay between 55 and 80 s/veh and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is high, progression is unfavorable, and the cycle length is long. Individual cycle failures are frequent. Average travel speed is one-third of free flow speeds. The facility is generally at full capacity.
F	Describes operations with control delay exceeding 80 s/veh or a volume-to-capacity ratio greater than 1.0. This level is typically assigned when the volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue. Extremely slow speeds with average delay of 80 seconds or more. Frequent stop and go conditions.

Transportation Concept Report (TCR) State Route (SR) 63

The TCR defines the appropriate route concept level of service (LOS) and facility type(s) for SR 63. The route is currently operating at a LOS of “B” and “C.” By the year 2020, in some segments it will drop to a LOS “D” and “E”. Route Segment 10 runs between 0.1 mile north of Avenue 403 to Avenue 422 and is the ultimate transportation corridor concept.

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Complete Streets

The Board of Supervisors approved the Complete Streets Program on December 2016 (**see Attachment A-6**). The Complete Streets Programs Goals, Policies, Objectives, and Standards are hereby incorporated by reference. Included in the plan were policies and implementation measures as provided below. These projects have been included on the TCAG Measure R list as Complete Streets.

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Road
5. First Drive – SR 63 to Road 124

Orosi

1. Avenue 413 – Road 124 to SR 63
2. Avenue 419
3. Avenue 416 – SR-63 to Dinuba
4. Road 130 (Strong interest from school district)
5. Road 124

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Goals, Policies, and Standards

The intent of the Cutler/Orosi Community Circulation Element is to establish a comprehensive multi-modal transportation system that is efficient, environmentally and financially sound, and coordinated with the Land Use Element.

Goal 1: Design and implement a multi-modal transportation system that will serve projected future travel demand, minimize congestion, and address future growth in Cutler/Orosi.

Policies and Standards:

1. Utilize existing infrastructure and utilities to the maximum extent practical and provide for the logical, timely, and economically efficient extension of infrastructure and services.
2. Designate streets according to the following functional classifications:
 - a) Freeways and Expressways carry regional traffic through the community with access only at interchanges with major streets.
 - b) Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the urban area and connect with important county roads and state highways. They also provide for the distribution and collection of through traffic to and from collector and local streets.
 - c) Collectors provide for traffic movement between arterial and local streets, traffic movement within and between neighborhoods and major activity centers, and limited direct access to abutting properties.
 - d) Local streets provide for direct access to abutting properties and for very localized traffic movements within residential, commercial and industrial areas.All facility-types above (except freeways) should be capable of accommodating transit and paratransit vehicles. Furthermore, all facility-types except freeway should include provisions for active modes of transportation (walking and cycling).
3. Develop and apply consistent standards for new streets (and existing streets where feasible without substantial ROW takes) based on the roadway classification.
4. Require applicants for new development projects to dedicate needed ROW and construct and/or upgrade to County standards the streets and roads that will serve their projects.
5. Plan new arterial and collector streets as needed to improve access and enhance the develop potential of land designated for commercial and industrial uses.
6. Improvement standards for local and minor streets shall include perpendicular curbs, gutters and adequate street lighting at intersections.
7. Access to arterials by driveways, local and minor streets, and alleys should be controlled as needed in order to ensure efficient traffic flow and safety along these streets.
8. Local streets should be designed to discourage high traffic volumes and through traffic.
9. Develop a Circulation Map showing the public street system. Designated streets and recommended rights-of-way should be indicated on this map.
10. Allow standards for new street development to be altered or refined where it can be demonstrated that projected traffic flows can be accommodated.
11. Plan for peak-hour Level of Service (LOS) "D" or better throughout the circulation network.

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12. Make intersection improvements to the existing major street system selectively, favoring traffic engineering solutions rather than major structural improvements. This could include signalization, intersection channelization, use of directional signs, and diversion of traffic onto underutilized streets.
13. Use complete streets concepts in the design of new local streets where such techniques will improve safety and manage traffic flow.
14. Ensure the street network provides efficient routes for emergency vehicles, meeting necessary street widths, turn around radius, and other factors as determined by the County in consultation with fire and other emergency service providers.
15. Cooperate with local, regional, State and federal agencies to plan for, establish and maintain good connectivity to an efficient multimodal regional transportation system.

Goal 2: Provide designated routes and loading standards that reduce the noise and safety concerns associated with truck traffic.

Policies and Standards:

1. Designate truck routes for use by heavy commercial and industrial traffic. Initially, designated truck routes shall be:
 - SR 63
 - SR 201
 - Avenue 416
2. Design interior street systems for commercial and industrial subdivisions to accommodate the movement of heavy trucks.
3. Restrict heavy-duty truck through-traffic in residential areas and plan land uses so that trucks do not need to traverse these areas.

Design off-street loading facilities for all new commercial and industrial developments so that they do not face surrounding roadways or residential neighborhoods. Truck backing and maneuvering to access loading areas shall not be permitted on the public road system, except when specifically permitted by the County Engineer.

Goal 3: Provide safe and convenient pedestrian access between residential neighborhoods, parks, open space, and schools that service those neighborhoods.

Policies and Standards:

1. Provide a safe walking environment for pedestrians.
 - a) New development should include safe and pleasant designs, which promote pedestrian access to arterials and collectors and consider the location of community services, such as schools, parks and neighborhood shopping activity centers in the accessibility of their design for all persons.
 - b) Require the installation of sidewalks as an integral part of all street construction where appropriate.

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- c) Require street lighting within the rights-of-way of all public streets.
- d) Include pedestrian signal indicators as an integral part of the installation of traffic signals.
- 2. Maximize visibility and access for pedestrians and encourage the removal of barriers (walls, easements, and fences) for safe and convenient movement of pedestrians. Special emphasis should be placed on the needs of disabled persons considering ADA regulations.
- 3. Plan for pedestrian access consistent with road design standards while designing street and road projects. Provisions for pedestrian paths or sidewalks and timing of traffic signals to allow safe pedestrian street crossing shall be included.
- 4. Collaborate with the Cutler/Orosi Schools and the School District to ensure that schoolchildren have adequate transportation routes available, such as a local pedestrian or bike paths, or local bus service.
- 5. Encourage safe pedestrian walkways within commercial, office, industrial, residential, and recreational developments that comply with the Americans with Disabilities Act (ADA) requirements.
- 6. Coordinate with TCaT and private bus operators to ensure that pedestrian facilities are provided along and/or near transit routes, whenever feasible. New land developments may be required to provide pedestrian facilities due to existing or future planned transit routes even if demand for a pedestrian facility is not otherwise warranted.
- 7. Review all existing roadways without pedestrian facilities when they are considered for improvements (whether maintenance or upgrade) to determine if new pedestrian facilities are warranted. New roadways should also be assessed for pedestrian facilities.

Goal 4: Ensure the provision of adequate off-street parking for all land uses.

Policies and Standards:

- 1. Require all new development to identify adequate on street and off-street parking based on expected parking needs.
- 2. Encourage shared parking among nearby uses with complementary parking demand patterns.
- 3. Provide adequate loading areas within off-street parking areas for all commercial and manufacturing land uses.
- 4. Anticipate parking needs at proposed and expected activity centers, particularly commercial areas.

Goal 5: Provide a transportation system that is integrated with the region.

Policies and Standards:

- 1. Coordinate local transportation planning with the TCAG Congestion Management Plan to ensure eligibility for state and federal funding.
- 2. Incorporate the Regional Transportation Plan, and the Tulare County Short- and Long-Range Transit Plans into the Community Plan Circulation Element, and encourage the active participation of Caltrans in the design of highway capital improvement projects.

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Goal 6: Encourage the use of public transit services to reduce reliance on the automobile.

Policies and Standards:

1. Encourage transit alternatives to meet the basic transportation needs of the young, the elderly, the handicapped, and people without access to an automobile.
 - a) Consider development of an integrated transit center within Cutler/Orosi where all transit services can connect with each other as well as with private ridesharing.
 - b) Encourage and provide for ridesharing, park and ride, and other programs that can reduce emissions, save energy, and reduce monetary costs for firms and workers.
2. Planning and development of arterial and collector streets shall include design features which can be used as future public transit stops.
3. Support the expansion and improvement of transit systems and ride sharing programs to reduce the production of automobile emissions.
4. Support the use of alternate fuel vehicles and fueling stations for public transit vehicles, and County public agency vehicles.
5. Support TCaT and other transit operators' programs to foster transit usage.
6. Support all operator efforts to maximize revenue sources for short and long range transit needs that utilize all funding mechanisms available including federal grants, state enabling legislation, and farebox revenue. This can be accomplished through TCAG and the Tulare County Transit Agency (TCaT) through the development of the Short and Long Range Transit Plans.
7. Support programs developed by transit agencies/operators to provide paratransit service.
8. Incorporate the potential for public transit service in the design of developments identified as major trip attractions (i.e. community centers and employment centers).
9. Explore potential development of a park-n-ride lot in Cutler/Orosi.
10. Support continued improvements to AMTRAK rail passenger service within Tulare County and throughout the San Joaquin Valley.

Goal 8: Provide efficient goods movement

Policies and Standards:

1. Encourage the efficient movement of goods and people by rail through a shift of a portion of the goods previously moved by trucks onto the rail freight system.
2. Implement Street and highway projects to provide convenient and economical goods movement, including access to rail terminals, in areas where large concentrations of truck traffic exist.
3. Identify street and highway improvement and maintenance projects that will improve goods movement and implement projects that are economically feasible.
4. Encourage use of rail for goods movement whenever feasible.

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Goal 9: Provide safe and convenient facilities for non-motorized modes of transportation that enhance the future livability and character of Cutler/Orosi.

Policies and Standards:

1. Consider developing a Bikeway plan for Cutler/Orosi based on the following facility designations:
 - a) Bike Path (Class I). A special pathway for the exclusive use of bicycles, which is separated from motor vehicle facilities by space or a physical barrier. It is identified by guide signing and pavement markings.
 - b) Bike Lane (Class II). A lane on the paved area of a road for preferential use by bicycles. It is usually located along the right edge of the paved area or between the parking lane and the first motor vehicle lane. It is identified by a "Bike Lane" guide sign, special lane lines, and other pavement markings.
 - c) Bike Route (Class III). A recommended route for bicycle travel along existing rights-of-way, which is signed but not striped.
 - d) Bikeway. All facilities that explicitly provide for bicycle travel. The bikeway can be anything from a separate facility to a simple signed street.
2. Give priority to bikeways that will serve the highest concentration of cyclists and destination areas of highest demand, especially Cutler/Orosi Elementary Schools.
3. Provide bikeways in proximity to major traffic generators such as commercial centers, schools, recreational areas, and major public facilities.
4. Develop a visually clear, simple, and consistent bicycle system with standard signs and markings, as designated by the State of California Traffic Control Devices Committee and the State Bikeway Committee.
5. Support the installation of bike parking racks at public and private places of assembly such as parks, schools, employment sites, churches, and retail commercial developments.
6. Provide non-motorized alternatives for commuter travel as well as recreational opportunities.
7. Provide separate rights-of-way for non-motorized facilities whenever economically and physically feasible.
8. Develop bikeways in compliance with the standards established in the Caltrans Highway Design Manual or other appropriate standards.

Goal 10: Design, construct, and operate the transportation system in a manner that maintains a High level of environmental quality.

Policies and Standards:

1. Control dust and mitigate other environmental impacts during all stages of roadway construction.
2. Protect residents from transportation generated noise hazards. Increased setbacks, walls, landscaped berms, other sound absorbing barriers, or a combination thereof shall be provided along four lane highways in order to protect adjacent noise-sensitive land uses from traffic generated noise impacts. Additionally, noise generators such as commercial, manufacturing, and/or industrial activities shall use these techniques to mitigate exterior noise levels to no more than 60 decibels.

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3. Review and monitor proposals for expansion of pipelines for the transport of suitable products and materials, and require mitigation of environmental impacts.
4. Encourage the use of non-polluting vehicles for both public and private uses.
5. Include noise mitigation measures in the design of roadway projects in Cutler/Orosi.

Goal 11: Support the use of Transportation Demand Management (TDM) strategies to reduce dependence on the single-occupant vehicle, increase the ability of the existing transportation system to carry more people, and enhance mobility along congested corridors.

Policies and Standards:

1. New development shall consider Transportation System Management and Transportation Demand Management as strategies for the mitigation of traffic and parking congestion. Public transit, traffic management, ride sharing and parking management are to be used to the greatest extent practical to implement transportation management strategies.
2. Coordinate with Caltrans, TCAG, transit agencies and other responsible agencies to identify the need for additional park-n-ride facilities along major commuter travel corridors.

Goal 12: Utilize Intelligent Transportation Systems (ITS) to improve the safety and performance of the surface transportation system using new technology in detection, communication, computing, and traffic control.

Policies and Standards:

1. Encourage the integration of Intelligent Transportation Systems (ITS) consistent with the principles and recommendations referenced in the TCAG Regional Transportation Plan

Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2010 Tulare County Regional Bicycle Transportation Plan (**see Figure 31**) has a proposed Class II Bike Project.⁴⁹ In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns.

The Caltrans Highway Design Manual (HDM), which governs bicycle facility design in California, distinguishes three (3) different types of bicycle facilities.

Class I: Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County Class I facilities will primarily be implemented through TCAG. Currently there is a proposed Class I on Avenue 416.

⁴⁹ TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

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Class II bikeways that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. No proposed Class II bicycle facilities in the Cutler/Orosi area.

Class III: Bikeway that provides designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III bicycle facilities are not proposed in Cutler/Orosi areas.

Although not signed on many local roads in Cutler/Orosi, bicyclists are allowed to use the side of the road or share the road on all County roadway facilities excluding freeways.

Multiuse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. No multiuse trails exist or are proposed in Cutler/Orosi.

Pedestrian Paths and Sidewalks

A review of facilities for pedestrian travel in Cutler/Orosi presents a broad array of conditions in which pedestrians are accommodated. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the competition of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

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Paratransit Service

Services are transportation services such as carpooling, vanpooling, taxi service, and dial-a-ride programs. The County supports reliable and efficient paratransit service by encouraging development of service systems that satisfy the transit needs of the elderly and physically handicapped.

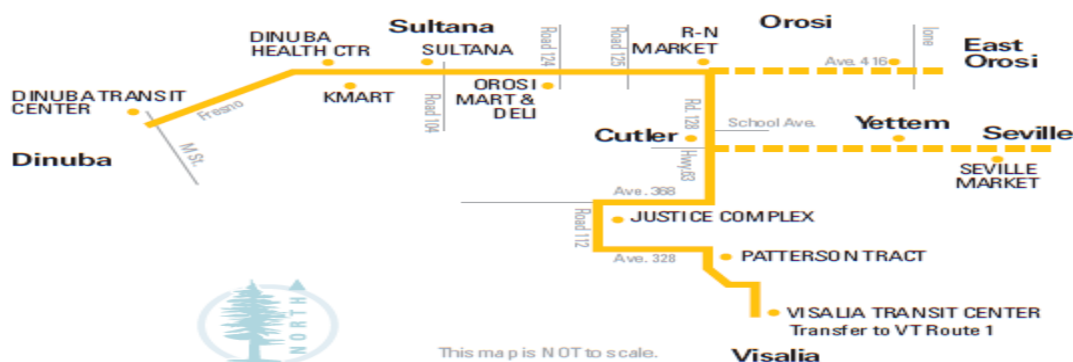
Park-and-Ride Lots

Park-and-Ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the Community's population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community along State Route 65.

Transit and Bus Stops

The Tulare County Transit Agency (TCaT) operates fixed-route services that link communities with each other and with Visalia and Dinuba's urban transit systems. Cutler/Orosi (see **Figure 32**) is connected via TCaT North County Route 10. Route 10 (see **Figure 33**) has twelve northbound and southbound buses serving Cutler/Orosi on weekdays and four buses in each direction on Saturdays and Sundays. Stops are currently located at the Road 1287 and School Avenue in Cutler/Orosi has two stops at R-N Market and Orosi Mart & Deli. (See TCaT website at <http://www.tularecog.org/bustimes/>. TCaT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Cutler/Orosi, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

Figure 32 – TCaT North County Route 10



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Figure 33 - TCaT Route 10 Bus Schedule

Northbound

WEEKDAY SERVICE

VISALIA Transit Center	VISALIA Justice Complex	SEVILLE Seville Market	CUTLER Rt. 128 & School Ave	E OROSI Ave. 416 & Jonia Rd.	OROSI R-H Market	OROSI Orosi Mart & Deli	SULTANA Ave. 416 & Rd. 104	DINUBA K-Mart	DINUBA Transit Ctr
6:15	6:34	—	6:44	—	6:48	6:50	6:54	6:57	7:04
7:15	7:34	—	7:44	—	7:49	7:51	7:55	7:58	8:05
8:15	8:34	—	8:44	8:49	8:53	8:55	8:59	9:02	9:09
9:10	9:29	9:40	9:49	—	9:53	9:55	9:59	10:02	10:09
10:15	10:34	—	10:44	10:51	10:55	10:57	11:01	11:04	11:11
11:15	11:34	—	11:44	—	11:48	11:50	11:54	11:57	12:05
1:15	1:34	—	1:44	—	1:48	1:50	1:54	1:57	2:04
2:10	2:29	2:40	2:49	—	2:53	2:55	2:59	3:02	3:09
3:15	3:34	—	3:44	—	3:48	3:50	3:54	3:57	4:04
4:15	4:34	—	4:44	—	4:48	4:50	4:54	4:57	5:04
5:15	—	—	5:40	—	5:44	5:46	5:50	5:53	6:00
6:15	—	6:36	6:45	—	6:49	6:51	6:55	6:58	7:05

WEEKEND SERVICE

10:25	10:44	—	10:54	—	10:58	11:00	11:04	11:07	11:14
1:09	1:28	—	1:38	—	1:42	1:44	1:48	1:51	1:58
2:58	3:17	—	3:27	—	3:31	3:33	3:37	3:40	3:47
4:47	5:06	5:17	5:27	—	5:31	5:33	5:37	5:40	5:47

Southbound

WEEKDAY SERVICE

DINUBA Transit Ctr	DINUBA K-Mart	SULTANA Ave. 416 & Rd. 104	OROSI Orosi Mart & Deli	OROSI R-H Market	E OROSI Ave. 416 & Jonia Rd.	CUTLER Rt. 128 & School Ave	SEVILLE Seville Market	VISALIA Justice Complex	VISALIA Transit Center
6:15	6:21	6:24	6:28	6:30	—	6:34	—	6:45	7:05
7:10	7:17	7:20	7:24	7:26	—	7:30	7:40	7:51	8:10
8:15	8:21	8:24	8:28	8:30	—	8:34	—	8:45	9:05
9:15	9:21	9:24	9:28	9:30	—	9:34	—	9:45	10:05
10:15	10:21	10:24	10:28	10:30	—	10:34	—	10:45	11:05
11:15	11:21	11:24	11:28	11:30	—	11:34	—	11:45	12:05
1:05	1:11	1:14	1:18	1:20	1:25	1:30	—	1:41	2:01
2:15	2:21	2:24	2:28	2:30	—	2:34	—	2:45	3:05
3:15	3:21	3:24	3:28	3:30	3:34	3:41	—	3:52	4:12
4:15	4:21	4:24	4:28	4:30	—	4:34	—	4:45	5:05
5:15	5:21	5:24	5:28	5:30	—	5:34	5:47	—	6:10
6:15	6:21	6:24	6:28	6:30	6:34	6:41	—	—	7:05

WEEKEND SERVICE

9:20	9:26	9:29	9:33	9:35	—	9:39	9:50	10:00	10:20
11:19	11:25	11:28	11:32	11:34	—	11:38	—	11:49	12:09
2:03	2:09	2:12	2:16	2:18	—	2:22	—	2:33	2:53
3:52	3:58	4:01	4:05	4:07	—	4:11	—	4:22	4:42

Light type = AM

Bold type = PM

① Free transfer to VT Route 1

② Accept transfer from VT Route 1 with additional fee

① Transferencia gratis a VT Ruta 1

② Se acepta transferencia de VT Ruta 1 con cargo adicional

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AMTRAK

The Hanford AMTRAK station, located approximately 45 miles southwest in Kings County is the closest station to Cutler/Orosi. Amtrak's San Joaquin route links Hanford to Sacramento to the north and Bakersfield to the south. An Amtrak bus can be taken from Bakersfield to Los Angeles Union station where Amtrak's interstate routes can be accessed along with California's Pacific Surfliner route. In Sacramento, additional interstate routes can be accessed along with the capital corridor route linking Sacramento to the Bay Area.

Aviation

The nearest airport is Sequoia Field Airport, which is located approximately seven (7) miles to the southwest. The nearest operational general aviation airport is Fresno Yosemite International Airport (FAT), approximately 60 miles north of Cutler/Orosi, is the principal passenger and airfreight airport in the central San Joaquin Valley. Visalia Municipal Airport, is approximately 40 miles northwest.

Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles south of Cutler/Orosi and offers direct flights to several destinations.

State Route 63

State Route (SR) 63 runs north and south beginning at SR 137 in the City of Tulare in Tulare County and ends at SR 180, near Squaw Valley in Fresno County. The route is located solely in Caltrans District 6, in Tulare and Fresno Counties. The route runs approximately 38 miles in length. SR 63 intersects with SR 201 before ending at SR 180. Route Segment 10 runs between 0.1 mile north of Avenue 403 to Avenue 422 and is the ultimate transportation corridor concept. Currently there are no projects funded for SR 63.⁵⁰

State Route 201

State Route 201 runs west–east in Fresno and Tulare Counties. The route covers just over 25 miles and is located solely in District 6. SR 201 was constructed in 1939 and reconstructed in 1974. There is a break in the route where it meets SR 63 and picks up again at Avenue 384 and continues to head east.⁵¹

State Route 99

State Route (SR) 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South. Cutler/Orosi is located approximately 20 miles east of SR 99.

⁵⁰ Transportation Concept Report State Route 63, California Department of Transportation, December 2014

⁵¹ Transportation Concept Report State Route 201, California Department of Transportation, December 2017

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Implementation Strategy

The purpose of this chapter is to prescribe a proposed approach to implement the general plan recommendations contained in chapters I through IV of the Cutler/Orosi Community Plan. There are several components that comprise the Cutler/Orosi Community Plan implementation strategy:

1. Zoning Code Changes
2. Complete Streets
3. Infrastructure

Zoning District Changes

As part of this Implementation Program for the Community Plan for Cutler/Orosi, there are a variety of changes to existing zoning districts. These changes are described below.

Revise Chapter 16 of the Zoning Code

Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

Elimination of SR Combining Zone

The SR combining zone requires site plan review for most uses in the combining zone. This alternative would eliminate the SR combining zone designation in the community of Cutler/Orosi. The elimination of the SR combining zone would only affect commercial zoning districts, C-2-SR, C-3-SR, and P-O-SR. This zone change would only affect approximately thirty (30) parcels in the C-2-SR, four (4) parcels in the C-3-SR, and one (1) parcel in the P-O-SR within Cutler/Orosi and as such, elimination of this combining zone would not have a noticeable effect on Cutler/Orosi.

Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Cutler/Orosi (see **Attachments A-2 and A-3**).

Zoning Map Update

The current Zoning Map for Cutler/Orosi will be amended to be compatible with the Land Use Map outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other Cutler/Orosi Proposed Zoning. In addition, there are a number parcels that need to re-zoned to adhere to the airport land use plan.

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Attachments

- A-1 Use Permit Requirement Changes (Zone Change Text)
- A-2 Mixed Use Overlay District (Zone Change Text)
- A-3 Development Standards (Mixed Use Zoning Districts)
- A-4 General Plan Land Use and Zoning Consistency Matrix
- A-5 Funding Sources
- A-6 Complete Streets
- A-7 Public Outreach

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Attachment A-1 Use Permit Requirement Changes

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A-1 Use Permit Requirement Changes (Zone Change Text)

H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (nighttime), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles), traffic (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1

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Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	
Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1

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Bookbinding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1

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Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1

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residential/School Site.		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3, *C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tearoom or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Attachment A-2 Mixed Use Overlay Districts

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A-2 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the community of Cutler/Orosi, unless otherwise provided in this Ordinance.

- PURPOSE** **A.** The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.
- APPLICATION** **B.** This overlay zone only applies to the community of Cutler/Orosi.
- USE** **C.** No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Cutler/Orosi.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses:

All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the

neighborhood, or to the general welfare of the county. All uses shall

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

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limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy-five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall,

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fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Cutler/Orosi. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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Attachment A-3 Development Standards (Mixed Use Zoning Districts)

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A-3 Development Standards (Mixed Use Zoning District)

To promote Economic Development within the Cutler/Orosi Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Cutler/Orosi. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Cutler/Orosi Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Cutler/Orosi. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be

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integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades, which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths, which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow, which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

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A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

ROOFS AND AWNINGS

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

SITE PLANNING

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

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SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

LANDSCAPING

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

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LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or rights-of-way should be screened to a height of thirty-six (36) inches above the grade with landscaping and/or low high quality fencing.

REFUSE AND STORAGE AREAS

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

LIGHTING

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

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WALLS AND FENCES

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

STREETSCAPE

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

SIGNAGE

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

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SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths

(3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

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SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage,

one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

SERVICE STATIONS AND CAR WASHES

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration

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topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

AUTO REPAIR SERVICES

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

CONSUMER STORAGE FACILITIES

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

SPECIAL CONDITIONS

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be

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consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

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Attachment A-4General Plan Land Use and Zoning Consistency Matrix

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Attachment A-4 General Plan Land Use and Zoning Consistency Matrix

Tulare County Zoning Districts General Plan Land Use Designations	R-A	R-1	R-2	R-3	C-1	C-2	C-3	M-1	M-2	AE-20	AE-40	O	CO	PO
Urban Reserve Residential														
Low Density Residential														
Low-Medium Density Residential														
Medium Density Residential														
Medium-High Density Residential														
High Density Residential														
Neighborhood Commercial														
General Commercial														
Community Commercial														
Highway Commercial														
Town Center														
Service Commercial														

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Tulare County Zoning Districts	R-A	R-1	R-2	R-3	C-1	C-2	C-3	M-1	M-2	AE-20	AE-40	O	CO	PO
Office Commercial														
Commercial Recreation														
Urban Reserve Commercial														
Mixed Use														
Planned Community Area														
Light Industrial														
Heavy Industrial														
Urban Reserve Industrial														
Public/Quasi-Public														
Public Recreation														

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Attachment A- 5 Funding Source

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CalTrans Active Transportation Program (ATP):

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation. The projects associated with the Completes Streets Program for the Community of Goshen will be suggested at the next available round of ATP funding.

Tulare County Measure R

On November 7, 2006, the voters of Tulare County approved Measure R, imposing a 1/2 cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

Local Projects (35% of Measure R Funding)

The Measure R Expenditure Plan allocated 35% of revenues to local programs. Each city and the county will receive funding based on a formula using population, maintained miles, and vehicles miles traveled. The funding will help cities and the county to meet scheduled maintenance needs and to rehabilitate their aging transportation systems.

Regional Projects (50% of Measure R Funding)

The Regional Projects Program comprises 50% of Measure R and includes specific funding for: interchange improvements, regional bridges, regional signals, regional widening projects, and signal synchronization projects. These projects provide for the movement of goods, services, and people throughout Tulare County. Major highlights of this program include the funding of regional projects throughout the county.

Bike /Transit /Environmental Projects (14% of Measure R Funding)

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

San Joaquin Valley Air Pollution Control District (SJAPCD) Bike Path Grants

The District has a grants program for the construction of bicycle infrastructure projects, including Class I (Bicycle Path Construction) or Class II (Bicycle Lane Striping) projects. These grants provide funding to assist with the development or expansion of a comprehensive bicycle-transportation network.

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Strategic Growth Council Grants (SGC)

Affordable Housing - Sustainable Communities

The SGC will allocate 50% of its Cap and Trade funding toward disadvantaged communities and 50% for affordable housing. Projects will include: affordable housing that supports infill and compact development, transit capital and programs that support transit ridership, active transportation projects (infrastructure, and non-infrastructure), TOD projects, capital projects that implement complete streets, projects that reduce CHG emissions by reducing auto trips and VMT, acquisition of easements or other approaches to protect agricultural lands under threat of development, planning to support SCS (sustainable communities scope) implementation, including local plans, must be in draft or adopted SCS, subject to SGC guidelines.

Congestion Mitigation Air Quality (CMAQ) Tulare County Association of Governments (TCAG) Funds

The CMAQ funds are allocated through the TCAG. The CMAQ program funds transportation projects or programs that will contribute to improved air quality standards. Projects include: transportation activities, transportation control measures, public-private partnerships, alternative fuel programs, traffic flow improvements, transit, bicycle/pedestrian projects, rideshare activities, telecommuting, planning, experimental pilot projects, intermodal freight, and public outreach.

DOT: TIGER

TIGER is a multimodal, merit-based discretionary grant program that funds surface transportation capital projects, including transit and rail. Open to state, tribal, local agencies, and subdivisions.

California Department Block Grant (CDBG)

The CDBG Economic Development grant provides assistance to local businesses and low-income microenterprise owners to create or preserve jobs for low-income workers in rural communities. Funding includes planning and evaluation studies related to any activity eligible for these allocations, business lending, and public infrastructure.

Choice Neighborhoods

Choice Neighborhoods Planning Grants support the development of comprehensive neighborhood revitalization plans, which focused on directing resources to address three core goals: Housing, People and Neighborhoods. To achieve these core goals, communities must develop and implement a comprehensive neighborhood revitalization strategy, or Transformation Plan. The Transformation Plan will become the guiding document for the revitalization of the public and/or assisted housing units while simultaneously directing the transformation of the surrounding neighborhood and positive outcomes for families. Choice Neighborhoods Implementation Grants support those communities that have undergone a comprehensive local planning process and are ready to implement their “Transformation Plan” to redevelop the neighborhood.

California Department of Water Resources Prop 50 (Contaminant Removal)

Funds are available to disadvantage communities for developing UV or Ozone systems to disinfect drinking water or to set up pilot/demonstration sites.

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Drought Response Funding California State Water Resources Control Board (SWRCB)

The Governor and Legislature have directed Department of Water Resource to expedite the solicitation and award of \$200 million (of the \$472.5 million) in IRWM funding to support projects and programs that provide immediate regional drought preparedness, increase local water supply reliability and the delivery of safe drinking water, assist water suppliers and regions to implement conservation programs and measures that are not locally cost-effective, and/or reduce water quality conflicts or ecosystem conflicts created by the drought.

DWR: Water-Energy Grant Program

The 2014 Water-Energy grant supports the implementation of residential, commercial, and institutional water efficiency programs or projects that reduce Green House Gas emissions and also reduce water and energy use. Funding will go toward urban water management, groundwater management, and surface water diversion.

CDPH Clean Water SRF

The Safe Drinking Water State Revolving Fund (SDWSRF) provides funding to correct public water system deficiencies based upon a prioritized funding approach that addresses the systems' problems that pose public health risks, systems with needs for funding to comply with requirements of the Safe Drinking Water Act, and systems most in need on a per household affordability basis.

iBank (Infrastructure State Revolving Fund Program and Economic Development Bank)

iBank provides low cost, long term financing for local governments to fund a variety of public infrastructure projects. (Although this is not a grant, loan rates are largely determined by level of distress within a disadvantaged community).

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Attachment A- 6 Complete Streets

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**Tulare County Complete Streets
Cutler/Orosi
Final**

Prepared by:

**Tulare County Resource
Management Agency**



Tulare County Complete Streets – Cutler/Orosi

Prepared for:

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December, 2016



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Appendix A – Proposed Complete Streets Projects

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Appendix C – Bicycle, Bus and Pedestrian Plan

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Appendix F – Cutler 30% Submittal Plan Set

Appendix G – Cost Estimates for Orosi

Appendix H – Orosi 30% Submittal Plan Set

Appendix I - Complete Streets Outreach



Introduction

Complete Streets Vision

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.”

The Tulare County Resource Management Agency (RMA) is committed to fully integrating modal options in its General Plan and various Community Plans within Tulare County. This includes supporting projects that enhance walking and bicycling infrastructure. Additionally, RMA will improve access to public transportation facilities and services. This includes supporting urban development patterns and Americans with Disabilities Act (ADA) infrastructure that allow for greater accessibility to transit stops and stations. Finally, RMA continues to improve safety for all users and encourages street connectivity to create a comprehensive, integrated and connected circulation network. This is particularly important for those who rely on transportation infrastructure to be physically active and for students who walk or bike to school.

Steady population growth in Tulare County has directly impacted transportation needs. In the past, many of Tulare County’s federal, state, and local funding sources were used to develop new or improved traffic signals, interchanges, provide more travel lanes and to maintain existing roadway facilities. Historically, these funding sources have run well short of what is needed. The typical roadway transportation project that adds capacity and infrastructure is insufficient given these conditions. The RMA must adhere to its vision, which is to “provide a safe, convenient and effective County transportation system that enhances mobility and air quality for residents and visitors.”

Recent RMA and RMA-supported projects have already fulfilled some of these desires. There are already expanded bus transit routes in the County and more are being constructed for implementation in the near future. New transit centers are being placed throughout the County and efforts are underway to add more bicycle lanes and routes. Recent planning studies are looking to improve roadway safety, pedestrian safety, and access management between roadways and building developments. These efforts are consistent with green house gas (GHG) emissions reductions efforts to reduce vehicle miles travelled (VMT) set forth under SB 375.

Promoting Complete Streets projects can offer Tulare County the ability to reduce traffic congestion, improve air quality, and increase the quality of life of residents by providing safe, convenient, and comfortable routes for walking, bicycling, and public transportation. Integration of Complete Streets into Tulare County’s existing policies allows the potential to prevent chronic diseases, reduce motor vehicle related injury and deaths, improve environmental health, stimulate economic development, and ensure access of transportation options for all people in Tulare County.

Complete Streets Definition

Complete Streets are roadways designed to safely and comfortably accommodate all users, regardless of age, ability or mode of transportation. Users include motorists, cyclists, pedestrians and all vehicle types, including public transportation, emergency responders, and freight and delivery trucks among others. In addition to providing safety and access for all users, Complete Street design treatments take into account accommodations for disabled persons as required by the ADA. Design considerations for connectivity and access management are also taken into account for non-motorized users of the facility.

Implementation of Complete Street design treatments will be based on whether it connects the networks for all modes, whether it improves the functionality for all users, and whether it is appropriate given the surrounding context of the community. The final elements of a Complete Street roadway will be largely based on these factors. At a minimum, a Complete Street roadway includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

Complete Streets Attributes

While every street cannot be designed to serve all users equally, there are opportunities to enhance service for all users while maintaining its principal transportation function. Complete Streets incorporate community values and support adjacent land uses while ensuring safety and mobility. Proper applications of Complete Streets concepts support sustainable growth and preservation of scenic, aesthetic and historic resources.

Report Outcomes

As a part of the Circulation Element for the Community Plan Update, this Complete Streets Report (Implementation Work Plan) and the following Implementation and Policies Section achieved the following outcomes:

- (i) *Addressed* congestion, climate change and oil dependence by shifting to lower-carbon modes;
- (ii) *Improved* safety by addressing shoulders, sidewalks, better bus placement, traffic speed reduction, treatments for travelers with disabilities;
- (iii) *Created* “livable communities” by encouraging walking and bicycling for health, and by providing a safe walking and bicycling environment as an essential part of improving transportation movement and safety within the roadways studied.

These outcomes are achieved by the following:

- (a) *Included all users* namely, pedestrians, bicyclists, transit vehicles and users, and motorists. In drafting this report, all users were invited to comment on how the County could better serve the community. The implementation of complete streets directly shifts the emphasis to lower-carbon (using) modes of transportation. The shift from the gas using automobile to pedestrian and bicycle transport is achieved through the creation of sidewalks, improving sidewalks, and including bike lanes and/or bike routes for a wider ranger of people to use. The shift to transit is included in improving policies, programs and facilities in the operations of County’s transit systems.
- (b) *Created a comprehensive integrated and connected network* that supports “livable communities” that promote a safe interwoven fabric are provide for by the Policies Section using the transportation goals in the 2030 General Plan Circulation Element and by further defining complete streets network (see Appendix C).
- (c) *Emphasized flexibility* recognizing that all streets with these communities are different, and thus, balancing user needs. No one standard was applied to all streets and the street designs were adjusted to existing conditions, differing jurisdictions and the desires of the community.

- (d) *Considered both new and retrofit projects*, including design, planning, maintenance, and operation, for the entire right-of-way within these communities. In addition to the various sections discussed below Appendix A – D include plans that show the plans, designs, and existing and proposed maintenance plans and operations of the Complete Streets Plan.
- (e) *Used the latest and best design standards*. By using newer design standards as represented in the preliminary design plans versus the County’s Roadway Standards the County is able to provide wider sidewalks and include such amenities as traffic calming measures (bulbouts).
- (f) *Conducted extensive public outreach* to ascertain the solutions that best fit within the context of these communities. This culminated in two meetings, wherein the Community provided final feedback on the preliminary designs.

Conclusions and Future Funding Opportunities

The intended effect of identifying the outcomes and reaching the conclusions in this report is that future funding opportunities will be enhanced because the Community will be supported by fully updated Community Plans. The conclusion to the report includes the Circulation Element of the Community Plan including the policies, and plans. The other conclusion to the report includes preliminary design drawings.

Specifically, the funding sources that are found in the Funding Section will be pursued actively by Tulare County to complete the work identified in the studies include, but are not limited to, the following:

- **California Safe Routes to Schools Funds**
- **Federal Safe Routes to Schools Funds**
- **Highway Safety Improvement Funds**
- **Federal Transportation Activity Program (TAP) Funds**
- **Federal Transit Funds**
- **Federal Communities Putting Prevention to Work Grant**
- **Federal Highway Administration Pedestrian Safety and Design**
- **Strategic Growth Council**
- **Walkable and Livable Communities Institute**
- **California’s Local Public Health and Built Environment Program**
- **State Cap and Trade Funding**

Implementation

Selection of Communities

An effort is under way in Tulare County to implement Complete Streets Policies in the unincorporated communities within Tulare County's boundary. Just as the County updated its General Plan in 2012, many of the Community Plans are going through the update process. As a result of the Community Plan update process, several public meeting have been held in order to garner input from the local residents and business owners. Balancing the needs of what the people want while following local, state and federal policies and laws with a limited amount of available funding is the principal challenge in each community.

Transportation and related infrastructure costs tend to be exceedingly high may take years to implement. For purposes of this Study, four transportation corridors were selected within the community (see Appendix A), and two roadway segments in the community were selected to be evaluated for implementation of Complete Street standards. These roadway segments generally represent the highest volume roadways with a blend of residential and mixed land uses that also provide for regional access. Local streets and freeways were not selected, however tying into these facilities is considered.

General themes that were voiced from residents in each community related to transportation included the need for:

- Sidewalks
- Better road conditions
- Safe walking and biking areas
- Street lights
- Pedestrian crossings
- Safe (lower) vehicle speeds
- Improved drainage
- Increased transit stops
- Improved connectivity (railroad crossings)

Given the information provided by the residents and business owners, conceptual layouts and designs based upon the citizens concerns were presented to collect input. Based upon the community planning process, the following sections identify proposed projects for each community.

Avenue 416 – SR 63 to Road 140 (Orosi)

As selected as the highest priority in Orosi, Avenue 416, between SR 63 and Road 140, the project proposes to install new curb, gutter, sidewalk, at designated intersections, pedestrian ramps, relocate utilities and drainage. In the spirit of Safe Routes to School, land uses along this corridor include residential, highway commercial (mini-marts, service stations), the Elementary School and other Public and Quasi Public Land Uses, with the school and children as the main focus of the safety improvements. This project will include two travel lanes, a two-way turn lane, cross walks, parallel parking, street lights, improved/new bus stops, street signage and sidewalks with curb and gutter for drainage.

George Road/2nd Drive - Avenue 407 to SR 63 (Cutler)

The George Road/2nd Drive extends between Avenue 407 and SR63 near Cutler School. This project proposes to install new curb, gutter, sidewalk, pedestrian ramps and drainage facilities along portions of

the north and south sides of George Road/2nd Drive. Land uses affected by this project include commercial, residential and quasi-public (school).

Project Phasing

Tulare County RMA is proposing two types of projects coming from the community based upon the complexity of the project. The first types of projects could be built with limited improvement. They would be considered Phase 1 Projects and would have only minor needs for storm drain facilities, fence relocations, utility conflicts, etc. Phase 2 Projects are more inclusive and would be classified as medium to long range projects. These projects would need other infrastructure improvements such as storm water basins, major storm drain improvements, utilities to be undergrounded, Caltrans encroachment permits etc.

Phase 1 Projects	Phase 2 Projects
<ul style="list-style-type: none">• Curb, gutter & sidewalk (storm drain water into existing system); pedestrian ramps; bulb outs (where appropriate)• Street lights• Bus shelters, benches, trash receptacles, etc.• Fence relocations• Street signage and striping• Minor utility conflicts• Minor land acquisition	<ul style="list-style-type: none">• Curb, gutter & sidewalk (new drainage system)• Major storm drain facilities (new pipelines and storm water basins)• Utility relocations (undergrounding)• Major land acquisition• Railroad crossing improvements• Caltrans Bridge Improvements

Complete Street Policies

Complete Street Goals

The purpose of the RMA Complete Streets Policy is to create a comprehensive and uniform Complete Streets vision and policy for Tulare County. This will allow the implementing entities to incorporate Complete Streets guidelines and standards into both development and redevelopment actions. The County's goals are:

- Tulare County's transportation network will be supported through a variety of feasible transportation choices, which allows for sustainable growth.
- The livability of neighborhoods and commercial centers located along the County's transportation corridors will be enhanced by a safe and inviting pedestrian environment.
- The design of multimodal roadway facilities will not compromise the needs of larger vehicles such as transit vehicles, fire trucks and freight delivery trucks.
- Inclusion of Complete Streets design elements will allow for design flexibility on different street functions and neighborhood contexts.
- Inclusion of Complete Streets design elements will improve the integration of land use and transportation, while encouraging economic revitalization through infrastructure improvements.

Complete Streets Objectives

- To create an integrated and connected transportation network that supports transportation choices and sustainable growth.
- To ensure that all transportation modes are accommodated to the extent possible in all public roadway facilities in the County.
- To develop and use the latest design standards and guidelines in the design of Complete Streets.
- To provide flexibility in the implementation of this policy so that streets chosen for implementation of Complete Streets elements can be developed to fit within the context of their principal purpose and surroundings without compromising the safety of users and needs of larger vehicles.

Complete Streets Policies

Tulare County General Plan Policies

The Tulare County General Plan Update (2030) in complying with AB 1358 calls for 4 Complete Streets related principles including:

Principle 1: County-wide Collaboration

Support countywide transportation plans that provide choices in travel modes.

Principle 2: Connectivity

Emphasize connectivity among cities, communities, and hamlets to ensure County residents have access to jobs and services.

Principle 3: Community Circulation

Anticipate and provide transit, traffic, and roadway connections that support the interconnectivity of all communities.

Principle 4: Pedestrian and Bicycle Facilities

Plan for the development and expansion of pedestrian paths and bicycle facilities that provide residents, with alternative modes of travel.

These principles are expressed mainly in following policies including:

- TC-1.6 Intermodal Connectivity
- TC-1.7 Intermodal Freight Villages
- TC-5.1 Bicycle/Pedestrian Trail System
- TC-5.2 Non-motorized Modes in Planning and Development

Complete Street Policy Design Criteria

1. Tulare County promotes the incorporation of Complete Streets concepts and design standards in all appropriate new and retrofit County public streets (except State highways and freeways).
2. Tulare County will seek every opportunity to provide funding for the planning, design, and implementation of Complete Streets.
3. New Class I Multi-use Paths should be a minimum of eight (8) feet wide.
4. New Class II Bike Lanes should be a minimum of five (5) feet wide.
5. New sidewalks should be a minimum of five (5) feet wide.
6. Bulb-outs should be considered in areas of higher speed (35 mph or greater) where sufficient turning radii for trucks is available or as determined by the County Engineer.
7. As determined by the County Engineer, installation of posted speed limit vehicle activated traffic calming signs (VATCS) are encouraged in instances of high speed to promote safety.
8. Transit shelters and benches are encouraged at all County transit stops if FTA grants are available.
9. Street lighting and cross walk are encouraged to promote safety if considered feasible by the County Engineer.
10. Design policies should be consistent with the Tulare County Improvement Standards; other references include existing design guides, such as those issued by Caltrans, AASHTO and the ADA Accessibility Guidelines.

11. Public streets excluded from this policy include those where:
 - Complete streets concepts are in conflict with existing laws, codes, or ordinances.
 - Compliance with this policy would conflict with goals or physical conditions related to the unique aspects of the location.
12. Exceptions from Complete Street Policies:
 - Accommodation is not necessary where non-motorized use is prohibited, such as freeways.
 - Cost of accommodation is excessively disproportionate to the need or probable use as determined by the County Engineer.
 - A documented absence of current or future need.

Complete Street Mobility Plan

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” Essentially, this bill requires a circulation element to plan for multimodal transportation accommodating all modes of transportation where appropriate, including walking, biking, car travel, and transit. The current functional classification system plan is shown in Appendix B (Circulation Plan).

The Complete Streets Act also requires circulation elements to consider the multiple users of the transportation system, including children, adults, seniors, and the disabled. For further clarity, AB 1358 tasks the Governor’s Office of Planning and Research to release guidelines for compliance with this legislation by January 1, 2014. Implementation of complete streets principles should be tailored to the individual jurisdiction and the individual roadway. The Complete Streets Program for Tulare County focuses on a network-based approach that has been tailored to the needs of the Community of Cutler/Orosi. Another principle that is being applied is under SB 743, requiring a change to evaluating traffic using Vehicle Miles Traveled verses Level of Service under CEQA analysis, and under AB 32 in reducing Green House Gasses.

Complete Streets: According to the National Complete Streets Coalition, complete streets are a means by which, “... planners and engineers (can) build road networks that are safer, more livable, and welcoming to everyone.... Instituting a complete streets policy ensures that transportation planners and engineers consistently design and operate the entire roadway with all users in mind – including bicyclists, public transportation vehicles and riders, and pedestrians of all ages and abilities.”

Network-Based Complete Streets: Combines individual travel mode networks into one multimodal transportation system, integrating infrastructure where appropriate, ultimately ensuring that all users can safely and efficiently access their destination.

Vehicle Miles Traveled (VMT): Vehicle miles traveled is the metric that identifies the total distance traveled in a car per driver. VMT drives roadway needs (the more people who drive, the more capacity and maintenance are needed on the roadway system). Under the Tulare County Climate Action Plan, in reducing VMT green house gas emissions are reduced and the County has an overall target of reducing 6% of its green house gas emissions through a reduction in VMT.

Community Plans adopt these principles, which are combined into the following mission statement:

The Community Complete Streets Network comprises four types of facilities—vehicular, pedestrian, bicycle, and public transit. This complete streets approach will enable residents to choose which travel mode best suits them. It also will ensure that streets are designed with the users in mind—accommodating for businesses, children, the elderly, bicyclists, and transit users.

Caltrans and Complete Streets

Under Caltrans District Order 64-R1, Caltrans requires that a Complete Streets Implementation Action Plan be developed and implemented for Caltrans owned and maintained Streets. Their Implementation Action plan provides a background by which the Tulare County Completes Street Plan will be implemented.

TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS)

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Cutler/Orosi. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describe bicycle circulation patterns and Pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

Tulare County Climate Action Plan (CAP)

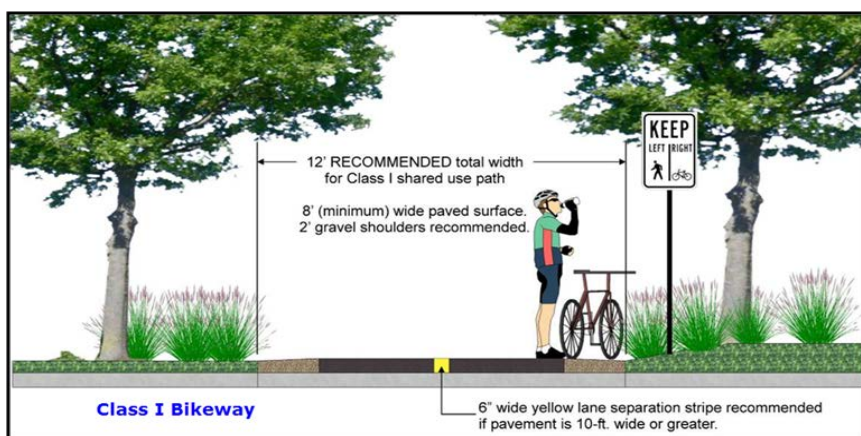
The Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% through a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets.

Bicycle Facilities

Bicycle facilities consist of Class I, Class II, and Class III facilities as defined below. In Tulare County, this General Plan and the Bicycle Transportation Plan envision a system of bicycle lanes on roadways that will connect the activity centers of the communities to the residents. County has identified pedestrian corridors on the Community of Cutler/Orosi Bicycle, Bus and Pedestrian Plan (see Appendix C).

Class I

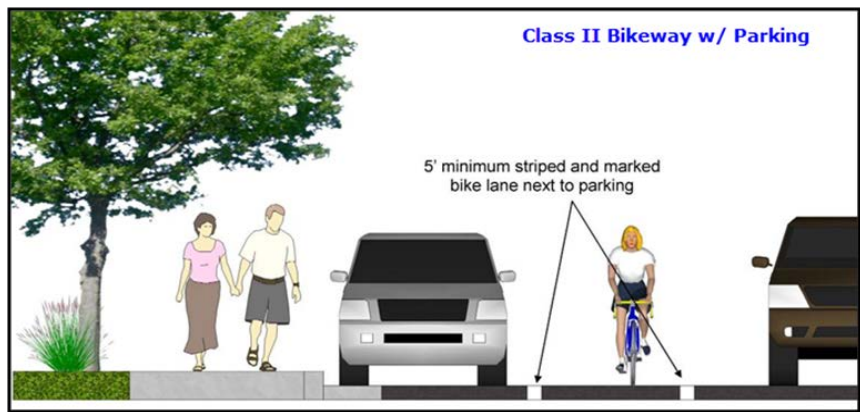
Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County, Class I facilities will primarily be implemented through TCAG. Future bicycle facilities have also been identified through the *Bicycle Transportation Plan* (TCAG - 2010). There is a proposed Class I on Ave. 416.



bicycle facilities in Cutler/Orosi.

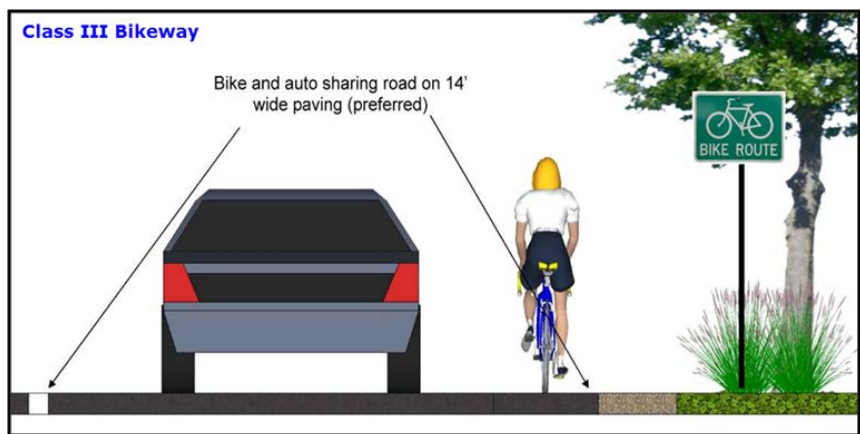
Class II

Bikeway that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. For the purposes of Complete Streets, the County is not proposing Class II bicycle facilities.



Class III

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III facilities are not proposed.



Although not signed on many local roads in Cutler/Orosi, bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways.

Pedestrian Facilities

Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

Multiuse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. No multiuse trails exist or are proposed in Cutler/Orosi.

Transit Facilities

Transit options give users the ability to get to a destination without relying on the automobile. This also provides other community benefits, including reduced vehicle miles traveled (VMT). Reducing VMT will help the County achieve their greenhouse gas reduction target,



Pass Sales Outlets

Dinuba Transit Center	189 Merced St., Dinuba
Porterville Transit Center	35 W. Oak St., Porterville
Tulare County Government Plaza	5961 S. Mooney Blvd., Visalia
Tulare Transit Center	360 N. 'K' St., Tulare
Visalia Transit Center	Oak & Santa Fe, Visalia

Tulare County Area Transit (TCAT)

Public transportation services and facilities in Tulare County consist of public bus service, paratransit service, and could also consider park-and-ride locations.

Public Bus Service

Public bus service is provided by Tulare County Area Transit (TCAT) in rural areas such as Cutler/Orosi and by local City transit agencies in transitioning areas, which enables commuters to travel within the communities and adjacent cities with minimal transfers. Existing transit routes and designated bus stops are shown in the following figures.

Transit service is provided in Cutler/Orosi through the Tulare County Area Transit (TCAT). Additionally, Tulare County has provided guidance for including transit within facilities. These guidelines should be applied when considering new development to ensure appropriate connectivity and design features to support bus service.

Paratransit Service

Paratransit is an alternative mode of passenger transportation that does not follow fixed routes or schedules. Typically, vans or minibuses are used to provide paratransit service. Paratransit services vary considerably on the degree of flexibility they provide their customers. The most flexible systems offer on-demand, call-up, door to door service from any origin to any destination in a service area.

Park-and-Ride Lots

Park-and-ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the community's population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community along State Route 65.

Cost Benefits Analysis, Implementation, and Funding Mechanisms

Caltrans lists the following benefits of Complete Streets in their implementation plan. They include:

- Increased Transportation Choices: Streets that provide travel choices can give people the option to avoid traffic congestion, and increase the overall capacity of the transportation network.
- Economic Revitalization: Complete streets can reduce transportation costs and travel time while increasing property values and job growth in communities.
- Improved Return on Infrastructure Investments: Integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project spares the expense of retrofits later.

- Quality of Place: Increased bicycling and walking are indicative of vibrant and livable communities.
- Improved Safety: Design and accommodation for bicyclists and pedestrians reduces the incidence of crashes.
- More Walking and Bicycling: Public health experts are encouraging walking and bicycling as a response to the obesity epidemic. Streets that provide room for bicycling and walking help children get physical activity and gain independence.

Benefits of Complete Streets

The health benefits from walking and bicycle riding include increased overall health, and a reduction in air quality and green house emissions. According to the Caltrans accepted, Victoria Transport Policy Institute, walking has a \$.25 per mile health benefit, while the cost of Greenhouse Gas (GHG) reductions is \$23 per ton. According to the Federal Highway Administration, sidewalks reduce incidences to pedestrians over 80%.¹ According to Caltrans, the average costs of highway incidents are stated below.

Cost of Highway Accident	Dollars Per Accident
Fatal Accident	\$4,800,000
Injury Accident	\$67,400
Property Damage Only (PDO) Accident	\$10,200
Average Cost per Accident	\$52,500
Cost of an Event	Dollars Per Event
Cost of a Fatality	\$4,400,000
Cost of an Injury	
Level A (Severe)	\$221,400
Level B (Moderate)	\$56,500
Level C (Minor)	\$26,900
Cost of Property Damage	\$2,500
<i>Source: California Department of Transportation</i>	

The collision history within the Cutler/Orosi area can be traced using the Transportation Injury Management System hosted by UC Berkeley SafeTrec. When reviewing the 5 year (2009-2013) injury collision history, a total of 96 collisions have occurred involving a pedestrian or bicyclist and a motor vehicle. (See Figure below).

¹ http://www.dot.ca.gov/hq/tpp/offices/eab/benefit_cost/LCBCA-economic_parameters.html



Source: Transportation Injury Management System, UC Berkeley 2009-2013

Since there are currently no sidewalks many pedestrians walk in the street. Vehicles cannot distinguish roadway with walking paths when curbs do not exist. Any amount of collisions involving pedestrians or bicyclists should be of great concern, especially adjacent to a local school.

Factors such as speed, visibility and general pedestrian/bicyclist awareness are identified as contributing factors relating to the incidents. As a result, the project was developed with these specific factors in mind. The proposed design improvements have been proven to be effective and have also been proven to reduce speeds, increase visibility and increase overall awareness of pedestrian activity.

Community Specific Complete Street Implementation Measures

As part of a network-based approach, the County has identified (and will implement through pursuing further roadway studies and infrastructure design updates) a complete network for pedestrians. The County will also work to deliver infrastructure to support all modes of transportation. In addition to the General Plan Circulation Element Implementation Section, the key implementation measures include:

1. Evaluating Roadways as potential Bike/Pedestrian travel routes,
2. Completing pedestrian infrastructure, as appropriate,
3. Providing safe and accessible pedestrian facilities in high use areas,
4. Designing and building sidewalks for safer routes to school,
5. Designating roadways for bicycle routes that are aligned with the Tulare County comprehensive bicycle network,
6. Coordination with County Transit, and,
7. Submitting the following list of project and cost to TCAG and Caltrans for consideration under further grant funding opportunities.

Measure R

Bike/Transit/Environmental Projects (14% of Measure R Funding)

On November 7, 2006, the voters of Tulare County approved Measure R, imposing a ½ cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

Active Transportation Program (ATP)

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation.

Citizen Feedback

Public Outreach Efforts

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues.

Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Agency Complete Streets and Community Plan Outreach (2015) – is located in Appendix I.

Cutler/Orosi Public Meetings

- Complete Streets Meeting February 2, 2015
- Complete Streets Meeting March 3, 2015



Community feedback was gathered at the February and March meetings and incorporated into the design of the Complete Street Plans and further discussed in the February and March, 2015 meetings to receive further community feedback. These designs were edited to include feasible improvements and cost estimates were assigned to each project within the respective community for each study roadway segment.

Design Facilities

Improvement Standards

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Transportation related facilities for public use are built within existing right of way (R/W) owned by a public agency, e.g., county, city or state. Within this R/W is a standard cross section, which is a term that is used to define the configuration of existing or proposed roadways at right angles to the centerline (CL). Typical sections show the width, thickness and descriptions of the pavement section, as well as the geometrics of the graded roadbed, side improvements and side slopes.

In Tulare County, the two most common cross sections are shown for two or four lane roads, varying in width based upon the number of lanes, parking, sidewalks, shoulders, bike lanes, etc. Figure 1 shows the cross section for two lane roads and Figure 2 identifies a typical four lane cross section.

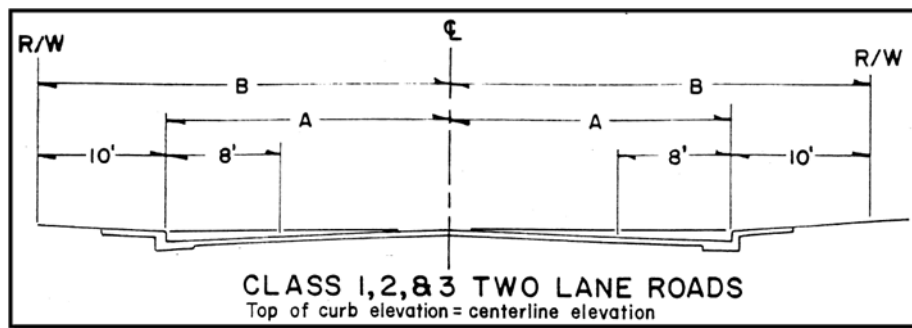


Figure 1 - Tulare County Class 1, 2 & 3 Two Lane Roads

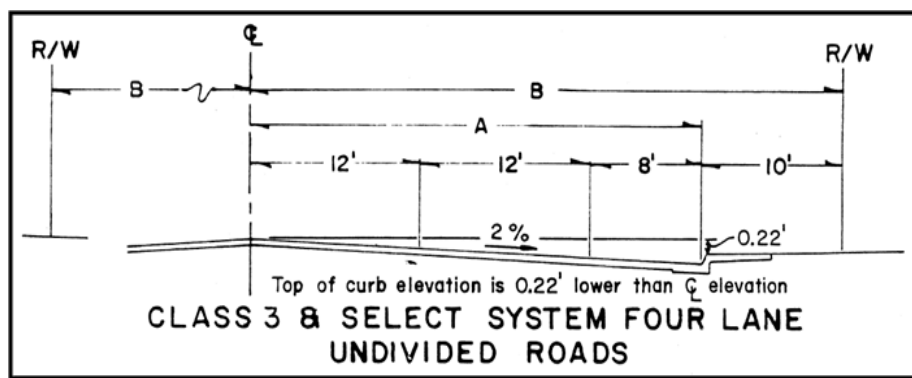


Figure 2 - Tulare County Class 3 Four Lane Road

Tulare County Pavement Management System

Pavement Management

Pavement management is the process of planning the maintenance and repair of a network of roadways or other paved facilities in order to optimize pavement conditions over the entire network. Pavement management incorporates life cycle costs into a more systematic approach to minor and major road maintenance and reconstruction projects. The needs of the entire network as well as budget projections are considered before projects are executed. Pavement management encompasses the many aspects and tasks needed to maintain a quality pavement inventory, and ensure that the overall condition of the road network can be sustained at desired levels.

Pavement Management System

The Tulare County Pavement Management System (PMS) is a planning tool used to aid pavement management decisions. PMS software programs model future pavement deterioration due to traffic and weather, and recommend maintenance and repairs to the road's pavement based on the type and age of the pavement and various measures of existing pavement quality. Measurements can be made by persons on the ground, visually from a moving vehicle, or using automated sensors mounted to a vehicle. PMS software assists RMA staff to create composite pavement quality rankings based on pavement quality measures on roads or road sections. Recommendations are usually biased towards preventive maintenance, rather than allowing a road to deteriorate until it needs more extensive reconstruction.

Typical tasks performed by Tulare County PMS include:

- Inventory pavement conditions, identifying good, fair and poor pavements;
- Assign importance ratings for road segments, based on traffic volumes, road functional class, and community demand;
- Schedule maintenance of good roads to keep them in good condition; and,
- Schedule repairs of poor and fair pavements as remaining available funding allows.

Research has shown that it is far less expensive to keep a road in good condition than it is to repair it once it has deteriorated. This is why pavement management systems place the priority on preventive maintenance of roads in good condition, rather than reconstructing roads in poor condition. In terms of lifetime cost and long term pavement conditions, this will result in better system performance.

The County is proposing a Road Maintenance Plan (see Appendix D) for the community of Cutler/Orosi that is a result of the PMS.

Projects

Complete Streets Project Plans

The plans and projects in the appendices are identified as part of the complete streets policy to identify corridors for various user types and to demonstrate examples of design policies. These plans and are the result of input obtained through the community outreach process, multiple Tulare County agencies and divisions and professional engineering consultants.

The six projects on Orosi and five projects in Cutler have been identified herein that represent the priority improvements to the backbone of the complete streets network within Cutler/Orosi. One project in Cutler and one project in Orosi will be developed to a 30% design stage and the remaining four projects have been preliminarily scoped and budgetary estimates have been prepared. These projects were developed to provide the County and various funding agencies with a list of projects to move toward funding, design, and ultimately construction.

Orosi

1. Avenue 416 – SR 63 to Road 140 (East Orosi)
2. Avenue 413 – Road 124 to SR 63
3. Avenue 419
4. Avenue 416 – SR-63 to Dinuba
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

General Road Concerns

1. Flooding: Intersection of Orosi Drive and 2nd Drive.
2. Stop Sign: Intersection of Road 124 at Aceves Avenue.
3. Traffic Speeding Concerns: Road 124 (Avenue 408 to Avenue 416).
4. Request for additional Crosswalks: Road 127 (El Monte School, Students cross mid-block at multiple locations primarily north of the school).
5. Sight Distance Problem with parked vehicles: Road 127 at Avenue 416 (Northbound left turn from Road 127 to westbound Ave 416).
6. Traffic Signal at Orosi High School: SR 63 (Road 128)
7. Traffic Speeding: Avenue 416 (There has been an accident along this road).
8. Lights & Sidewalks: Avenue 63.
9. Sidewalks: Avenue 63 and Ella Avenue.

Complete Streets Funding Opportunities

The following sections identify opinions of probable cost estimates for Complete Street transportation related improvements in Cutler/Orosi. As shown in the tables, the funding sources include local, state and federal programs. Typically, local matches are required for acquiring state and federal funds. Measure R, a Tulare County sales tax for transportation, is available for such matches.

Cost Estimates

Detailed cost estimates are included in Appendix E and G.

Appendix A –

Proposed Complete Streets Projects

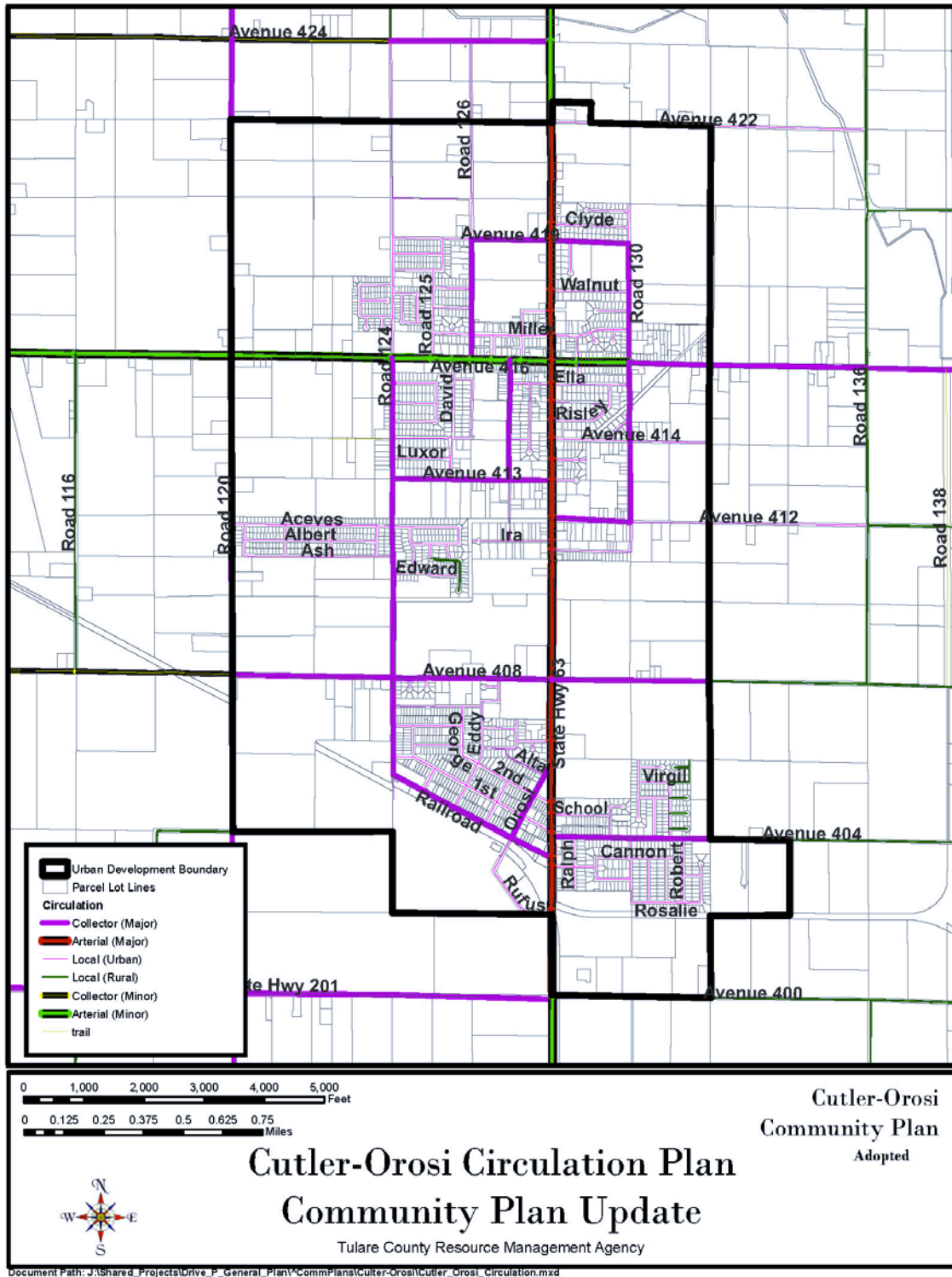
Orosi

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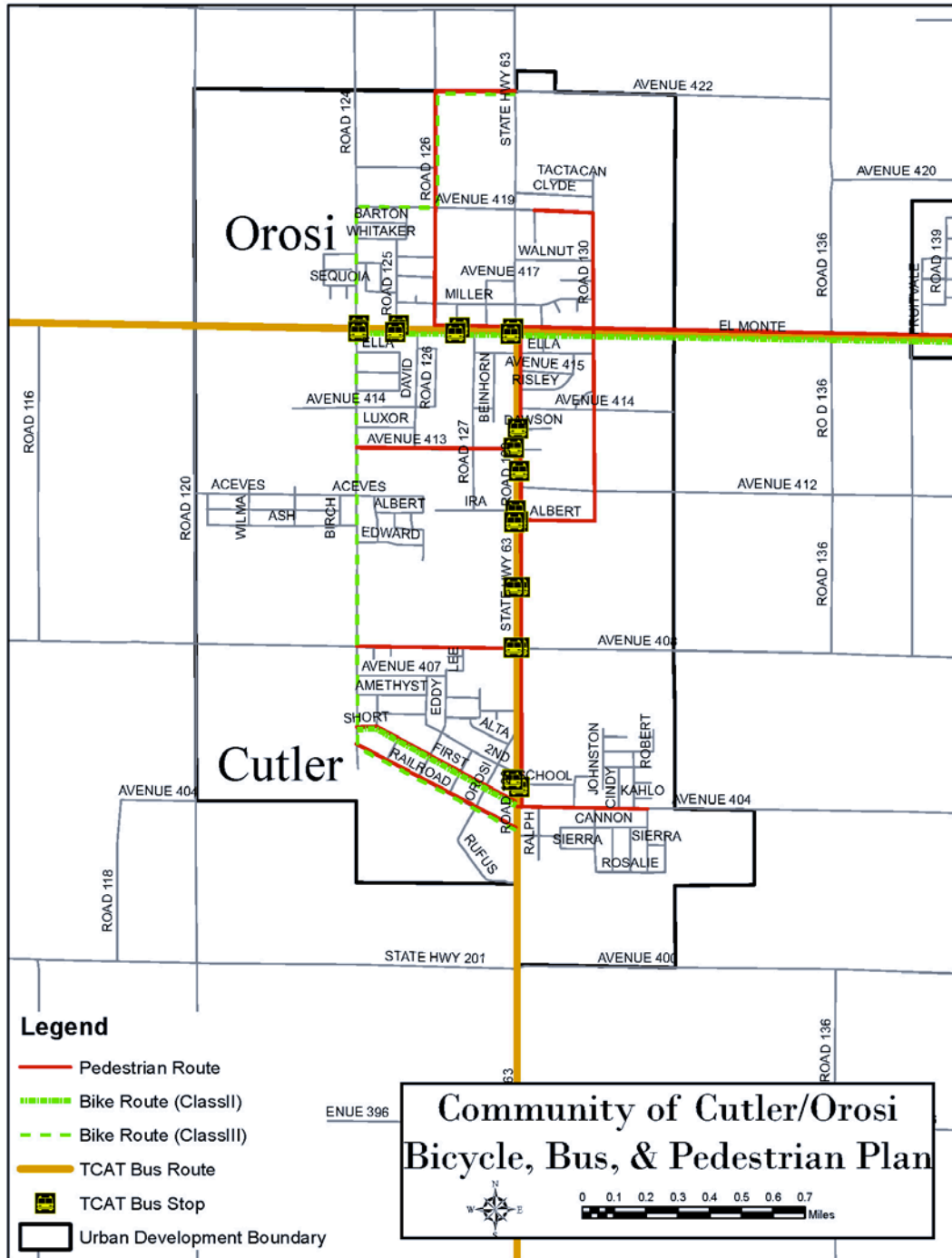
Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

Appendix B – Circulation Plan

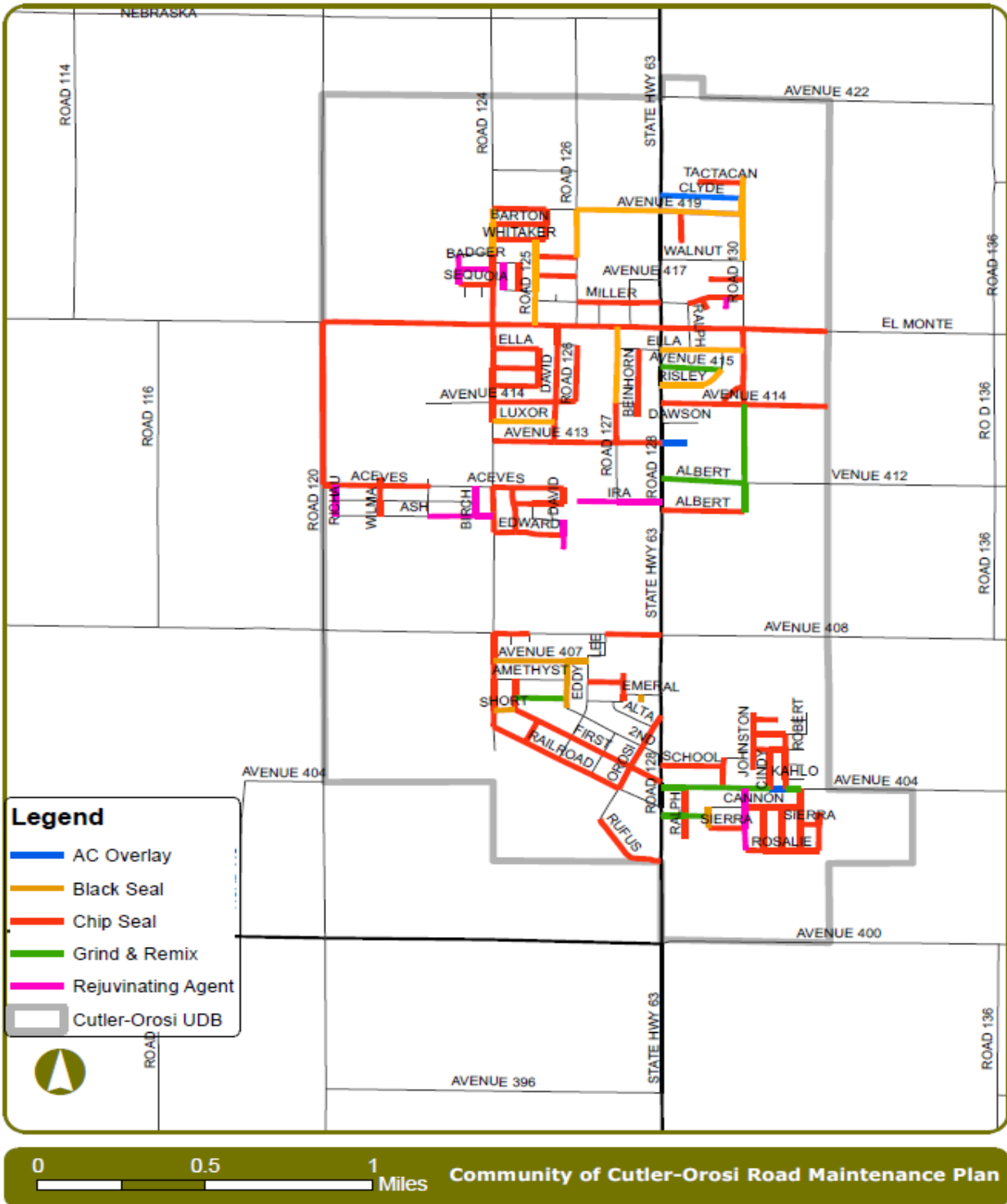


Appendix C – Bicycle, Bus, and Pedestrian Plan



Appendix D

Road Maintenance Plan



Appendix E

Cost Estimates for Cutler

PRELIMINARY COST ESTIMATE (30-Percent)					
Cutler Complete Street Improvements				Date:	10/21/2015
County of Tulare					
Construction					
ITEM		ITEM DESCRIPTION	UNIT OF	ESTIMATED	ITEM
NO	(F)		MEASURE	QUANTITY	PRICE
1	-	MOBILIZATION	LS	1	\$ 148 900.00
2	-	JOB SITE MANAGEMENT	LS	1	\$ 8 000.00
3	-	WATER POLLUTION CONTROL PROGRAM	LS	1	\$ 4 500.00
4	-	CONSTRUCTION AREA SIGNS	LS	1	\$ 7 500.00
5	-	TRAFFIC CONTROL SYSTEM	LS	1	\$ 30 000.00
6	-	RELOCATE FENCE (CHAIN LINK)	LF	772	\$ 50.00
7	-	RELOCATE FENCE (IRON)	LF	896	\$ 90.00
8	-	RELOCATE FENCE (BRICK AND IRON)	LF	99	\$ 150.00
9	-	RELOCATE POWER POLE	EA	1	\$ 40 000.00
10	-	RELOCATE DRAIN INLET	EA	2	\$ 1 500.00
11	-	RELOCATE SIGN	EA	4	\$ 300.00
12	-	RELOCATE FIRE HYDRANT	EA	1	\$ 6 000.00
13	-	ADJUST WATER VALVE COVER TO GRADE	EA	9	\$ 800.00
14	-	ADJUST WATER METER BOX TO GRADE	EA	9	\$ 3 500.00
15	-	ADJUST FIRE HYDRANT TO GRADE	EA	3	\$ 5 500.00
16	-	ADJUST MANHOLE TO GRADE	EA	10	\$ 1 200.00
17	-	ADJUST DRAIN INLET TO GRADE	EA	1	\$ 1 500.00
18	-	RESET MAILBOX	EA	18	\$ 300.00
19	-	REMOVE TREE	EA	1	\$ 800.00
20	-	CLEARING AND GRUBBING	LS	1	\$ 10 000.00
21	(F)	ROADWAY EXCAVATION	CY	3 989	\$ 60.00
22	(F)	CLASS 2 AGGREGATE BASE	CY	2 384	\$ 60.00
23	-	HOT MIX ASPHALT (TYPE A)	TON	1 724	\$ 110.00
24	-	48" MANHOLE	EA	11	\$ 6 000.00
25	-	MINOR CONCRETE (CURB AND GUTTER)	LF	4 280	\$ 25.00
26	-	MINOR CONCRETE (SIDEWALK)	SQFT	17 056	\$ 7.00
27	-	MINOR CONCRETE (DRIVEWAYS AND CURB RAMPS)	SQFT	6 258	\$ 15.00
28	-	DETECTABLE WARNING SURFACE	SQFT	245	\$ 45.00
29	-	SIGNING & STRIPING	LS	1	\$ 15 000.00
30	-	EROSION CONTROL	LS	1	\$ 5 000.00
31	-	MISCELLANEOUS ITEMS	LS	1	\$ 327 146.00
Total - Construction Items 1-31					\$1 784 631
Contingency (25%)					\$ 446 158
Recommended Total Construction Budget					\$2 230 789
Non-Construction Related Costs					
ITEM		ITEM DESCRIPTION	UNIT OF	ESTIMATED	ITEM
NO	(F)		MEASURE	QUANTITY	PRICE
32	-	Environmental Clearance	% of CON	5%	\$1 784 631.36
33	-	Right of Way Acquisition (Capital)	SQFT	1 081	\$ 5.00
34	-	Right of Way Acquisition (Support)	Parcel	5	\$ 5 000.00
35	-	Final Engineering Design	% of CON	15%	\$1 784 631.36
36	-	Construction Support	% of CON	2%	\$1 784 631.36
37	-	Construction Management	% of CON	15%	\$1 784 631.36
38	-	Utility Relocations	LS	1	\$ 75 000.00
Total - Non-Construction Items 33-38					\$765 717
Total Construction & Non-Construction Items					\$2 996 506

Preliminary Cost Estimate (30-Percent)

Avenue 408, Cutler

Tulare County Complete Streets

6/9/2016
55-7300-14/2144

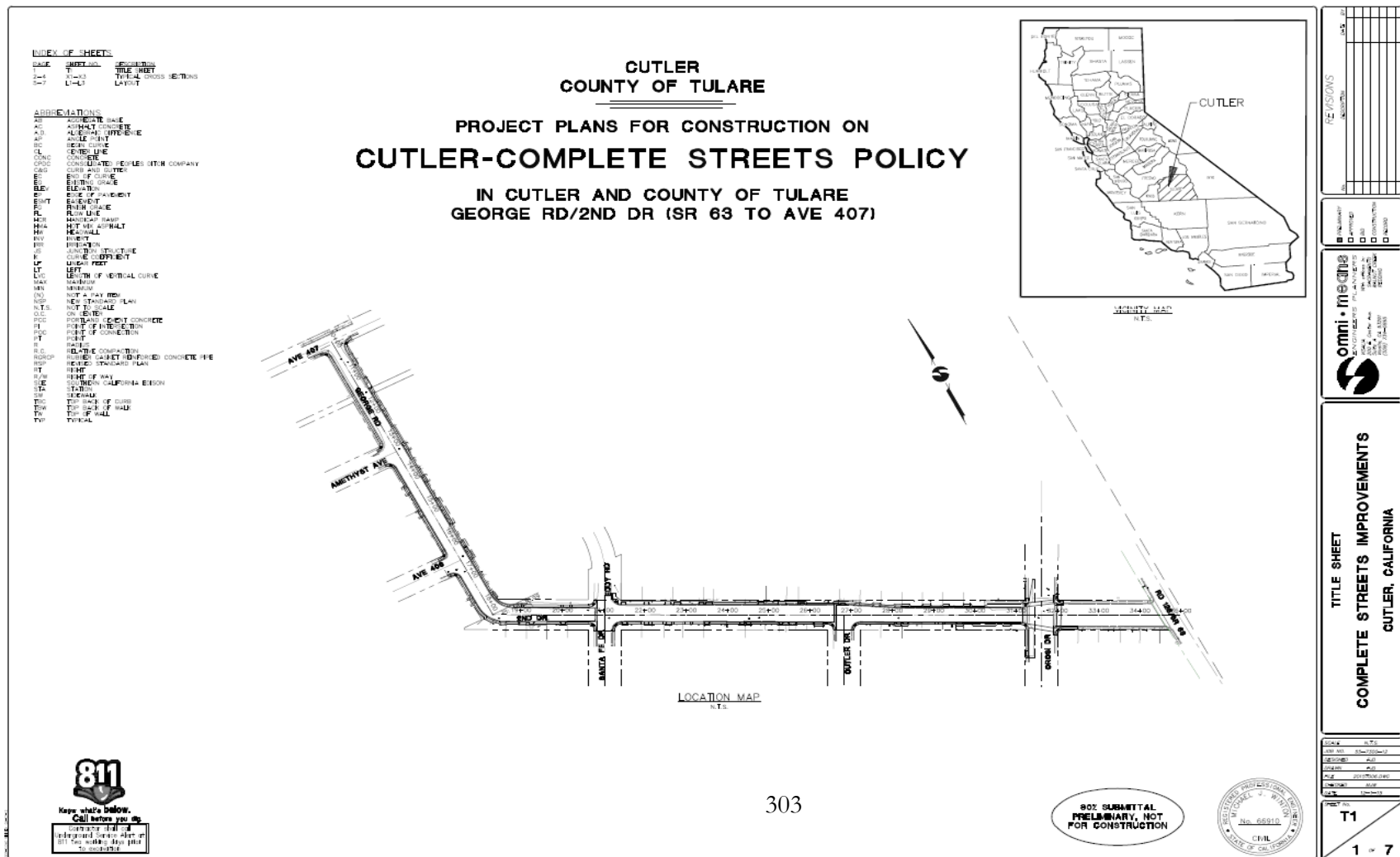
Construction Costs:

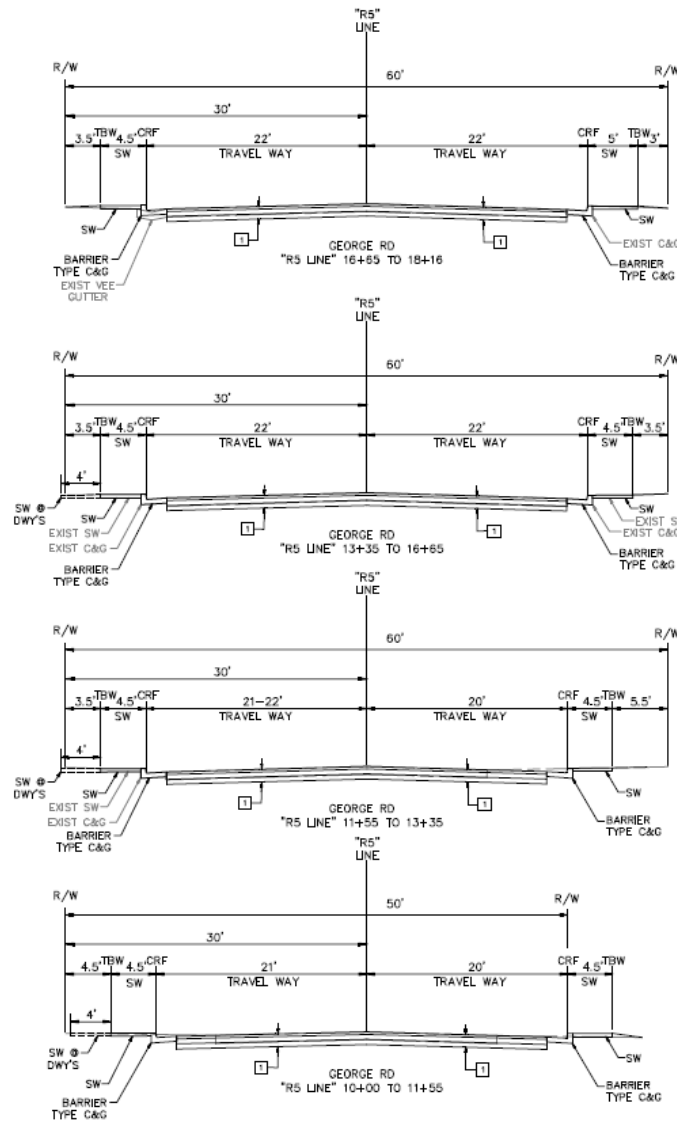
No.	Item Description	Units	Quantity	Unit Cost	Total
1	Job Site Management	LS	1	\$8,000.00	\$8,000.00
2	Water Pollution Control Program	LS	1	\$4,500.00	\$4,500.00
3	Construction Area Signs	LS	1	\$7,500.00	\$7,500.00
4	Traffic Control	LS	1	\$30,000.00	\$30,000.00
5	Remove Chain Link Fence	LF	31	\$12.00	\$372.00
6	Remove Wooden Fence	LF	12	\$20.00	\$240.00
7	Remove Traffic Stripe	LF	2992	\$1.00	\$2,992.00
8	Remove Pavement Marking	SQFT	456	\$2.70	\$1,231.20
9	Reconstruct Drain Inlet	EA	2	\$3,500.00	\$7,000.00
10	Remove Sign	EA	16	\$130.00	\$2,080.00
11	Remove Irrigation Standpipe	EA	1	\$1,500.00	\$1,500.00
12	Remove Metal Post	EA	3	\$75.00	\$225.00
13	Remove Concrete (Curb & Gutter)	LF	2307	\$15.00	\$34,605.00
14	Remove Concrete Sidewalk	SQFT	3787	\$4.50	\$17,041.50
15	Relocate Fence (Chain Link)	LF	33	\$80.00	\$2,640.00
16	Reset Mailbox	EA	1	\$300.00	\$300.00
17	Adjust Manhole to Grade	EA	6	\$1,200.00	\$7,200.00
18	Adjust Fire Hydrant to Grade	EA	4	\$5,500.00	\$22,000.00
19	Relocate Fire Hydrant	EA	2	\$6,000.00	\$12,000.00
20	Adjust Water Meter Box to Grade	EA	3	\$800.00	\$2,400.00
21	Adjust Water Valve Frame & Cover to Grade	EA	4	\$1,200.00	\$4,800.00
22	Clearing and Grubbing	LS	1	\$10,000.00	\$10,000.00
23	Remove Bollard	EA	3	\$100.00	\$300.00
24	Roadway Excavation (F)	CY	1150	\$60.00	\$69,000.00
25	Class 2 Aggregate Base (F)	CY	790	\$60.00	\$47,400.00
26	Hot Mix Asphalt (Type B)	TON	630	\$110.00	\$69,300.00
27	Roadside Sign - One Post	EA	15	\$250.00	\$3,750.00
28	Roadside Sign - Two Posts	EA	1	\$400.00	\$400.00
29	Paint Curb	SQFT	83	\$2.25	\$186.75
30	Storm Drain System	LS	1	\$75,000.00	\$75,000.00
31	Minor Concrete (Curb)	LF	156	\$22.00	\$3,432.00
32	Minor Concrete (Gutter)	SF	381	\$10.00	\$3,806.10
33	Detectable Warning Surface	SQFT	297	\$45.00	\$13,365.00
34	Minor Concrete (Curb and Gutter)	LF	5003	\$25.00	\$125,075.00
35	Minor Concrete (Sidewalk)	SQFT	34516	\$7.00	\$241,612.00
36	Minor Concrete (Driveway & Curb Ramps)	SQFT	2367	\$15.00	\$35,505.00
37	Thermoplastic Traffic Stripe	LF	3347	\$1.25	\$4,183.75
38	Thermoplastic Pavement Marking	SQFT	600	\$6.00	\$3,600.00
39	Erosion Control	LS	1	\$5,000.00	\$5,000.00
40	Mobilization	LS	1	\$82,500.00	\$82,500.00
41	Minor/ Supplemental Items	%	25%	\$879,542.30	\$219,885.58
	Subtotal (Construction Costs)				\$1,161,927.88
	Construction Contingency			25%	\$ 290,481.97
	Total Construction Costs				\$1,452,409.84
	Total Construction Budget (Rounded)				\$1,452,500.00

Non-Construction Related Costs:					
No.	Item Description	Units	Quantity	Unit Cost	Total
42	Environmental Clearance	% of CON	5%	\$1,162,000.00	\$58,100.00
43	Right of Way/TCE Acquisition (Capital)	SQFT	1630	\$5.00	\$8,150.00
44	Right of Way/TCE Acquisition (Support)	Parcel	2	\$5,000.00	\$10,000.00
45	Final Engineering Design	% of CON	15%	\$1,162,000.00	\$174,300.00
46	Construction Support	% of CON	2%	\$1,162,000.00	\$23,240.00
47	Construction Management	% of CON	15%	\$1,162,000.00	\$174,300.00
48	Utility Relocations	LS	1	\$100,000.00	\$100,000.00
	Total Non-Construction Related Costs				\$548,090.00
	Total Project Capital Cost				\$ 2,000,590.00
	Rounded				\$ 2,000,000.00

Appendix F

Cutler 30% Submittal Plan Set





TYPICAL STRUCTURAL SECTIONS:

1. 30-YEAR DESIGN
 T=5, R=20
 0.25' HMA (TYPE B)
 0.70' AB (CLASS 2)
 0.50' COMPACTED NATIVE

**80% SUBMITTAL
 PRELIMINARY, NOT
 FOR CONSTRUCTION**



REVISIONS

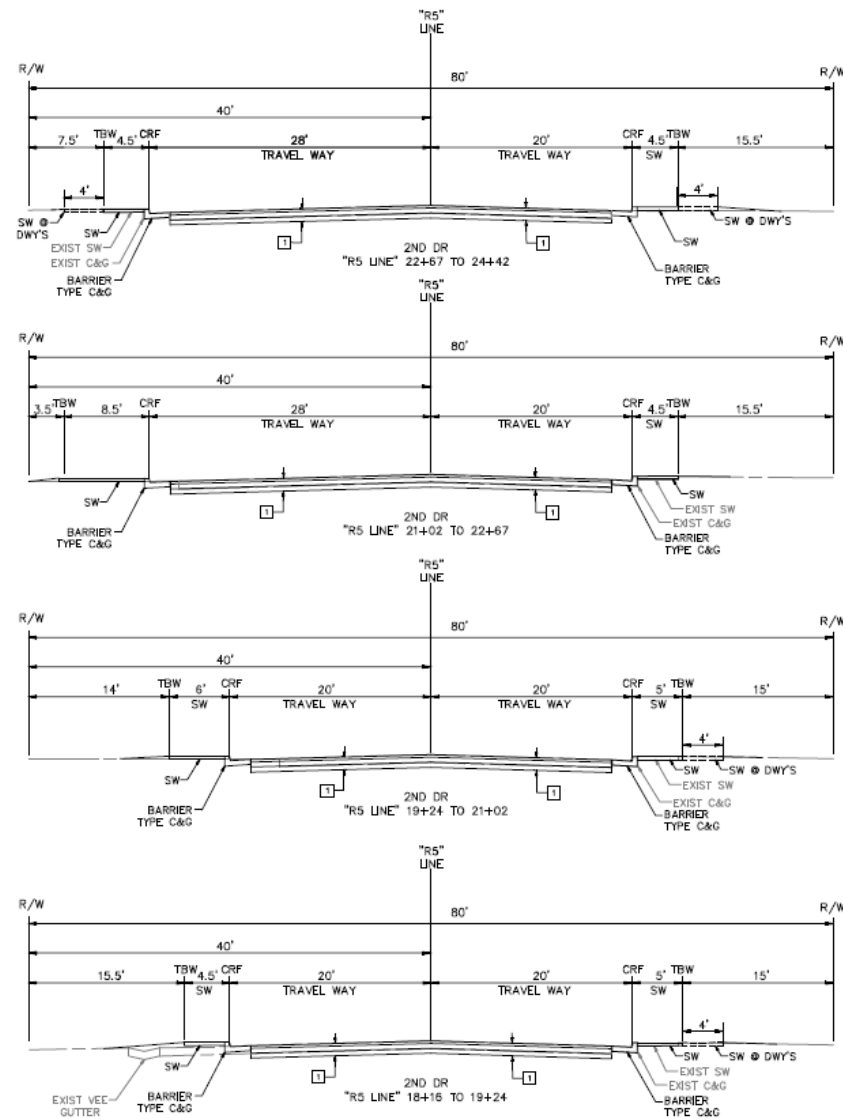
NO.	DATE	DESCRIPTION

DESIGNED	BY
CHECKED	BY
APPROVED	BY
DATE	

omni + motion
 ENGINEERS PLANNERS
 10000 WILSON AVENUE
 SUITE 200
 DUBLIN, CA 94568
 (916) 231-1000

**TYPICAL SECTIONS
 COMPLETE STREETS IMPROVEMENTS
 CUTLER, CALIFORNIA**

SCALE	1" = 1'
DATE	06/20/2017
DESIGNED	KCB
DRAWN	KCB
CHECKED	AWP
DATE	10/1/16
SHEET NO.	X1
2 of 7	



TYPICAL STRUCTURAL SECTIONS

1 20-YEAR DESIGN
 TI=0, R=20
 0.25' HMA (TYPE B)
 0.70' AB (CLASS 2)
 0.50' COMPACTED NATIVE

30% SUBMITTAL
 PRELIMINARY, NOT
 FOR CONSTRUCTION

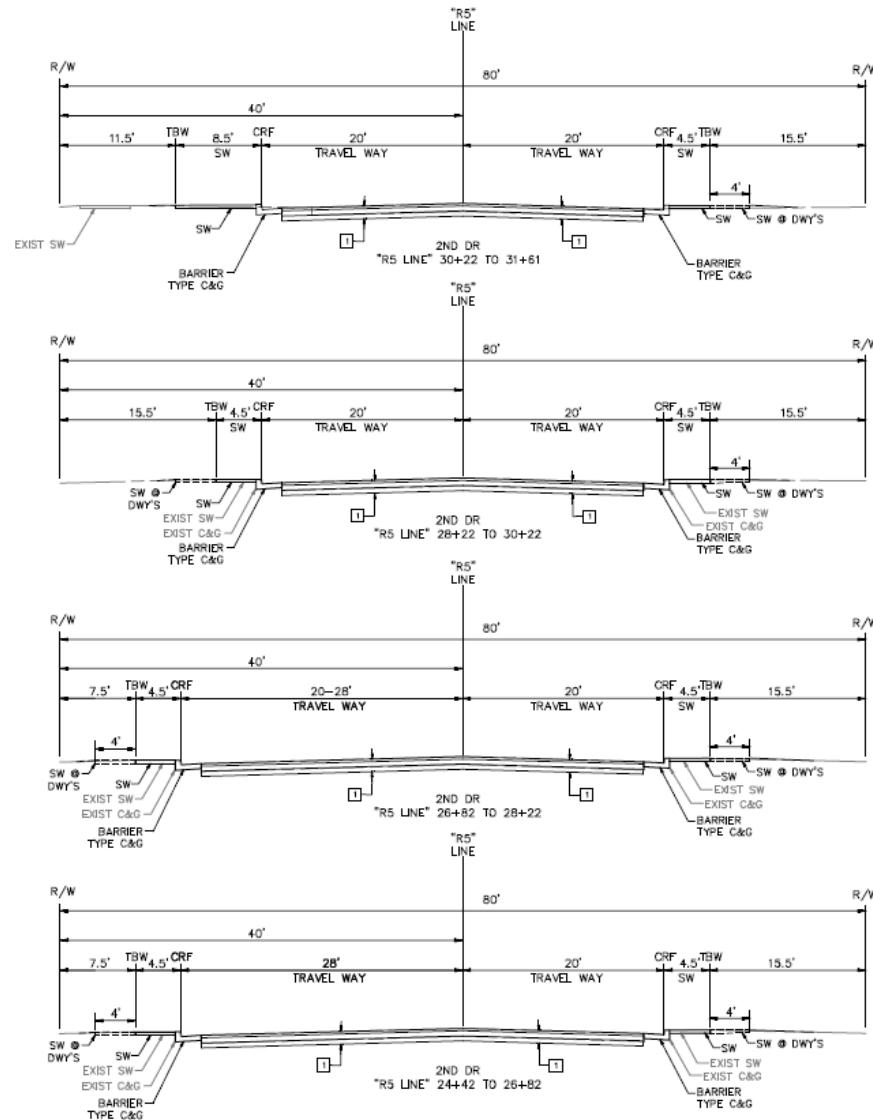


REVISIONS	DATE	BY



TYPICAL SECTIONS
 COMPLETE STREETS IMPROVEMENTS
 CUTLER, CALIFORNIA

SCALE	1" = 4'
JOB NO.	20-1700-12
DESIGNED	J.P.
CHECKED	J.P.
PLZ	20190502JWG
DATE	10-1-19
PROJECT	X2
SHEET	3 OF 7



TYPICAL STRUCTURAL SECTIONS:

- 1 20-YEAR DESIGN
- 11=3, 11=20
- 0.25' HMA (TYPE B)
- 0.70' AB (CLASS 2)
- 0.50' COMPACTED NATIVE

REVISIONS	DATE	BY	CHKD

PREPARED BY	DESIGNED BY
CHECKED BY	APPROVED BY
DATE	DATE

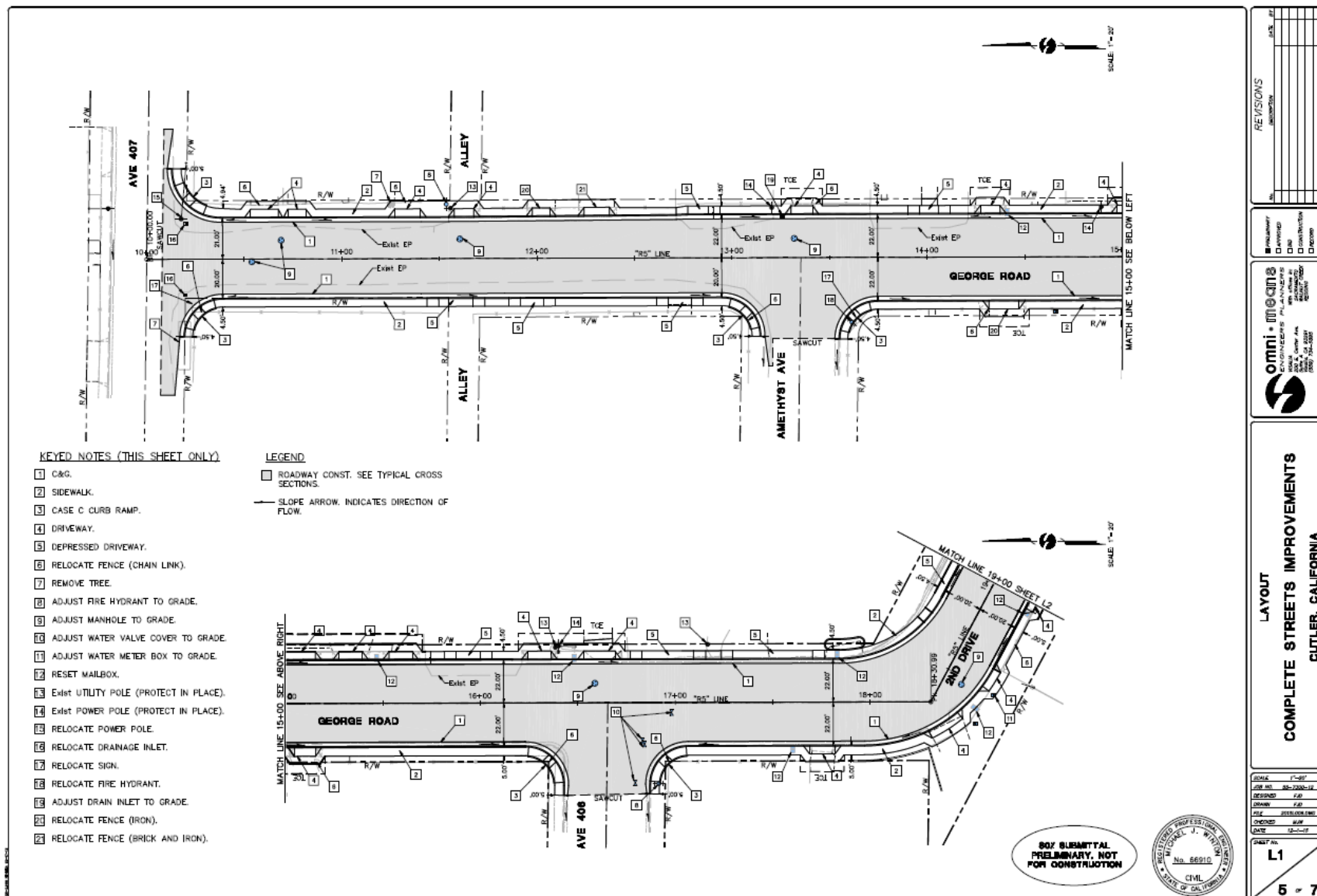
omni + m
ENGINEERING PLANNERS
ARCHITECTS
CONSULTANTS
1000 10th Ave
San Francisco, CA 94103
(415) 774-0000

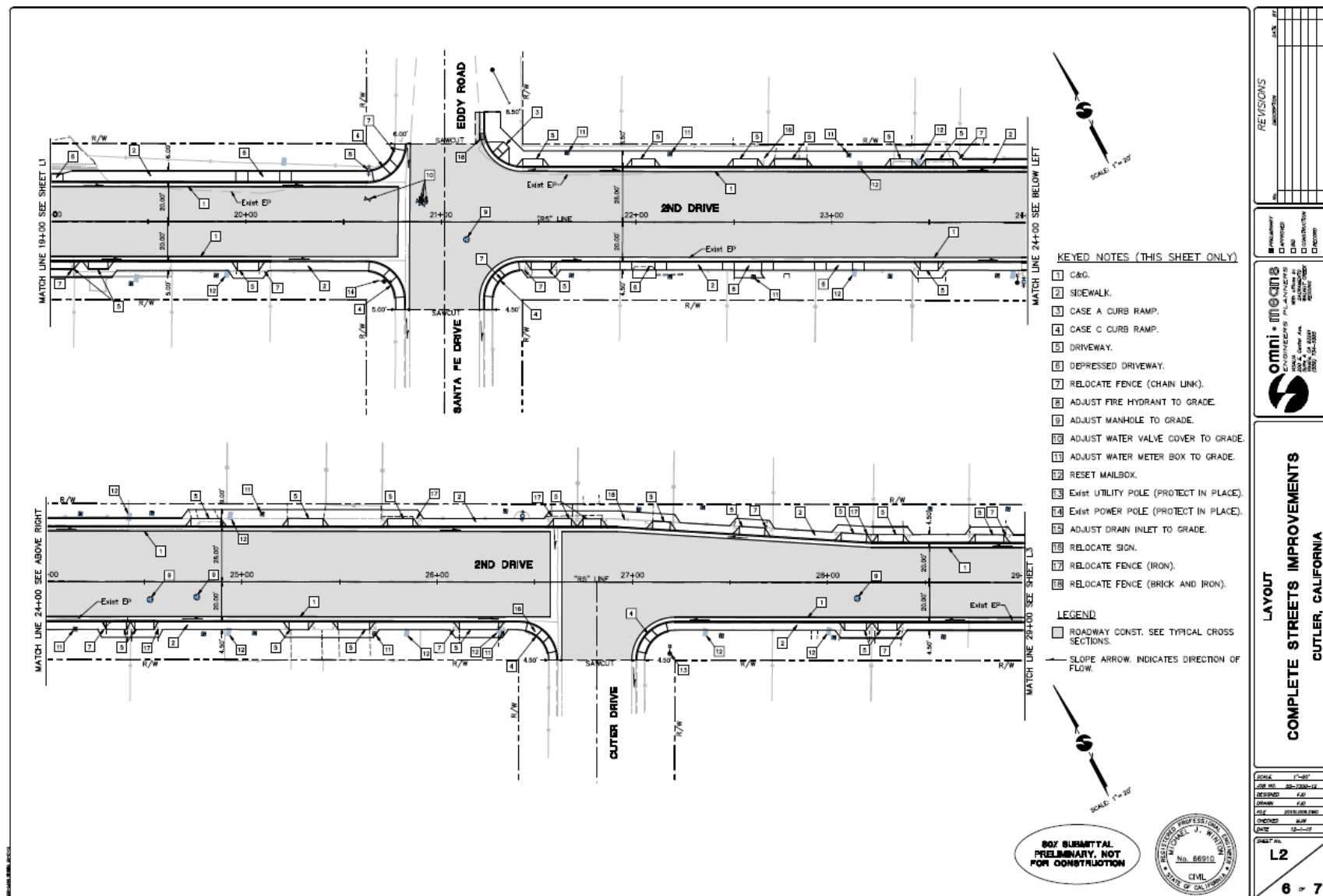
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COMPLETE STREET'S IMPROVEMENTS
CUTLER, CALIFORNIA

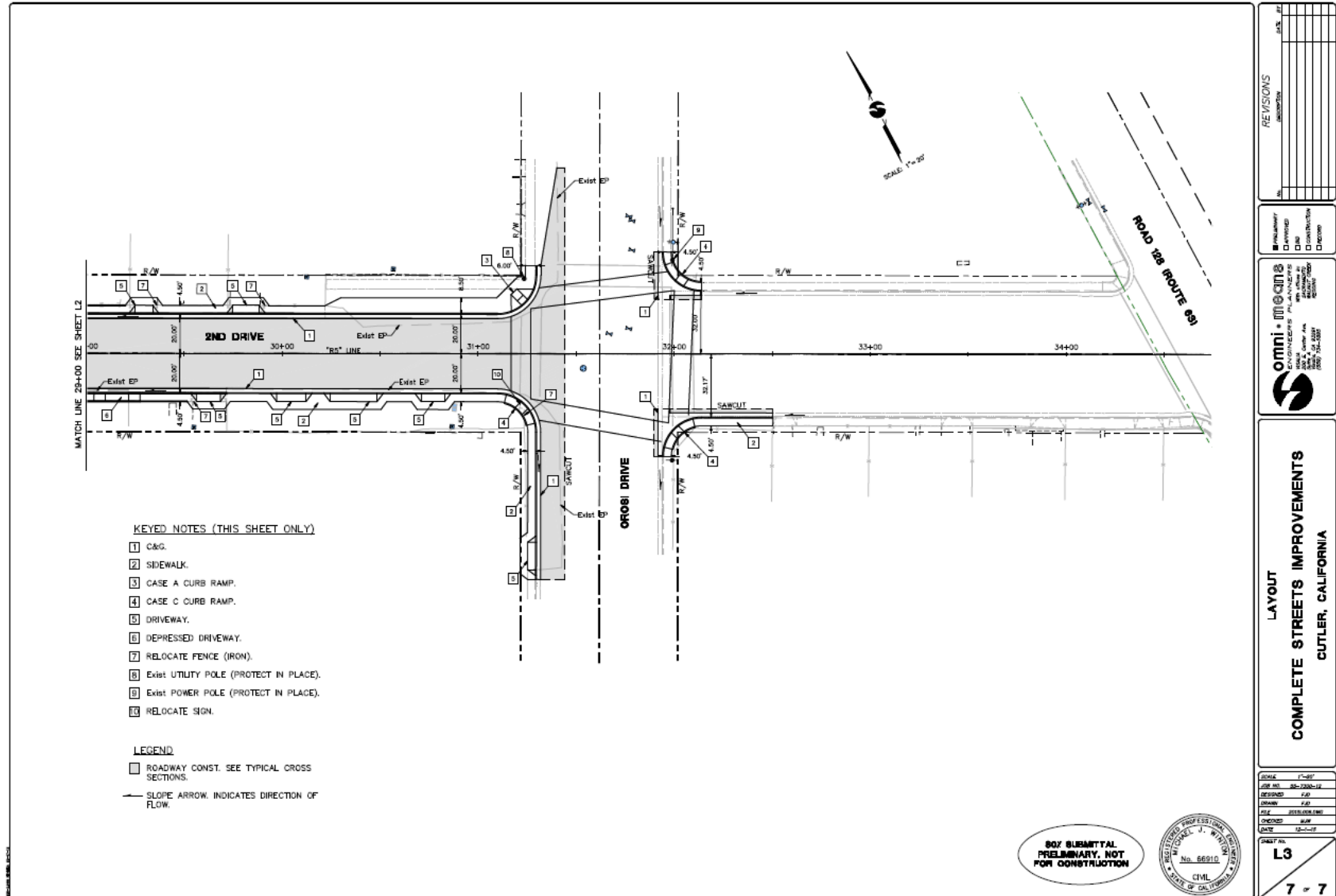
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JOB NO.	00-730-12
DATE	2/01
DESIGN	KJD
FILE	00/000000
CHECKED	SW
CAD	02-10-01
PROJECT NO.	X3
4	7

80% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION







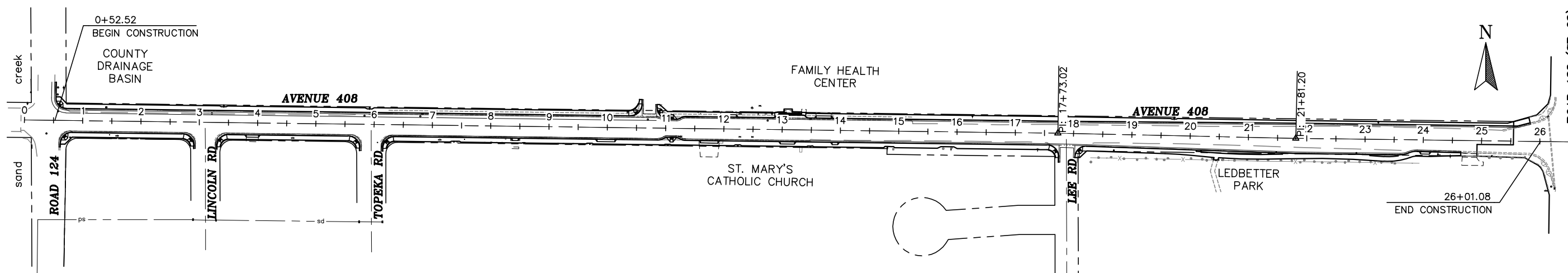


<u>PAGE</u>	<u>SHEET NO.</u>	<u>DESCRIPTION</u>
1	T1	COVER
2	X1	TYPICAL SECTIONS
3-5	L1-L3	LAYOUT
6-7	C1-C2	CONSTRUCTION DETAILS
8-10	PD1-PD3	SIGNING AND STRIPING PLAN

PROJECT PLANS FOR CONSTRUCTION OF CUTLER-COMplete STREETS POLICY

A map of California showing its county boundaries and names. A line extends from the word "CUTLER" to a specific location in the Central Valley, between Fresno and Kings counties. This location is marked with a black triangle and a hatched rectangular area. A north arrow is located in the upper right corner of the map.

LOCATION MAP
N.T.S.



VICINITY MAP
N.T.S.



Contractor shall call
Underground Service Alert at
811 two working days prior
to excavation

**30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION**

APPROVED BY:		
JABED KHAN, P.E.	TULARE COUNTY RESOURCE MANAGEMENT AGENCY	DATE
APPROVED BY:		
MICHAEL J. WINTON P.E.	PROJECT MANAGER OMNI-MEANS, LTD.	DATE

[illegible]

**TULARE COUNTY
RESOURCE MANAGEMENT
AGENCY**
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA



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ENGINEERS PLANNERS

With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

VSALIA
200 E. Center Ave.
Suite A CA 93291
Visalia, CA 93291
(559) 734-5885

COVER
AVENUE 408 - CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY CA

SCALE	NO SCALE
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144T004.DWG
DATE	6-9-16
SHEET No.	

T1

1 OF 10

TYPICAL STRUCTURAL SECTIONS:

- 1

20-YEAR DESIGN

Ti=5.5, R=25 (ASSUMED)

0.25' HMA (TYPE B)

0.70' AB (CLASS 2) (95% RC)

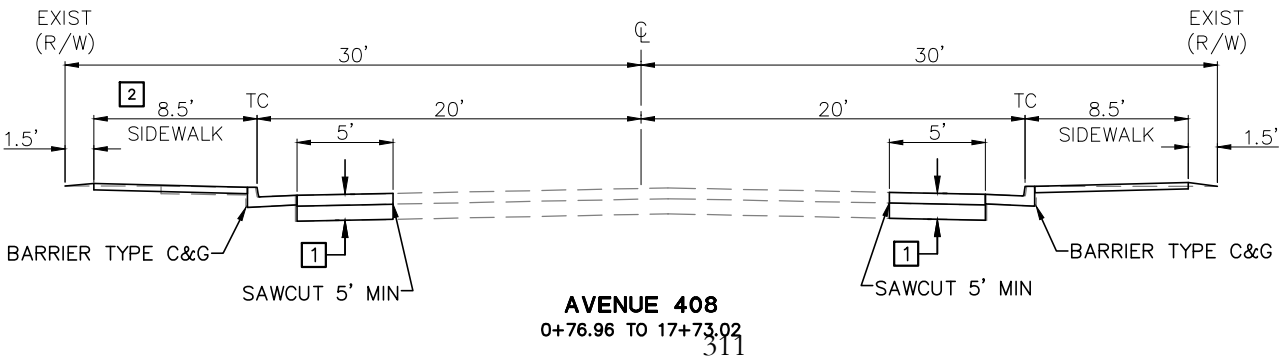
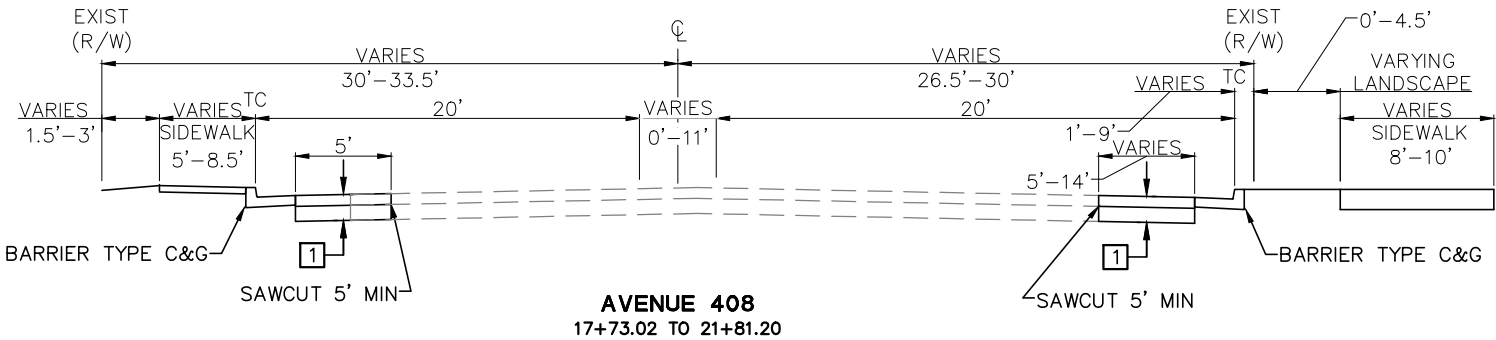
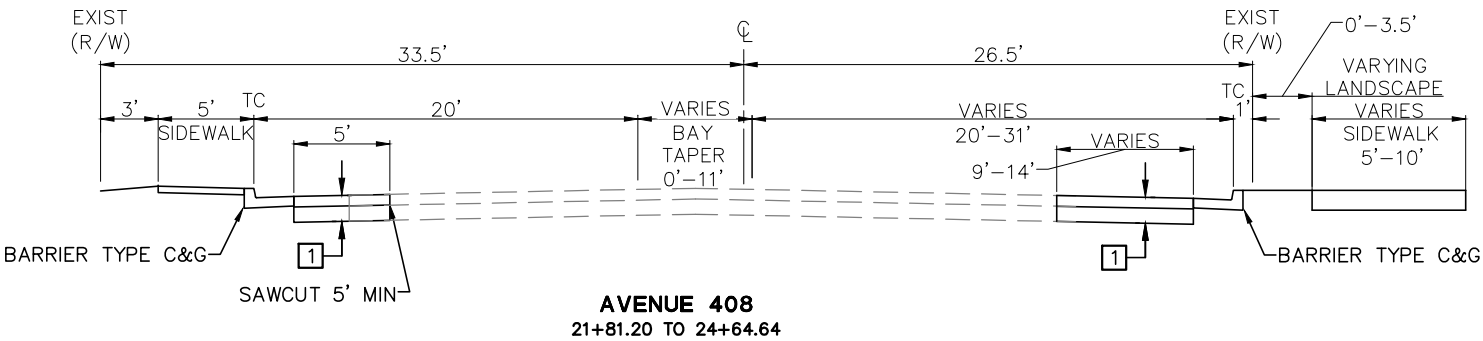
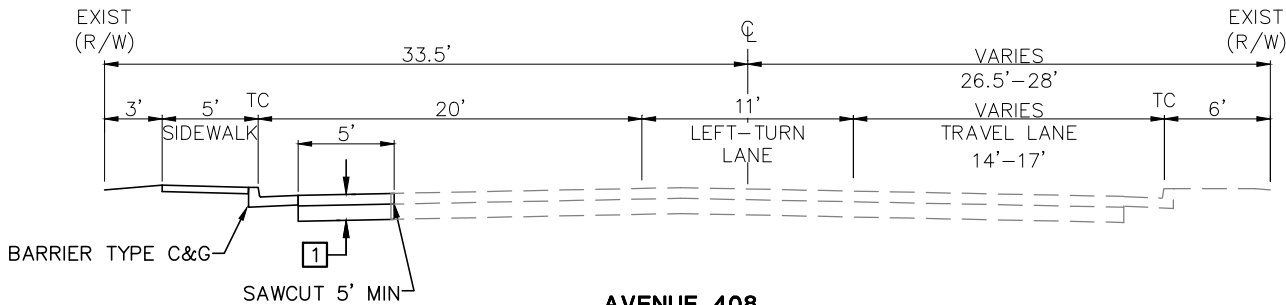
1.00' COMPACTED NATIVE (95% RC)

- 2

LOADING ZONE TURNOUT

STA 12+84.81 TO STA 13+45.35

SEE CONSTRUCTION DETAILS.



30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS		DATE	BY
No.	DESCRIPTION		

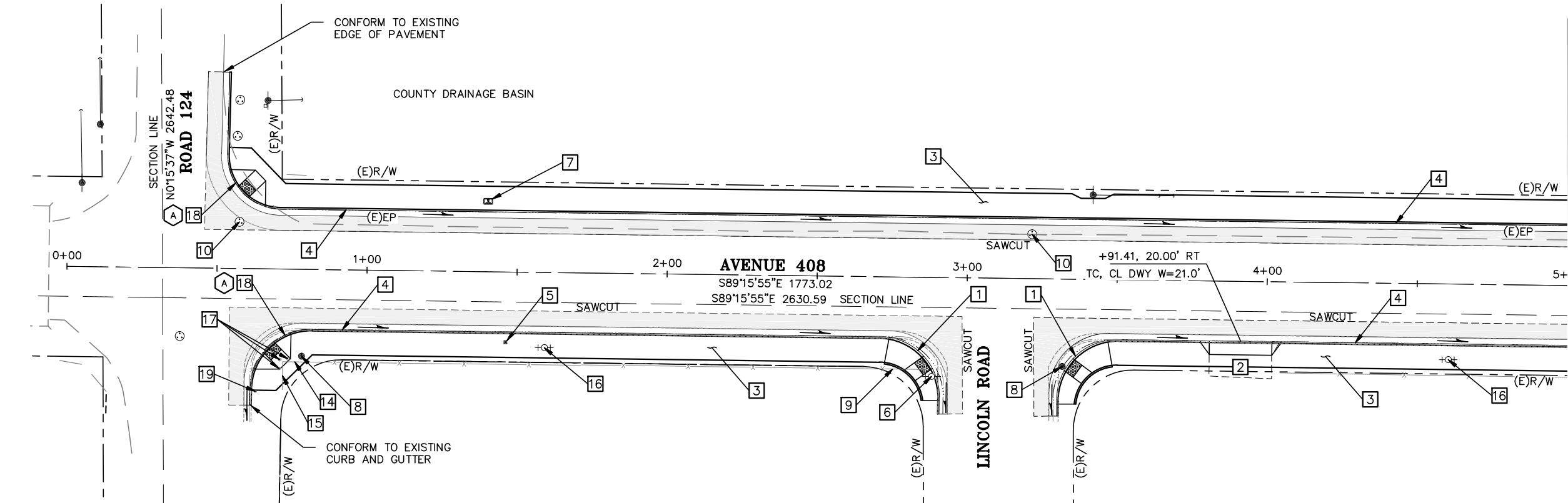
TULARE COUNTY
RESOURCE MANAGEMENT
AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 824-7000
WWW.TULARECOUNTY.CA.GOV/RMA



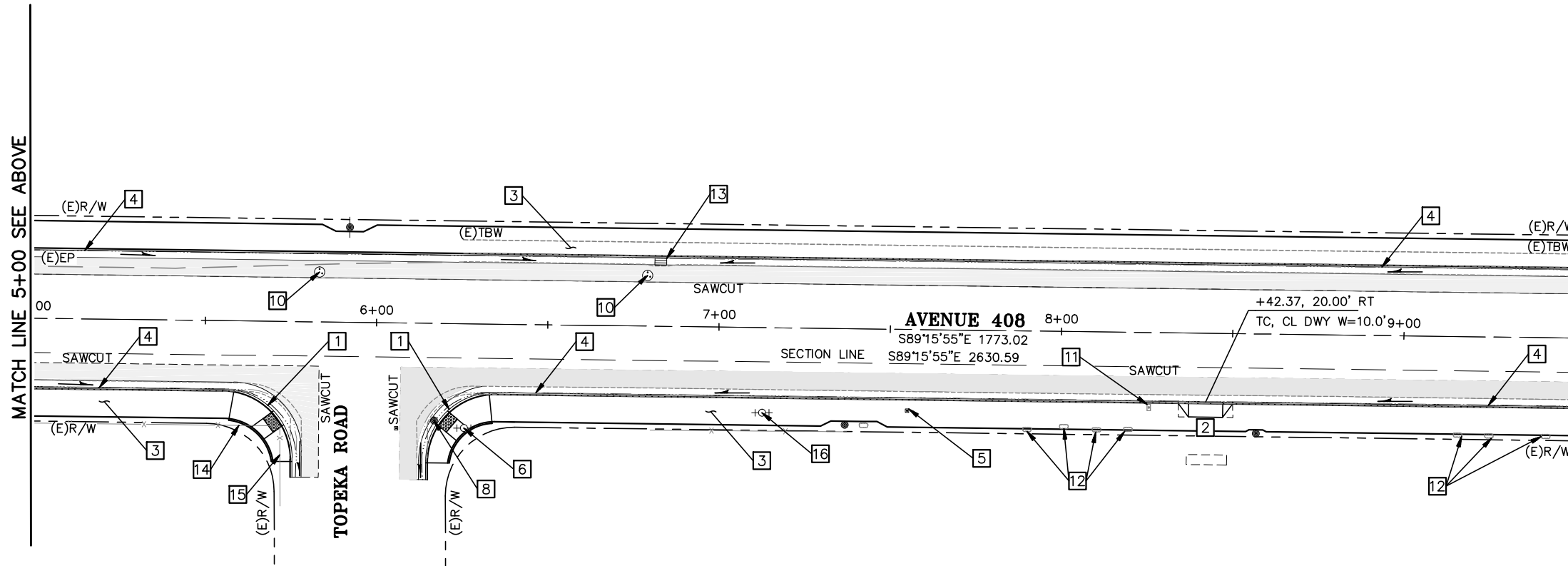
omni•means
ENGINEERS PLANNERS
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5685
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

TYPICAL SECTIONS
AVENUE 408 – CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=5'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144X004.DWG
DATE	6-9-16
SHEET No.	X1



MATCH LINE 5+00 SEE BELOW



CONSTRUCTION NOTES (THIS SHEET ONLY):

- 1 MODIFIED CASE C CURB RAMP. SEE SHEET C1.
- 2 TULARE COUNTY URBAN DRIVEWAY. SEE SHEET C1.
- 3 SIDEWALK.
- 4 TULARE COUNTY BARRIER TYPE CURB AND GUTTER PER PLATE NO. A-19. SEE SHEET C1.
- 5 ADJUST WATER METER BOX TO GRADE.
- 6 RELOCATE FIRE HYDRANT.
- 7 ADJUST TELEPHONE PULL BOX TO GRADE (BY OTHERS).
- 8 RELOCATE UTILITY POLE (BY OTHERS).
- 9 REMOVE FENCE (WOODEN) TO R/W.
- 10 ADJUST MANHOLE TO GRADE.
- 11 RESET MAILBOX.
- 12 REMOVE BUSHES.
- 13 RECONSTRUCT DRAINAGE INLET.
- 14 REMOVE FENCE (CHAIN LINK).
- 15 INSTALL FENCE (CHAIN LINK) AT R/W.
- 16 ADJUST FIRE HYDRANT TO GRADE.
- 17 REMOVE METAL POSTS.
- 18 CURB RAMP PER DETAIL A ON SHEET C1.
- 19 REMOVE IRRIGATION STANDPIPE.

GENERAL NOTES (ALL L-SHEETS):

1. SEE SIGNING AND STRIPING PLAN FOR EXISTING SIGNS TO BE REMOVED OR RELOCATED.

LEGEND:

- ROADWAY CONSTRUCTION
- TEMPORARY CONSTRUCTION EASEMENT
- SLOPE ARROW, INDICATES DIRECTION OF FLOW



Contractor shall call Underground Service Alert at 811 two working days prior to excavation

30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION



REVISIONS	
No.	DESCRIPTION

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
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With offices in:
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WALNUT CREEK
REDDING
SAN LUIS OBISPO
200 E. Center Ave.
Visalia, CA 93291
(559) 734-5685

LAYOUT
AVENUE 408 - CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144L004.DWG
DATE	6-9-16
SHEET No.	

L1

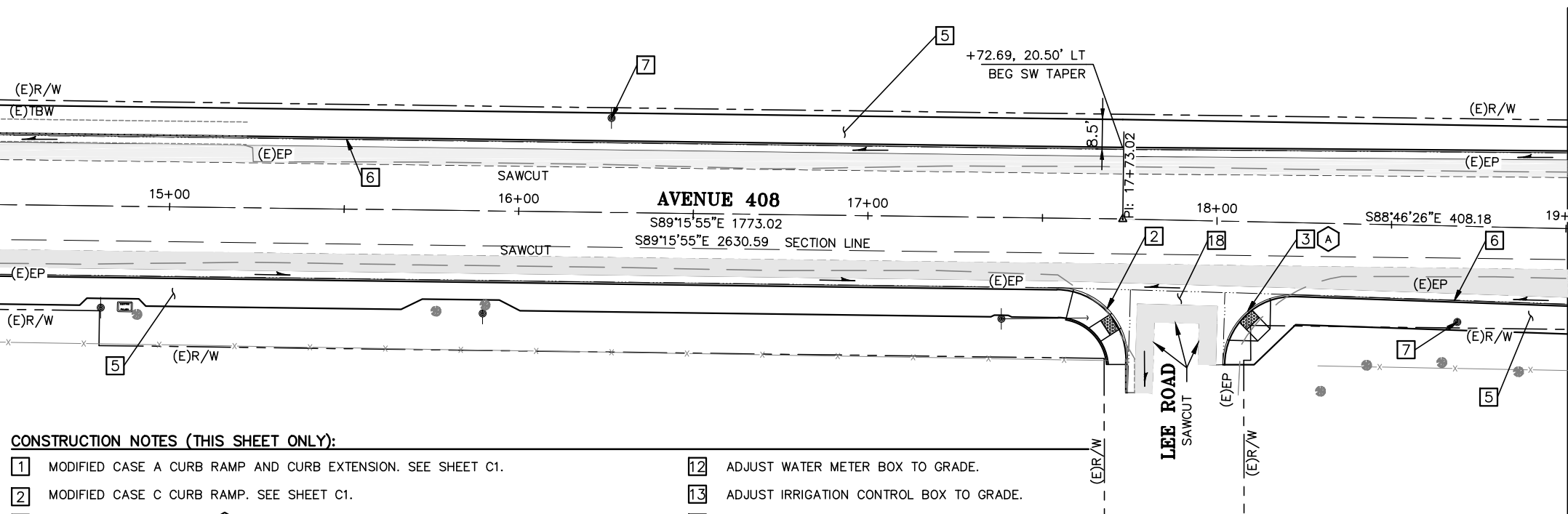
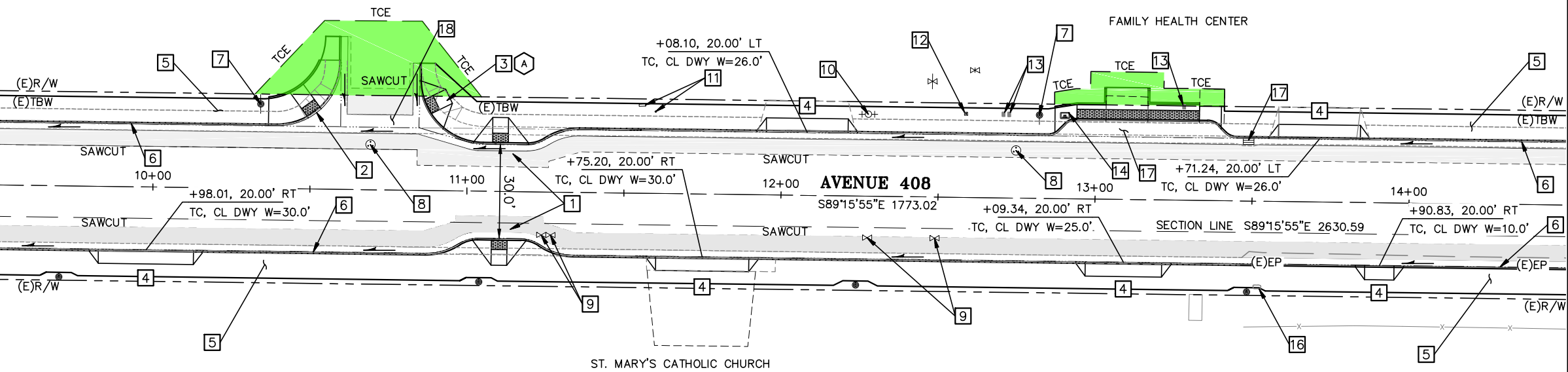
3 of 10

MATCH LINE 9+50 SEE SHEET L1

MATCH LINE 14+50 SEE ABOVE

MATCH LINE 19+00 SEE SHEET L3

MATCH LINE 14+50 SEE BELOW



CONSTRUCTION NOTES (THIS SHEET ONLY):

- | | | | |
|----|--|----|--|
| 1 | MODIFIED CASE A CURB RAMP AND CURB EXTENSION. SEE SHEET C1. | 12 | ADJUST WATER METER BOX TO GRADE. |
| 2 | MODIFIED CASE C CURB RAMP. SEE SHEET C1. | 13 | ADJUST IRRIGATION CONTROL BOX TO GRADE. |
| 3 | CURB RAMP PER DETAIL A ON SHEET C1. | 14 | ADJUST ELECTRICAL PULL BOX TO GRADE (BY OTHERS). |
| 4 | TULARE COUNTY URBAN DRIVEWAY. SEE SHEET C1. | 15 | RECONSTRUCT DRAINAGE INLET. |
| 5 | SIDEWALK. | 16 | REMOVE BUSH. |
| 6 | TULARE COUNTY BARRIER TYPE CURB AND GUTTER PER PLATE NO. A-19. SEE SHEET C1. | 17 | CONSTRUCT LOADING ZONE TURNOUT. SEE SHEET C2 |
| 7 | EXIST UTILITY POLE TO REMAIN (PROTECT IN PLACE). | 18 | CONSTRUCT CONTINUOUS GUTTER CURB RETURN. SEE SHEET C2. |
| 8 | ADJUST MANHOLE TO GRADE. | | |
| 9 | ADJUST WATER VALVE FRAME AND COVER TO GRADE. | | |
| 10 | ADJUST FIRE HYDRANT TO GRADE. | | |
| 11 | RELOCATE TELEPHONE PEDESTAL (BY OTHERS). | | |

LEGEND:

- ROADWAY CONSTRUCTION
- TEMPORARY CONSTRUCTION EASEMENT
- SLOPE ARROW, INDICATES DIRECTION OF FLOW



Contractor shall call
Underground Service Alert at
811 two working days prior
to excavation

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
No.	DESCRIPTION

TULARE COUNTY
RESOURCE MANAGEMENT
AGENCY

5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 824-7000
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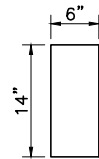
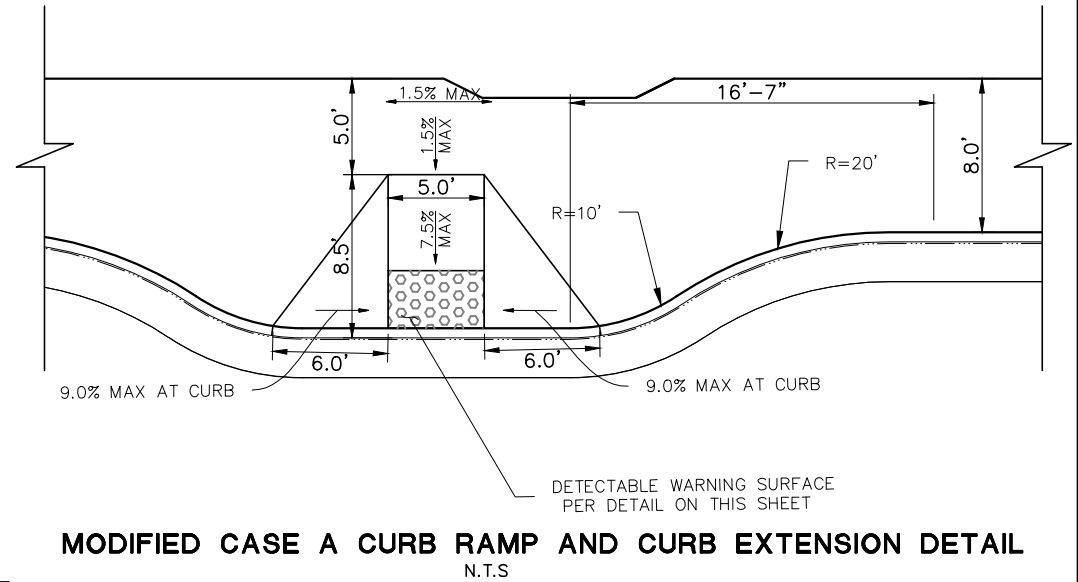
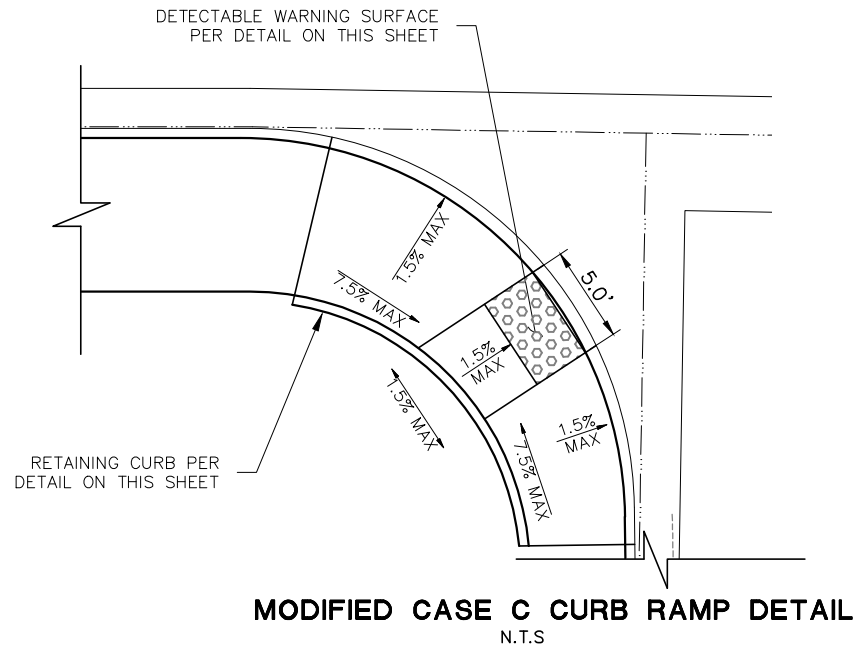
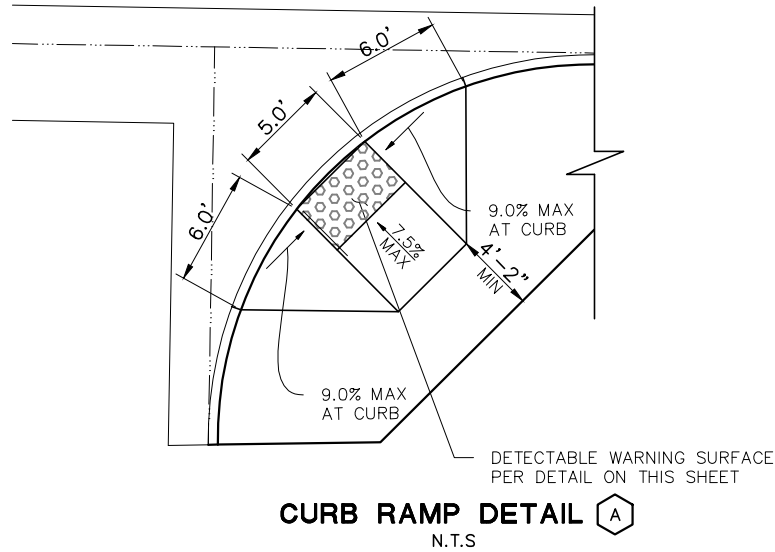
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ENGINEERS PLANNERS

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Visalia, CA 93291
(559) 734-5685

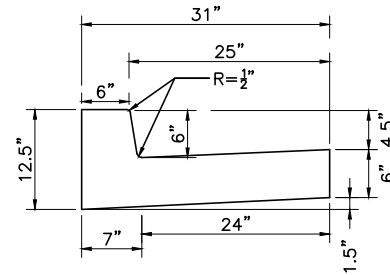
LAYOUT
AVENUE 408 - CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144L004.DWG
DATE	6-9-16
SHEET No.	

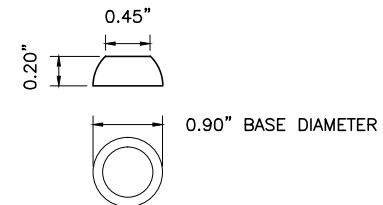
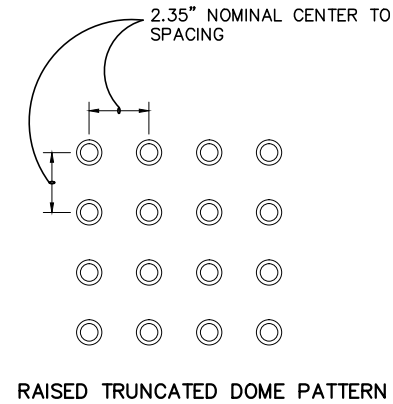
L2
4 OF 10



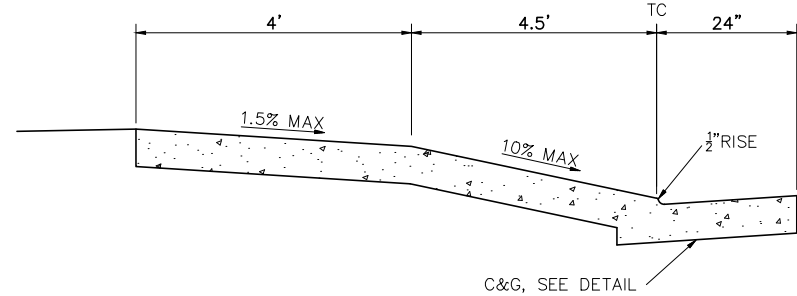
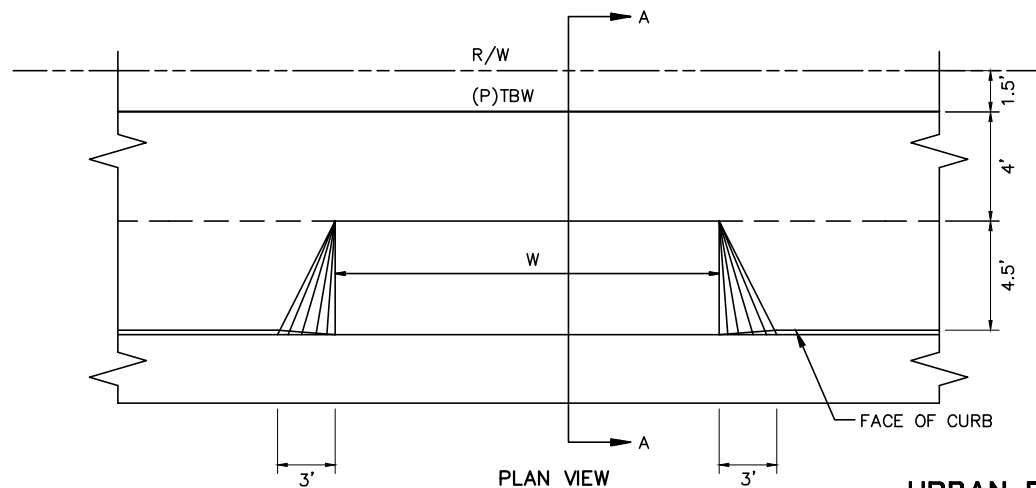
- NOTES:
1. ALL CONCRETE SHALL BE MINOR CONCRETE.



- NOTES:
1. ALL CONCRETE SHALL BE MINOR CONCRETE.
 2. BARRIER TYPE CURB AND GUTTER SHALL HAVE A MINIMUM OF 0.15 FEET PER 100 FEET.
 3. AREA BETWEEN BACK OF CURB AND AND PROPERTY LINE SHALL BE BACK FILLED AND SLOPED TO DRAIN TO GUTTER.



- NOTES:
1. SHALL BE INSTALLED AT THE BOTTOM OF ALL CURB RAMPS.
 2. SHALL BE THE FULL WIDTH OF RAMP AND SHALL BE A MIN. OF 36" IN DEPTH
 3. SHALL BE PREMIXED FEDERAL YELLOW COLORED POLYMER CONCRETE MATERIAL.
 4. ALL DETECTABLE WARNING PANELS INSTALLED WITH NEW IMPROVEMENTS SHALL BE WET SET TYPE/CAST IN PLACE TYPE PANELS.
 5. A 4'-0" WIDE DETECTABLE WARNING SURFACE MAY BE USED ON A 4'-2" WIDE CURB RAMP.
 6. DETECTABLE WARNING SURFACES SHALL CONFORM TO THE REQUIREMENTS IN THE STANDARDS SPECIFICATIONS.

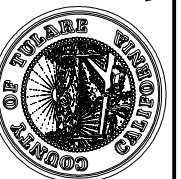


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PRELIMINARY, NOT
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REVISIONS	DATE	BY
No.		
DESCRIPTION		

**TULARE COUNTY
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5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 824-7000
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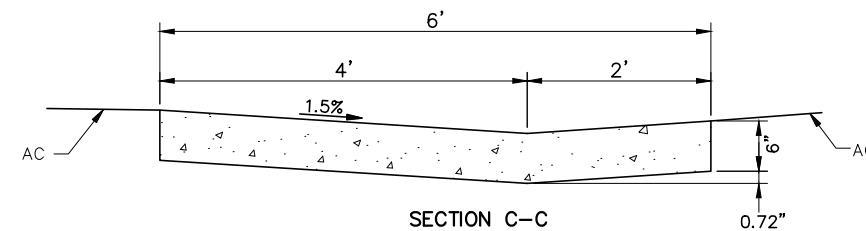
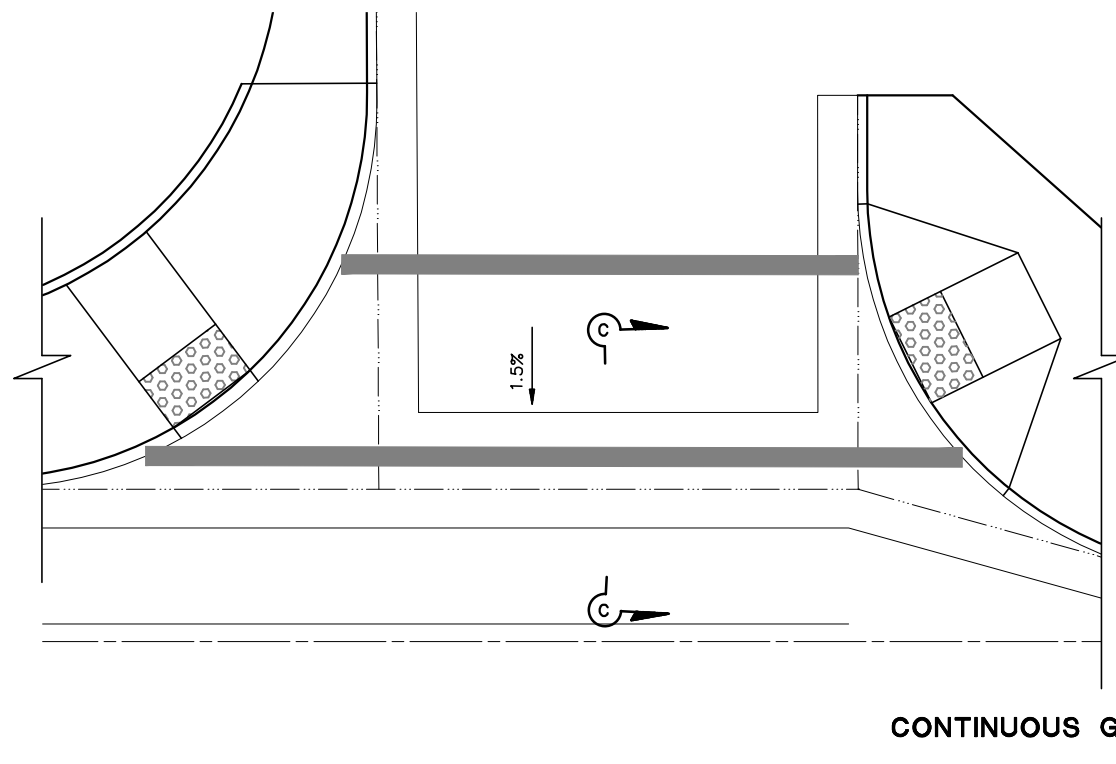
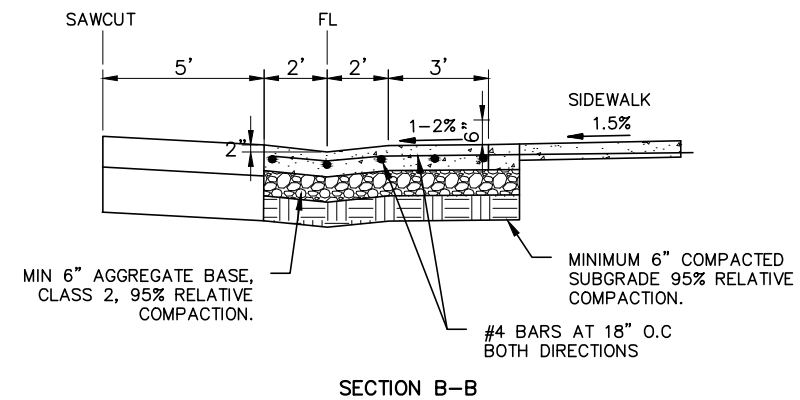
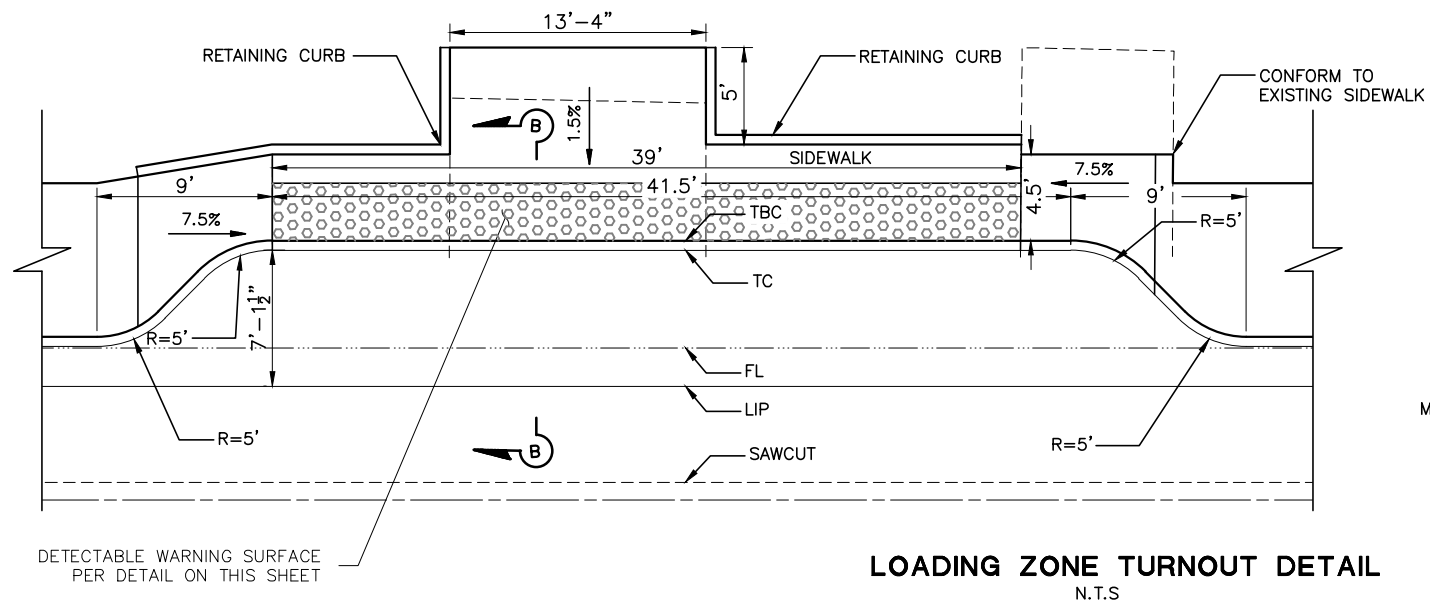
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Suite A
Visalia, CA 93291
(559) 734-5685
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

CONSTRUCTION DETAILS
AVENUE 408 - CUTLER
**TULARE COUNTY
COMPLETE STREETS**
TULARE COUNTY, CA

SCALE	AS SHOWN
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144C004.DWG
DATE	6-9-16
SHEET No.	

C1

6 OF 10



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**TULARE COUNTY
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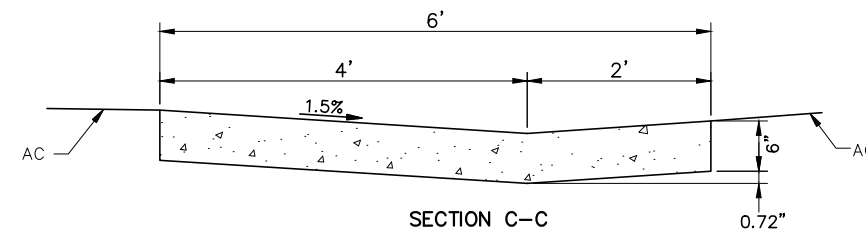
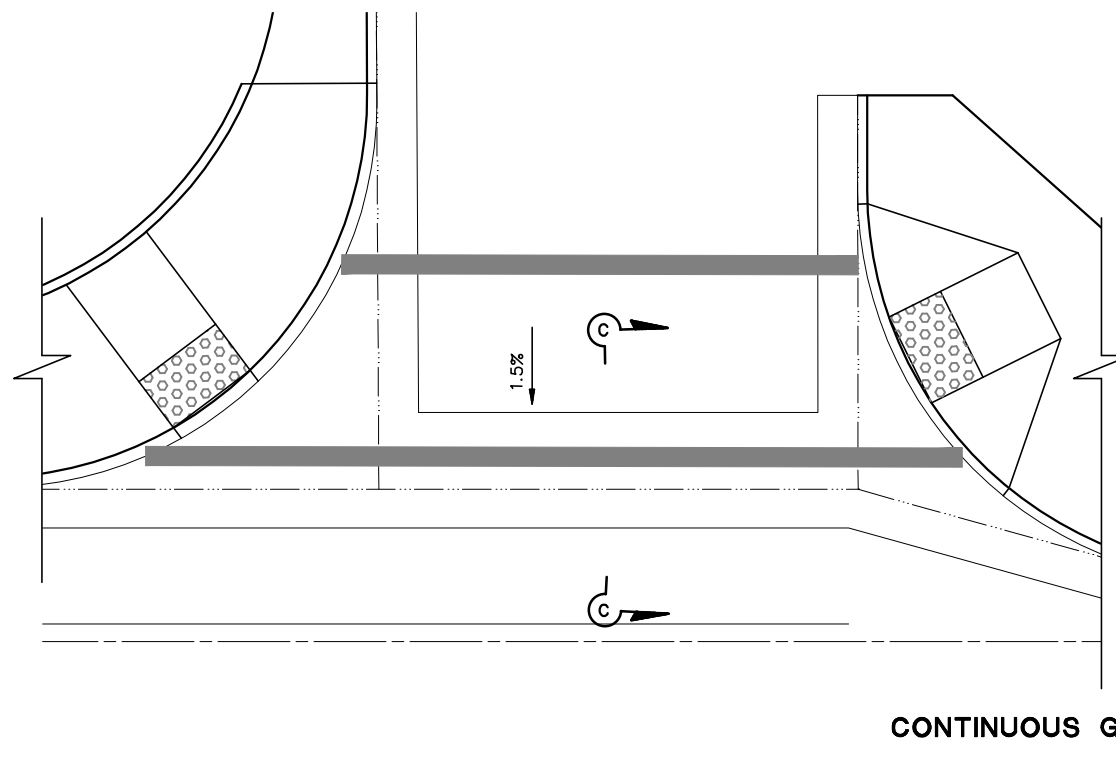
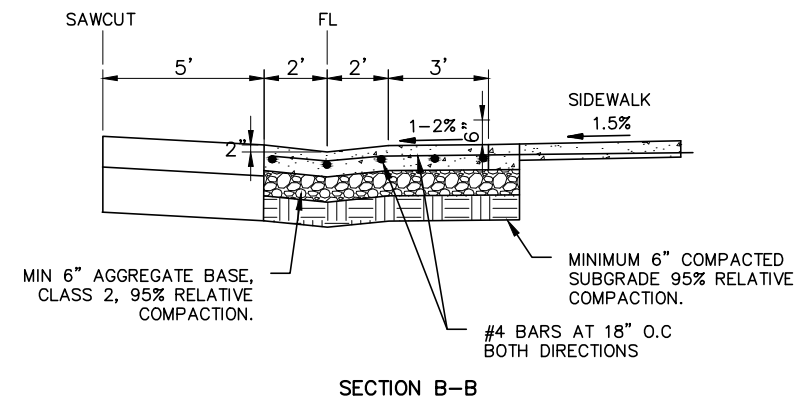
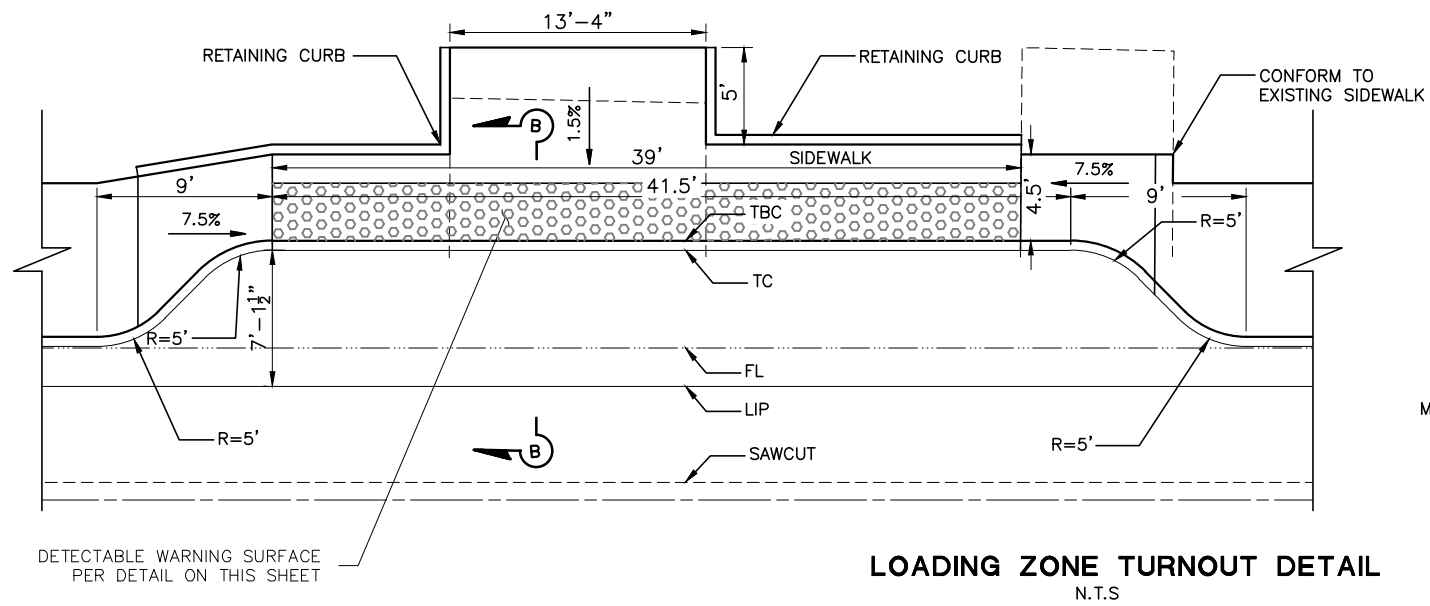
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5805

CONSTRUCTION DETAILS
AVENUE 408 – CUTLER
**TULARE COUNTY
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TULARE COUNTY, CA

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VISION	RMA
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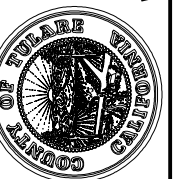
7 OF 10



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CONSTRUCTION DETAILS
AVENUE 408 – CUTLER
**TULARE COUNTY
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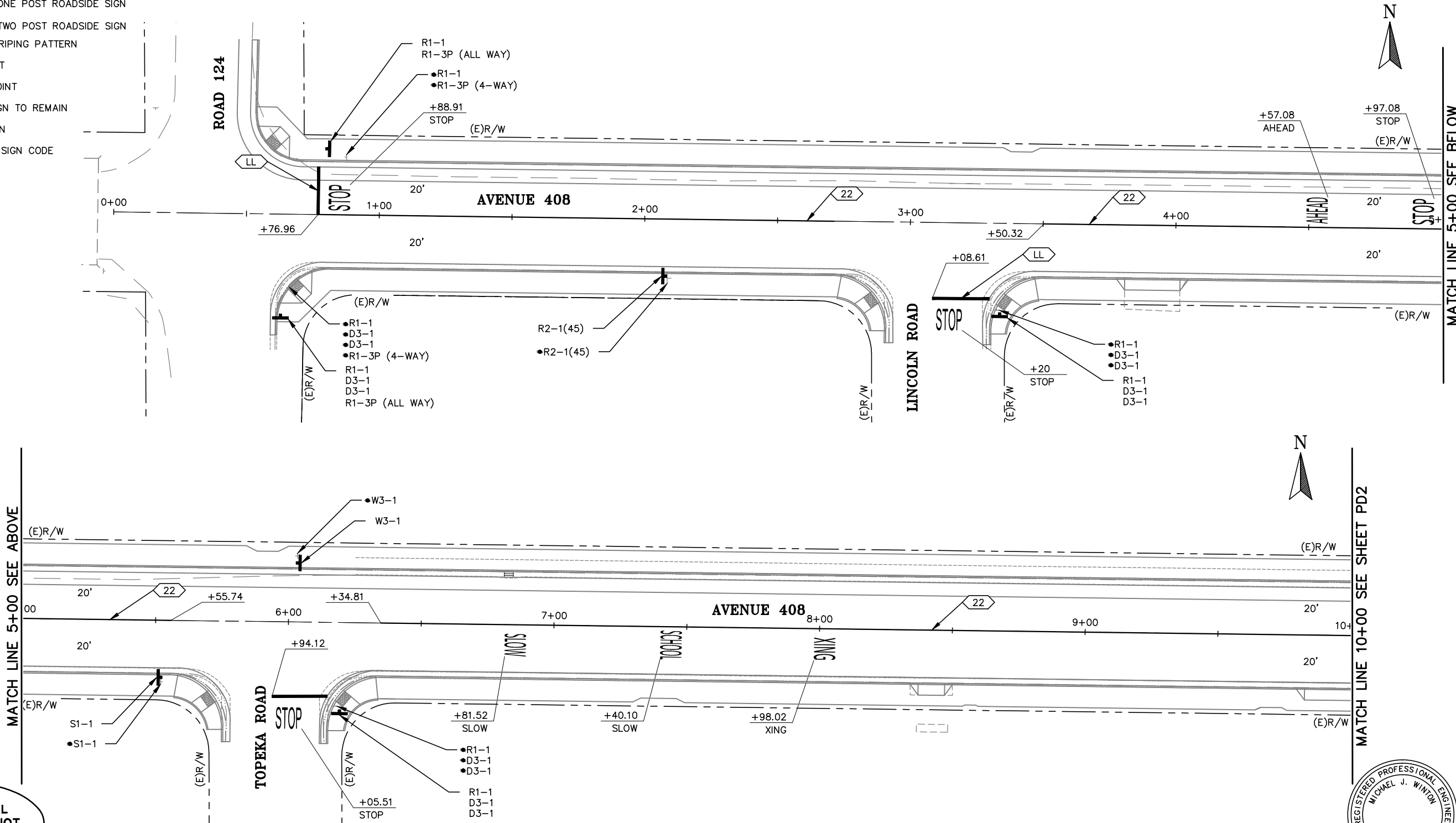
7 OF 10

LEGEND: (SHEETS PD1-PD3)

- # STRIPING DETAIL NUMBER PER STATE STD PLANS
- CW THERMOPLASTIC CROSSWALK (CW) PER STATE STD PLANS
- LL THERMOPLASTIC LIMIT LINE (LL)
- STOP INSTALL PAVEMENT MARKING, WORD AS INDICATED, PER STATE STANDARD PLANS
- LIMIT OF STRIPING PATTERN
- EXISTING ONE POST ROADSIDE SIGN
- EXISTING TWO POST ROADSIDE SIGN
- PROPOSED ONE POST ROADSIDE SIGN
- PROPOSED TWO POST ROADSIDE SIGN
- LIMIT OF STRIPING PATTERN
- ANGLE POINT
- TANGENT POINT
- EXISTING SIGN TO REMAIN
- REMOVE SIGN
- (CA) CALIFORNIA SIGN CODE

GENERAL NOTES:(SHEETS PD1-PD3)

1. WORK SHALL BE DONE IN ACCORDANCE WITH THE 2015 EDITION OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND SPECIFICATIONS, THE 2014 EDITION OF THE CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) AND SIGN SPECIFICATION SHEETS, THE LATEST EDITIONS OF THE TULARE COUNTY STANDARD PLANS AND SPECIFICATIONS, AND THE SPECIAL PROVISIONS.
2. THESE PLANS ARE ACCURATE FOR SIGNING AND STRIPING ONLY.
3. ALL SIGNS SHALL BE AVERY DENNISON T6500 SERIES REFLECTIVE SHEETING AND SHALL BE COVERED WITH AVERY DENNISON 0L1000 ANTI-GRAFFITI OVERLAY FILM.
4. ALL STRIPING SHALL BE THERMOPLASTIC PAINT.
5. ALL CROSSWALKS SHALL BE 10 FEET WIDE, MEASURED FROM INSIDE TO INSIDE OF MARKING.
6. REMOVE ALL STRIPING AND MARKINGS CONFLICTING WITH NEW PAVEMENT DELINEATION.
7. LANE WIDTHS ARE MEASURED TO TOP FACE OF CURB OR STRIPE, WHICH EVER COMES FIRST.



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SIGNING AND STRIPING PLAN
AVENUE 408 - CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE 1"=20'

DIVISION RMA

JOB NO. 55-7300-14

DESIGNED JAV

DRAWN JAV

CHECKED MJW

FILE 2144PD004.DWG

DATE 6-9-16

SHEET No.

PD1

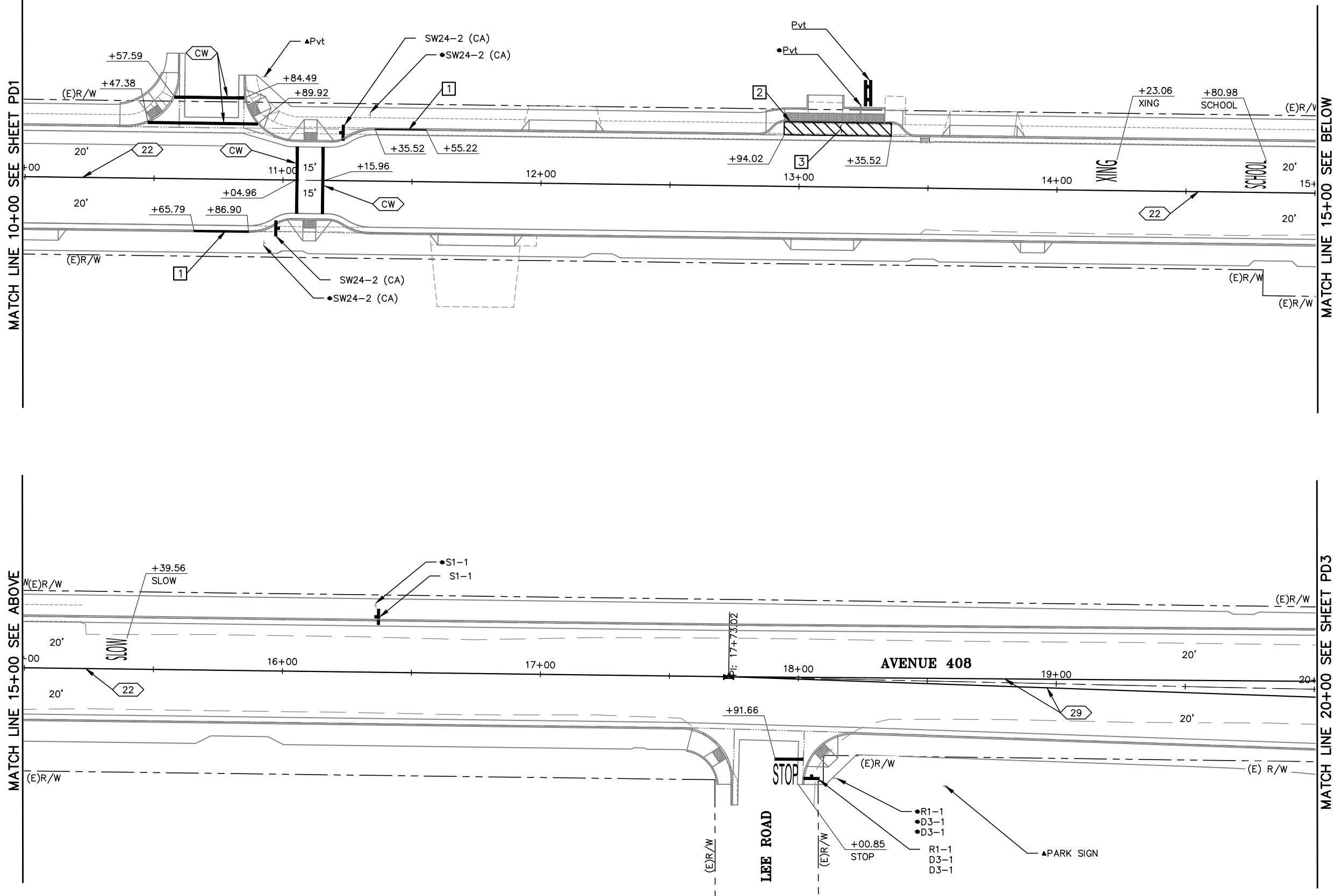
8 OF 10

MATCH LINE 10+00 SEE SHEET PD1

MATCH LINE 15+00 SEE ABOVE

MATCH LINE 15+00 SEE BELOW

MATCH LINE 20+00 SEE SHEET PD3



CONSTRUCTION NOTES (THIS SHEET ONLY):

- 1 PAINT CURB RED.
- 2 PAINT 41.5-FT OF CURB YELLOW WITH LOADING ZONE LEGEND.
- 3 ACCESS AISLE WILL HAVE A 4-IN WHITE BORDER STRIPE. DIAGONALS WILL BE 4-IN AND PAINTED WHITE, 3-FT O.C.

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SIGNING AND STRIPING PLAN
AVENUE 408 – CUTLER
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DESIGNED JAV

DRAWN JAV

CHECKED MJW

FILE 2144PD004.DWG

DATE 6-9-16

SHEET No.

PD2

9 OF 10

Appendix G
Cost Estimates for Orosi

PRELIMINARY COST ESTIMATE (30-Percent)						
Orosi Complete Street Improvements				Date:	10/21/2015	
County of Tulare						
Construction						
ITEM		ITEM DESCRIPTION	UNIT OF	ESTIMATED	ITEM	TOTAL
NO	(F)		MEASURE	QUANTITY	PRICE	
1		MOBILIZATION	LS	1	\$ 91 500.00	\$ 91 500
2	-	JOB SITE MANAGEMENT	LS	1	\$ 8 000.00	\$ 8 000
3	-	WATER POLLUTION CONTROL PROGRAM	LS	1	\$ 4 500.00	\$ 4 500
4	-	CONSTRUCTION AREA SIGNS	LS	1	\$ 10 000.00	\$ 10 000
5	-	TRAFFIC CONTROL SYSTEM	LS	1	\$ 30 000.00	\$ 30 000
6	-	RELOCATE FENCE (CHAIN LINK)	LF	286	\$ 50.00	\$ 14 300
7	-	RELOCATE FENCE (IRON)	LF	114	\$ 90.00	\$ 10 304
8	-	RELOCATE FENCE (BRICK AND IRON)	LF	105	\$ 150.00	\$ 15 695
9	-	RELOCATE FENCE (WOODEN)	LF	46	\$ 40.00	\$ 1 840
10	-	RESET MAILBOX	EA	54	\$ 300.00	\$ 16 200
11	-	RESET STREET SIGN	EA	8	\$ 500.00	\$ 4 000
12	-	REMOVE TREE	EA	15	\$ 800.00	\$ 12 000
13	-	BARRIER POST	EA	3	\$ 500.00	\$ 1 500
14	-	TYPE 3 MARKERS FOR BARRIER POSTS	EA	3	\$ 25.00	\$ 75
15	-	ADJUST DRAIN INLET TO GRADE	EA	1	\$ 1 500.00	\$ 1 500
16	-	ADJUST WATER VALVE COVER TO GRADE	EA	5	\$ 800.00	\$ 4 000
17	-	ADJUST FIRE HYDRANT TO GRADE	EA	1	\$ 2 000.00	\$ 2 000
18	-	ADJUST GAS VALVE FRAME AND COVER TO GRADE	EA	3	\$ 800.00	\$ 2 400
19	-	ADJUST WATER METER BOX TO GRADE	EA	8	\$ 3 500.00	\$ 28 000
20	-	ROADSIDE DITCH	LF	5 374	\$ 10.00	\$ 53 740
21	-	CLEARING AND GRUBBING	LS	1	\$ 10 000.00	\$ 10 000
22	(F)	ROADWAY EXCAVATION	CY	2 617	\$ 60.00	\$ 157 000
23	(F)	CLASS 2 AGGREGATE BASE	CY	1 728	\$ 60.00	\$ 103 651
24	-	HOT MIX ASPHALT (TYPE A)	TON	1 475	\$ 110.00	\$ 162 250
25	-	12" REINFORCED CONCRETE PIPE	LF	1 024	\$ 100.00	\$ 102 400
26	-	18" REINFORCED CONCRETE PIPE	LF	82	\$ 165.00	\$ 13 530
27	-	MINOR CONCRETE (MINOR STRUCTURE-TYPE GO INLET)	EA	3	\$ 3 500.00	\$ 10 500
28	-	MINOR CONCRETE (CURB AND GUTTER/DIKE)	LF	717	\$ 25.00	\$ 17 928
29	-	MINOR CONCRETE (SIDEWALK)	SQFT	4 016	\$ 7.00	\$ 28 111
30	-	MINOR CONCRETE (DRIVEWAYS AND CURB RAMPS)	SQFT	2 140	\$ 15.00	\$ 32 094
31	-	DETECTABLE WARNING SURFACE	SQFT	15	\$ 45.00	\$ 675
32	-	SIGNING & STRIPING	LS	1	\$ 8 000.00	\$ 8 000
33	-	EROSION CONTROL	LS	1	\$ 5 000.00	\$ 5 000
34	-	MISCELLANEOUS ITEMS	LS	1	\$ 217 800.00	\$ 217 800
		Total - Construction Items 1-34				\$1 180 493
		Contingency (25%)				\$ 295 123
		Recommended Total Construction Budget				\$1 475 617
Non-Construction Related Costs						
ITEM		ITEM DESCRIPTION	UNIT OF	ESTIMATED	ITEM	TOTAL
NO	(F)		MEASURE	QUANTITY	PRICE	
35	-	Environmental Clearance	% of CON	5%	\$1 180 493.46	\$ 59 025
36	-	Final Engineering Design	% of CON	15%	\$1 180 493.46	\$ 177 074
37	-	Construction Support	% of CON	2%	\$1 180 493.46	\$ 23 610
38	-	Construction Management	% of CON	15%	\$1 180 493.46	\$ 177 074
		Total - Non-Construction Items 35-38				\$ 436 783
		Total Construction & Non-Construction Items				\$1 912 399

Preliminary Cost Estimate (30-Percent)

Avenue 413, Orosi
Tulare County Complete Streets

6/9/2016
55-7300-14/2144

Construction Costs:

No.	Item Description	Units	Quantity	Unit Cost	Total
1	Job Site Management	LS	1	\$8,000.00	\$8,000.00
2	Construction Area Signs	LS	1	\$7,500.00	\$7,500.00
3	Traffic Control	LS	1	\$30,000.00	\$30,000.00
4	Water Pollution Control Program	LS	1	\$4,500.00	\$4,500.00
5	Remove Roadside Sign	EA	8	\$130.00	\$1,040.00
6	Remove Pavement Markings	SQFT	228	\$2.70	\$615.60
7	Remove Concrete (Curb & Gutter)	LF	3750	\$15.00	\$56,250.00
8	Remove Concrete Sidewalk	SQFT	6400	\$4.50	\$28,800.00
9	Reconstruct Inlet	EA	4	\$3,500.00	\$14,000.00
10	Reset Mailbox	EA	21	\$300.00	\$6,300.00
11	Relocate Chain Link Fence	LF	400	\$50.00	\$20,000.00
12	Relocate Iron Fence	LF	340	\$90.00	\$30,600.00
13	Relocate Wooden Fence	LF	80	\$40.00	\$3,200.00
14	Relocate Brick Posts and Iron Fence	LF	80	\$150.00	\$12,000.00
15	Relocate Water Valve	EA	1	\$1,400.00	\$1,400.00
16	Relocate Light Pole	EA	1	\$1,500.00	\$1,500.00
17	Relocate Irrigation Structure	LS	1	\$20,000.00	\$20,000.00
18	Relocate Fire Hydrant	EA	2	\$6,000.00	\$12,000.00
19	Adjust Inlet	EA	1	\$3,000.00	\$3,000.00
20	Adjust Manhole to Grade	EA	5	\$1,200.00	\$6,000.00
21	Adjust Valve Frame and Cover to Grade	EA	9	\$1,200.00	\$10,800.00
22	Adjust Water Meter Box to Grade	EA	11	\$800.00	\$8,800.00
23	Adjust Pullbox to Grade	EA	3	\$850.00	\$2,550.00
24	Adjust Fire Hydrant to Grade	EA	4	\$5,500.00	\$22,000.00
25	Clearing and Grubbing	LS	1	\$11,000.00	\$11,000.00
26	Remove Tree	EA	6	\$1,400.00	\$8,400.00
27	Roadway Excavation (F)	CY	720	\$60.00	\$43,200.00
28	Erosion Control	LS	1	\$5,000.00	\$5,000.00
29	Class 2 Aggregate Base (F)	CY	600	\$60.00	\$36,000.00
30	Hot Mix Asphalt (Type B)	TON	480	\$110.00	\$52,800.00
31	Roadside Sign - One Post	EA	24	\$250.00	\$6,000.00
32	Storm Drain System	LS	1	\$75,000.00	\$75,000.00
33	Minor Concrete (Curb)	LF	190	\$22.00	\$4,180.00
34	Minor Concrete (Gutter)	SQFT	590	\$10.00	\$5,900.00
35	Detectable Warning Surface	SQFT	155	\$45.00	\$6,975.00
36	Minor Concrete (Curb and Gutter)	LF	4390	\$25.00	\$109,750.00
37	Minor Concrete (Driveway & Curb Ramps)	SQFT	4970	\$15.00	\$74,550.00
38	Minor Concrete (Sidewalk)	SQFT	31270	\$7.00	\$218,890.00
39	Thermoplastic Traffic Stripe	LF	4830	\$1.25	\$6,037.50
40	Thermoplastic Pavement Marking	SQFT	496	\$6.00	\$2,976.00
41	Mobilization	LS	1	\$92,800.00	\$92,800.00
42	Minor/ Supplemental Items	%	0	\$977,514.10	\$244,378.53
Subtotal (Construction Costs)					\$1,314,692.63
Construction Contingency				25%	\$ 328,673.16
Total Construction Costs					\$1,643,365.78
Total Construction Budget (Rounded)					\$1,643,400.00

Non-Construction Related Costs:					
No.	Item Description	Units	Quantity	Unit Cost	Total
42	Environmental Clearance	% of CON	5%	\$1,162,000.00	\$58,100.00
43	Right of Way/TCE Acquisition (Capital)	SQFT	1630	\$5.00	\$8,150.00
44	Right of Way/TCE Acquisition (Support)	Parcel	2	\$5,000.00	\$10,000.00
45	Final Engineering Design	% of CON	15%	\$1,162,000.00	\$174,300.00
46	Construction Support	% of CON	2%	\$1,162,000.00	\$23,240.00
47	Construction Management	% of CON	15%	\$1,162,000.00	\$174,300.00
48	Utility Relocations	LS	1	\$100,000.00	\$100,000.00
Total Non-Construction Related Costs					\$548,090.00
Total Project Capital Cost					\$ 2,000,590.00
Rounded					\$ 2,000,000.00

Appendix H - Orosi Submittal Plan Set

OROSI
COUNTY OF TULARE

PROJECT PLANS FOR CONSTRUCTION ON
OROSI-COMPLETE STREETS POLICY
IN OROSI AND COUNTY OF TULARE
AVE 416 (FROM SR 63 TO RD 130)

INDEX OF SHEETS

PAGE	SHEET NO.	DESCRIPTION
1	T1	TITLE SHEET
2-3	X1-X2	TYPICAL SECTIONS
4-12	L1-L9	LAYOUT

ABBREVIATIONS

AB	AGGREGATE BASE
AC	ASPHALT CONCRETE
A.D.	ALGEBRAIC DIFFERENCE
AP	ANGLE POINT
BC	BEGIN CURVE
CL	CENTER LINE
CONC	CONCRETE
CPDC	CONSOLIDATED PEOPLES DITCH COMPANY
C&G	CURB AND GUTTER
EC	END OF CURVE
EG	EXISTING GRADE
ELEV	ELEVATION
EP	EDGE OF PAVEMENT
ESMT	EASEMENT
FG	FINISH GRADE
FL	FLOW LINE
HCR	HANDICAP RAMP
HMA	HOT MIX ASPHALT
HW	HEADWALL
INV	INVERT
IRR	IRRIGATION
JS	JUNCTION STRUCTURE
K	CURVE COEFFICIENT
LF	LINEAR FEET
LT	LEFT
LVC	LENGTH OF VERTICAL CURVE
MAX	MAXIMUM
MIN	MINIMUM
(N)	NOT A PAY ITEM
NSP	NEW STANDARD PLAN
N.T.S.	NOT TO SCALE
O.C.	ON CENTER
PCC	PORTLAND CEMENT CONCRETE
PI	POINT OF INTERSECTION
POC	POINT OF CONNECTION
PT	POINT
R	RADIUS
R.C.	RELATIVE COMPACTION
RGRCP	RUBBER GASKET REINFORCED CONCRETE PIPE
RSP	REVISED STANDARD PLAN
RT	RIGHT
R/W	RIGHT OF WAY
SCE	SOUTHERN CALIFORNIA EDISON
STA	STATION
SW	SIDEWALK
TBC	TOP BACK OF CURB
TBW	TOP BACK OF WALK
TW	TOP OF WALL
TYP	TYPICAL



VICINITY MAP
N.T.S.



LOCATION MAP
N.T.S.



Contractor shall call
Underground Service Alert at
811 two working days prior
to excavation

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



TITLE SHEET
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	N.T.S.
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015T005.DWG
CHECKED	MJW
DATE	12-1-15

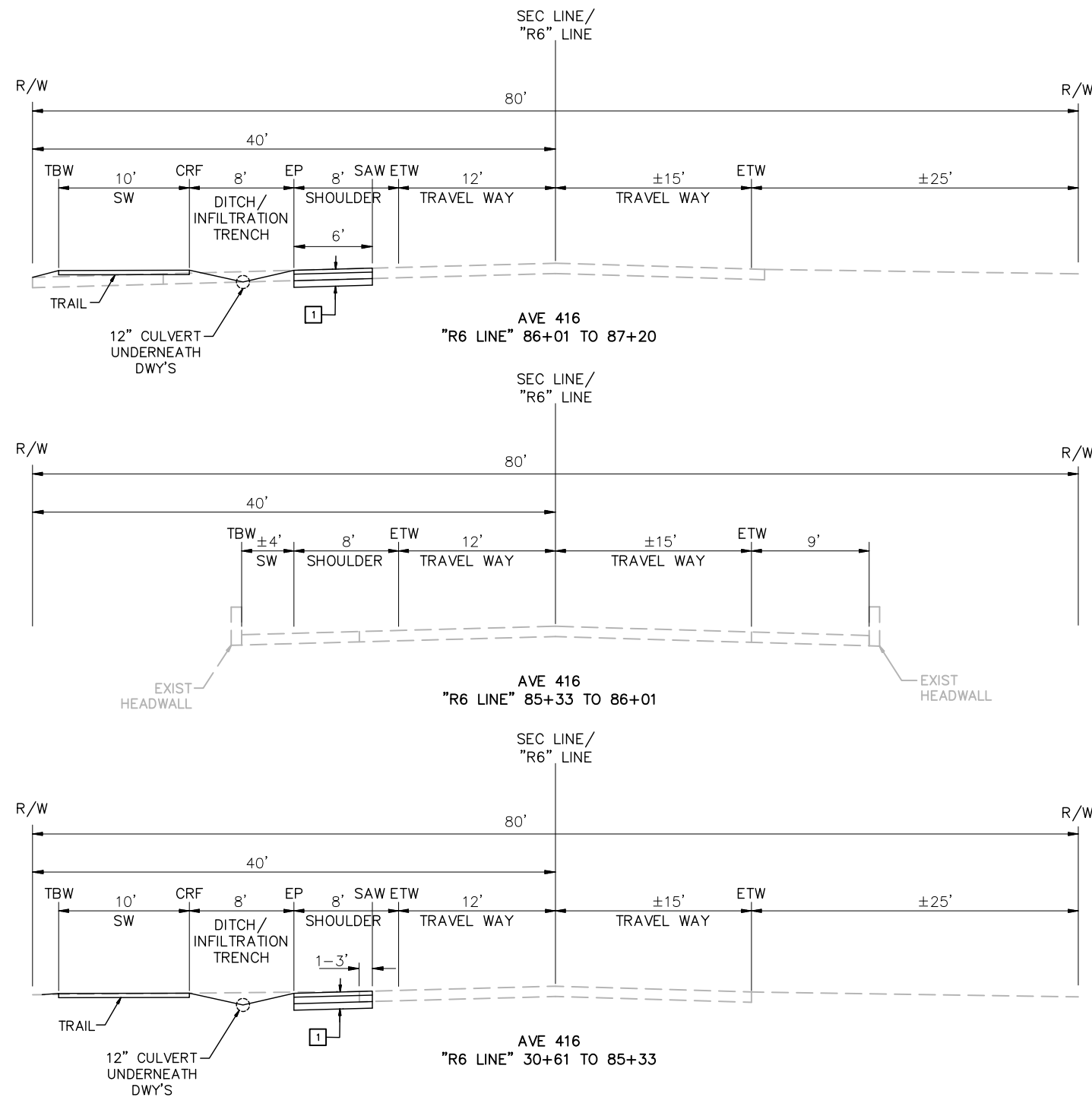
SHEET No.
T1

1 OF 12

REVISIONS

No.	DESCRIPTION	DATE	BY

<input checked="" type="checkbox"/> PRELIMINARY	<input type="checkbox"/> APPROVED
<input type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> RECORD	



TYPICAL STRUCTURAL SECTIONS:

- 1 20-YEAR DESIGN
TI=5.5, R=25
0.25' HMA (TYPE A)
0.70' AB (CLASS 2)
0.50' COMPACTED NATIVE



TYPICAL SECTIONS COMPLETE STREETS IMPROVEMENTS PHASE 1 - RD 140 TO RD 130 OROSI, CALIFORNIA

SCALE	1" = 5'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015X005.DWG
CHECKED	MJW
DATE	12-21-15

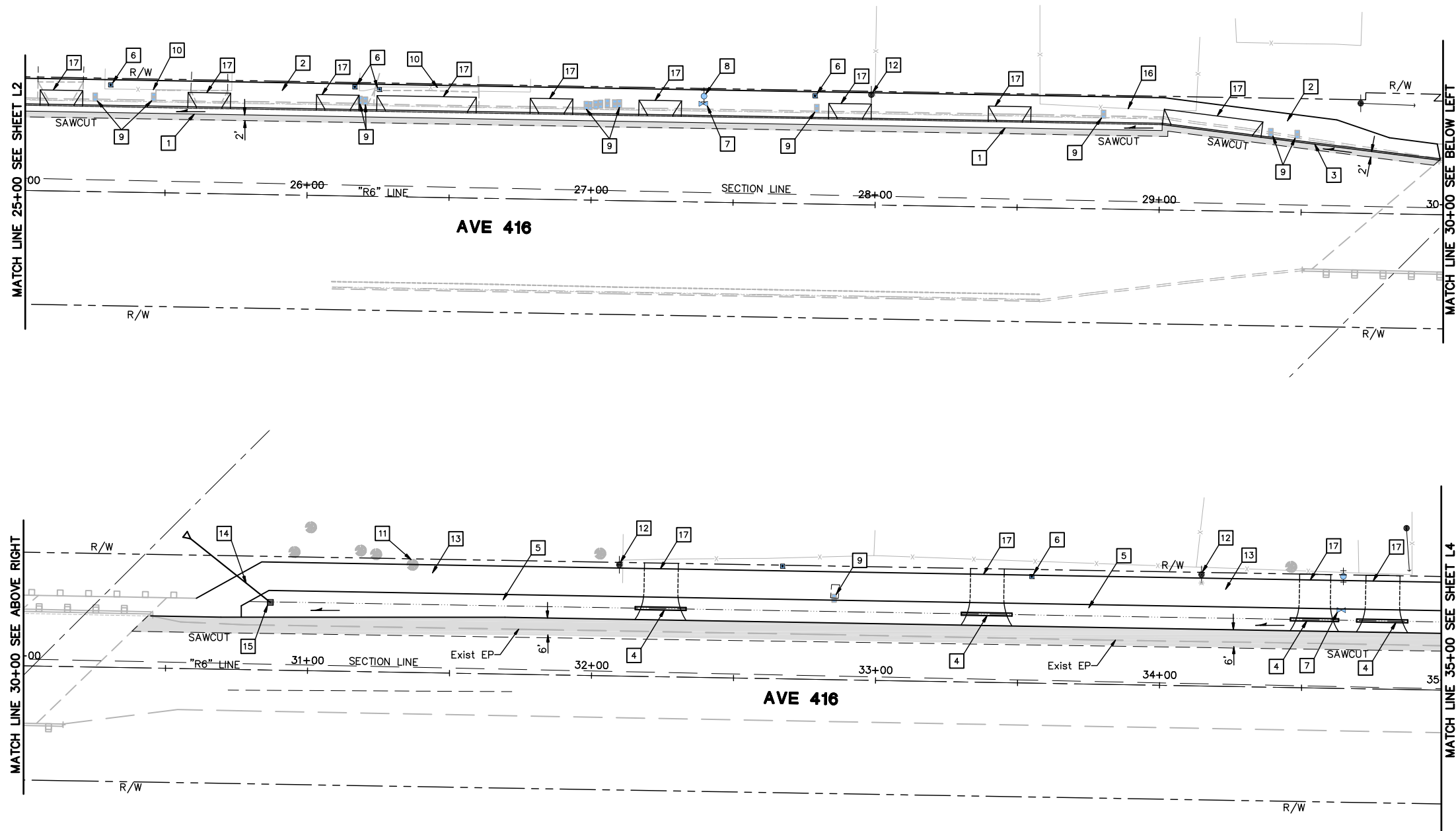
SHEET No.

X2

3 OF 12

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION





KEYED NOTES (THIS SHEET ONLY)

- | | |
|--------------------------------------|---|
| 1 C&G. | 10 RELOCATE FENCE (BRICK AND IRON). |
| 2 SIDEWALK. | 11 REMOVE TREE. |
| 3 HMA DIKE. | 12 Exist UTILITY POLE. PROTECT IN PLACE. |
| 4 12 IN. CULVERT. | 13 CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB). |
| 5 DITCH/INFILTRATION TRENCH. | 14 18 IN. CULVERT. |
| 6 ADJUST WATER METER BOX TO GRADE. | 15 DRAIN INLET. |
| 7 ADJUST WATER VALVE COVER TO GRADE. | 16 RELOCATE FENCE (CHAIN LINK). |
| 8 ADJUST FIRE HYDRANT TO GRADE. | 17 DRIVEWAY. |
| 9 RESET MAILBOX. | |

LEGEND

- ROADWAY CONST.
- SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
DATE	BY

<input checked="" type="checkbox"/> PRELIMINARY	<input type="checkbox"/> APPROVED
<input type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> RECORD	

omni • means

ENGINEERS PLANNERS

With offices in:
VISALIA
300 E. Center Ave.
Visalia, CA 93291
(559) 734-5895

With offices in:
SACRAMENTO
SACRAMENTO CREEK
REDDING

LAYOUT

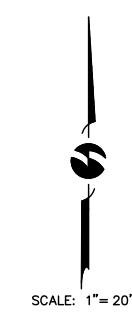
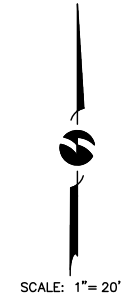
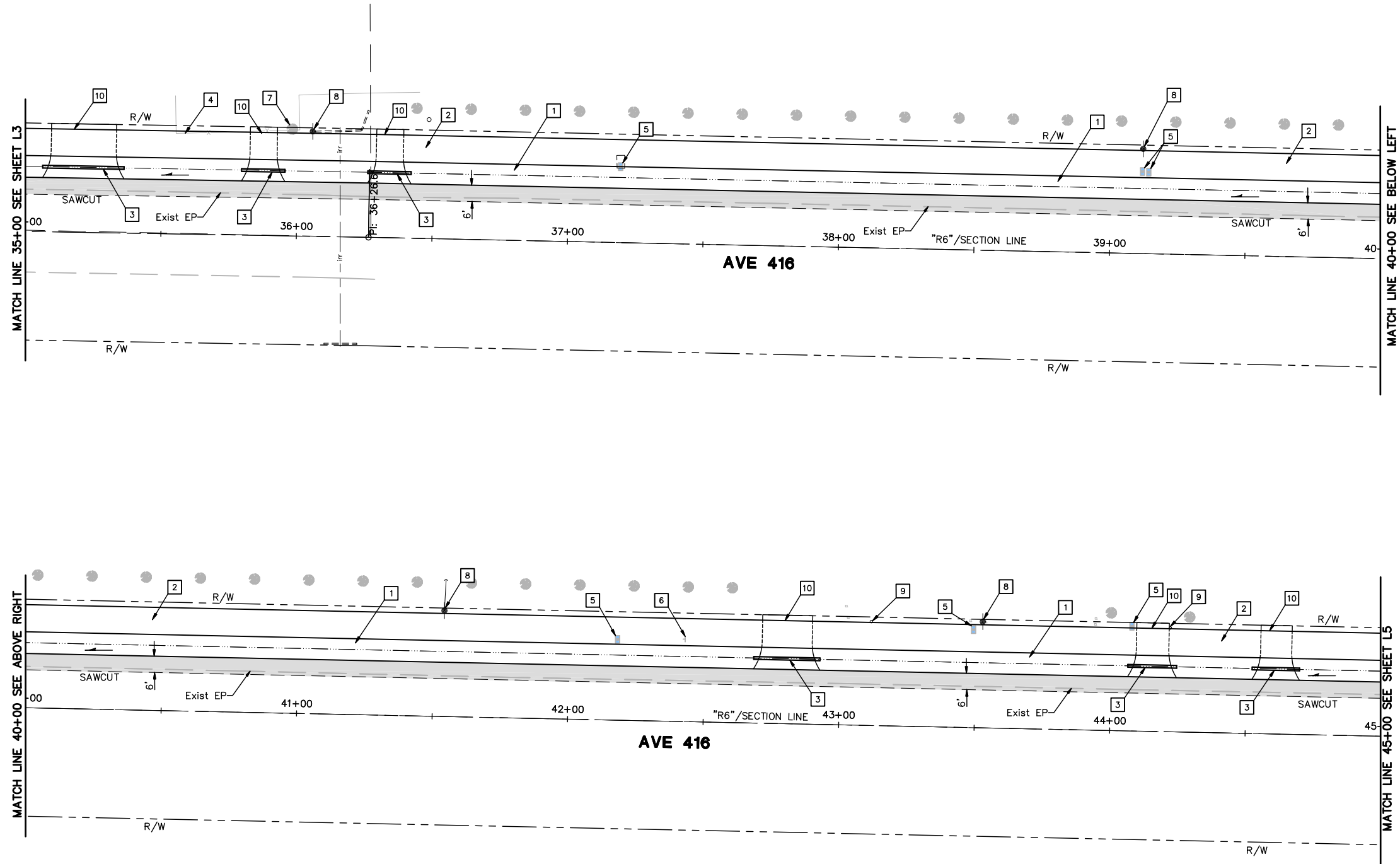
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.

L3

6 OF 12



KEYED NOTES (THIS SHEET ONLY)

- | | |
|--|---|
| 1 DITCH/INFILTRATION TRENCH. | 6 RESET SIGN. |
| 2 CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB). | 7 REMOVE TREE. |
| 3 12 IN. CULVERT. | 8 Exist UTILITY POLE. PROTECT IN PLACE. |
| 4 RELOCATE FENCE (WOODEN). | 9 Exist POST. PROTECT IN PLACE. |
| 5 RESET MAILBOX. | 10 DRIVEWAY. |

LEGEND

- ROADWAY CONST.
- SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
NO.	DESCRIPTION

<input checked="" type="checkbox"/> PRELIMINARY	<input type="checkbox"/> APPROVED
<input type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> RECORD	

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ENGINEERS PLANNERS

With offices in:
SACRAMENTO
VANDERGRAD
REDDING

100 E. Center Ave.
Vandegr., CA 95971
(530) 734-5895

LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

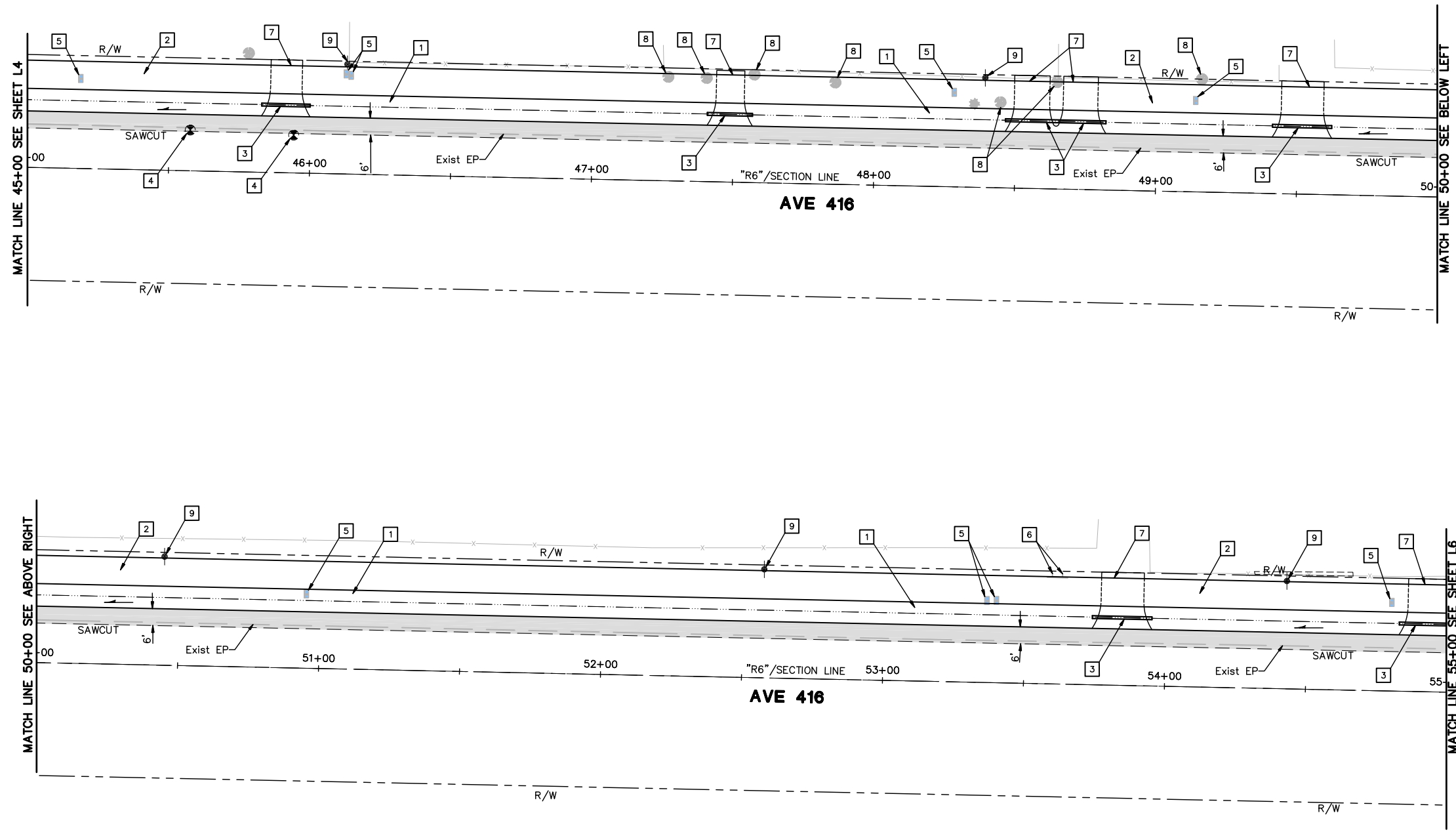
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DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.

L4

7 OF 12

FILED: 12/01/15

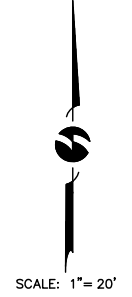


KEYED NOTES (THIS SHEET ONLY)

- | | |
|--|---|
| 1 DITCH/INFILTRATION TRENCH. | 6 RESET SIGN. |
| 2 CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB). | 7 DRIVEWAY. |
| 3 12 IN. CULVERT. | 8 REMOVE TREE. |
| 4 ADJUST GAS VALVE COVER TO GRADE. | 9 Exist UTILITY POLE. PROTECT IN PLACE. |
| 5 RESET MAILBOX. | |

LEGEND

- ROADWAY CONST.
 SLOPE ARROW. INDICATES DIRECTION OF FLOW.



REVISIONS	
NO.	DESCRIPTION

<input checked="" type="checkbox"/> PRELIMINARY	<input type="checkbox"/> APPROVED
<input type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> RECORD	

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 ENGINEERS PLANNERS
 With offices in:
 VISALIA Center Ave.
 500 E. Center Ave.
 VISALIA, CA 93291
 (559) 734-5865

LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

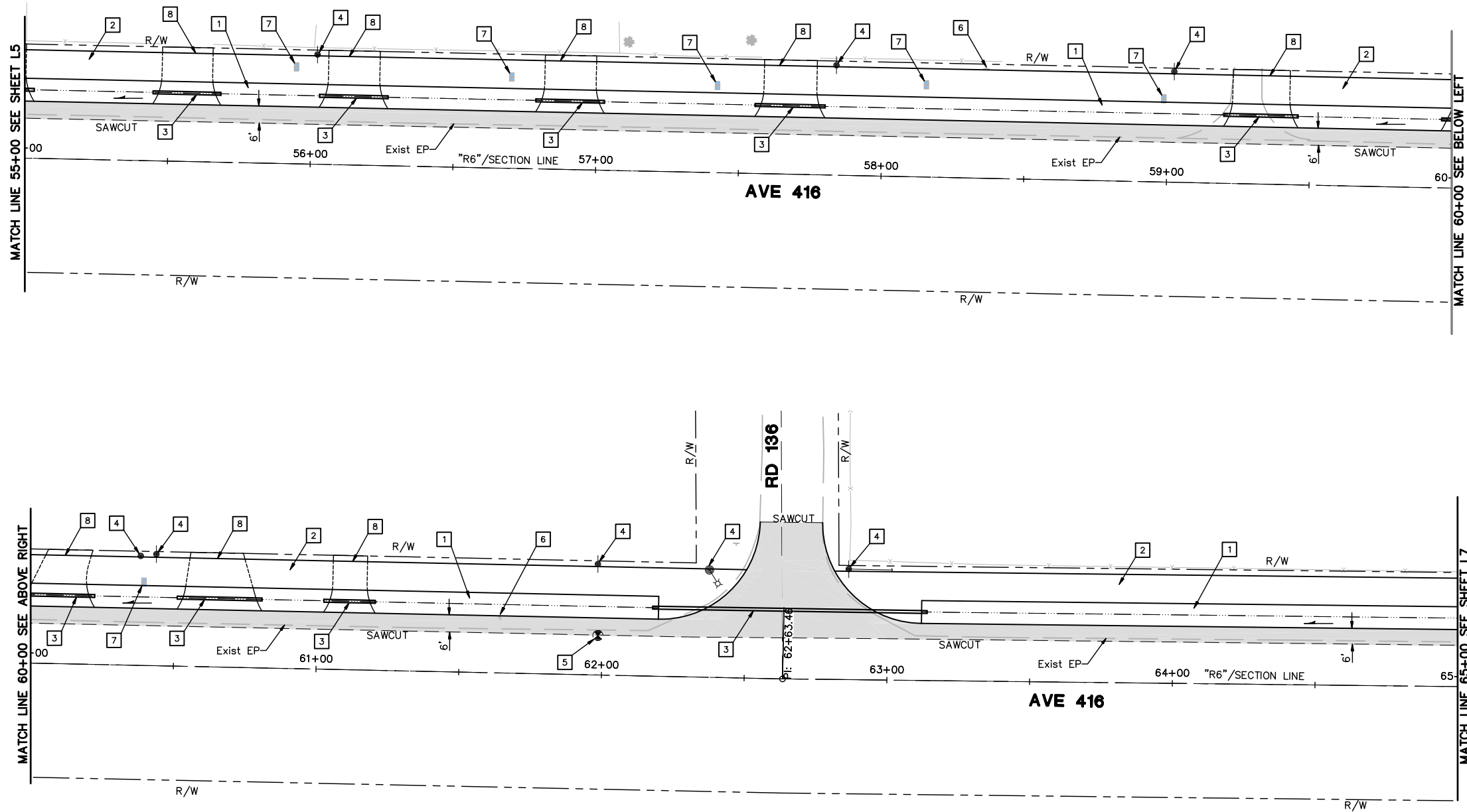
30% SUBMITTAL
 PRELIMINARY, NOT
 FOR CONSTRUCTION



SHEET No.

L5

8 OF **12**



KEYED NOTES (THIS SHEET ONLY)

- 1 DITCH/INFILTRATION TRENCH.
- 2 CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
- 3 12 IN. CULVERT.
- 4 Exist UTILITY POLE. PROTECT IN PLACE.
- 5 ADJUST GAS VALVE TO GRADE.
- 6 RESET SIGN.
- 7 RESET MAILBOX.
- 8 DRIVEWAY.

LEGEND

- ROADWAY CONST.
- SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



LAYOUT
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.

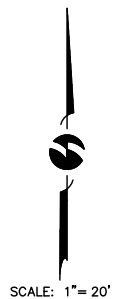
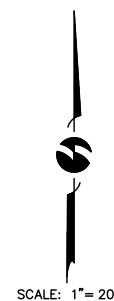
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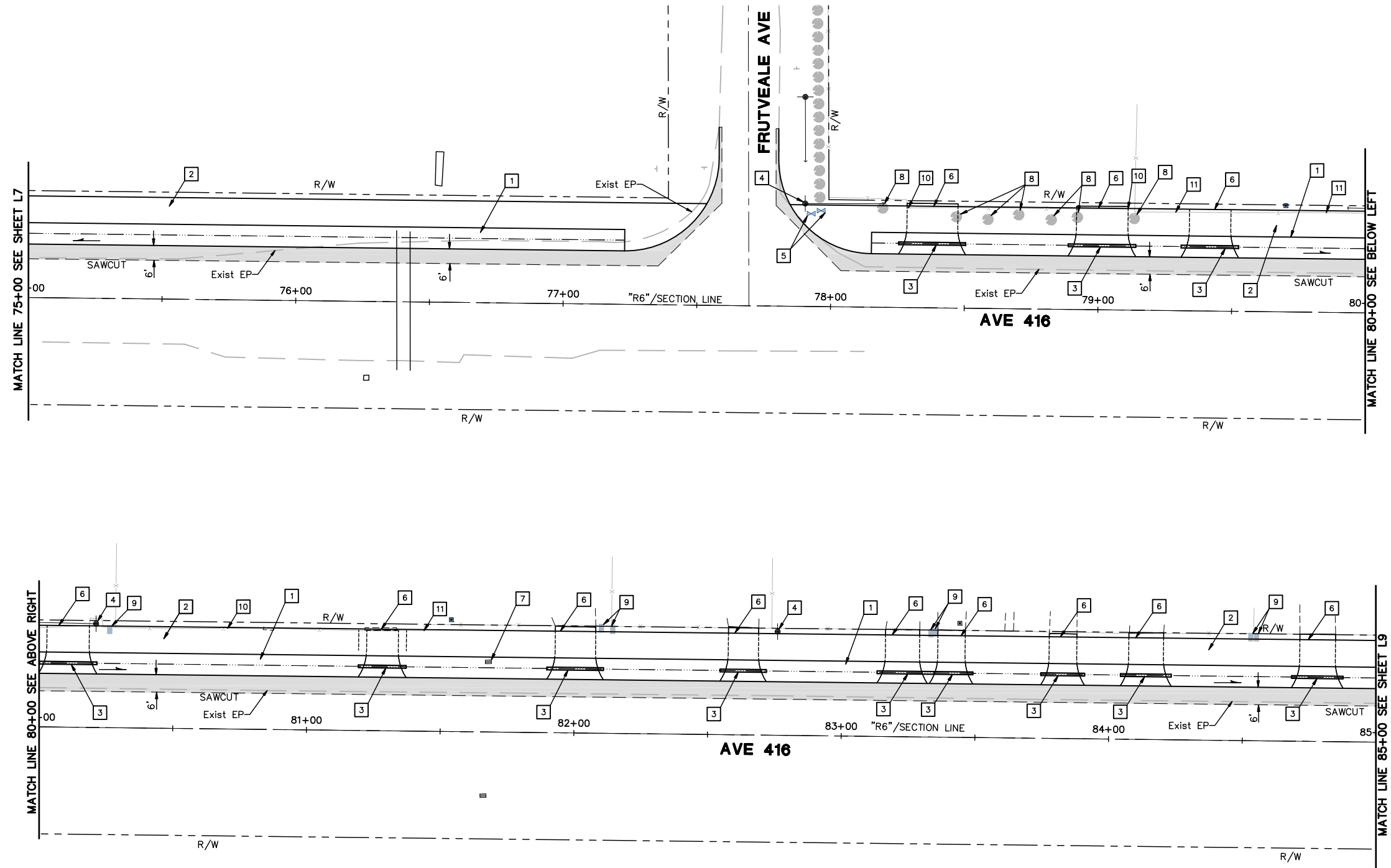
9 OF 12

REVISIONS

No.	DESCRIPTION	DATE	BY

<input checked="" type="checkbox"/> PRELIMINARY	<input type="checkbox"/> APPROVED
<input type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> RECORD	





KEYED NOTES (THIS SHEET ONLY)

- | | |
|--|---------------------------------|
| 1 DITCH/INFILTRATIN TRENCH. | 7 REMOVE DRAIN INLET. |
| 2 CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB). | 8 REMOVE TREE. |
| 3 12 IN. CULVERT. | 9 RESET MAILBOX. |
| 4 Exist UTILITY POLE. PROTECT IN PLACE. | 10 RELOCATE FENCE (IRON). |
| 5 ADJUST WATER VALVE COVER TO GRADE. | 11 RELOCATE FENCE (CHAIN LINK). |
| 6 DRIVEWAY. | |

LEGEND

- ROADWAY CONST.
- SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
DATE	BY

<input checked="" type="checkbox"/> PRELIMINARY	<input type="checkbox"/> APPROVED
<input type="checkbox"/> BID	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> RECORD	

ENGINEERS PLANNERS
With offices in:
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300 E. Center Ave.
Visalia, CA 93291
(559) 734-5895

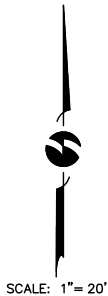
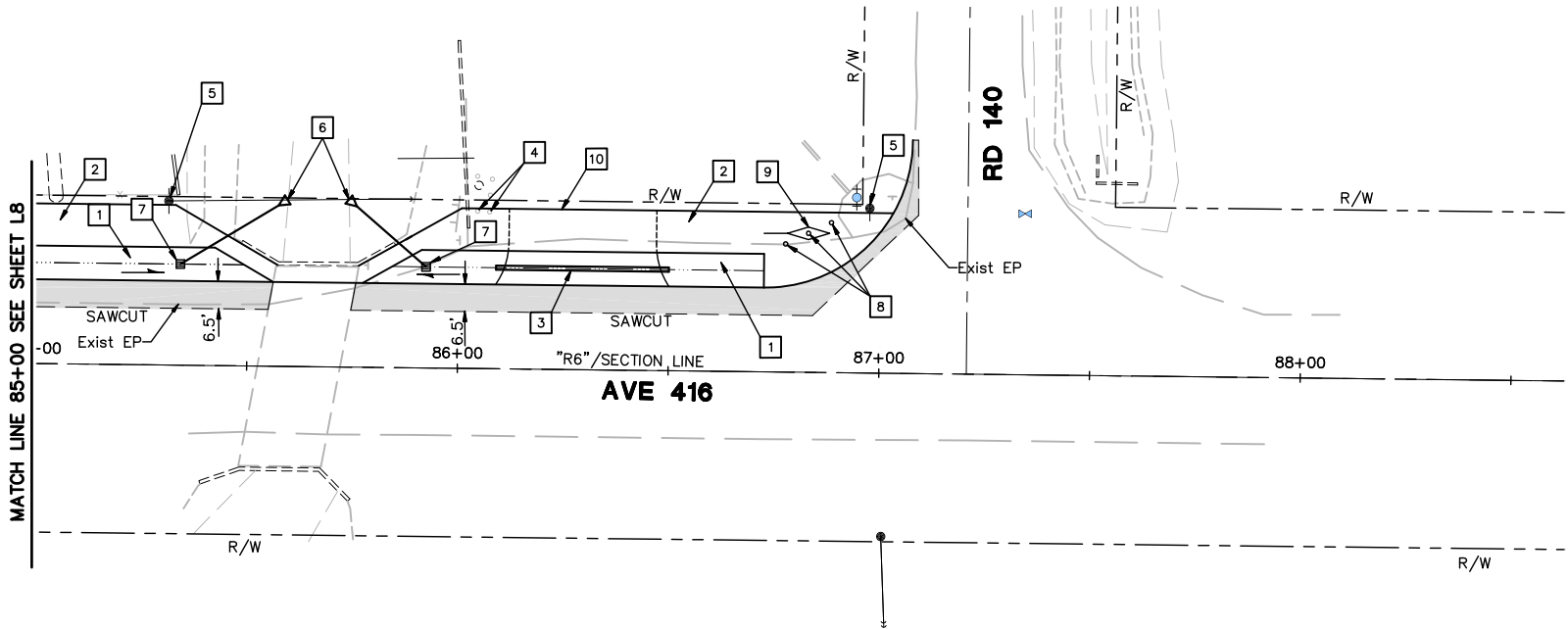
LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L8

11 OF 12



KEYED NOTES (THIS SHEET ONLY)

- 1 DITCH/INFILTRATION TRENCH.
- 2 CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
- 3 12 IN. CULVERT.
- 4 REMOVE BOLLARD.
- 5 Exist UTILITY POLE. PROTECT IN PLACE.
- 6 18 IN. CULVERT.
- 7 DRAIN INLET.
- 8 REMOVABLE BOLLARD POST WITH TYPE 3 MARKER.
- 9 ENVELOPE STRIPPING NORMAL SOLID YELLOW LINE (MUTCD FIG.9C-8).
- 10 DRIVEWAY.

LEGEND

- ROADWAY CONST.
- SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.

L9

12 OF 12

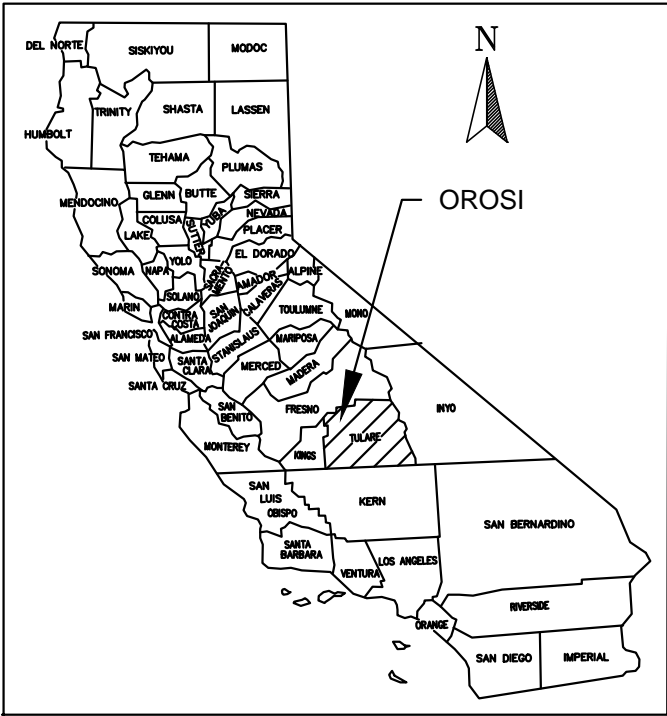
REVISIONS

No.	DESCRIPTION	DATE	BY

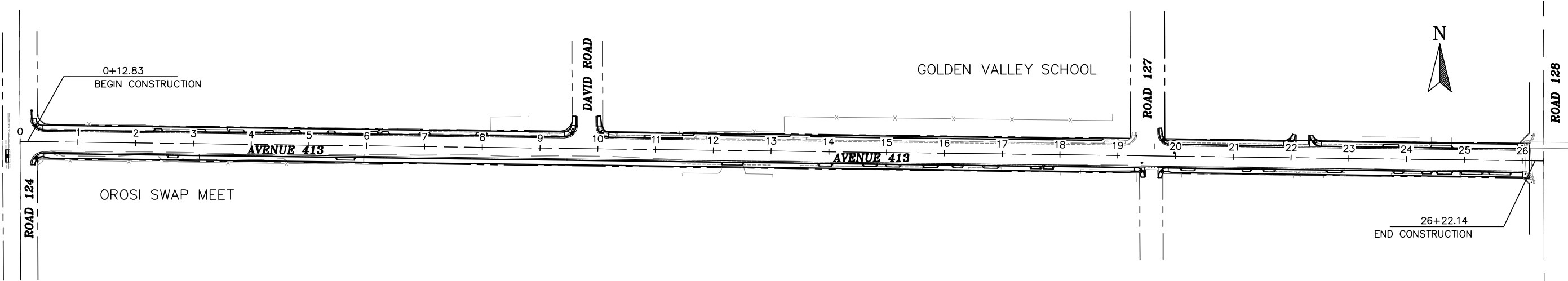
- ☒ PRELIMINARY
- ☐ APPROVED
- ☐ BID
- ☐ CONSTRUCTION
- ☐ RECORD

INDEX OF SHEETS table with 3 columns: PAGE, SHEET NO., DESCRIPTION

STATE OF CALIFORNIA
COUNTY OF TULARE
PROJECT PLANS FOR CONSTRUCTION OF
OROSI-COMPLETE STREETS POLICY
IN OROSI AND COUNTY OF TULARE
AVENUE 413 (FROM ROAD 124 TO STATE ROUTE 63)



LOCATION MAP
N.T.S.



VICINITY MAP
N.T.S.

811 logo and text: Know what's below. Call before you dig. Contractor shall call Underground Service Alert at 811 two working days prior to excavation

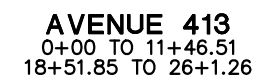
30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

APPROVED BY: JABED KHAN, P.E. TULARE COUNTY RESOURCE MANAGEMENT AGENCY
APPROVED BY: MICHAEL J. WINTON P.E. PROJECT MANAGER OMNI-MEANS LTD.



Revisions table, Tulare County Resource Management Agency contact info, omni-means logo, project title: AVENUE 413 - OROSI TULARE COUNTY COMPLETE STREETS, and sheet info: T1, 1 of 9

1 20-YEAR DESIGN
 TI=5.5, R=25 (ASSUMED)
 0.25' HMA (TYPE B)
 0.70' AB (CLASS 2) (95% RC)
 1.00' COMPACTED NATIVE (95% RC)



A circular professional engineer seal for Michael J. Winton, State of California, No. 66910, Civil. The seal features the text "REGISTERED PROFESSIONAL ENGINEER" around the top inner edge and "STATE OF CALIFORNIA" around the bottom inner edge. The name "MICHAEL J. WINTON" is inscribed in the center. Below the name, the number "No. 66910" is printed, followed by the word "CIVIL" in a larger font.

[illegible]

**TULARE COUNTY
RESOURCE MANAGEMENT
AGENCY**

5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
WWW.TULARECOUNTY.CA.GOV/RMA



omni • means
ENGINEERS PLANNERS

With offices in:
 VISALIA
 200 E. Center Ave.
 Suite A
 Visalia, CA 93291
 (559) 734-5695

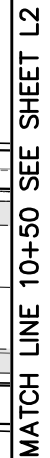
SAN FRANCISCO
 WALNUT CREEK
 REDDING

TYPICAL SECTIONS
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

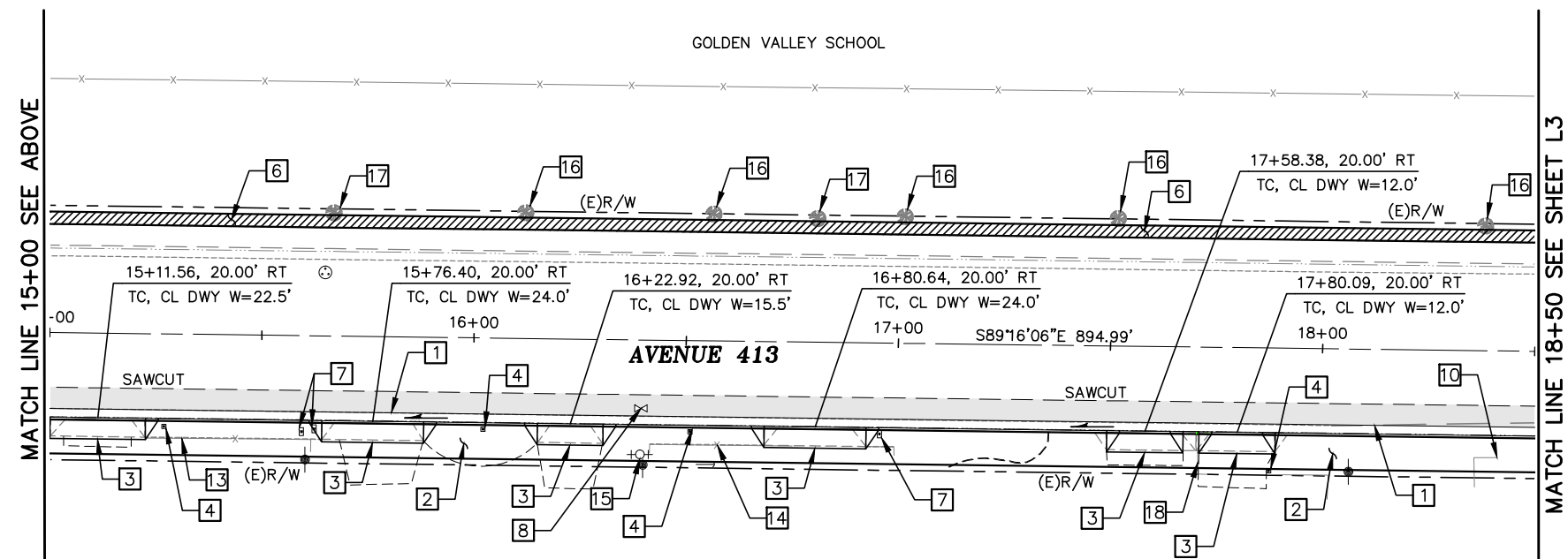
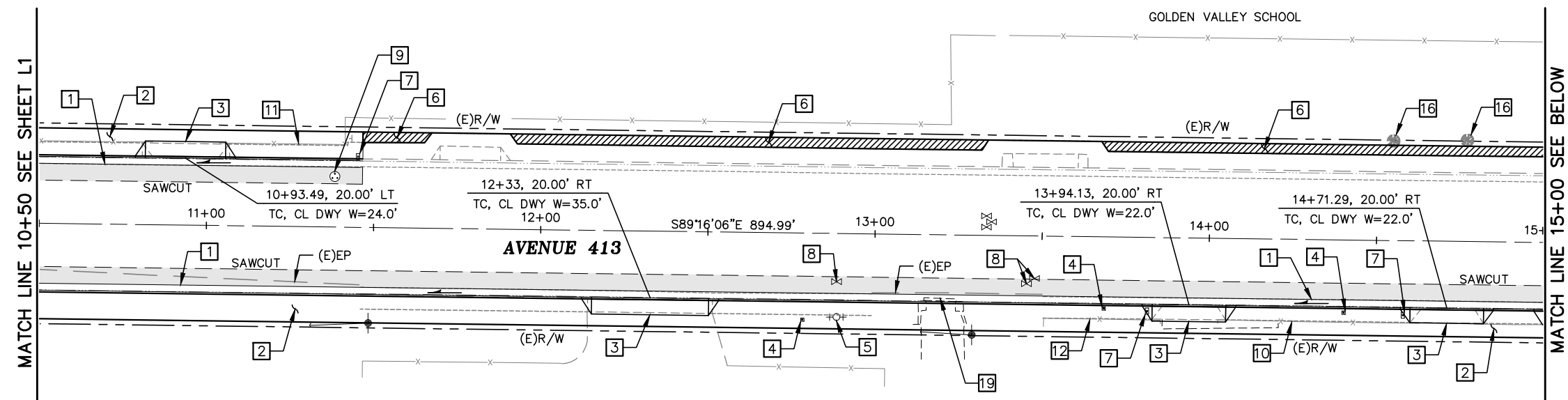
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VISION	RMA
PB NO.	55-7300-14
SIGNED	JAV
DRAWN	JAV
CHECKED	SMH
FILE	2144X003.DWG
DATE	6-9-16

X1

2 OF 9



3 OF 9

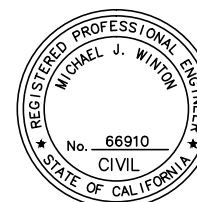


CONSTRUCTION NOTES (THIS SHEET ONLY):

LEGEND:



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[illegible]

**TULARE COUNTY
RESOURCE MANAGEMENT
AGENCY**
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
WWW.TULARECOUNTY.CA.GOV/RMA



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ENGINEERS PLANNERS

With offices in:

MSA/IA
200 E. Center Ave.
Suite 200
Vasilla, CA 93291
(559) 734-5895

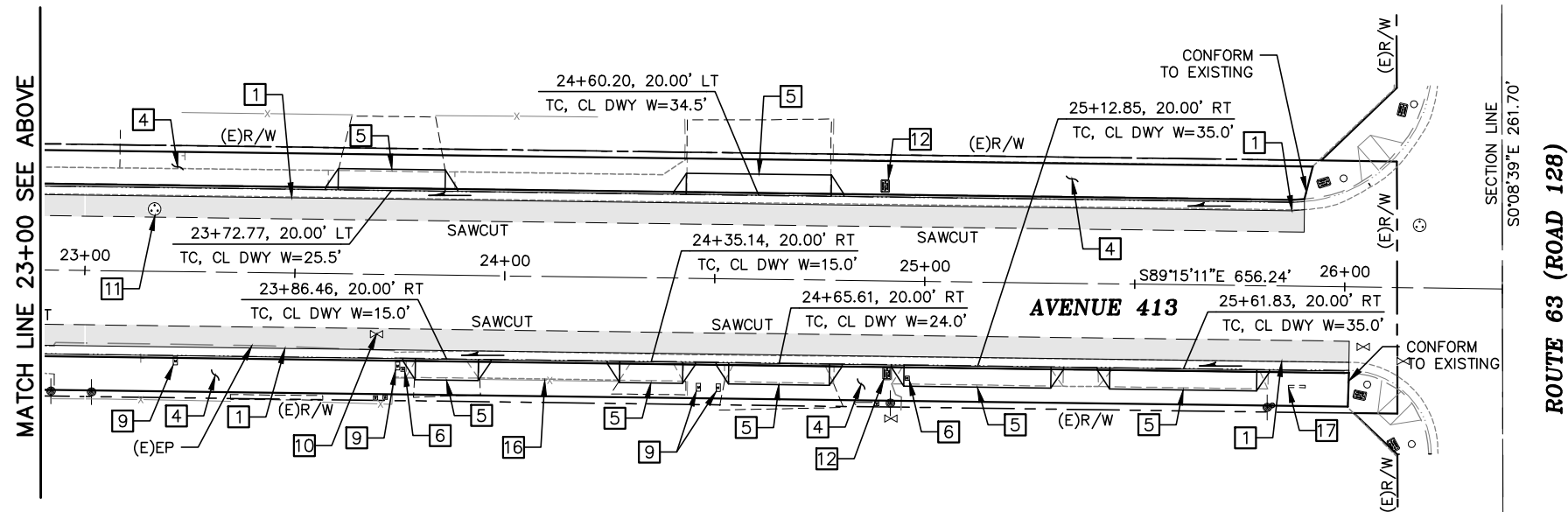
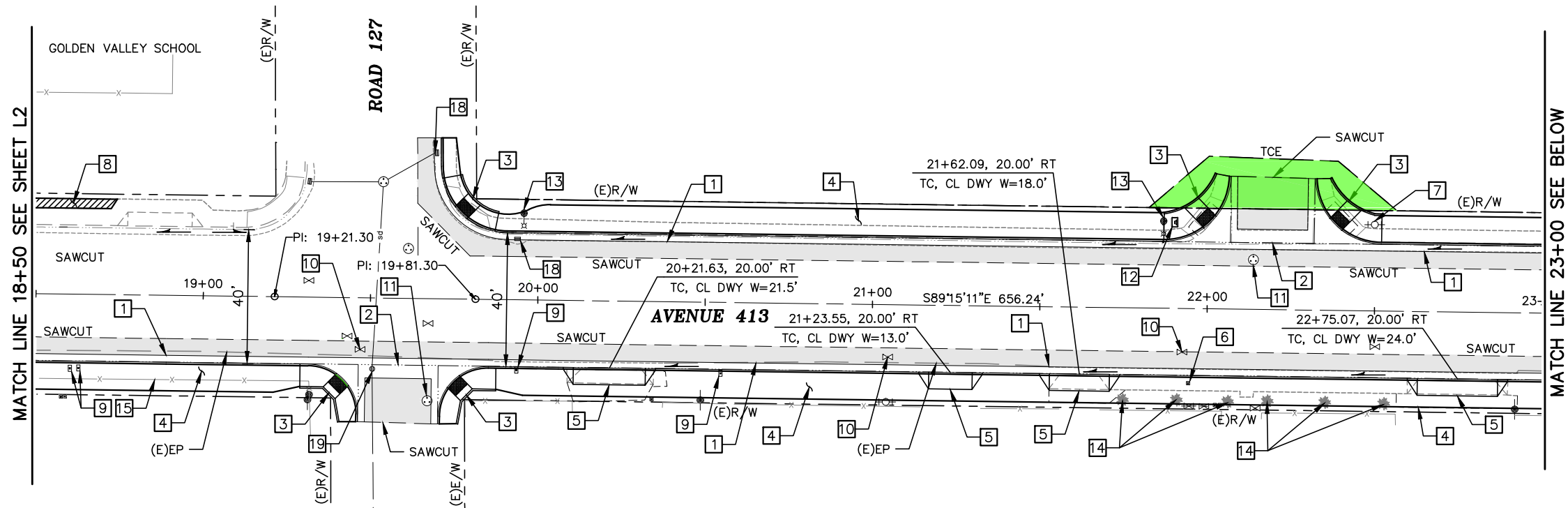
SAN FRANCISCO
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

LAYOUT
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JLR
CHECKED	SMH
FILE	2144.L003.DWG
DATE	6-9-16
SHEET No.	

L2

4 OF 9



CONSTRUCTION NOTES (THIS SHEET ONLY):

- | | |
|---|--|
| 1 TULARE COUNTY STANDARD BARRIER TYPE CURB AND GUTTER PER PLATE NO. A-19. SEE SHEET C1. | 11 ADJUST MANHOLE TO GRADE. |
| 2 CONSTRUCT CONTINUOUS GUTTER CURB RETURN PER DETAIL ON SHEET C1. | 12 ADJUST PULLBOX TO GRADE. |
| 3 CASE C RAMP (COUNTY STD). SEE SHEET C1. | 13 EXIST LIGHT POLE TO REMAIN, PROTECT IN PLACE. |
| 4 SIDEWALK. | 14 REMOVE TREES/SHRUBS. |
| 5 TULARE COUNTY STANDARD URBAN DRIVEWAY. SEE SHEET C1. | 15 RELOCATE FENCE (CHAIN LINK). |
| 6 ADJUST WATER METER BOX TO GRADE. | 16 RELOCATE FENCE (IRON). |
| 7 ADJUST FIRE HYDRANT TO GRADE. | 17 REMOVE CONCRETE WHEEL STOP. |
| 8 WIDEN SIDEWALK 3'. | 18 RECONSTRUCT INLET. |
| 9 RESET MAILBOX. | 19 ADJUST INLET TO GRADE. |
| 10 ADJUST WATER VALVE FRAME AND COVER TO GRADE. | |

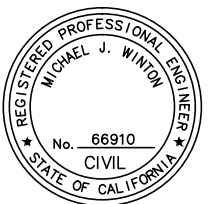
LEGEND:

- ROADWAY CONSTRUCTION.
- 3' SIDEWALK WIDENING.
- TEMPORARY CONSTRUCTION EASEMENT (TCE).
- SLOPE ARROW, INDICATES DIRECTION OF FLOW.



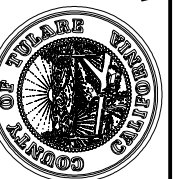
Contractor shall call Underground Service Alert at 811 two working days prior to excavation

**30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION**



REVISIONS	DATE	BY

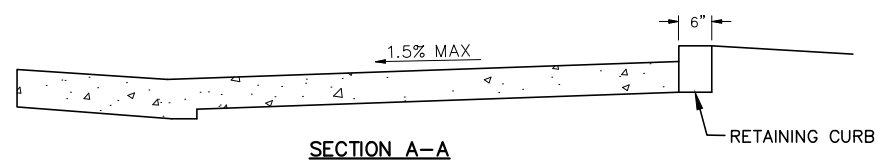
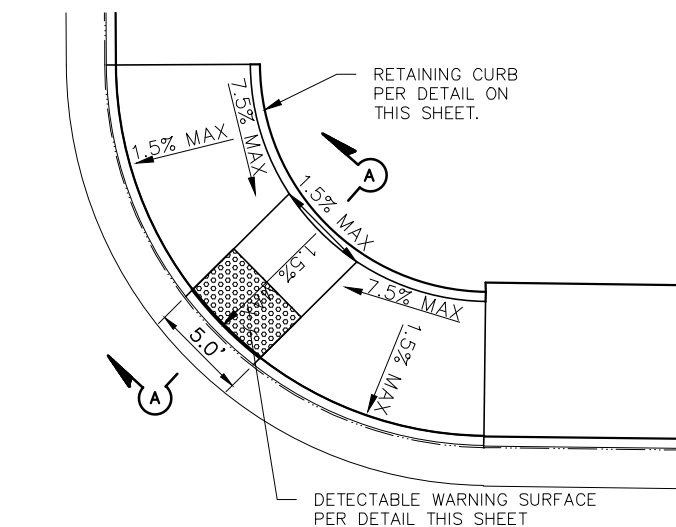
**TULARE COUNTY
RESOURCE MANAGEMENT
AGENCY**
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 824-7000
WWW.TULARECOUNTY.CA.GOV/RMA



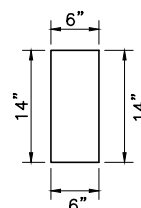
omni means
ENGINEERS PLANNERS
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5885

LAYOUT
AVENUE 413 - OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JLR
CHECKED	SMH
FILE	2144L003.DWG
DATE	6-9-16
SHEET No.	L3

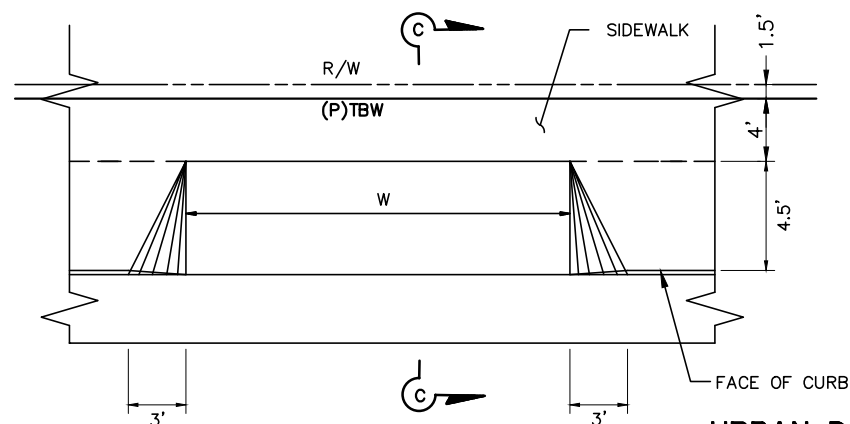


CASE C CURB RAMP DETAIL
N.T.S.

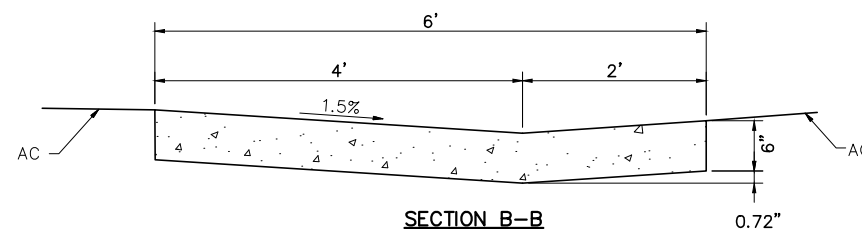
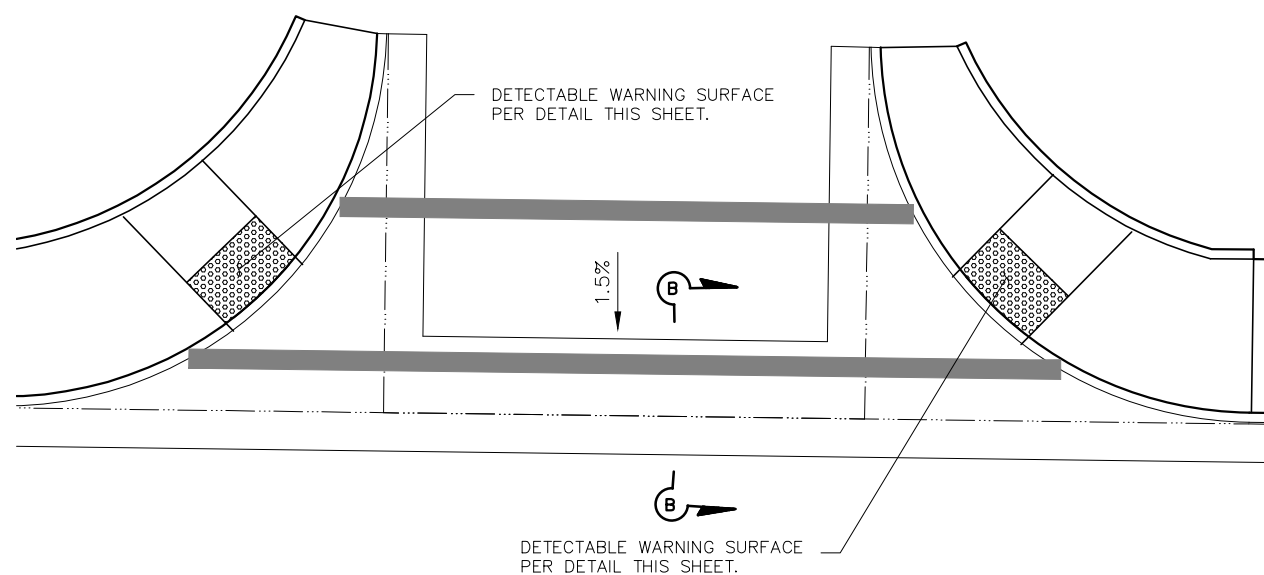


- NOTES:
1. ALL CONCRETE SHALL BE MINOR CONCRETE.

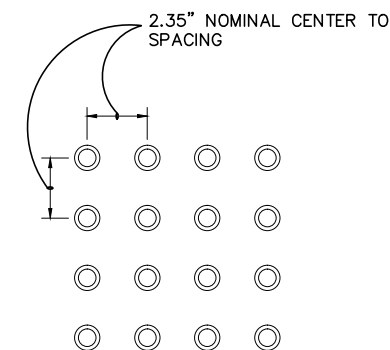
RETAINING CURB DETAIL
SCALE: 1"=1'-0"



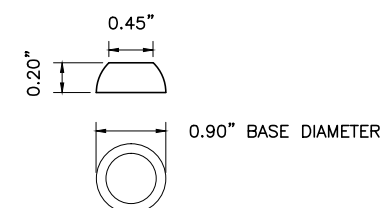
URBAN DRIVEWAY DETAIL
N.T.S.



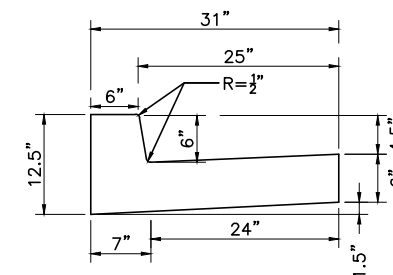
CONTINUOUS GUTTER CURB RETURN DETAIL
N.T.S.



RAISED TRUNCATED DOME PATTERN



**RAISED TRUNCATED DOME
DETECTABLE WARNING SURFACE**
N.T.S.

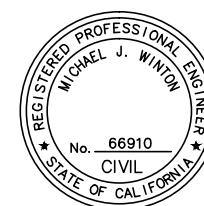


- NOTES:
1. ALL CONCRETE SHALL BE MINOR CONCRETE.
 2. BARRIER TYPE CURB AND GUTTER SHALL HAVE A MINIMUM OF 0.15 FEET PER 100 FEET.
 3. AREA BETWEEN BACK OF CURB AND AND PROPERTY LINE SHALL BE BACK FILLED AND SLOPED TO DRAIN TO GUTTER.

CURB & GUTTER - COUNTY
SCALE: 1"=1'-0"

- NOTES:
1. SHALL BE INSTALLED AT THE BOTTOM OF ALL CURB RAMPS.
 2. SHALL BE THE FULL WIDTH OF RAMP AND SHALL BE A MIN. OF 36" IN DEPTH
 3. SHALL BE PREMIXED FEDERAL YELLOW COLORED POLYMER CONCRETE MATERIAL.
 4. ALL DETECTABLE WARNING PANELS INSTALLED WITH NEW IMPROVEMENTS SHALL BE WET SET TYPE/CAST IN PLACE TYPE PANELS.
 5. A 4'-0" WIDE DETECTABLE WARNING SURFACE MAY BE USED ON A 4'-2" WIDE CURB RAMP.
 6. DETECTABLE WARNING SURFACES SHALL CONFORM TO THE REQUIREMENTS IN THE STANDARDS SPECIFICATIONS.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	DATE	BY
No.		
DESCRIPTION		

**TULARE COUNTY
RESOURCE MANAGEMENT
AGENCY**
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 824-7000
WWW.TULARECOUNTY.CA.GOV/RMA



omni•means
ENGINEERS PLANNERS
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5885

CONSTRUCTION DETAILS
AVENUE 413 - OROSI
**TULARE COUNTY
COMPLETE STREETS**
TULARE COUNTY, CA

SCALE	AS SHOWN
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JLR
DRAWN	JLR
CHECKED	SMH
FILE	2144C003.DWG
DATE	6-9-16
SHEET No.	

C1

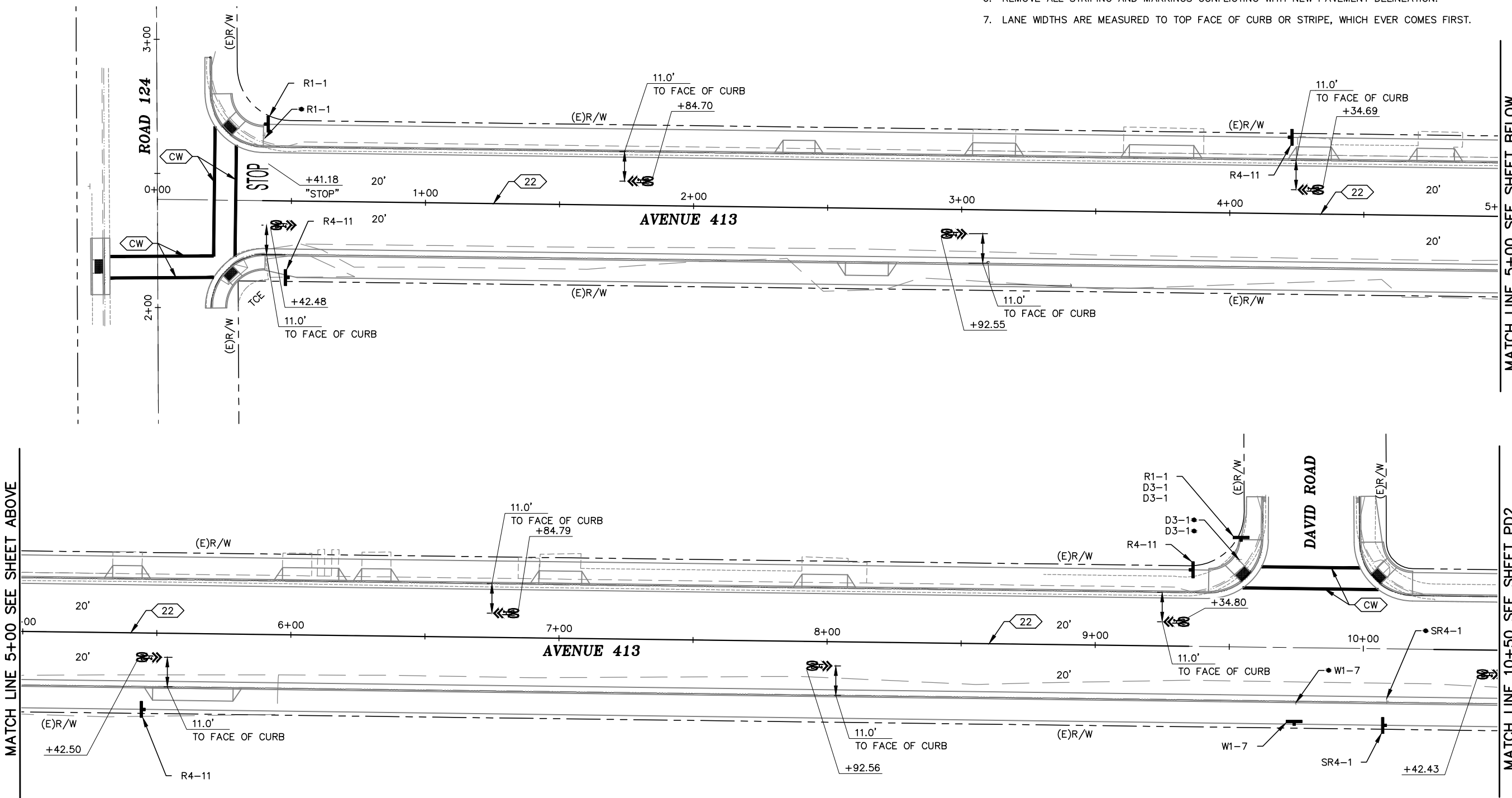
6 OF 9

LEGEND (PD1-PD3)

#	STRIPING DETAIL NUMBER PER STATE STD PLANS	⚡	LIMIT OF STRIPING PATTERN	(CA)	CALIFORNIA SIGN CODE
CW	THERMOPLASTIC CROSSWALK (CW) PER STATE STD PLANS; 10' WIDE FROM INSIDE TO INSIDE OF MARKINGS	▲	ANGLE POINT		
LL	THERMOPLASTIC LIMIT LINE (LL)	○	TANGENT POINT		
➡➡	SHARED ROADWAY BICYCLE MARKING PER CALTRANS RSP A24C	—	EXISTING ONE POST ROADSIDE SIGN		
STOP	STOP LEGEND MARKING PER STATE STD PLANS	⌚	PROPOSED ONE POST ROADSIDE SIGN		
		●	REMOVE SIGN		

NOTES: (SHEETS PD1-PD3)

1. WORK SHALL BE DONE IN ACCORDANCE WITH THE 2015 EDITION OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND SPECIFICATIONS, THE 2014 EDITION OF THE CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) AND SIGN SPECIFICATION SHEETS, THE LATEST EDITIONS OF THE TULARE COUNTY STANDARD PLANS AND SPECIFICATIONS, AND THE SPECIAL PROVISIONS.
2. THESE PLANS ARE ACCURATE FOR SIGNING AND STRIPING ONLY.
3. ALL SIGNS SHALL BE AVERY DENNISON T6500 SERIES REFLECTIVE SHEETING AND SHALL BE COVERED WITH AVERY DENNISON 0L1000 ANTI-GRAFFITI OVERLAY FILM.
4. ALL STRIPING SHALL BE THERMOPLASTIC PAINT.
5. ALL CROSSWALKS SHALL BE 10 FEET WIDE, MEASURED FROM INSIDE TO INSIDE OF MARKING.
6. REMOVE ALL STRIPING AND MARKINGS CONFLICTING WITH NEW PAVEMENT DELINEATION.
7. LANE WIDTHS ARE MEASURED TO TOP FACE OF CURB OR STRIPE, WHICH EVER COMES FIRST.



MATCH LINE 5+00 SEE SHEET BELOW

MATCH LINE 10+50 SEE SHEET PD2

MATCH LINE 5+00 SEE SHEET ABOVE

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REVISIONS

No.	DESCRIPTION	DATE	BY

**TULARE COUNTY
RESOURCE MANAGEMENT
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SIGNING AND STRIPING PLAN
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE 1"=20'

DIVISION RMA

JOB NO. 55-7300-14

DESIGNED JLR

DRAWN JLR

CHECKED SMH

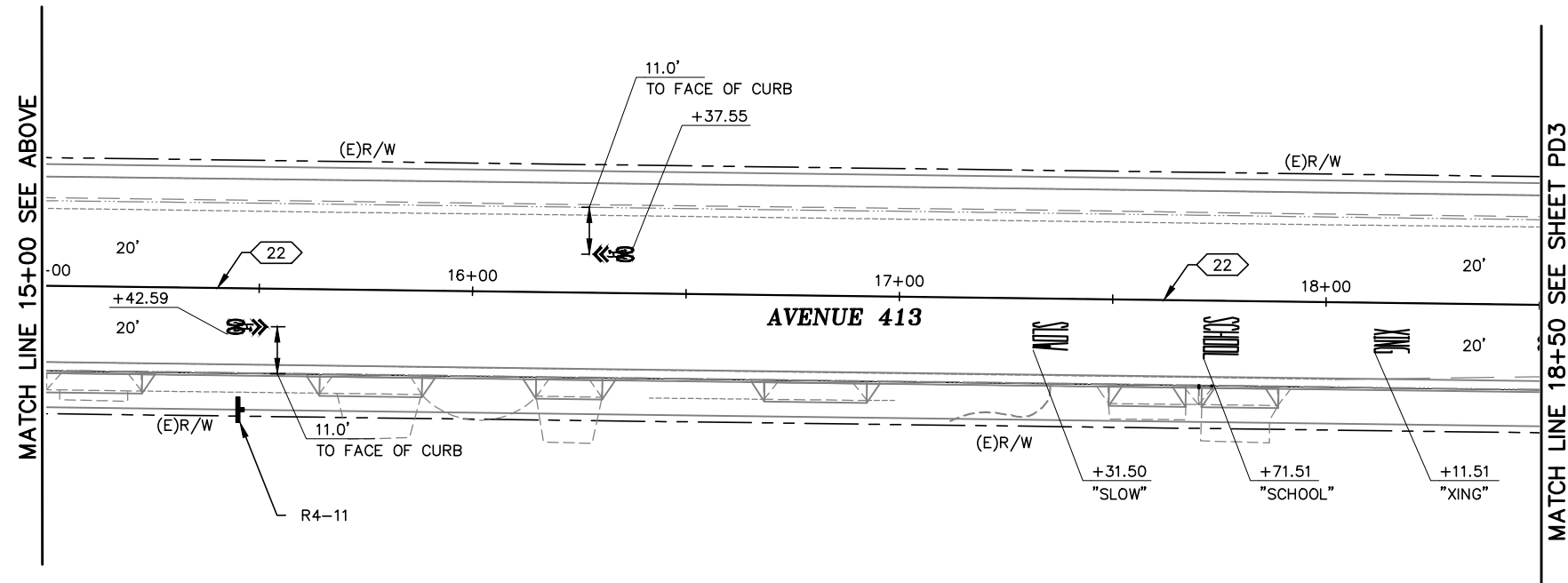
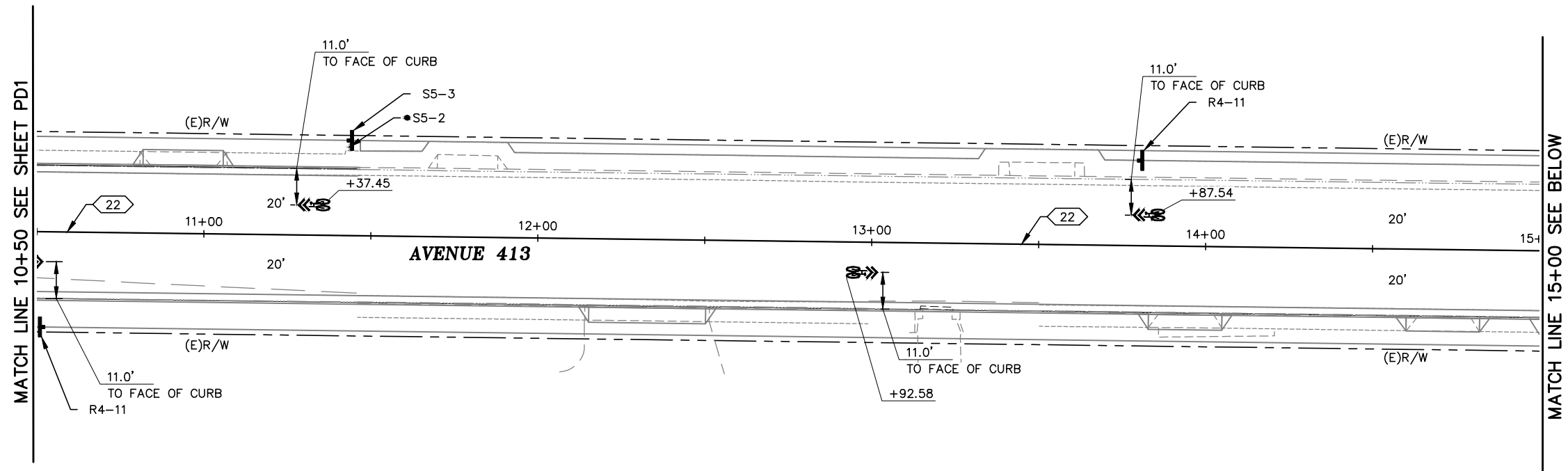
FILE 2144PD003.DWG

DATE 6-9-16

SHEET No.

PD1

7 OF 9



30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS		DATE BY	
No.	DESCRIPTION		



**TULARE COUNTY
RESOURCE MANAGEMENT
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SIGNING AND STRIPING PLAN
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
 TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JLR
DRAWN	JLR
CHECKED	SMH
FILE	2144PD003.DWG
DATE	6-9-16
SHEET No.	PD2

8 OF 9
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Appendix I –

Complete Streets Outreach

**Cutler-Orosi Community Meeting hosted by the Tulare County Resource Management Agency
Complete Streets Meeting/ Community Plan Kick off Meeting
Tuesday, March 3, 2015
Golden Valley Elementary School- Cafeteria
41456 Rd. 127
Orosi, CA 93647
5:30p.m.**

County staff Kyria Fierros, Dave Bryant and Aaron Bock from County Economic Development and Planning Branch from the Tulare County Resource Management Agency (RMA) attended the community meeting in Orosi, CA on March 3, 2015. This was the second meeting RMA held in Orosi.

On February 12, 2015, RMA gave an informational update about the Community Plan and Complete Streets program at the Cutler-Orosi Joint Unified School Board meeting.

At the meeting the Board suggested RMA hold a separate meeting with the community allowing all local residents the opportunity to come to a meeting. The Board also requested information on the past improvements completed from the previous Community Plan of 1988 and the amount of projects implemented since the passing of Measure R in 2006.

On March 3, 2015 RMA with the help from Roel Alvarado, Principal at Cutler Elementary, translated the meeting in English to Spanish.

RMA staff gave a PowerPoint explaining the Community Plan process and the Complete Streets program letting the public know we were seeking input on priority streets. Dave Bryant updated the community on infrastructure improvements from the previous community plan of 1988 and Kyria Fierros presented on the Measure R funds.

According to our Road Department there the total amount of funds used from Measure R funds since 2006:

Cutler \$242K

Orosi \$900K

Total: \$1.1 million

The five Streets County identified for the complete streets by the community were:

Orosi

1. Avenue 416 – SR 63 to Rd 140 (East Orosi) **43 Votes**

2. Avenue 413 – Rd 124 to SR 63 **11 Votes**
3. Avenue 419 **3 Votes**
4. Avenue 416 – SR-63 to Dinuba **0 Votes**

Additional Considerations

5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Rd/2nd Drive - Ave 407 to SR 63 **36 Votes**
2. Avenue 408 – Rd 124 to SR 63 **14 Votes**
3. Railroad Drive – SR 63 to Rd 124 **9 Votes**
4. Avenue 404 – SR 63 to Robert Rd **6 Votes**
5. First Drive – SR 63 to Rd 124 **5 Votes**

General Road Concerns

- Orosi Drive at 2nd Drive-Flooding
- Road 124 at Aceves Ave-Stop Sign
- Road 124 (Ave 408 to Ave 416) Traffic Speeding Concerns
- El Monte School (Road 127) Request for additional Crosswalks(Students cross mid-block at multiple locations primarily north of the school).
- Road 127 at Ave 416 (Sight Distance Problem with parked vehicles, Northbound left turn from Road 127 to westbound Ave 416).
- SR 63 (Road 128) Request for Traffic Signal at Orosi High School(Walnut Ave?)
- Avenue 416 there is lots of traffic speeding. There has been accident there
- Avenue 63 there are no lights sidewalks
- Avenue 63 and Ella need for sidewalks.

Development:

Citizens expressed their concerns for a pharmacy. They stated the closest one is in Dinuba and even then there is very limited bus transportation. A pharmacy would be a great asset because local residents can walk to their local pharmacy than have to travel to another city.

Water Issue:

Superintendent Valdez voiced her concerned for the need of clean water for the children and families. Staff discussed the County- Wide bottle water program for the residents and let them know we qualify those who are on a dried well or show contamination in their water. Follow up information was made available.

Outreach:

The County provided 4,000 fliers to the school district where the fliers were distributed amongst the schools. A recorded voice message was made to the households of all the parents of the children Monday evening.

Conclusion:

We concluded the meeting by telling the superintendent we would host another meeting at their community parent meeting in the months to come. We are currently moving forward with updating Cutler-Orosi's community plan. Last time it was updated was 1988. We will hold 2-3 more meetings to receive community input.

County staff in attendance: Aaron Bock, Kyria Fierros, Dave Bryant

The meeting was held in English and Spanish.

The meeting started at 5:30p.m. And ended at 7:15 p.m.







The Tulare County Resource Management Agency will be hosting a
**Cutler-Orosi Community Plan Update and
Complete Streets Workshop** on
*Tuesday, March 3rd, 2015, at 5:30p.m. at the
Golden Valley School-Cafeteria
41465 Road 127
Orosi, CA 93647*

We will be discussing updating the existing Cutler-Orosi Community Plan which will include proposed Land Use, Zoning, and Circulation changes in the area. Our discussion will also include a new Complete Streets Program. We will be discussing proposed new changes in the area and are asking for community input on these important Community projects. This is the start of a series of meetings we will have with the community.

For more information call: Kyria at 559-624-7154



El condado de Tulare va dar una **Presentación Informativa de
Planificación y Ingeniería** para la comunidad de Cutler-Orosi el día
*Marties, 3 de Marzo a las
5:30 p.m.* en la
**Cafeteria de Golden Valley School
41465 Road 127
Orosi, CA 93647**

Por favor venga y únase a nosotros para un seguimiento de la presentación de Comunidades Saludables y discusión de usos de la tierra. Vamos a discutir propuestas de nuevos cambios en el área y pedir opiniones de la comunidad. Esta es parte de una serie de reuniones que el condado esta haciendo.

Si quiere mas informacion llame a: Kyria -559-624-7154

Draft Cutler/Orosi Community Plan 2020 Update

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Draft Cutler/Orosi Community Plan 2020 Update

Attachment A- 7 Public Outreach

Draft Cutler/Orosi Community Plan 2020 Update

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Draft Cutler/Orosi Community Plan 2020 Update

Appendices

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

Draft Cutler/Orosi Community Plan 2020 Update

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Draft Cutler/Orosi Community Plan 2020 Update

Appendix A: Planning Commission Resolutions

Draft Cutler/Orosi Community Plan 2020 Update

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Draft Cutler/Orosi Community Plan 2020 Update

Appendix B: Board of Supervisors Resolution

Draft Cutler/Orosi Community Plan 2020 Update

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