DRAFT CUTLER-OROSI COMMUNITY PLAN 2021 UPDATE

Tulare County Resource Management Agency
Economic Development and Planning Branch

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Cutler-Orosi Community Plan 2021 Update

Adopted: ----

Tulare County Board of Supervisors
Resolution No. ----

Tulare County Planning Commission
Recommendations:
Resolutions No.

Cutler-Orosi Community Plan Update: GPA 18-003
Section 18.9 Zoning Ordinance (Mixed Use): PZC 18-009
Section 16 Zoning Ordinance (By Right) PZC 18-010
Zoning District Map: PZC 18-011

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Executive Summary

Introduction

The Cutler-Orosi Community Plan was adopted by the Tulare County Board of Supervisors (BOS) on August 30, 1988 (by Resolution No. 88-1051). Subsequent to the adoption of the Community Plan a series of General Plan Amendments were adopted as follows: GPA 89-03 adopted by the BOS on November 6, 1990 by Resolution 90-1346; GPA 94-004, GPA 94-005, and GPA 95-001 on November 7, 1995 by Resolution No. 95-1272; GPA 95-005 and GPA 95-006 were adopted by the BOS on April 23, 1996 by Resolution No. 96-0335; GPA 98-004 adopted by the BOS on July 13, 1999 by Resolution No. 99-0480; GPA 00-001 adopted by the BOS on October 10, 2000 by Resolution No. 2000-771; GPA 00-005 adopted by BOS on August 27, 2002 by Resolution No. 2002-0652; and GPA 09-003 adopted by BOS on October 14, 2014 by Resolution No. 2014-0717. The 1988 Cutler-Orosi Community Plan is a collection of goals, objectives, and policies for the physical development of the community. The primary purpose of the plan was to outline community goals regarding physical development and to promote the general welfare of the communities. The plan serves as a general guide for both public and private decisions affecting the community, and provides for the overall direction, density, and type of growth consistent with the needs of the communities.

General Plan Amendments

The proposed Cutler-Orosi Community Plan 2021 Update is consistent with the Tulare County General Plan 2030 Update (2012) and will include the following primary goals and objectives:

1) Land Use and Environmental Planning - Promote development within planning areas next to the Regional SR 63 Corridor in order to implement the following General Plan goals:

- Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters, such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals;
- Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
- Reduce development pressure on agriculturally designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
- Reduce vehicle miles travelled throughout the County, thereby positively affecting air quality and greenhouse gas reduction; and
- Help to improve the circulation and transit transportation system within this community, including, but not limited to, laying the groundwork for the construction of key projects such as Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths.

2) Improvements for a "disadvantaged community" - It is expected that the community planning areas will be improved for the following reasons:

- With faster project processing resulting from an updated community plan, increased employment opportunities are more likely to be provided by the private sector as proposed project developments can be approved as expeditiously as possible;
- Increased housing grant awards are more likely to occur based on updated community plans that are consistent with the policies of the General Plan 2030 Update (August 2012) and the recently adopted Housing Element 2015 Update (November 2015); and • With updated community plans, enhanced infrastructure grant awards are more likely, thereby providing access to funding to install or upgrade road, water, wastewater, and storm water facilities.

3) Strengthening Relationship with TCAG - An important benefit of this expedited community plan process will be the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG) in that this and other community plans will help to facilitate the funding and implementation of several key transportation programs, such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects.

By pursuing these transportation programs through a heightened collaborative process, the likelihood of getting actual projects in the ground will be realized faster than historically achieved. In doing so, these communities and others can become safer and healthier by providing a more efficient transportation network.

The proposed Cutler-Orosi Community Plan 2021 Update amends the 1988 Cutler-Orosi Community Plan with this proposed General Plan Amendment and implements the 2012 Tulare County General Plan 2030 Update:

Planning Framework Element (Urban Boundaries). The Planning Framework Element is revised to update the Urban Development Boundary Part 1, of the Tulare County General Plan. The intent is that the County's UDB is coterminous, as administratively feasible, with the Sphere of Influence (SOI) adopted by Tulare County Local Agency Formation Commission.

Open Space Element. The Environmental Resources Management Element is amended to revise the "Urban Expansion Area" designation on the Open Space Map Part 1, Figure 8-1 of the Tulare County General Plan to reflect the area within the revised Urban Development Boundary of Cutler-Orosi.

Land Use, Transportation and Circulation Elements Part 1. This Plan Amendment incorporates the following: The County's General Plan land use designations, circulation functional classification, and development policies into the Cutler-Orosi Community Plan 2021 Update.

Community Plan Updates Part III. The proposed Community Plan for Cutler-Orosi is updated with this proposed amendment.

Zone Ordinance Amendments

Section 18.9: "MU" Mixed-Use Combining Zone. This amendment will establish the Mixed-Use Combining Zone(s) within the UDB of Cutler-Orosi. Currently, this overlay zone applies to 34 communities including Ivanhoe, Plainview, Woodville, Poplar/Cotton Center, Three Rivers, Goshen, Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Earlimart, Alpaugh, East Orosi, London, Richgrove, Sultana, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, Tooleville, Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem. The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicle miles traveled if residential uses are mixed with uses for employment.

Section 16 H: Additional "By-Right" Uses. This amendment will establish additional by-right uses within the UDB of Cutler-Orosi. The allowance of additional by-right uses applies to 34 communities including Lemon Cove, Ivanhoe, Plainview, Woodville, Poplar/Cotton Center, Three Rivers, Goshen, Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Earlimart, Alpaugh, East Orosi, London, Richgrove, Sultana, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, Tooleville, Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem. The purpose of adding additional by-right uses is to promote sustainability; economic development and prosperity by providing design flexibility, streamline the approval process and reduce vehicles miles traveled by locating residential uses within proximity to employment areas.

Ordinance No. 352: Zoning District Ordinance Map. This amendment to the Zoning District Ordinance Map will rezone properties within the Cutler-Orosi UDB to be consistent with the 2020 Cutler-Orosi Community Plan.

Community Plan Context

The two communities are predominantly rural, agriculturally related service centers. They not only serve as an area where agriculturally oriented enterprises, such as packing houses and cold storage facilities are located, but also as a residential community where many of the areas farm workers reside. Persons residing in smaller surrounding communities, like East Orosi, Sultana, and Yettem, travel to Cutler-Orosi for incidental shopping, school, and banking purposes. For major shopping, leisure services and medical care, persons will generally travel to Dinuba (approximately five miles west), or Visalia (approximately 15 miles south).

The objective in the preparation of the Cutler-Orosi Community Plan 2021 Update is to develop a plan, which can accurately reflect the needs and priorities of the unincorporated communities of Cutler-Orosi. In addition, the County has prepared an Environmental Impact Report to address environmental-related issues. The Environmental Impact Report (EIR) is significant as it can be used to assist in fostering future economic development, grants, and economic development opportunities, by providing environmental clearance when tiering off the General Plan's and Community Plan's EIR's.

Cutler-Orosi are currently designated as Unincorporated Communities in the 2030 Tulare County General Plan (2012). Cutler-Orosi is situated about one-half mile apart along State Route 63, a rural highway running north/south through eastern Tulare County. It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding and to stimulate economic development within the community.

As with any community plan, the contents of this document are not intended to be absolute. Planning is a continuous process and, to be effective, requires periodic re-evaluation and revision to reflect changing needs and priorities. This Plan, therefore, should be reviewed on a periodic basis with the assistance and participation of local citizens, groups, and agencies. By doing so, it is envisioned that the Cutler-Orosi Community Plan will continue to provide meaningful and necessary guidance toward the development of the community in the foreseeable future.

California Government Code (Section 65300 et seq.) requires that each local agency, city or county, prepare and adopt comprehensive long-term general plans for the physical development of lands within its jurisdiction. A general plan must function as "a statement of development policies" and must include a diagram and text setting forth goals, policies, standards, and plan proposals. The plan must, on the minimum, include the following elements: land use, circulation, housing, conservation, noise, safety, and open space. State law also provides that a local agency may include one or more several optional elements depending on the needs and characteristics of the jurisdiction.

In Tulare County, the General Plan has historically been developed on a countywide basis or by large geographic sub-areas (such as rural valley, foothill, and mountain), with development policies, emphasizing county-wide and area-wide issues and concerns. In establishing land use planning policies on an area-wide basis, it has been recognized that several unincorporated communities, including Cutler-Orosi, have localized land use needs and issues that should be addressed in a more specific manner particular to its community, geographic features, location of major roadways (such as State Route 63), population characteristics, availability of water, and other issues unique to the community's area. Therefore, the Cutler-Orosi Community Plan 2021 Update has been prepared with an emphasis on these considerations with particular focus on land use and circulation.

Location

Cutler-Orosi are located in California's central San Joaquin Valley, in the easterly Valley floor portion of Tulare County **(see Figure 1)**. The two adjacent communities lie in the midst of one of the most productive agricultural regions in the world, and are virtually surrounded by field crops, orchards, and vineyards.

Cutler-Orosi are located in northern Tulare County approximately 16 miles east of State Route (SR) 99 and approximately 15 miles north of Visalia, the County seat. Both communities are located along State Route (SR) 63 about one-half mile apart. The Tulare County/Fresno County Line is located approximately 3.3 miles northwest of Cutler. The communities are situated at the base of the Sierra Nevada Mountain foothills.

Cutler is generally bound by Avenue 402 in the south, Avenue 408 in the north, Road 120 in the west, and the Bowhay Ditch in the east and encompasses approximately 0.8 square miles of land **(see Figure 2)**. Cutler is located south of and adjacent to the unincorporated community of Orosi. Cutler is an agriculturally oriented service community surrounded on the south, west and east by lands in agricultural production, vacant lands, and scattered residential homes.

Orosi is generally bound by Avenue 408 in the south, Avenue 424 in the north, Road 120 in the west, and the Bowhay Ditch and Sand Creek in the east and encompasses approximately 2.4 square miles of land. State Route (SR) 63 directly serves Orosi. Orosi is located north of and adjacent to the community of Cutler. Orosi is an agriculturally oriented service community surrounded on the north, west and east by lands in agricultural production, vacant lands, and scattered residential homes. The unincorporated community of East Orosi is located to the northeast.

Cutler-Orosi are located in Sections 07, 08, 17, 18, 19, & 20, Township 16 South, Range 25 East; MDB&M, and can be found within the Orange Cove South Quadrant, United States Geological Survey 7.5 minute topographic quadrangle. Comparatively flat and topographically almost featureless, Cutler-Orosi lies at an elevation ranging from 375 feet above mean sea level (msl), near the northeasterly end of Orosi, to 355 feet msl at the west end of Cutler. The community is situated on a very gentle gradient to the southwest. The coordinates of Cutler-Orosi are Latitude: 36º 31' 29N" and Longitude: 119º 17' 20".

Figure 1 - Vicinity Map

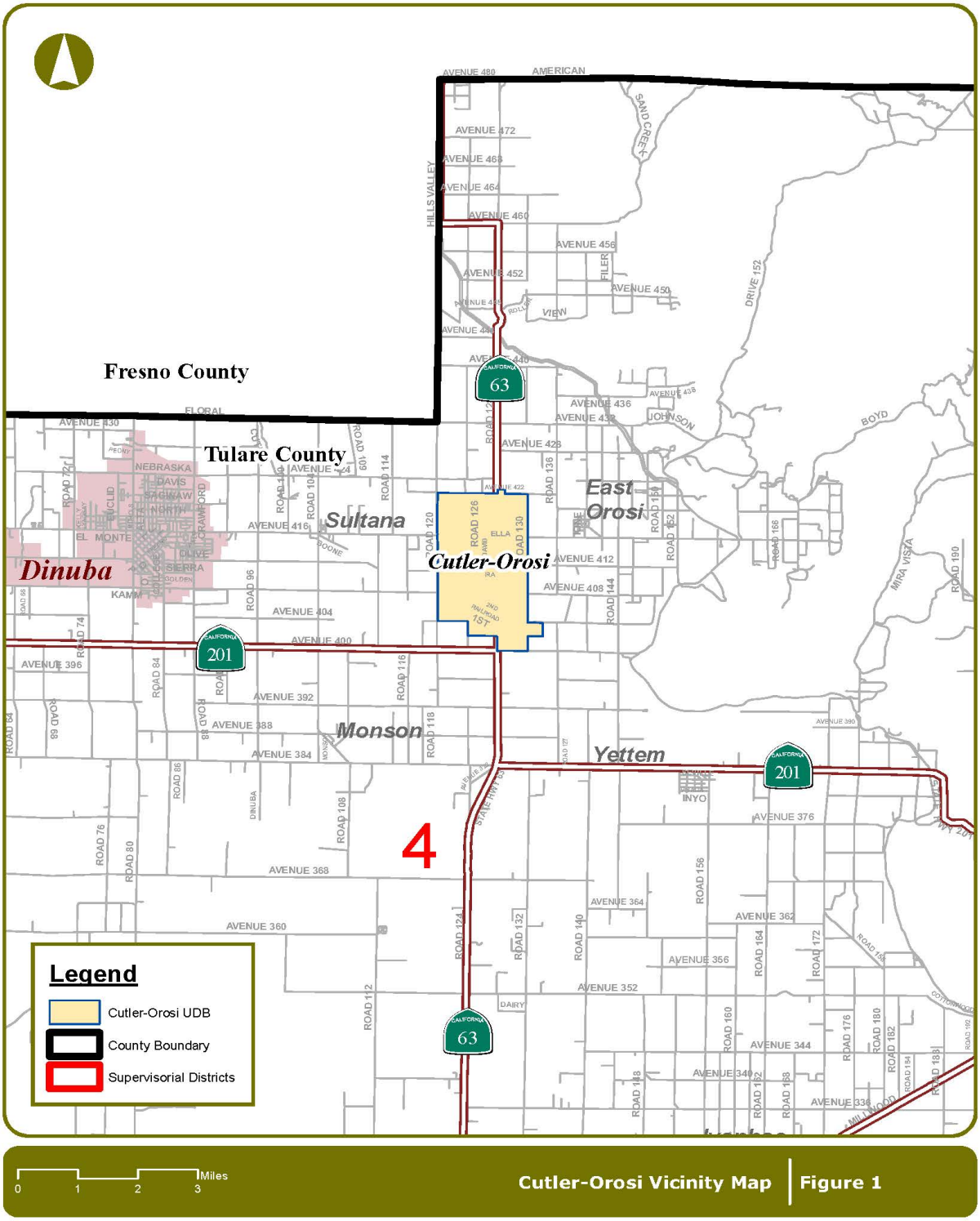
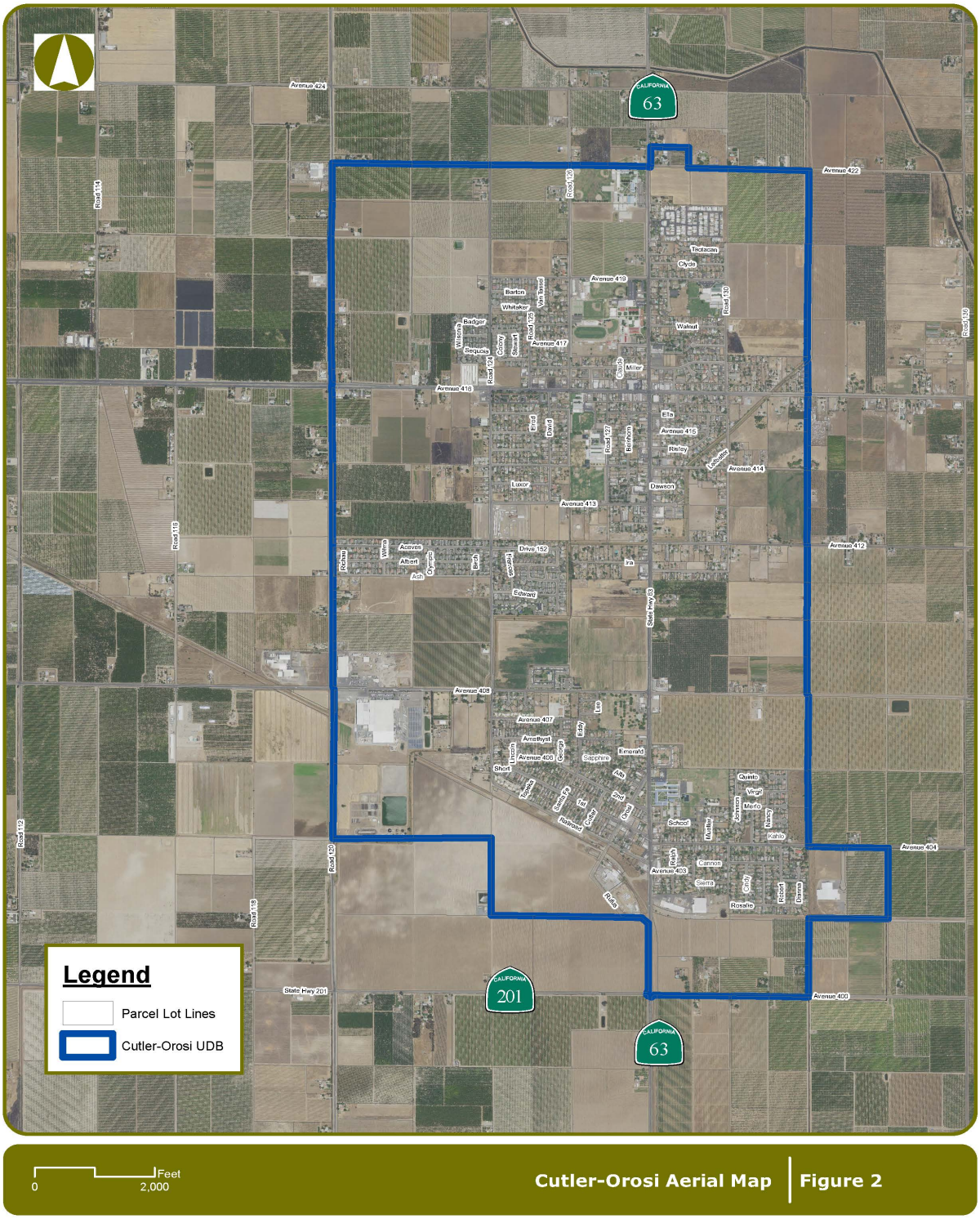


Figure 2 - Aerial Map



Historical Perspective

"Generally, the growth of the two communities has been well planned. Both have developed in a contiguous fashion and have remained compact through infilling. The two communities are separated from each other by distances ranging from one quarter to one half mile. One of the reasons for the open space between the two communities is that Sand Creek was previously subject to periodical floods thereby making some of this land undevelopable prior to current flood control improvements. Most of the land that separates Cutler-Orosi is presently used for agriculture.

Cutler-Orosi are surrounded by agricultural lands. Crops grown on these lands include field crops, deciduous fruit orchards and vineyards. Unlike many Valley communities, there is little rural residential development (1 to 5 acre homesites) surrounding either community.

Cutler is bisected north and south by SR 63. It was bound on the south by the Atchison Topeka Santa Fe Railroad and agricultural land, on the north and east by agricultural land, and on the west by the railroad, the wastewater treatment plant and two major packinghouses. The western half of Cutler is almost fully developed, whereas the eastern half is less than 50 percent urbanized. The Atchison Topeka Santa Fe Railroad tracks that bound Cutler to the south is now abandoned right-of-way **(see Figure 3)**.[1] The railroad tracks and crossties were removed. The cobble and gravel covered railbed and footprint of the former railway are still visible.

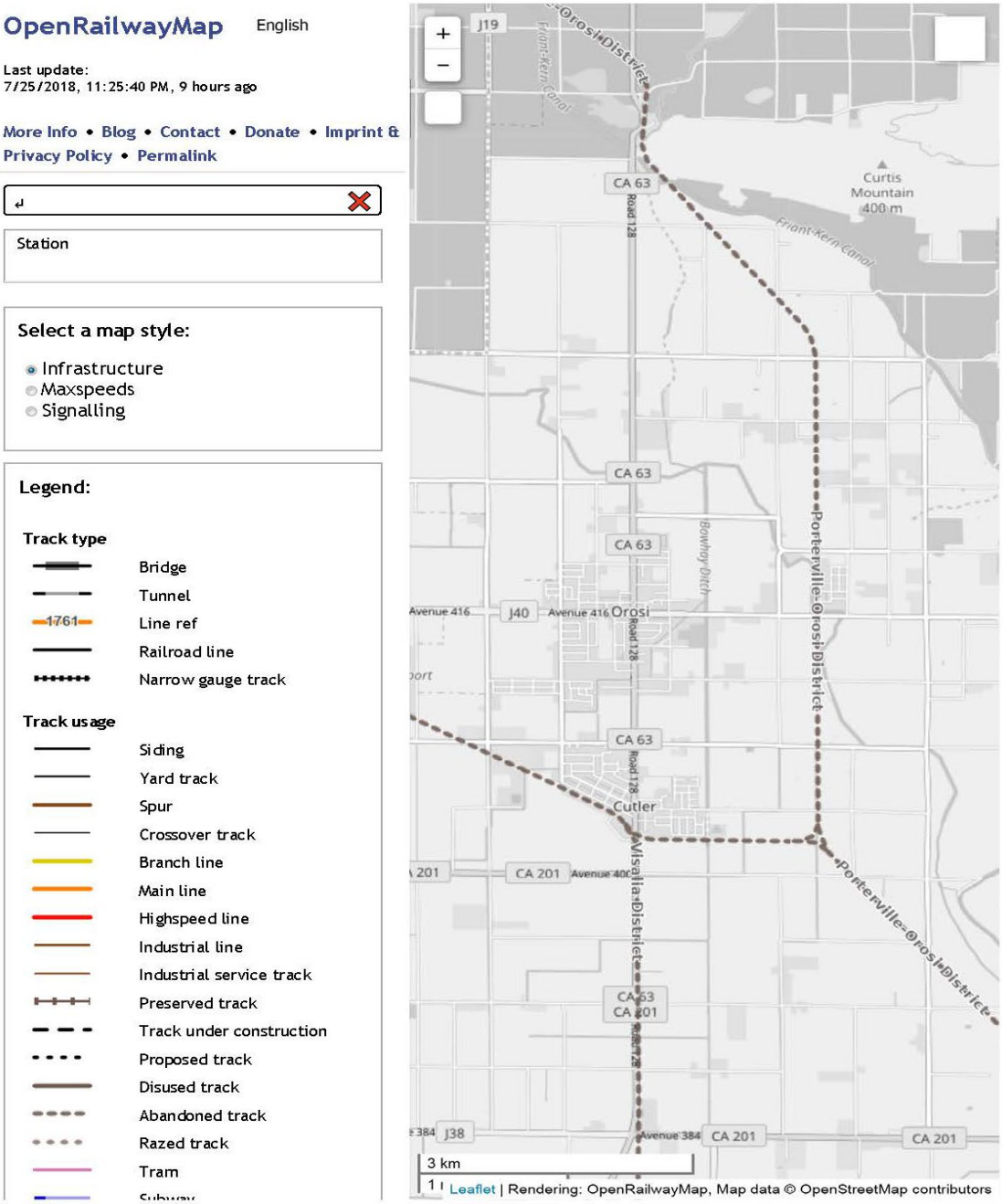
Residential development has occurred on the east side of Cutler. Development to the south and southeast has been restricted by a number of features, including the railbed footprint, the wastewater treatment plant, industry, and lands under the Williamson Act. To the north, Development northward has historically been restricted by flooding from Sand Creek prior to existing flood control improvements. Commercial development is concentrated along both sides of SR 63, while industrial uses are situated along the railbed footprint. Cutler Elementary School and two parks are also located adjacent to SR 63.

State Route 63 and Avenue 416 divide Orosi into four neighborhood quadrants. Each quadrant supports a mix of single family, mobile home and rural residential development. Almost all the multifamily development is located in the southwest quadrant, except the southeast, support a school. Residential development has occurred in the northeast and southwest quadrants. Development to the south and east has historically been restricted by flooding and irregularly shaped parcels, which are difficult to develop.

Orosi's commercial district is concentrated along SR 63 and along the west side of Avenue 416. New Commercial development has not occurred in Orosi in recent years. The proximity of Dinuba and Visalia make commercial development in Orosi somewhat risky due to competition from these neighboring communities. Orosi has little industrial development, and what little there is dispersed in the southern part of the community."[2]

[1] Open Railway Map https//www.openrailwaymap.org.
[2] Cutler-Orosi Community Plan, adopted August 30, 1988 by Tulare County Board of Supervisors, Resolution No. 88-1051.

Figure 3 - Abandoned Railroad Track



<u>Cutler-Orosi Charrette, November 2001</u>

"The Cutler-Orosi Community Design Charrette was conducted from November 1 – 5, 2001. Its

goal was to create a conceptual plan for improvements for all roadway users along State Route 63 and the intersecting main streets of Cutler-Orosi. It was funded through a Caltrans Community-Based Transportation Planning Grant, and grew out of community visioning work initiated through a Great Valley Center Legacy grant. The design team included two transportation planners, a traffic engineer, a landscape architect, and an architect. All events were conducted in both English and Spanish.

Community participation during the charrette was exceptional. Over 130 people attended the opening event, and approximately 300 people, including County and Caltrans District 6 staff took part during the five-day event. A multi-phased plan with several low-cost implementation steps to improve safety and help stimulate investment in Cutler-Orosi was developed.

Recommendations that were recommended included a series of community-based actions that identified little cost but anticipated delivering a high impact. Recommendations that will require more time include interim safety and beautification measures such as sidewalk completion, curb extensions at critical crossings, the narrowing of State Route 63 north of Avenue 416, and street tree plantings.

In the longer term vision, commuter traffic to and from Dinuba and heavy trucks were recommended to be rerouted off SR 63 through the construction of a truck bypass on County Road 120. By moving a significant portion of through traffic off SR 63, it allows for significant enhancements and traffic calming which are appropriate for a pedestrian-friendly town center that the community envisions on the vacant land between the two towns. A median, on street parking, bicycle lanes, and the removal of two traffic lanes were recommended for nearly the entire corridor in this later phase.

This report is one of three undertaken by the County for the previously established Cutler-Orosi Redevelopment Project Area. Along with the concurrent water and sewer needs assessment study, the charrette report included recommendations of the Cutler-Orosi Commercial Development Plan."[3]

[3] Cutler-Orosi Charrette Report, November 2001, by Walkable Communities, Inc. and the Local Government Commission prepared for County of Tulare Redevelopment Agency and Local Government Commission. Page 5.

Cutler-Orosi Background Report

Community Profile

Planning Area

Cutler-Orosi are two unincorporated communities located in northern Tulare County. Both communities are located along State Route 63 about one half mile apart. The population for Cutler-Orosi is 5,850 and 7,760 persons in 2017. Cutler-Orosi are surrounded by agricultural lands. The Cutler-Orosi Urban Development Boundary (UDB) area consists of approximately 2,441.9 acres **(see Figure 4)**. Cutler is bisected north and south by State Route (SR) 63. SR 63 and Avenue 416 divides Orosi into four neighborhood quadrants.

Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2017, Cutler's median household income was $31,939 and Orosi's median household income was $35,798, whereas the State of California's median household income was $67,169. Median household income for Cutler was 47.5% and Orosi median household income was 53.3% of the State of California's median household income, and therefore considered severely disadvantaged communities.

Climate

The southern San Joaquin Valley climate is influenced to a great extent by the Coast Ranges to the west which prevent the cool, moisture-laden maritime air from reaching the valley. It is generally characterized as a Mediterranean climate (one of three similar zones in the world). The area in general has a climate that tends to be clear, sunny, warm and dry. The mean temperatures range from a low of 34° F. (l.l° C) in January to a high of 100° F. (37.7° C) in July. Because of the Coast Ranges, the average rainfall for the area is very low, ranging from three to nine inches per year, with 90% of the yearly precipitation between November and April. There are periods in winter when the valley floor is covered with dense wet ground fogs with winds typically light and from the north.

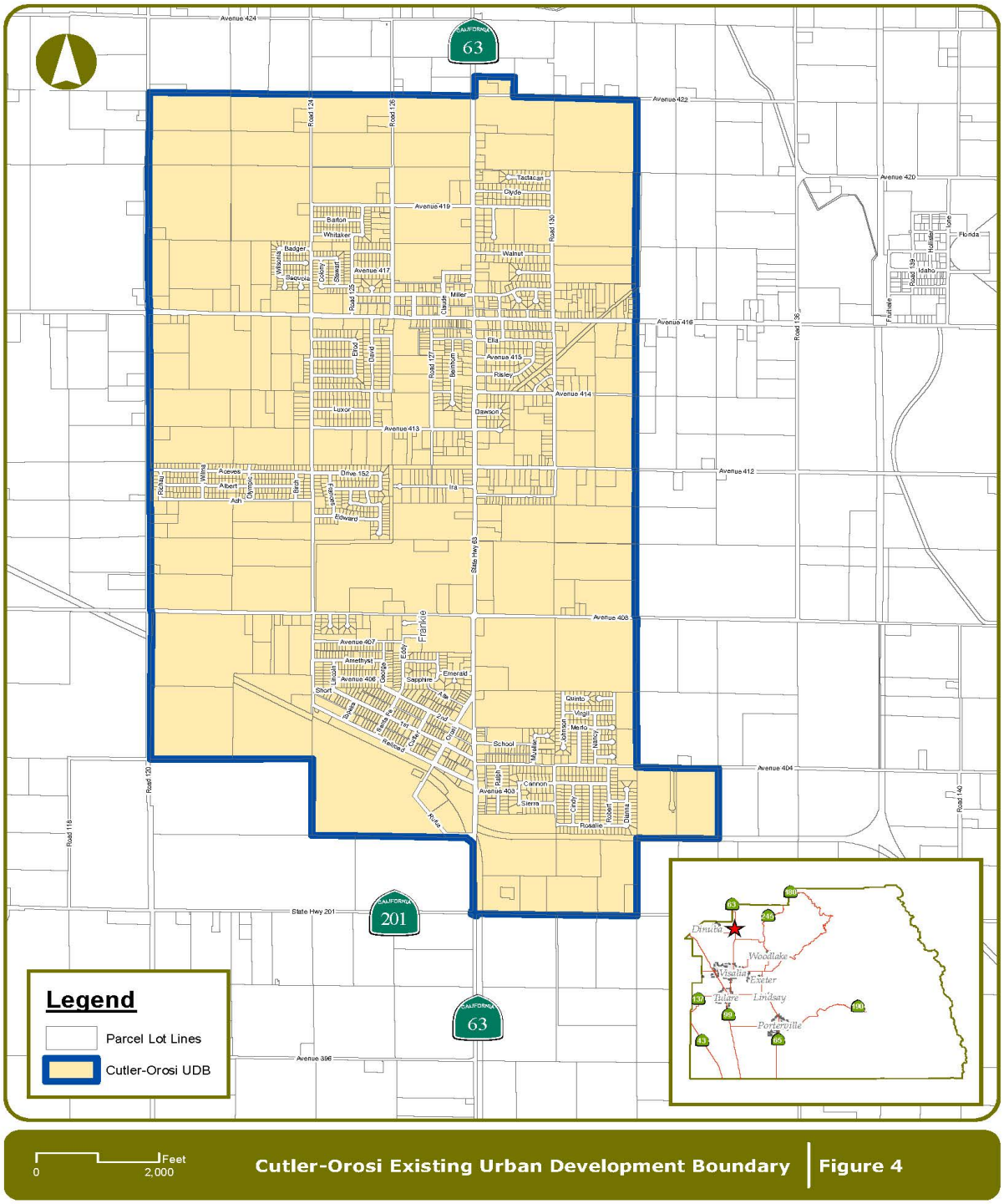
Topography

The communities are situated on relatively level terrain with a slight fall towards the southwest. The major natural features in the area include the Sierran foothills located two miles to the east, and Sand Creek, which flows between the two communities. Average elevation above sea level for Cutler is 374 and 361 feet for Orosi.

Existing Urban Development Boundary

The existing Cutler-Orosi Urban Development Boundary (UDB) area **(see Figure 4)** consists of approximately 2,441.9-acres (including rights-of-way). Within the existing Cutler-Orosi UDB, approximately 1,245.4-acres are currently zoned for urban uses and approximately 956.9 acres are zoned for agricultural uses. Cutler-Orosi are surrounded by agricultural lands, crops grown on these lands include field crops, deciduous fruit orchards, and vineyards. Unlike many Valley communities, there is little rural residential development (1 to 5 acre homesites) surrounding either community. The UDB includes areas within the Cutler Public Utility District (CPUD) and the Orosi Public Utility District (OPUD) in order to provide service area consistency between these two boundaries.

Figure 4 - Cutler-Orosi Existing Urban Development Boundary



Existing Land Use Plan

Land use patterns in Cutler-Orosi are typical of other Valley communities. Commercial development and apartments are situated on the more heavily traveled streets, industrial development is located along railroads, schools, and parks are integrated with residential districts, and the entire community is surrounded by agriculture.

The purpose of the Cutler-Orosi Community Plan is to establish land use policies to guide existing and future development to the year 2030. The general intent of these policies is to protect the health, safety, and welfare of persons living in Cutler-Orosi. In more specific terms, the policies serve to identify the most appropriate locations and arrangement of different types of land uses based upon environmental, circulation, infrastructure/services, and planning concerns.

The County of Tulare, through existing policies, has encouraged both incorporated and unincorporated communities to establish urban development and land use patterns, which are compact and contiguous. This policy position has reduced "leap frog" (that is, dis-contiguous) development within the County, thereby helping preserve agricultural lands, and minimizing land use conflicts between urban and agricultural areas.

Residential

To provide for a variety of living environments and opportunities for affordable housing, the 1988 Cutler-Orosi Community Plan establishes three residential densities: low, medium, and high. Low density residential allows six units or less per acre; medium density allows 4 to 14 units per acre and high density allows 15 to 29 units per acre.

> Low Density Residential The Low Density areas are planned to accommodate single-family homes on individual lots where urban services (i.e. community water and sewer) are provided. Properties designated low density residential generally lack adequate infrastructure to warrant higher densities, or serve as a transitional use between urban and agricultural uses. This residential designation promotes a rural environment where livestock and small farming operations are allowed.
>
> Medium Density Residential Medium Density Residential areas are planned to accommodate single-family homes on individual lots where urban services (i.e. community water and sewer) are provided, at higher densities than the area designated for Low Density Residential Development. Medium Density Residential is applied to many areas of the residential land in Cutler-Orosi. Properties with this designation are, or will become, the single-family neighborhoods of each community. This designation is generally applied to properties that are free of excessive noise and through traffic, are in close proximity to parks and schools, are provided with off-site sewer and water, and are within the immediate service area of fire and police services.
>
> High Density Residential High Density Residential designation provides for residential development with a wide range of densities and housing types. High density residential is the designation reserved for multiple family units or apartments. This Plan has attempted to

ensure that no one quadrant of either community is overburdened with apartments. In addition, multiple family development presents many more design options that can be used to help mitigate noise situations. Therefore, this Plan recommends that most of the high-density residential development be located along arterial or collector streets, which can handle greater amounts of traffic and where noise levels are usually greater than most single-family subdivisions can tolerate.

In addition, this designation has also been applied to areas of Orosi, which contain irregular parcels in terms of size and shape. It is the strategy of this Plan that a higher level of land use many encourage property owners to privately redevelop their land. This redevelopment could lead to removal of dilapidated residential units, a better utilization of the land for residential development, and reduce the residential demands for outlying agricultural properties, thereby preserving agricultural land.

Residential Reserve

Land designated for future residential use, should remain in accordance with Policy 5.1. It should be noted that a general plan amendment is not agricultural use until it is determined that conditions warrant conversion to residential use, needed to develop land in a reserve classification.

General Commercial

Commercial development first appeared near the intersection of SR 63 and Avenue 416, and have since spread in strip fashion along these routes.

Service Commercial

Orosi contains one-area approximately 12 acres of service commercial, located south of Avenue 416. Cutler contains two areas (approximately 68 acres and approximately 11 acres of Service Commercial) along the railbed footprint.

Professional Office

Professional Office contains approximately 16.6 acres. In Orosi, Land Use Designation Professional Office is located along Avenue 416 and SR 63. Family Healthcare Network is located in Cutler on Avenue 408.

Industrial

Currently, industry in the Cutler-Orosi area is concentrated along the railbed. Included in this area are packinghouses, cold storage facilities, a box manufacturing plant, and an agricultural chemical company. Orosi has a five (5) acre parcel south of Avenue 416. Cutler is along the railbed and on east side of SR 63

Industrial Reserve

Land within the Plan Area which is recognized as suitable for industrial uses or agriculturally-related industries and is designated for eventual conversion to commercial use, but which is expected to be

left in exclusive agricultural zoning until it is determined that conditions warrant conversion to industrial use, in accordance with Policy 5.1.

Agriculture

Agriculture is the foundation of Tulare County's economy. For this reason, it is important that agricultural lands be preserved and that agricultural operations remain free of adjacent incompatible land uses, which may hamper the operation. The Cutler-Orosi Community Plan takes into consideration surrounding agricultural operations and their needs to be free of intruding urban uses. Where possible, the UDB follows a road, railroad, or creek so that there is some spatial distance between future urban uses and agriculture.

Park

Ledbetter Park is approximately 11 acres in size and is located one mile northwest of Cutler on Road 124/SR 63.

Adopted Land Use Plan

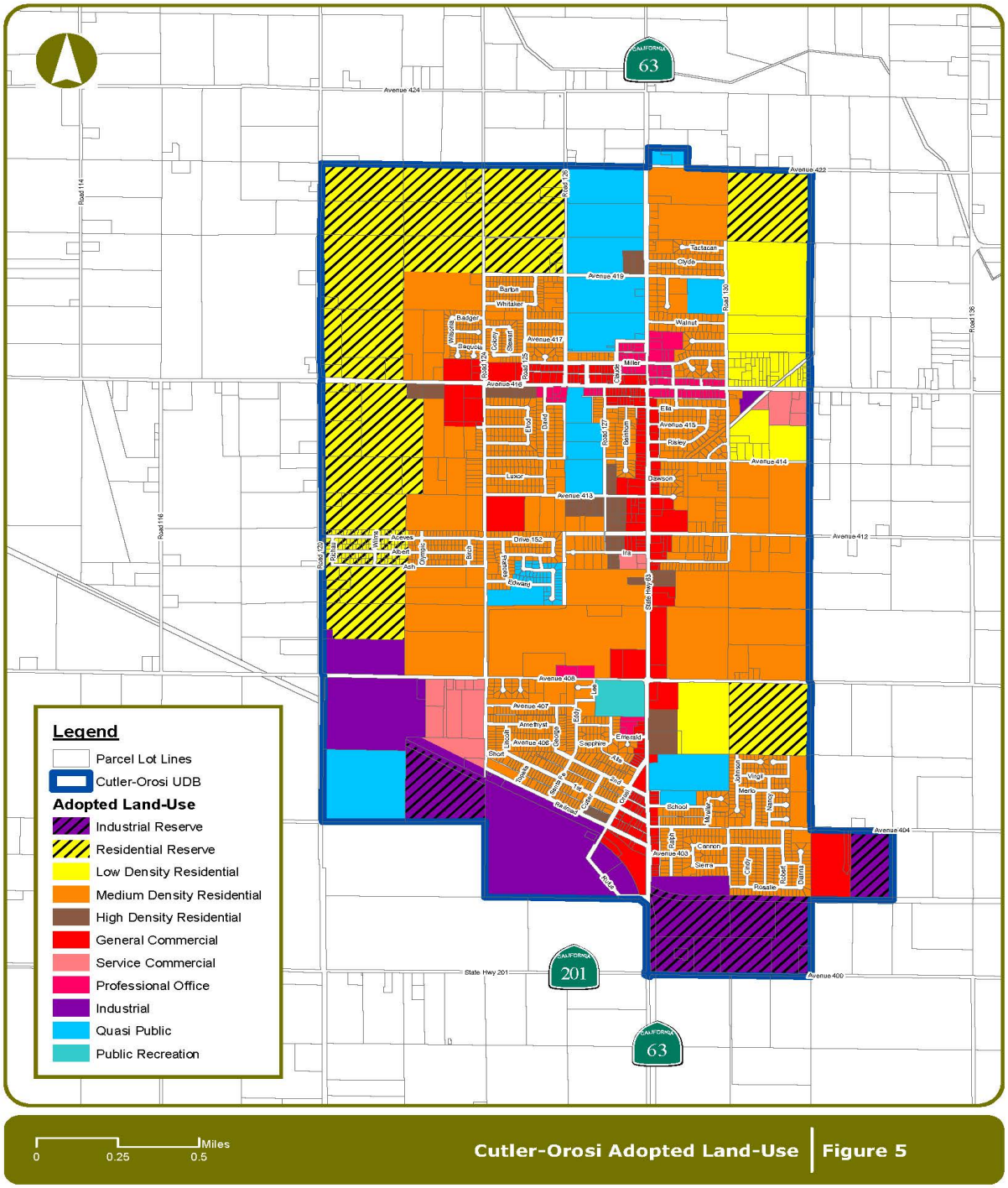
Table 1 shows that a majority of the land in the 1988 adopted Cutler-Orosi Community Plan (As Amended) area is designated Residential (1,014 acres). In total, there is about 2,412.3 acres of designated lands in the Cutler-Orosi Community Planning Area **(see Figure 5)** and approximately 231.3 acres within the plan area is dedicated to rights-of-way.

Table 1 Cutler-Orosi Adopted Land Use Plan 1988

Designation	Total Acreage	Percentage
General Commercial	140.4	5.74
High Density Residential	38.7	1.58
Industrial	168.3	6.89
Industrial Reserve	135.0	5.53
Low Density Residential	125.1	5.12
Medium Density Residential	850.2	34.82
Professional Office	28.6	1.17
Public Recreation	11.9	0.49
Quasi-Public	201.1	8.23
Residential Reserve	459.9	18.83
Service Commercial	42.8	1.75
(blank)	8.3	0.34
Unclassified (Right-of-Way)	231.3	9.47
Total	**2,441.9**	**100**

Source: Tulare County GIS

Figure 5 – Cutler-Orosi Adopted Land Use Plan Map



Existing Zoning Districts

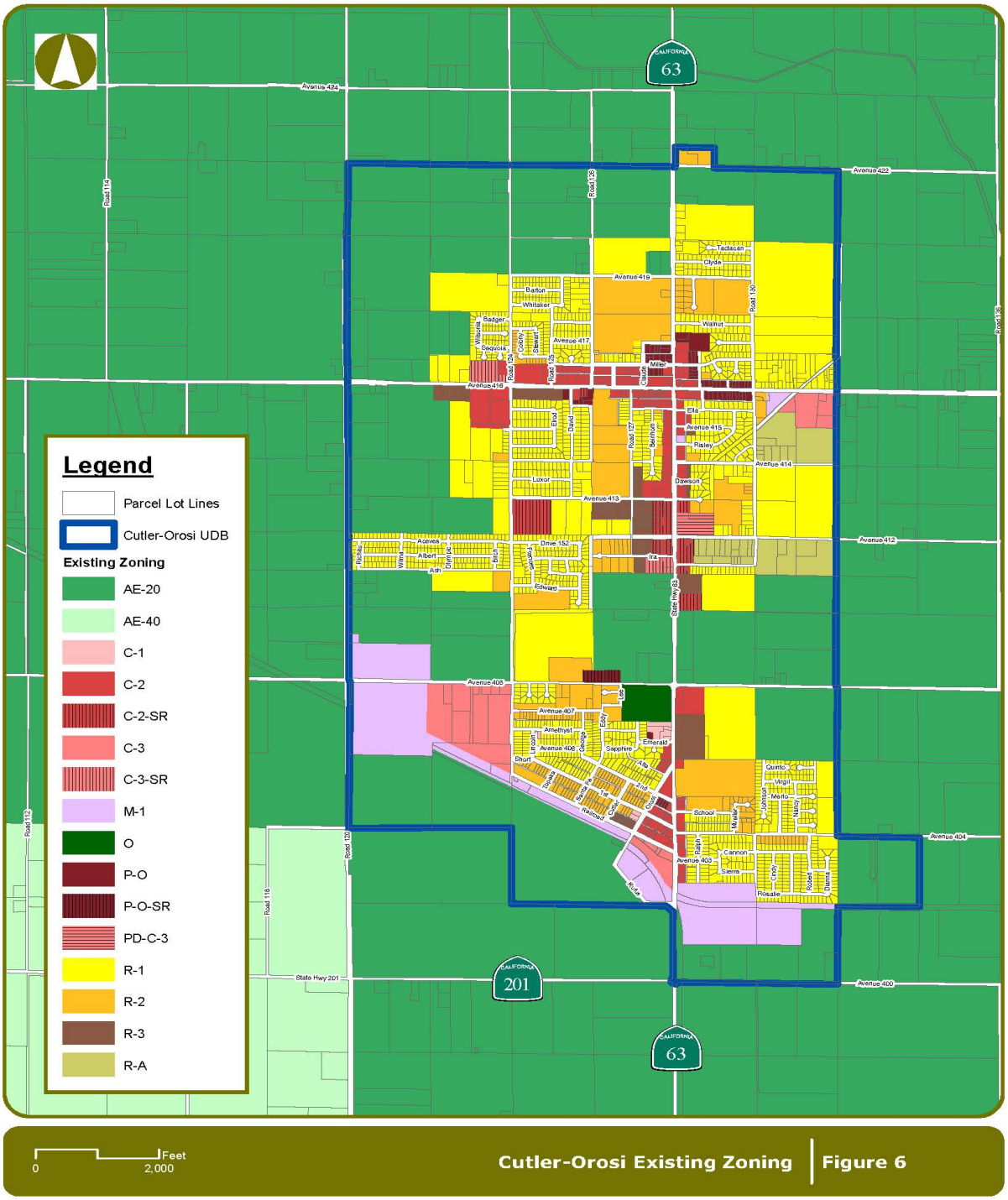
The Zoning designations within the existing 1988 Cutler-Orosi Community Plan Update are shown in **Figure 6**, as demonstrated in **Table 2**.

Table 2 Existing Zoning Districts

Zoning Designations	Existing Acres	Percent
AE-20	956.9	39.1
C-1	3.5	0.1
C-2	69.8	2.8
C-2-SR	23.2	0.9
C-3	55.1	2.2
C-3-SR	7.5	0.3
M-1	130.8	5.3
O	11.9	0.4
PD-C-3	5.4	0.2
P-O	16.6	0.6
P-O-SR	3.1	0.1
R-1	644.3	26.3
R-2	189.1	7.7
R-3	36.3	1.4
R-A	50.3	2.0
Z	6.8	0.2
Unclassified (Right-of-Way)	231.3	9.4
Total	**2,441.9**	**100**

Source: Tulare County GIS

Figure 6 - Cutler-Orosi Existing Zoning Districts



Demographics

Introduction

An important part of planning is having information that describes the characteristics of a Community's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in the Community plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Community allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Community. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of a smaller community such as Cutler-Orosi.

Historic Population Growth

The rate of population growth over a 20-year period, 1960 - 1980, in the unincorporated County and Cutler-Orosi grew from 3,239 to 7,225 persons, an average annual growth rate of 6.1 percent.

Recent Population Growth

In 2000, Cutler-Orosi's population was 11,809. The population increased to 13,610 by 2017. The male population increased from 6,371 in 2000 to 7,313 in 2017. The female population increased from 5,438 in 2000 to 6,297 in 2017 **(see Table 3)**.

Table 3 Population (2000 and 2017)

	2000			2017		
Geography	**Total Population (2000)**	**Male (2000)**	**Female (2000)**	**Total Population (2017)**	**Male (2017)**	**Female (2017)**
California	33,871,648	16,874,892	16,996,756	39,982,847	19,366.579	19,616,268
Tulare County	368,021	184,010	184,011	458,809	229,488	229,321
Cutler CDP	4,491	2,508	1,983	5,850	3,291	2,559
Orosi CDP	7,318	3,863	3,455	7,760	4,022	3,738
Cutler Percentage	-	55.8	44.2	-	51.8	48.2
Orosi Percentage	-	52.8	47.2	-	56.3	43.7

Source: California Department of Finance

Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County **(see Table 4)** is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."[4]

Table 4 Projected Annual Growth Rates

	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate is applied to Cutler-Orosi.

Median Age

The median age in Cutler went up from 23.5 in 2000 to 24.5 in 2017 and in Orosi from 24.6 in 2000 to 28.8 in 2017. Cutler-Orosi's median age is lower than the median age of Tulare County and of the State of California **(see Table 5)**.

Table 5 Median Age (2000 and 2017)

Geography	2000 Median age (years)	2017 Median age (years)
California	33.3	36.1
Tulare County	29.2	30.6
Cutler CDP	23.5	24.5
Orosi CDP	24.6	28.8

Source: California Department of Finance

Cutler-Orosi has a higher percentage of persons age 18 and under at 38.6% and 32.4%; respectively, than Tulare County (31.4%) and the State of California (23.4%). Cutler-Orosi also has a lower elderly population. Persons 60 years old and older made up 2.0% in Cutler and 4.1% in Orosi, comparatively, persons 60 years and older in Tulare County was 13.2% and in the State of California was 16.4% **(see Table 6)**.

Table 6 Age Percentage 2017

Geography	Persons Under 5 years	Persons Under 18 years	Persons Age 21+	Persons Age 60+	Persons Age 65+
California	6.4%	23.4%	72.4%	16.4%	13.2%
Tulare County	8.6%	31.4%	63.9%	13.2%	10.7%
Cutler CDP	8.1%	38.6%	57.4%	2.0%	5.4%
Orosi CDP	6.4%	32.4%	63.0%	4.1%	8.0%

Source: California Department of Finance

[4] Tulare County Regional Blueprint, page 7

Ethnicity and Race

In 2000 **(see Table 7)**, 34.5 % of Cutler's and 48.4% of Orosi's population were white, 0.4% for both Cutler-Orosi were African American, 1.2% for Cutler's and 0.5% for Orosi were Native American, 0.8% for Cutler and 10.2% for Orosi were Asian, and 4.5% for Cutler and 5.5% were Orosi were two races or more. Approximately 92.1% for Cutler and 77.6% for Orosi were Hispanic (of any race).

Table 7 Race and Ethnicity (2000)

	Total Populatio n	White	Hispanic or Latino (of any race)	Black or African American	American Indian and Alaska Native	Asian	Total Population of Two or More Races
California	33,871,648	20,170,059	10,966,556	2,263,882	333,346	3,697,513	1,607,646
Tulare County	368,021	213,751	186,846	5,852	5,737	12,018	16,938
Cutler CDP	4,491	1,547	4,136	17	53	37	204
Orosi CDP	7,318	2,153	5,679	26	39	747	406
Cutler % of Total	-	34.5%	92.1%	0.4%	1.2%	0.8%	4.5%
Orosi % of Total	-	48.4%	77.6%	0.4%	0.5%	10.2%	5.5%

Source: California Department of Finance

In 2017 **(see Table 8)**, 1.6% of Cutler's and 3.3% of Orosi's population were white. Approximately 98.8% of Cutler and 85.58% of Orosi were Hispanic (of any race). Between 2000 and 2017, the proportion of the White population declined in both Cutler-Orosi; from 34.5% to 1.6% and in Cutler and from 48.4% to 3.3% in Orosi. During this time, the African American population declined in both Cutler and Orosi. The Asian population percentage increased in Orosi from 0.8% to 10.6% and declined in Cutler from 0.8% to 0.0%. The two or more race demographic declined in both communities from 4.5% to 0.0% in Cutler and 5.5% to 0.6% in Orosi. The Hispanic (of any race) increased from 92.1% to 98.8% in Cutler and increased from 77.6% to 85.5% in Orosi.

Table 8 Race and Ethnicity (2017)

	Total Population	White	Hispanic or Latino (of any race)	Black or African American	American Indian and Alaska Native	Asian	Total Population of Two or More Races
California	38,982,847	14,777,594	15,105,806	2,161,459	117,813	5,427,928	1,140,164
Tulare County	458,809	135,372	291,867	5,973	3,029	14,622	6,709
Cutler CDP	5,850	94	5,756	0	0	0	0
Orosi CDP	7,760	255	6,632	0	0	826	47
Cutler % of Total	-	1.6%	98.8%	0.0%	0.0%	0.0%	0.0%
Orosi % of Total	-	3.3%	85.5%	0.0%	0.0%	10.6%	0.6%

Source: California Department of Finance

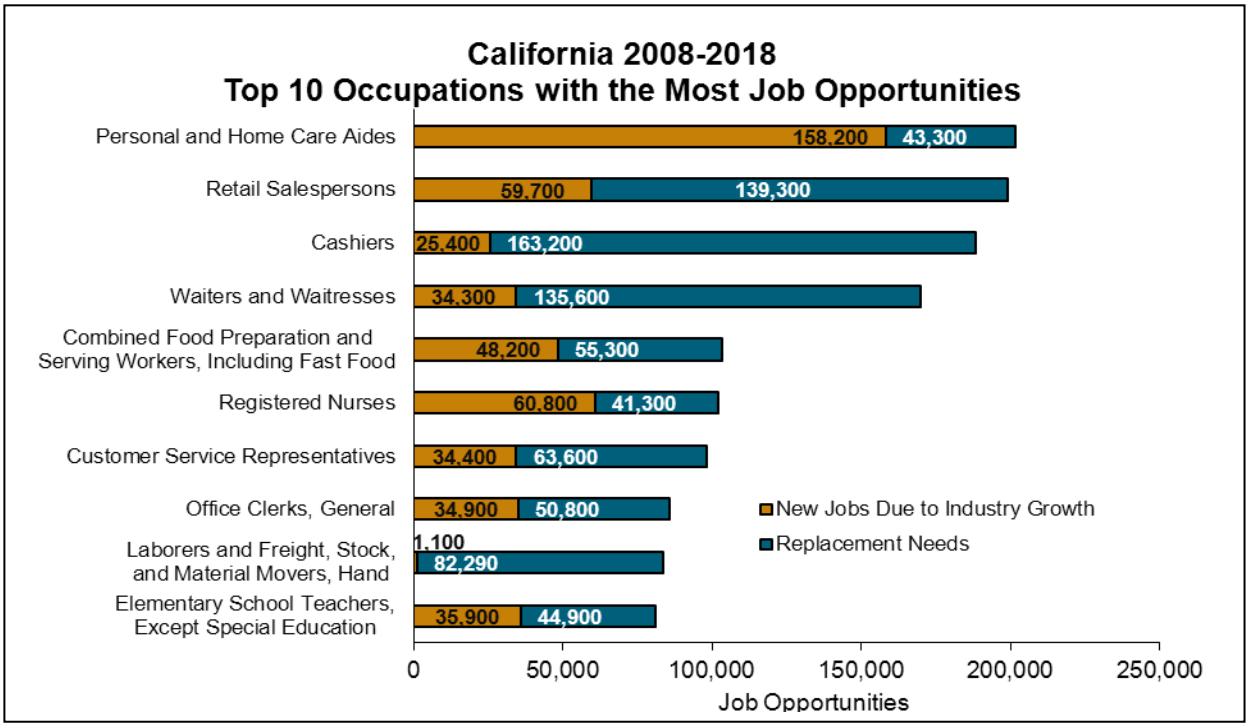
Economic Conditions

Employment Projections California

"By the end of the 2008-2018 projection period, total nonfarm employment in California is projected to grow to nearly 16.5 million jobs. This exceeds peak job level of just over 15.2 million jobs reached before the Great Recession by over 1.2 million jobs. From June 2007 to June 2009, 1.1 million jobs were lost (not seasonally adjusted). Over the 2008-to-2018 projections period, nonfarm employment is expected to rebound by 1,511,100 jobs as the economy recovers from these recessionary job losses. More than 50 percent of all projected nonfarm job growth is in education services (private), health care, social assistance, and professional and business services. The largest number of new jobs is expected in education services, health care, and social assistance, with a gain of more than 421,000 jobs.

Factors fueling the economic recovery in California include the state's population growth and a rise in foreign imports and exports… The state's population increased by more than 3.3 million from 2000 to 2010 and the California Department of Finance projects the population will increase by another 4.3 million from 2010 to 2020. A steady increase in foreign imports and exports has strengthened the wholesale, retail, and transportation industry sectors."[5]

[5] California Labor and Market and Economic Analysis, 2012, page 27



Source: California Employment Development Dept., California Labor and Market and Economic Analysis, 2012

Tulare County's Local Economy

Similar to the broader Central Valley area, Tulare County's economy has been largely based on agriculture, food processing, and manufacturing, while professional services jobs have been limited. Tulare is the second most productive agricultural county in a State that itself is by far the most productive in the nation. Overall, agribusinesses produced $6 billion in commodities in 2016 with the County considered one of the largest milk producers in the United States."[6]

Tulare County is also a major distribution hub because of its central location in the State, 200 miles north of Los Angeles and 225 miles south of San Francisco. The County's employment base has been significantly impacted by the recent downturn with unemployment increasing to 18.3 percent in January 2010, significantly above the historic range of between 8.5 and 18.2 since 1990. In 2008, the median household income was approximately $44,000.

The county's major employers are Tulare County government, Porterville Development Center, both (Kaweah Delta Healthcare, and Ruiz Food Products). The top 20 employers combine for about 19,300 jobs, or 11 percent of the overall county employment. The major distributors include Jo-Ann Fabrics, VF Distribution, Wal-Mart, and Best Buy Electronics that combine for nearly 3.5 million square feet of distribution space. The county's overall industrial market includes about 23 million square feet of building space."[7]

[6] 2017 Tulare County annual Crop and Livestock Report
[7] Visalia General Plan Update: Existing Conditions Report, page 3-16

Unemployment in Tulare County

According to the 2030 Update of the Tulare County General Plan, Tulare County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Nearly 20% of the employment in Cutler-Orosi is agriculturally related according to the Tulare County Housing Element. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production.

According to the California Department of Finance, the 2013-2017 American Community Survey **(see Table 9)** indicated that the unemployment rate in Cutler was 13.1% and Orosi had an unemployment rate of 20.0% while Tulare County's unemployment rate was 10.0%. The State of California's unemployment rate was 7.7%.

Table 9 American Community Survey: Unemployment 2013-2017

Geography	Population	Total Civilian Labor Force	Unemployment
California	38,982,847	19,485,061	7.7%
Tulare County	458,809	193,225	10.0%
Cutler CDP	5,850	2,208	13.1%
Orosi CDP	7,760	3,334	20.0%

Source: California Department of Finance

Income

Mean and Median income **(see Table 10)** in Cutler-Orosi is very low compared to Tulare County and the State of California. Average median household income for Cutler was $31,939 and Orosi was $35,798 compared to $44,871 for Tulare County and $67,169 for the State of California.

Table 10 - 2013-2017 American Community Survey: Income

Geography	Median household income (dollars)	Mean household income (dollars)	Median family income (dollars)	Mean family income (dollars)	Per capita income (dollars)
California	$67,169	$96,104	$76,975	$106,970	$33,128
Tulare County	$44,871	$62,325	$47,280	$65,927	$18,927
Cutler CDP	$31,939	$36,990	$30,760	$32,501	$8,436
Orosi CDP	$35,798	$46,444	$41,379	$40,839	$12,163

Source: California Department of Finance

The mean family income for Cutler was $32,501 and Orosi was $40,839 compared to $65,927 for Tulare County and $106,970 for the State of California. Average per capita income for Cutler was also low at $8,436 and Orosi was $12,163 compared to $18,927 for Tulare County and $33,128 for the State of California.

Poverty

According to the California Department of Finance, the 2013-2017 American Community Survey **(see Table 11)** indicated that 39.7% of all families living in Cutler lived below the poverty line and 24.3% of all families in Orosi lived below the poverty line. For all people Cutler (47.5%) and Orosi (25.7%) had a higher level of poverty compared to Tulare County at 27.1% and the State of California at 15.1%. The highest differential was the poverty rate of persons under 18 years. Poverty rate for persons under 18 years for Cutler was 61.6% and Orosi was 46.0% compared to 36.2% for Tulare County and 20.8% for the State of California.

Table 11 - 2013-2017 American Community Survey: Poverty

Geography	All families	Married couple families	Families with female householder, no husband present	All people	Persons under 18 years
California	11.1%	6.6%	26.0%	15.1%	20.8%
Tulare County	23.0%	15.4%	42.2%	27.1%	36.2%
Cutler CDP	39.7%	27.7%	52.0%	47.5%	61.6%
Orosi CDP	24.3%	21.3%	29.9%	25.7%	46.0%

Source: California Department of Finance

Housing Characteristics

Housing Units

During the ensuing years between 2000 and 2017 **(see Table 12)**, the number of housing units in Cutler increased from 973 to 1,293, and in Orosi increased from 1,741 to 2,076. This represents an increase of 32.9% for Cutler, and 19.2% for Orosi.

Table 12 - Housing Units (2000 & 2017)

Geography	2000 Total housing units	2017 Total housing units	Percent Increase
California	13,680,081	13,996,299	2.3%
Tulare County	141,696	146,712	3.5.%
Cutler CDP	973	1,293	32.9%
Orosi CDP	1,741	2,076	19.2%

Source: California Department of Finance

Housing Types

According to the California Department of Finance, the 2013-2017 American Community Survey (**see Table 13)** indicated that 69.6% of Cutler housing units and 80.0 of Orosi; respectively, were 1-unit detached. In Tulare County 45.4% of the housing units were 1-unit detached, and in California 0.6% of housing units were 1-unit detached.

Table 13 - 2013-2017 American Community Survey: Unit Types				
	California	**Tulare County**	**Cutler CDP**	**Orosi CDP**
Total housing units	13,996,299	146,712	1,293	2,076
1-unit detached	8,131,716	110,555	900	1,661
%	0.6%	45.4%	69.6%	80.0%
1-unit attached	978,110	3,866	26	28
%	1.5%	2.6%	2.0%	1.3%
2 units	343,548	4,084	153	106
%	11.5%	2.8%	11.8%	5.1%
3 or 4 units	775,541	8,342	146	68
%	10.9%	5.7%	11.3%	3.3%
5 to 9 units	857,711	4,084	47	58
%	6.1%	2.8%	3.6%	2.8%
10 to 19 units	728,840	1,667	21	55
%	5.2%	1.1%	1.6%	2.6%
20 or more units	1,647,167	4,027	0	15
%	11.8%	2.7%	0.0%	0.7%
Mobile home	518,818	9,931	0	85
%	3.7%	6.8%	0.0%	4.1%

During the ensuing years between 2010 and 2017 **(see Table 14)**, the home ownership percentage in California decreased by approximately 2.67%. In Tulare County, that percentage decreased by approximately 4.74%. In Cutler, the homeownership percentage decreased by approximately 8.0% and increased by 15.6% in Orosi. While the average household size for both owner- and renter-occupied units decreased in the State of California, Cutler, and Orosi; Tulare County's average household size increased for owner-occupied units but decreased for renter-occupied units.

Table 14 - Ownership and Household Size (2010 & 2017)						
	2010			**2017**		
Geography	**Percent Ownership**	**Average household size of owner-occupied units**	**Average household size of renter-occupied units**	**Percent Ownership**	**Average household size of owner-occupied units**	**Average household size of renter-occupied units**
California	56%	2.95	2.83	54.5%	3.00	2.91
Tulare County	59%	3.24	3.52	56.2%	3.27	3.46
Cutler CDP	43.7%	4.51	4.69	40.2%	3.68	5.34
Orosi CDP	56.3%	4.41	4.43	65.1%	3.91	3.82

Source: California Department of Finance

Housing Conditions

According to the 2015 Cutler Community Housing Condition Survey, approximately 15% of the housing stock in Cutler **(see Table 15)** was sound and 75% of the housing stock were deteriorated and in need of replacing. Conversely, 87% of Orosi's housing stock **(see Table 16)** is sound, and only 15% is deteriorated or dilapidated.

Table 15 - Housing Conditions Survey (Cutler)

Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Cutler	43	15	35	12	162	57	18	6	24	9	282

Source: Tulare County 2015 Housing Condition Survey, Tulare County 2015 Housing Element

Table 16 - Housing Conditions Survey (Orosi)

Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Orosi	482	87	17	3	14	3	9	2	31	6	553

Source: Tulare County 2015 Housing Condition Survey, Tulare County 2015 Housing Element

The percentage of substandard housing in Cutler-Orosi increased between 1992 and 2015. The percentage was 30% in 1992, 14% in 2003, 17% in 2009, and 76% in 2015 **(see Table 17)**.

Table 17 - Percentages of Substandard Housing Units in Tulare County Unincorporated Community 1992-2015

	1992 Survey Results	2003 Survey Results	2009 Survey Results	2015 Survey Results
Cutler-Orosi	30	14	17	76

Source: 1992, 2003, 2009, 2015 Tulare County Housing Survey of Unincorporated Communities, 2015 Housing Element

Age of Structures

According to the US Census, the 2013-2017 Community Survey **(see Tables 18 and 19)** noted that 24.9% of the housing structures in Cutler were built between 1980 and 1989 and 26.4% of housing structures in Orosi were built between 1960 and 1969.

Table 18 - 2013-2017 American Community Survey: Age of Structures in Cutler

Age of Structures	Number	Percentage
Built 2014 or later	0	0.0%
Built 2010 to 2013	25	1.9%
Built 2000 to 2009	214	16.6%
Built 1990 to 1999	121	9.4%
Built 1980 to 1989	322	24.9%
Built 1970 to 1979	244	18.9%
Built 1960 to 1969	141	10.9%
Built 1950 to 1959	74	5.7%
Built 1940 to 1949	83	6.4%
Built 1939 or earlier	69	5.3%
Total:	1,293	-

Source: US Census

Table 19 - 2013-2017 American Community Survey: Age of Structures in Orosi

Age of Structures	Number	Percentage
Built 2014 or later	0	0.0%
Built 2010 to 2013	0	0.0%
Built 2000 to 2009	277	13.3%
Built 1990 to 1999	402	19.4%
Built 1980 to 1989	95	4.6%
Built 1970 to 1979	342	16.5%
Built 1960 to 1969	548	26.4%
Built 1950 to 1959	172	8.3%
Built 1940 to 1949	59	2.8%
Built 1939 or earlier	181	8.7%
Total:	2,076	-

Source: US Census

Household Size (Overcrowding)

In 2017 the average owner occupied household size in Cutler was 3.68 and Orosi was 3.91 persons per household **(see Table 20)** and the average renter household size in Cutler was 5.34 and in Orosi was 3.82. By definition, the most common measure of overcrowding is persons per room in a dwelling unit.[8] More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

Table 20 Average Household Size

Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.27	3.46
Cutler	3.68	5.34
Orosi	3.91	3.82

2013-2017 American Community Survey 5-Year Estimates

[8] U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf

Vacancy Rate

In 2000, the vacancy rate **(see Table 21)** in Cutler was 6.6% and Orosi was 3.6%, which was lower than Tulare County at 7.7% and higher than the State of California at 5.8%. In 2010, the vacancy rate in Cutler was 4.5% and Orosi was 4.1%, which is lower than Tulare County at 8.0% and the

Table 21 - Vacancy Rate (2000 &2010)

Geography	2000			2010		
	Vacancy rate	Homeowner vacancy rate	Rental vacancy rate	Vacancy rate	Homeowner vacancy rate	Rental vacancy rate
California	5.8%	1.4%	10.7%	8.1%	2.1%	6.3%
Tulare County	7.7%	1.8%	5.8%	8.0%	2.4%	5.8%
Cutler CDP	6.6%	0.6%	2.4%	4.5%	1.0%	4.0%
Orosi CDP	3.6%	0.3%	5.5%	4.1%	2.6%	3.7%

Source: California Department of Finance

State of California at 8.1%. While the State of California's rental vacancy rate decreased from 10.7% to 6.3%, the rental vacancy rate in Cutler increased from 2.4% to 4.0% and Orosi decreased from 5.5% to 3.7% between 2000 and 2010. While Tulare County's rental vacancy rate remained at 5.8% during this decade.

Regional Housing Needs Assessment (RHNA)

"State housing element law assigns the responsibility for preparing the Regional Housing Needs Assessment (RHNA) for the Tulare County region to the Tulare County Association of Governments (TCAG). The RHNA is updated prior to each housing element cycle. The current RHNA, [adopted on June 30, 2014] is for the fifth housing element cycle and covers a 9.75-year projection period (January 1, 2014 – September 30, 2023). The Regional Housing Needs Plan (RHNP) describes the methodology developed to allocate the region's housing needs in four income categories (very low, low, moderate, and above moderate) among Tulare County's eight cities and the unincorporated county in accordance with the objectives and factors contained in State law."[9]

The growth projections applied in the Tulare County Housing Element Update are based upon growth projections developed by the State of California. A "Regional Housing Needs Assessment Plan" provides a general measure of each local jurisdiction's responsibility in the provision of housing to meet those needs. The TCAG was responsible for allocating the State's projections to each local jurisdiction within Tulare County including the County unincorporated area, which is reflected in the Housing Element.

"The Sustainable Communities and Climate Protection Act of 2008 (SB 375) was passed to support the State's climate action goals…to reduce greenhouse gas (GHG) emissions through coordinated transportation and land use planning. The bill mandates each of California's Metropolitan Planning

[9] TCAG. Final Regional Housing Needs Plan for Tulare County 2014-2023. Page i. Accessed July 2021 at: http://www.tularecog.org/wp-content/uploads/2015/07/Final-Regional-Housing-Needs-Plan-for-Tulare-County-2014-2023.pdf

Organizations (MPO) to prepare a sustainable communities strategy as part of its regional transportation plan (RTP). The SCS contains land use, housing and transportation strategies that, if implemented, would allow the region to meet its GHG reduction targets."[10]

"In the past, the RHNA was undertaken in dependently from the RTP. SB 375 requires that the RHNA and RTP/SCS processes be undertaken together to better integrate housing, land use, and transportation planning. The law recognizes the importance of planning for housing and land use in creating sustainable communities where residents of all income levels have access to jobs, services, and housing using transit, or by walking and bicycling."[11]

"In addition to the RHNA requirements, SB 375 requires that TCAG address the region's housing needs in the SCS of the RTP, to include sections on state housing goals (Government Code Section 65080(b)(2)(B)(vi)); identify areas within the region sufficient to house all the population of the region (including all economic segments of the population) over the course of the planning period for the RTP (out to 2040 for the 2040 RTP/SCS); and identify areas within the region sufficient to meet the regional housing needs."[12]

The RHNA housing results are summarized in **Figure 7A**. The Tulare County RHNA Plan recommends that the County provide land use and zoning for approximately 7,081 units per year in the unincorporated portions of the County. The County administratively agreed to a housing share of 7,081 units (726 units per year over the 9.75-year RHNA planning period). The RTP allocates 30% of population to the County. The RHNA bases the housing needs assessment on this percentage.

[10] Ibid. 5.
[11] Ibid.
[12] Op. Cit.

Figure 7A - RHNA 2014-2023

Tulare County Region January 1, 2014 to September 30, 2023								
Jurisdiction	1/1/2014 Housing Unit Control Totals	2023 Housing Unit Control Totals	Estimated 9/30/2023 Housing Units		2024 Housing Unit Control Totals	Housing Units 9/30/2023 (Based on Allocation of 26,910 units)		
			Housing Units	Percent of Total		Total Housing Units	Percent of Total	Net New Housing Units 1/1/2014-9/30/2023
	A	B	C	D	E	F	G	H
Dinuba	6,223	7,106	7,186	4.05%	7,212	7,188	4.05%	965
Exeter	3,803	4,305	4,426	2.50%	4,365	4,428	2.50%	625
Farmersville	2,878	3,253	3,343	1.89%	3,298	3,344	1.89%	466
Lindsay	3,384	3,858	3,972	2.24%	3,914	3,974	2.24%	590
Porterville	17,764	20,331	20,952	11.82%	20,639	20,960	11.82%	3,196
Tulare	20,022	22,908	23,606	13.32%	23,255	23,616	13.32%	3,594
Visalia	47,380	55,411	57,379	32.37%	56,386	57,401	32.37%	10,021
Woodlake	2,187	2,486	2,558	1.44%	2,521	2,559	1.44%	372
Unincorporated County	46,774	52,477	53,834	30.37%	53,151	53,855	30.37%	7,081
Total	**150,415**	**172,134**	**177,255**	**100.00%**	**174,741**	177,325	100.00%	26,910

Sources
Columns A, B, and E: TCAG 2040 Demographic Forecast (2013)
Column C: Estimated using trendline growth between Columns B and E
Column D: Column C divided by countywide total from Column C (177,225)
Column F: Proportionally scaled up from Column D to 9/30/2023 countywide total (177,325)
Column G: Column F divided by countywide total from Column F (177,325)
Column H: Column A subtracted from Column F

Also, as noted in the RHNA, "An underlying principle of the RHNA Methodology is to ensure that affordable housing is equitably distributed throughout the region. The Methodology applies an adjustment factor based on disparities in household income across the TCAG region. The adjustment factor assigns a higher proportion of units affordable to lower income households to jurisdictions that currently have a lower proportion of affordable households compared to the regional average, and assigns a lower proportion of affordable units to jurisdictions that currently have a higher proportion of affordable households than the regional average. The Methodology is intended to help the region achieve income parity (the same proportion of affordable units in each community) by 2050. Table 1 [of the RHNA, Figure 7B in the Community Plan Update] summarizes the overall allocation of units to each jurisdiction and the allocation by the four income categories."[13] It is noted that the RHNA allocation is County-wide and is not specific to Cutler-Orosi.

[13] Op. Cit. 19.

Figure 8B - RHNA 2014-2023
Allocation by Income Category

Table 1: 2014-2023 Final RHNA Allocations by Income Category
Tulare County Region
January 1, 2014 to September 30, 2023

	Total RHNA Allocation	Very Low-Income Allocation		Low-Income Allocation		Affordable Allocation (Combined Low + Very Low-Income)		Moderate-Income Allocation		Above Moderate-Income Allocation	
		Units	Percent of Total RHNA	Units	Percent of Total RHNA	Units	Percent of Total RHNA	Units	Percent of Total RHNA	Units	Percent of Total RHNA
Dinuba	965	211	21.9%	163	16.9%	374	38.8%	121	12.5%	470	48.7%
Exeter	625	143	22.9%	125	20.0%	268	42.9%	85	13.6%	272	43.5%
Farmersville	466	74	15.9%	65	13.9%	139	29.8%	68	14.6%	259	55.6%
Lindsay	590	80	13.6%	80	13.6%	160	27.1%	82	13.9%	348	59.0%
Porterville	3,196	623	19.5%	576	18.0%	1,199	37.5%	566	17.7%	1,431	44.8%
Tulare	3,594	920	25.6%	609	16.9%	1,529	42.5%	613	17.1%	1,452	40.4%
Visalia	10,021	2,616	26.1%	1,931	19.3%	4,547	45.4%	1,802	18.0%	3,672	36.6%
Woodlake	372	71	19.1%	41	11.0%	112	30.1%	69	18.5%	191	51.3%
Unincorporated County	7,081	1,477	20.9%	1,065	15.0%	2,542	35.9%	1,169	16.5%	3,370	47.6%
Total	**26,910**	**6,215**	**23.1%**	**4,655**	**17.3%**	**10,870**	**40.4%**	**4,575**	**17.0%**	**11,465**	**42.6%**

Note: Percentages may not sum to 100 percent due to rounding

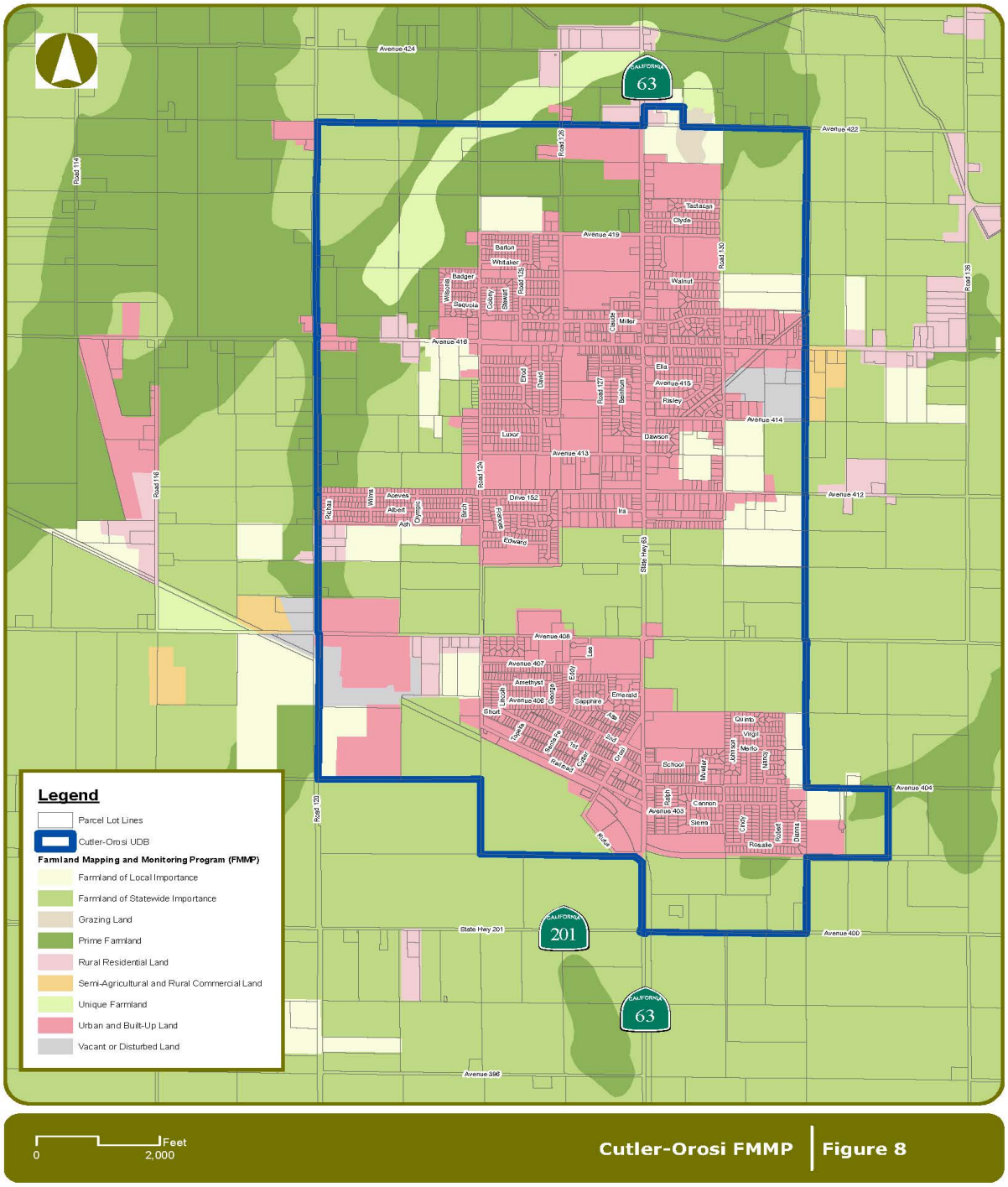
Natural and Cultural Resources

Agriculture

The California Department of Conservation, Division of Land Resource Protection, maintains the Farmland Mapping and Monitoring Program (FMMP), which monitors the conversion of the state's farmland to and from agricultural use. The program monitors a wide variety of farmland types: Prime Farmland is farmland with the best combination of physical and chemical soil features to sustain long-term agricultural production; Farmland of Statewide Importance is Prime Farmland but has minor shortcomings, such as greater slopes or less ability to store soil moisture; and Unique Farmland has lesser quality soils used for the production of the state's leading agricultural crops.

The area within the existing 2,441.9-acre UDB is designated in the 2017 FMMP maps **(see Figure 8)**. Of these, approximately 1,246.9 acres are designated Urban and Built-up Land, approximately 956.9 acres are designated Prime Farmland.

Figure 9 - FMMP Map



0 Feet 2,000

Cutler-Orosi FMMP | **Figure 8**

Williamson Act Lands

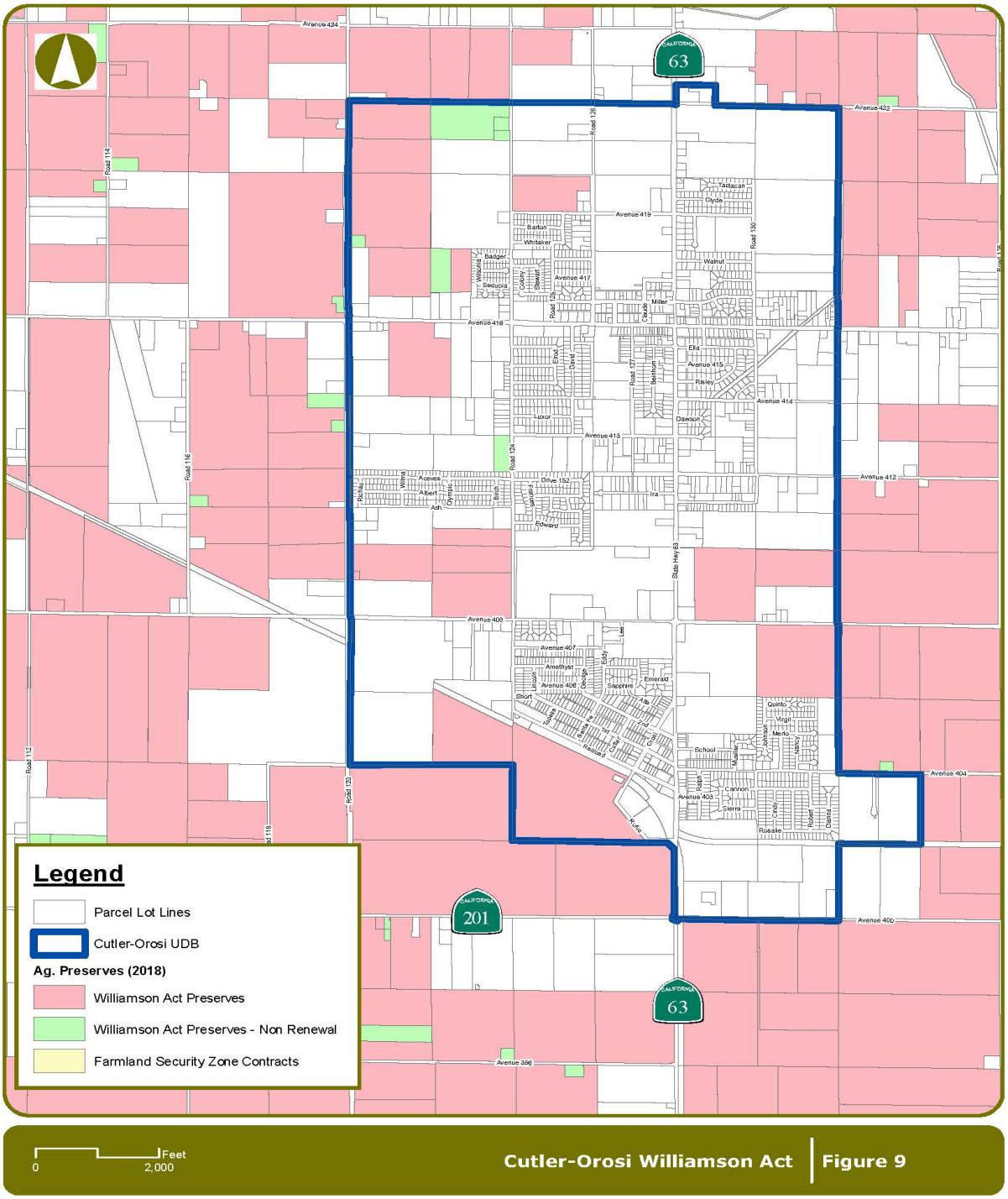
Agricultural land is a resource that must be conserved just like air and water resources. It is also economically important and provides other benefits such as wildlife habitat, groundwater recharge, and open space, which contributes to the rural character of the area.

The importance of agricultural land is underscored by the level of attention state planning law has placed on it. Three mandatory elements of the general plan: 1) land use; 2) open space; and 3) conservation, all require local governments to include a discussion of agricultural lands in their general plans. The County's planning policies also underscore agricultural land importance to the local economy and environment as well.

Within the area, there are several farms in the Williamson Act. The Williamson Act is designed to keep productive farmland of a contract, that the land will not be converted to an urban type use. Land in the Williamson Act is required to remain in agriculture for a 10-year period. Unless a notice of non-renewal is filed, the contract is automatically self-renewing every year for an additional 10-year period.

Fifteen (15) parcels in Cutler-Orosi have entered into land conservation contracts and **(see Figure 9)** are subject to the Williamson Act and within the planning area (proposed UDB)

Figure 10 - Williamson Act Map



Air Quality

The Cutler-Orosi Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State 0_3 1-hour standard, non-attainment for the State 0_3 8-hour standard, non-attainment for the State PM_{10} standard, non-attainment for the federal and State $PM_{2.5}$ standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NO_x emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

The Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies
- Adopt Local Air Quality Mitigation Fee Program
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps
- Adopt ordinance limiting wood burning appliances/fireplace installations
- Fugitive dust regulation enforcement coordinated with SJVUAPCD
- Energy efficiency incentive programs
- Local alternative fuels programs
- Coordinate location of land uses to separate odor generators and sensitive receptors

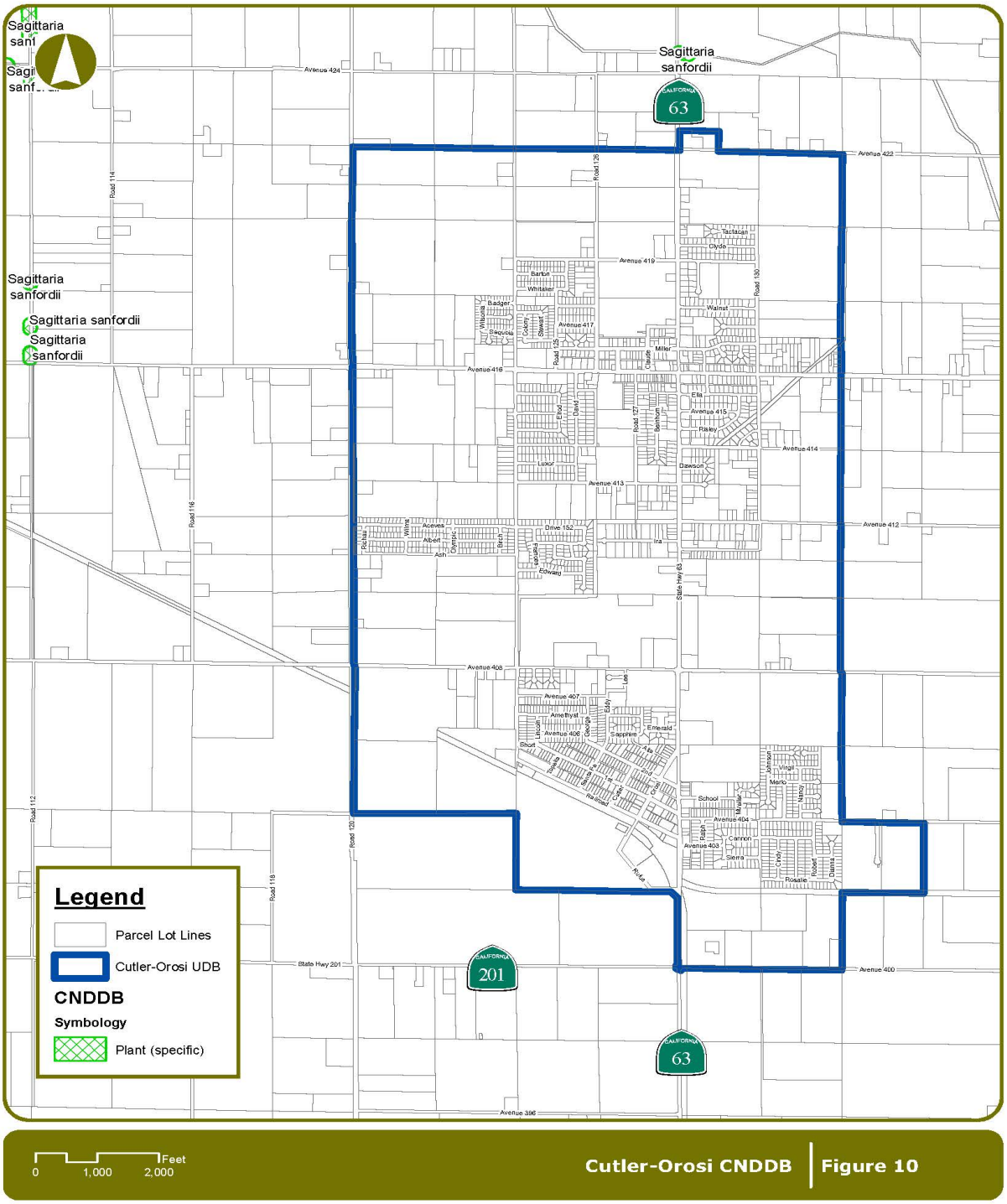
Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Cutler-Orosi is located near the southern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of Cutler-Orosi and within the community itself include SR 63, approaching and departing jet aircraft, and industrial firms emitting dust and odors, and agricultural activities. Dust and odors are concerns of residents within the area, particularly from nearby agricultural operations.

Biological Resources

Cutler-Orosi is situated within a matrix of agricultural lands, industrial complexes, and residential/commercial development. A California Natural Diversity Database (CNDDB) search conducted on July 25, 2018, **(see Figure 10)** indicated there are special status species within the Orange Cove South Quadrant Species List (which includes the Cutler-Orosi Planning Area) consisting of three animal species and one plant species: California tiger salamander (Ambystoma californiense, Federal and State threatened); vernal pool fairy shimp (Branchinecta lynchi, Federal Threatened); and vernal pool tadpole shrimp (Lepidurus packardi Federally endangered); and San Joaquin Valley adobe sunburst (Pseudobahia peirsonii, Federal Threatened and State Endangered).

Figure 11 - Cutler-Orosi CNDDB Map



Cultural Resources

"Tulare County lies within a culturally rich province of the San Joaquin Valley. Studies of the prehistory of the area show inhabitants of the San Joaquin Valley maintained fairly dense populations situated along the banks of major waterways, wetlands, and streams. Tulare County was inhabited by aboriginal California Native American groups consisting of the Southern Valley Yokuts, Foothill Yokuts, Monache, and Tubatulabal. Of the main groups inhabiting the Tulare County area, the Southern Valley Yokuts occupied the largest territory."[14]

"California's coast was initially explored by Spanish (and a few Russian) military expeditions during the late 1500s. However, European settlement did not occur until the arrival into southern California of land-based expeditions originating from Spanish Mexico starting in the 1760s. Early settlement in the Tulare County area focused on ranching. In 1872, the Southern Pacific Railroad entered Tulare County, connecting the San Joaquin Valley with markets in the north and east. About the same time, valley settlers constructed a series of water conveyance systems (canals, dams, and ditches) across the valley. With ample water supplies and the assurance of rail transport for commodities such as grain, row crops, and fruit, a number of farming colonies soon appeared throughout the region."[15]

"The colonies grew to become cities such as Tulare, Visalia, Porterville, and Hanford. Visalia, the County seat, became the service, processing, and distribution center for the growing number of farms, dairies, and cattle ranches. By 1900, Tulare County boasted a population of about 18,000. New transportation links such as SR 99 (completed during the 1950s), affordable housing, light industry, and agricultural commerce brought steady growth to the valley. The California Department of Finance estimated the 2007 Tulare County population to be 430,167."[16]

On October 23, 2018, the Southern San Joaquin Valley Information Center, Bakersfield (SSJVIC) conducted a cultural resources records search at the request of RMA Planning Branch staff. According to the information provided by the SSJVIC, there have been 17 previous cultural resource study conducted within the project area and no additional studies conducted within the one-half mile radius. However, until the specific location of a development proposal occurs, the locations and nature of the resources will remain confidential and will only be shared with an applicant and remain confidential until otherwise determined by the courts.

There are two (2) recorded cultural resources within the project area. There is one recorded resource within the one-half mile resource. These resources consist of two historic era buildings and one historic era canal. The Orosi Branch Library has been given a National Register status code of 1S, indicating the individual property has been listed in the National Register of Historic Places by the Keeper. It is also listed in the California Register of Historical Resources. There are no recorded cultural resources within the project area or radius that are listed in the National Register of Historic Places, the California Register of Historical Resources, the California Points of Historical Interest, California Inventory of Historic Resources, or the California State Historic Landmarks.

[14] Tulare County 2030 General Plan. Page 8-5.
[15] Ibid. Page 8-5.
[16] Op. Cit. Page 8-6.

Orosi Branch Library located at El Monte Way/Avenue 416 across from Eddy Road, just west of SR 63, was built in 1921 with a $3,000 grant from the Carnegie Foundation; while the foundation had issued the grant in 1917, construction was held up for four years by World War I. The wood frame library is a California bungalow, a plain style. The Orosi Branch Library was added to the National Register of Historic Placed on August 25, 1983.

Native American Consultation

The Native American Heritage Commission (NAHC) was contacted on October 8, 2018 with a request that they conduct a sacred lands files (SLF) search. The NAHC provided the results of its SLF search dated October 18, 2018 indicating "negative results" (that is, no sacred lands are known to be located in the Lemon Cove Planning area). The following Native American tribes were contacted on October 24, 2018 in order to solicit their interest regarding tribal consultation: Kern Valley Indian Community, Kern Valley Indian Community, Santa Rosa Rancheria Tachi Yokut Tribe, Tubatulabals of Kern Valley, Tule River Indian Tribe, and Wuksache Indian Tribe/Eshom Valley Band.

Geology & Seismic Hazards

The southern San Joaquin Valley is a broad arid plain, essentially level underlain by about 28,000 feet of marine and continental strata with the sediments derived from areas now occupied by mountain ranges. "Seismicity varies greatly between the two major geologic provinces represented in Tulare County. The Central Valley is an area of relatively low tectonic activity bordered by mountain ranges on either side. The Sierra Nevada Mountains, partially located within Tulare County, are the result of movement of tectonic plates, which resulted in the creation of the mountain range. The Coast Range on the west side of the Central Valley is also a result of these forces, and the continued uplifting of Pacific and North American tectonic plates continues to elevate these ranges. The remaining seismic hazards in Tulare County generally result from movement along faults associated with the creation of these ranges."[17]

"Groundshaking is the primary seismic hazard in Tulare County because of the county's seismic setting and its record of historical activity. Thus, emphasis focuses on the analysis of expected levels of groundshaking, which is directly related to the magnitude of a quake and the distance from a quake's epicenter. Magnitude is a measure of the amount of energy released in an earthquake, with higher magnitudes causing increased groundshaking over longer periods of time, thereby affecting a

[17] General Plan Background Report, page 8-5

larger area. Groundshaking intensity, which is often a more useful measure of earthquake effects than magnitude, is a qualitative measure of the effects felt by population. The valley portion of Tulare County is located on alluvial deposits, which tend to experience greater groundshaking intensities than areas located on hard rock. Therefore, structures located in the valley will tend to suffer greater damage from groundshaking than those located in the foothill and mountain areas. However, existing alluvium valleys and weathered or decomposed zones are scattered throughout the mountainous portions of the county, which could also experience stronger intensities than the surrounding solid rock areas. The geologic characteristics of an area can therefore be a greater hazard than its distance to the epicenter of the quake."[18]

"There are three faults within the region that have been, and will be, principal sources of potential seismic activity within Tulare County. These faults are described below:

- **San Andreas Fault.** The San Andreas Fault is located approximately 40 miles west of the Tulare County boundary. This fault has a long history of activity, and is thus the primary focus in determining seismic activity within the county. Seismic activity along the fault varies along its span from the Gulf of California to Cape Mendocino. Just west to Tulare County lies the "Central California Active Area," where many earthquakes have originated.
- **Owens Valley Fault Group.** The Owens Valley Fault Group is a complex system containing both active and potentially active faults, located on the eastern base of the Sierra Nevada Mountains. The Group is located within Tulare and Inyo Counties and has historically been the source of seismic activity within Tulare County.
- **Clovis Fault.** The Clovis Fault is considered to be active within the Quaternary Period (within the past two million years), although there is no historic evidence of its activity, and is therefore classified as "potentially active." This fault lies approximately six miles south of the Madera County boundary in Fresno County. Activity along this fault could potentially generate more seismic activity in Tulare County than the San Andreas or Owens Valley fault systems. In particular, a strong earthquake on the Fault could affect northern Tulare County. However, because of the lack of historic activity along the Clovis Fault, inadequate evidence exists for assessing maximum earthquake impacts."[19]

According to the five County Seismic Safety Element[20] and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the General Plan Health and Safety (GPHSE)[21] Cutler / Orosi area is located in the "V1 zone: an area of "low" seismic risk. The San Andreas Fault is the nearest active seismic area, located approximately 60 miles to the west. The Element states that active faults do not exist in Tulare County.

Soils Characteristics

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are

[18] General Plan Background Report, page 8-7
[19] General Plan Background Report, pages 8-6 and 8-7
[20] Tulare County Association of Governments. Five County Seismic Safety Element, 1974. Page 15
[21] Faults identified in Tulare County 2030 General Plan, Figure 10-1 and on the California Geological Survey website http://www.quake.ca.gov/gmaps/FAM/faultactivitymap.html

located in Cutler-Orosi **(see Figure 11)**. The following soil types for Cutler-Orosi are provided below.

Calgro/Calgro, consists of moderately deep to a duripan, moderately well drained soils formed in alluvium derived from granitic rock source.

Crosscreek/Kai Association, is formed by the chemical and mechanical alteration of the Kai soil that originally formed in alluvium derived from granitic rock sources. The soil is well drained, and the permeability is moderately slow above the duripan due to sodicity and very slow in the duripan. The available water capacity is moderate to high and the shrink-swell potential is moderate.

Exeter loam, 0 to 2 percent slopes, consists of moderately deep to a duripan, moderately well drained soils that formed in alluvium mainly from granitic sources.

Flamen Loam is an alluvium derived mainly from granitic rock sources and is found on stream terraces. The soil has moderate shrink-swell capacity, is deep to duripan and is moderately well drained. Flamen loam is classified as prime farmland when it is irrigated and has a Class II agricultural rating. .

Greenfield sandy loam, 0 to 9 percent slopes, consists of deep, well drained soils that formed in moderately coarse and coarse textured alluvium derived from granitic and mixed rock sources. Greenfield sandy loam is located in the northwest quadrant of Orosi. It is a class I agricultural soil which is well-suited for urbanization, including buildings, streets and roads, and septic tanks.

Hanford Sandy loam, 0 to 2 percent slopes, consists of very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains and alluvial fans.

Honcut sandy loam, 0 to 2 percent slopes, consists of very deep, well drained soils that formed in moderately coarse textured alluvium from basic igneous and granitic rocks. Honcut soils are on floodplains and moderately sloping alluvial fans.

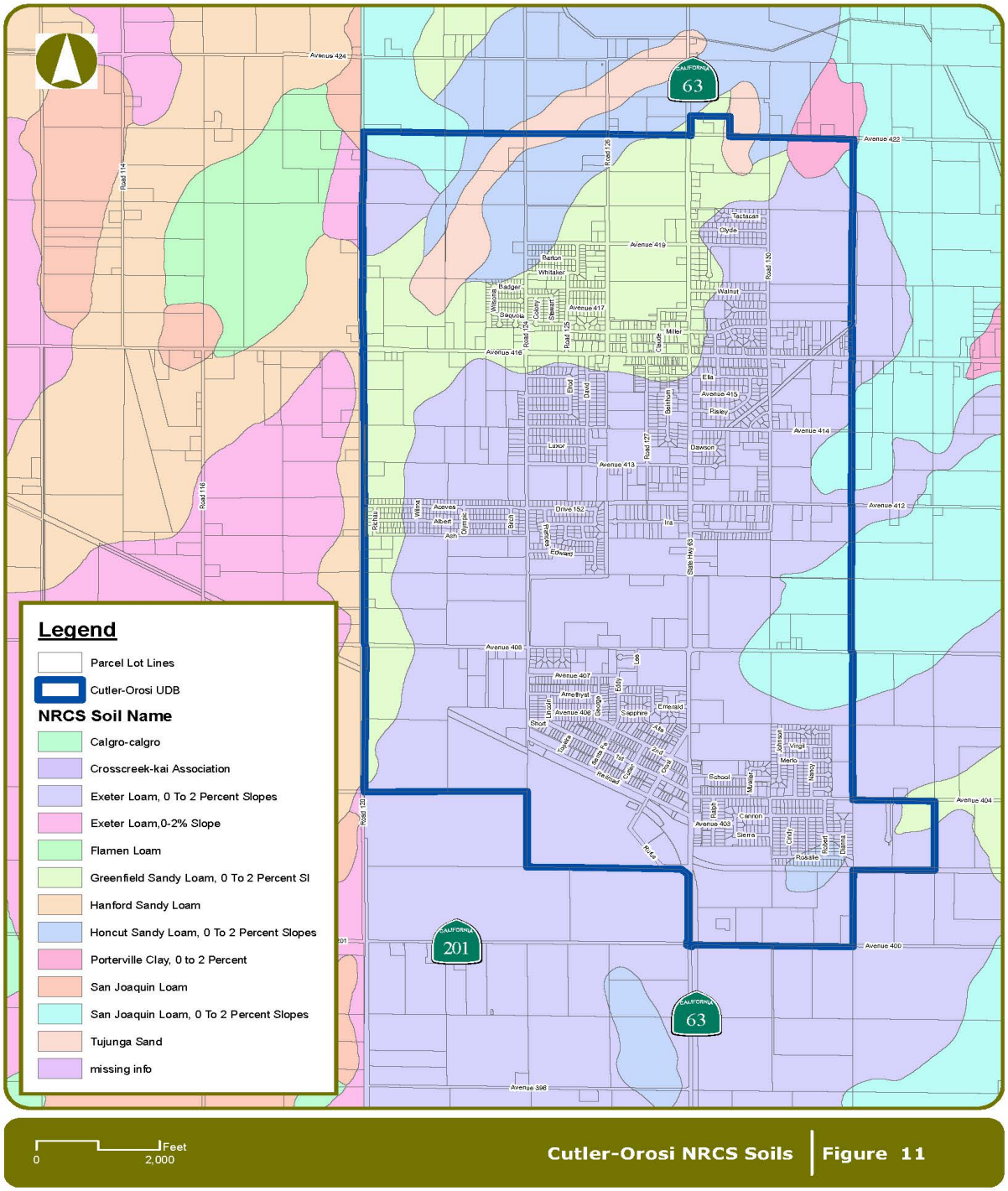
Porterville Clay, 0 to 2 percent slopes, consists of 32± inches of brown and dark reddish brown clay overlying dark reddish gray clay and sandy clay with a high shrink swell potential.

San Joaquin Loam, is moderately deep to a hardpan, well drained and nearly level. This soil is suitable for orchards, vineyards, and cultivated crops but is somewhat limited by the presence of hardpan which restricts root growth. This problem can be alleviated by ripping and shattering the hardpan. The soil is poorly suited to urban uses because of a high clay content, very slow permeability and a cemented hardpan. Septic tank filter fields are severely limited for these reasons.

San Joaquin loam, 0 to 2 percent slopes, consists of moderately deep to a duripan, well and moderately well drained soils that formed in alluvium derived from mixed but dominantly granitic rock sources.

Tujunga sand, consists of very deep, somewhat excessively drained soils that formed in alluvium from granitic sources. Tujunga soils are on alluvial fans and floodplains, including urban areas.

Figure 12 - NRCS Soils Map



Greenhouse Gases

"Executive Order S-3-05 was signed by Governor Schwarzenegger on June 1, 2005. This executive order established [GHG] emission reduction targets for California. Specifically, the executive order established the following targets:

- By 2010, reduce GHG emissions to 2000 levels.
- By 2020, reduce GHG emissions to 1990 levels.
- By 2050, reduce GHG emissions to 80 percent below 1990 levels.

The executive order additionally ordered that the Secretary of the California Environmental Protection Agency (Cal EPA) would coordinate oversight of the efforts among state agencies made to meet the targets and report to the Governor and the State Legislature biannually on progress made toward meeting the GHG emission targets. Cal EPA was also directed to report biannually on the impacts to California of global warming, including impacts to water supply, public health, and agriculture, the coastline, and forestry, and prepare and report on mitigation and adaptation plans to combat these impacts.

In response to the EO [executive order], the Secretary of Cal EPA created the Climate Action Team (CAT), composed of representatives from the Air Resources Board; Business, Transportation, & Housing; Department of Food and Agriculture; Energy Commission; California Integrated Waste Management Board (CIWMB); Resources Agency; and the Public Utilities Commission (PUC). The CAT prepared a recommended list of strategies for the state to pursue to reduce climate change emission in the state…"[22]

In 2006, California passed the California Global Warming Solutions Act of 2006 (Assembly Bill 32; California Health and Safety Code Division 25.5, Sections 38500, et seq.), which requires the CARB to design and implement emission limits, regulations, and other measures, such that feasible and cost-effective statewide GHG emissions are reduced to 1990 levels by 2020.

The Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The San Joaquin Valley Air Pollution Control District (SJVAPCD) adopted the *Climate Change Action Plan* (CCAP) in August 2008. "The (CCAP) directed the District Air Pollution Control Officer to develop guidance to assist Lead Agencies, project proponents, permit applicants, and interested parties in assessing and reducing the impacts of project specific greenhouse gas (GHG) emissions on global climate change.

On December 17, 2009, the San Joaquin Valley Air Pollution Control District (SJVAPCD) adopted

[22] Tulare County General Plan 2030 Update RDEIR, pages 3.4-4 to 3.4-5

the guidance: *Guidance for Valley Land-use Agencies in Addressing GHG Emission Impacts for New Projects* under CEQA, and the policy: District Policy – Addressing GHG Emission Impacts for Stationary Source Projects Under CEQA When Serving as the Lead Agency. The guidance and policy rely on the use of performance based standards, otherwise known as Best Performance Standards (BPS), to assess significance of project specific greenhouse gas emissions on global climate change during the environmental review process, as required by CEQA.

"The Tulare County Climate Action Plan (CAP) (last updated in 2016), serves as a guiding document for County of Tulare ("County") actions to reduce greenhouse gas emissions and adapt to the potential effects of climate change. The CAP is an implementation measure of the 2030 General Plan Update. The General Plan provides the supporting framework for development in the County to produce fewer greenhouse gas emissions during Plan buildout. The CAP builds on the General Plan's framework with more specific actions that will be applied to achieve emission reduction targets consistent with California legislation."[23]

Federal Emergency Management Agency (FEMA)

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0345E, Panel No. 345 dated June 16, 2009, **(see Figure 12)** shows the majority of the Cutler-Orosi footprint is within Flood Zone AO, AE, AH, X (shaded), and X (unshaded). A substantial portion of Cuter/Orosi are subject to 100 and 500 year flood hazard. FEMA requires development in Flood Zones AE to be constructed so that a building's ground floor elevation is above the flood contour line existing in the flood are.

"Flooding is a natural occurrence in the Central Valley because it is a natural drainage basin for thousands of watershed acres of Sierra Nevada and Coast Range foothills and mountains. Two kinds of flooding can occur in the Central Valley: general rainfall floods occurring in the late fall and winter in the foothills and on the valley floor; and snowmelt floods occurring in the late spring and early summer. Most floods are produced by extended periods of precipitation during the winter months. Floods can also occur when large amounts of water (due to snowmelt) enter storage reservoirs, causing an increase in the amount of water that is released."[24] "Flood events in the Tulare Lake region are caused by rainfall, snowmelt, and the resultant rising of normally dry lakes. Although significant progress has been made to contain floodwaters in the region, improvements to the flood control system are still needed to lessen the flood risk to life and property."[25]

"Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). "Floodplain" or "flood-prone area" means any land area susceptible to being inundated by water from any source. "Base Flood" is the flood having a one percent chance of being equaled or exceeded in any given year. "One-hundred-year flood" or "100 year flood" has the same meaning as "base flood." "Special flood hazard area" is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. "Floodway" means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood

[23] Tulare County Climate Action Plan, page 1
[24] General Plan Background Report, page 8-13
[25] California Water Plan Update 2009, Tulare Lake, page TL-28 to TL-29

without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms."[26] Although some areas of Tulare County have experienced major flooding along its major rivers, the Cutler-Orosi Plan Area has not. There are portions of Cutler-Orosi, however, that are within and adjacent to the FEMA 500 and 100 year flood zones. According to the Tulare County General Plan Update, substantial flooding could occur in Tulare County if the two (2) major dams were to experience failure. The primary source of floodwaters comes from Sand Creek. Sand Creek runs in a southwesterly direction through the northwestern part of Tulare County.

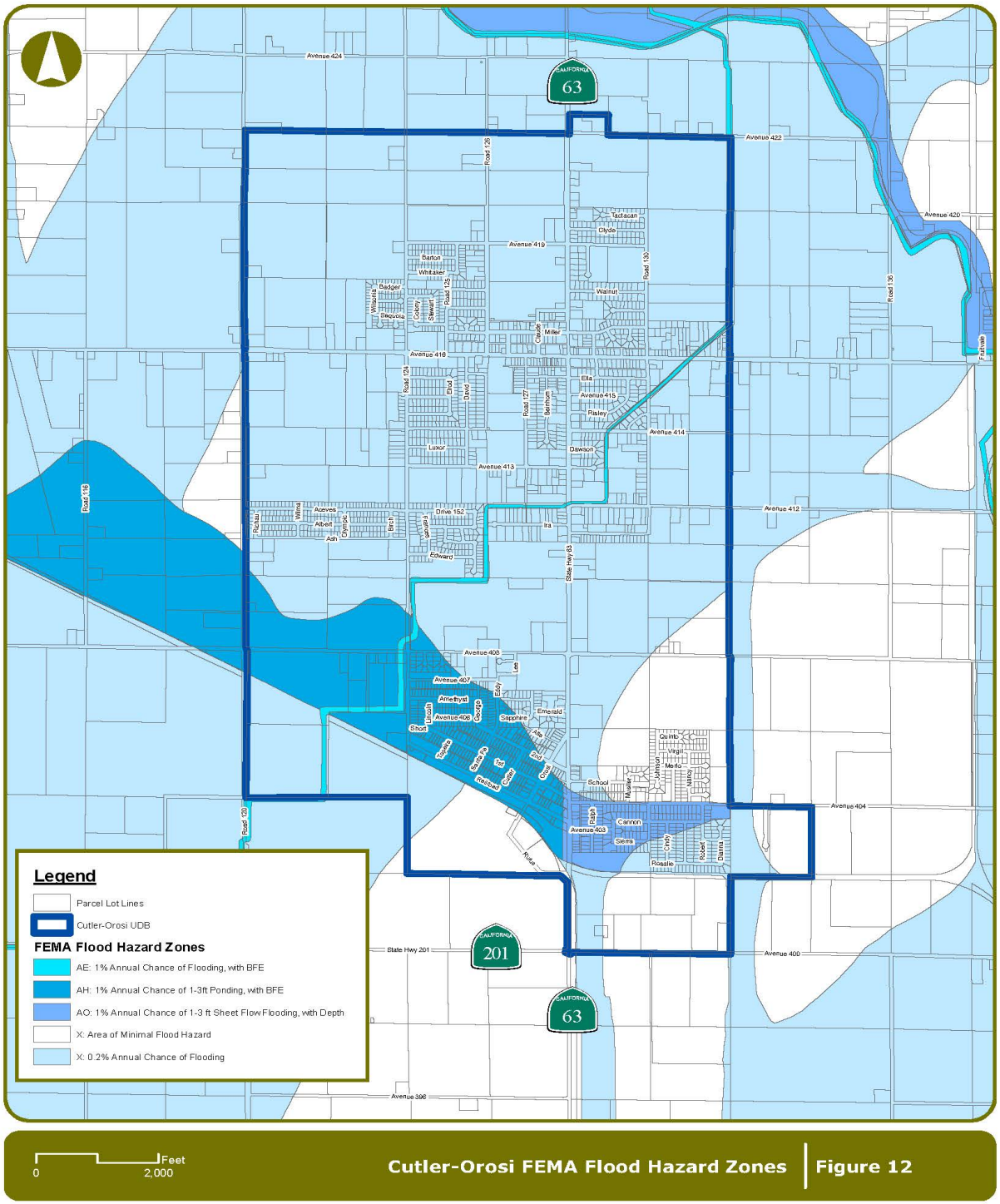
Sand Creek lies within the Sand Creek Dam inundation zone due to potential flood concerns if there was a dam failure. Sand Creek in Tulare County, California and displayed on the Monson USGS quad topo map. Sand Creek Dam is used for drinking water, fish and wildlife protection and flood control, among other things. Construction of the dam was completed in 1980. At normal levels, it has a surface area of 55 acres. The dam is owned by Tulare County Resource Management Agency. Sand Creek is rock fill. Its height is 60 feet with a length of 933 feet. Normal storage is 1,050 acre. It drains an area of 26.3 square miles. [FEMA Special Flood Hazard Areas and that the inundation zone must be included on hazard disclosures pertaining to real estate contracts].

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by actively adopting minimum regulatory standards as set forth by the Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: www.fema.gov. As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: https://tularecounty.ca.gov/rma/index.cfm/public-works/flood-information/.

On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: https://msc.fema.gov/portal.

[26] General Plan Background Report, page 8-14

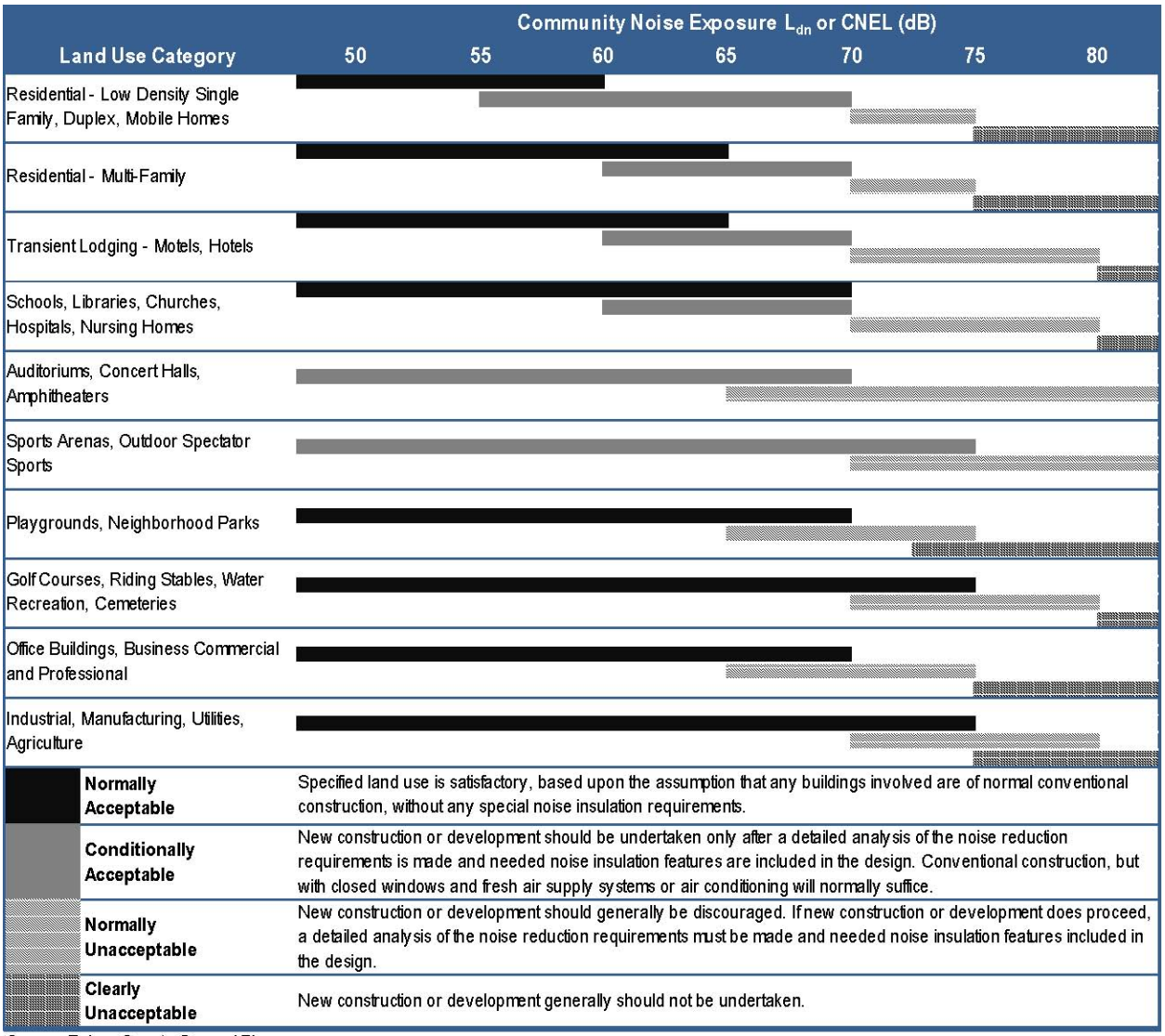
Figure 13 – FEMA Flood Map



Noise

State of California General Plan Guidelines (California Governor's Office of Planning and Research, 2003) identifies guidelines for the Noise Elements of city and county General Plans, including a sound level/land-use compatibility chart that categorized, by land use, outdoor Ldn ranges in up to four categories (normally acceptable, conditionally acceptable, normally unacceptable, and clearly unacceptable). These guidelines provide the State's recommendations for city and county General Plan Noise Elements **(see Figure 13)**.

Figure 14 - Community Noise Exposure



Source: Tulare County General Plan

The 2010 Recirculated Environmental Impact Report (RDEIR) prepared for the Tulare County General Plan Update included the following information regarding freeway and railroad noise. Baseline traffic noise contours for major roads in the County were developed using Sound32 (Caltrans' computer implementation of the FHWA Traffic Noise Prediction Model). Table 3.5-3 in the RDEIR summarized the daily traffic volumes, the predicted Ldn noise level at 100 feet from the roadway centerline is approximately 79 feet, and the distance from the roadway centerline to the 60-, 65-, and 70-dB-Ldn contours are 82 feet, 1,813 feet, and 3,907 feet respectively.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands, which have existing or projected noise levels exceeding 60 decibels (dBa) Ldn. This decibel figure is considered the maximum normally acceptable noise level for single-family residential areas. The two primary noise sources are SR 63 and Avenue 416. Together, these noise sources place a portion of the urbanized portion of Cutler-Orosi within the 60 dB Ldn noise contour. Roadways and traffic noise are the dominant source of ambient noise in the County.

The Health and Safety Element of the Tulare County General Plan adopted two countywide goals regarding noise in 2012. They are: 1) Protect the citizens of Tulare County from the harmful effects of exposure to excessive noise; and 2) Protect the economic base of Tulare County by preventing the encroachment of incompatible land uses near known noise-producing industries, railroads, airports and other sources. The Tulare County General Plan 2030 models noise contours for lands adjacent to freeways, airports, and local industries for the base year (1986) and provides projected contours for the year 2010. The noise contours were prepared in terms of either the community noise equivalent level (CNEL) or day-night average decibel level (Ldn), which is descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise. **Table 22** summarizes the daily traffic volumes on SR 63 from Avenue 400 to Emerald Drive, Emerald Drive to Avenue 416, Avenue 416 to Avenue 422 and on Avenue 416 from Road 120 to SR 63 and SR 63 to Boyd Drive.

Table 22 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
SR 63 Ave 400 to Emerald Dr.	8,300	34	74	159	343
SR 63 Emerald Dr. to Ave 416	13,000	43	92	198	426
SR 63 Ave 416 to Ave 422	7,200	29	62	133	287
Ave 416 Road 120 to SR 63	8,000	37	79	171	368
Ave 416 SR 63 to Boyd Dr	850	8	18	38	83

Source: 2010 General Plan Background Report

Infrastructure

Infrastructure is defined as "the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities." In regards to Cutler-Orosi, this Community Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, wells, water distribution piping and storage tanks, curbs, gutters, streets, sidewalks, etc.

Sustainable Groundwater Management Act SGMA

On September 16, 2014, governor Jerry Brown signed into law a three-bill legislative package, composed of AB 1739 (Dickinson), SB 1168 (Pavley), and SB 1319 (Pavley), collectively known as the Sustainable Groundwater Management Act (SGMA). The SGMA provides local governments and stakeholders the time needed to implement the complex law. Completion of plans in critically over drafted basins timeframe is January 31, 2020, and high- and medium-priority basins achieve sustainability 20-years after adoption of their plan (2040).

Groundwater Sustainability Agency (GSA)

The Cutler Public Utility District (CPUD) and the Orosi Public Utility District (OPUD) are located within the Kings River East Groundwater Sustainability Agency (GSA).

Surface Water

"There is no natural surface water supply in the vicinity of the CPUD or the OPUD. A surface water supply for domestic purposes will have to be transported to the area through Alta Irrigation District's open channels, the Friant-Kern Canal, a dedicated pipeline or a combination of all three. The Alta Irrigation District surface water supply originates in the Kings River watershed, with their headgate on the Kings River being located downstream of Piedra. Storage of their water supply is provided by Pine Flat Dam."[27]

Ground Water

"The CPUD and the OPUD presently rely entirely on groundwater for domestic water supply purposes."[28]

Domestic Water and Wastewater

In May 3, 2006, by Resolution 06-021, Tulare County Local Agency Formation Commission (LAFCO) adopted the Cutler Public Utility District (CPUD) and Orosi Public Utility District (OPUD) Municipal Service Review (MSR). The agencies in Tulare County were divided into three (3) categories: agencies subject to a full comprehensive study; agencies subject to a questionnaire study; and agencies exempt from a MSR study. The Cutler Public Utility District (CPUD) and Orosi Public Utility District (OPUD) was subject to a full comprehensive study.

"The Orosi Public Utility District (OPUD) and the Cutler Public Utility District (CPUD) provide domestic water to the residents of the unincorporated communities of Orosi and Cutler, respectively.
Each district relies solely on groundwater to meet the water demands of its customers. OPUD presently utilizes four wells and CPUD utilizes two active wells."[29]

"Currently, each district has sufficient water supply to meet existing water demands."

Table 23 - Description of Existing Infrastructure

Community	Drinking Water			Waste Water*		
	No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
Cutler	1,032	1,032	0	1,255	1,255	0
Orosi	1,788	3,788	2,000	2,162	2,162	0

*** Tulare County Housing Element Action Program 9, Data current as of May 2012**

[27] Water Supply Study Cutler-Orosi Area, Dennis R. Keller/James H. Wegley, Consulting Civil Engineers, February 2007, page 4-9.
[28] Ibid page 4-1
[29] Water Supply Study Cutler – Orosi Area, February 2007, Dennis R. Keller/James H. Wegly, Consulting Civil Engineers.

Cutler Public Utility District (CPUD)

"CPUD has a total of four developed wells. Two of the wells are active and two of the wells are inactive at this time **(see Figure 14)**. The two inactive wells (Well Nos. 3 and 4) were taken out of service because water test results exceeded the Maximum Contaminant Level (MCL) limit of nitrates. Well Nos. 5 and 6 are the two active wells that supply water for the community."[30]

"There is a well within CPUD (Well No. 7) that is not owned by CPUD. The well is owned by the Tulare County Redevelopment Agency and is used for fire flow at a local industry. Well No. 8 was completed in April 2006. Water quality testing; however, has revealed high nitrate concentrations approaching the MCL. Future use of Wells No. 8 is uncertain. Well No. 9 was drilled on the site for a proposed blending tank facility for CPUD. The well facility, when completed, will allow for water from Well Nos. 3 and 4 to be used in combination with flows from Well No. 5 and Well No. 9. The availability of sufficient quantities of low nitrate concentration water from CPUD's wells is uncertain."[31]

"The CPUD utilizes one elevated water storage tank for water system storage and pressure. The tank holds 50,000 gallons. The tank is connected to the distribution system by a common fill inlet and outlet configuration."[32]

"The CPUD's water supply is derived from four existing deep underground wells that have a total maximum production efficiency of 2,930 GPM, or 4.22 MGD."[33]

The CPUD water system **(see Table 23) s**upports 1,032 total connections including three industry-packing houses, and one box plant. Based upon a calculation performed in accordance with General Order 103, published by the California Public Utilities Commission, it is concluded that the District's water system is currently operating at or near its capacity, and cannot support additional connections at this time.

The amount of developable land available, including the availability of infrastructure, are two factors that have limited community growth from occurring, including affordable housing objectives, and commercial enterprise.

Currently, the District charges a flat rate for water service in the community. The District should consider installing water meters on all connections to their water system.

"Lovell High School, which is operated by the Cutler-Orosi Joint Unified School District, has requested water capacity from the Cutler PUD. The PUD plans to provide the school with water service pending the approval and implementation of the blending tank project. The school is located at the northwest quadrant of Avenue 392 and State Route 63, which is currently outside of the Cutler PUD boundary and sphere of influence (SOI). It is anticipated that the PUD would

[30] Water Supply Study Cutler – Orosi Area, February 2007, Dennis R. Keller/James H. Wegly, Consulting Civil Engineers page 2-2.
[31] Ibid. page 2-2
[32] Ibid. page 2-3.
[33] Tulare County Housing Element – Action Program 9, page 2-2.

provide water service to the school on a contractual basis".[34]

Orosi Public Utility District (OPUD)

The Orosi PUD's water supply is derived from four existing deep underground wells that have a total maximum production efficiency of approximately 2,930 GPM, or 4.22 MGD. The District also has a water storage tank with a capacity of approximately 750,000 gallons **(see Table 23)**.

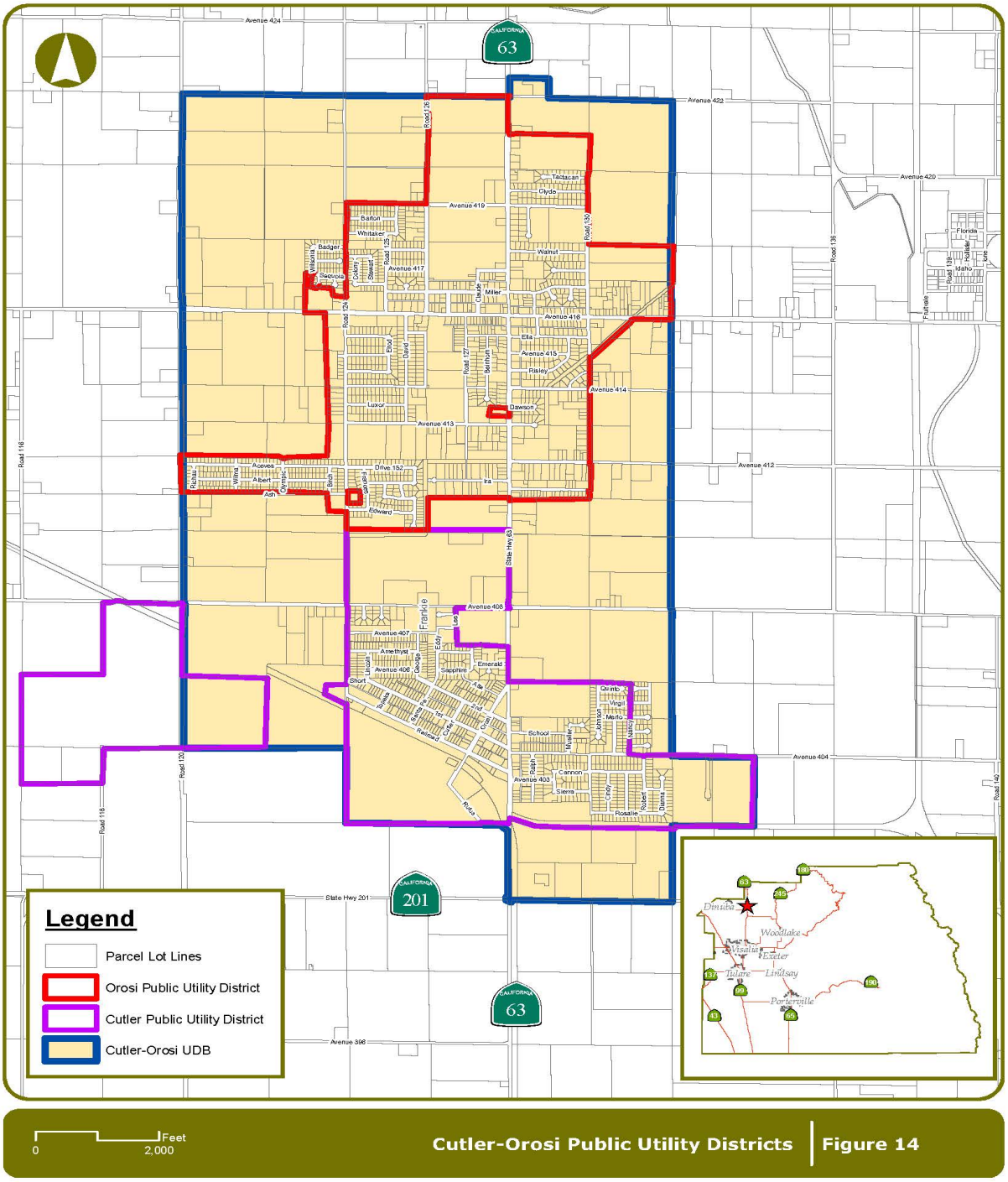
"OPUD has a total of six developed wells."[35] "Four of the wells are active and two of the wells are inactive at this time. Well No. 6 is inactive and was taken out of service because water test results exceeded the MCL limit for nitrates. Well No. 9 is also considered inactive due to high nitrates and is not connected to the system because of a development dispute. Wells Nos. 4, 5A, 7, and 8 are the four active wells that supply water for the community."[36]

"OPUD has one ground level water storage tank and four hydropneumatic tanks that also provide some limited water storage. The ground level tank has a capacity of 750,000 gallons and delivers water to the system through two booster pumps located at the site of Well No. 5A. There is a 10,000 gallon hydropneumatic tank at each of the active wells. OPUD's water supply and distribution system is shown on Figure 2-3 [in the Water Supply Study 2007]."[37]

"The Orosi PUD water system supports 1,788 total connections to their water system including 1,639 residential connections, 132 commercial connections, 3 agricultural connections, and 14 connections, which are inactive."[38]

[34] Tulare County LAFCO Group 2 Municipal Service Reviews, page 3-2.
[35] Water Supply Study Cutler – Orosi Area, February 2007, Dennis R. Keller/James H. Wegly, Consulting Civil Engineers, page 2-4
[36]. Ibid.
[37] Op. Cit.. 2-5
[38] Tulare County LAFCO Group 2 Municipal Service Reviews, page 4-1

Figure 15 - Inventory of Water Service in Cuter/Orosi



Sanitary Sewer

In March 1980, the Cutler Public Utility District entered into the Joint Wastewater Treatment and Disposal Facilities Agreement with the Orosi Public Utility District, forming the Cutler-Orosi Joint Power Wastewater Authority for the purpose of operating a wastewater treatment and disposal facility. Under the terms of the Agreement, which expires July 1, 2022, the Cutler Public Utility District owns 50 percent of the property and 40 percent of the plant and equipment of Authority. The Orosi Public Utility District owns 50 percent of the property and 60 percent of the plant and equipment of the Authority.

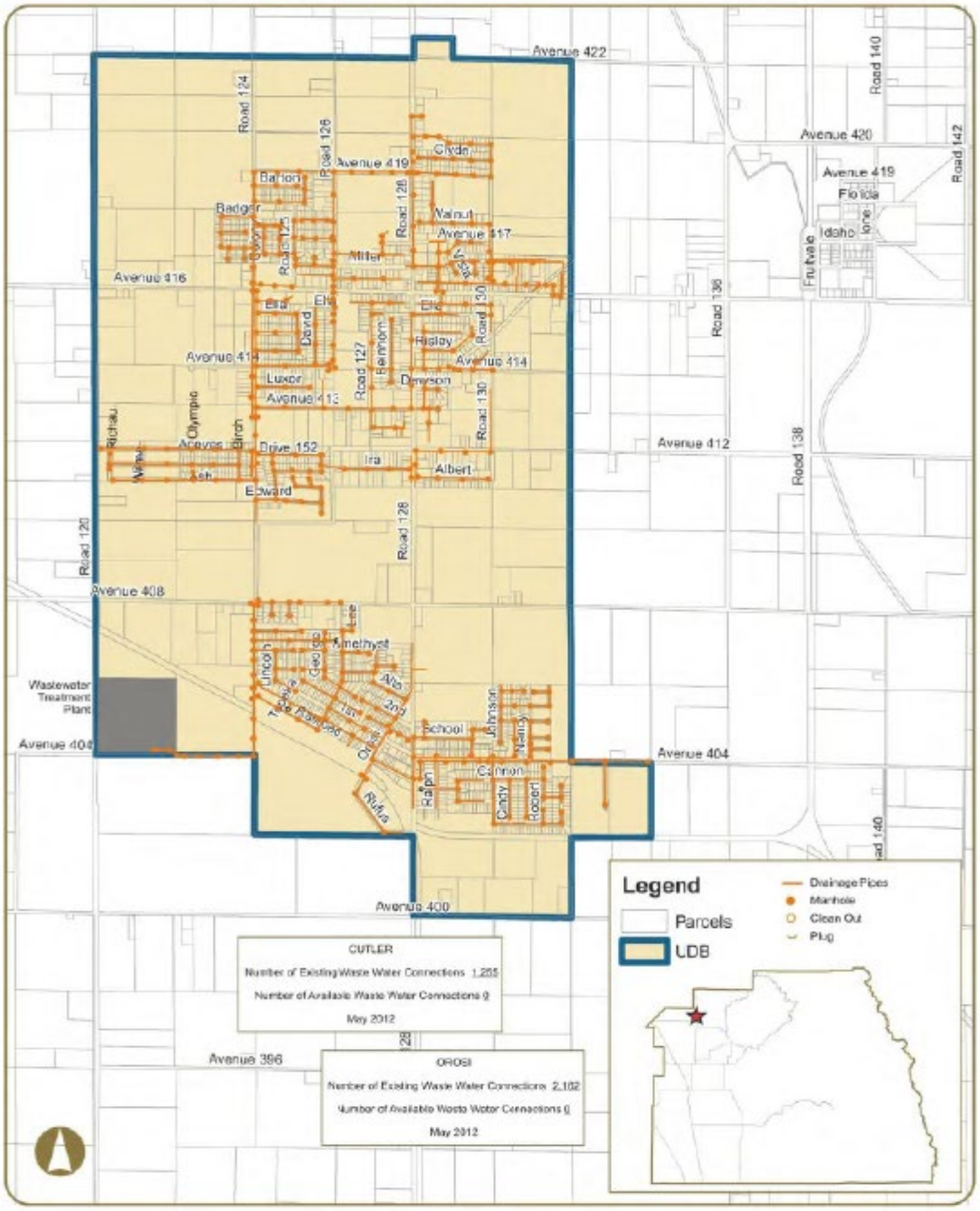
Figure 15 "graphically displays the approximate location of the sewer system and wastewater treatment plant. The Cutler PUD is currently allocated 1,255 equivalent dwelling units of capacity at the Cutler-Orosi Wastewater Treatment Facility (WWTF). The Orosi PUD is currently allocated 2,162 equivalent dwelling units of capacity at the WWTF. The Cuter and Orosi PUDs are currently under a building moratorium, and have waiting lists for additional sewer connections.

According to Cutler-Orosi PUD staff, the sanitary sewer collection system is very old and pipe leaks and breaks cause significant problems including groundwater inflow/infiltration and cross contamination with groundwater. The Orosi PUD is implementing a phased sewer collection system rehabilitation/replacement project, and has awarded a contract for the construction of the phase 1 improvements.

Treatment and disposal of the collected effluent is provided at the Cutler-Orosi WWTF, jointly owned and operated by the Cutler PUD and Orosi PUD. The Cutler-Orosi WWTF serves the communities of Cutler, Orosi, East Orosi, Yettem, Seville, and Sultana. It operates under the provisions of Waste Discharge Requirements (WDR) Order No. 97-106, issued by the California Regional Water Quality Control Board (RWQCB). The average dry weather flow at the WWTF is approximately 1.40 MGD, with a historical high flow of 1.89 MGD. Flow at the WWTF is greater during winter months than in summer months due to inflow/infiltration of storm water into the collection system during winter months, and ex-filtration during dry summer months. The PUDs will be able to more accurately predict the remaining capacity at the WWTF once repairs are made to leaking pipes throughout the collection system.

The Cutler PUD and Orosi PUD are working with Tulare County to secure funding that will be used to correct deficiencies that would increase the capacity of the WWTF. Proposed improvements will modernize the facility and add capacity to bring the serviceable operational limits to 2.4 MGD.

Figure 16 - Inventory of Sewer Service for Cutler-Orosi



Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

Storm Drainage

A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drainpipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration

Table 24 identifies the location of drainage inlets and sumps in Cutler-Orosi.

Table 24 - Location of Existing Storm Drainage Facilities

No.	East-West Roadway	North-South Roadway	Type
1	1st Drive	Road 124	Inlet
2	1st Drive	Topeka Drive	Inlet
3	1st Drive	Santa Fe Drive	Inlet
4	1st Drive	Cutler Drive	Inlet
5	1st Drive	Orosi Drive	Inlet
6	1st Drive	Road 128	Inlet
7	2nd Drive	Eddy Avenue	Inlet
8	2nd Drive	Road 128	Inlet
9	Amethyst Avenue	Lincoln Road	Inlet
10	Amethyst Avenue	George Road	Inlet
11	Amethyst Avenue	Eddy Avenue	Inlet
12	Avenue 404	Road 128	Inlet
13	Avenue 404	Mueller Road	Inlet
14	Avenue 404	Road 130	Inlet
15	Avenue 406	Eddy Avenue	Inlet
16	Avenue 406	Alta Drive	Inlet
17	Avenue 407	Road 124	Inlet
18	Avenue 413	David Road	Sump
19	Avenue 413	Road 127	Inlet
20	Avenue	East 413of Road 128	Inlet
21	Avenue 414	David Road	Sump

Table 24 - Location of Existing Storm Drainage Facilities			
No.	**East-West Roadway**	**North-South Roadway**	**Type**
22	Avenue 414	Road 127	Sump
23	Avenue 414	East of Road 128	Inlet
24	Avenue 414	Road 128	Inlet
25	Avenue 414	Sand Creek	Inlet
26	Avenue 414	Road 130	Inlet
27	Avenue 415	East of Road 128	Inlet
28	Avenue 416	Road 124	Inlet
29	Avenue 416	Road 125	Inlet
30	Avenue 416	David Road	Inlet
31	Avenue 416	Road 126	Inlet
32	Avenue 416	Eddy Road	Inlet
33	Avenue 416	Claude Road	Inlet
34	Avenue 416	Road 130	Inlet
35	Avenue 417	Claude Road	Sump
36	Avenue 419	Between Ralph Rd and Road 130	Sump
37	Cannon Avenue	East of Road 130	Inlet
38	Dawson Avenue	East of Road 128	Inlet
39	Ella Avenue	David Road	Sump
40	Ella Avenue	East of Road 128	Inlet
41	Ella Avenue	Road 130	Inlet
42	Emerald Avenue	Road 127	Inlet
43	Ira Avenue	West end	Sump
44	Luxor Avenue	Road 124	Inlet
45	Miller Avenue	Road 125	Sump
46	Miller Avenue	Eddy Road	Sump
47	Miller Avenue	Claude Road	Sump
48	Railroad Drive	Road 124	Inlet
49	Railroad Drive	Topeka Drive	Sump
50	Railroad Drive	Santa Fe Drive	Sump
51	Railroad Drive	Between Santa Fe Drive and Cutler Drive	Inlet
52	Railroad Drive	Cutler Drive	Sump
53	Railroad Drive	Orosi Drive	Sump
54	Risley Avenue	Road 124	Inlet
55	Risley Avenue	East of Road 128	Inlet
56	Rosalie Avenue	Road 130	Inlet
57	Rosalie Avenue	Nancy Road	Inlet
58	Sierra Avenue	Road 128	Inlet
59	South of Avenue 408	Lincoln Road	Inlet
60	South of Avenue 408	Topeka Road	Inlet
61	Walnut Avenue	Road 128	Sump

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

Solid Waste

Pena's Disposal, a private company, provides solid waste disposal services for the Community of Cutler-Orosi. Solid waste generated in Cutler-Orosi can be disposed of at the Visalia Landfill (located at 22466 Road 80, Visalia, California).

Energy Natural Gas/Electricity

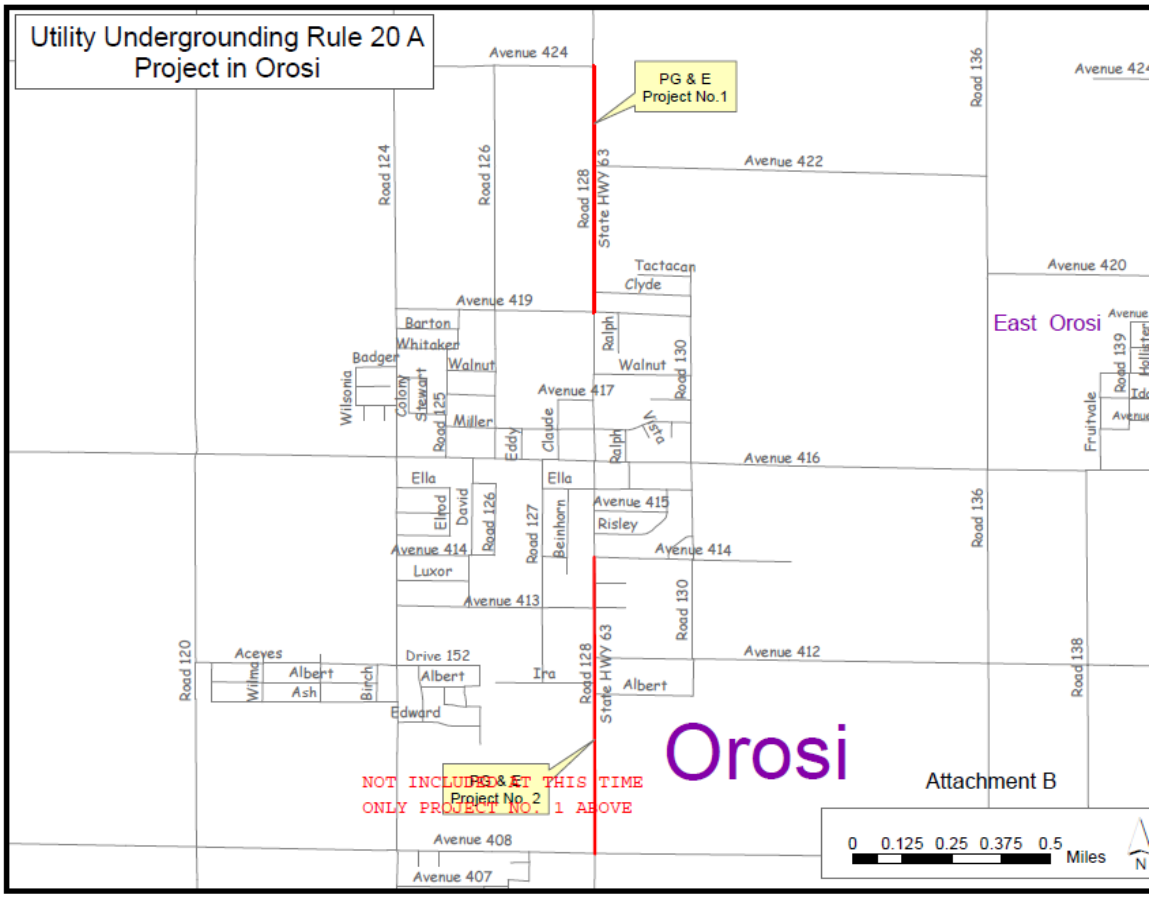
Pacific Gas & Electric (PGE) provides electrical and gas power in Cutler-Orosi.

Undergrounding Electricity (Rule 20A)

The California Public Utilities Commission (CPUC) Rule 20 includes policies and procedures for the undergrounding of overhead power lines. Utility ratepayers for projects of public benefit fund rule 20A projects.

Southern California Edison has one proposed Rule 20A project in Orosi **(see Figure 16)**: State Route 63 (Road 128) from Avenue 419 to Avenue 424. The Board of Supervisors will consider the formation of an underground utility district for this project.

Figure 17 - Rule 20A Utility Underground Districts in Orosi



Public Services

Tulare County Sheriff

Police protection services are provided in Cutler-Orosi by the Tulare County Sheriff's Department sub-station, located at 12800 Avenue 416, in Orosi. The Substation covers approximately 289 square miles serving a rural population to include the unincorporated communities of Cutler, East Orosi, Orosi, Seville, Sultana, Traver and Yettem. The Substation runs a four-shift operation, which includes 23 deputies, four (4) sergeants and one (1) lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. In addition, general shift staffing the communities of Cutler-Orosi are assigned a Community Based Officer assigned specifically to those areas. The substation is open for walk-ins from 8:00 am to 5:00 pm Monday thru Friday. After hours and weekends there is a phone provided outside the substation that calls directly into the dispatch center. The substation provides patrol services 24-hours per day, 365 days per year. Additional Sheriff Resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

Tulare County Fire Department

The Tulare County Fire Department provides fire protection and emergency medical services for Cutler-Orosi. Tulare County Fire Station #4 is located at 40779 Road 128, Cutler-Orosi Fire Station. Cutler-Orosi Fire Station has Patrol 4, Engine 4, and Engine 204 assigned to this location. Paid On-Call Fire Fighters are assigned to this Station and they respond when called out to an incident.

There are 267 fire hydrants found within Cutler-Orosi **(see Table 25)**. These fire hydrants are located within the County rights-of-way. **Figure 17** display Existing Fire Hydrants in Cutler-Orosi

Table 25: Existing Fire Hydrants Cutler-Orosi

No.	Location		
1	Avenue 422 west of Road 128	134	Avenue 413 east of Road 128
2	Road 128 south of Avenue 422	135	Dawson Avenue east of Road 128
3	Road 128 south of Avenue 422	136	Dawson Avenue east of Road 128
4	Orosi Mobile Home Estates east of Road 128	137	Beinhorn Road south of Avenue 414
5	Orosi Mobile Home Estates east of Road 128	138	Beinhorn Road south of Avenue 414
6	Orosi Mobile Home Estates east of Road 128	139	Road 127 north of Avenue 413
7	Orosi Mobile Home Estates east of Road 128	140	Avenue 413 and Road 127
8	Orosi Mobile Home Estates east of Road 128	141	Avenue 413 east of Road 127
9	Road 128 north of Clyde Avenue	142	Avenue 413 east of Road 127
10	Tactacan Avenue west of Road 130	143	Avenue 414 east of Road 124
11	Tactacan Avenue and Road 130	144	Road 124 south of Avenue 414
12	Tactacan Avenue west of Road 130	145	David Road south of Avenue 414
13	Clyde Avenue and Road 130	146	Road 124 south of Luxor Avenue
14	Road 130 south of Clyde Avenue	147	Avenue 413 east of Road 124
15	Clyde Avenue west of Road 130	148	David Road south of Luxor Avenue
16	Clyde Avenue east of Road 128	149	Avenue 413 east of David Road
17	Avenue 419 east of Ralph Road	150	Avenue 413 east of David Road
18	Road 128 south of Avenue 419	151	Avenue 413 east of David Road
19	Road 126 north of Avenue 419	152	Ira Avenue and Road 127
20	Avenue 419 west of Road 126	153	Ira Avenue west of Road 127
21	Avenue 419 west of Van Tassel Road	154	David Road and Aceves avenue
22	Avenue 419 west of Van Tassel Road	155	Edward Avenue east of Road 124
23	Avenue 419 east of Road 124	156	Edward Avenue east of Road 124
24	Road 124 north of Barton Avenue	157	Edward Avenue east of Road 124
25	Road 124 and Barton Avenue	158	Road 124 north of Edward Avenue
26	Road 124 south of Barton Avenue	160	Aceves Avenue west of David Road
27	Barton Avenue east of Road 124	161	Albert Avenue east of Frances Road

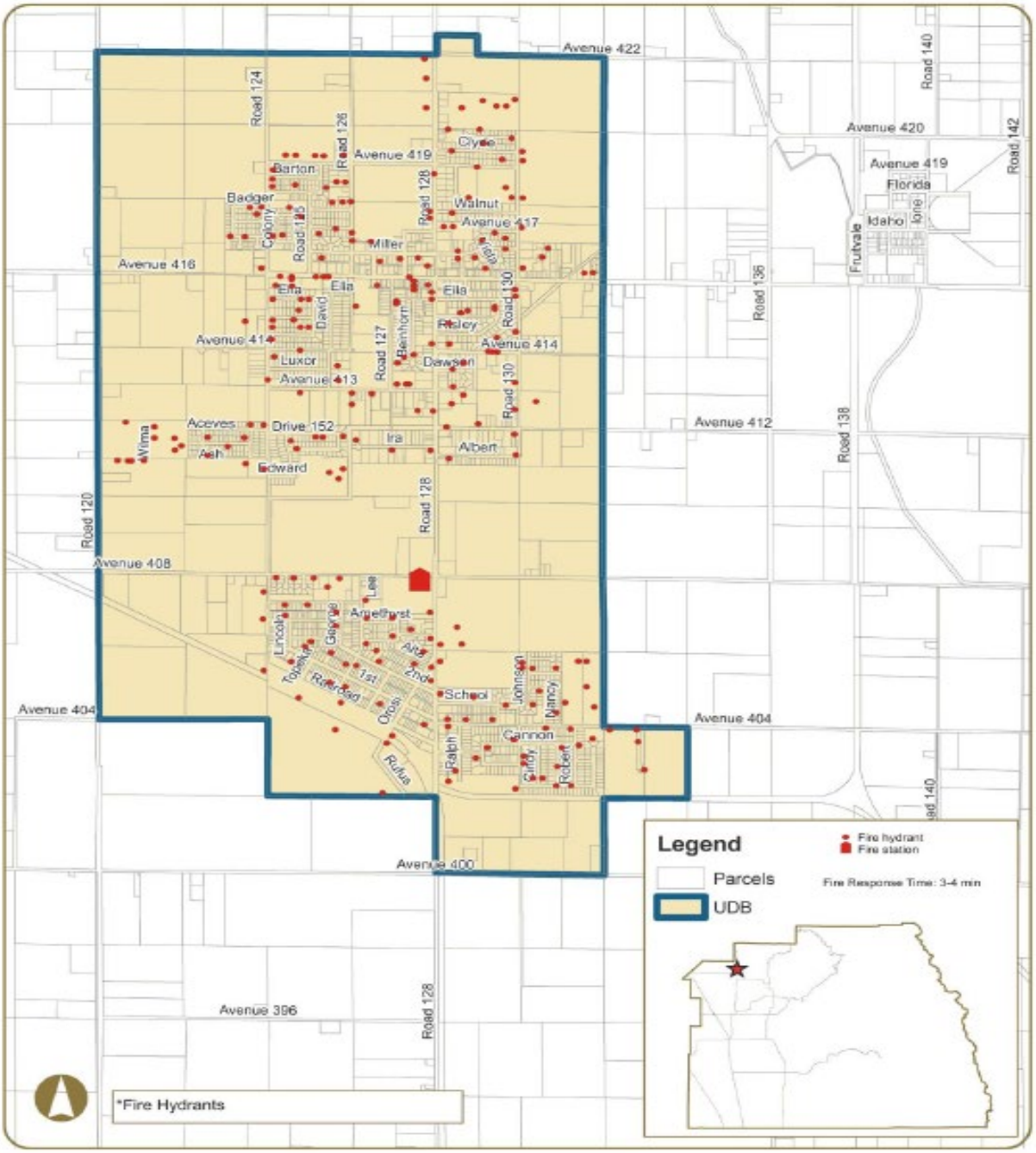
Table 25: Existing Fire Hydrants Cutler-Orosi			
28	Van Tassel Road south of Barton Avenue	162	Frances Road south of Aceves Avenue
29	Road 126 south of Avenue 419	163	Aceves Avenue and Road 124
30	Road 126 south of Avenue 419	164	Aceves Avenue and Birch Road
31	Road 126 north of Walnut Avenue	165	Birch Road south of Aceves Avenue
32	Road 126 north of Walnut Avenue	166	Ash Avenue and Birch Road
33	Road 126 north of Walnut Avenue	167	Albert Avenue west of Birch Road
34	Ralph Road south of Avenue 419	168	Ash Avenue east of Olympic Street
35	Road 130 north of Walnut Avenue	169	Olympic Street south of Aceves Avenue
36	Road 130 north of Walnut Avenue	170	Albert Avenue and Olympic Street
37	Road 130 north of Walnut Avenue	171	Albert Avenue west of Olympic Street
38	Walnut Avenue east of Road 128	172	Albert Avenue west of Olympic Street
39	Walnut Avenue and Road 128	173	Albert Avenue and Wilma Road
40	Road 128 south of Walnut Avenue	174	Aceves Avenue and Wilma Road
41	Road 125 south of Walnut Avenue	175	Ash Avenue west of Wilma Road
42	Dennison Drive west of Stewart Street	176	Ash Avenue west of Wilma Road
43	Colony Street north of Buenna Vista Avenue	177	Ash Avenue east of Richau Street
44	Colony Street north of Buenna Vista Avenue	178	Ash Avenue east of Richau Street
45	Badger Avenue and Road 124	179	Aceves Avenue east of Richau Street
46	Badger Avenue west of Road 124	180	Avenue 408 and Road 124
47	Road 124 south of Badger Avenue	181	Avenue 408 east of Lincoln Road
48	Sequoia Avenue and Granite Court	182	Avenue 408 east of Topeka Road
49	Sequoia Avenue and Wilsonia Avenue	183	Avenue 408 east of Topeka Road
50	Avenue 417 east of Road 125	184	Avenue 408 east of Topeka Road
51	Avenue 417 west of Road 126	185	Road 124 south of Avenue 408
52	Avenue 417 and Road 126	186	Avenue 407 and Eddy Road
53	Road 126 south of Avenue 417	187	Avenue 407 east of Road 124
54	Miller Avenue east of Road 126	188	Avenue 407 east of Road 124
55	Road 128 south of Walnut Avenue	189	George Road south of Avenue 407
56	Road 128 south of Walnut Avenue	190	George Road south of Amethyst Avenue
57	Road 130 and Avenue 417	191	Amethyst Avenue west of Lincoln Road
58	Avenue 417 west of Road 130	192	Road 124 and Amethyst Avenue
59	Avenue 417 west of Road 130	193	Amethyst Avenue and Eddy Road
60	Avenue 417 west of Road 130	194	Amethyst Avenue and Road 127
61	Avenue 416 east of Road 130	195	Road 128 south of Avenue 408
62	Avenue 416 east of Road 130	196	Emerald Avenue and Road 128
63	Road 130 north of Avenue 416	197	Emerald Avenue east of Road 127
64	Miller Road east of Road 130	198	Avenue 406 and Road 127
65	Miller Road east of Road 130	199	Road 128 north of Emerald Avenue
66	Miller Road and Pacifica Court	200	Road 128 south of Emerald Avenue

Table 25: Existing Fire Hydrants Cutler-Orosi			
67	Miller Road and Vista Court	201	Road 128 south of Emerald Avenue
68	Lincoln Road north of Avenue 416	202	Avenue 406 and Eddy Road
69	Miller Avenue and Ralph Road	203	Avenue 406 west of George Road
70	Miller Avenue and Ralph Road	204	Avenue 406 west of George Road
71	Road 128 north of Avenue 416	205	George Road south of Avenue 406
72	Miller Avenue west of Road 128	206	Topeka Drive south of 1st Drive
73	Miller Avenue and Claude Road	207	Road 124 south of Railroad Drive
74	Eddy Road south of Miller Avenue	208	Road 128 north of Alta Drive
75	Miller Avenue east of Road 125	209	Orosi Drive and Road 128
76	Avenue 416 and Road 124	210	Orosi Drive south of Road 128
77	Avenue 416 east of Road 124	211	Road 128 north of 2nd Drive
78	Avenue 416 east of Road 124	212	Alta Drive west of Orosi Drive
79	Avenue 416 east of Road 124	213	2nd Drive west of Cutler Drive
80	Avenue 416 east of Road 124	214	Eddy Road north of 2nd Drive
81	Road 124 south of Avenue 416	215	Santa Fe Drive south of 2nd Drive
82	Avenue 416 east of Road 125	216	Santa Fe Drive south of 2nd Drive
83	Avenue 416 east of Road 125	217	Santa Fe Drive south of 1st Drive
84	Avenue 416 east of Road 125	218	Santa Fe Drive south of 1st Drive
85	Road 126 south of Ella Avenue	219	Railroad Drive east of Santa Fe Drive
86	Ella Avenue and Road 124	220	Road 124 south of Railroad Drive
87	Ella Avenue east of Road 124	221	Cutler Drive south of 1st Drive
88	Ella Avenue west of Elrod Road	222	Railroad Drive east of Santa Fe Drive
89	Avenue 415 west of Elrod Road	223	Orosi Drive south of Railroad Drive
90	Avenue 415 west of Elrod Road	224	Orosi Drive south of Railroad Drive
91	Elrod Road south of Avenue 415	225	Avenue 402 west of Road 128
92	Risley Avenue west of Elrod Road	226	2nd Drive and Road 128
93	Avenue 415 and Road 124	227	Road 128 south of 1st Drive
94	Road 124 north of Risley Avenue	228	School Avenue east of Road 128
95	Road 124 south of Risley Avenue	229	Quinto Court and Johnston Road
96	Road 124 south of Avenue 415	230	Virgil Avenue and Johnston Road
97	Avenue 416 east of Eddy Road	231	Virgil Avenue and Johnston Road
98	Road 127 and Ella Avenue	232	Virgil Avenue west of Nancy Road
99	Road 127 and Ella Avenue	233	Robert Road south of Rivera Court
100	Avenue 416 and Claude Road	234	Robert Road south of Rivera Court
101	Avenue 416 east of Claude Road	235	Merlo Court east of Nancy Road
102	Avenue 416 east of Claude Road	236	Merlo Avenue and Cindy Road
103	Avenue 416 east of Claude Road	237	Johnston Road south of Merlo Avenue
104	Road 128 south of Avenue 416	238	School Avenue and Mueller Road
105	Road 128 south of Avenue 416	239	Antonia Avenue and Nancy Road
106	Ella Avenue at Road 128	240	Kahlo Court and Nancy Road
107	Road 130 south of Avenue 416	241	Kahlo Court east of Nancy Road
108	Ella Avenue and Road 130	242	Avenue 404 east of Road 128

Table 25: Existing Fire Hydrants Cutler-Orosi			
109	Mueller Road south of Ella Avenue	243	Avenue 404 east of Road 128
110	Mueller Road south of Ella Avenue	244	Avenue 404 and Ralph Road
111	Avenue 415 east of Road 128	245	Avenue 404 and Mueller Road
112	Avenue 415 east of Road 128	246	Avenue 404 and Cindy Road
113	Avenue 415 east of Road 128	247	Avenue 404 and Robert Road
114	Ledbetter Drive and Road 130	248	Avenue 404 east of Robert Road
115	Ledbetter Drive west of Road 130	249	Avenue 404 east of Robert Road
116	Avenue 414 west of Road 130	250	Private Dwy east of Robert Road south of Avenue 404
117	Avenue 415 and Ledbetter Drive	251	Private Dwy east of Robert Road south of Avenue 404
118	Avenue 415 west of Ledbetter Drive	252	Dianna Road north of Sierra Avenue
119	Avenue 414 east of Road 128	253	Cannon Avenue and Robert Road
120	Road 128 north of Avenue 414	254	Cannon Avenue and Nancy Road
121	Road 130 south of Avenue 414	255	Nancy Road south of Cannon Avenue
122	Road 130 north of Avenue 412	256	Rosalie Avenue and Nancy Road
123	Road 130 north of Avenue 412	257	Rosalie Avenue and Robert Road
124	Avenue 412 and Road 130	258	Cindy Road north of Rosalie Avenue
125	Albert Avenue and Road 130	259	Cindy Road north of Rosalie Avenue
126	Avenue 412 west of Road 130	260	Rosalie Avenue and Road 130
127	Albert Avenue east of Road 128	261	Sierra Avenue and Road 130
128	Albert Avenue and Road 128	262	Road 130 north of Sierra Avenue
129	Avenue 412 east of Road 128	263	Cannon Avenue and Road 130
130	Road 128 north of Avenue 412	264	Cannon Avenue north of Sierra Avenue
131	Road 128 north of Avenue 412	265	Sierra Avenue and Cannon Avenue
132	Road 128 south of Avenue 413	266	Ralph Road south of Avenue 403
133	Avenue 413 east of Road 128	267	Ralph Road south of Avenue 403

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

Figure 18 - Inventory of Fire Infrastructure in Cutler-Orosi



Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

Schools

The Cutler-Orosi Community Plan Area is within the Cutler-Orosi Joint Unified School District located within its boundaries. It offers pre-school through 12th grade education and has a 2019-2020 enrollment of 4,123 students **(see Table 26)**. School enrollment has been variable since 2000. According to records from the California Department of Education, enrollment in the Cutler-Orosi Joint Unified School District in 2019-20[39], of these students approximately 3,936 are Hispanic, 9 are Asian, 73 are White, 48 are African American, 2 are Two or More Races, 5 are American Indian or Alaska Native, 119 are Filipino, and 2 are Pacific Islander. One hundred percent of the students participate in the Free or Reduced Price Meal (FRPM).

Table 26 - Cutler-Orosi Joint Unified - Student Enrollment 2000-2017

Year	Enrollment	Change
2000-2001	3,844	
2001-2001	3,914	+70
2002-2003	3,981	+7
2003-2004	4,017	+36
2004-2005	4,026	+9
2005-2006	4,058	+32
2006-2007	4,023	-35
2007-2008	4,062	+39
2008-2009	4,128	+66
2009-2010	4,162	+34
2010-2011	4,178	+16
2011-2012	4,133	-45
2012-2013	4,128	-5
2013-2014	4,114	-14
2014-2015	4,083	-14
2015-2016	4,095	+12
2016-2017	4,126	+31
2017-2018	4,125	-1
2018-2019	4,151	+26
2019-2020	4,123	-28

California Department of Education

[39] California Department of Education, Data enrollment, https://dq.cde.ca.gov/dataquest/dqcensus/EnrEthGrd.aspx?cds=5471860&agglevel=district&year=2019-20

Family Education Center

The Family Resource Center (FRC) is located at the Family Education Center, 40802 Road 128 in Orosi, California, on the northeast corner of SR 63 and Avenue 408. This location places the FRC halfway between Cutler-Orosi, with a Tulare County Area Transit stop directly in from of the FRC. Anyone living within the Cutler-Orosi school district boundaries may be eligible to receive resource and referral services.

Their ultimate goals are to support families, meet individual needs, build upon strengths, share responsibility, and engage the community in efforts to create safe, nurturing environments for Cutler-Orosi community children to grow and learn.

Libraries

"The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch."[40] The closest library is located in Orosi **(see Table 27)**.

Table 27 - Library Location & Hours

Branch	Address	Service Hours (2003)
Orosi	Orosi Branch 12646 Avenue 416 Orosi, CA 93647	Wednesday, Thursday, and Friday 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 6:00 p.m.

Library hours current as of April 2019

Parks

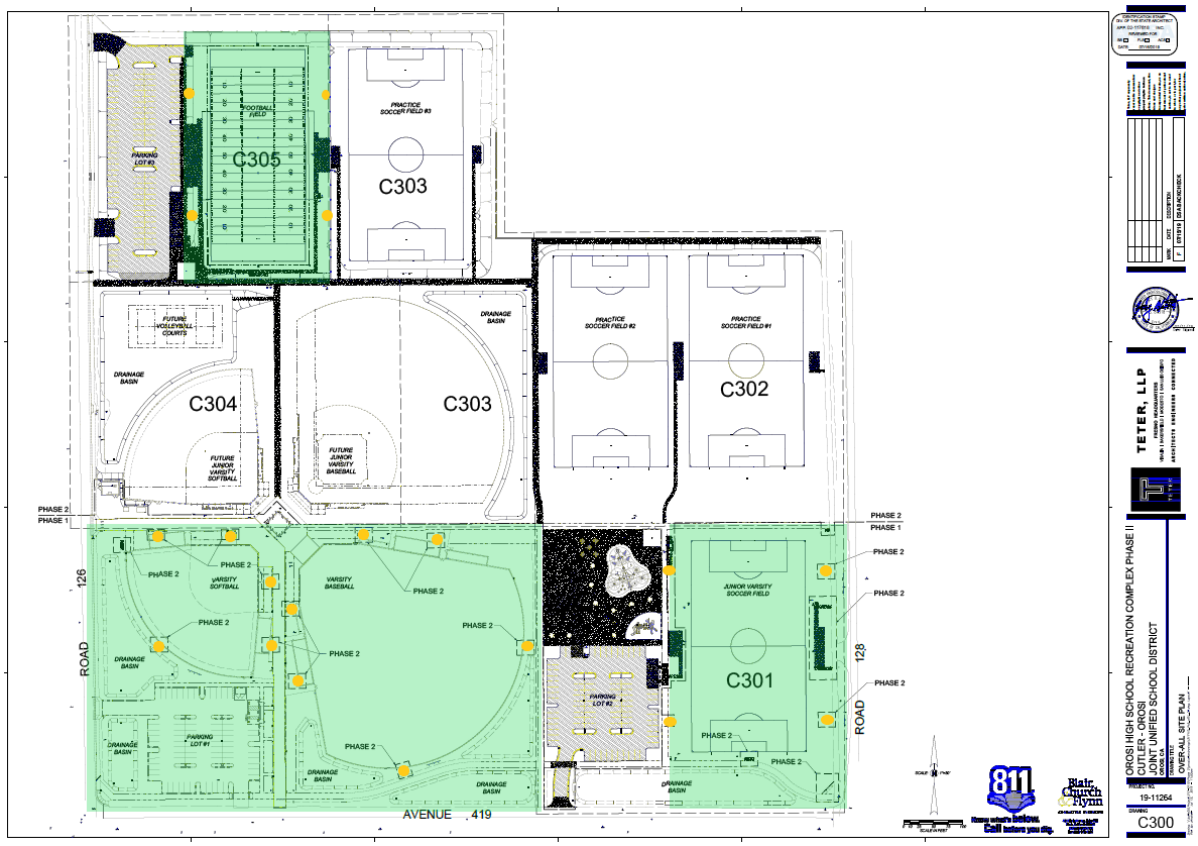
Cutler-Orosi's nearest park is Ledbetter Park located at 40779 Road 124 in Cutler, California.

The Cutler-Orosi Joint Unified School District has been in the process of developing the Orosi High School Recreation Complex (approximately 32-acres) located at 41815 Road 128 in Orosi, California. Phase I currently contains football, soccer, baseball, and softball fields that were constructed in the spring/summer of 2019.

The Orosi High School Recreation Complex Phase II proposes the installation of 19 light standards within the previously developed recreational complex area in Orosi, California. Eight 70-foot light standards would be installed around the existing football field and junior varsity soccer field, six 60- to 80-foot light standards would be installed around the existing baseball field, and five 60- to 70 foot light standards would be installed around the existing softball field **(see Figure 18)**.

[40] General Plan Background Report, page 7-96

Figure 19 Orosi High School Recreation Sports Park



Roads

Road Conditions

There are various roadways in Cutler-Orosi that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt, then immediately covered with aggregate, and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.

Table 28 lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 19** graphically displays this information on a map.

Table 28 - Road Maintenance Strategies

No.	Roadway	Limits	Repair Code
1	Aceves Avenue	Road 124 to David Road	OLAY
2	Albert Avenue	Road 128 to Road 130	CHIP
3	Albert Avenue	Frances Road to David Road	CHIP
4	Alta Drive	Orosi Drive to Avenue 406	CHIP
5	Amethyst Avenue	Road 124 to George Road	CHIP
6	Amethyst Avenue	Eddy Road to Road 127	CHIP
7	Avenue 403	Robert Road to Dianna Road	OLAY
8	Avenue 404	Road 128 to Cindy Road	GRX
9	Avenue 404	Cindy Road to Nancy Road	OLAY
10	Avenue 404	Nancy Road to Robert Road	GRX
11	Avenue 406	Lincoln Road to George Road	CHIP
12	Avenue 406	Eddy Road to Road 127	CHIP
13	Avenue 408	Topeka Road to Lee Road	GRX
14	Avenue 413	Road 124 to Road 128	CHIP
15	Avenue 413	Road 128 to East end	OLAY
16	Avenue 414	Road 127 to Beinhorn Road	CHIP
17	Avenue 415	Road 124 to Elrod Road	OLAY
18	Avenue 415	Road 128 to Mueller Road	CHIP
19	Avenue 417	Road 130 to West end	CHIP
20	Avenue 419	Road 128 to Road 130	CHIP
21	Badger Avenue	Wilsonia Avenue to Road 124	CHIP
22	Barton Avenue	Road 124 to Van Tassel Road	CHIP
23	Beinhorn Road	Avenue 414 to South end	OLAY
24	Beinhorn Road	Avenue 414 to Ella Avenue	CHIP
25	Buena Vista Avenue	Colony Street to Road 125	CHIP
26	Cindy Road	Rosalie Avenue to Cannon Avenue	OLAY
27	Cindy Road	Avenue 404 to Merlo Avenue	CHIP
28	Clyde Avenue	Road 128 to Road 130	OLAY
29	David Road	Albert Avenue to Aceves Avenue	CHIP
30	Dawson Avenue	Road 128 to East end	CHIP

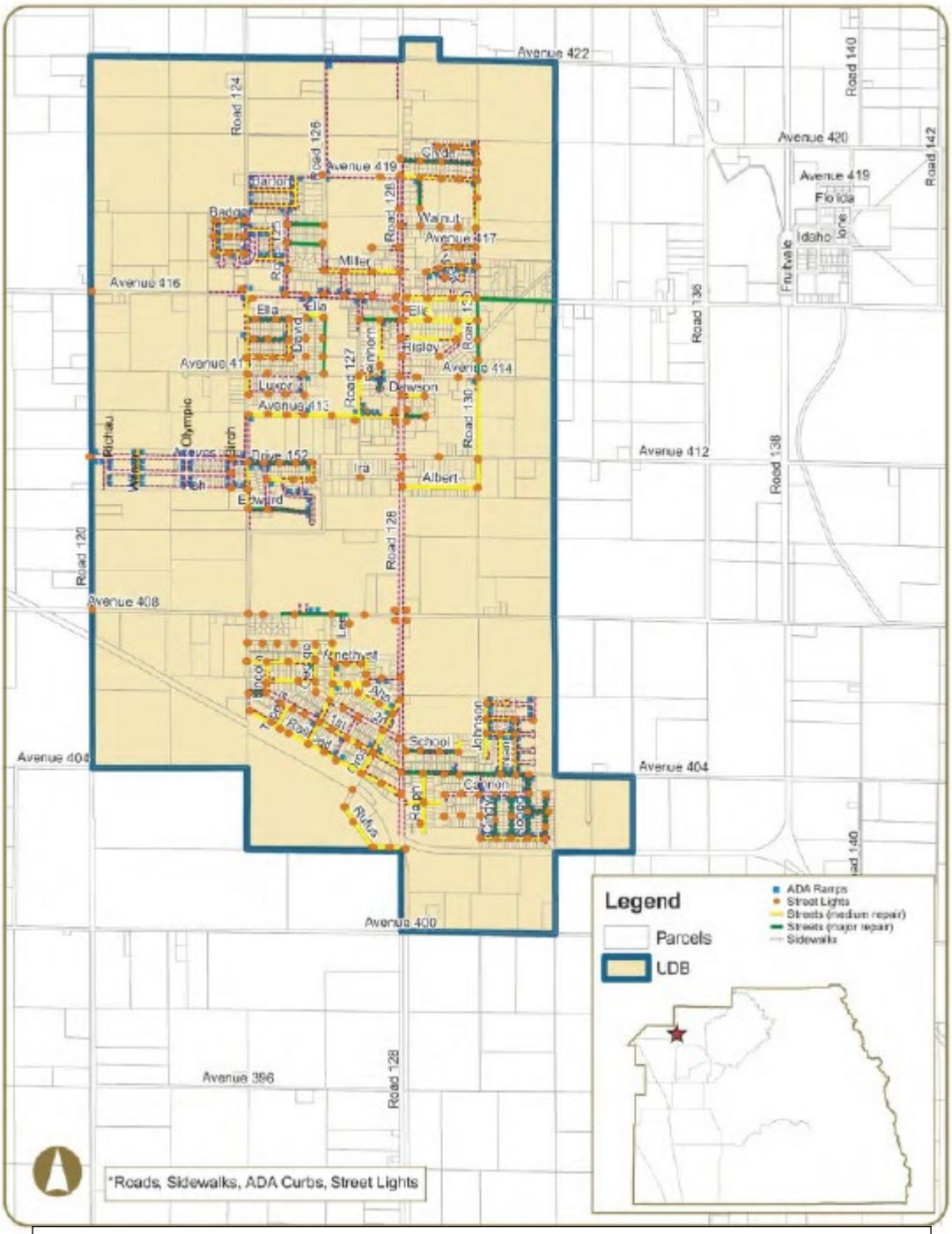
Table 28 - Road Maintenance Strategies			
31	Dennison Avenue	Road 124 to Stewart Street	CHIP
32	Dianna Road	Rosalie Avenue to North end	OLAY
33	Eddy Road	Santa Fe Drive to Avenue 407	CHIP
34	Edward Avenue	Road 124 to David Road	OLAY
35	El Monte Way	Road 128 to Road 130	CHIP
36	El Monte Way	Road 130 to Road 136	OLAY
37	El Monte Way	Elrod Road to Road 126	GRX
38	Ella Avenue	Road 124 to Elrod Road	OLAY
39	Ella Avenue	David Road to George Road	CHIP
40	Ella Avenue	Road 127 to Road 128	GRX
41	Ella Avenue	Road 128 to Road 130	CHIP
42	Elrod Road	Risley Avenue to Ella Avenue	CHIP
43	First Drive	Road 128 to Santa Fe Drive	CHIP
44	George Road	Second Street to Avenue 407	CHIP
45	Johnston Road	South end (Merlo Avenue) to North end (Quinto Court)	CHIP
46	Lincoln Road	First Drive to Amethyst Avenue	CHIP
47	Merlo Avenue	Johnston Road to Nancy Road	CHIP
48	Miller Avenue	Road 126 to Road 128	CHIP
49	Miller Avenue	Ralph Road to Road 130	CHIP
50	Mueller Road	School Avenue to North end	CHIP
51	Mueller Road	Avenue 415 to Ella Avenue	CHIP
52	Nancy Road	Rosalie Avenue to Cannon Avenue	OLAY
53	Nancy Road	Avenue 404 to Virgil Avenue	CHIP
54	Orosi Drive	Railroad Drive to Road 128	CHIP
55	Pacifica Court	Miller Avenue to South end	CHIP
56	Quinto Court	Johnston Road to East end	CHIP
57	Railroad Drive	Road 124 to Road 128	CHIP
58	Ralph Road	Avenue 404 to South end	CHIP
59	Ralph Road	Ella Avenue to El Monte Way	CHIP
60	Ralph Road	Avenue 419 to South end	OLAY
61	Risley Road	Road 124 to Elrod Road	CHIP
62	Road 124	Edward Avenue to Aceves Avenue	CHIP
63	Road 124	Luxor Avenue to El Monte Way	CHIP
64	Road 126	Avenue 414 to Ella Avenue	GRX
65	Road 127	Avenue 406 to North end	CHIP
66	Road 127	Avenue 413 to Avenue 414	CHIP
67	Road 130	Albert Avenue to Avenue 414	CHIP
68	Road 130	Avenue 414 to El Monte Way	GRX
69	Road 130	Walnut Avenue to North end	CHIP
70	Robert Road	Rosalie Avenue to Avenue 404	OLAY
71	Rosalie Avenue	Road 130 to Dianna Road	CHIP
72	Rufus Drive	Road 128 to Orosi Drive	CHIP
73	Santa Fe Drive	Railroad Drive to Second Drive	CHIP
74	School Avenue	Road 128 to Mueller Road	GRX
75	Sequoia Avenue	Wilsonia Avenue to Road 124	CHIP
76	Short Avenue	Road 124 to Lincoln Road	GRX

Table 28 - Road Maintenance Strategies			
77	Sierra Avenue	Road 128 to Road 129	CHIP
78	Stewart Street	Buena Vista Avenue to Dennison Avenue	CHIP
79	Tactacan Avenue	Road 130 to West end	CHIP
80	Topeka Drive	Railroad Drive to First Drive	CHIP
81	Twin Peaks Court	Wilsonia Avenue to East end	CHIP
82	Van Tassel Road	Whitaker Avenue to Avenue 419	CHIP
83	Virgil Avenue	Johnston Road to Nancy Road	CHIP
84	Walnut Avenue	Elrod Road to Road 126	GRX
85	Whitaker Avenue	Road 124 to Van Tassel Road	CHIP
86	Wilma Street	Ash Avenue to North end	CHIP
87	Wilsonia Avenue	Sequoia Avenue to Badger Avenue	CHIP

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

OLAY = Overlay resurfacing operation
CHIP = Chip Seal
GRXx = Grind and remix
ACST = asphalt reconstruction
RCST = cold mix reconstruction

Figure 20 - Inventory of Roadway Facilities in Cutler-Orosi



Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

Sidewalks

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

The County and VRPA Technologies surveyed existing sidewalks within the Community. **Table 29** identifies the location of existing sidewalks in Cutler-Orosi. **Figure 19** also displays this information graphically. The sidewalks represented in **Table 29** and **Figure 19** do not distinguish between ADA compliant sidewalks and noncompliant sidewalks. The majority of sidewalks represented below were constructed prior to current ADA guidelines and would be considered non-ADA compliant facilities. Such noncompliant facilities would require complete reconstruction to be considered ADA compliant.

Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
1	1st Drive	Lincoln Road to Cutler Drive	North side
2	1st Drive	150' east of Cutler Drive to 150' east of Orosi Drive	North side
3	1st Drive	Lincoln Road to Topeka Drive	South side
4	1st Drive	175' east of Topeka Drive to Santa Fe Drive	South side
5	1st Drive	75' east of Santa Fe Drive to Cutler Drive	South side
6	1st Drive	150' east of Cutler Drive to Road 128	South side
7	2nd Drive	175' east of Santa Fe Drive to Road 128	North side
8	2nd Drive	225' east of Santa Fe Drive to 150' east of Cutler Drive	South side
9	2nd Drive	Road 128 to 250' west	South side
10	Aceves Avenue	Road 120 to David Road	North side
11	Aceves Avenue	Road 120 to David Road	South side
12	Albert Avenue	Richau Street to Birch Road	North side
13	Albert Avenue	Richau Street to Birch Road	South side
14	Albert Avenue	Rancho Court to David Road	North side
15	Albert Avenue	Central Drive to David Road	South side
16	Alta Drive	250' west of Orosi Drive to 250' west	North side
17	Amethyst Avenue	150' west of Lincoln Road to 300' east of Lincoln Road	North side
18	Amethyst Avenue	George Road to 375' west	South side

Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
19	Amethyst Avenue	Eddy Avenue to 475' east	North side
20	Antonia Avenue	Nancy Road to east end	North side
21	Antonia Avenue	Nancy Road to east end	South side
22	Ash Avenue	Richau Street to Road 124	North side
23	Ash Avenue	Rancho Court to David Road	North side
24	Ash Avenue	Rancho Court to David Road	South side
25	Avenue 403	Ralph Road to 175' west	North side
26	Avenue 404	Road 128 to Robert Road	North side
27	Avenue 404	175' east of Ralph Road to Mueller Road	South side
28	Avenue 404	Road 130 to Robert Road	South side
29	Avenue 406	George Road to 275' west	North side
30	Avenue 408	Topeka Road to 450' east of Villa de Guadalupe	North side
31	Avenue 413	Road 127 to Road 128	North side
32	Avenue 413	Road 128 to east end	South side
33	Avenue 414	Road 124 to David Road	South side
34	Avenue 414	Road 127 to Beinhorn Road	North side
35	Avenue 414	Road 127 to Beinhorn Road	South side
36	Avenue 415	Road 124 to Elrod Road	North side
37	Avenue 415	Road 124 to Elrod Road	South side
38	Avenue 415	Mueller Road to 300' west	South side
39	Avenue 416	650' west of Road 124 to Road 124	North side
40	Avenue 416	Road 125 to Ella Avenue	North side
41	Avenue 416	Road 126 to Road 130	North side
42	Avenue 416	225' west of Road 124 to Road 128	South side
43	Avenue 417	Road 130 to west end	North side
44	Avenue 417	Road 130 to west end	South side
45	Avenue 419	Road 124 to Road 130	South side
46	Avenue 419	Ralph Road to Road 130	North side
47	Avenue 422	Road 126 to Road 128	South side
48	Badger Avenue	Wilsonia Avenue to Road 124	North side
49	Badger Avenue	Wilsonia Avenue to Road 124	South side
50	Barton Avenue	Road 124 to Van Tassel Road	North side
51	Barton Avenue	Road 124 to Van Tassel Road	South side
52	Beinhorn Road	South end to 575' north of Avenue 414	West side
53	Beinhorn Road	South end to 125' north of Avenue 414	East side
54	Beinhorn Road	Ella Avenue to 600' south	East side
55	Birch Road	Ash Avenue to Aceves Avenue	East side
56	Birch Road	Ash Avenue to Aceves Avenue	West side
57	Buenna Vista Avenue	Road 124 to Road 125	North side
58	Buenna Vista Avenue	Road 124 to Road 125	South side
59	Cannon Avenue	Sierra Avenue to Robert Road	North side

Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
60	Cannon Avenue	Sierra Avenue to Robert Road	South side
61	Central Drive	Albert Avenue to Ash Avenue	East side
62	Central Drive	Albert Avenue to Ash Avenue	West side
63	Cindy Road	Cannon Avenue to Rosalie Avenue	East side
64	Cindy Road	Cannon Avenue to Rosalie Avenue	West side
65	Cindy Road	Avenue 404 to Merlo Avenue	West side
66	Clyde Avenue	Road 128 to Road 130	North side
67	Clyde Avenue	Road 128 to Road 130	South side
68	Colony Street	Dennison Drive to Buenna Vista Avenue	East side
69	Colony Street	Dennison Drive to Buenna Vista Avenue	West side
70	Cutler Drive	Railroad Drive to 2nd Drive	West side
71	Cutler Drive	2nd Drive to 200' south	East side
72	David Road	Aceves Avenue to Albert Avenue	East side
73	David Road	Ash Avenue to south end	West side
74	David Road	Avenue 414 to Luxor Avenue	West side
75	David Road	Avenue 416 to 200' south	East side
76	David Road	Avenue 416 to 200' south	West side
77	Dennison Drive	Road 124 to Stewart Street	North side
78	Dennison Drive	Road 124 to Stewart Street	South side
79	Dianna Road	Rosalie Avenue to north end	East side
80	Dianna Road	Rosalie Avenue to north end	West side
81	Eddy Avenue	Amethyst Avenue to 200' south	East side
82	Eddy Avenue	Amethyst Avenue to 175' north	West side
83	Eddy Road	Miller Avenue to Avenue 416	East side
84	Eddy Road	Miller Avenue to Avenue 416	West side
85	Edward Avenue	Road 124 to David Road	South side
86	Edward Avenue	Frances Road to David Road	North side
87	Ella Avenue	Road 124 to Elrod Road	North side
88	Ella Avenue	Road 124 to Elrod Road	South side
89	Ella Avenue	Beinhorn Road to Road 128	South side
90	Elrod Road	Ella Avenue to Risley Avenue	East side
91	Elrod Road	Ella Avenue to Risley Avenue	West side
92	Emerald Avenue	Road 127 to Road 128	South side
93	Frances Road	Aceves Avenue to Edward Avenue	East side
94	Frances Road	Aceves Avenue to Edward Avenue	West side
95	George Road	Amethyst Avenue to Avenue 406	West side
96	Granite Court	Sequoia Avenue to south end	East side
97	Granite Court	Sequoia Avenue to south end	West side
98	Johnston Road	North of Quinto Court to south end	East side
99	Kahlo Court	Nancy Road to east end	North side
100	Kahlo Court	Nancy Road to east end	South side

Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
101	Lincoln Road	Short Avenue to 400' north	West side
102	Lincoln Road	Short Avenue to 300' north	East side
103	Luxor Avenue	Road 124 to David Road	North side
104	Merlo Avenue	Johnston Road to east end	North side
105	Merlo Avenue	Johnston Road to east end	South side
106	Miller Avenue	Road 126 to Claude Road	North side
107	Miller Avenue	Road 126 to Road 128	South side
108	Miller Avenue	Ralph Road to Road 130	North side
109	Miller Avenue	Ralph Road to Road 130	South side
110	Mueller Road	Avenue 404 to 175' north	West side
111	Nancy Road	Cannon Avenue to Rosalie Avenue	East side
112	Nancy Road	Cannon Avenue to Rosalie Avenue	West side
113	Nancy Road	Avenue 404 to Virgil Avenue	West side
114	Nancy Road	Avenue 404 to Virgil Avenue	East side
115	David Road	Avenue 416 to 200' south	East side
116	Olympic Street	Ash Avenue to north of Aceves Avenue	East side
117	Olympic Street	Ash Avenue to north of Aceves Avenue	West side
118	Orosi Drive	2nd Drive to Road 128	East side
119	Orosi Drive	2nd Drive to Road 128	West side
120	Orosi Drive	1st Drive to 200' north	West side
121	Orosi Drive	1st Drive to 200' north	East side
122	Orosi Drive	Railroad Drive to 200' north	East side
123	Pacifica Court	Miller Avenue to south end	East side
124	Pacifica Court	Miller Avenue to south end	West side
125	Paradise Court	Sequoia Avenue to south end	East side
126	Paradise Court	Sequoia Avenue to south end	West side
127	Quinto Court	Johnston Road to east end	North side
128	Quinto Court	Johnston Road to east end	South side
129	Railroad Drive	Road 124 to Topeka Drive	North side
130	Railroad Drive	225' east of Santa Fe Drive to Cutler Drive	North side
131	Railroad Drive	Orosi Drive to Road 128	North side
132	Ralph Road	Avenue 419 to 300' south	East side
133	Ralph Road	Avenue 419 to 300' south	West side
134	Ralph Road	Miller Avenue to Avenue 416	East side
135	Rancho Court	Albert Avenue to Ash Avenue	East side
136	Rancho Court	Albert Avenue to Ash Avenue	West side
137	Richau Street	Aceves Avenue to Ash Avenue	East side
138	Richau Street	Aceves Avenue to Ash Avenue	West side
139	Risley Avenue	Road 124 to Elrod Road	North side
140	Risley Avenue	Road 124 to Elrod Road	South side
141	Risley Avenue	Bend to Mueller Road	North side

Table 29 - Location of Existing Sidewalks			
No.	**Roadway**	**Limits**	**Location**
142	Risley Avenue	Bend to Mueller Road	South side
143	Rivera Court	Robert Road to west end	North side
144	Rivera Court	Robert Road to west end	South side
145	Road 124	Railroad Drive to Short Avenue	East side
146	Road 124	Avenue 413 to Aceves Avenue	East side
147	Road 124	Avenue 413 to Ash Avenue	West side
148	Road 124	400' south of Edward Avenue to 150' north of Ash Avenue	East side
149	Road 124	Avenue 414 to Luxor Avenue	East side
150	Road 124	Ella Avenue to Avenue 415	West side
151	Road 124	Ella Avenue to Risley Avenue	East side
152	Road 124	Avenue 419 to Buenna Vista Avenue	East side
153	Road 124	Badger Avenue to Buenna Vista Avenue	West side
154	Road 125	Whittaker Avenue to Avenue 419	East side
155	Road 125	Whittaker Avenue to Avenue 419	West side
156	Road 126	Avenue 422 to Avenue 419	East side
157	Road 126	Avenue 417 to Miller Avenue	West side
158	Road 127	Avenue 413 to 450' north	East side
159	Road 127	Avenue 416 to 500' south	West side
160	Road 127	Avenue 416 to Ella Avenue	East side
161	Road 128	Avenue 422 to 750' south	West side
162	Road 128	Avenue 419 to 550' south of Avenue 403	West side
163	Road 128	Clyde Avenue to 550' south of Avenue 403	East side
164	Road 130	Rosalie Avenue to Avenue 404	East side
165	Road 130	North end to 175' north of Walnut Avenue	West side
166	Road 130	North end to Avenue 419	East side
167	Road 130	Walnut Avenue to Avenue 416	East side
168	Robert Road	Avenue 404 to Rosalie Avenue	East side
169	Robert Road	Avenue 404 to Rosalie Avenue	West side
170	Robert Road	Virgil Avenue to north end	East side
171	Robert Road	Virgil Avenue to north end	West side
172	Rosalie Avenue	Road 130 to Dianna Road	North side
173	Rosalie Avenue	Road 130 to Dianna Road	South side
174	Santa Fe Drive	Railroad Drive to 125' north of 1st Drive	West side
175	Santa Fe Drive	1st Drive to 2nd Drive	East side
176	School Avenue	Road 128 to Mueller Road	North side
177	School Avenue	400' east of Road 128 to Mueller Road	South side
178	Sequoia Avenue	Wilsonia Avenue to Road 124	North side
179	Sequoia Avenue	Wilsonia Avenue to Road 124	South side
180	Short Avenue	Road 124 to Lincoln Road	North side
181	Short Avenue	Road 124 to Lincoln Road	South side

Table 29 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
182	Sierra Avenue	Robert Road to Dianna Road	North side
183	Sierra Avenue	Robert Road to Dianna Road	South side
184	Stewart Street	Dennison Drive to Buenna Vista Avenue	East side
185	Stewart Street	Dennison Drive to Buenna Vista Avenue	West side
186	Tactacan Avenue	Road 130 to west end	North side
187	Tactacan Avenue	Road 130 to west end	South side
188	Topeka Drive	Railroad Drive to 1st Drive	West side
189	Twin Peaks Avenue	Wilsonia Avenue to east end	North side
190	Twin Peaks Avenue	Wilsonia Avenue to east end	South side
191	Van Tassel Road	Avenue 419 to Whittaker Avenue	East side
192	Van Tassel Road	Avenue 419 to Whittaker Avenue	West side
193	Virgil Avenue	Johnston Road to Robert Road	North side
194	Virgil Avenue	Johnston Road to Robert Road	South side
195	Vista Court	Miller Avenue to south end	East side
196	Vista Court	Miller Avenue to south end	West side
197	Whittaker Avenue	Road 124 to Van Tassel Road	North side
198	Whittaker Avenue	Road 124 to Van Tassel Road	South side
199	Wilma Road	Ash Avenue to north of Aceves Avenue	East side
200	Wilma Road	Ash Avenue to north of Aceves Avenue	West side
201	Wilsonia Avenue	Badger Avenue to Sequoia Avenue	East side
202	Wilsonia Avenue	Badger Avenue to Sequoia Avenue	West side

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure

ADA Curb Ramps

The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself, which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are various ADA compliant curb ramps located within Cutler-Orosi and are listed in **Table 30** and displayed in **Figure 19**.

Table 30 - Location of Existing ADA Ramps			
No.	**East-West Roadway**	**North-South Roadways**	**Location**
1	1st Drive	Santa Fe Drive	NW Corner
2	1st Drive	Santa Fe Drive	SW Corner
3	1st Drive	Cutler Drive	SW Corner
4	1st Drive	Orosi Drive	NE Corner
5	1st Drive	Orosi Drive	NW Corner
6	Aceves Avenue	Road 120	NE Corner
7	Aceves Avenue	Road 120	SE Corner
8	Aceves Avenue	Richau Street	SE Corner
9	Aceves Avenue	Richau Street	SW Corner
10	Aceves Avenue	Wilma Road	NE Corner
11	Aceves Avenue	Wilma Road	NW Corner
12	Aceves Avenue	Wilma Road	SE Corner
13	Aceves Avenue	Wilma Road	SW Corner
14	Aceves Avenue	Olympic Street	NE Corner
15	Aceves Avenue	Olympic Street	NW Corner
16	Aceves Avenue	Olympic Street	SE Corner
17	Aceves Avenue	Olympic Street	SW Corner
18	Aceves Avenue	Birch Road	SE Corner
19	Aceves Avenue	Birch Road	SW Corner
20	Aceves Avenue	Road 124	NW Corner
21	Aceves Avenue	Road 124	SW Corner
22	Aceves Avenue	Road 124	NE Corner
23	Aceves Avenue	Road 124	SE Corner
24	Aceves Avenue	Frances Road	SE Corner
25	Aceves Avenue	Frances Road	SW Corner
26	Aceves Avenue	David Road	SW Corner
27	Albert Avenue	Richau Street	NE Corner
28	Albert Avenue	Richau Street	SE Corner
29	Albert Avenue	Wilma Road	SW Corner
30	Albert Avenue	Wilma Road	NE Corner
31	Albert Avenue	Wilma Road	SE Corner
32	Albert Avenue	Olympic Street	NE Corner
33	Albert Avenue	Olympic Street	NW Corner
34	Albert Avenue	Olympic Street	SE Corner
35	Albert Avenue	Olympic Street	SW Corner
36	Albert Avenue	Birch Road	NW Corner
37	Albert Avenue	Birch Road	SW Corner
38	Albert Avenue	Frances Road	SE Corner
39	Alta Drive	Orosi Drive	NW Corner
40	Alta Drive	Orosi Drive	SW Corner
41	Amethyst Avenue	Eddy Avenue	SE Corner

Table 30 - Location of Existing ADA Ramps			
No.	**East-West Roadway**	**North-South Roadways**	**Location**
42	Amethyst Avenue	George Road	SW Corner
43	Antonia Avenue	Nancy Road	NE Corner
44	Antonia Avenue	Nancy Road	SE Corner
45	Ash Avenue	Richau Street	NE Corner
46	Ash Avenue	Wilma Road	NE Corner
47	Ash Avenue	Wilma Road	NW Corner
48	Ash Avenue	Olympic Street	NE Corner
49	Ash Avenue	Olympic Street	NW Corner
50	Ash Avenue	Birch Road	NE Corner
51	Ash Avenue	Birch Road	NW Corner
52	Ash Avenue	Road 124	NW Corner
53	Ash Avenue	Rancho Court	NE Corner
54	Ash Avenue	Central Drive	NE Corner
55	Ash Avenue	Central Drive	NW Corner
56	Ash Avenue	David Road	SW Corner
57	Avenue 404	Mueller Road	NW Corner
58	Avenue 404	Cindy Road	NE Corner
59	Avenue 404	Cindy Road	NW Corner
60	Avenue 404	Nancy Road	NE Corner
61	Avenue 404	Nancy Road	NW Corner
62	Avenue 404	Robert Road	SE Corner
63	Avenue 404	Robert Road	SW Corner
64	Avenue 406	Eddy Avenue	SE Corner
65	Avenue 408	Villa de Guadalupe	NE Corner
66	Avenue 408	Villa de Guadalupe	NW Corner
67	Avenue 413	Road 124	SE Corner
68	Avenue 413	David Road	NE Corner
69	Avenue 413	Road 127	NE Corner
70	Avenue 413	Road 127	NW Corner
71	Avenue 413	Sequoia View Apts.	NE Corner
72	Avenue 413	Sequoia View Apts.	NW Corner
73	Avenue 414	David Road	SE Corner
74	Avenue 414	David Road	SW Corner
75	Avenue 414	Beinhorn Road	SW Corner
76	Avenue 414	Beinhorn Road	NE Corner
77	Avenue 414	Beinhorn Road	SE Corner
78	Avenue 414	Road 130	SW Corner
79	Avenue 415	Road 124	NW Corner
80	Avenue 415	Elrod Road	NW Corner
81	Avenue 416	Road 124	NE Corner
82	Avenue 416	Road 124	NW Corner

Table 30 - Location of Existing ADA Ramps			
No.	**East-West Roadway**	**North-South Roadways**	**Location**
83	Avenue 416	Road 124	SE Corner
84	Avenue 416	Road 124	SW Corner
85	Avenue 416	Road 125	NW Corner
86	Avenue 416	David Road	SE Corner
87	Avenue 416	David Road	SW Corner
88	Avenue 416	Road 126	NE Corner
89	Avenue 416	Road 126	NW Corner
90	Avenue 416	Eddy Road	NE Corner
91	Avenue 416	Eddy Road	NW Corner
92	Avenue 416	Road 127	SE Corner
93	Avenue 416	Road 127	SW Corner
94	Avenue 419	Road 124	SE Corner
95	Avenue 419	Van Tassel Road	SW Corner
96	Avenue 419	Van Tassel Road	SE Corner
97	Avenue 419	Road 126	NE Corner
98	Avenue 419	Road 126	SE Corner
99	Avenue 419	Ralph Road	SW Corner
100	Avenue 419	Road 130	SW Corner
101	Avenue 422	Road 126	SE Corner
102	Badger Avenue	Wilsonia Avenue	SE Corner
103	Badger Avenue	Road 124	NW Corner
104	Badger Avenue	Road 124	SW Corner
105	Barton Avenue	Road 124	NE Corner
106	Barton Avenue	Road 124	SE Corner
107	Barton Avenue	Van Tassel Road	NW Corner
108	Barton Avenue	Van Tassel Road	SW Corner
109	Buenna Vista Avenue	Colony Street	NE Corner
110	Buenna Vista Avenue	Stewart Street	NE Corner
111	Buenna Vista Avenue	Stewart Street	NW Corner
112	Buenna Vista Avenue	Road 125	NW Corner
113	Buenna Vista Avenue	Road 125	SW Corner
114	Cannon Avenue	Road 130	NE Corner
115	Cannon Avenue	Road 130	SE Corner
116	Cannon Avenue	Cindy Road	SE Corner
117	Cannon Avenue	Cindy Road	SW Corner
118	Cannon Avenue	Nancy Road	SE Corner
119	Cannon Avenue	Nancy Road	SW Corner
120	Cannon Avenue	Robert Road	NW Corner
121	Cannon Avenue	Robert Road	SW Corner
122	Dennison Drive	Road 124	NE Corner
123	Dennison Drive	Road 124	SE Corner

Table 30 - Location of Existing ADA Ramps			
No.	**East-West Roadway**	**North-South Roadways**	**Location**
124	Dennison Drive	Colony Street	SE Corner
125	Dennison Drive	Colony Street	SW Corner
126	Dennison Drive	Stewart Street	SW Corner
127	Edward Avenue	David Road	NW Corner
127	Edward Avenue	David Road	SW Corner
128	Ella Avenue	David Road	SE Corner
130	Ella Avenue	Beinhorn Road	SE Corner
131	Emerald Avenue	Pearl Road	SE Corner
132	Emerald Avenue	Pearl Road	SW Corner
133	Kahlo Court	Nancy Road	NE Corner
134	Kahlo Court	Nancy Road	SE Corner
135	Luxor Avenue	David Road	NW Corner
136	Merlo Avenue	Nancy Road	NE Corner
137	Merlo Avenue	Nancy Road	SE Corner
138	Merlo Avenue	Johnston Road	NE Corner
139	Merlo Avenue	Johnston Road	SE Corner
140	Merlo Avenue	Cindy Road	SE Corner
141	Merlo Avenue	Cindy Road	SW Corner
142	Merlo Avenue	Nancy Road	NW Corner
143	Merlo Avenue	Nancy Road	SW Corner
144	Miller Avenue	Road 130	NW Corner
145	Miller Avenue	Road 130	SW Corner
146	Miller Avenue	Pacifica Court	SW Corner
147	Miller Avenue	Pacifica Court	SE Corner
148	Miller Avenue	Vista Court	SE Corner
149	Miller Avenue	Vista Court	SW Corner
150	Miller Avenue	Ralph Road	SE Corner
151	Quinto Court	Johnston Road	NE Corner
152	Quinto Court	Johnston Road	SE Corner
153	Railroad Drive	Cutler Drive	NE Corner
154	Railroad Drive	Cutler Drive	NW Corner
155	Rivera Court	Robert Road	NW Corner
156	Rivera Court	Robert Road	SW Corner
157	Rosalie Avenue	Road 130	NE Corner
158	Rosalie Avenue	Cindy Road	NE Corner
159	Rosalie Avenue	Cindy Road	NW Corner
160	Rosalie Avenue	Nancy Road	NE Corner
161	Rosalie Avenue	Nancy Road	NW Corner
162	Rosalie Avenue	Robert Road	NE Corner
163	Rosalie Avenue	Robert Road	NW Corner
164	Rosalie Avenue	Dianna Road	NW Corner

Table 30 - Location of Existing ADA Ramps			
No.	**East-West Roadway**	**North-South Roadways**	**Location**
165	Sequoia Avenue	Granite Court	SW Corner
166	Sequoia Avenue	Paradise Court	SW Corner
167	Sequoia Avenue	Wilsonia Avenue	NE Corner
168	Sequoia Avenue	Road 124	NW Corner
169	Sierra Avenue	Robert Road	NE Corner
170	Sierra Avenue	Robert Road	NW Corner
171	Sierra Avenue	Robert Road	SE Corner
172	Sierra Avenue	Robert Road	SW Corner
173	Tactacan Avenue	Road 130	NW Corner
174	Tactacan Avenue	Road 130	SW Corner
175	Twin Peaks Avenue	Wilsonia Avenue	NE Corner
176	Twin Peaks Avenue	Wilsonia Avenue	SE Corner
177	Virgil Avenue	Johnston Road	NE Corner
178	Virgil Avenue	Johnston Road	SE Corner
179	Virgil Avenue	Nancy Road	SE Corner
180	Virgil Avenue	Nancy Road	SW Corner
181	Virgil Avenue	Robert Road	NW Corner
182	Whittaker Avenue	Road 124	NE Corner
183	Whittaker Avenue	Road 124	SE Corner
184	Whittaker Avenue	Road 125	SE Corner
185	Whittaker Avenue	Road 125	SW Corner
186	Whittaker Avenue	Van Tassel Road	NW Corner

Source: County of Tulare Public Works, August 2013

Street Lights

Streetlights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

Table 31 identifies the location of existing streetlights that are maintained by Tulare County, in Cutler-Orosi, as well as their specifications. **Figure 19** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type "W" represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete.

Table 31 - Existing Street Lights

No	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	1st Drive	Lincoln Road	NE Corner	N/A	5800	W	S	PG&E
2	1st Drive	Topeka Drive	NE Corner	1526	5800	W	S	PG&E
3	1st Drive	Santa Fe Drive	NW Corner	1528	5800	W	S	PG&E
4	1st Drive	Cutler Drive	SW Corner	1582	5800	W	S	PG&E
5	1st Drive	Orosi Drive	SE Corner	1523	5800	W	W	PG&E
6	1st Drive	Between Topeka Drive	North Side	1526	5800	W	S	PG&E
7	1st Drive	Between Santa Fe Drive	South Side	1560	5800	W	N	PG&E
8	1st Drive	Between Cutler Drive	South Side	1523	5800	W	W	PG&E
9	2nd Drive	George Road	West Side	N/A	5800	W	NE	PG&E
10	2nd Drive	Orosi Drive	NE Corner	1524	5800	W	S	PG&E
11	2nd Drive	Road 128	East Side	1606	5800	M	E	PG&E
12	2nd Drive	Santa Fe Drive	SW Corner	1586	5800	W	N	PG&E
13	2nd Drive	Between Santa Fe Drive and Cutler Drive	South Side	N/A	5800	W	N	PG&E
14	2nd Drive	Cutler Drive	SE Corner	1562	5800	W	N	PG&E
15	Aceves Avenue	Road 124	NE Corner	2025	N/A	M	E	PG&E
16	Aceves Avenue	Frances Road	North Side	2026	N/A	M	S	PG&E
17	Aceves Avenue	Between Frances Road and David Road	North Side	2027	N/A	N/A	S	PG&E
18	Aceves Avenue	Between Frances Road and David Road	North Side	N/A	N/A	N/A	S	PG&E
19	Aceves Avenue	David Road	East Side	2029	N/A	W	W	PG&E
20	Aceves Avenue	Birch Road	North Side	2642	5800	N/A	S	PG&E
21	Aceves Avenue	Road 120	NE Corner	N/A	5800	S	W	PG&E
22	Albert Avenue	Frances Road	West Side	N/A	N/A	N/A	E	PG&E
23	Albert Avenue	Between Rancho Court	North Side	2251	N/A	N/A	S	PG&E
24	Albert	David Road	East Side	N/A	N/A	N/A	W	PG&E

Table 31 - Existing Street Lights

	Avenue							
25	Albert Avenue	Birch Road	East Side	2643	5800	N/A	W	PG&E
26	Albert Avenue	Road 128	SE Corner	2449	5800	N/A	W	PG&E
27	Albert Avenue	Road 130	South Side	2280	5800	N/A	N	PG&E
28	Alta Drive	South of Avenue 406	West Side	1600	5800	W	S	PG&E
29	Alta Drive	Between Avenue 406 and Orosi Drive	South Side	N/A	5800	N/A	N	PG&E
30	Alta Drive	Orosi Drive	SW Corner	1602	5800	W	E	PG&E
31	Amethyst Avenue	Road 124	SE Corner	1544	5800	W	E	PG&E
32	Amethyst Avenue	Lincoln Road	NE Corner	N/A	5800	W	S	PG&E
33	Amethyst Avenue	Road 125	North Side	1579	5800	W	S	PG&E
34	Amethyst Avenue	George Road	East Side	1580	5800	W	W	PG&E
35	Amethyst Avenue	Eddy Avenue	West Side	N/A	5800	W	E	PG&E
36	Amethyst Avenue	East of Eddy Avenue	South Side	1580	5800	W	N	PG&E
37	Amethyst Avenue	Road 127	East Side	1595	5800	W	W	PG&E
38	Ash Avenue	Birch Road	NW Corner	2644	5800	N/A	S	PG&E
39	Ash Avenue	Road 124	NW Corner	N/A	N/A	N/A	E	PG&E
40	At south end	Ralph Road	East Side	1534	5800	W	S	PG&E
41	Avenue 404	Road 128	NE Corner	1520	5800	W	W	PG&E
42	Avenue 404	Ralph Road	SE Corner	1525	5800	W	N	PG&E
43	Avenue 404	Mueller Road	South Side	1505	5800	W	N	PG&E
44	Avenue 404	Nancy Road	NW Corner	2758	5800	M	S	PG&E
45	Avenue 404	Between Ralph Road and Mueller Road	South Side	1554	5800	W	N	PG&E
46	Avenue 404	Cindy Road	South Side	2390	5800	S	N	PG&E
47	Avenue 404	Robert Road	SE Corner	2022	5800	S	N	PG&E
48	Avenue 406	Lincoln Road	West Side	1582	5800	W	E	PG&E
49	Avenue 406	Eddy Avenue	West Side	N/A	N/A	N/A	E	PG&E
50	Avenue 406	Between Eddy Avenue and Alta Drive	North Side	N/A	N/A	N/A	S	PG&E
51	Avenue 406	Between Alta Drive and Road 127	North Side	N/A	N/A	N/A	S	PG&E
52	Avenue 406	Between Lincoln Road and George Road	North Side	N/A	N/A	N/A	S	PG&E
53	Avenue 406	George Road	East Side	N/A	N/A	N/A	W	PG&E
54	Avenue 407	George Road	SE Corner	1538	5800	W	N	PG&E
55	Avenue 407	Eddy Avenue	SE Corner	1539	5800	W	W	PG&E
56	Avenue 407	Road 124	West Side	1575	5800	W	E	PG&E
57	Avenue 407	Lincoln Road	South Side	1576	5800	W	N	PG&E
58	Avenue 407	Topeka Drive	South Side	1578	5800	W	N	PG&E
59	Avenue 407	Between Topeka Drive	South Side	N/A	N/A	N/A	N	PG&E
60	Avenue 408	Road 120	NW Corner	2762	9500	W	S	PG&E
61	Avenue 408	Road 124	NW Corner	1483	5800	N/A	S	PG&E
62	Avenue 408	Lincoln Road	NW Corner	1586	5800	W	S	PG&E
63	Avenue 408	Villa De	NW Corner	1569	5800	N/A	S	PG&E

Table 31 - Existing Street Lights

		Guadalupe Apts.						
64	Avenue 408	West of Lee Road	North Side	1593	5800	N/A	S	PG&E
65	Avenue 408	SR 63	NW Corner	N/A	5800	N/A	S	PG&E
66	Avenue 408	SR 63	NE Corner	N/A	5800	N/A	W	PG&E
67	Avenue 408	SR 63	SE Corner	N/A	5800	N/A	N	PG&E
68	Avenue 408	SR 63	SW Corner	N/A	5800	N/A	E	PG&E
69	Avenue 408	West of SR 63	North Side	N/A	N/A	N/A	S	PG&E
70	Avenue 412	SR 63	SE Corner	N/A	5800	N/A	N	PG&E
71	Avenue 412	Between Road 128 and Road 130	South Side	N/A	5800	N/A	N	PG&E
72	Avenue 412	Road 130	SE Corner	2279	N/A	W	N	PG&E
73	Avenue 413	Road 124	SE Corner	1668	N/A	N/A	W	PG&E
74	Avenue 413	Between Road 124 and David Road	North Side	1678	N/A	N/A	S	PG&E
75	Avenue 413	Between Road 124 and	North Side	1680	N/A	N/A	S	PG&E
76	Avenue 413	David Road	NW Corner	1682	N/A	N/A	SE	PG&E
77	Avenue 413	Between David Road and Road 127	South Side	1683	N/A	N/A	N	PG&E
78	Avenue 413	Road 127	NE Corner	N/A	N/A	N/A	S	PG&E
79	Avenue 413	Road 127	South Side	N/A	N/A	N/A	N	PG&E
80	Avenue 413	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
81	Avenue 413	SR 63	NW Corner	N/A	N/A	N/A	S	PG&E
82	Avenue 413	SR 63	SE Corner	N/A	N/A	N/A	N	PG&E
83	Avenue 413	SR 63	SW Corner	N/A	N/A	N/A	E	PG&E
84	Avenue 413	East end	North Side	2639	5800	N/A	S	PG&E
85	Avenue 414	David Road	SE Corner	1651	5800	W	W	PG&E
86	Avenue 414	Ledbetter Drive	NW Corner	1641	5800	W	SE	PG&E
87	Avenue 414	Road 127	SE Corner	1636	5800	N/A	W	PG&E
88	Avenue 414	Road 126	East Side	1676	N/A	N/A	W	PG&E
89	Avenue 414	Road 124	West Side	1667	N/A	N/A	E	PG&E
90	Avenue 414	East of Road 124	North Side	1669	N/A	N/A	S	PG&E
91	Avenue 414	Road 130	NE Corner	1662	5800	N/A	W	PG&E
92	Avenue 414	SR 63	NE Corner	N/A	5800	N/A	W	PG&E
93	Avenue 414	East of Road 128	North Side	1660	N/A	N/A	S	PG&E
94	Avenue 415	SR 63	NE Corner	1715	5800	W	W	PG&E
95	Avenue 415	Mueller Road	SE Corner	1643	5800	W	W	PG&E
96	Avenue 415	Road 128	NE Corner	1666	5800	W	N	PG&E
97	Avenue 415	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
98	Avenue 415	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
99	Avenue 415	Elrod Road	NW Corner	2033	N/A	S	S	PG&E
100	Avenue 416	Road 124	NW Corner	3338	9500	W	S	PG&E
101	Avenue 416	Road 124	SE Corner	3341	9500	W	N	PG&E
102	Avenue 416	Road 125	NW Corner	1648	5800	O	S	PG&E
103	Avenue 416	David Road	NW Corner	1647	5800	O	S	PG&E
104	Avenue 416	Road 126	NW Corner	1611	5800	O	S	PG&E
105	Avenue 416	Eddy Road	NW Corner	2187	5800	O	S	PG&E
106	Avenue 416	Road 127	NW Corner	1632	5800	M	S	PG&E
107	Avenue 416	Claude Road	NE Corner	1613	5800	O	S	PG&E
108	Avenue 416	Ralph Road	NE Corner	2188	5800	W	S	PG&E
109	Avenue 416	Road 130	NE Corner	1649	5800	W	S	PG&E
110	Avenue 416	Road 120	NW Corner	3259	16000	W	S	PG&E
111	Avenue 416	Lincoln Road	NE Corner	2188	5800	N/A	S	PG&E
112	Avenue 416	SR 63	NE Corner	2358	N/A	N/A	W	PG&E
113	Avenue 416	SR 63	NW Corner	2357	N/A	N/A	S	PG&E
114	Avenue 416	SR 63	SE Corner	2356	N/A	N/A	N	PG&E
115	Avenue 416	SR 63	SW Corner	2355	16000	N/A	E	PG&E

Table 31 - Existing Street Lights

116	Avenue 417	Road 125	NE Corner	1639	5800	W	W	PG&E
117	Avenue 417	Road 126	SW Corner	1634	5800	W	E	PG&E
118	Avenue 417	SR 63	SW Corner	2189	5800	M	E	PG&E
119	Avenue 417	Claude Road	SW Corner	1631	5800	W	E	PG&E
120	Avenue 417	Road 130	SW Corner	2199	5800	S	E	PG&E
121	Avenue 417	West of Road 130	South Side	2198	5800	S	N	PG&E
122	Avenue 418	SR 63	West Side	1637	5800	N/A	E	PG&E
123	Avenue 419	Ralph Road	NW Corner	1655	5800	W	S	PG&E
124	Avenue 419	Road 126	SW Corner	1689	N/A	W	E	PG&E
125	Avenue 419	Claude Road	South Side	1690	N/A	W	N	PG&E
126	Avenue 419	SR 63	SW Corner	1716	N/A	W	W	PG&E
127	Avenue 419	Between Road 129 and Road 130	North Side	1696	5800	W	S	PG&E
127	Avenue 419	Between Road 129 and	North Side	1690	5800	W	S	PG&E
128	Avenue 419	Road 130	East Side	1698	5800	W	W	PG&E
130	Badger Avenue	Wilsonia Avenue	West Side	2920	N/A	N/A	E	PG&E
131	Badger Avenue	Between Wilsonia	North Side	2921	N/A	N/A	S	PG&E
132	Badger Avenue	Road 124	SW Corner	2922	N/A	N/A	E	PG&E
133	Between Avenue 414 and Ella Avenue	David Road	East Side	1671	N/A	W	W	PG&E
134	Between Avenue 414	David Road	East Side	1672	N/A	N/A	W	PG&E
135	Between Avenue 414	Road 126	West Side	1674	N/A	N/A	E	PG&E
136	Between Avenue 414 and Ella Avenue	Road 126	West Side	1675	N/A	N/A	E	PG&E
137	Cannon Avenue	Between Sierra Avenue and Road 130	North Side	1549	5800	W	W	PG&E
138	Cannon Avenue	Road 130	NE Corner	1550	5800	W	W	PG&E
139	Cannon Avenue	Robert Road	East Side	2023	5800	S	W	PG&E
140	Cannon Avenue	Nancy Road	North Side	2021	5800	S	S	PG&E
141	Cannon Avenue	Cindy Road	North Side	2020	5800	S	S	PG&E
142	Cannon Avenue	Road 130	NE Corner	N/A	N/A	N/A	W	PG&E
143	Clyde Avenue	SR 63	NE Corner	1695	16000	W	W	PG&E
144	Clyde Avenue	East of SR 63	North Side	1707	5800	W	S	PG&E
145	Clyde Avenue	Between SR 63 and Road 130	North Side	N/A	5800	N/A	S	PG&E
146	Clyde Avenue	Road 130	East Side	1710	5800	W	W	PG&E
147	Dawson Avenue	SR 63	NE Corner	1652	5800	M	W	PG&E
148	Dawson Avenue	East end	East Side	1652	5800	N/A	W	PG&E
149	Edward Avenue	Road 124	SE Corner	2249	N/A	N/A	N	PG&E
150	Edward	Frances Road	South Side	2249	N/A	M	S	PG&E

Table 31 - Existing Street Lights

	Avenue							
151	Ella Avenue	Beinhorn Road	SW Corner	1654	5800	W	N	PG&E
152	Ella Avenue	David Road	NE Corner	1650	5800	W	W	PG&E
153	Ella Avenue	Road 127	NW Corner	1629	5800	W	E	PG&E
154	Ella Avenue	Ralph Road	SE Corner	1645	5800	W	N	PG&E
155	Ella Avenue	Mueller Road	NW Corner	1646	5800	W	S	PG&E
156	Ella Avenue	SR 63	SW Corner	1615	5800	W	E	PG&E
157	Ella Avenue	Elrod Road	NE Corner	2037	N/A	S	SW	PG&E
158	Ella Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
159	Ella Avenue	Between Road 124 and Elrod Road	North Side	2035	N/A	S	S	PG&E
160	Ella Avenue	Road 126	North Side	1673	N/A	N/A	S	PG&E
161	Ella Avenue	Road 130	East Side	1665	9500	W	W	PG&E
162	Emerald Avenue	Road 127	West Side	1598	5800	W	E	PG&E
163	Emerald Avenue	Pearl Road	North Side	1590	5800	W	S	PG&E
164	Emerald Avenue	SR 63	East Side	1604	9500	S	E	PG&E
165	Hazel Avenue	Lee Road	NW Corner	1540	5800	W	E	PG&E
166	Ira Avenue	SR 63	East Side	N/A	5800	N/A	W	PG&E
167	Ira Avenue	Road 127	North Side	3036	N/A	W	S	PG&E
168	Ledbetter Drive	Road 130	East Side	1663	5800	W	W	PG&E
169	Luxor Avenue	Road 124	SW Corner	1653	5800	N/A	E	PG&E
170	Luxor Avenue	David Road	SW Corner	1681	N/A	N/A	E	PG&E
171	Luxor Avenue	Between Road 124 and	South Side	1677	N/A	N/A	N	PG&E
172	Luxor Avenue	Between Road 124 and David Road	South Side	N/A	N/A	N/A	N	PG&E
173	Merlo Avenue	Johnston Road	SW Corner	2759	5800	W	E	PG&E
174	Merlo Avenue	Cindy Road	SE Corner	2756	5800	M	N	PG&E
175	Merlo Avenue	Nancy Road	SW Corner	2757	5800	M	E	PG&E
176	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E
177	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
178	Miller Avenue	Eddy Road	NW Corner	1618	5800	W	S	PG&E
179	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
180	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
181	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
182	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
183	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
184	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
185	North of Avenue 414	Beinhorn Road	West Side	1687	N/A	N/A	E	PG&E
186	North of	Dianna Road	East Side	2269	5800	S	W	PG&E

Table 31 - Existing Street Lights

	Rosalie Avenue							
100	North of Rosalie Avenue	Nancy Road	East Side	2077	5800	S	W	PG&E
101	North of Rosalie Avenue	Cindy Road	East Side	2076	5800	S	W	PG&E
102	North of Sierra Avenue	Dianna Road	North Side	2079	5800	S	S	PG&E
103	Orosi Drive	Road 128	SW Corner	1607	5800	W	E	PG&E
104	Quinto Court	Johnston Road	West Side	2651	5800	M	E	PG&E
105	Railroad Drive	Orosi Drive	SE Corner	1522	5800	W	N	PG&E
106	Railroad Drive	Santa Fe Drive	NW Corner	1527	5800	W	S	PG&E
107	Railroad Drive	Road 128	East Side	N/A	5800	W	W	PG&E
108	Railroad Drive	Road 124	East Side	1543	5800	N/A	SW	PG&E
109	Railroad Drive	Lincoln Road	North Side	N/A	5800	W	S	PG&E
110	Railroad Drive	Topeka Drive	NE Corner	1591	5800	W	S	PG&E
111	Railroad Drive	Between Topeka Drive and Santa Fe Drive	North Side	1592	5800	N/A	S	PG&E
112	Railroad Drive	Between Santa Fe Drive and Cutler Drive	North Side	N/A	5800	W	S	PG&E
113	Railroad Drive	Cutler Drive	South Side	N/A	5800	W	N	PG&E
114	Railroad Drive	Between Orosi Drive and Road 128	South Side	1522	5800	W	N	PG&E
115	Risley Avenue	Between Road 128 and Avenue 415	South Side	1642	5800	W	N	PG&E
116	Risley Avenue	Road 124	NE Corner	2044	N/A	N/A	W	PG&E
117	Risley Avenue	Between Road 124 and Elrod Road	North Side	2045	N/A	S	S	PG&E
118	Risley Avenue	Elrod Road	NW Corner	2047	N/A	W	W	PG&E
119	Risley Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
120	Risley Avenue	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
121	Rosalie Avenue	Road 130	NE Corner	453	5800	S	W	PG&E
122	Rosalie Avenue	Nancy Road	South Side	2180	5800	S	N	PG&E
123	Rosalie Avenue	Robert Road	South Side	2267	5800	S	N	PG&E
124	Rosalie Avenue	Dianna Road	South Side	2268	5800	S	NW	PG&E
125	Rosalie Avenue	Cindy Road	South Side	N/A	N/A	N/A	N	PG&E
126	Rufus Drive	Orosi Drive	North Side	1566	5800	W	S	PG&E
127	Rufus Drive	Orosi Drive	East Side	1533	5800	W	W	PG&E
127	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E

Table 31 - Existing Street Lights								
128	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
130	Miller Avenue	Eddy Road	NW Corner	1618	5800	W	S	PG&E
131	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
132	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
133	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
134	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
135	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
136	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
137	North of Avenue 414	Beinhorn Road	West Side	1687	N/A	N/A	E	PG&E
138	North of Rosalie Avenue	Dianna Road	East Side	2269	5800	S	W	PG&E
139	North of Rosalie Avenue	Nancy Road	East Side	2077	5800	S	W	PG&E
140	North of Rosalie Avenue	Cindy Road	East Side	2076	5800	S	W	PG&E
141	North of Sierra Avenue	Dianna Road	North Side	2079	5800	S	S	PG&E
142	Orosi Drive	Road 128	SW Corner	1607	5800	W	E	PG&E
143	Quinto Court	Johnston Road	West Side	2651	5800	M	E	PG&E
144	Railroad Drive	Orosi Drive	SE Corner	1522	5800	W	N	PG&E
145	Railroad Drive	Santa Fe Drive	NW Corner	1527	5800	W	S	PG&E
146	Railroad Drive	Road 128	East Side	N/A	5800	W	W	PG&E
147	Railroad Drive	Road 124	East Side	1543	5800	N/A	SW	PG&E
148	Railroad Drive	Lincoln Road	North Side	N/A	5800	W	S	PG&E
149	Railroad Drive	Topeka Drive	NE Corner	1591	5800	W	S	PG&E
150	Railroad Drive	Between Topeka Drive and Santa Fe Drive	North Side	1592	5800	N/A	S	PG&E
151	Railroad Drive	Between Santa Fe Drive and Cutler Drive	North Side	N/A	5800	W	S	PG&E
152	Railroad Drive	Cutler Drive	South Side	N/A	5800	W	N	PG&E
153	Railroad Drive	Between Orosi Drive and Road 128	South Side	1522	5800	W	N	PG&E
154	Risley Avenue	Between Road 128 and Avenue 415	South Side	1642	5800	W	N	PG&E
155	Risley Avenue	Road 124	NE Corner	2044	N/A	N/A	W	PG&E
156	Risley Avenue	Between Road 124 and Elrod Road	North Side	2045	N/A	S	S	PG&E

Table 31 - Existing Street Lights								
157	Risley Avenue	Elrod Road	NW Corner	2047	N/A	W	W	PG&E
158	Risley Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
159	Risley Avenue	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
160	Rosalie Avenue	Road 130	NE Corner	453	5800	S	W	PG&E
161	Rosalie Avenue	Nancy Road	South Side	2180	5800	S	N	PG&E
162	Rosalie Avenue	Robert Road	South Side	2267	5800	S	N	PG&E
163	Rosalie Avenue	Dianna Road	South Side	2268	5800	S	NW	PG&E
164	Rosalie Avenue	Cindy Road	South Side	N/A	N/A	N/A	N	PG&E
165	Rufus Drive	Orosi Drive	North Side	1566	5800	W	S	PG&E
166	Rufus Drive	Orosi Drive	East Side	1533	5800	W	W	PG&E
167	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E
168	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
169	Miller Avenue	Eddy Road	NW Corner	1618	5800	W	S	PG&E
170	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
171	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
172	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
173	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
174	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
175	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
176	North of Avenue 414	Beinhorn Road	West Side	1687	N/A	N/A	E	PG&E
177	North of Rosalie Avenue	Dianna Road	East Side	2269	5800	S	W	PG&E
178	North of Rosalie Avenue	Nancy Road	East Side	2077	5800	S	W	PG&E
179	North of Rosalie Avenue	Cindy Road	East Side	2076	5800	S	W	PG&E
180	North of Sierra Avenue	Dianna Road	North Side	2079	5800	S	S	PG&E
181	Orosi Drive	Road 128	SW Corner	1607	5800	W	E	PG&E
182	Quinto Court	Johnston Road	West Side	2651	5800	M	E	PG&E
183	Railroad Drive	Orosi Drive	SE Corner	1522	5800	W	N	PG&E
184	Railroad Drive	Santa Fe Drive	NW Corner	1527	5800	W	S	PG&E
185	Railroad Drive	Road 128	East Side	N/A	5800	W	W	PG&E
186	Railroad Drive	Road 124	East Side	1543	5800	N/A	SW	PG&E
187	Railroad Drive	Lincoln Road	North Side	N/A	5800	W	S	PG&E

Table 31 - Existing Street Lights

188	Railroad Drive	Topeka Drive	NE Corner	1591	5800	W	S	PG&E
189	Railroad Drive	Between Topeka Drive and Santa Fe Drive	North Side	1592	5800	N/A	S	PG&E
190	Railroad Drive	Between Santa Fe Drive and Cutler Drive	North Side	N/A	5800	W	S	PG&E
191	Railroad Drive	Cutler Drive	South Side	N/A	5800	W	N	PG&E
192	Railroad Drive	Between Orosi Drive and Road 128	South Side	1522	5800	W	N	PG&E
193	Risley Avenue	Between Road 128 and Avenue 415	South Side	1642	5800	W	N	PG&E
194	Risley Avenue	Road 124	NE Corner	2044	N/A	N/A	W	PG&E
195	Risley Avenue	Between Road 124 and Elrod Road	North Side	2045	N/A	S	S	PG&E
196	Risley Avenue	Elrod Road	NW Corner	2047	N/A	W	W	PG&E
197	Risley Avenue	Between Road 124 and Elrod Road	North Side	N/A	N/A	N/A	S	PG&E
198	Risley Avenue	SR 63	NE Corner	N/A	N/A	N/A	W	PG&E
199	Rosalie Avenue	Road 130	NE Corner	453	5800	S	W	PG&E
200	Rosalie Avenue	Nancy Road	South Side	2180	5800	S	N	PG&E
201	Rosalie Avenue	Robert Road	South Side	2267	5800	S	N	PG&E
202	Rosalie Avenue	Dianna Road	South Side	2268	5800	S	NW	PG&E
203	Rosalie Avenue	Cindy Road	South Side	N/A	N/A	N/A	N	PG&E
204	Rufus Drive	Orosi Drive	North Side	1566	5800	W	S	PG&E
205	Rufus Drive	Orosi Drive	East Side	1533	5800	W	W	PG&E
206	Miller Avenue	Road 125	SE Corner	1638	5800	W	W	PG&E
207	Miller Avenue	Road 126	NW Corner	1628	5800	W	E	PG&E
208	Miller Avenue	Eddy Road	NW Corner	1618	5800	W	S	PG&E
209	Miller Avenue	Claude Road	NW Corner	1627	5800	W	E	PG&E
210	Miller Avenue	SR 63	NW Corner	1633	5800	O	E	PG&E
211	Miller Avenue	Ralph Road	SE Corner	1704	5800	W	W	PG&E
212	Miller Avenue	Road 130	East Side	3033	5800	W	W	PG&E
213	Miller Avenue	Pacifica Court	SE Corner	3327	5800	S	W	PG&E
214	Miller Avenue	Vista Court	SE Corner	3326	5800	S	N	PG&E
215	Rufus Drive	Road 127	North Side	1566	5800	W	S	PG&E

Source: Tulare County Housing Element – Action Program 9 Existing Infrastructure,

Cutler-Orosi Community Plan Update

The Need for a Community Plan

Purpose

The Cutler-Orosi Community Plan was adopted in 1988. Conditions in Cutler-Orosi have changed and policies and implementation strategies should be updated to address existing conditions. This community plan update will be used to foster economic development by identifying opportunities for development. This community plan is also a part of the implementation of the San Joaquin Valley Regional Blueprint, Tulare County Regional Blueprint, Transportation Concept Report State Route 63-District, and the Tulare County 2030 General Plan.

Relationship to the General Plan

Tulare County's General Plan provides a comprehensive statement of the objectives, themes and policies, which the community is seeking to achieve in the areas of land use, growth management, community design, transportation, open space, parks and public facilities, environmental conservation, health and safety, noise, and housing. This Community Plan, as an instrument which promulgates and is an extension of the General Plan, incorporates, by definition, the stated general objectives, themes and policies and, where more specific objectives and policies are stated, makes reference to such objectives and policies and provides further elaboration on the ways in which the Community Plan is responsive to this guidance. Relevant General Plan goals, policies, and programs that provide direction and input to this Community Plan are provided in this document. In addition, this plan has specific policies for the Community of Cutler-Orosi.

Planning Framework

San Joaquin Valley Regional Blueprint

"The San Joaquin Valley Blueprint is the result of an unprecedented effort of the eight Valley Regional Planning Agencies (RPA), that include the Fresno Council of Governments, the Kern Council of Governments, the Kings County Association of Governments, the Madera County Transportation Commission, the Merced County Association of Governments, the San Joaquin Council of Governments, the Stanislaus Council of Governments, and the Tulare County Association of Governments, to develop a long-term regional growth strategy for the future of the San Joaquin Valley. Following three-years of visioning and outreach by the eight Valley RPAs, the Regional Policy Council (RPC), the decision-making body for the Valleywide process, adopted the Valley Blueprint in April 2009.

The Blueprint is a long-range vision for a more efficient, sustainable, and livable future for the Valley. The Valleywide Blueprint is made up three elements: a 2050 growth scenario diagram that identifies areas of existing development, new development, and future regional transit and highway improvements; a Valleywide average target density of 6.8 units per acre for new residential growth to the year 2050; and a set of 12 Smart Growth Principles. Importantly, the Blueprint recognizes and

incorporates by reference the visioning and outreach efforts undertaken by the eight Valley Regional Planning Agencies."[41]

Tulare County Regional Blueprint

"TCAG and its member agencies felt that it was important to prepare a Tulare County Regional Blueprint that clarified Tulare County's role in the Blueprint process. The Tulare County Regional Blueprint is a stand-alone policy document that is consistent with the San Joaquin Valley Regional Blueprint. This document represents Tulare County's local vision and goals as a participant in the San Joaquin Valley Regional Blueprint process."[42] Key elements of the preferred growth scenario outlined in the Tulare County Regional Blueprint include 25% increase in overall density and focused growth in urban areas.

TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Cutler-Orosi. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describes bicycle circulation patterns and pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

Senate Bill 244, Housing Element Implementation

Senate Bill (SB) 244, passed by the California Legislature and signed into law in 2011, requires California municipalities analyze the inequality and infrastructure deficits within disadvantaged unincorporated communities (DUCs), which lack basic community infrastructure like sidewalks, safe drinking water, and adequate waste processing. As a part of this process and the implementation of the Housing Element the County continues to identify housing related infrastructure needs, such as; water, sewer, natural gas or streetlights, using community needs assessments, housing condition surveys, public comments at community meetings, redevelopment implementation plans and amendments, community plans and other relevant information from the Health & Human Services Agency (HHSA) Environmental Health Services, Regional Water Quality Control Board, public utility districts, community services districts and other agencies. The County of Tulare prepared a 2016 Disadvantage Unincorporated Communities Assessment (infrastructure needs assessment) of the County in conjunction with SB 244 and Action Program 9 as part of the 2015 Tulare County Housing Element Update.

Transportation Concept Report (TRC)-State Route (SR) 63

The purpose of the TCR is to evaluate current, project conditions along the route, and communicate the vision for the development of each route in each Caltrans District during a 20-25 year planning horizon. The TRC is developed with the goals of increasing safety, improving mobility, providing excellent stewardship, and meeting community and environmental needs along the corridor through integrated management of the transportation network, including the highway, transit, pedestrian,

[41] San Joaquin Valley Blueprint Roadmap Guidance Framework, page i
[42] Tulare County Regional Blueprint, page 3

bicycle, freight, operational improvements, and travel demand management components of the corridor.

Transportation Concept Report (TRC)-State Route (SR) 201

The TRC is a planning document that identifies the existing and future route conditions as well as future needs for each route on the State Highway System. The TRC is developed with the goals of increasing safety, improving mobility, providing excellent stewardship, and meeting community and environmental needs along the corridor through integrated management of the transportation network, including the highway, transit, pedestrian, bicycle, freight, operational improvements, and travel demand management components of the corridor.

Tulare County 2030 General Plan Implementation

This Community Plan is intended to implement the Tulare County 2030 General Plan. The General Plan Policies relevant to this Community Plan are outlined in General Plan Policies Section below. In addition to the General Plan Policies, this Community Plan outlines policies specific to Cutler-Orosi. Following are the ways in which this Community Plan implements the General Plan:

- Update Zoning Map to match the Community Plan Land Use Map.
- Addition of Design Standards to replace use permit standards.
- Update Zoning text to outline allowed uses in this Community Plan.
- Introduction of a Mixed Use Overlay Zoning District
- Provides a Market Analysis of the Cutler-Orosi Area.
- Provides an updated analysis of Cutler-Orosi's population and housing characteristics.
- Defines an economic development strategy.

Community Plan Update Project Description and Objectives

On December 10, 2014, the Tulare County Board of Supervisors (BOS) approved General Plan Implementation (GPI 13-004) by Resolution 2103-0860 for the Planning Branch proposal to update the Cutler-Orosi Community Plan. The project Study Area Boundary will assess the potential project impacts from the proposed land use changes, for the areas south of Avenue 422 and north of Avenue 400, east of Road 116 and west of Road 134. The project EIR is based on a projected annual population growth rate of 1.3%. Additional growth beyond the 1.3% annual growth rate will require further growth analysis pursuant to CEQA. The Cutler-Orosi Community Plan 2021 Update components are described later in this section will become consistent with the General Plan 2030 Update, and will include the following primary goals and objectives.

1) Land Use and Environmental Planning - Promote development within planning areas next to the Regional State Route 63 Corridor in order to implement the following General Plan goals:
   a) Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals;
   b) Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;

c) Reduce development pressure on agriculturally designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
d) Reduce vehicle miles travelled throughout the County, thereby positively affecting air quality and greenhouse gas reduction; and
e) Help to improve the circulation, transit and railroad transportation system within this community, including, but not limited to, laying the groundwork for the construction of key projects such as Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths.

2) Improvements for a "disadvantaged community" - It is expected that the community planning areas will be improved for the following reasons:
   a) With faster project processing resulting from an updated community plan, increased employment opportunities are more likely to be provided by the private sector as proposed project developments can be approved as expeditiously as possible;
   b) Increased housing grant awards are more likely to occur based on updated community plans that are consistent with the policies of the recently adopted (August 2013) General Plan Update and Housing Element; and
   c) With updated community plans, enhanced infrastructure grant awards are more likely, thereby providing access to funding to install or upgrade road, water, wastewater, and storm water facilities.
3) Strengthening Relationship with TCAG - An important benefit of this expedited community plan process will be the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG) in that this and other community plans will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects.

By pursuing these transportation programs through a heightened collaborative process, the likelihood of getting actual projects in the ground will be realized faster than historically achieved. In doing so, these communities and others can become safer and healthier by providing a more efficient transportation network.

Community Outreach Process

Community outreach is vital to the success of the community plan update, because it provided the opportunity for residents to share their ideas and concerns in Cutler-Orosi. This also helps guide decisions on priorities for the community and identify potential challenges. The update process included extensive community and policymaker engagements that were conducted in English and Spanish.

Cutler-Orosi Public Meetings

In order to ensure that the Cutler-Orosi Community Plan 2021 Update was a community-driven, the County conducted public outreach meetings from 2015 through 2020. Broad public input was obtained through a series of workshops where residents, employees, property owners, as well as representatives from the school district and the public utility district, and the surrounding neighborhoods, weighed in on issues and provided recommendations **(see Attachment A-7)**.

Complete Streets

- Complete Streets Meeting February 2, 2015
- Complete streets Meeting March 3, 2015

Cutler-Orosi Joint Unified School Board District meeting

- March 14, 2019

Cutler School meetings

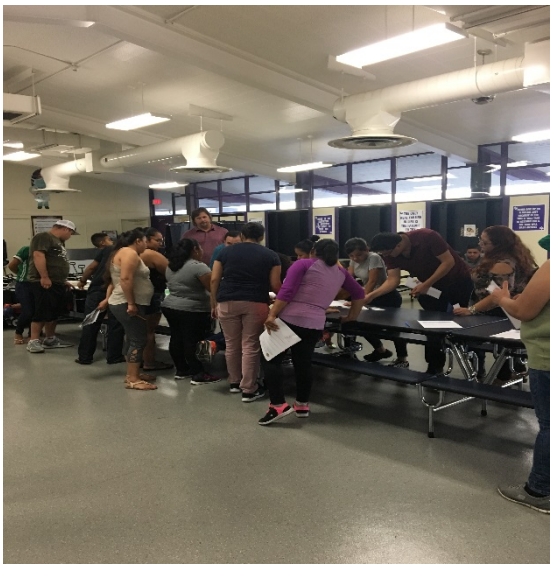
- May 2, 2019
- May 7, 2019

2019 LCAP Community Input Forums

County of Tulare Resource Management Agency (RMA) staff joined Yolanda Valdez, Superintendent, Cutler-Orosi Joint Unified School District, for Back to School Nights at the following schools in Cutler-Orosi. The focus of these workshops were to understand the community's vision and priorities for Cutler-Orosi.

- Cutler Elementary School August 20, 2019
- Orosi High School, August 21, 2019
- Golden Valley Elementary School August 22, 2019
- Palm Elementary School August 27, 2019
- Lovell High School August 28, 2019
- El Monte Middle School September 3, 2019
- Family Education Center September 4, 2019
- Family Education Center September 10, 2019

To reach a larger selection of community members the County of Tulare worked with Yolanda Valdez, Superintendent, of the Cutler-Orosi Joint Unified School District to attend Back to School Nights at the below schools. Tables were set up for County staff to lay out maps of the Cutler-Orosi area to give the participants hands on opportunity discuss with the parents living in Cutler-Orosi School District and opportunity for them to show us their vision. Participants identified a number of public safety concerns, largely relating to infrastructure. These concerns included a need for new lights in neighborhoods, flashing lights, crosswalks, widening of dangerous sidewalks, and pedestrian related concerns along various roadways. Feedback from participants has been combined into a vision map and spreadsheet addressing the Public Safety Improvement Concerns **(see Figure 20)**.



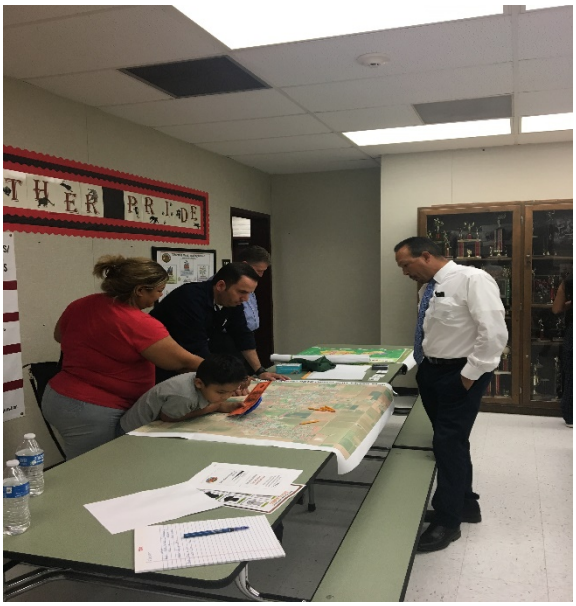
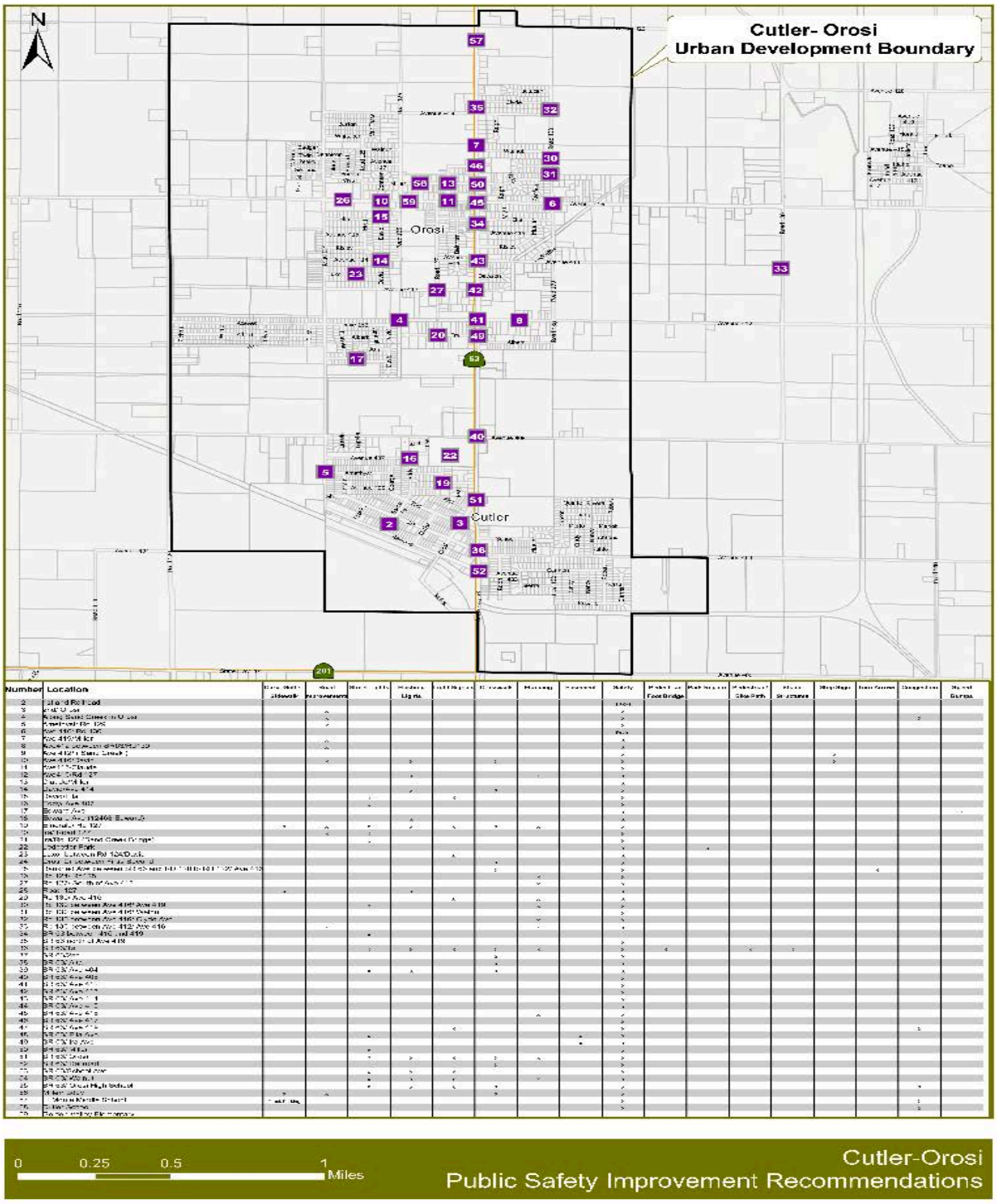


Figure - 21 Cutler-Orosi Community Concerns



Number	Location	Sidewalk	Road Improvements	Street Lights	Flashing Lights	Crosswalk	Markings	Crossing Guard	Safety	Pedestrian Footbridge	Pedestrian Tunnel	Pedestrian Sidepath	Bike Lanes	Stop Signs	Speed Control	Crosswalk Signal
2	Grand Ave / Road								Yes							
3	[illegible]		✓						✓							
4	[illegible]		✓						✓						✓	
5	[illegible]		✓						✓							
6	[illegible]								Yes							
7	[illegible]		✓						✓							
8	[illegible]		✓						✓							
9	[illegible]								✓					✓		
10	[illegible]		✓		✓		✓		✓					✓		
11	[illegible]								✓							
12	[illegible]				✓			✓	✓							
13	[illegible]								✓							
14	[illegible]				✓		✓		✓							
15	[illegible]			✓		✓			✓							
16	[illegible]			✓					✓							✓
17	[illegible]								✓							
18	[illegible]				✓				✓							
19	[illegible]	✓	✓	✓	✓	✓	✓	✓	✓							
20	[illegible]		✓	✓					✓							
21	[illegible]								✓							
22	[illegible]								✓		✓					
23	[illegible]					✓			✓							
24	[illegible]						✓		✓							
25	[illegible]						✓		✓						✓	
26	[illegible]						✓		✓							
27	[illegible]						✓		✓							
28	[illegible]	✓			✓				✓							
29	[illegible]					✓	✓		✓							
30	[illegible]			✓			✓		✓							
31	[illegible]						✓		✓							
32	[illegible]		✓				✓		✓							
33	[illegible]			✓					✓							
34	[illegible]			✓					✓							
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47	[illegible]					✓			✓						✓	
48	[illegible]							✓	✓							
49	[illegible]							✓	✓							
50	[illegible]			✓					✓							
51	[illegible]			✓	✓	✓	✓	✓	✓							
52	[illegible]			✓	✓	✓	✓	✓	✓							
53	[illegible]			✓	✓	✓	✓		✓							
54	[illegible]			✓	✓	✓	✓		✓							
55	[illegible]			✓	✓	✓	✓		✓						✓	
56	[illegible]	✓	✓				✓		✓							
57	[illegible]								✓						✓	
58	[illegible]								✓						✓	

Cutler-Orosi/East Orosi Bus Tour October 1, 2019

County of Tulare RMA staff joined Superintendent Yolanda Valdez and Supervisor Eddie Valero for a guided bus tour around Cutler, Orosi, and East Orosi informing them of infrastructure needs within these communities. The tour included the Orosi Recreation Sports Complex Phase 1 and explanation of the proposed Phase 2 prior to the scheduled Cutler-Orosi Town Hall meeting.

Cutler Orosi Town Hall Meeting October 1, 2019

Tulare County Supervisor Eddie Valero and Superintendent Yolanda Valdez held a Town Hall at the Cutler-Orosi Joint Union School District Board Chambers. At the Town Hall meeting Supervisor Valero welcomed the public and made introductions. Presentations were made by the Cutler-Orosi after School Program. Tulare County staff presented updates and answered questions for the different County Departments as follows:

- Tulare County Water Resource Program
- Tulare County Fire Department
- Tulare County Sheriff's Department
- Tulare County Planning Department
- Resource Management Agency – Code Enforcement
- Resource Management Agency – Roads
- Resource Management Agency – Transit
- Tulare County Animal Services
- Tulare County Parks and Recreation

Cutler Public Utility District

- March 19, 2019
- April 19, 2019

Orosi Public Utility District

- April 9, 2019

2019/2020 County Transportation Improvement Program 19/20 CTIP

Tulare County receives funding for transportation projects from a variety of sources. These sources can be divided into three categories: Local, State, and Federal. Local sources consist primarily of Measure R (1/2 cent sales tax specifically for transportation and some Maintenance Assessment district revenues. State sources include Local Transportation Funds (from sales tax), and SB1 and Highway User Tax Account (Gas taxes). Federal sources include a variety of competitive grant funds as well as allocations from transportation spending legislation, which can vary from year to year. A road fund balance does exist for transportation projects, emergencies, and other necessary reserves. No property taxes or County general funds are applied to road maintenance and construction within Tulare County.

The CTIP includes all new projects that will be started in the FY 2019/2020. Two projects for the community of Cutler and one project for the community of Orosi, was approved by the Tulare County Board of Supervisors on June 18, 2019, by Resolution No. 2019-0542.

2017 CTIP:

- Avenue 416 Crosswalk and ADA Ramps budgeted at $200,000 (Construction summer 2019).

2018 CTIP:

- Orosi- ADA Improvement & Blade Patching budgeted at $500,000 (Construction in Summer 2019)
- Cutler- ADA Improvement & Blade Patching budgeted at $300,000 (Construction in Summer 2019)
- Cutler- Road 144 and Avenue 384 to Avenue 416 budgeted at $1,600,000 (Construction in Progress)

2019 CTIP (Proposed projects):

- Orosi- ADA Improvement & Blade Patching budgeted at $200,000 (Construction in Summer 2020)
- Cutler- ADA Improvement & Blade Patching budgeted at $200,000 (Construction in Progress)

Summer 2020

- George Road and 2nd Drive in Cutler. Pedestrian improvements (sidewalk and drainage improvements to be specific) with an estimated cost of $2,500,000.

Sustainability

Climate Change/Adaptation/Resiliency/Sustainability

According to the 2017 Tulare County Multi Jurisdiction Local Hazard Mitigation Plan (LHMP), Climate change has occurred throughout the history of the planet. Due to variations in the earth's inclination to the sun, volcanic activity and other factors such as asteroids impacts, the amount of solar radiation reaching the earth's surface. The temperature of the planet correlates to the amount of solar radiation arriving at the surface and with it the climate.

In relatively recent history, the last glacial period, popularly known as the Ice Age, occurred from c. 110,000 to 12,000 years ago. This most recent glacial period is part of a larger pattern of glacial and interglacial periods known as the Quaternary glaciation (c. 2,588,000 years ago to present). From this point of view, scientists consider this "ice age" to be merely the latest glaciation event in a much larger ice age, one that dates back over two million years and is still ongoing. During this last glacial period, there were several changes between glacier advance and retreat. The Last Glacial Maximum, the maximum extent of glaciation within the last glacial period, was approximately 22,000 years ago. While the general pattern of global cooling and glacier advance was similar, local differences in the development of glacier advance and retreat make it difficult to compare the details from continent to continent. Generally, the pattern of temperature variation and glaciation has lagged atmospheric carbon dioxide (CO2) content.

The Tulare County (County) has prepared the 2017 Multi-Jurisdictional Local Hazard Mitigation Plan (MJLHMP) to assess the natural, technological, and human-caused risks to County communities, to reduce the potential impact of the hazards by creating mitigation strategies. The 2017 MJLHMP represents the County's commitment to create a safer, more resilient community by taking actions to reduce risk and by committing resources to lessen the effects of hazards on the people and property of the County.

The plan complies with The Federal Disaster Mitigation Act (DMA 2000), Federal Register 44 CFR Parts 201 and 206, which modified the Robert T. Stafford Disaster Relief and Emergency Assistance Act (Stafford Act) by adding a new section, 322 - Mitigation Planning. This law, as of November 1, 2004, requires local governments to develop and submit hazard mitigation plans as a condition of receiving Federal Emergency Management Agency (FEMA) Hazard Mitigation Grant Program (HMGP) and other mitigation project grants. The County; the Cities of Dinuba, Exeter, Farmersville, Lindsay, Porterville, Tulare, Visalia, and Woodlake; the Tule River Tribe; and Special District staffs have coordinated preparation of the MJLHMP in cooperation with stakeholders, partner agencies and members of the public.

Tulare County Climate Action Plan

Tulare County adopted a Climate Action Plan (CAP) on August 28, 2012. The CAP is an implementation measure of the 2030 General Plan Update. The policies, regulations, and programs considered in the CAP include those by federal, state, and local governments.

"The Tulare County Climate Action Plan (CAP) serves as a guiding document for County of Tulare

("County") actions to reduce greenhouse gas emissions and adapt to the potential effects of climate change. The CAP is an implementation measure of the 2030 General Plan Update. The General Plan provides the supporting framework for development in the County to produce fewer greenhouse gas emissions during Plan buildout. The CAP builds on the General Plan's framework with more specific actions that will be applied to achieve emission reduction targets consistent with California legislation."[43]

Tulare County General Plan Policies (Sustainability)

The Tulare County General Plan has a number of policies that apply to projects within County of Tulare. General Plan policies that relate to Sustainability include the following.

LU-1.1 Smart Growth and Healthy Communities

The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including:

1. Creating walkable neighborhoods,
2. Providing a mix of residential densities,
3. Creating a strong sense of place,
4. Mixing land uses,
5. Directing growth toward existing communities,
6. Building compactly,
7. Discouraging sprawl,
8. Encouraging infill,
9. Preserving open space,
10. Creating a range of housing opportunities and choices,
11. Utilizing planned community zoning to provide for the orderly pre-planning and long term development of large tracks of land which may contain a variety of land uses, but are under unified ownership or development control, and
12. Encouraging connectivity between new and existing development.

LU-1.8 Encourage Infill Development

The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

LU-7.15 Energy Conservation

The County shall encourage the use of solar power and energy conservation building techniques in all new development.

LU-7.16 Water Conservation

The County shall encourage the inclusion of "extra-ordinary' water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development.

[43] Tulare County Climate Action Plan, page 1

LU-7.17 Shared Parking Facilities
The County shall encourage, where feasible, the use of shared parking facilities. Such areas could include developments with different day/night uses.

AQ-3.3 Street Design
The County shall promote street design that provides an environment, which encourages transit use, biking, and pedestrian movements.

AQ-3.5 Alternative Energy Design
The County shall encourage all new development, including rehabilitation, renovation, and redevelopment, to incorporate energy conservation and green building practices to maximum extent feasible. Such practices include, but are not limited to building orientation and shading, landscaping, and the use of active and passive solar heating and water systems.

AQ-3.6 Mixed Land Uses
The County shall encourage the clustering of land uses that generate high trip volumes, especially when such uses can be mixed with support services and where they can be served by public transportation.

TCAG Sustainable Communities Strategy (2018 Regional Transportation Plan)
AB 32 set emission targets for the State of California. SB 375 requires the California Air Resources Board to set greenhouse gas emission targets for different regions in California. Under SB 375 Metropolitan Planning Organizations like TCAG are required to create a Sustainable Communities Strategy. TCAG included this strategy in the 2018 Regional Transportation Plan. Highlights of the implementation strategies include:

- Encourage jurisdictions in Tulare County to consider bicycle lanes, public transit, transit-oriented and mixed-use development, pedestrian networks, train and other Complete Streets development during updates of general plan or other local plans.
- Implement a Complete Streets Program whereby agencies will prepare plans to accommodate all transportation users, including pedestrians, bicyclists, transit riders, and motor vehicle operators and riders, and implement those plans as aggressively as feasible.
- Provide for continued coordination and evaluation of the planned circulation system among cities and the county.
- Fund the development of capital improvement programs for Complete Streets and active transportation-type plans, as funds are available.
- Evaluate intersections, bridges, interchanges, and rail grade crossings for needed safety improvements.
- Develop funding strategies for safety projects in cooperation with Caltrans and member agencies.
- Examine alternative funding sources for streets, roads, state highways, rail systems, transit, bicycle, pedestrian, and other transportation mode improvements.
- Utilize Cap and Trade funds available for transit, if available, for projects in Tulare County.
- Encourage local agencies to support implementation of bicycle support facilities such as bike

racks, showers, and other facilities during the project review process.

- Utilize Cap and Trade funds available for bicycle and pedestrian projects, if available, for projects in Tulare County.
- Encourage mixed-use developments in urbanized areas.
- Encourage provision of an adequate supply of housing for the region's workforce and adequate sites to accommodate business expansion to minimize interregional trips and long-distance commuting.
- Support and participate in efforts and coalitions promoting use of Cap and Trade funding for projects that help reduce greenhouse gas emissions in Tulare County.
- Support investment in bicycle and pedestrian systems, giving attention to projects and networks that will allow residents to walk and bicycle to frequented destinations, including schools, parks, healthcare institutions and transit stops.
- Provide environmental justice communities opportunities for input into transportation plans, programs, and projects in a manner consistent with Title VI of the 1964 Civil Rights Act and Executive Order 12898 on Environmental Justice, including the prohibition of intentional discrimination and adverse disparate impact with regard to race, ethnicity or national origin.

These implementation strategies are compatible with the Tulare County General Plan policies.

Urban Development Boundary

Urban Development Boundaries (UDBs) are officially adopted and mapped County lines delineating the area expected for urban growth in cities and unincorporated communities over a 20-year period. Within UDB boundaries, the County and cities will coordinate plans, policies and standards related to building construction, subdivision development, land use and zoning regulations, street and highway construction, public utility systems, environmental studies, and other closely related matters affecting the orderly development of urban fringe areas. These boundaries provide an official definition of the interface between future urban and agricultural land uses.[44]

"For unincorporated communities, the UDB is a County adopted line dividing land to be developed from land to be protected for agricultural, natural, open space, or rural uses. It serves as the official planning area for communities over a 20-year period. Land within an unincorporated UDB is assumed appropriate for development and is not subject to the Rural Valley Lands Plan or Foothill Growth Management Plan (RVLP Policy 1-1)"[45]

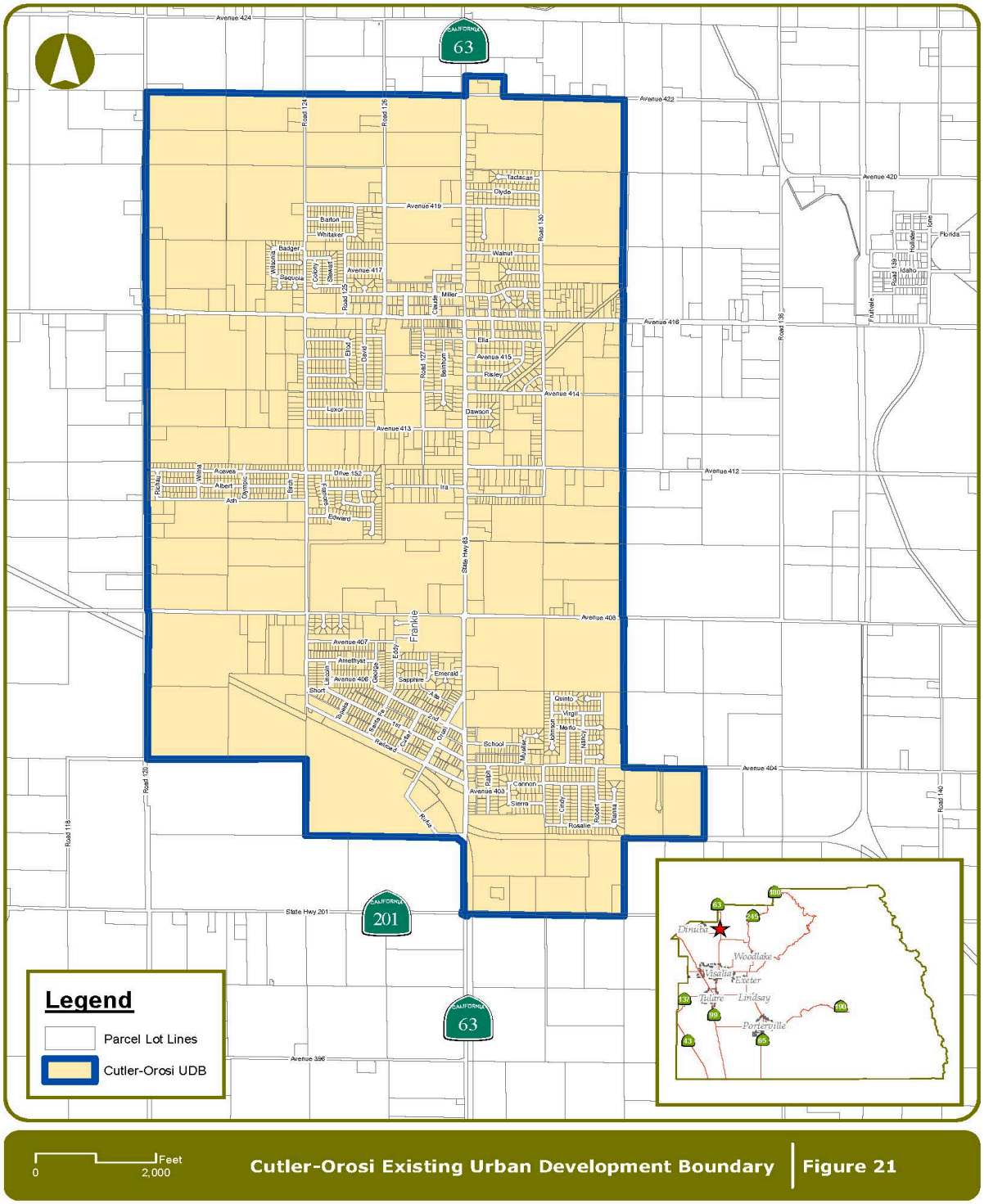
The purpose of this chapter is to review the adequacy of the adopted Urban Development Boundary (UDB) and determine through analysis contained in this chapter whether modifications may be required. The proposed UDB functions as the planning area boundary of the Cutler-Orosi Community Plan **(see Figure 21)**. The UDB line establishes a twenty-year growth boundary for the community of Cutler-Orosi. Over the years, services will be extended into this area, which will allow new growth to occur. Since the UDB line defines the area where growth will occur, it is logical that it also serves as the planning area boundary for this plan.

[44] TCGPU Part 1, Goals and Policies Report, Planning Framework, page 2-3
[45] TCGPU Part 1, Goals and Policies Report, Planning Framework, page 2-3

In addition to defining there in which future development of the community of Cutler-Orosi will occur, designating an urban boundary can provide local agencies and citizens with other benefits, including:

1. Encouraging coordination between land use planning and the provision of governmental services.
2. Identifying and resolving potential interagency conflicts regarding service areas.
3. Encouraging efficient, economical and effective delivery of public services.
4. Allowing property owners to identify the type and level of service their lands presently receive or may receive in the future.
5. Assisting in the County's efforts to preserve open space and productive agricultural land.

Figure 22 - Existing Urban Development Boundary



Policy Framework

In determining a UDB and planning area for the Cutler-Orosi Community Plan, it is important to not only fulfill the requirements of State planning law, but to the greatest degree possible, fulfill the local goals and policies that regulate land uses in the area. As discussed earlier, there are many County policies that guide development in Cutler-Orosi area. However, those which have direct effect on the establishment of the community's urban boundary include the following policies in the Tulare County General Plan Planning Framework Element:

PF-2.1 Urban Development Boundaries – Communities

The County shall limit urban development to the area within the designated UDB for each community. Each community's UDB is defined as shown on Figures 2.2-2 thru 2.2-22.

PF-2.2 Modification of Community UDB

1. The County may consider modification to a community UDB under any of, but not limited to the following circumstances:
   a. The location of the UDB shall be evaluated during preparation or update of a community plan.
   b. All community UDBs should be reviewed on a five-year cycle to reflect changes in growth and development patterns.
   c. A request for expansion of the UDB boundary can be applied for as part of a General Plan Amendment to the Land Use Diagram.
   d. At the request of a special district or the community.
   e. A UDB should be considered for expansion at such time as land for infill becomes limited. This condition is considered satisfied when 80 percent of the non-Williamson Act land within the UDB is developed for urban uses.
   f. UDBs should not be expanded onto Prime Farmland if Farmland of Statewide Importance or of lesser quality is available and suitable for expansion.

2. Prior to approval of a UDB boundary expansion, the County shall ensure that infrastructure can be provided to serve the new areas added to the UDB and that sufficient water supplies are also available. This may require preparation of an infrastructure master plan that includes methods of financing of improvements and maintenance, as well as representation/documentation of availability and sufficiency of long-term water supplies.

3. Preservation of productive agricultural lands shall be the highest priority when considering modifications. Expansion of a UDB to include additional agricultural land shall only be allowed when other non-agricultural lands are not reasonably available to the community or are not suitable for expansion.

PF-2.3 UDB and Other Boundaries

The County shall provide notice and opportunity for special districts, school districts, and other service providers to comment when evaluating the expansion of a Community's UDB.

PF-2.8 Inappropriate Land Use

Areas within UDBs are hereby set aside for those types of urban land uses, which benefit from urban services. Permanent uses, which do not benefit from such urban services, shall be discouraged within the UDBs. This is not intended to apply to agricultural or agricultural supported uses, including the cultivation of land or other uses accessory to the cultivation of land, provided that such accessory uses are time-limited through special use permit procedures.

Existing Adopted Land Use Plan

Existing Land Use Plan

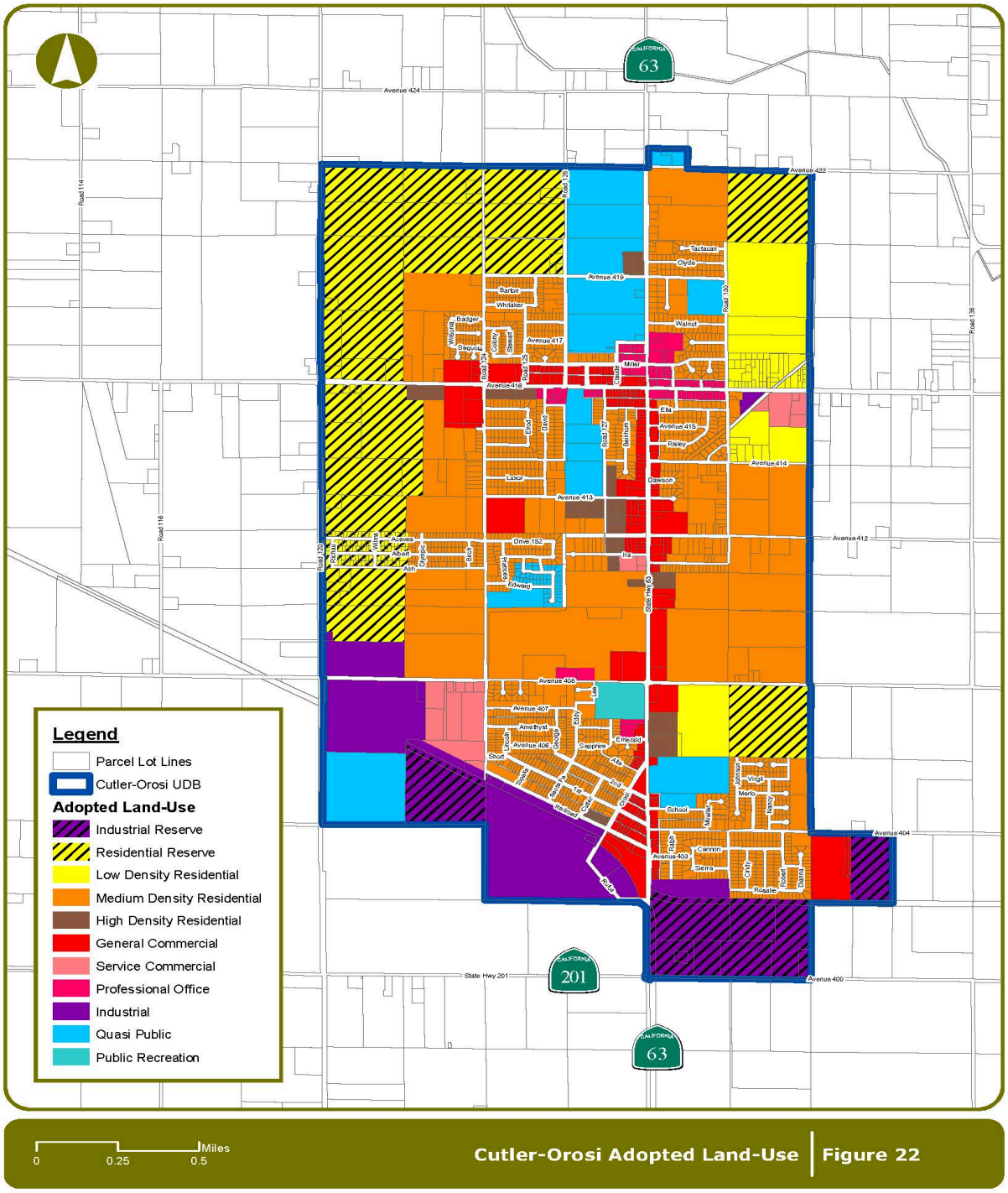
Table 32 land in the adopted Cutler-Orosi Community Planning area is designated Residential (1,014 acres). In total, there is about 2,412.3 acres of designated lands in the Cutler-Orosi Community Planning Area **(see Figure 22)**. Approximately 231.3 acres within the plan area is dedicated to rights-of-way.

Table 32 – Adopted Land Use

Designation	Total Acreage	Percentage
General Commercial	140.4	5.74
High Density Residential	38.7	1.58
Industrial	168.3	6.89
Industrial Reserve	135.0	5.53
Low Density Residential	125.1	5.12
Medium Density Residential	850.2	34.82
Professional Office	28.6	1.17
Public Recreation	11.9	0.49
Quasi-Public	201.1	8.23
Residential Reserve	459.9	18.83
Service Commercial	42.8	1.75
(blank)	8.3	0.34
Unclassified (Right-of-Way)	231.3	9.47
Total	**2,441.9**	**100**

Source: Tulare County GIS

Figure 23 – Adopted Land Use Plan Map as Amended



Cutler-Orosi Adopted Land-Use | Figure 22

Existing Adopted Zoning Districts

Existing Zoning Districts

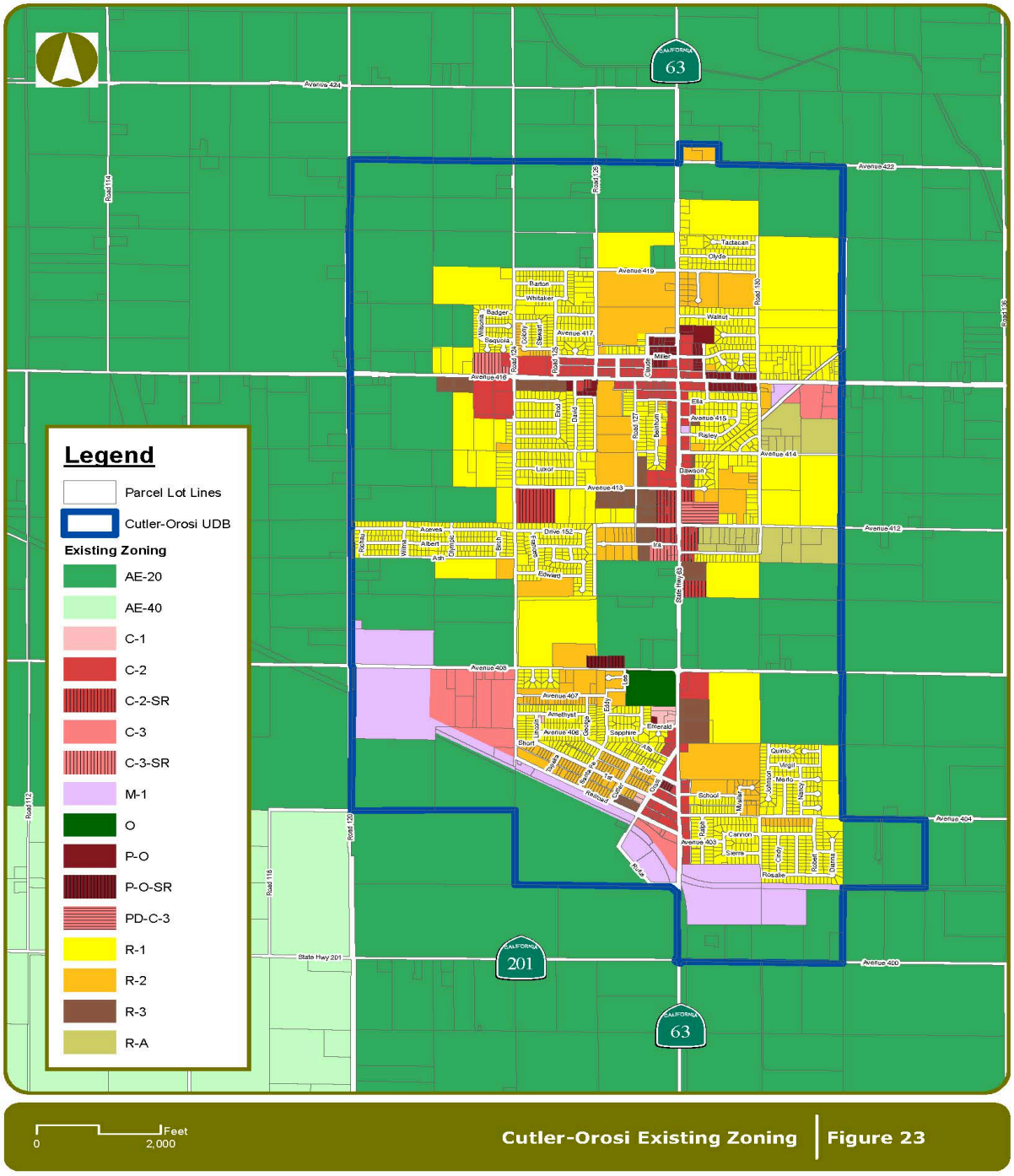
The zoning designations within the existing Cutler-Orosi Community Plan UDB are shown in **Figure 21** and demonstrated in **Table 33.**

Table 33 - Existing Zoning Districts

Zoning Designations	Existing Acres	Percent
AE-20	956.9	39.1
C-1	3.5	0.1
C-2	69.8	2.8
C-2-SR	23.2	0.9
C-3	55.1	2.2
C-3-SR	7.5	0.3
M-1	130.8	5.3
O	11.9	0.4
PD-C-3	5.4	0.2
P-O	16.6	0.6
P-O-SR	3.1	0.1
R-1	644.3	26.3
R-2	189.1	7.7
R-3	36.3	1.4
R-A	50.3	2.0
Z	6.8	0.2
Unclassified (Right-of-Way)	231.3	9.4
Total	**2,441.9**	**100**

Source: Tulare County GIS

Figure 24 - Existing Zoning Districts Map



General Plan Policies

The following adopted policies from the Tulare County General Plan are applicable to the Cutler-Orosi Community Plan. Policies throughout the General Plan use the terminology "shall" and "should." For the purposes of interpreting the policies in the General Plan, the term "shall" indicates a mandatory or required action or a duty to undertake an action unless the context indicates otherwise, in which case the term is synonymous with "should." The term "should," indicates a directive subject to discretion and requires at least review or consideration and, in that context, substantial compliance with the spirit or purpose of these General Plan policies. The term "may" indicates at the sole discretion of the County.

Land Use Policies

PF-1.3 Land Uses in UDBs/HDBs
The County shall encourage those types of urban land uses that benefit from urban services to develop within UDBs and HDBs. Permanent uses, which do not benefit from urban services shall be discouraged within these areas. This shall not apply to agricultural or agricultural support uses, including the cultivation of land or other uses accessory to the cultivation of land provided that such accessory uses are time-limited through Special Use Permit procedures.

PF-2.6 Land Use Consistency
The County shall require all community plans, when updated, to use the same land use designations as used in this Countywide General Plan (See Chapter 4-Land Use). All community plans shall also utilize a similar format and content. The content may change due to the new requirements such as Global Climate Change and Livable Community Concepts, as described on the table provided (Table 2.1: Community Plan Content). Changes to this format may be considered for unique and special circumstances as determined appropriate by the County. Until such time as a Community Plan is adopted for those communities without existing Community Plans, the land use designation shall be Mixed Use, which promotes the integration of a compatible mix of residential types and densities, commercial uses, public facilities, services and employment opportunities.

PF-2.8 Inappropriate Land Use
Areas within UDBs are hereby set aside for those types of urban land uses, which benefit from urban services. Permanent uses, which do not benefit from such urban services shall be discouraged within the UDBs. This is not intended to apply to agricultural or agricultural supported uses, including the cultivation of land or other uses accessory to the cultivation of land, provided that such accessory uses are time-limited through special use permit procedures.

PF-4.7 Avoiding Isolating Unincorporated Areas
The County may oppose any annexation proposal that creates an island, peninsula, corridor, or irregular boundary. The County will also encourage the inclusion of unincorporated islands or peninsulas adjacent to proposed annexations.

LU-1.2 Innovative Development
The County shall promote flexibility and innovation through the use of planned unit developments, development agreements, specific plans, Mixed Use projects, and other

innovative development and planning techniques.

LU-1.3 Prevent Incompatible Uses
The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.

LU-1.4 Compact Development
The County shall actively support the development of compact mixed-use projects that reduce travel distances.

LU-3.1 Residential Developments
The County shall encourage new major residential development to locate near existing infrastructure for employment centers, services, and recreation.

LU-3.2 Cluster Development
The County shall encourage proposed residential development to be clustered onto portions of the site that are more suitable to accommodating the development, and shall require access either directly onto a public road or via a privately maintained road designed to meet County road standards.

LU-3.3 High-Density Residential Locations
The County shall encourage high-density residential development (greater than 14 dwelling units per gross acre) to locate along collector roadways and transit routes, and near public facilities (e.g., schools, parks), shopping, recreation, and entertainment.

LU-3.4 Mountain, Rural, and Low-Density Residential
The Mountain, Rural, and Low-Density Residential development located outside of a UDB shall be subject to the following requirements:

1. Able to meet the Rural Valley Lands Plan policies, Foothill Growth Management Plan policies, or Mountain Framework Plan policies and requirements,
2. Areas which qualify for minimum densities greater than 1 unit per 10 acres must meet the following characteristics (unless clustering is used):
   a. Average slopes must be below a 30 percent grade,
   b. Not identified as a moderate-to-high landslide hazard area, and
   c. Access to new development is provided via an existing publicly maintained road or via a new road improved consistent with adopted County standards.

LU-1.8 Encourage Infill Development
The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

ED-2.11 Industrial Parks
As part of new or updated community plans, the County shall designate sites for industrial development to meet projected demand.

Housing Policy 3.24
When locating agricultural industry in rural areas, a determination should be made that there are transit opportunities and an adequate employment base living within a reasonable distance to the site.

AQ-3.2 Infill near Employment
The County shall identify opportunities for infill development projects near employment areas within all unincorporated communities and hamlets to reduce vehicle trips.

AQ-3.6 Mixed Land Uses
The County shall encourage the clustering of land uses that generate high trip volumes, especially when such uses can be mixed with support services and where they can be served by public transportation.

PFS-8.4 Library Facilities and Services
The County shall encourage expansion of library facilities and services as necessary to meet the needs (e.g., internet access, meeting rooms, etc.) of future population growth.

Circulation Policies

Q-3.3 Street Design
The County shall promote street design that provides an environment, which encourages transit use, biking, and pedestrian movements.

LU-7.3 Friendly Streets
The County shall encourage new streets within UDBs to be designed and constructed to not only accommodate traffic, but also serve as comfortable pedestrian and cyclist environments. These should include, but not be limited to:

1. Street tree planting adjacent to curbs and between the street and sidewalk to provide a buffer between pedestrians and automobiles, where appropriate,
2. Minimize curb cuts along streets,
3. Sidewalks on both sides of streets, where feasible,
4. Bike lanes and walking paths, where feasible on collectors and arterials, and
5. Traffic calming devices such as roundabouts, bulb-outs at intersections, traffic tables, and other comparable techniques.

LU-7.4 Streetscape Continuity
The County shall ensure that streetscape elements (e.g., street signs, trees, and furniture) maintain visual continuity and follow a common image for each community.

LU-1.10 Roadway Access
The County shall require access to public roadways for all new development.

SL-2.1 Designated Scenic Routes and Highways
The County shall protect views of natural and working landscapes along the County's highways and roads by maintaining a designated system of County scenic routes and State scenic highways by:

1. Requiring development within existing eligible State scenic highway corridors to adhere to land use and design standards and guidelines required by the State Scenic Highway Program,
2. Supporting and encouraging citizen initiatives working for formal designation of eligible segments of State Highway 198 and State Highway 190 as State scenic highways,
3. Formalizing a system of County scenic routes throughout the County (see Figure 7-1), and
4. Requiring development located within County scenic route corridors to adhere to local design guidelines and standards.

SL-4.1 Design of Highways
The County shall work with Caltrans and Tulare County Association of Governments (TCAG) to ensure that the design of SR 99 and other State Highways protects scenic resources and provide access to vistas of working and natural landscapes by:

1. Limiting the construction of sound walls that block views of the County's landscapes (incorporate setbacks to sensitive land uses to avoid noise impacts whenever feasible),

2. Using regionally-appropriate trees and landscaping and incorporating existing landmark trees,
3. Preserving historic and cultural places and vistas,
4. Avoiding excessive cut and fill for roadways along State scenic highways and County scenic routes, and along areas exposed to a large viewing area, and
5. Promote highway safety by identifying appropriate areas for traffic pullouts and rest areas.

SL-4.2 Design of County Roads
The County's reinvestment in rural County roads outside urban areas should, in addition to meeting functional needs and safety needs, preserve the experience of traveling on the County's "country roads" by:
1. Maintaining narrow as possible rights-of-ways,
2. Limiting the amount of curbs, paved shoulders, and other "urban" edge improvements,
3. Preserving historic bridges and signage, and
4. Promote County road safety by identifying appropriate areas for traffic pullout.

TC-1.1 Provision of an Adequate Public Road Network
The County shall establish and maintain a public road network comprised of the major facilities illustrated on the Tulare County Road Systems to accommodate projected growth in traffic volume.

TC-1.2 County Improvement Standards
The County's public roadway system shall be built and maintained consistent with adopted County Improvement Standards, and the need and function of each roadway, within constraints of funding capacity.

TC-1.6 Intermodal Connectivity
The County shall ensure that, whenever possible, roadway, highway, and public transit systems will interconnect with other modes of transportation. Specifically, the County shall encourage the interaction of truck, rail, and air-freight/passenger movements.

TC-1.7 Intermodal Freight Villages
The County shall consider the appropriate placement of intermodal freight villages in locations within the Regional Growth Corridors.

TC-1.8 Promoting Operational Efficiency
The County shall give consideration to transportation programs that improve the operational efficiency of goods movement, especially those that enhance farm-to-market connectivity

TC-1.9 Highway Completion
The County shall support State and Federal capacity improvement programs for critical segments of the State Highway System. Priority shall be given to improvements to State Routes 65, 99, and 198, including widening and interchange projects in the County.

TC-1.10 Urban Interchanges
The County shall work with TCAG to upgrade State highway interchanges from rural to urban standards within UDBs.

TC-1.11 Regionally Significant Intersections
To enhance safety and efficiency, the County shall work to limit the frequency of intersections along regionally significant corridors.

TC-1.12 Scenic Highways and Roads
The County shall work with appropriate agencies to support the designation of scenic highways and roads in the County.

TC-1.16 County Level of Service (LOS) Standards
The County shall strive to develop and manage its roadway system (both segments and intersections) to meet a LOS of "D" or better in accordance with the LOS definitions established by the Highway Capacity Manual.

TC-1.18 Balanced System
The County shall strive to meet transportation needs and maintain LOS standards through a balanced Multimodal Transportation Network that provides alternatives to the automobile.

TC-2.3 Amtrak Service
The County shall encourage Amtrak to add passenger service to the Union Pacific corridor in the County.

TC-4.7 Transit Ready Development
The County shall promote the reservation of transit stops in conjunction with development projects in likely or potential locations for future transit facilities.

TC-5.1 Bicycle/Pedestrian Trail System
The County shall coordinate with TCAG and other agencies to develop a Countywide integrated multi-purpose trail system that provides a linked network with access to recreational, cultural, and employment facilities, as well as offering a recreational experience apart from that available at neighborhood and community parks.

TC-5.2 Consider Non-Motorized Modes in Planning and Development
The County shall consider incorporating facilities for non-motorized users, such as bike routes, sidewalks, and trails when constructing or improving transportation facilities and when reviewing new development proposals. For developments with 50 or more dwelling units or non-residential projects with an equivalent travel demand, the feasibility of such facilities shall be evaluated.

TC-5.3 Provisions for Bicycle Use
The County shall work with TCAG to encourage local government agencies and businesses to consider including bicycle access and provide safe bicycle parking facilities at office buildings, schools, shopping centers, and parks.

TC-5.4 Design Standards for Bicycle Routes
The County shall utilize the design standards adopted by Caltrans and as required by the Streets and Highway Code for the development, maintenance, and improvement of bicycle routes.

TC-5.5 Facilities
The County shall require the inclusion of bicycle support facilities, such as bike racks, for new major commercial or employment locations.

TC-5.7 Designated Bike Paths
The County shall support the creation and development of designated bike paths adjacent to or separate from commute corridors.

TC-5.8 Multi-Use Trails
The County shall encourage the development of multi-use corridors (such as hiking, equestrian, and mountain biking) in open space areas, along power line transmission corridors, utility easements, rivers, creeks, abandoned railways, and irrigation canals.

TC-5.9 Existing Facilities
The County shall support the maintenance of existing bicycle and pedestrian facilities.

Housing Policies

LU-3.5 Rural Residential Designations
The County shall not re-zone any new areas for residential development in the RVLP area, unless it can be shown that other objectives, such as buffers and the relationship of the development to surrounding uses, can be achieved.

Housing Guiding Principle 1.1
Endeavor to improve opportunities for affordable housing in a wide range of housing types in the communities throughout the unincorporated area of the County.

Housing Policy 1.11
Encourage the development of a broad range of housing types to provide an opportunity of choice in the local housing market.

Housing Policy 1.13
Encourage the utilization of modular units, prefabricated units, and manufactured homes.

Housing Policy 1.14
Pursue an equitable distribution of future regional housing needs allocations, thereby providing a greater likelihood of assuring a balance between housing development and the location of employment opportunities.

Housing Policy 1.15
Encourage housing counseling programs for low-income homebuyers and homeowners.

Housing Policy 1.16
Review community plans and zoning to ensure they provide for adequate affordable residential development.

Housing Guiding Principle 1.2
Promote equal housing opportunities for all persons regardless of race, religion, sex, marital status, ancestry, national origin, color, family status, disability, or any other arbitrary basis.

Housing Guiding Principle 1.3
Strive to meet the housing needs of migrant and non-migrant farmworkers and their families with a suitable, affordable and satisfactory living environment.

Housing Policy 1.31
Encourage the provision of farmworker housing opportunities in conformance with the Employee Housing Act.

Housing Guiding Principle 1.4
Enhance and support emergency shelters and transitional and supportive housing programs that assist the homeless and others in need.

Housing Policy 1.51
Encourage the construction of new housing units for "special needs" groups, including senior citizens, large families, single heads of households, households of persons with physical and/or mental disabilities, minorities, farmworkers, and the homeless in close proximity to transit, services, and jobs.

Housing Policy 1.52
Support and encourage the development and improvement of senior citizen group housing, convalescent homes, and other continuous care facilities.

Housing Policy 1.55
Encourage development of rental housing for large families, as well as providing for other housing needs and types.

Housing Guiding Principle 1.6
Assess and amend County ordinances,

standards, practices and procedures considered necessary to carry out the County's essential housing goal of the attainment of a suitable, affordable and satisfactory living environment for every present and future resident in unincorporated areas.

Housing Policy 2.14
Create and maintain a matrix of Infrastructure Development Priorities for Disadvantaged Unincorporated Communities in Tulare County thorough analysis and investigation of public infrastructure needs and deficits, pursuant to Action Program 9.

Housing Guiding Principle 2.2
Require proposed new housing developments located within the development boundaries of unincorporated communities to have the necessary infrastructure and capacity to support the development.

Housing Policy 2.21
Require all proposed housing within the development boundaries of unincorporated communities is either (1) served by community water and sewer, or (2) that physical conditions permit safe treatment of liquid waste by septic tank systems and the use of private wells.

Housing Guiding Principle 3.1
Encourage "smart growth" designed development that serves the unincorporated communities, the environment, and the economy of Tulare County.

Housing Policy 3.11
Support and coordinate with local economic development programs to encourage a "jobs to housing balance" throughout the unincorporated area.

Housing Policy 3.23
Prepare new and/or updated community plans that provide adequate sites for a variety of types of housing within the development boundaries of community.

Conservation Policies

AG-1.1 Primary Land Use
The County shall maintain agriculture as the primary land use in the valley region of the County, not only in recognition of the economic importance of agriculture, but also in terms of agriculture's real contribution to the conservation of open space and natural resources.

AG-1.4 Williamson Act in UDBs and HDBs
The County shall support non-renewal or cancellation processes that meet State law for lands within UDBs and HDBs.

AG-1.5 Substandard Williamson Act Parcels
The County may work to remove parcels that are less than 10 acres in Prime Farmland and less than 40 Acres in Non-Prime Farmland from Williamson Act Contracts (Williamson Act key term for Prime/Non-Prime).

AG-1.6 Conservation Easements
The County shall consider developing an Agricultural Conservation Easement Program (ACEP) to help protect and preserve agricultural lands (including "Important Farmlands"), as defined in this Element. This program may require payment of an in-lieu fee sufficient to purchase a farmland conservation easement, farmland deed restriction, or other farmland conservation mechanism as a condition of approval for conservation of important agricultural land to non-agricultural use. If available, the ACEP shall be used for replacement lands determined to be of statewide significance (Prime or other Important Farmlands), or

sensitive and necessary for the preservation of agricultural land, including land that may be a part of a community separator as part of a comprehensive program to establish community separators. The in-lieu fee or other conservation mechanism shall recognize the importance of land value and shall require equivalent mitigation.

AG-1.7 Preservation of Agricultural Lands
The County shall promote the preservation of its agricultural economic base and open space resources through the implementation of resource management programs such as the Williamson Act, Rural Valley Lands Plan, Foothill Growth Management Plan or similar types of strategies and the identification of growth boundaries for all urban areas located in the County.

AG-1.8 Agriculture within Urban Boundaries
The County shall not approve applications for preserves or regular Williamson Act contracts on lands located within a UDB and/or HDB unless it is demonstrated that the restriction of such land will not detrimentally affect the growth of the community involved for the succeeding 10 years, that the property in question has special public values for open space, conservation, other comparable uses, or that the contract is consistent with the publicly desirable future use and control of the land in question. If proposed within a UDB of an incorporated city, the County shall give written notice to the affected city pursuant to Government Code §51233.

AG-1.10 Extension of Infrastructure into Agricultural Areas
The County shall oppose extension of urban services, such as sewer lines, water lines, or other urban infrastructure, into areas designated for agriculture use unless necessary to resolve a public health situation. Where necessary to address a public health issue, services should be located in public rights-of-way in order to prevent interference with agricultural operations and to provide ease of access for operation and maintenance. Service capacity and length of lines should be designed to prevent the conversion of agricultural lands into urban/suburban uses.

AG-1.11 Agricultural Buffers
The County shall examine the feasibility of employing agricultural buffers between agricultural and non-agricultural uses, and along the edges of UDBs and HDBs. Considering factors include the type of operation and chemicals used for spraying, building orientation, planting of trees for screening, location of existing and future rights-of-way (roads, railroads, canals, power lines, etc.), and unique site conditions.

AG-1.13 Agricultural Related Uses
The County shall allow agriculturally related uses, including value-added processing facilities by discretionary approvals in areas designated Valley or Foothill Agriculture, subject to the following criteria:

1. The use shall provide a needed service to the surrounding agricultural area which cannot be provided more efficiently within urban areas or which requires location in a non-urban area because of unusual site requirements or operational characteristics;
2. The use shall not be sited on productive agricultural lands if less productive land is available in the vicinity;
3. The operational or physical characteristics of the use shall not have a significant adverse impact on water resources or the use or management of surrounding agricultural properties within at least one-quarter (1/4) mile radius;
4. A probable workforce should be located nearby or be readily available; and

5. For proposed value-added agricultural processing facilities, the evaluation under criterion "1" above shall consider the service requirements of the use and the capability and capacity of cities and unincorporated communities to provide the required services.

AG-1.16 Schools in Agricultural Zones
The County shall discourage the location of new schools in areas designated for agriculture, unless the School District agrees to the construction and maintenance of all necessary infrastructure impacted by the project.

AG-2.6 Biotechnology and Biofuels
The County shall encourage the location of industrial and research oriented businesses specializing in biotechnologies and biofuels that can enhance agricultural productivity, enhance food-processing activities in the County, provide for new agriculturally related products and markets, or otherwise enhance the agricultural sector in the County.

LU-7.12 Historic Buildings and Areas
The County shall encourage preservation of buildings and areas with special and recognized historic, architectural, or aesthetic value. New development should respect architecturally and historically significant buildings and areas. Landscaping, original roadways, sidewalks, and other public realm features of historic buildings or neighborhoods shall be restored or repaired wherever feasible.

LU-7.13 Preservation of Historical Buildings
The County shall encourage and support efforts by local preservation groups to identify and rehabilitate historically significant buildings.

LU-7.14 Contextual and Compatible Design
The County shall ensure that new development respects Tulare County's heritage by requiring that development respond to its context, be compatible with the traditions and character of each community, and develop in an orderly fashion, which is compatible with the scale of surrounding structures.

LU-7.15 Energy Conservation
The County shall encourage the use of solar power and energy conservation building techniques in all new development.

LU-7.16 Water Conservation
The County shall encourage the inclusion of "extra-ordinary' water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development

Housing Guiding Principle 4.1
Support and encourage County ordinances, standards, practices and procedures that promote residential energy conservation.

Housing Policy 4.13
Promote energy efficiency and water conservation.

Housing Policy 4.21
Promote energy conservation opportunities in new residential development.

Housing Policy 5.24
Encourage the development of suitable replacement housing when occupied housing units are demolished due to public action.

ERM-1.1 Protection of Rare and Endangered Species

The County shall ensure the protection of environmentally sensitive wildlife and plant life, including those species designated as rare, threatened, and/or endangered by State and/or Federal government, through compatible land use development.

ERM-1.2 Development in Environmentally Sensitive Areas

The County shall limit or modify proposed development within areas that contain sensitive habitat for special status species and direct development into less significant habitat areas. Development in natural habitats shall be controlled so as to minimize erosion and maximize beneficial vegetative growth.

ERM-1.3 Encourage Cluster Development

When reviewing development proposals, the County shall encourage cluster development in areas with moderate to high potential for sensitive habitat.

ERM-1.4 Protect Riparian Areas

The County shall protect riparian areas through habitat preservation, designation as open space or recreational land uses, bank stabilization, and development controls.

ERM-1.5 Riparian Management Plans and Mining Reclamation Plans

The County shall require mining reclamation plans and other management plans to include measures that protect, maintain, and restore riparian resources and habitats.

ERM-1.8 Open Space Buffers

The County shall require buffer areas between development projects and significant watercourses, riparian vegetation, wetlands, and other sensitive habitats and natural communities. These buffers should be sufficient to assure the continued existence of the waterways and riparian habitat in their natural state.

ERM-4.1 Energy Conservation and Efficiency Measures

The County shall encourage the use of solar energy, solar hot water panels, and other energy conservation and efficiency features in new construction and renovation of existing structures in accordance with State law.

ERM-4.2 Streetscape and Parking Area Improvements for Energy Conservation

The County shall promote the planting and maintenance of shade trees along streets and within parking areas of new urban development to reduce radiation heating.

ERM-7.1 Soil Conservation

The County of Tulare shall establish the proper controls and ordinances for soil conservation.

WR-1.4 Conversion of Agricultural Water Resources

For new urban development, the County shall discourage the transfer of water used for agricultural purposes (within the prior ten years) for domestic consumption except in the following circumstances:

1. The water remaining for the agricultural operation is sufficient to maintain the land as an economically viable agricultural use,
2. The reduction in infiltration from agricultural activities as a source of groundwater recharge will not significantly impact the groundwater basin.

WR-1.5 Expand Use of Reclaimed Wastewater
To augment groundwater supplies and to conserve potable water for domestic purposes, the County shall seek opportunities to expand groundwater recharge efforts

WR-1.6 Expand Use of Reclaimed Water
The County shall encourage the use of tertiary treated wastewater and household gray water for irrigation of agricultural lands, recreation and open space areas, and large landscaped areas as a means of reducing demand for groundwater resources.

WR-3.3 Adequate Water Availability
The County shall review new development proposals to ensure the intensity and timing of growth will be consistent with the availability of adequate water supplies. Projects must submit a Will-Serve letter as part of the application process, and provide evidence of adequate and sustainable water availability prior to approval of the tentative map or other urban development entitlement.

HS-9.2 Walkable Communities
The County shall require where feasible, the development of parks, open space, sidewalks and walking and biking paths that promote physical activity and discourage automobile dependency in all future communities.

PF-1.4 Available Infrastructure
The County shall encourage urban development to locate in existing UDBs and HDBs where infrastructure is available or may be established in conjunction with development. The County shall ensure that development does not occur unless adequate infrastructure is available, that sufficient water supplies are available or can be made available, and that there are adequate provisions for long-term management and maintenance of infrastructure and identified water supplies.

<u>Open-Space Policies</u>

LU-2.3 Open Space Character
The County shall require that all new development requiring a County discretionary approval, including parcel and subdivision maps, be planned and designed to maintain the scenic open space character of open space resources including, but not limited to, agricultural areas, rangeland, riparian areas, etc., within the view corridors of highways. New development shall utilize natural landforms and vegetation in the least visually disruptive way possible and use design, construction and maintenance techniques that minimize the visibility of structures on hilltops, hillsides, ridgelines, steep slopes, and canyons.

SL-1.3 Watercourses
The County shall protect visual access to, and the character of, Tulare County's scenic rivers, lakes, and irrigation canals by:

1. Locating and designing new development to minimize visual impacts and obstruction of views of scenic watercourses from public lands and right-of-ways, and
2. Maintaining the rural and natural character of landscape viewed from trails and watercourses used for public recreation.

ERM-5.1 Parks as Community Focal Points
The County shall strengthen the role of County parks as community focal points by providing community center/recreation buildings to new and existing parks, where feasible.

ERM-5.2 Park Amenities
The County shall provide a broad range of active and passive recreational opportunities within community parks. When possible, this should include active sports fields and facilities, community center/recreation buildings, children's play areas, multi-use areas and trails, sitting areas, and other specialized uses as appropriate.

ERM-5.3 Park Dedication Requirements
The County shall require the dedication of land and/or payment of fees, in accordance with local authority and State law (for example the Quimby Act), to ensure funding for the acquisition and development of public recreation facilities.

ERM-5.5 Collocated Facilities
The County shall encourage the development of parks near public facilities such as schools, community halls, libraries, museums, prehistoric sites, and open space areas and shall encourage joint-use agreements whenever possible.

ERM-5.6 Location and Size Criteria for Parks
Park types used in Tulare County are defined as follows:

- **Neighborhood Play Lots (Pocket Parks)**. The smallest park type, these are typically included as part of a new development to serve the neighborhood in which they are contained. Typical size is one acre or less. If a park of this type is not accessible to the general public, it cannot be counted towards the park dedication requirements of the County. Pocket Parks can be found in communities, hamlets, and other unincorporated areas.
- **Neighborhood Parks**. Neighborhood parks typically contain a tot lot and playground for 2-5 year olds and 5-12 year olds, respectively, one basketball court or two half-courts, baseball field(s), an open grassy area for informal sports activities (for example, soccer), and meandering concrete paths that contain low-level lighting for walking or jogging. In addition, neighborhood parks typically have picnic tables and a small group picnic shelter. These park types are typically in the range of 2 to 15 acres and serve an area within a ½-mile radius. Neighborhood parks can be found in communities, hamlets, and other unincorporated areas.
- **Community Parks**. Community parks are designed to serve the needs of the community as a whole. These facilities can contain the same facilities as the neighborhood park. In addition, these parks can contain sports facilities with night lighting, community centers, swimming pools, and facilities of special interest to the community. These parks are typically 15 to 40 acres in size and serve an area within a 2-mile radius. Community parks can be found in communities, planned community areas, and large hamlets.
- **Regional Parks**. Regional parks are facilities designed to address the needs of the County as a whole. These facilities may have an active recreation component (play area, group picnic area, etc.), but the majority of their area is maintained for passive recreation (such as hiking or horseback riding), and natural resource enjoyment. Regional parks are typically over 200 acres in size, but smaller facilities may be appropriate for specific sites of regional interest.

The following guidelines should be observed in creating and locating County parks:

1. The County shall strive to maintain an overall standard of five or more acres of

County-owned improved parkland per 1,000 population in the unincorporated portions of the County,

2. Neighborhood play lots (pocket parks) are encouraged as part of new subdivision applications as a project amenity, but are not included in the calculation of dedication requirements for the project,
3. Neighborhood parks at three acres per 1,000 population, if adjoining an elementary school and six acres per 1,000 population if separate [ERME IV-C; Open Space; Policy 3; Pg. 101],
4. Community parks at one-acre per 1,000 population if adjoining a high school and two acres per 1,000 population if separate [ERME IV-C; Open Space; Policy 4; Pg. 101],
5. Regional parks at one-acre per 1,000 population,
6.
7. Only public park facilities shall be counted toward Countywide parkland standards, and
8. A quarter mile walking radius is the goal for neighborhood parks.

ERM-5.12 Meet Changing Recreational Needs

The County shall promote the continued and expanded use of national and State forests, parks, and other recreational areas to meet the recreational needs of County residents.

ERM-5.13 Funding for Recreational Areas and Facilities

The County shall support the continued maintenance and improvement of existing recreational facilities and expansion of new recreational facilities opportunities for County, State, and Federal lands. The County shall strive to obtain adequate funding to improve and maintain existing parks, as well as construct new facilities.

ERM-5.15 Open Space Preservation

The County shall preserve natural open space resources through the concentration of development in existing communities, use of cluster development techniques, maintaining large lot sizes in agricultural areas, discouraging conversion of lands currently used for agricultural production, limiting development in areas constrained by natural hazards, and encouraging agricultural and ranching interests to maintain natural habitat in open space areas where the terrain or soil is not conducive to agricultural production.

HS-9.1 Healthy Communities

To the maximum extent feasible, the County shall strive through its land use decisions to promote community health and safety for all neighborhoods in the County by encouraging patterns of development that are safe and influence crime prevention, promote a high-quality physical environment and encourage physical activity by means such as sidewalks and walking and biking paths that discourage automobile dependency in existing communities.

<u>Noise Policies</u>

HS-8.7 Inside Noise

The County shall ensure that in instances where the windows and doors must remain closed to achieve the required inside acoustical isolation, mechanical ventilation or air conditioning is provided.

HS-8.8 Adjacent Uses

The County shall not permit development of new industrial, commercial, or other noise-generating land uses if resulting noise levels will exceed 60 dB Ldn (or CNEL) at the boundary of areas designated and zoned for residential or other noise-sensitive uses, unless it is determined to be necessary to promote

the public health, safety and welfare of the County.

HS-8.9 County Equipment
The County shall strive to purchase equipment that complies with noise level performance standards set forth in the Health and Safety Element.

HS-8.10 Automobile Noise Enforcement
The County shall encourage the CHP, Sheriff's office, and local police departments to actively enforce existing sections of the California Vehicle Code relating to adequate vehicle mufflers, modified exhaust systems, and other amplified noise.

HS-8.11 Peak Noise Generators
The County shall limit noise generating activities, such as construction, to hours of normal business operation (7 a.m. to 7 p.m.). No peak noise generating activities shall be allowed to occur outside of normal business hours without County approval.

HS-8.14 Sound Attenuation Features
The County shall require sound attenuation features such as walls, berming, heavy landscaping, between commercial, industrial, and residential uses to reduce noise and vibration impacts.

HS-8.15 Noise Buffering
The County shall require noise buffering or insulation in new development along major streets, highways, and railroad tracks.

<u>Safety Policies</u>

HS-1.1 Maintain Emergency Public Services
The County shall ensure that during natural catastrophes and emergency situations, the County can continue to provide essential emergency services.

HS-1.9 Emergency Access
The County shall require, where feasible, road networks (public and private) to provide for safe and ready access for emergency equipment and provide alternate routes for evacuation.

HS-1.10 Emergency Services Near Assisted Living Housing
In approving new facilities, such as nursing homes, housing for the elderly and other housing for the mentally and physically infirm, to the extent possible, the County shall ensure that such facilities are located within reasonable distance of fire and law enforcement stations.

HS-5.2 Development in Floodplain Zones
The County shall regulate development in the 100-year floodplain zones as designated on maps prepared by FEMA in accordance with the following:

1. Critical facilities (those facilities which should be open and accessible during emergencies) shall not be permitted.
2. Passive recreational activities (those requiring non-intensive development, such as hiking, horseback riding, picnicking) are permissible.
3. New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-5.8 Road Location
The County shall plan and site new roads to minimize disturbances to banks and existing channels and avoid excessive cuts and accumulations of waste soil and vegetative debris near natural drainage ways.

HS-5.9 Floodplain Development Restrictions
The County shall ensure that riparian areas and drainage areas within 100-year floodplains are free from development that may adversely impact floodway capacity or characteristics of natural/riparian areas or natural groundwater recharge areas.

HS-5.10 Flood Control Design
The County shall evaluate flood control projects involving further channeling, straightening, or lining of waterways until alternative multipurpose modes of treatment, such as wider berms and landscaped levees, in combination with recreation amenities, are studied.

HS-5.11 Natural Design
The County shall encourage flood control designs that respect natural curves and vegetation of natural waterways while retaining dynamic flow and functional integrity.

HS-7.4 Upgrading for Streets and Highways
The County shall evaluate and upgrade vital streets and highways to an acceptable level for emergency services.

PFS-7.1 Fire Protection
The County shall strive to expand fire protection service in areas that experience growth in order to maintain adequate levels of service.

PFS-7.6 Provision of Station Facilities and Equipment
The County shall strive to provide sheriff and fire station facilities, equipment (engines and other apparatus), and staffing necessary to maintain the County's service goals. The County shall continue to cooperate with mutual aid providers to provide coverage throughout the County.

PFS-7.11 Locations of Fire and Sheriff Stations/Sub-stations
The County shall strive to locate fire and sheriff sub-stations in areas that ensure the minimum response times to service calls.

PFS-7.12 Design Features for Crime Prevention and Reduction
The County shall promote the use of building and site design features as means for crime prevention and reduction.

PFS-8.3 Location of School Sites
The County shall work with school districts and land developers to locate school sites consistent with current and future land uses. The County shall also encourage siting new schools near the residential areas that they serve and with access to safe pedestrian and bike routes to school.

<u>Other</u>

PF-2.7 Improvement Standards in Communities
The County shall require development within the designated UDBs to meet an urban standard for improvements. Typical improvements shall include curbs, gutters, sidewalks, and community sewer and water systems.

ERM-6.3 Alteration of Sites with Identified Cultural Resources
When planning any development or alteration of a site with identified cultural or archaeological resources, consideration should be given to ways of protecting the resources. Development should be permitted in these areas only after a site specific investigation has been conducted pursuant to CEQA to define the extent and value of resource, and

mitigation measures proposed for any impacts the development may have on the resource.

HS-8.5 State Noise Standards
The County shall enforce the State Noise Insulation Standards (California Administrative Code, Title 24) and Chapter 35 of the Uniform Building Code (UBC). Title 24 requires that interior noise levels not exceed 45 dB Ldn (or CNEL) with the windows and doors closed within new developments of multi-family dwellings, condominiums, hotels, or motels. Where it is not possible to reduce exterior noise levels within an acceptable range the County shall require the application of noise reduction technology to reduce interior noise levels to an acceptable level.

HS-8.6 Noise Level Criteria
The County shall ensure noise level criteria applied to land uses other than residential or other noise-sensitive uses are consistent with the recommendations of the California Office of Noise Control (CONC).

WR-3.9 Establish Critical Water Supply Areas
The County shall designate Critical Water Supply Areas to include the specific areas used by a municipality or community for its water supply system, areas critical to groundwater recharge, and other areas possessing a vital role in the management of the water resources in the County.

PFS-6.1 Telecommunications Services
The County shall work with telecommunication providers to ensure that all residents and businesses have access to telecommunications services, including broadband internet service. To maximize access to inexpensive telecommunications services, the County shall encourage marketplace competition from multiple service providers.

PFS-8.4 Library Facilities and Services
The County shall encourage expansion of library facilities and services as necessary to meet the needs (e.g., internet access, meeting rooms, etc.) of future population growth.

PFS-8.5 Government Facilities in Community Centers
The County shall actively support development and expansion of federal, State, County, districts, and other governmental offices and facilities where infrastructure exists within community core areas.

S-9.1 Expansion of Gas and Electricity Facilities
The County shall coordinate with gas and electricity service providers to plan the expansion of gas and electrical facilities to meet the future needs of County residents.

General Plan Policies that Relate to Health

PF-2.7 Improvement Standards in Communities
The County shall require development within the designated UDBs to meet an urban standard for improvements. Typical improvements shall include curbs, gutters, sidewalks, and community sewer and water systems.

Land Use Element

LU-1.1 Smart Growth and Healthy Communities
The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including:

1. Creating walkable neighborhoods,
2. Providing a mix of residential densities,
3. Creating a strong sense of place,
4. Mixing land uses,
5. Directing growth toward existing communities,
6. Building compactly,
7. Discouraging sprawl,
8. Encouraging infill,
9. Preserving open space,
10. Creating a range of housing opportunities and choices,
11. Utilizing planned community zoning to provide for the orderly pre-planning and long term development of large tracks of land which may contain a variety of land uses, but are under unified ownership or development control, and
12. Encouraging connectivity between new and existing development.

LU-1.2 Innovative Development
The County shall promote flexibility and innovation through the use of planned unit developments, development agreements, specific plans, Mixed Use projects, and other innovative development and planning techniques.

LU-1.3 Prevent Incompatible Uses
The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.

LU-1.4 Compact Development
The County shall actively support the development of compact mixed-use projects that reduce travel distances.

LU- 1.5 Paper Subdivision Consolidations
The County shall encourage consolidation of paper parcels/subdivisions, especially those lots that are designated Valley Agriculture (VA), Foothill Agriculture (FA), or Resource Conservation (RC), are irregular in shape, inadequate in size for proper use, or lack infrastructure.

LU-1.6 Permitting Procedures and Regulations
The County shall continue to ensure that its permitting procedures and regulations are consistent and efficient.

LU-1.7 Development on Slopes
The County shall require a preliminary soils report for development projects in areas with shallow or unstable soils or slopes in excess of 15 percent. If the preliminary soil report indicates soil conditions could be unstable, a detailed geologic/hydrologic report by a registered geologist, civil engineer, or engineering geologist shall be

required demonstrating the suitability of any proposed or additional development.

LU-1.8 Encourage Infill Development
The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

LU-1.10 Roadway Access
The County shall require access to public roadways for all new development.

LU-3.1 Residential Developments
The County shall encourage new major residential development to locate near existing infrastructure for employment centers, services, and recreation.

LU-3.3 High-Density Residential Locations
The County shall encourage high-density residential development (greater than 14 dwelling units per gross acre) to locate along collector roadways and transit routes, and near public facilities (e.g., schools, parks), shopping, recreation, and entertainment.

LU-3.6 Project Design
The County shall require residential project design to consider natural features, noise exposure of residents, visibility of structures, circulation, access, and the relationship of the project to surrounding uses. Residential densities and lot patterns will be determined by these and other factors. As a result, the maximum density specified by General Plan designations or zoning for a given parcel of land may not be attained.

LU-7.3 Friendly Streets
The County shall encourage new streets within UDBs to be designed and constructed to not only accommodate traffic, but also serve as comfortable pedestrian and cyclist environments. These should include, but not be limited to:

1. Street tree planting adjacent to curbs and between the street and sidewalk to provide a buffer between pedestrians and automobiles, where appropriate,
2. Minimize curb cuts along streets,
3. Sidewalks on both sides of streets, where feasible,
4. Bike lanes and walking paths, where feasible on collectors and arterials, and
5. Traffic calming devices such as roundabouts, bulb-outs at intersections, traffic tables, and other comparable techniques.

LU-7.5 Crime Prevention through Design
The County shall encourage design of open space areas, bicycle and pedestrian systems, and housing projects so that there is as much informal surveillance by people as possible to deter crime.

LU-7.15 Energy Conservation
The County shall encourage the use of solar power and energy conservation building techniques in all new development.

LU-7.16 Water Conservation
The County shall encourage the inclusion of "extra-ordinary' water conservation and demand management measures for residential, commercial, and industrial

indoor and outdoor water uses in all new urban development.

Housing Element

Housing Policy 1.51
Encourage the construction of new housing units for "special needs" groups, including senior citizens, large families, single heads of households, households of persons with physical and/or mental disabilities, minorities, farmworkers, and the homeless in close proximity to transit, services, and jobs.

Housing Policy 3.12
Support locally initiated programs to provide neighborhood parks and recreational facilities for residential areas within unincorporated communities.

Housing Policy 3.13
Encourage subdivision and housing unit design, which provides for a reasonable level of safety and security.

Housing Policy 3.16
Actively seek federal, state, and private foundation grant funds for park and recreation facilities in unincorporated areas, including dual-use storm drainage ponding basins/recreation parks.

Housing Policy 4.21
Promote energy conservation opportunities in new residential development.

Housing Policy 4.22
Enforce provisions of the Subdivision Map Act regulating energy-efficient subdivision design.

Housing Policy 5.21
Administer and enforce the relevant portions of the Health and Safety Code.

Housing Policy 5.26
Prohibit concentrations of dwelling units near potentially incompatible agricultural uses as defined in the Animal Confinement Facilities Plan.

Environmental Resources Element

ERM-5.2 Park Amenities
The County shall provide a broad range of active and passive recreational opportunities within community parks. When possible, this should include active sports fields and facilities, community center/recreation buildings, children's play areas, multi-use areas and trails, sitting areas, and other specialized uses as appropriate.

Air Quality Element

AQ-1.3 Cumulative Air Quality Impacts
The County shall require development to be located, designed, and constructed in a manner that would minimize cumulative air quality impacts. Applicants shall be required to propose alternatives as part of the State CEQA process that reduce air emissions and enhance, rather than harm, the environment.

AQ-1.4 Air Quality Land Use Compatibility
The County shall evaluate the compatibility of industrial or other developments, which are likely to cause undesirable air pollution with regard to proximity to sensitive land uses, and wind direction and circulation in an effort to alleviate effects upon sensitive receptors.

AQ-1.7 Support Statewide Climate Change Solutions

The County shall monitor and support the efforts of Cal/EPA, CARB, and the SJVAPCD, under AB 32 (Health and Safety Code §38501 et seq.), to develop a recommended list of emission reduction strategies. As appropriate, the County will evaluate each new project under the updated General Plan to determine its consistency with the emission reduction strategies.

AQ-1.8 Greenhouse Gas Emissions Reduction Plan/Climate Action Plan

The County will develop a Greenhouse Gas Emissions Reduction Plan (Plan) that identifies greenhouse gas emissions within the County as well as ways to reduce those emissions. The Plan will incorporate the requirements adopted by the California Air Resources Board specific to this issue. In addition, the County will work with the Tulare County Association of Governments and other applicable agencies to include the following key items in the regional planning efforts.

1. Inventory all known, or reasonably discoverable, sources of greenhouse gases in the County,
2. Inventory the greenhouse gas emissions in the most current year available, and those projected for year 2020, and
3. Set a target for the reduction of emissions attributable to the County's discretionary land use decisions and its own internal government operations.

AQ-2.2 Indirect Source Review

The County shall require major development projects, as defined by the SJVAPCD, to reasonably mitigate air quality impacts associated with the project. The County shall notify developers of SJVAPCD Rule 9510 – Indirect Source Review requirements and work with SJVAPCD to determine mitigations, as feasible, that may include, but are not limited to the following:

1. Providing bicycle access and parking facilities,
2. Increasing density,
3. Encouraging mixed use developments,
4. Providing walkable and pedestrian-oriented neighborhoods,
5. Providing increased access to public transportation,
6. Providing preferential parking for high-occupancy vehicles, car pools, or alternative fuels vehicles, and
7. Establishing telecommuting programs or satellite work centers.

AQ-2.3 Transportation and Air Quality

When developing the regional transportation system, the County shall work with TCAG to comprehensively study methods of transportation, which may contribute to a reduction in air pollution in Tulare County. Some possible alternatives that should be studied are:

1. Commuter trains (Light Rail, Amtrak, or High Speed Rail) connecting with Sacramento, Los Angeles, and San Francisco, with attractive services scheduled up and down the Valley,
2. Public transportation such as buses and light rail, to serve between communities of the Valley, publicly subsidized if feasible,
3. Intermodal public transit such as buses provided with bicycle racks, bicycle parking at bus stations, bus service to train stations and airports, and park and ride facilities, and
4. Community transportation systems supportive of alternative transportation modes, such as cycling or walking trails,

with particular attention to high-density areas.

AQ-3.2 Infill near Employment
The County shall identify opportunities for infill development projects near employment areas within all unincorporated communities and hamlets to reduce vehicle trips.

AQ-3.3 Street Design
The County shall promote street design that provides an environment, which encourages transit use, biking, and pedestrian movements.

AQ-3.4 Landscape
The County shall encourage the use of ecologically based landscape design principles that can improve local air quality by absorbing CO_2, producing oxygen, providing shade that reduces energy required for cooling, and filtering particulates. These principles include, but are not limited to, the incorporation of parks, landscaped medians, and landscaping within development.

AQ-3.5 Alternative Energy Design
The County shall encourage all new development, including rehabilitation, renovation, and redevelopment, to incorporate energy conservation and green building practices to maximum extent feasible. Such practices include, but are not limited to building orientation and shading, landscaping, and the use of active and passive solar heating and water systems.

AQ-3.6 Mixed Land Uses
The County shall encourage the clustering of land uses that generate high trip volumes, especially when such uses can be mixed with support services and where they can be served by public transportation.

Health and Safety Element

HS-1.4 Building and Codes
Except as otherwise allowed by State law, the County shall ensure that all new buildings intended for human habitation are designed in compliance with the latest edition of the California Building Code, California Fire Code, and other adopted standards based on risk (e.g., seismic hazards, flooding), type of occupancy, and location (e.g., floodplain, fault).

HS-1.5 Hazard Awareness and Public Education
The County shall continue to promote awareness and education among residents regarding possible natural hazards, including soil conditions, earthquakes, flooding, fire hazards, and emergency procedures.

HS-1.6 Public Safety Programs
The County shall promote public safety programs, including neighborhood watch programs, child identification and fingerprinting, public awareness and prevention of fire hazards, and other public education efforts.

HS-1.7 Safe Housing and Structures
The County shall continue to seek grant funding for the rehabilitation of deteriorated and dilapidated structures and provide available information regarding housing programs and other public *services.*

HS-1.9 Emergency Access
The County shall require, where feasible, road networks (public and private) to provide for safe and ready access for

emergency equipment and provide alternate routes for evacuation.

HS-1.10 Emergency Services near Assisted Living Housing
In approving new facilities, such as nursing homes, housing for the elderly and other housing for the mentally and physically infirm, to the extent possible, the County shall ensure that such facilities are located within reasonable distance of fire and law enforcement stations.

HS-4.3 Incompatible Land Uses
The County shall prevent incompatible land uses near properties that produce or store hazardous waste.

HS-4.4 Contamination Prevention
The County shall review new development proposals to protect soils, air quality, surface water, and groundwater from hazardous materials contamination.

HS-4.5 Increase Public Awareness
The County shall work to educate the public about household hazardous waste and the proper method of disposal.

HS-4.6 Pesticide Control
The County shall monitor studies of pesticide use and the effects of pesticide on residents and wildlife and require mitigation of the effects wherever feasible and appropriate.

HS-4.8 Hazardous Materials Studies
The County shall ensure that the proponents of new development projects address hazardous materials concerns through the preparation of Phase I or Phase II hazardous materials studies for each identified site as part of the design phase for each project. Recommendations required to satisfy federal or State cleanup standards outlined in the studies will be implemented as part of the construction phase for each project.

HS-5.1 Development Compliance with Federal, State, and Local Regulations
The County shall ensure that all development within the designated floodway or floodplain zones conforms with FEMA regulations and the Tulare County Flood Damage Prevention Ordinance.

New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-5.2 Development in Floodplain Zones
The County shall regulate development in the 100-year floodplain zones as designated on maps prepared by FEMA in accordance with the following:

4. Critical facilities (those facilities, which should be open and accessible during emergencies) shall not be permitted.
5. Passive recreational activities (those requiring non-intensive development, such as hiking, horseback riding, picnicking) are permissible.
6. New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-8.5 State Noise Standards
The County shall enforce the State Noise Insulation Standards (California Administrative Code, Title 24) and Chapter

35 of the Uniform Building Code (UBC). Title 24 requires that interior noise levels not exceed 45 dB Ldn (or CNEL) with the windows and doors closed within new developments of multi-family dwellings, condominiums, hotels, or motels. Where it is not possible to reduce exterior noise levels within an acceptable range, the County shall require the application of noise reduction technology to reduce interior noise levels to an acceptable level.

HS-9.1 Healthy Communities
To the maximum extent feasible, the County shall strive through its land use decisions to promote community health and safety for all neighborhoods in the County by encouraging patterns of development that are safe and influence crime prevention, promote a high-quality physical environment and encourage physical activity by means such as sidewalks and walking and biking paths that discourage automobile dependency in existing communities.

HS-9.2 Walkable Communities
The County shall require where feasible, the development of parks, open space, sidewalks and walking and biking paths that promote physical activity and discourage automobile dependency in all future communities.
Water Resource Element

WR-2.1 Protect Water Quality
All major land use and development plans shall be evaluated as to their potential to create surface and groundwater contamination hazards from point and non-point sources. The County shall confer with other appropriate agencies, as necessary, to assure adequate water quality review to prevent soil erosion; direct discharge of potentially harmful substances; ground leaching from storage of raw materials, petroleum products, or wastes; floating debris; and runoff from the site.

WR-2.2 National Pollutant Discharge Elimination System (NPDES) Enforcement
The County shall continue to support the State in monitoring and enforcing provisions to control non-point source water pollution contained in the U.S. EPA NPDES program as implemented by the Water Quality Control Board.

WR-2.3 Best Management Practices (BMPs)
The County shall continue to require the use of feasible BMPs and other mitigation measures designed to protect surface water and groundwater from the adverse effects of construction activities, agricultural operations requiring a County Permit and urban runoff in coordination with the Water Quality Control Board.

Transportation and Circulation Element

TC-1.18 Balanced System
The County shall strive to meet transportation needs and maintain LOS standards through a balanced Multimodal Transportation Network that provides alternatives to the automobile.

TC-1.19 Balanced Funding
The County shall promote a balanced approach to the allocation of transportation funds to optimize the overall County transportation system.

TC-4.1 Transportation Programs
The County shall support the continued coordination of transportation programs provided by social service agencies,

particularly those serving elderly and/or handicapped.

TC-4.2 Determine Transit Needs
The County will continue to work with TCAG, cities, and communities in the County to evaluate and respond to public transportation needs.

TC-4.3 Support Tulare County Area Transit
The County shall request the support of TCAG for development of transit services outlined in the County's Transit Development Plan (TDP). Efforts to expand Tulare County Area Transit should be directed towards:

1. Encouraging new and improving existing transportation services for the elderly and disabled, and
2. Providing intercommunity services between unincorporated communities and cities.

TC-4.4 Nodal Land Use Patterns that Support Public Transit
The County shall encourage land uses that generate higher ridership including; high density residential, employment centers, schools, personal services, administrative and professional offices, and social/recreational centers, to be clustered within a convenient walking distance of one another.

TC-5.1 Bicycle/Pedestrian Trail System
The County shall coordinate with TCAG and other agencies to develop a Countywide integrated multi-purpose trail system that provides a linked network with access to recreational, cultural, and employment facilities, as well as offering a recreational experience apart from that available at neighborhood and community parks.

TC-5.2 Consider Non-Motorized Modes in Planning and Development
The County shall consider incorporating facilities for non-motorized users, such as bike routes, sidewalks, and trails when constructing or improving transportation facilities and when reviewing new development proposals. For developments with 50 or more dwelling units or non-residential projects with an equivalent travel demand, the feasibility of such facilities shall be evaluated.

TC-5.3 Provisions for Bicycle Use
The County shall work with TCAG to encourage local government agencies and businesses to consider including bicycle access and provide safe bicycle parking facilities at office buildings, schools, shopping centers, and parks.

Public Facilities and Services Element

PFS-1.1 Existing Development
The County shall generally give priority for the maintenance and upgrading of County-owned and operated facilities and services to existing development in order to prevent the deterioration of existing levels-of-service.

PFS-1.3 Impact Mitigation
The County shall review development proposals for their impacts on infrastructure (for example, sewer, water, fire stations, libraries, streets, etc). New development shall be required to pay its proportionate share of the costs of infrastructure improvements required to serve the project to the extent permitted by State law. The lack of available public or

private services or adequate infrastructure to serve a project, which cannot be satisfactorily mitigated by the project, may be grounds for denial of a project or cause for the modification of size, density, and/or intensity of the project.

PFS-1.4 Standards of Approval
The County should not approve any development unless the following conditions are met:

1. The applicant can demonstrate all necessary infrastructure will be installed and adequately financed,
2. Infrastructure improvements are consistent with adopted County infrastructure plans and standards, and
3. Funding mechanisms are provided to maintain, operate, and upgrade the facilities throughout the life of the project.

PFS-1.5 Funding for Public Facilities
The County shall implement programs and/or procedures to ensure that funding mechanisms necessary to adequately cover the costs related to planning, capital improvements, maintenance, and operations of necessary public facilities and services are in place, whether provided by the County or another entity.

PFS-1.6 Funding Mechanisms
The County shall use a wide range of funding mechanisms, such as the following, to adequately fund capital improvements, maintenance, and on-going operations for publicly owned and/or operated facilities:

1. Establishing appropriate development impact fees,
2. Establishing assessment districts, and
3. Pursuing grant funding.

PFS-1.7 Coordination with Service Providers
The County shall work with special districts, community service districts, public utility districts, mutual water companies, private water purveyors, sanitary districts, and sewer maintenance districts to provide adequate public facilities and to plan/coordinate, as appropriate, future utility corridors in an effort to minimize future land use conflicts.

PFS-1.8 Funding for Service Providers
The County shall encourage special districts, including community service districts and public utility districts to:

1. Institute impact fees and assessment districts to finance improvements,
2. Take on additional responsibilities for services and facilities within their jurisdictional boundaries up to the full extent allowed under State law, and
3. Investigate feasibility of consolidating services with other districts and annexing systems in proximity to promote economies of scale, such as annexation to city systems and regional wastewater treatment systems.

PFS-1.9 New Special Districts
When feasible, the County shall support the establishment of new special districts, including community service districts and public utility districts, to assume responsibility for public facilities and services.

PFS-1.10 Homeowner Associations
The County shall support the creation of homeowner associations, condominium associations, or other equivalent organizations to assume responsibility for specific public facilities and services.

PFS-1.11 Facility Sizing
The County shall ensure that publicly-owned and operated facilities are designed to meet the projected capacity needed in their service area to avoid the need for future replacement to achieve upsizing. For facilities subject to incremental sizing, the initial design shall include adequate land area and any other elements to easily expand in the future.

PFS-1.12 Security
The County shall seek to minimize vulnerability of public facilities to natural and man-made hazards and threats.

PFS-2.1 Water Supply
The County shall work with agencies providing water service to ensure that there is an adequate quantity and quality of water for all uses, including water for fire protection, by, at a minimum, requiring a demonstration by the agency providing water service of sufficient and reliable water supplies and water management measures for proposed urban development.

PFS-2.2 Adequate Systems
The County shall review new development proposals to ensure that the intensity and timing of growth will be consistent with the availability of adequate production and delivery systems. Projects must provide evidence of adequate system capacity prior to approval.

PFS-2.3 Well Testing
The County shall require new development that includes the use of water wells to be accompanied by evidence that the site can produce the required volume of water without impacting the ability of existing wells to meet their needs.

PFS-2.4 Water Connections
The County shall require all new development in UDBs, UABs, Community Plans, Hamlet Plans, Planned Communities, Corridor Areas, Area Plans, existing water district service areas, or zones of benefit, to connect to the community water system, where such system exists. The County may grant exceptions in extraordinary circumstances, but in these cases, the new development shall be required to connect to the water system when service becomes readily available.

PFS-2.5 New Systems or Individual Wells
Where connection to a community water system is not feasible per PFS-2.4: Water Connections, service by individual wells or new community systems may be allowed if the water source meets standards for quality and quantity.

PFS-3.1 Private Sewage Disposal Standards
The County shall maintain adequate standards for private sewage disposal systems (e.g., septic tanks) to protect water quality and public health.

PFS-3.2 Adequate Capacity
The County shall require development proposals to ensure the intensity and timing of growth is consistent with the availability of adequate wastewater treatment and disposal capacity.

PFS-3.3 New Development Requirements
The County shall require all new development, within UDBs, UABs, Community Plans, Hamlet Plans, Planned Communities, Corridor Areas, Area Plans, existing wastewater district service areas, or

zones of benefit, to connect to the wastewater system, where such systems exist. The County may grant exceptions in extraordinary circumstances, but in these cases, the new development shall be required to connect to the wastewater system when service becomes readily available.

PFS-3.4 Alternative Rural Wastewater Systems

The County shall consider alternative rural wastewater systems for areas outside of community UDBs and HDBs that do not have current systems or system capacity. For individual users, such systems include elevated leach fields, sand filtration systems, evapotranspiration beds, osmosis units, and holding tanks. For larger generators or groups of users, alternative systems, including communal septic tank/leach field systems, package treatment plants, lagoon systems, and land treatment, can be considered.

PFS-3.7 Financing

The County shall cooperate with special districts when applying for State and federal funding for major wastewater related expansions/upgrades when such plans promote the efficient solution to wastewater treatment needs for the area and County.

PFS-4.2 Site Improvements

The County shall ensure that new development in UDBs, UABs, Community Plans, Hamlet Plans, Planned Communities, Corridor Areas, and Area Plans includes adequate stormwater drainage systems. This includes adequate capture, transport, and detention/retention of stormwater.

PFS-4.3 Development Requirements

The County shall encourage project designs that minimize drainage concentrations and impervious coverage, avoid floodplain areas, and where feasible, provide a natural watercourse appearance.

PFS-4.4 Stormwater Retention Facilities

The County shall require on-site detention/retention facilities and velocity reducers when necessary to maintain existing (pre-development) storm flows and velocities in natural drainage systems. The County shall encourage the multi-purpose design of these facilities to aid in active groundwater recharge.

PFS-4.5 Detention/Retention Basins Design

The County shall require that stormwater detention/retention basins be visually unobtrusive and provide a secondary use, such as recreation, when feasible.

PFS-4.6 Agency Coordination

The County shall work with the Army Corps of Engineers and other appropriate agencies to develop stormwater detention/retention facilities and recharge facilities that enhance flood protection and improve groundwater recharge.

PFS-4.7 NPDES Enforcement

The County shall continue to monitor and enforce provisions to control non-point source water pollution contained in the U.S. Environmental Protection Agency National Pollution Discharge Elimination System (NPDES) program.

PFS-5.1 Land Use Compatibility with Solid Waste Facilities

The County shall ensure that solid waste facility sites (for example, landfills) are

protected from the encroachment by sensitive and/or incompatible land uses.

Fire Staffing and Reponses Time Standards			
	Demographics	**Staffing/ Response Time**	**% of Calls**
Urban	>1,000 people/sq. mi.	15 FF/9 min.	90
Suburban	500-100 people/sq. mi.	10 FF/10 min.	80
Rural	<500 people/sq. mi.	6 FF/14 min.	80
Remote*	Travel Dist.>8 min.	4 FF/no specific response time	90

*Upon assembling the necessary resources at the emergency scene, the fire department should have the capacity to safety commence an initial attach within 2 minutes, 90% of the time.
FF: fire fighters

PFS-5.8 Hazardous Waste Disposal Capabilities

The County shall require the proper disposal and recycling of hazardous materials in accordance with the County's Hazardous Waste Management Plan.

PFS-7.2 Fire Protection Standards

The County shall require all new development to be adequately served by water supplies, storage, and conveyance facilities supplying adequate volume, pressure, and capacity for fire protection.

PFS-7.5 Fire Staffing and Response Time Standards

The County shall strive to maintain fire department staffing and response time goals consistent with National Fire Protection Association (NFPA) standards.

PFS-7.12 Design Features for Crime Prevention and Reduction

The County shall promote the use of building and site design features as means for crime prevention and reduction.

PFS-8.2 Joint Use Facilities and Programs

The County shall encourage the development of joint school facilities, recreation facilities, and educational and service programs between school districts and other public agencies.

PFS-8.3 Location of School Sites

The County shall work with school districts and land developers to locate school sites consistent with current and future land uses. The County shall also encourage siting new schools near the residential areas that they serve and with access to safe pedestrian and bike routes to school.

PFS-9.1 Expansion of Gas and Electricity Facilities

The County shall coordinate with gas and electricity service providers to plan the expansion of gas and electrical facilities to meet the future needs of County residents.

PFS-9.2 Appropriate Siting of Natural Gas and Electric Systems

The County shall coordinate with natural gas and electricity service providers to locate and design gas and electric systems that minimize impacts to existing and future residents.

PFS-9.3 Transmission Corridors

The County shall work with the Public Utilities Commission and power utilities so that transmission corridors meet the following minimum requirements:

1. Transmission corridors shall be located to avoid health impacts on residential lands and sensitive receptors, and
2. Transmission corridors shall not impact the economic use of adjacent properties.

Goals, Objectives, and Policies specific to Cutler-Orosi

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements, which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives.

Taken as a whole, goals, objectives and policies provide the guidelines as to how the community is to grow in terms of type, quantity and quality of development. The goals, objectives and policies are an integral part of the plan itself and the final land use map and plan description must reflect the goals, objectives and policies of the community. The plan map then is a graphic portrayal of what goals, objectives and policies are intended to accomplish.

The goals, objectives and policies of this community plan are divided into four categories. The four areas are: Community Development, Housing, Economic Base, and Environmental Quality.

****** Draft Ground Water Management Act Policy ********

Goal: That the County and Kings River East Groundwater Sustainability Agency work collaboratively under the Tulare County General Plan to assist the Cutler PUD and Orosi PUD in establishing conservation measures and credits in order to sustainably grow water and sewer infrastructure consistent with the Projected Growth Rates considered in the General Plan of Tulare County.

Objective: To not inhibit the Cutler-Orosi Community Plan projected growth rates due to the Groundwater Management Act. Instead to use thoughtful localized conservation measures, funding and credits for storm water retention / groundwater reclamation that can be utilized by the School District and PUD's in order meet the demands of the Kings River East GSA and the Management Area.

Policy 1: The County as a member of the Kings River East GSA will use its role as a GSA member to assist the Cutler PUD, Orosi PUD, and Cutler-Orosi Joint Unified School District to enhance and establish conservation measures that reduce the demand requirements consistent with previous drought measures (2014-15) that are still the law of California, and under the precepts of SGMA.

Policy 2: The Cutler PUD, the Orosi PUD, and Cutler-Orosi Joint Unified School District should work with the Kings River East GSA to establish credits for storm water retention / ground water reclamation consistent with the County General Plan.

Policy 3: The Cutler PUD and the Orosi PUD seek and be given credits consistent with the Kings River East GSA's ultimate definitions of available reclamation credits for recycled wastewater effluent land applications.

Policy 4: That the Cutler-Orosi Joint Unified School District be given credits for any ground water reclamation they can supply through utilization of their existing storm water detention basins.

Community Development

GOAL I: Foster a cohesive community with easy access to necessary services and support facilities

Objective I: Prevent premature urban-type development on agriculturally productive lands.

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.
2. The County shall carefully coordinate the extension of water and sewer services in the Plan Area with the Cutler PUD and the Orosi PUD to promote orderly and efficient development patterns.

GOAL II: Avoid land use conflicts through planning separation of uses.

Objective I: Promote concentrations of similar or compatible uses.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.
3. The County shall ensure that new development respects Tulare County's heritage by requiring that development respond to its context, be compatible with the traditions and character of each community, and develop in an orderly fashion, which is compatible with the scale of surrounding structures.
4. The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.
5. Land well suited for industrial development because of access, availability of infrastructure and proximity to similar land uses should be designated for industry and protected from the encroachment of incompatible uses.
6. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.
7. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
8. Locate high-density residential uses in close proximity to planned shopping areas.
9. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
10. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
11. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
12. The County shall work with the Cutler-Orosi Unified School District in facilitating the location and establishment of new school sites, or expansion of existing sites, as needed.
13. The County shall work with the Cutler-Orosi School District to provide safe routes to school.
14. The County will solicit recommendations from all interested public agencies on matters regarding the Cutler-Orosi

Community Plan.

15. The Cutler-Orosi Community Plan should be reviewed every five years to determine if amendments are appropriate.
16. When considering any land use proposal, capital expenditure or other matters of community importance, the County will request input from the local service district and other affected agencies.

Objective II: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

Objective III: Encourage land uses adjacent to State Route 63 and Avenue 416 which are consistent with noise impacts.

Policies:

1. Encourage commercial and/or industrial development to locate adjacent to SR 216 where access is appropriate for such development.
2. Require installation of walls, berms or heavy planting along SR 63 in conjunction with any new residential development.
3. Discourage new residential development on vacant lots within areas proposed for commercial and industrial development.
4. Encourage the eventual conversion of existing residential uses within areas proposed for commercial and industrial development to nonresidential uses; except living quarters used in conjunction with a business.
5. Phase-out existing non-conforming commercial and industrial uses within planned residential areas by zoning such areas residential, and by enforcement of local zoning regulations pertaining to illegal buildings and uses.
6. The County shall designate and zone sufficient amounts of land to accommodate existing and projected industrial, commercial, residential, and public (e.g., parks and recreational) needs of the community.
7. Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

GOAL III: Achieve development densities consistent with levels of available service.

Objective I: Urbanization in the planning areas should be contiguous and compact.

Policies:

1. The County shall encourage high-density residential development (greater than 14 dwelling units per gross acre) to locate along collector roadways and transit routes, and near public facilities (e.g., schools, parks), shopping, recreation, and entertainment.
2. The County shall review development proposals for their impacts on infrastructure (for example, sewer, water, fire stations, libraries, streets, etc.). New development shall be required to pay its proportionate share of the costs of infrastructure improvements required to serve the project to the extent permitted by State law. The lack of available public or private services or adequate infrastructure to serve a project, which cannot be satisfactorily mitigated by the project, may be grounds for denial of a project or cause for the modification of size, density, and/or intensity of the project.
3. The extension of water and sewer facilities

into the planning area shall be coordinated with the policies of this Plan and the goals and policies of the Tulare County General Plan. Development in the planning area shall pay their fair share for services.

Objective II: Encourage merger of existing vacant substandard lots within the townsite of Cutler-Orosi.

Policies:

1. Conduct a study of the Cutler-Orosi townsite area to determine the impact of a comprehensive vacant lot merger action and undertake such a merger, if feasible.

GOAL IV: Coordinate Community Development Decisions with the Cutler PUD and Orosi PUD.

Objective I: Ensure that all development can be served by the Cutler Public Utility District (PUD) and Orosi PUD during the planning period.

Policies:

1. Coordinate zoning with availability of utilities and community services.
2. Promote commercial and industrial development with wastewater discharge characteristics, which can be accommodated by the Cutler PUD and Orosi PUD.
3. Encourage industries with excessive effluent to pre-treat Cutler-Orosi wastewater system.
4. Encourage coordination between developers and the Cutler-Orosi throughout the application and development process to prevent time delays and to assure that the Cutler-Orosi can accommodate the needs of any proposed development.
5. Before the issuance of any land use permit, the Tulare County Resource Management Agency must receive confirmation from the Cutler-Orosi Wastewater Treatment Plant that water and sewer service requirements can be accommodated.
6. Assist the Cutler-Orosi Wastewater Treatment Plant in applications for grant funds to carry out their capital improvement program for providing, maintaining and improving their sewer and water systems to serve new and existing developments, which implement the goals and objectives of this Plan and of the Tulare County General Plan.
7. Prohibit to the extent allowed by law all development from holding, diverting and/or disposing of storm water run-off at locations, or in such a manner, as to cause groundwater recharge contributable to raising the groundwater to an unsafe level in the vicinity of the Cutler/Orosi wastewater treatment facilities.
8. Investigate the necessity of preparing a drainage plan, within five years of adoption of the Community Plan, for diverting and disposing of storm water runoff and excess irrigation water at a location, or locations, where the retention or disposition of such water will not contribute to raising the groundwater level in the vicinity of the Cutler-Orosi wastewater treatment facilities.
9. Before the issuance of any land use permit, the Tulare County Economic and Planning Department will require all project applications for new development or redevelopment to include storm water disposal plans in accordance with the recommendations of the Tulare County Public Works Department and Caltrans to prevent runoff flows into the State highway rights-of-way.

Housing

GOAL V: Provide safer and adequate housing for all citizens within the community.

Objective I: Reduce deficiencies in existing housing stock.

Policies:

1. Through the Housing Element process, Tulare County shall strive to minimize or eliminate blight in Cutler-Orosi.
2. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
3. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
4. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
5. The County will strive to ensure that there is an adequate amount of planned residential land to meet the housing needs of Cutler-Orosi.
6. The County will ensure that there are adequate sites and will work with the Cutler PUD and Orosi PUD and other agencies to ensure that there are adequate public facilities to support future housing needs in Cutler-Orosi.
7. The County will work diligently towards the rehabilitation of the housing stock in Cutler-Orosi.
8. The County will attempt to maintain a balance between owner and renter-occupied housing stock in Cutler-Orosi.
9. Sites for multi-family development shall be identified which do not overburden any one area of the community or neighborhood. Large developments should be located on collector or arterial streets.

Objective II: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Cutler-Orosi.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.

Objective III: Provide a role for mobile homes in satisfying community-housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.

Economic Base

GOAL VI: Develop a strong and diversified economy.

Objective I: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. New service commercial uses should be located away from existing or planned residential areas or mitigation measures should be incorporated into the design of the project that will eliminate any undesirable conditions.
4. The County shall encourage industrialization in Cutler-Orosi, especially industries that provide year-round employment, and which require close or near highway access.
5. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective II: Provide the services necessary to support new industrial and commercial development.

Policies:
1. Encourage the Cutler PUD and Orosi PUD to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective III: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

Policies:
1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

Environmental Quality and Public Safety

GOAL VII:
Preserve and enhance the quality of life for present and future generation of Cutler-Orosi citizens.

Objective I: Upgrade the level of community health, sanitation and safety.

Policies:
1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas, which will upgrade the community image and improve safety.
2. The County shall, within its authority, protect the public from danger to life and property caused by fire.
3. The County shall, within its authority, protect the public against crime against people and property.

Objective II: Provide sufficient open space for community recreation needs.

Policies:
1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective III: Protect Agricultural Lands:

1. Land within the respective Urban Development Boundary of Cutler-Orosi,

which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

2. The following criteria shall be used to determine when conversion to urban use is appropriate:
   a. The property is not subject to an agricultural preserve contract;
   b. Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
   c. At least 30% the property boundaries are contiguous on at least one side to existing urban development.
3. Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.
4. Agricultural uses outside the UDB shall be protected from conflicting urban uses by aligning the UDB along streets, canals or other man-made or natural features in order to buffer the two uses to the extent possible.
5. The County (and developers) shall carefully coordinate the extension of public water and sewer services in the planning area with Cutler Public Utility District (PUD) and Orosi PUD, to promote logical and orderly development patterns.
6. New agricultural preserves and contracts shall not be approved for properties within Cutler PUD and Orosi PUD.
7. Commercial and residential uses will be required to connect to public services provided by the Cutler PUD and Orosi PUD.
8. Large lot agricultural zoning such as AE-20 shall be applied as a holding zone to properties, which do not meet the criteria set forth in policy Agriculture Policy 2 above.
9. Promote growth along SR 63 for industrial and commercial uses to preserve adjacent agricultural lands consistent with the Corridor Framework Policies in the Tulare County General Plan.

Objective IV: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Cutler-Orosi.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Cutler-Orosi.
4. Carefully evaluate proposed heavy industrial uses to be located east of SR 63 to assure that such uses will not have an adverse impact on the community.

General Plan Framework

Value Statements

1. The beauty of the County and the health and safety of its residents will be protected and enhanced.
2. The County will create and facilitate opportunities to improve the lives of all County residents.
3. The County will protect its agricultural economy while diversifying employment opportunities.
4. Every community will have the opportunity to prosper from economic growth.
5. Growth will pay its own way providing sustainable, high quality infrastructure and services.

Framework Concepts

- ***Concept 1: Agriculture***
  One of the most identified assets in Tulare County is the rich agricultural land on the valley floor and in the foothills. The General Plan identifies agriculture not only as an economic asset to the County but also as a cultural, scenic, and environmental element to be protected and to insure that the utilization of these resources may continue to economically succeed.
- ***Concept 2: Land Use***
  Tulare County has a number of unincorporated communities that will grow and develop and natural resource lands (agriculture, mineral extraction, and open space) that will be preserved and permitted to expand. It is anticipated that much of the projected population growth will require a range of housing choices, neighborhood support services, and employment producing uses that are centrally located in cities and unincorporated communities. The County will also utilize its goals and policies to guide the conversion of agricultural and natural resource lands to urban uses.
- ***Concept 3: Scenic Landscapes***
  The scenic landscapes in Tulare County will continue to be one of its most visible assets. The Tulare County General Plan emphasizes the enhancement and preservation of these resources as critical to the future of the County. The County will continue to assess the recreational, tourism, quality of life, and economic benefits that scenic landscapes provide and implement programs that preserve and use this resource to the fullest extent.
- ***Concept 4: Natural and Cultural Resources***
  As Tulare County develops its unincorporated communities, the County will ensure that development occurs in a manner that limits impacts to natural and cultural resources through the implementation of its Goals and Policies and through proper site planning and design techniques.

Guiding Principles

- ***Principle 1: Opportunities***
  Provide opportunities for small unincorporated communities to grow or improve quality of life and their economic viability.
- ***Principle 2: Reinvestment***
  Promote reinvestment in existing unincorporated communities in a way that enhances the quality of life and their economic viability in these locations.
- ***Principle 3: Protection of Resources***
  Protect the County's important agricultural resources and scenic natural lands from urban encroachment through the implementation of Goals and Policies of the General Plan.
- ***Principle 4: Limit Rural Residential Development***
  Strictly limit rural residential development potential in important agricultural areas outside of unincorporated communities, hamlets, and city UDBs, UDBs (i.e., avoid rural residential sprawl).
- ***Principle 5: Agricultural Facilities***
  Allow existing and outdated agricultural facilities in rural areas to be retrofitted and used for new agricultural related businesses (including non-agricultural uses) if they provide employment.
- ***Principle 6: Planning Coordination and Cooperation***
  Enhance planning coordination and cooperation with the agencies and organizations with land management responsibilities in and adjacent to Tulare County.

Preliminary Assessment of Land Needs

Market Analysis

Renter Affordability

According to the US Census Bureau, the 2013-2017 American Community Survey **(see Table 34)** data indicated that in 2017 the cost of rent in Cutler-Orosi was lower than in Tulare County and the State of California, but that rent constituted a larger percentage of household income. The median rent was $755 in Cutler and $873 in Orosi, whereas the median rent was $877 in Tulare County and $1,358 in the State of California, respectively. In Cutler, the percentage of households paying 35% or more of income on housing was 63.9% and in Orosi, it was 68.2%, while the percentage of households paying 35% or more of income on housing was 47.2% in Tulare County and 47.0% in the State of California.

Table 34 - 2013-2017 American Survey: Renter Cost

Geography	Median Rent	Gross Rent as a % of Household Income					
		Less than 15.0 %	15.0% to 19.9%	20.0% to 24.9%	25.0% to 29.9%	30.0% to 34.9%	35.0% or more
California	$1,358	9.6%	10.9%	12.1%	11.5%	9.6%	46.4%
Tulare County	$877	10.6%	10.5%	12.1%	10.7%	8.7%	47.2%
Cutler CDP	$755	12.0%	13.1%	5.7%	1.1%	4.0%	63.9%
Orosi CDP	$873	11.5%	14.1%	6.2%	0.0%	0.0%	68.2%

Source: 2013-2017 American Community Survey 5-Year Estimates

Owner Affordability

According to the US Census Bureau, the 2013-2017 American Community Survey data indicated that in 2017 the cost of a mortgage in Cutler-Orosi was lower in Tulare County and the State of California. The mortgage constituted a smaller percentage of household income compared to Tulare County and the State of California. The median owner cost (with mortgage) was $927.00 in Cutler and $1,132.00 in Orosi, whereas the median owner cost was $1,345 in Tulare County and $2,206 in the State of California, respectively. In Cutler, the percentage of households paying 35% or more of income on housing was 29.9% and in Orosi 53.0%. The percentage of households paying 35% or more of income on housing was 31.7% in Tulare County and 30.7% in the State of California **(see Table 35).**

Table 35 - 2013-2017 American Community Survey: Owner Cost						
Geography	Median Owner Cost (with mortgage)	Mortgage as a % of Household Income				
		Less than 20.0%	20.0% to 24.9%	25.0% to 29.9%	30.0% to 34.9%	35.0% or more
California	$2,206	32.5%	19.6%	12.5%	9.0%	30.7%
Tulare County	$1,345	36.2%	14.5%	10.4%	7.1%	31.7%
Cutler CDP	$927	62.9%	0.0%	7.0%	14.8%	29.9%
Orosi CDP	$1,132	52.4%	14.5%	0.0%	0.0%	53.0%
Source: 2013-2017 American Community Survey 5-Year Estimates						

Existing Businesses

According to the US Business Directory, there were 90 existing businesses in the Cutler area in 2018, http://us-business.info/directory/cutler-ca/ **(see Table 36)** and 186 existing businesses in the Orosi area http://us-business.info/directory/orosi-ca/ **(see Table 37).**

Table 36: Existing Businesses in Cutler

99 Cents Plus Store	Cutler Orosi Waste Water	Ledbetter Park	St. Mary's Religious Education CCD
Abby's Video & Deli	E R Holden & Sons	Lovell Continuation School	T Rod INC
Aden Market	El Progreso	M I Salon	Tulare County Child Care
Arnold Trucking	El Ranchero Perez	Magnolia Market	Tulare County Fire Department
Avila's Tires	Family Healthcare Network	Martinez Accounting	Tulare County Sheriff's Office
Awasthi, Sarvamitra, MD	First Southern Baptist Church	Mini Fashions Outlet	Twin Girls Farms
Baba, Steven, DDS	Fresco Market Place	Monterey Water Company	US Post Office
Barsamian Farms	George Brothers Ranch Shop	Mulholland Citrus	Valero
Big Discount Store	Golden Sierra Cold Storage	Mundi Diesal	Warren & Baerg Manufacturing INC
Birreria	Golden Star Citrus	Nacho Auto Repair	Wawona Garage & Machine Shop
Birrieria Apatzingan	Golden State Vintners	O&R Trucking	Wawona Packing
C & E Ananian	Green Luck Landscape & Maintenance	Pacific Trellis Fruit	Western Farm Service INC
Carniceria Moyahua	Guerrero Martinez Tax Service	Paramount Citrus Association	Wileman Brothers & Elliott INC
Christian Worship Center	HB Gills INC	Pena's Auto Sales	
Comp Tek	J & L Tree Service	Pena's Recycling	

		Center	
Crop Production Service	J V Farm Labor Service	Phil's Lock & Key	
Cutler Liquor	Jaime Lisa A (MA)	Panaderia Esesarte	
Cutler Child Care Center	Junior's #2 Fashion	Rene G Ortega Concrete	
Cutler Market		Rosewood Villas	
Cutler Bakery	Karon's	Rubalcaba Grocery	
Cutler Orosi Senior Center	Kathy Ruvalcaba	Ruvalcaba Meat Market	
Cutler School	Kathy's Style Shoppe	Saint Mary's Parish Hall	
Cutler Supermarket	Kaweah Container INC	Shell	
Cutler Orosi Unified School District	LA Esperanza	St. Mary's Catholic Church	
Cutler PUD	LA Fiesta Food		

Table 37: Existing Businesses in Orosi

1st Baptist Church	Cevallo's Bakery	Gil's Auto Wrecking	McDonalds
99 Cents & More	Citricove Orchards	Golden Valley School	McPhaill Citrus Ranch
A & Engraving	Cutler Orosi Unified SCHL District	Golden Villa Mini-Mart	Mickie's Hair Salon
A S Oriental Seafood Market	CSET	Glenn's Orosi Mini Storage	Monterey Water Company
Abe-EL Produce	Cutler Orosi Special Education	Golden West Labor	Midway Auto Parts
Abel Sahagun Insurance	Cutler Rexall Pharmacy	GSF Nut Company	Mountain View AG Services INC
Academy West Insurance Services INC	Cuts N Styles	Gonzalez Serge	N & R Transportation
Agrape Christian Superstore	Davila Memorials & Granite Works	Good Choice Insurance Service	Nakatsuchi Hirofumi DDS
Allied Insurance	Diana's Daycare	Green Valley Medical Clinic	Navarro, Isaac R
Aleman 99 Cents Y Mas	Dollar Store & More	Gonzalez, Anna M. MD	Nava Smog
Aguilar, Valerie (MA)	Doneright Electricians	Green Medical Caregiver	Novedandes Nana
Alejandra Flowers	East Orosi Community Service District	Guevara's PM Roofing Co.	Orlopp Turkey Breeding Farms
Aldaz Pedro	East Orosi Market	H & R Block	Orosi Auto Repair & Tire Service
Alta Vista Apartments	Ecology Sound Farms	Happy Apple Co	Orosi Barber Shop
Amigos Transmission & Auto Repair	El Cesar	Hong Kong Chop Suey	Orosi Branch Library
Aleman Clothing	El Lago	Iglesia Ni Christo-Church	Orosi Center
Angie's Beauty Salon Alta Vista Apartments	El Mexicano-Soccer Accessories	Ikard & Ikard	Orosi Food Mart
Arturo's Portable Toilet Service	El Monte School Office	JL Recycle Center	Orosi Family Medical Care
Augies Farm Labor Service	El Pio Pio Restaurant	K & K Market	Orosi Flowers N More

Auto Title Experts	El Progreso Tortilleria	Kaleka, Virender S. MD	Orosi Donuts
Bank of the West	El Rincon Market	Kaspian's Liquor	Orosi ER Dental Center
Bay Area P O S	Faith Bible Church of God	Kay Bee Farm	Orosi MH Estates
Bayardo, Carlos, MD	Family Dollar	Kwick Korner	Orosi Mart & Deli
Beneje's Drive In	Family Health Care Network	La Bonita Supermercado	Orosi Medical Supply
Big O Gas & Deli	Fancher Creek Packing	La Mexicana	Orosi High School
BHK Nut Corp	Farm Labor Contractor	Lara's Meat Market	Orosi Urgent Care CTR Medical
Butler Ranches	First Baptist Church	Lawson Packing	Orosi Swap Meet
Boss Concrete	Freedom Transport	Leon Service Station	Orosi Public Utility District
Calvary Temple	Gabriel's Auto Mechanic	Little Caesars Pizza	Orosi Mini Mart
Camilo Galacgac	Galindo Electric	Lopez, Aileen J, MD	Orosi Pizza House
Cely's Beauty	Garcia Window Screens	M & G Farms	Orosi Wireless
Centro Cristiano Vida Abundante	Giannandrea Rita	Madrid George	P Marquez Enterprises
Palm Elementary School	Papich Construction Co.	Paramount Citrus	Pop's Propane SVC
R-N Market	RBM Industries	Rising C Ranches	Rose City Transportation
S & J Ranch INC	Saint Germain	Sal's Tobacco & More	Salon Expressions
Sanchez, Alma R. DDS	Sequoia Presbyterian Church	Sand Creek Apartments	Secera Vending
Sequoia View Apartments	Seventh-Day Adventist Church	Smart Money	Singh, Gurteg, DDS
Sierra Pacific Materials	St Germain Botanicals	Suarez, Boris M	Super 7
Subway	Taqueria Santa Fe	Templo De Jesus	Templo La Paz Mennonite Brethren
The Car Wash	The Saul-on	Tulare County Child Care	Trevino Construction
Trevino & Son Farm Labor	Town & Country Christian School	Tulare 2010 Community LP	U-Haul
United Pentecostal Church	US Post Office	VM Logistics	Valero Cristina MD
Valley Smog	Velozz Communication	Video Castle	Villa De Guadalupe Apts
Vidrio, Maria	W & E Electric Service	Wawona Garage & Machine Shop	YR Pizza Planet
Z's Communication			

Market Feasibility

The community of Cutler **(see Table 37)** already has seven (7) grocery stores including Aden Market, Cutler Market, Cutler Supermarket, Fresco Market Place, La Fiesta Food, Magnolia Market, and Rubalcaba Grocery and two (2) convenience stores: Cutler Liquor and Shell Mini Mart. Cutler also has five (5) restaurants including: Abby's Video & Deli, Birrieria Apatzingan, El Progreso, El Ranchero Perez, La Esperanza. There is also Avila's Tires, Pena's Auto Sales, Western Farm Service, 99 Cent Plus Store, and Big Discount Store located in Cutler. Without a substantial increase in households, the discretionary income of Cutler residents would not support a new large commercial area.

The community of Orosi **(see Table 37)** already has 10 grocery stores including A S Oriental Seafood Market, East Orosi Market, El Progreso Tortilleria, El Rincon Market, K & K Market, Orosi Food Mart, R-N Market, Saint Germain, and Super 7, six (6) convenience stores: Kaspian's Liquor, Kwick Korner, Golden Villa Mini-Mart, Orosi Mini Mart, Orosi Mart & Deli, Orosi Food Mart, and has 12 restaurants including: Benje's Drive In, El Cesar, El Lago, El Pio Pio Restaurant, Hong Kong Chop Suey, La Mexicana, Little Caesars Pizza, McDonalds, Orosi Pizza House, Subway, Taqueria Santa Fe, and YR Pizza Planet. There is also Alejandra Flowers, Aleman Clothing, Family Dollar, Orosi Auto Repair & Tire Service, and Bank of the West, located in Orosi. Without a substantial increase in households, the discretionary income of Orosi residents would not support a new large commercial area.

Assessment of Land Needs

Population Growth Forecast

The projected Year 2030 combined population of Cutler-Orosi is 16,099 and the projected increase in combined population from 2017 to 2030 is 2,489persons. **(see Table 38, 39, and 40)**.

Population Growth Forecast

Table 38 - Cutler-Orosi Population Projections

Growth Rate	2017	2020	2030
Cutler	**5,850**	**6,081**	**6,920**
Orosi	**7,760**	**8,067**	**9,179**

Demand Forecast

With the existing 2,441.9 acre Cutler-Orosi Urban Development Boundary, approximately 1,246 acres are urbanized. By dividing the estimated 2030 population of 16,099 by 1,246 urbanized acres, a ratio of 12.92 persons per urbanized acres is calculated. The forecasted increase in population from

2017 to 2030 is 2,489 persons. Projecting this ratio into the future (2,489 persons divided by 12.92) suggests that an additional 198 acres will be needed by the Year 2030.

Based on the data and analysis contained above and forecasted population and housing estimates below, the following table includes the Year 2030 square footage and residential unit demand forecast for the Cutler-Orosi planning area.

Population and Housing Units

Table 39 Population Projection (Cutler)		
Year	**Population**	**Growth (%)**
2017	5,850	0.013
2018	5,926	0.013
2019	6,003	0.013
2020	6,081	0.013
2021	6,160	0.013
2022	6,240	0.013
2023	6,321	0.013
2024	6,404	0.013
2025	6,487	0.013
2026	6,571	0.013
2027	6,657	0.013
2028	6,743	0.013
2029	6,831	0.013
2030	6,920	0.013

Table 40 Population Projection (Orosi)		
Year	**Population**	**Growth (%)**
2017	7,760	0.013
2018	7,861	0.013
2019	7,963	0.013
2020	8,067	0.013
2021	8,171	0.013
2022	8,278	0.013
2023	8,385	0.013
2024	8,494	0.013
2025	8,605	0.013
2026	8,717	0.013
2027	8,830	0.013
2028	8,945	0.013
2029	9,061	0.013
2030	9,179	0.013

Population and Housing Units

The Year 2017 baseline population was determined by projecting the 2017 American Community Survey data population by an annual growth rate of 1.3% annually. The Survey indicated that in Year 2017 the community of Cutler had 1,293 dwelling units (including vacant dwellings) with a population of 5,850. At an annual growth rate of 1.3%, the projected housing units are 1,344 and 1,529 in Years 2020 and 2030, respectively, and projected population is 6,081 and 6,920 in Years 2020 and 2030, respectively. In the community of Orosi had 2,076 dwelling units (including vacant dwellings) with a population of 7,760. At an annual growth rate of 1.3%, the projected housing units are 2,158 and 2,456 in Years 2020 and 2030, respectively, and projected population is 8,067 and 9,179 in Years 2020 and 2030, respectively.

Table 41 Housing Projection (Cutler)		
Year	**Housing**	**Growth (%)**
2017	1,293	0.013
2018	1,310	0.013
2019	1,327	0.013
2020	1,344	0.013
2021	1,362	0.013
2022	1,379	0.013
2023	1,397	0.013
2024	1,415	0.013
2025	1,434	0.013
2026	1,452	0.013
2027	1,471	0.013
2028	1,490	0.013
2029	1,510	0.013
2030	1,529	0.013

Table 42 Housing Projection (Orosi)		
Year	**Housing**	**Growth (%)**
2017	2,076	0.013
2018	2,103	0.013
2019	2,130	0.013
2020	2,158	0.013
2021	2,186	0.013
2022	2,214	0.013
2023	2,243	0.013
2024	2,272	0.013
2025	2,302	0.013
2026	2,332	0.013
2027	2,362	0.013
2028	2,393	0.013
2029	2,424	0.013
2030	2,456	0.013

Opportunities & Constraints

Opportunities

Complete Streets

The Complete Streets Act of 2007 (Assembly Bill 1358) requires counties when updating General Plans, to identify how the jurisdiction will provide for the routine accommodation of all users of the roadway including motorists, pedestrians, bicyclists, individuals with disabilities, seniors, and users of public transportation.

Affordable Housing

The community of Cutler has a median income of $31,939 and Orosi medium income is $35,798, which is less than 80% of the State median income of $67,169. Approximately 55% of the households in Cutler and 47.0% of the households in Orosi spend 52.69% or more of their income on housing. As such, there is a high need for affordable housing.

Moreover, 63.9% of Cutler's renters and 68.2% of Orosi's renters spent over 35% or more of their income on rent. Average household size of renters was 5.34 for Cutler and 3.82 for Orosi's. In addition, 29.9% of Cutler's owner-occupied units and was 53.0% for Orosi spent over 35% or more of their income on mortgages. Average household size of owner-occupied units was 3.68 for Cutler and was 3.91 for Orosi.

It is very likely that many children in Cutler-Orosi share bedrooms. As there are no bus lines in Cutler-Orosi, reduced parking is not a realistic strategy to reduce development costs. Affordable housing will require more land in Cutler-Orosi than would typically be required in an area where public transit is available. In terms of siting, medium to high-density housing should be located along collector streets and/or arterials.

Urban Development Boundary

Although State planning law does not define specific requirements for establishing planning area boundaries, it is generally agreed that the planning boundaries should include the territory within a community's probable ultimate physical boundaries and service area. Urban Development Boundaries provide a planning framework that promotes the viability of communities, hamlets, and cities while protecting the agricultural, open space, scenic, cultural, historic, and natural resource heritage of the County. In the past, the County used three key planning tools to guide urban development in all unincorporated areas of the County. The first was the Urban Boundaries Element; the second are the Area Plans; the third are the General Plans for identified incorporated cities and Community Plans for unincorporated communities. In 1974, Tulare County added an Urban Boundaries Element to its General Plan. The element required the designation of an urban boundary for every "viable" unincorporated community in the county. The Urban Boundaries Element also established Urban Improvement Areas (20-year planning boundaries) for certain communities. The 1974 Urban Boundaries Element designated both an Urban Area Boundary and an Urban Improvement Area for Cutler-Orosi.

In 1983, the Urban Boundaries Element was amended to create Urban Development Boundaries (UDBs, which are also to function as 20-year planning boundaries) and to Change the function of the Urban Area Boundary to simply a "comment line" around incorporated cities. Under the 1983 amendment, Urban Area Boundaries are no longer established around unincorporated communities - and Urban Improvement Areas are to be phased out over time (replaced with UDBs) as each community's boundaries are updated.

For unincorporated communities as per the Planning Framework Element of the General Plan, the UDB is a County adopted line dividing land to be developed from land to be protected for agricultural, natural, open space, or rural uses. It serves as the official planning area for communities over a 20-year period. Land within an unincorporated UDB is assumed appropriate for development and is not subject to the Rural Valley Lands Plan or Foothill Growth Management Plan.

Constraints

There are several constraints or restrictions which will impact the nature and location of future development within the community. In particular, these constraints pertain to existing problems of

public health and safety; acceptable noise levels impacts of deteriorating housing, lack of a full range of community services. Following are constraints that were recognized in the preparation of this plan.

Noise Contours

There are a variety of sources that produce noise in the Cutler-Orosi Plan Area and include traffic, railroad operations, airport operations, and agricultural operations. Traffic noise is the most dominant source of ambient noise in the County, according to the Tulare County General Plan EIR **(see Table 43)**. SR 63 and Avenue 416 run through the Cutler-Orosi Plan Area and would be the largest source of traffic noise in the area due to the high volumes of traffic. Noise from SR 63 adversely impacts an area through central Cutler-Orosi making properties in close proximity to the highway less desirable for new housing construction.

Table 43 - Noise Levels

Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
SR 63 Ave 400 to Emerald Dr.	8,300	34	74	159	343
SR 63 Emerald Dr. to Ave 416	13,000	43	92	198	426
SR 63 Ave 416 to Ave 422	7,200	29	62	133	287
Ave 416 Road 120 to SR 63	8,000	37	79	171	368
Ave 416 SR 63 to Boyd Dr	850	8	18	38	83

Source: 2010 General Plan Background Report

Sand Creek

Sand Creek, an ephemeral stream, which carries local storm water runoff southerly to Cottonwood Creek. Cottonwood Creek flows into Cross Creek, and flows to the Tule River. Sand Creek lies between the communities of Cutler-Orosi. Periodic flooding of Sand Creek has previously precluded the quarter to half-mile separating the communities from developing to urban uses prior to existing flood control improvements. Sand Creek usually is dry during the summer.

Sand Creek is located within the FEMA Special Flood Hazard Areas and that the inundation zone must be included on hazard disclosures pertaining to real estate contracts.

Sand Creek lies within the Sand Creek Dam inundation zone due to dam failure. Sand Creek is located in Tulare County, California and displayed on the Monson USGS quad topo map. Sand Creek Dam is used for drinking water, fish and wildlife protection and flood control, among other things. Construction of the dam was completed in 1980. At normal levels it has a surface area of 55 acres. The dam is owned by Tulare County Resource Management Agency. Sand Creek is rock fill and its height is 60 feet with a length of 933 feet. Normal storage is 1,050 acre, and it drains an area of 26.3 square miles.

Tulare County Economic Development Strategy

Tulare County's current Economic Development Strategy focuses on tourism, the agricultural industry and pursuing grants.

Agriculture

Tulare County has a booth at the World Agricultural Exposition (Ag Expo) every year. The Economic Development Office uses the event to promote Tulare County tourism and business opportunities. Partnering with the County's Purchasing Department the Ag Expo provides an excellent method to market directly to the global agriculture related businesses attending the Ag Expo and sell surplus county equipment.

Grants

- State Water Resources Control Board – State Revolving Fund: $500,000 for Traver Community Wastewater System Improvements Planning Study and Design, once plans are near complete we will apply for construction funding between $8 and 10 million.
- State Water Resources Control Board: have applied and received $5 million in construction funding for Phase 1 of the Yettem Seville Water System.
- County Measure R funding $575k for sidewalks and ADA improvements in Goshen.
- ATP Active Transportation Program- Statewide competitive $2 million funding is anticipated for Safe Routes to School and ADA improvements in and around three (3) Goshen.
- Low Carbon Transit Program funding $147,474.00.
- Prop 84 – Goshen Neighborhood Improvement Program funding $2,153,900.00.
- Yettem & Seville Project Phase 1 funding $4,300,200.00
- Navigation Aids at Sequoia Field Airport funding $340,200.00
- Transit Operations & Maintenance Facility (TOMF) funding $10,800,000.00

Solar Projects

In Tulare County, there have been 13 Utility Scale Solar Projects that have a capacity of 198 MW. There are nine (9) projects in /under Construction with a capacity of 260 MW. In terms of total solar projects (including Utility Scale, Solar on Dairies, Commercial Solar, and Residential Solar) there have been 1570 projects built that accounts for a capacity of 227.5 MW. The Corridor offers realistic potential to locate solar projects closer to the urban areas and outside of the direct line-of-sight viewshed of the Highway 99 Corridor. **Figure 24** provides a summary of solar development in Tulare County.

Figure 25 - Solar Development in Tulare County

Utility Scale Solar Projects		
Phase of Construction	**No. of Permits**	**Total Capacity (MW)**
Under Review	2	740
Pre-construction/Under Construction	2	90
Constructed	24	388
Total	**28**	**1218**
Solar Projects on Dairies		
Phase of Construction	**No. of Permits**	**Total Capacity (MW)**
Under Review	2	2.16
Pre-construction/Under Construction	9	8.30
Constructed	61	47.63
Total	**72**	**58.09**
Other Commercial Solar Projects		
Phase of Construction	**No. of Permits**	**Total Capacity (MW)**
Under Review	1	0.06
Pre-construction/Under Construction	16	6.46
Constructed	265	73.56
Total	**282**	**80.08**
Anaerobic Digesters		
Phase of Construction	**No. of Permits**	**Total Capacity (MW)**
Under Review	2	
Pre-construction/Under Construction	52	
Constructed	12	
Total	**66**	
Residential Solar Projects (based on 7 kw/sfd average)		
Phase of Construction	**No. of Permits**	**Total Capacity (MW)**
Under Review	16	0.26
Pre-construction/Under Construction	147	2.24
Constructed	3174	24.14
Total	**3337**	**26.64**
Solar Project Totals		
Phase of Construction	**No. of Permits**	**Total Capacity (MW)**
Under Review	23	742.48
Pre-construction/Under Construction	226	124.00
Constructed	3536	533.33
Total	**3785**	**1399.81**

SWOT ANALYSIS	
Strengths	**Weaknesses**
▪ Tulare County has a strong agricultural economy. ▪ There is an elementary school ▪ There is a High School in Cutler-Orosi ▪ There a number of highway commercial businesses. ▪ Cutler-Orosi is located on SR 63 and Avenue 416. ▪ There is a variety of commercial and industrial uses. ▪ The cost of living is low in Tulare County. ▪ Water supply has reserve of 70,000 gpd. ▪ The sewer system is operating at approximately 65% of its capacity. ▪ Recreational sports complex and a community park	▪ Cutler ($31,939) and Orosi ($35,798) are considered severely disadvantaged communities. ▪ Sidewalks are needed. ▪ The Storm drain system is inadequate improvements are required.
Opportunities	**Threats**
▪ Vacant land is available. ▪ Complete Streets ▪ Cutler-Orosi is located next to State Route 63 where the average annual daily traffic (AADT) along SR 63 in the study area was approximately 12,100 south of Avenue 416 and 7,300 south of Avenue 400 in 2017[46]) ▪ A Mixed Use Overlay Zone would allow for additional development. ▪ There is growth in distribution jobs in California. ▪ Tulare County Area Transit Agency (TCaT) has bus routes that link Cutler-Orosi with Visalia and Dinuba.	▪ There has been a gradual decline in the number businesses and jobs. ▪ Cutler-Orosi competes with Visalia, Tulare, Dinuba, and other parts of the County. Cutler-Orosi also competes with the City of Kingsburg and Selma in Fresno County. ▪ Tulare County has a limited budget for road improvements. Budgeting for roadway maintenance could be reduced depending upon the financial health of Public Works branch. ▪ Small businesses are sensitive to changes in tax rates, minimum wage, requirement of health care provisions and other nationally established policies and requirements. ▪ Farms are getting larger and relying more on automation. This can reduce jobs and incomes. ▪ National and Statewide economic trends can impact Cutler-Orosi's economy.

[46] Cutler-Orosi Community Plan Update Transportation Impacts Study (TIS). Page 11. Prepared by VRPA Technologies, Inc. and included in Appendix "E" of the Update's Draft EIR.

Reducing Barriers to Economic Development

California Competes

"The California Competes Tax Credit is an income tax credit available to businesses that want to locate in California or stay and grow in California. Tax credit agreements will be negotiated by GO-Biz and approved by a newly created "California Competes Tax Credit Committee," consisting of the State Treasurer, the Director of the Department of Finance, the Director of GO-Biz, one appointee from the Senate, and one appointee from the Assembly."[47]

"The California Competes Tax Credit only applies to state income tax owed to the Franchise Tax Board. The credit is non-refundable, and in the case where the credit allowed exceeds tax owed, the excess may be carried over to reduce the tax in the following year, and the succeeding five years if necessary, until exhausted."[48]

"The value of the credit will be based on the following factors:

- The number of jobs the business will create or retain in this state.
- The compensation paid or proposed to be paid by the business to its employees, including wages and fringe benefits.
- The amount of investment in this state by the business.
- The extent of unemployment or poverty where the business is located.
- The incentives available to the business in this state, including incentives from the state, local government, and other entities.
- The incentives available to the business in other states.
- The duration of the business' proposed project and the duration the business commits to remain in this state.
- The overall economic impact in this state of the business.
- The strategic importance of the business to the state, region, or locality.
- The opportunity for future growth and expansion in this state by the business.
- The extent to which the anticipated benefit to the state exceeds the projected benefit to the business from the tax credit."[49]

"The tentative amount of credits that GO-Biz can allocate is as follows:

- $30 million in fiscal year 2013/14.
- $150 million in fiscal year 2014/15.
- $200 million in each fiscal year 2015/16 through 2017-18."[50]

"…25 percent of the total credits available each year is expressly reserved for small businesses (gross receipts of less than $2 million during the previous taxable year)."[51]

[47] California Competes Tax Credit FAQ
[48] Ibid.
[49] Ibid.
[50] Ibid.
[51] Ibid.

Tulare County Strategy

In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

Infrastructure

The water system is at capacity and the wastewater system is near limiting capacity. In order for more development to occur, service levels for water and wastewater need to be expanded. Grant funding is needed to increase service levels."

Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

Education

Tulare County has five satellite campuses for four-year universities: California State University-Fresno, University of California-Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Table 44 - Estimated Education Attainment of Cutler-Orosi Population

	Percent Less than 9th grade	Percent 9th to 12th grade, no diploma	Percent High School graduate (includes equivalency)	Percent bachelor's degree or higher
California	9.9%	8.0%	20.6%	20.1%
Tulare County	20.5%	11.4%	25.4%	9.4%
Cutler	43.9%	17.1%	24.9%	0.2%
Orosi	34.1%	14.7%	21.8%	6.2%

Source: 2013-2017 American Community Survey, 5-year

Based on the 2013-2017 American Community Survey **(see Table 44)**, the educational barrier in Cutler-Orosi begins in grade school. Of the adults age 25 and older, Cutler 43.9% and Orosi 34.1% had an educational level of less than 9th grade. This limits the types of jobs that these adults are qualified for. Improving educational attainment needs to begin in elementary school. As part of the safe routes to schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This should make it easier for children to get to school.

Home Occupation Regulations

Encouraging small-scale entrepreneurship is one way to foster economic development. Although some home occupations are allowed by right, these home occupations have regulations which limit the type and scope of allowed businesses. By reducing some of these regulations, we can eliminate a few barriers to small-scale entrepreneurship. The following are proposed changes to the Zoning Ordinance.

Eliminate:
Section 15.A.7.a.6 "No one other than residents of the dwelling shall be employed in the conduct of a home occupation."
Section 15.A.7.b.6 "No one other than residents of the dwelling shall be employed in the conduct of a home occupation."
Section 15.A.7.c.5 "A rural home occupation shall be limited in employment to residents of the property and not more than one (1) additional person."

Replace with:
"Employment in a rural occupation shall be limited to residents of the property and not more than three (3) additional non-resident persons."

Add:
All home occupations located within the Cutler-Orosi UDB will not require a use permit.

Health Care

Health care is important for economic development, as businesses need healthy employees. Cutler Orosi Medical Clinic in Cutler, Adventist Health Medical Office in Orosi, and Family HealthCare Network provides healthcare serves for the Cutler-Orosi area. Other medical offices are located in the communities of Visalia, Kingsburg, and Dinuba.

Marketing Strategy

Place

In terms of real estate, location is a major factor (influence) in development and expansion of businesses. There are locational advantages in Cutler-Orosi, access to SR 63 and Avenue 416. These locational advantages are discussed below.

- *Access to State Route (SR) 63:* There is freeway entry and exit points, which provides convenient access and/or egress to SR 63.
- *General Commercial.* One of the goals of this Community Plan is to strengthen the current businesses located in Cutler-Orosi by taking advantage of existing industrial and commercial uses. With the establishment of industrial clusters, existing businesses can attract other type of similar business. Along SR 63 and Avenue 416 are commercial clusters. This cluster includes grocery stores, meat market, bakery, discount stores, fashion shops, beauty solons, and auto maintenance.
- *Quality of Life:* It is possible to increase the desirability of visiting and living in Cutler-Orosi. With physical improvements, the quality of life can be improved. There are a number of potential projects that can improve the quality of life in Cutler-Orosi. New recreational facilities and aesthetic improvements have most the visible effect on the quality of life as the perception of these improvements project change and image enhancements. Physical improvements could include the following:
  - Orosi- ADA Improvement & Blade Patching,
  - Cutler- ADA Improvement & Blade,

- George Road and 2nd Drive in Cutler. Pedestrian improvements (sidewalk and drainage improvements to be specific).

Product

The products to be marketed in Cutler-Orosi are vacant land and available buildings.

- *Vacant Land:* Staff has prepared a vacant parcel inventory, which identifies vacant parcels of various sizes, and various zoning districts **(see Figure 25 and Table 45).**

- *Available Buildings:* There are limited opportunities to use buildings for commercial or industrial uses. When individual spaces become available, it would behoove a property owner to coordinate with a real estate agent to lease out each space.

- *Inventory:* The Tulare County Economic Development Department is reaching out to local real estate brokers to develop a parcels availability database. In addition, the County will be providing links from its Economic Development Webpage to individual broker webpages, which can serve to advertise the availability, location, size, etc., of parcels.

Figure 26 - Vacant Parcels Map

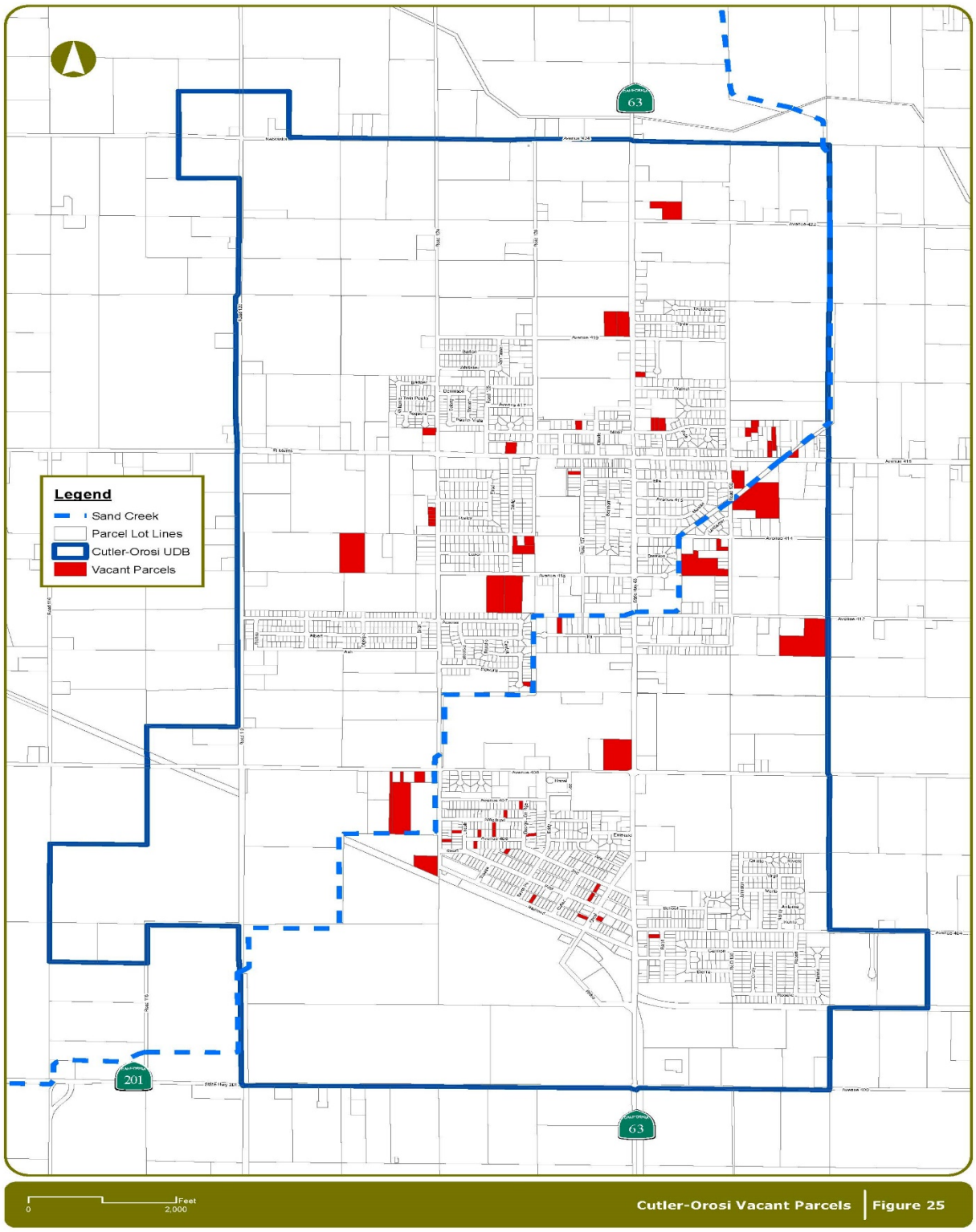


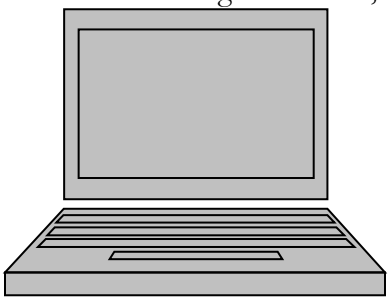
Table 45 - List of Vacant Parcels

APN #	Acres	Zone	APN #	Acres	Zone
21100011	0.52	C-2	23061004	0.27	P-O
23033007	0.64	C-2	23052006	0.31	R-1
21370003	0.15	R-1	23080048	0.16	R-1
21260055	4.64	AE-20	23090022	1.19	R-2
21303017	0.20	R-1	23061006	0.48	P-O, Z
21370004	0.15	R-1	23170033	3.48	R-1
21360014	5.14	AE-20	25150035	7.36	R-A
23010030	1.66	AE-20	25050041	2.30	R-2
21370002	0.16	R-1	32173004	0.03	R-1
21370005	0.15	R-1	32070007	0.16	R-2
21370006	0.15	R-1	32083025	0.15	R-1
21270037	0.44	R-2	32083021	0.15	R-1
23010029	1.60	AE-20	32050025	0.58	C-3
23211017	0.30	R-2	32085014	0.16	R-1
23170032	3.50	R-1	32081016	0.15	R-1
23222012	0.25	R-1	32142017	0.17	C-2
23222013	0.18	R-1	32083030	0.15	R-1
23080065	0.27	R-1	32082007	0.16	R-1
23090026	6.52	R-A	32133012	0.17	R-3
23193003	1.21	R-1	32132006	0.17	R-2
23193001	0.51	R-1	32050035	1.07	C-3
23080072	0.55	R-1	32085018	0.16	R-1
23080003	0.51	R-1	32132010	0.17	R-2
23080030	0.23	R-1	32113009	0.13	R-1
23080004	0.26	R-1	32084002	0.15	R-1
23136009	0.20	R-1	32050037	5.07	C-3
23222007	4.19	R-2	32171004	0.21	R-1
23061005	0.27	P-O	32134020	0.17	R-2
			32260002	1.92	C-3, M-1

Promotion

Promotion of Cutler-Orosi and properties in Cutler-Orosi is a continual need for effective economic development. The following tasks can be used to promote the community of Cutler-Orosi.

- *Companies to Target:* As Cutler-Orosi is a small community along the SR 63 corridor, economic development should be focused on enhancing existing assets. Based on the existing businesses, the County of Tulare should target the following types of businesses:
    1. Agricultural Food Packing
    2. Cold Storage
    3. Food Processing (Dairy-related products such as milk, ice cream, yogurt, butter, etc.)
    4. Distribution Centers



- *Online Presence:* Content is one of the most important aspects of an online presence. The amount of content provides relevancy and usefulness. The following could be done to improve the County of Tulare's online presence:
    1. Constantly improve (and keep current) the Economic Development website
    2. Additional information about Tulare County could be added to this website. Pictures from Tech Fair Photo Contest are available for use by Tulare County Departments.
    3. Write and submit articles for trade magazines and bloggers.
    4. Develop a Tulare County Economic Development Blog.
    5. In addition to social media sites Facebook and Twitter, Tulare County's Economic Development Department could utilize other Social Media sites such as Instagram, Youtube, Flicker, etc.

- *Brochures:* Brochures provide specific and relevant information. Brochures should be designed for the following.
    1. Specific Site Handouts brochures with the following information: APN, address, size, price, contact information, pictures of site, and pictures of the neighborhood, zoning designation, and a location map.
    2. A Food Production Industry Cluster brochure with the following information: a list of available properties, pictures of the area, prices of land, highlights of the Agricultural Industry in Tulare County.
    3. A Solar/Alternative Energy Industry Cluster brochure with the following information: a list of available properties, pictures of the area, prices of land, highlights of the Solar/Alternative Energy Industry in Tulare County.
    4. A brochure that lists the real estate brokers in Tulare County. This brochure should include contact information such as name, address, phone and email.

- *Advertising:* Tulare County's Economic Development Department and the community of Cutler-Orosi can be advertised in targeted trade journals. These trade journals could include:
    1. California Leagues of Food Processors
    2. Food Manufacturing Magazine
    3. Food Processing Magazine

4. Food Business News Magazine
5. Logistics Business Magazine
6. Food and Beverage Packing Magazine
7. Packing World
8. Packing Digest

- *Events:* Conferences and other events are excellent venues for networking. Cutler-Orosi could be promoted at the following conferences.
  1. World Ag Expo
  2. Food Shows

- *Videos:* Promotional videos could be available online and displayed on conferences.
  1. Powerpoint presentation video of pictures and statistics.
  2. Videos of movies filmed in Tulare County.

Development Suitability Analysis

The purpose of a development suitability analysis is to determine the areas of the community and surrounding vicinity, which could most appropriately accommodate new growth. It is a means of identifying areas free of development constraints and areas in which improvements must be made before urban only to determine the location of the Cutler-Orosi Urban Development Boundary, but development can be allowed. This development suitability analysis will be used not also to help establish land use patterns for the community's future growth.

To determine development suitability, factors that either encourage or constrain development were selected and mapped. Each factor was assigned a suitability rating to show the level of influence it will have on potential new development Suitability ratings used in this study include:

Very High: *These areas are "infill lands", or are surrounded on three sides by existing urbanized land and are, or can be easily be, serviced with urban services.*

High: *These areas are lands free of development constraints and which are either adjacent to existing development and are, or can be, served with community sewer and water service. Wastewater service areas but which are near existing urban development.*

Moderate: *These lands are outside existing boundaries of local water and these lands also exhibit other qualities that make them suitable for future development.*

Low: *These lands should be precluded from intensive development until certain constraining conditions can be changed or corrected. Examples of such constraints include flooding and Williamson Act Lands.*

‘

From the process of mapping of the various factors potentially influencing the develop ability of the community and surrounding environs, a composite analysis of the application of these factors to each segment of the planning area is possible. The patterns, which emerged from this composite analysis, have enabled the overall developed suitability of the planning area to be mapped, in conformance with the ratings described above.

It is recommended, based on the Market Analysis, Opportunities and Constraints analysis as contained above the existing configuration of the Urban Development Boundary, as adopted is appropriate and reflective of the development suitability criteria contained above, except to include the Cutler PUD and Orosi PUD plan and surrounding areas consistent with the Sphere of Influence for the CPUD and OPUD.

Proposed Planning Areas

Policy Plan

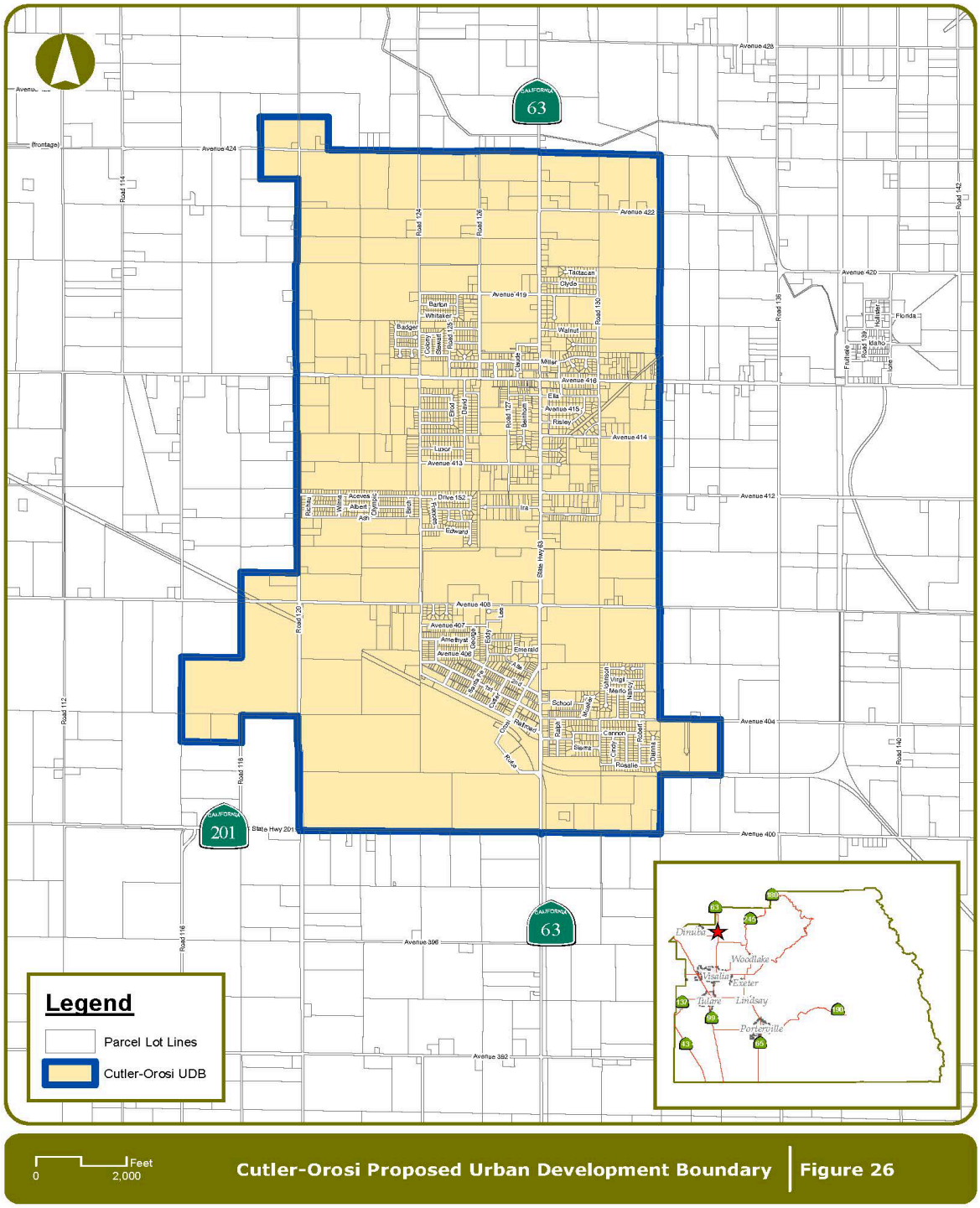
This chapter of the Cutler-Orosi Community Plan prescribes the policy framework, which will govern the development of the community over the term of the planning period (through the year 2030). It includes text, which sets out explicit policy statements about the quality, character, and manner in which development m the community will take place.

The plan, although long range in scope, is to be used on a day to day basis to guide the decisions of County staff, the Planning Commission, and the Board of Supervisors as they affect community development. Further, it will provide residents and property owners in the community with direction and guidelines regarding the evolution and growth of their town and its resources. In addition, importantly, this plan will aid other public agencies and entities, such as the school district and the water company, in their own long-range planning and capital expenditure programming. Each subsequent section of this chapter addresses a topical aspect of the community-planning environment. For each aspect, background discussion of relevant issues is included, policies are stated, and implementation programs and activities are outlined.

Proposed Urban Development Boundary

The Community Plan Update encompasses the Cutler Public Utility District and Orosi Public Utility District including the Cutler-Orosi Wastewater Treatment Plant. The Community Plan Update proposes approximately 712.1-acre expansion to the existing Urban Development Boundary (UDB), and amendments to land use and zoning designations. As such, the proposed Community Plan Update will expand the existing 2,441.9-acre UDB **(see Figure 26)** by approximately 29.2%, for a total UDB area of approximately 3,154.0-acres.

Figure 27 Proposed Urban Development Boundary



General Plan Consistency

Policy Relationship to the General Plan

The Cutler-Orosi Community Plan is a component in Part III of the Tulare County General Plan and, as such, has the same force and effect as any other adopted element of the general plan. Structurally, the Cutler-Orosi Community Plan is part of the Land Use and Circulation Element of the overall general plan. The principal emphasis of the community plan is on establishing local land use and circulation system patterns and prescribing associated standards and policies. In addition to the specific prescriptions of the community plan, the broader policies and standards of the overall Land Use and Circulation Element apply to Cutler-Orosi.

Also applicable to Cutler-Orosi, and governing all future development in the community, are the other elements (e.g. Planning Framework, Environmental Resources Management, Air Quality, Health and Safety, Transportation and Circulation, etc.) of the Tulare County General Plan. In instances where the policies and/or standards of the Cutler-Orosi Community Plan are more specific or more restrictive than those in other elements of the general plan, the community plan shall take precedence and prevail.

Proposed Land Use Plan and Zoning Districts

Land Use Plan

One of the most important purposes of the Cutler-Orosi Community Plan is to establish land use patterns and development policies and standards for the community for the planning period, through the year 2030. The general intent of the land use plan for Cutler-Orosi is to identify the most appropriate types and distribution of land uses for the community, based on environmental, circulation, infrastructure, services, opportunities and constraints, urban development boundary suitability analysis and other economic capacities and concerns discussed in the previous chapters of the plan.

The County of Tulare, through existing policies, has encouraged both incorporated and unincorporated communities to establish urban development and land use patterns, which are compact and contiguous. This policy position has reduced so-called "leap frog" development County-wide, has helped preserve agricultural lands, and has minimized land use conflicts between urban and agricultural areas. The Cutler-Orosi Community Plan 2021 Update analysis supporting the establishment of an Urban Development Boundary (UDB) for Cutler-Orosi is described, and the UDB is delineated. This boundary is sensitive to and consistent with the Countywide policy of encouraging compact and contiguous urban growth, and at the same time enables the community to accommodate any foreseen demand for economic and population expansion. **Figure 27** depicts and prescribes the proposed land use pattern development recommended for Cutler-Orosi.

As illustrated in this proposed land use diagram, future urban development of the community will be concentrated on SR 63 and Avenue 416. The land use plan for the community provides for substantial expansion of other commercial development along SR 63 and Avenue 416; provides for the development of additional commercial uses potentially serving community-oriented demand;

and enables the development of additional residential uses to meet demand generated by forecasted population growth.

Proposed Land Use Plan

As suggested above and based on the forecasted growth and the recommended Urban Development boundary, on the economic Development/Market Analysis and Opportunities and Constraints analysis, the proposed land use plan **(see Table 46 and Figure 27)**

Table 46 - Proposed Land Use Plan

Land Use	Sum Acres
General Commercial	237.2
High Density Residential	70.0
Light Industrial	236.4
Low-Medium Density Residential	817.8
Medium Density Residential	274.4
Mixed Use	7.3
Neighborhood Commercial	0.8
Public Recreation	11.9
Public/Quasi-Public	343.7
Service Commercial	108.1
Urban Reserve-Light Industrial	98.0
Urban Reserve-Medium Density Residential	566.0
Urban Reserve-Low Medium Density Residential	285.0
Urban Reserve-Mixed Use	77.1
Unclassified (Right-of-Way)	19.9
Total	3,154.0

Proposed Land Use Designations

The following land use designations along with descriptions including density and intensity are recommended for Cutler-Orosi to address land demand needs through the 2030 planning horizon year.

Mixed Use (MU)

This designation establishes areas appropriate for the planned integration of some combination of retail; office; single and multi-family residential; hotel; recreation; limited industrial; public facilities or other compatible use. Mixed Use areas allow for higher density and intensity development, redevelopment, or a broad spectrum of compatible land uses ranging from a single use on one parcel to a cluster of uses. These areas are intended to provide flexibility in design and use for contiguous parcels having multiple owners, to protect and enhance the character of the area. The consideration of development proposals in Mixed Use areas should focus on compatibility between land uses, and

the development potential of a given area compared to the existing and proposed mix of land uses and their development impacts. Density bonuses for residential units of 25 % to 35% may be granted, according to the Density Bonus Ordinance or State law, to Mixed Use areas to encourage the development of affordable housing units, compact development in the implementation of development strategies that support the use of mass transit, reduction of air impacts, and policies.

Maximum Density: 1-30.0 Dwelling Units/Acre

Maximum Intensity: 0.5 FAR

Urban Reserve (UR)
This designation establishes a holding zone whereby properties shall remain zoned for agriculture or open space use until such a time as conversion to urban uses is deemed appropriate. The UR designation shall be appended by the intended future land use designation, for example, Urban Reserve Commercial (UR) and Urban Reserve Residential (UR). When a rezoning occurs without a General Plan amendment, the UR designation shall be removed from the parcel. This designation applies primarily within UDBs.

Minimum Parcel Size: 1 Dwelling Unit per 10 Acres

Maximum Intensity: 0.02 FAR

Low Density Residential (LDR)
This designation establishes areas for single-family residences with individual homes on lots generally ranging from 12,500 square feet to one acre. Uses typically allowed include detached single-family homes; secondary dwellings; and residential support uses such as churches, schools, and other necessary public utility and safety facilities.

This designation is typically found inside communities or on the outside edge of UDBs.

Maximum Density: 1-4 Dwelling Units/Acre

Low-Medium Density Residential (LMDR)
This designation establishes areas suitable for single-family neighborhoods at relatively low densities on lots ranging from generally 5,000 to 12,500 square feet in urbanized areas. Uses typically allowed include detached single-family homes; secondary dwellings; and residential support uses such as churches, schools, parks, medical facilities, and other necessary public utility and safety facilities. This designation is used only within UDBs.

Medium Density Residential (MDR)
This land designation establishes areas for single-family and low-density multi-family dwellings. Uses typically allowed include single-family dwellings, second units, townhomes, duplexes, triplexes, and mobile home parks. This designation is used only within UDBs.

Maximum Density: 4-14 Dwelling Units/Acre

High Density Residential (HDR)

This designation established areas for multi-family dwellings in urbanized areas. Uses typically allowed include duplexes, townhouses, and apartments located near schools, parks, and other public services. This designation is used only within UDBs. Dwelling Units are based on Gross Acreage and development shall be no less than that identified as the intensity per gross acreage High Density Residential designated lands.

Maximum Density: 14-30 Dwelling Units/Acre

Neighborhood Commercial (NC)

This designation establishes areas for small-scale, general retail, and service businesses that provide goods to the immediate surrounding area. Uses typically allowed include food and beverage retail sales; limited personal, medical, professional, and repair services; and retail sales. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

General Commercial (GC)

This designation establishes areas for small, localized retail, recreational, and service businesses that provide goods and services to the surrounding community. Uses typically allowed include eating and drinking establishments; food and beverage retail sales; limited personal, medical, professional services; repair services; and retail sales. Such facilities may range from a single use to a cluster of uses such as a shopping center. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

Service Commercial (SC)

This designation establishes areas for service commercial uses in urbanizing areas. Uses typically allowed include automotive-related or heavy equipment sales and services; building maintenance services; construction sales and services; and warehousing. This designation is found primarily within UDBs.

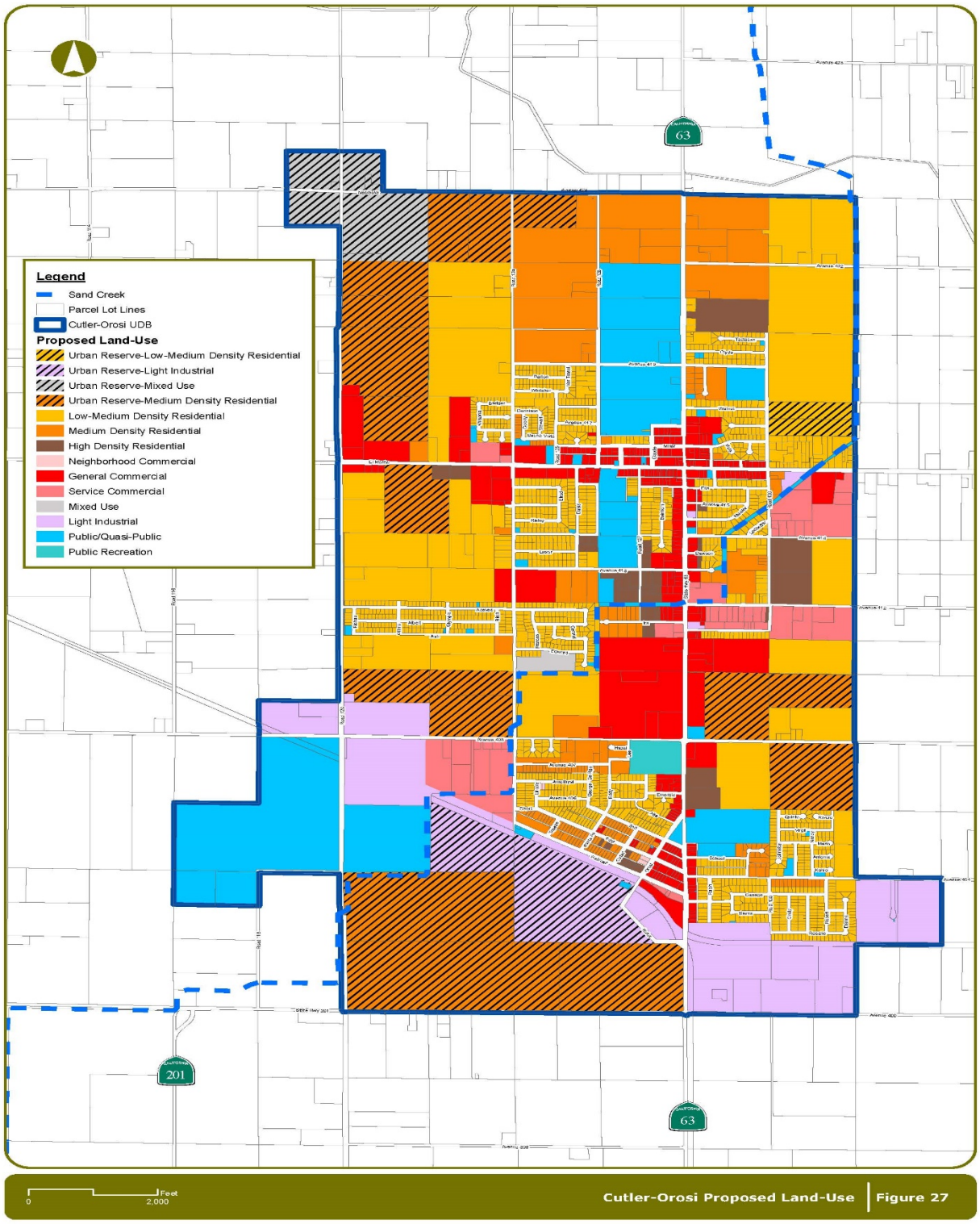
Maximum Intensity: 0.5 FAR

Light Industrial (LI)

This designation establishes areas for a range of non-intensive business park, industrial park, and storage uses that do not have detrimental noise or odor impacts on surrounding urban uses. Uses typically allowed include: warehousing; welding, and fabrication shops; manufacturing and processing; and business support uses such as retail or eating establishments that serve adjacent light industrial uses and employees. This designation is found primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

Figure 28 - Proposed Land Use Plan - Cutler-Orosi



Heavy Industrial (HI)

This designation establishes areas for the full range of industrial uses, which may cause noise or odor impacts on surrounding urban uses. Uses typically allowed include manufacturing; processing; fabrication; ethanol plants; warehouses; asphalt batch plants; mills; wood processing yards; and support uses such as retail or eating establishments that support adjacent heavy industrial uses and employees. This designation is found both primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

Proposed Zoning Districts

The proposed Zoning Districts Map **(see Figure 28)** for Cutler-Orosi is compatible to the Land Use Map outlined in the General Plan. Zoning changes that need to occur to allow the General Plan and Zoning Ordinance to be in conformity with each other **(see Table 47)**.

Table 47 - Proposed Zoning Districts

Zoning Districts	Acres
AE-10	893.8
C-1	0.3
C-1-MU	0.5
C-2-MU	253.3
C-3-MU	108.1
M-1	110.3
M-1-MU	127.5
M-2	39.9
R-1	805.9
R-1-MU	23.9
R-2	405.1
R-2-MU	15.3
R-3	30.1
R-3-MU	54.4
Unclassified (Right-of-Way)	285.6
Total	**3,154.0**

Source: Tulare County GIS

Proposed Zoning Districts

The **MU (Mixed Use) Overlay Combining** Zone allows a mix of uses that promotes flexibility in the types of entitlements that can be issued. All uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 uses are allowed.

The **R-1 (One Family)** Zone allows one-family dwellings units of a permanent character placed in permanent locations and one-family manufactured homes installed on a foundation system pursuant to Section 18551 of the California Health and Safety Code which comply with Subsection G of this Section. Private garages to accommodate not more than three (3) cars.

The **R-2 (Two Family)** Zone allows any use permitted in the R-1 Zone, two-family dwellings, multiple dwellings subject to site review, and incidental and accessory uses to the aforesaid.

The **R-3 (Multiple-Family)** Zone is any use permitted in the R-2 Two-Family Zone. Multiple dwellings; provided, however, that if more than four (4) dwelling units are proposed to be constructed on one (1) lot, the construction of such units shall be subject to approval of a site plan pursuant to the procedure set forth in Paragraph 1 of Subsection G of Section 16.2 of this Ordinance.

The **C-1 (Neighborhood Commercial)** Zone is intended for retail stores and personal service businesses, which are appropriately located in close proximity to residential areas, while minimizing the undesirable impact of such uses in the neighborhoods, which they serve.

The **C-2 (General Commercial)** Zone is intended for retail stores and businesses, which do not involve manufacturing and/or processing.

The **C-3 (Service Commercial)** Zone is intended for wholesale establishments and establishments engaged in repairing and servicing equipment, materials and products, but which do not involve the manufacturing, assembling, packaging or processing of articles of merchandise for distribution and retail sales.

The **M-1 (Light Manufacturing)** Zone is intended for establishments engaged in the manufacturing, assembling, packaging, treatment and processing of products other than those which may be obnoxious or offensive by reason of emission of odor, dust, smoke, gas, noise, or other similar causes.

The **M-2 (Heavy Manufacturing)** Zone is intended for establishments engaged in the M-1 Zone, gas, boiler works, ovens, mills, canning, plastics, machining, quarry, wood processing and other similar causes.

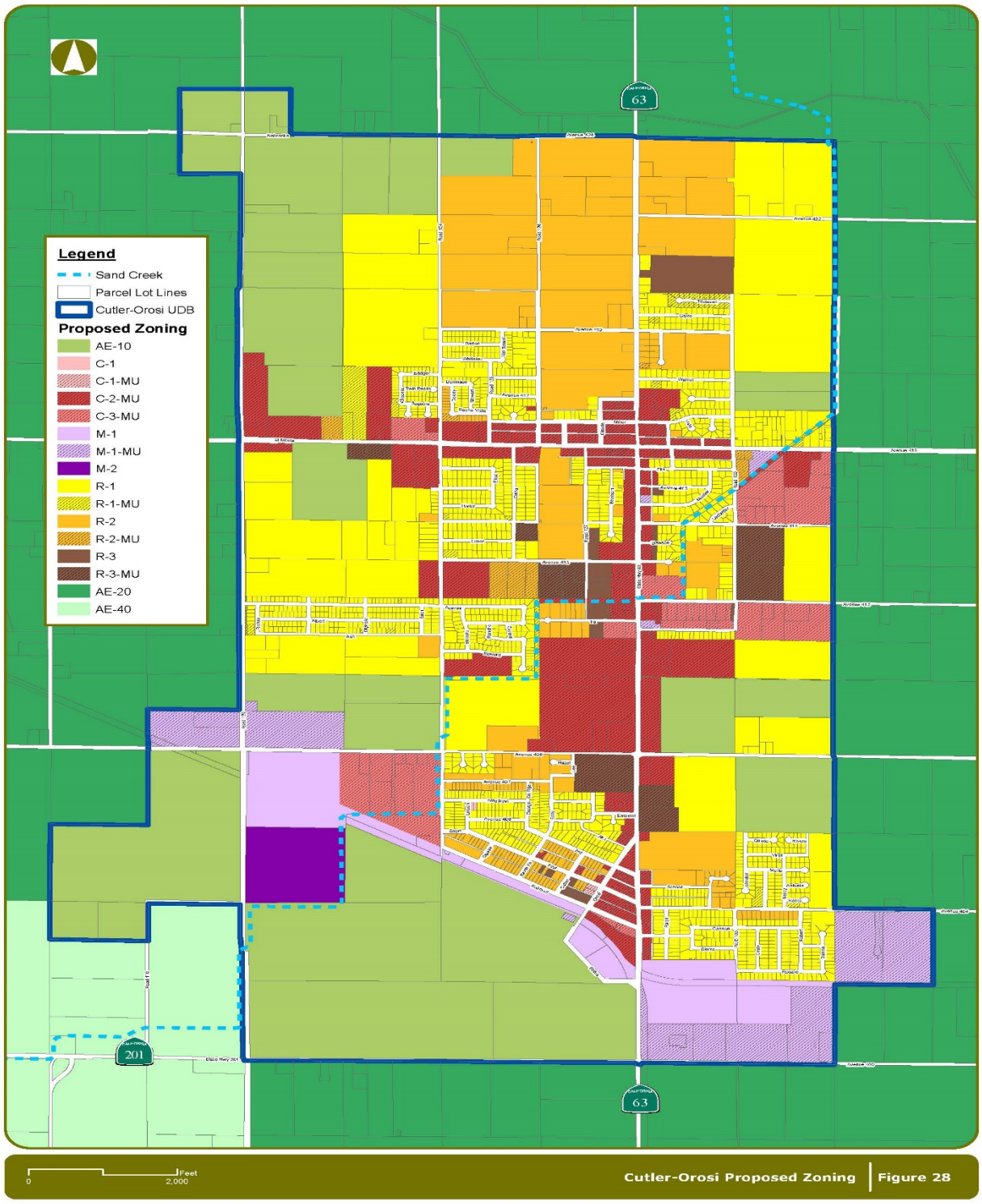
Mixed Use - Any combination of retail/commercial, service, office, residential, hotel, or other use in the same building or on the same site typically configured in one (1) of the following ways:

- **Vertical Mixed Use**. A single structure with the above floors used for residential or office use and a portion of the ground floor for retail/commercial or service uses.
- **Horizontal Mixed Use – Attached**. A single structure which provides retail/commercial or service use in the portion fronting the public or private street with attached residential or office uses behind.
- **Horizontal Mixed Use – Detached**. Two (2) or more structures on one (1) site which provide retail/commercial or service uses in the structure(s) fronting the public or private street, and residential or office uses in separate structure(s) behind or to the side.

Mixed Use allows for a variety of development projects. By allowing the community of Cutler-Orosi to respond to market forces, more opportunities are created for economic development and job development.

The **AE-10 (Exclusive Agricultural Zone 10 Acre Minimum)** The AE-10 Zone is an exclusive zone for intensive and extensive agricultural uses and for those uses, which are a necessary and integral part of intensive and extensive agricultural operations.

Figure 29 - Proposed Zoning Districts Map



Circulation Element

The purpose of this Circulation Element Update for the community of Cutler-Orosi is to provide for a safe, convenient and efficient transportation system. The Circulation Element has been designed to accommodate anticipated transportation needs based on the land use element. In compliance with state law, all city and county general plans must contain a circulation element that designates future road improvements and extensions, addresses non-motorized transportation alternatives, and identifies funding options. The intent of this Circulation Element is to:

- Identify transportation needs and issues within Cutler-Orosi, as well as regional relationships that affect the transportation system;
- Consider alternatives to the single-occupant vehicle as means of providing services and access to facilities; and
- Establish policies that coordinate the Cutler-Orosi transportation and circulation system with General Plan and area plan land use maps and provide direction for future decision-making.

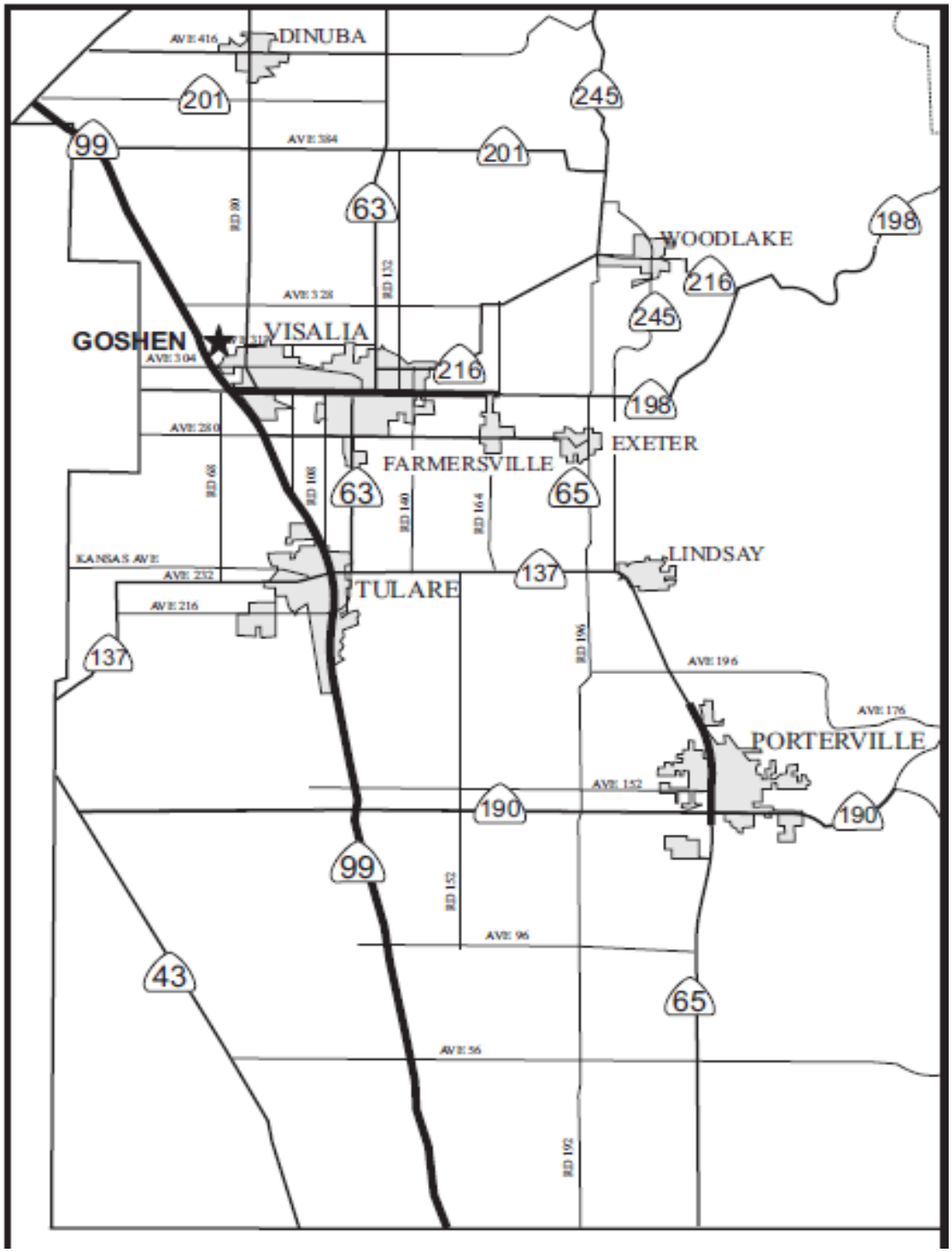
Figure 29 shows Cutler-Orosi in the context of its region. The transportation system within the Cutler-Orosi planning area includes SR 63, SR 201 and Avenue 416 as well as several County routes and local streets as shown on **Figure 30**. This figure also shows key intersections that were selected for detailed analysis.

Existing Transportation Conditions

Cutler-Orosi have an excellent circulation system in terms of access to other parts of the County. SR 201 and several County roads provide access to SR 99, 20 miles to the west, and the foothill region to the east. SR 99 is an important route used by industry to move goods to the southern and northern parts of the State. It also provides commuters with access to the Central Valley's larger metropolitan areas. SR 63 is another major north/south route in this region. It connects Cutler-Orosi with Visalia and it is the prime route between the two communities.

The communities also have a local circulation system, that when completed, will tie them together and create an excellent framework from which minor neighborhood streets can be built. Roads 120, 124, 128 SR 63, 130, and 136 traverse the planning area in a north/south fashion and Avenues 425, 416, 408, and 400 (SR 201) are laid out on an east-west axis. Several roadways must either be built or widened in order to create an efficient circulation system. Most notably, there is an absence of a north-south road east of SR 63 which would connect Cutler to Orosi, and a road system southwest of Cutler which would "open up" land for industrial development.

Figure 30 - Roadway Network of County



The internal circulation system of Cutler-Orosi is composed of paved streets with a curb-to-curb width of 40 feet. These "minor" streets provide circulation within each neighborhood of the community. Although they all have ample capacity for additional traffic, the condition of some of these streets is poor because they lack curbs and/or gutters or their pavement condition is deteriorating. Further, there are numerous areas in Orosi, which are composed of large-lot rural residential development and are not "linked" to adjacent neighborhoods.

Street and Highway System

Functional classification is the process by which streets and highways are grouped into classes according to the type of service they provide. Streets and highways are classified according to their primary function and may be assigned into several basic classifications:

- State Highways (which may be freeways, expressways or conventional highways)
- Arterials and Collectors
- Local Streets

State Highways connect regional destinations and generally pass through several jurisdictions. Traffic carrying capacity is maintained through access control at two-mile or more intervals, with shorter intervals between access points permitted in large urban areas. There are two designated State Routes within the Planning Area.

- State Route 63
- State Route 201

Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the community area and connect with important county roads and state highways. They also provide for the distribution and collection of through traffic to and from collector and local streets. There is one designated "Arterial" street within the Planning Area.

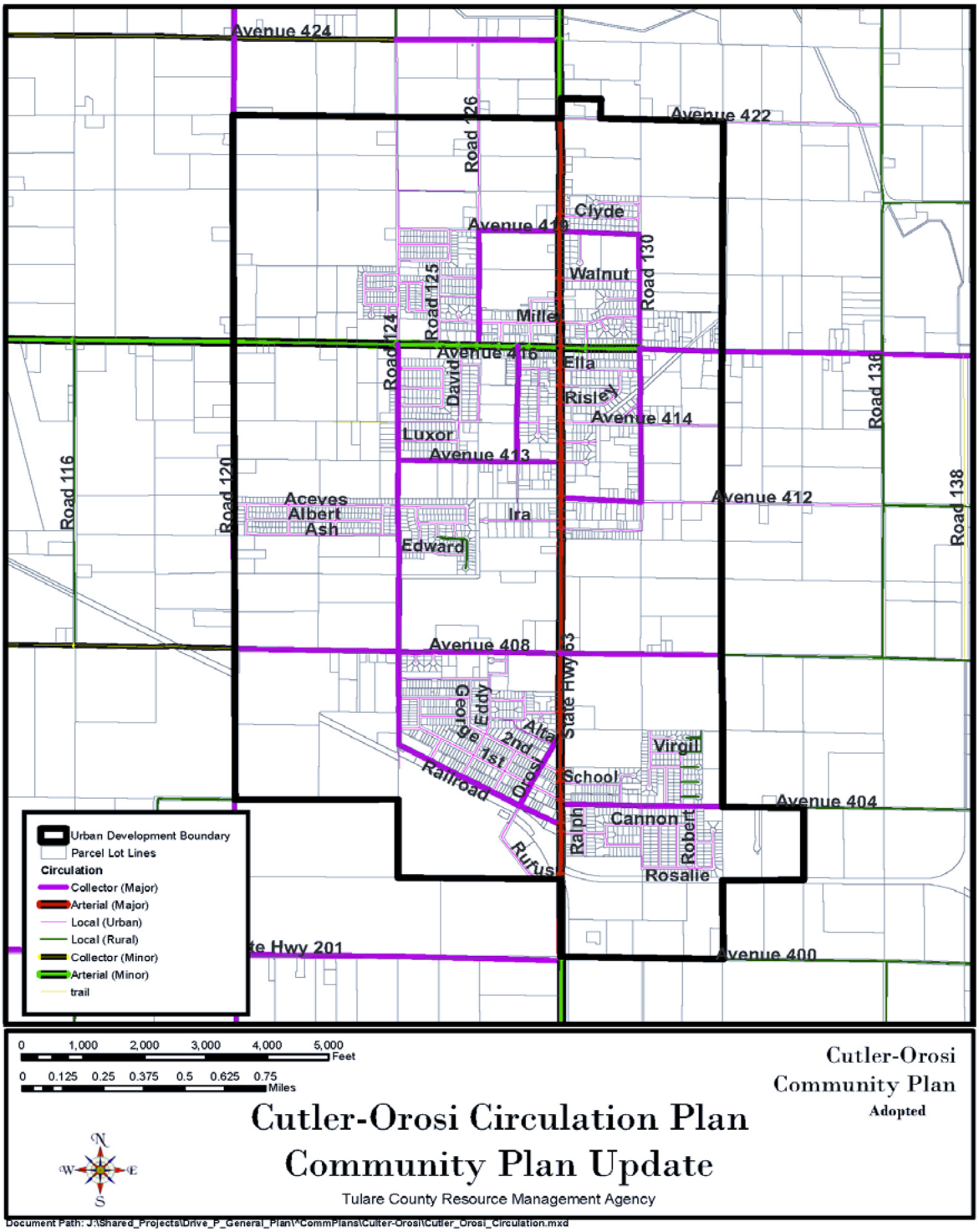
- Avenue 416

Collectors provide for traffic movement between arterial and local streets, traffic movement within and between neighborhoods and major activity centers, and limited direct access to abutting properties.

Local streets provide for direct access to abutting properties and for localized traffic movements within residential, commercial and industrial areas.

- All streets in the Circulation network are classified as local streets.

Figure 31 - Community of Cutler-Orosi Circulation Plan



Regional Transportation Planning

Tulare County Association of Governments (TCAG) Regional Transportation Plan

The Regional Transportation Plan (RTP) is a multi-modal, long-range planning document prepared by the Tulare County Association of Governments (TCAG). The RTP includes programs and policies for congestion management, transit, bicycles and pedestrians, roadways, freight, and finances for Tulare County. The RTP is prepared every four years and contains a listing of projects considered to be financially feasible within a 25-year planning time frame. All federally funded transportation projects must be consistent with the RTP.

The RTP for Tulare was last updated and adopted in July 2018[52]. The 2018 RTP is the second iteration in response to state legislation (SB 375) that requires that the RTP show reductions in greenhouse gas emissions from passenger vehicles (the 2014 RTP was the first iteration). Thus, there is a new emphasis in the RTP on promoting ridesharing (transit, van and carpools) and active transportation (walking and bicycling). To this end, the RTP now includes a Sustainable Communities Strategy (SCS), a blueprint for land use patterns and transportation facilities and services that will facilitate fewer vehicle trips and vehicle miles traveled. TCAG is in the process of updating the 2018 RTP with a 2022 version anticipated for completion in 2022.

San Joaquin Valley Air Quality Management Plan

The San Joaquin Valley Air Pollution Control District (SJVAPCD) has prepared the Air Quality Management Plan (AQMP) and various other regulations to reduce air emissions. Both the plan and several regulations aim to reduce emissions from mobile sources – automobiles and trucks, as well as other modes of transportation.

Measure R ½ Percent Sales Tax for Transportation

Measure R is the half-percent sales tax measure for transportation improvements passed by the voters of Tulare County in 2006 and managed by the Tulare County Transportation Authority (TCTA). The Measure provides funding for transportation projects (highway, transit, and ridesharing) over the 20-year duration of the Measure. Measure R funds are used by the County in Cutler-Orosi to repair streets, and to improve the existing and planned transportation system.

Public Transit and Active Transport Systems

"While the private automobile is the dominant mode of travel within Cutler-Orosi, as it is throughout Tulare County, other modes of transportation are important. The latest available Census survey data for Cutler-Orosi indicates that about 57 percent of commuters drive alone to work, while 43 percent use other means: 29 percent carpool or vanpool, 4 percent walked, 0 percent used public transportation and 1 percent worked at home."[53] The most recent available Census survey data for Cutler-Orosi indicates that about two-third of commuters drive alone to work, while one-third use other means: 16.2 percent carpool or vanpool, 1.4 percent walked, 0 percent used public

[52] TCAG. RTP Checklist for MPOs. Accessed July 2021 at: https://tularecog.org/tcag/planning/rtp/rtp-20181/rtp-checklist/

[53] Cutler-Orosi Community Plan Update Transportation Impacts Study (TIS). Page 22. Prepared by VRPA Technologies, Inc. and included in Appendix "E" of the Update's Draft EIR.

transportation and 9.0 percent worked at home.[54] While Cutler's data shows 67.4 drove alone, 20.1 carpooled, 11.7 percent worked from home, and 0 percent walked, bicycled, or used other means.[55]

"The Census bureau does not collect data on non-work trips, which represent a greater share of travel than work trips, but tend to be less concentrated in peak traffic periods. Off-peak trips also tend to have a greater proportion of shared ride and active (walk and bike) trips."[56] "While congestion is not a major issue in Cutler-Orosi, overreliance on automobiles creates other costs for both society and households, and means that many in the community who cannot drive (the young, the old, the disabled, the poor) must rely on those who can drive for their mobility. For this reason, it is important to encourage public transit systems and increased use of active modes of transportation, including bicycles and walking. The public transit system alternatives for Cutler-Orosi include fixed route public transit systems, common bus carriers, and other local agency transit and paratransit services."[57]

Transportation Demand Management

Transportation Demand Management (TDM) strategies reduce dependence on the single-occupant vehicle, increase the ability of the existing transportation system to carry more people, and enhance mobility. Examples of TDM strategies include telecommuting, flexible work hours, and electronic commerce that enable people to work and shop from home. According to CalVans, the major vanpool broker in the Valley, vanpools are becoming more prevalent for short-to-medium range commute trips, as well as for traditional long-distance usage: Key vanpool users include agricultural workers, and employees at large firms and government agencies. Park-n-ride facilities and carpooling will also continue to be a significant link between highway and transit modes. For the remainder of the study area, an overall rate of traffic growth of one percent per year was determined to be a reasonable forecast assumption. It is not likely that TDM strategies would be very effective due to Popular/Cotton Center's current and projected small population and lab

Road Capacity and Level of Service (LOS)

LOS is categorized by two parameters, uninterrupted flow and interrupted flow. Uninterrupted flow facilities have no fixed elements, such as traffic signals, that cause interruptions in traffic flow (e.g., freeways, highways, and controlled access, some rural roads). Interrupted flow facilities have fixed elements that cause an interruption in the flow of traffic such as stop signs and signalized intersections. The definitions and measurements used for determining level of service in interrupted and uninterrupted conditions are shown in **Tables 48 and 49.**

In Tulare County, General Plan Policy **TC – 1.16 County Level of Service (LOS) Standards** states; "The County shall strive to develop and manage its roadway system (both segments and intersections) to meet a LOS of "D" or better in accordance with the LOS definitions established by the Highway Capacity Manual."[58]

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[54] 2010 U.S. Census for Orosi https://data.census.gov/cedsci/profile?g=1600000US0654372 and for Cutler https://data.census.gov/cedsci/table?q=Cutler%20CDP&tid=ACSST5Y2019.S0801

[55] Cutler-Orosi Community Plan Update Transportation Impacts Study (TIS). Page 22. Prepared by VRPA Technologies, Inc. and included in Appendix "E" of the Update's Draft EIR

[56] Ibid.

[57] Op. Cit.

[58] *Tulare County General Plan 2030 Update, Part 1 – Goals and Policies Report. Page 13-7.*

Table 48 - Uninterrupted Traffic Flow Facilities	
LEVEL OF SERVICE	**DEFINITION**
A	Describes free-flow operations. Free-Flow Speed (FFS) prevails on the freeway, and vehicles are almost completely unimpeded in their ability to maneuver within the traffic stream. The effects of incidents or point breakdowns are easily absorbed.
B	Represents reasonably free-flow operations, and FFS on the freeway is maintained. The ability to maneuver within the traffic stream is only slightly restricted, and the general level of physical and psychological comfort provided to drivers is still high. The effects of minor incidents and point breakdowns are still easily absorbed.
C	Provides for flow with speeds near the FFS of the freeway. Freedom to maneuver within the traffic stream is noticeably restricted, and lane changes require more care and vigilance on the part of the driver. Minor incidents may still be absorbed, but the local deterioration in service quality will be significant. Queues may be expected to form behind any significant blockages.
D	At this level speeds begin to decline with increasing flows, with density increasing more quickly. Freedom to maneuver within the traffic stream is seriously limited and drivers experience reduced physical and psychological comfort levels. Even minor incidents can be expected to create queuing, because the traffic stream has little space to absorb disruptions.
E	Describes operation at capacity. Operations on the freeway at this level are highly volatile because there are virtually no useable gaps within the traffic stream, leaving little room to maneuver within the traffic stream. Any disruption to the traffic stream, such as vehicles entering from a ramp or changing lanes, can establish a disruption wave that propagates throughout the upstream traffic flow. At capacity, the traffic stream has no ability to dissipate even the most minor disruption, and any incident can be expected to produce a serious breakdown and substantial queuing, the physical and psychological comfort afforded to drivers is poor.
F	Describes breakdown, or unstable flow. Such conditions exist within queues forming behind bottlenecks. Breakdowns occur for a number of reasons: • Traffic incidents can temporarily reduce the capacity of a short segment, so that the number of vehicles arriving at a point is greater than the number of vehicles that can move through it. • Points of recurring congestion, such as merge or weaving segments and lane drops, experience very high demand in which the number of vehicles arriving is greater than the number of vehicles that can be discharged. • In analyses using forecast volumes, the projected flow rate can exceed the estimated capacity of a given location.

Table 49 - Interrupted Traffic Flow Facilities	
LEVEL OF SERVICE	**DEFINITION**
A	Describes operations with a control delay of 10 s/veh or less and a volume-to- capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is low and either progression is exceptionally favorable or the cycle length is very short. If it is due to favorable progression, most vehicles arrive during the green indication and travel through the intersection without stopping.
B	Describes operations with a control delay between 10 and 20 s/veh and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is low and either progression is highly favorable or the cycle length is short. More vehicles stop than with LOS A, with reasonably unimpeded travel between intersections.
C	Describes operations with control delay between 20 and 35 s/veh and a volume-to- capacity ratio no greater than 1.0. This level is typically assigned when progression is favorable or the cycle length is moderate. Individual cycle failures (i.e.one or more queued vehicles are not able to depart as a result of the insufficient capacity during the cycle) may begin to appear at this level. The number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping. May be longer queues and operations between locations may be more restricted.
D	Describes operations with control delay between 35 and 55 s/veh and a volume-to- capacity ratio no greater than 1.0. Travel speeds are about 40 percent below free flow speeds. This level is typically assigned when the volume-to-capacity ratio is high and either progression is ineffective or the cycle length is long. Many vehicles stop and individual cycle failures are noticeable.
E	Describes operations with control delay between 55 and 80 s/veh and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is high, progression is unfavorable, and the cycle length is long. Individual cycle failures are frequent. Average travel speed is one-third of free flow speeds. The facility is generally at full capacity.
F	Describes operations with control delay exceeding 80 s/veh or a volume-to-capacity ratio greater than 1.0. This level is typically assigned when the volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue. Extremely slow speeds with average delay of 80 seconds or more. Frequent stop and go conditions.

Transportation Concept Report (TCR) State Route (SR) 63

The TCR defines the appropriate route concept level of service (LOS) and facility type(s) for SR 63. The route is currently operating at a LOS of "B" and "C." By the year 2020, in some segments it will drop to a LOS "D" and "E". Route Segment 10 runs between 0.1 mile north of Avenue 403 to Avenue 422 and is the ultimate transportation corridor concept.

Complete Streets

The Board of Supervisors approved the Complete Streets Program on December 2016 **(see Attachment A-6)**. The Complete Streets Programs Goals, Policies, Objectives, and Standards are hereby incorporated by reference. Included in the plan were policies and implementation measures as provided below. These projects have been included on the TCAG Measure R list as Complete Streets.

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Road
5. First Drive – SR 63 to Road 124

Orosi

1. Avenue 413 – Road 124 to SR 63
2. Avenue 419
3. Avenue 416 – SR-63 to Dinuba
4. Road 130 (Strong interest from school district)
5. Road 124

Goals, Policies, and Standards

The intent of the Cutler-Orosi Community Circulation Element is to establish a comprehensive multi-modal transportation system that is efficient, environmentally and financially sound, and coordinated with the Land Use Element.

Goal 1: Design and implement a multi-modal transportation system that will serve projected future travel demand, minimize congestion, and address future growth in Cutler-Orosi.

Policies and Standards:

1. Utilize existing infrastructure and utilities to the maximum extent practical and provide for the logical, timely, and economically efficient extension of infrastructure and services.
2. Designate streets according to the following functional classifications:
   a) Freeways and Expressways carry regional traffic through the community with access only at interchanges with major streets.
   b) Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the urban area and connect with important county roads and state highways. They also provide for the distribution and collection of through traffic to and from collector and local streets.
   c) Collectors provide for traffic movement between arterial and local streets, traffic movement within and between neighborhoods and major activity centers, and limited direct access to abutting properties.
   d) Local streets provide for direct access to abutting properties and for very localized traffic movements within residential, commercial and industrial areas.

   All facility-types above (except freeways) should be capable of accommodating transit and paratransit vehicles. Furthermore, all facility-types except freeway should include provisions for active modes of transportation (walking and cycling).
3. Develop and apply consistent standards for new streets (and existing streets where feasible without substantial ROW takes) based on the roadway classification.
4. Require applicants for new development projects to dedicate needed ROW and construct and/or upgrade to County standards the streets and roads that will serve their projects.
5. Plan new arterial and collector streets as needed to improve access and enhance the develop potential of land designated for commercial and industrial uses.
6. Improvement standards for local and minor streets shall include perpendicular curbs, gutters and adequate street lighting at intersections.
7. Access to arterials by driveways, local and minor streets, and alleys should be controlled as needed in order to ensure efficient traffic flow and safety along these streets.
8. Local streets should be designed to discourage high traffic volumes and through traffic.
9. Develop a Circulation Map showing the public street system. Designated streets and recommended rights-of-way should be indicated on this map.
10. Allow standards for new street development to be altered or refined where it can be demonstrated that projected traffic flows can be accommodated.
11. Plan for peak-hour Level of Service (LOS) "D" or better throughout the circulation network.

12. Make intersection improvements to the existing major street system selectively, favoring traffic engineering solutions rather than major structural improvements. This could include signalization, intersection channelization, use of directional signs, and diversion of traffic onto underutilized streets.
13. Use Complete Streets concepts in the design of new local streets where such techniques will improve safety and manage traffic flow.
14. Ensure the street network provides efficient routes for emergency vehicles, meeting necessary street widths, turn around radius, and other factors as determined by the County in consultation with fire and other emergency service providers.
15. Cooperate with local, regional, State and federal agencies to plan for, establish and maintain good connectivity to an efficient multimodal regional transportation system.

Goal 2: Provide designated routes and loading standards that reduce the noise and safety concerns associated with truck traffic.

Policies and Standards:

1. Designate truck routes for use by heavy commercial and industrial traffic. Initially, designated truck routes shall be:
    - SR 63
    - SR 201
    - Avenue 416
2. Design interior street systems for commercial and industrial subdivisions to accommodate the movement of heavy trucks.
3. Restrict heavy-duty truck through-traffic in residential areas and plan land uses so that trucks do not need to traverse these areas.

Design off-street loading facilities for all new commercial and industrial developments so that they do not face surrounding roadways or residential neighborhoods. Truck backing and maneuvering to access loading areas shall not be permitted on the public road system, except when specifically permitted by the County Engineer.

Goal 3: Provide safe and convenient pedestrian access between residential neighborhoods, parks, open space, and schools that service those neighborhoods.

Policies and Standards:

1. Provide a safe walking environment for pedestrians.
    a) New development should include safe and pleasant designs, which promote pedestrian access to arterials and collectors and consider the location of community services, such as schools, parks and neighborhood shopping activity centers in the accessibility of their design for all persons.
    b) Require the installation of sidewalks as an integral part of all street construction where appropriate.
    c) Require street lighting within the rights-of-way of all public streets.

d) Include pedestrian signal indicators as an integral part of the installation of traffic signals.

2. Maximize visibility and access for pedestrians and encourage the removal of barriers (walls, easements, and fences) for safe and convenient movement of pedestrians. Special emphasis should be placed on the needs of disabled persons considering ADA regulations.
3. Plan for pedestrian access consistent with road design standards while designing street and road projects. Provisions for pedestrian paths or sidewalks and timing of traffic signals to allow safe pedestrian street crossing shall be included.
4. Collaborate with the Cutler-Orosi Schools and the School District to ensure that schoolchildren have adequate transportation routes available, such as a local pedestrian or bike paths, or local bus service.
5. Encourage safe pedestrian walkways within commercial, office, industrial, residential, and recreational developments that comply with the Americans with Disabilities Act (ADA) requirements.
6. Coordinate with TCaT and private bus operators to ensure that pedestrian facilities are provided along and/or near transit routes, whenever feasible. New land developments may be required to provide pedestrian facilities due to existing or future planned transit routes even if demand for a pedestrian facility is not otherwise warranted.
7. Review all existing roadways without pedestrian facilities when they are considered for improvements (whether maintenance or upgrade) to determine if new pedestrian facilities are warranted. New roadways should also be assessed for pedestrian facilities.

Goal 4: Ensure the provision of adequate off-street parking for all land uses.

Policies and Standards:

1. Require all new development to identify adequate on street and off-street parking based on expected parking needs.
2. Encourage shared parking among nearby uses with complementary parking demand patterns.
3. Provide adequate loading areas within off-street parking areas for all commercial and manufacturing land uses.
4. Anticipate parking needs at proposed and expected activity centers, particularly commercial areas.

Goal 5: Provide a transportation system that is integrated with the region.

Policies and Standards:

1. Coordinate local transportation planning with the TCAG Congestion Management Plan to ensure eligibility for state and federal funding.
2. Incorporate the Regional Transportation Plan, and the Tulare County Short- and Long-Range Transit Plans into the Community Plan Circulation Element, and encourage the active participation of Caltrans in the design of highway capital improvement projects.

Goal 6: Encourage the use of public transit services to reduce reliance on the automobile.

Policies and Standards:

1. Encourage transit alternatives to meet the basic transportation needs of the young, the elderly, the handicapped, and people without access to an automobile.
    a) Consider development of an integrated transit center within Cutler-Orosi where all transit services can connect with each other as well as with private ridesharing.
    b) Encourage and provide for ridesharing, park and ride, and other programs that can reduce emissions, save energy, and reduce monetary costs for firms and workers.
2. Planning and development of arterial and collector streets shall include design features which can be used as future public transit stops.
3. Support the expansion and improvement of transit systems and ride sharing programs to reduce the production of automobile emissions.
4. Support the use of alternate fuel vehicles and fueling stations for public transit vehicles, and County public agency vehicles.
5. Support TCaT and other transit operators' programs to foster transit usage.
6. Support all operator efforts to maximize revenue sources for short and long range transit needs that utilize all funding mechanisms available including federal grants, state enabling legislation, and farebox revenue. This can be accomplished through TCAG and the Tulare County Transit Agency (TCaT) through the development of the Short and Long Range Transit Plans.
7. Support programs developed by transit agencies/operators to provide paratransit service.
8. Incorporate the potential for public transit service in the design of developments identified as major trip attractions (i.e. community centers and employment centers).
9. Explore potential development of a park-n-ride lot in Cutler-Orosi.
10. Support continued improvements to AMTRAK rail passenger service within Tulare County and throughout the San Joaquin Valley.

Goal 7: Provide efficient goods movement

Policies and Standards:

1. Encourage the efficient movement of goods and people by rail through a shift of a portion of the goods previously moved by trucks onto the rail freight system.
2. Implement Street and highway projects to provide convenient and economical goods movement, including access to rail terminals, in areas where large concentrations of truck traffic exist.
3. Identify street and highway improvement and maintenance projects that will improve goods movement and implement projects that are economically feasible.
4. Encourage use of rail for goods movement whenever feasible.

Goal 8: Provide safe and convenient facilities for non-motorized modes of transportation that enhance the future livability and character of Cutler-Orosi.

Policies and Standards:

1. Consider developing a Bikeway plan for Cutler-Orosi based on the following facility designations:
   a) Bike Path (Class I). A special pathway for the exclusive use of bicycles, which is separated from motor vehicle facilities by space or a physical barrier. It is identified by guide signing and pavement markings.
   b) Bike Lane (Class II). A lane on the paved area of a road for preferential use by bicycles. It is usually located along the right edge of the paved area or between the parking lane and the first motor vehicle lane. It is identified by a "Bike Lane" guide sign, special lane lines, and other pavement markings.
   c) Bike Route (Class III). A recommended route for bicycle travel along existing rights-of-way, which is signed but not striped.
   d) Bikeway. All facilities that explicitly provide for bicycle travel. The bikeway can be anything from a separate facility to a simple signed street.
2. Give priority to bikeways that will serve the highest concentration of cyclists and destination areas of highest demand, especially Cutler-Orosi Elementary Schools.
3. Provide bikeways in proximity to major traffic generators such as commercial centers, schools, recreational areas, and major public facilities.
4. Develop a visually clear, simple, and consistent bicycle system with standard signs and markings, as designated by the State of California Traffic Control Devices Committee and the State Bikeway Committee.
5. Support the installation of bike parking racks at public and private places of assembly such as parks, schools, employment sites, churches, and retail commercial developments.
6. Provide non-motorized alternatives for commuter travel as well as recreational opportunities.
7. Provide separate rights-of-way for non-motorized facilities whenever economically and physically feasible.
8. Develop bikeways in compliance with the standards established in the Caltrans Highway Design Manual or other appropriate standards.

Goal 9: Design, construct, and operate the transportation system in a manner that maintains a High level of environmental quality.

Policies and Standards:

1. Control dust and mitigate other environmental impacts during all stages of roadway construction.
2. Protect residents from transportation generated noise hazards. Increased setbacks, walls, landscaped berms, other sound absorbing barriers, or a combination thereof shall be provided along four lane highways in order to protect adjacent noise-sensitive land uses from traffic generated noise impacts. Additionally, noise generators such as commercial, manufacturing, and/or industrial activities shall use these techniques to mitigate exterior noise levels to no more than 60 decibels.
3. Review and monitor proposals for expansion of pipelines for the transport of suitable products and materials, and require mitigation of environmental impacts.

4. Encourage the use of non-polluting vehicles for both public and private uses.
5. Include noise mitigation measures in the design of roadway projects in Cutler-Orosi.

Goal 10: Support the use of Transportation Demand Management (TDM) strategies to reduce dependence on the single-occupant vehicle, increase the ability of the existing transportation system to carry more people, and enhance mobility along congested corridors.

Policies and Standards:

1. New development shall consider Transportation System Management and Transportation Demand Management as strategies for the mitigation of traffic and parking congestion. Public transit, traffic management, ride sharing and parking management are to be used to the greatest extent practical to implement transportation management strategies.
2. Coordinate with Caltrans, TCAG, transit agencies and other responsible agencies to identify the need for additional park-n-ride facilities along major commuter travel corridors.

Goal 11: Utilize Intelligent Transportation Systems (ITS) to improve the safety and performance of the surface transportation system using new technology in detection, communication, computing, and traffic control.

Policies and Standards:

1. Encourage the integration of Intelligent Transportation Systems (ITS) consistent with the principles and recommendations referenced in the TCAG Regional Transportation Plan

Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2010 Tulare County Regional Bicycle Transportation Plan **(see Figure 31)** has a proposed Class II Bike Project.[59] In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns.

The Caltrans Highway Design Manual (HDM), which governs bicycle facility design in California, distinguishes three (3) different types of bicycle facilities.

Class I: Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County Class I facilities will primarily be implemented through TCAG. Currently there is a proposed Class I on Avenue 416.

Class II bikeways that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. No proposed Class II bicycle facilities in the Cutler-Orosi area.

[59] TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

Class III: Bikeway that provides designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III bicycle facilities are not proposed in Cutler-Orosi areas.

Although not signed on many local roads in Cutler-Orosi, bicyclists are allowed to use the side of the road or share the road on all County roadway facilities excluding freeways.

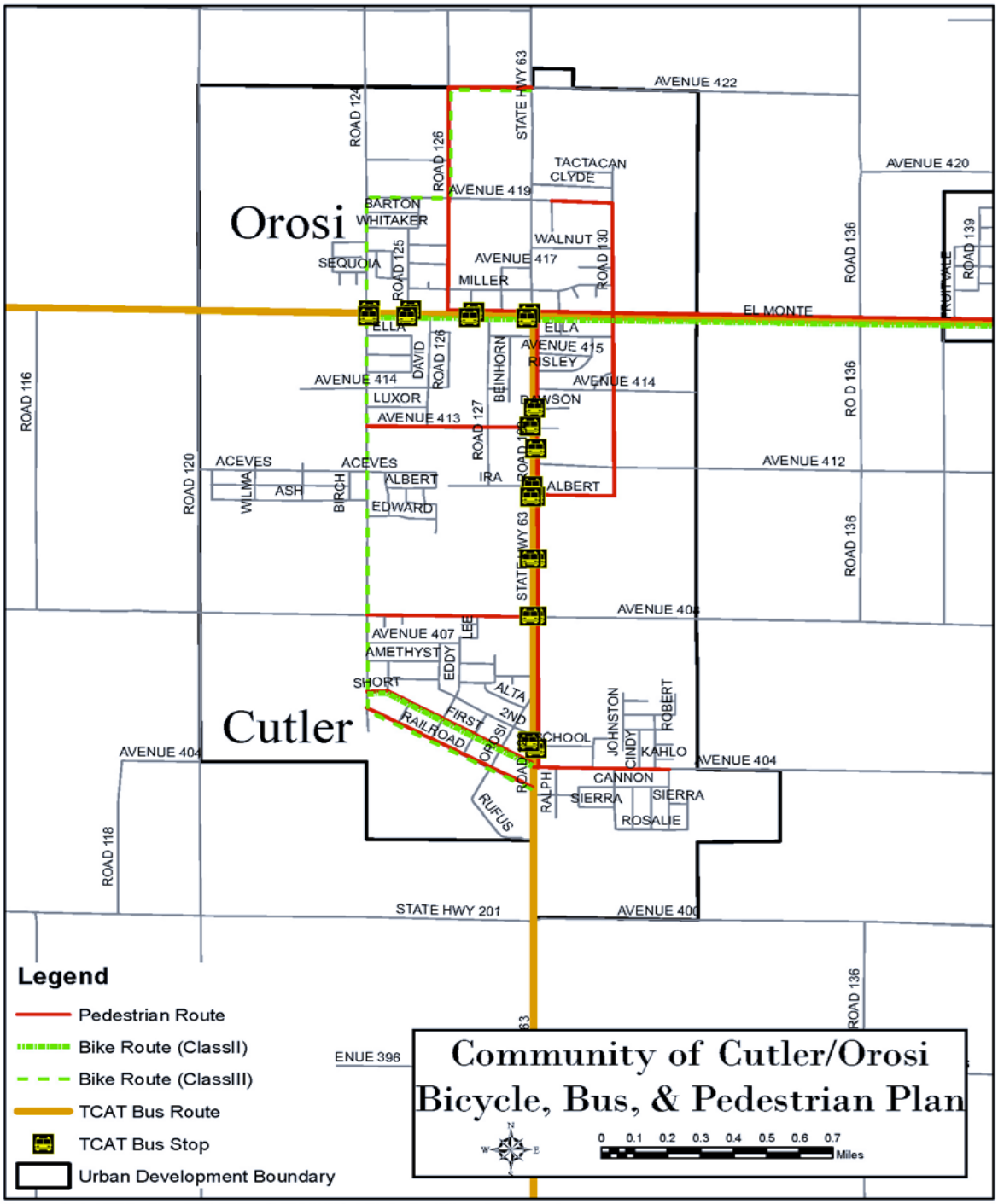
Multiuse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. No multiuse trails exist or are proposed in Cutler-Orosi.

Pedestrian Paths and Sidewalks

A review of facilities for pedestrian travel in Cutler-Orosi presents a broad array of conditions in which pedestrians are accommodated. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the competition of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

Figure 32 - Cutler-Orosi Bicycle & Pedestrian Plan



Paratransit Service

"Paratransit Services are transportation services such as carpooling, vanpooling, taxi service, and dial-a-ride programs. The County supports reliable and efficient paratransit service by encouraging development of service systems that satisfy the transit needs of the elderly and physically handicapped."[60]

Park-and-Ride Lots

Park-and-Ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the Community's population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community along State Route 65.

Transit and Bus Stops

The Tulare County Area Transit Agency (TCaT) operates fixed-route services that link communities with each other and with Visalia and Dinuba's urban transit systems. Cutler-Orosi **(see Figure 32)** is connected via TCaT North County Route 10. Route 10 **(see Figure 33)** has twelve northbound and southbound buses serving Cutler-Orosi on weekdays and four buses in each direction on Saturdays and Sundays. Stops are currently located at the Road 1287 and School Avenue in Cutler-Orosi has two stops at R-N Market and Orosi Mart & Deli. (See TCaT website at http://www.tularecog.org/bustimes/. TCaT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Cutler-Orosi, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

[60] Cutler-Orosi Community Plan Update Transportation Impacts Study (TIS). Page 21. Prepared by VRPA Technologies, Inc. and included in Appendix "E" of the Update's Draft EIR

Figure 33 – TCaT North County Route 10

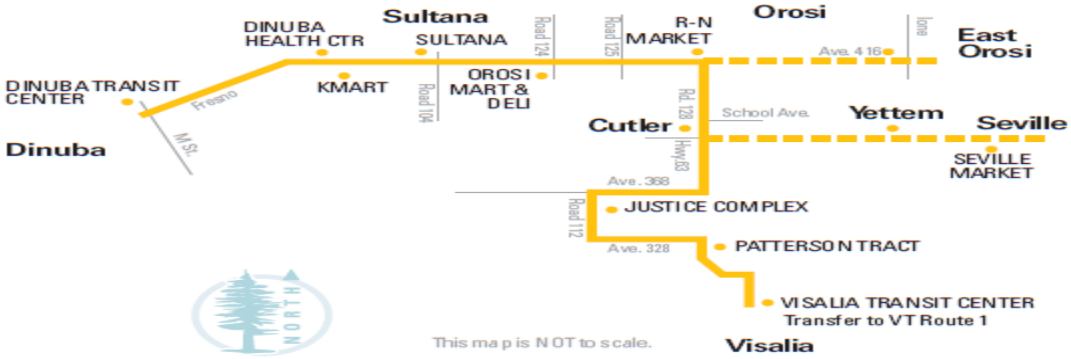


Figure 34 - TCaT Route 10 Bus Schedule

Northbound

WEEKDAY SERVICE									
VISALIA ❶ Transit Center	VISALIA Justice Complex	SEVILLE Seville Market	CUTLER Rd.128 & School Ave	E OROSI Ave.416 & Tone Rd.	OROSI R-N Market	OROSI Orosi Mart & Deli	SULTANA Ave.416 & Rd.104	DINUBA K-Mart	DINUBA Transit Ctr
6:15	6:34	—	6:44	—	6:48	6:50	6:54	6:57	7:04
7:15	7:34	—	7:44	—	7:49	7:51	7:55	7:58	8:05
8:15	8:34	—	8:44	8:49	8:53	8:55	8:59	9:02	9:09
9:10	9:29	9:40	9:49	—	9:53	9:55	9:59	10:02	10:09
10:15	10:34	—	10:44	10:51	10:55	10:57	11:01	11:04	11:11
11:15	11:34	—	11:44	—	11:48	11:50	11:54	11:57	**12:05**
1:15	**1:34**	—	**1:44**	—	**1:48**	**1:50**	**1:54**	**1:57**	**2:04**
2:10	**2:29**	**2:40**	**2:49**	—	**2:53**	**2:55**	**2:59**	**3:02**	**3:09**
3:15	**3:34**	—	**3:44**	—	**3:48**	**3:50**	**3:54**	**3:57**	**4:04**
4:15	**4:34**	—	**4:44**	—	**4:48**	**4:50**	**4:54**	**4:57**	**5:04**
5:15	—	—	**5:40**	—	**5:44**	**5:46**	**5:50**	**5:53**	**6:00**
6:15	—	**6:36**	**6:45**	—	**6:49**	**6:51**	**6:55**	**6:58**	**7:05**
WEEKEND SERVICE									
10:25	10:44	—	10:54	—	10:58	11:00	11:04	11:07	11:14
1:09	**1:28**	—	**1:38**	—	**1:42**	**1:44**	**1:48**	**1:51**	**1:58**
2:58	**3:17**	—	**3:27**	—	**3:31**	**3:33**	**3:37**	**3:40**	**3:47**
4:47	**5:06**	**5:17**	**5:27**	—	**5:31**	**5:33**	**5:37**	**5:40**	**5:47**

Southbound

WEEKDAY SERVICE									
DINUBA Transit Ctr	DINUBA K-Mart	SULTANA Ave.416 & Rd.104	OROSI Orosi Mart & Deli	OROSI R-N Market	E OROSI Ave.416 & Tone Rd.	CUTLER Rd.128 & School Ave	SEVILLE Seville Market	VISALIA Justice Complex	VISALIA ❶ Transit Center
6:15	6:21	6:24	6:28	6:30	—	6:34	—	6:45	7:05
7:10	7:17	7:20	7:24	7:26	—	7:30	7:40	7:51	8:10
8:15	8:21	8:24	8:28	8:30	—	8:34	—	8:45	9:05
9:15	9:21	9:24	9:28	9:30	—	9:34	—	9:45	10:05
10:15	10:21	10:24	10:28	10:30	—	10:34	—	10:45	11:05
11:15	11:21	11:24	11:28	11:30	—	11:34	—	11:45	12:05
1:05	**1:11**	**1:14**	**1:18**	**1:20**	**1:25**	**1:30**	—	**1:41**	2:01
2:15	**2:21**	**2:24**	**2:28**	**2:30**	—	**2:34**	—	**2:45**	3:05
3:15	**3:21**	**3:24**	**3:28**	**3:30**	**3:34**	**3:41**	—	**3:52**	4:12
4:15	**4:21**	**4:24**	**4:28**	**4:30**	—	**4:34**	—	**4:45**	5:05
5:15	**5:21**	**5:24**	**5:28**	**5:30**	—	**5:34**	**5:47**	—	6:10
6:15	**6:21**	**6:24**	**6:28**	**6:30**	**6:34**	**6:41**	—	—	7:05
WEEKEND SERVICE									
9:20	9:26	9:29	9:33	9:35	—	9:39	9:50	10:00	10:20
11:19	11:25	11:28	11:32	11:34	—	11:38	—	11:49	**12:09**
2:03	**2:09**	**2:12**	**2:16**	**2:18**	—	**2:22**	—	**2:33**	**2:53**
3:52	**3:58**	**4:01**	**4:05**	**4:07**	—	**4:11**	—	**4:22**	**4:42**

Light type = AM
Bold type = PM

❶ Free transfer to VT Route 1
❷ Accept transfer from VT Route 1 with additional fee

❶ Transferencia gratis a VT Ruta 1
❷ Se accepta transferencia de VT Ruta 1 con cargo adicional

AMTRAK

The Hanford AMTRAK station, located approximately 45 miles southwest in Kings County is the closest station to Cutler-Orosi. Amtrak's San Joaquin route links Hanford to Sacramento to the north and Bakersfield to the south. An Amtrak bus can be taken from Bakersfield to Los Angeles Union station where Amtrak's interstate routes can be accessed along with California's Pacific Surfliner route. In Sacramento, additional interstate routes can be accessed along with the capital corridor route linking Sacramento to the Bay Area.

Aviation

The nearest airport is Sequoia Field Airport, which is located approximately five (5) miles to the southwest. The nearest operational general aviation airport is Fresno Yosemite International Airport (FAT), approximately 30 miles northwest of Cutler-Orosi, is the principal passenger and airfreight airport in the central San Joaquin Valley. Visalia Municipal Airport, is approximately 15 miles southwest.

Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 75 miles south of Cutler-Orosi and offers direct flights to several destinations.

STATE ROUTES

"State Route (SR) 63 is the principle state highway serving the Cutler-Orosi Community. SR 63 primarily exists as an undivided four-lane road without bike lanes throughout Cutler-Orosi Community. On-street parking is currently permitted on the four-lane segments. The posted speed limit is generally 35-40 mph throughout the community (except for school zones with a posted speed of 25 mph). The posted speed limit outside of these communities is generally 55 mph. According to Caltrans' website, the average annual daily traffic (AADT) along SR 63 in the study area was approximately 12,100 south of Avenue 416 and 7,300 south of Avenue 400 in 2017.

SR 201-Avenue 400 (west of SR 63) – currently exists as an undivided two-lane road in the study area. The posted speed limit is generally 55 mph. According to Caltrans' website, the AADT along SR 201 in the study area was approximately 3,000 in 2017."[61]

State Route 63

State Route (SR) 63 runs north and south beginning at SR 137 in the City of Tulare in Tulare County and ends at SR 180, near Squaw Valley in Fresno County. The route is located solely in Caltrans District 6, in Tulare and Fresno Counties. The route runs approximately 38 miles in length. SR 63 intersects with SR 201 before ending at SR 180. Route Segment 10 runs between 0.1 mile north of Avenue 403 to Avenue 422 and is the ultimate transportation corridor concept. Currently there are no projects funded for SR 63.[62]

State Route 201

State Route 201 runs west –east in Fresno and Tulare Counties. The route covers just over 25 miles

[61] Ibid. Page 11.

[62] Transportation Concept Report State Route 63, California Department of Transportation, December 2014

and is located solely in District 6. SR 201 was constructed in 1939 and reconstructed in 1974. There is a break in the route where it meets SR 63 and picks up again at Avenue 384 and continues to head east.[63]

State Route 99

State Route (SR) 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South. Cutler-Orosi is located approximately 20 miles east of SR 99.

Implementation Strategy

The purpose of this chapter is to prescribe a proposed approach to implement the general plan recommendations contained in chapters I through IV of the Cutler-Orosi Community Plan. There are several components that comprise the Cutler-Orosi Community Plan implementation strategy:

1. Zoning Code Changes
2. Complete Streets
3. Infrastructure

Zoning District Changes

As part of this Implementation Program for the Community Plan for Cutler-Orosi, there are a variety of changes to existing zoning districts. These changes are described below.

Revise Chapter 16 of the Zoning Code

Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

Elimination of SR Combining Zone

The SR combining zone requires site plan review for most uses in the combining zone. This alternative would eliminate the SR combining zone designation in the community of Cutler-Orosi. The elimination of the SR combining zone would only affect commercial zoning districts, C-2-SR, C-3-SR, and P-O-SR. This zone change would only affect approximately thirty (30) parcels in the C-2-SR, four (4) parcels in the C-3-SR, and one (1) parcel in the P-O-SR within Cutler-Orosi and as such, elimination of this combining zone would not have a noticeable effect on Cutler-Orosi.

[63] Transportation Concept Report State Route 201, California Department of Transportation, December 2017

Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Cutler-Orosi **(see Attachments A-2 and A-3)**.

Zoning Map Update

The current Zoning Map for Cutler-Orosi will be amended to be compatible with the Land Use Map outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other Cutler-Orosi Proposed Zoning. In addition, there are a number parcels that need to re-zoned to adhere to the airport land use plan.

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Attachments

A-1 Use Permit Requirement Changes (Zone Change Text)
A-2 Mixed Use Overlay District (Zone Change Text)
A-3 Development Standards (Mixed Use Zoning Districts)
A-4 General Plan Land Use and Zoning Consistency Matrix
A-5 Funding Sources
A-6 Complete Streets
A-7 Public Outreach

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Attachment A-1 Use Permit Requirement Changes

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A-1 Use Permit Requirement Changes (Zone Change Text)

H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (nighttime), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles), traffic (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone

Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	
Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Bookbinding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1

Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1

Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1,C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-	O, C-1, C-2, C-3,

	1, R-1, R-2, R-3, PO	M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tearoom or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Attachment A-2 Mixed Use Overlay Districts

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A-2 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the community of Cutler-Orosi, unless otherwise provided in this Ordinance.

PURPOSE **A.** The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

APPLICATION **B.** This overlay zone only applies to the community of Cutler-Orosi.

USE **C.** No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Cutler-Orosi.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy-five (75) feet to uppermost part of roof.

2. Front Yard: 0 Feet

3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.

4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.

5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.

6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.

7. Distance between structures: The minimum distance between structures is 10 feet.

8. Parking: Off-street parking and loading shall be required in conformance with Section 15.

9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required

for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Cutler-Orosi. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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Attachment A-3 Development Standards (Mixed Use Zoning Districts)

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A-3 Development Standards (Mixed Use Zoning District)

To promote Economic Development within the Cutler-Orosi Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Cutler-Orosi. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Cutler-Orosi Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Cutler-Orosi. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent "storefront" windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are "tacked on" to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades, which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths, which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow, which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

ROOFS AND AWNINGS

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

SITE PLANNING

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are

compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

LANDSCAPING

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery
(Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or rights-of-way should be screened to a height of thirty-six (36) inches above the grade with landscaping and/or low high quality fencing.

REFUSE AND STORAGE AREAS

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

LIGHTING

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

WALLS AND FENCES

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

STREETSCAPE

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

SIGNAGE

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-

mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or

"lollipop signs."

- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

SERVICE STATIONS AND CAR WASHES

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

AUTO REPAIR SERVICES

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use off similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

CONSUMER STORAGE FACILITIES

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

SPECIAL CONDITIONS

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

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Attachment A-4General Plan Land Use and Zoning Consistency Matrix

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Attachment A-4 General Plan Land Use and Zoning Consistency Matrix

Tulare County Zoning Districts General Plan Land Use Designations	**R-A**	**R-1**	**R-2**	**R-3**	**C-1**	**C-2**	**C-3**	**M-1**	**M-2**	**AE-20**	**AE-40**	**O**	**CO**	**PO**
Urban Reserve Residential	■	■	■	■						■	■			
Low Density Residential	■	■								■	■			
Low-Medium Density Residential	■	■								■	■			
Medium Density Residential		■	■							■	■			
Medium-High Density Residential			■	■						■	■			
High Density Residential				■						■	■			
Neighborhood Commercial					■					■	■			
General Commercial						■				■	■			
Community Commercial						■				■	■			
Highway Commercial					■	■				■	■			
Town Center			■	■	■	■				■	■			
Service Commercial							■			■	■			

Tulare County Zoning Districts	**R-A**	**R-1**	**R-2**	**R-3**	**C-1**	**C-2**	**C-3**	**M-1**	**M-2**	**AE-20**	**AE-40**	**O**	**CO**	**PO**
Office Commercial					■	■	■			■	■			
Commercial Recreation										■	■		■	■
Urban Reserve Commercial					■	■	■			■	■			
Mixed Use		■	■	■	■	■	■	■		■	■	■	■	■
Planned Community Area	■	■	■	■	■	■	■	■		■	■	■	■	■
Light Industrial								■		■	■			
Heavy Industrial									■	■	■			
Urban Reserve Industrial								■	■	■	■	■		
Public/Quasi-Public		■	■	■					■	■	■	■		■
Public Recreation						■	■	■	■	■	■	■		

Attachment A- 5 Funding Source

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CalTrans Active Transportation Program (ATP):
On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation. The projects associated with the Completes Streets Program for the Community of Goshen will be suggested at the next available round of ATP funding.

Tulare County Measure R
On November 7, 2006, the voters of Tulare County approved Measure R, imposing a 1/2 cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than $652 million over 30 years to Tulare County's transportation needs.

Local Projects (35% of Measure R Funding)
The Measure R Expenditure Plan allocated 35% of revenues to local programs. Each city and the county will receive funding based on a formula using population, maintained miles, and vehicles miles traveled. The funding will help cities and the county to meet scheduled maintenance needs and to rehabilitate their aging transportation systems.

Regional Projects (50% of Measure R Funding)
The Regional Projects Program comprises 50% of Measure R and includes specific funding for: interchange improvements, regional bridges, regional signals, regional widening projects, and signal synchronization projects. These projects provide for the movement of goods, services, and people throughout Tulare County. Major highlights of this program include the funding of regional projects throughout the county.

Bike /Transit /Environmental Projects (14% of Measure R Funding)
The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

San Joaquin Valley Air Pollution Control District (SJAPCD) Bike Path Grants
The District has a grants program for the construction of bicycle infrastructure projects, including Class I (Bicycle Path Construction) or Class II (Bicycle Lane Striping) projects. These grants provide funding to assist with the development or expansion of a comprehensive bicycle-transportation network.

Strategic Growth Council Grants (SGC)
Affordable Housing - Sustainable Communities
The SGC will allocate 50% of its Cap and Trade funding toward disadvantaged communities and 50% for affordable housing. Projects will include: affordable housing that supports infill and compact development, transit capital and programs that support transit ridership, active transportation projects (infrastructure, and non-infrastructure), TOD projects, capital projects that implement Complete Streets, projects that reduce CHG emissions by reducing auto trips and VMT, acquisition of easements or other approaches to protect agricultural lands under threat of development, planning to support SCS (sustainable communities scope) implementation, including local plans, must be in draft or adopted SCS, subject to SGC guidelines.

Congestion Mitigation Air Quality (CMAQ) Tulare County Association of Governments (TCAG) Funds
The CMAQ funds are allocated through the TCAG. The CMAQ program funds transportation projects or programs that will contribute to improved air quality standards. Projects include: transportation activities, transportation control measures, public-private partnerships, alternative fuel programs, traffic flow improvements, transit, bicycle/pedestrian projects, rideshare activities, telecommuting, planning, experimental pilot projects, intermodal freight, and public outreach.

DOT: TIGER
TIGER is a multimodal, merit-based discretionary grant program that funds surface transportation capital projects, including transit and rail. Open to state, tribal, local agencies, and subdivisions.

California Deportment Block Grant (CDBG)
The CDBG Economic Development grant provides assistance to local businesses and low-income microenterprise owners to create or preserve jobs for low-income workers in rural communities. Funding includes planning and evaluation studies related to any activity eligible for these allocations, business lending, and public infrastructure.

Choice Neighborhoods
Choice Neighborhoods Planning Grants support the development of comprehensive neighborhood revitalization plans, which focused on directing resources to address three core goals: Housing, People and Neighborhoods. To achieve these core goals, communities must develop and implement a comprehensive neighborhood revitalization strategy, or Transformation Plan. The Transformation Plan will become the guiding document for the revitalization of the public and/or assisted housing units while simultaneously directing the transformation of the surrounding neighborhood and positive outcomes for families. Choice Neighborhoods Implementation Grants support those communities that have undergone a comprehensive local planning process and are ready to implement their "Transformation Plan" to redevelop the neighborhood.

California Department of Water Resources Prop 50 (Contaminant Removal)
Funds are available to disadvantage communities for developing UV or Ozone systems to disinfect drinking water or to set up pilot/demonstration sites.

Drought Response Funding California State Water Resources Control Board (SWRCB)
The Governor and Legislature have directed Department of Water Resource to expedite the solicitation and award of $200 million (of the $472.5 million) in IRWM funding to support projects and programs that provide immediate regional drought preparedness, increase local water supply reliability and the delivery of safe drinking water, assist water suppliers and regions to implement conservation programs and measures that are not locally cost-effective, and/or reduce water quality conflicts or ecosystem conflicts created by the drought.

DWR: Water-Energy Grant Program
The 2014 Water-Energy grant supports the implementation of residential, commercial, and institutional water efficiency programs or projects that reduce Green House Gas emissions and also reduce water and energy use. Funding will go toward urban water management, groundwater management, and surface water diversion.

CDPH Clean Water SRF
The Safe Drinking Water State Revolving Fund (SDWSRF) provides funding to correct public water system deficiencies based upon a prioritized funding approach that addresses the systems' problems that pose public health risks, systems with needs for funding to comply with requirements of the Safe Drinking Water Act, and systems most in need on a per household affordability basis.

iBank (Infrastructure State Revolving Fund Program and Economic Development Bank)
iBank provides low cost, long term financing for local governments to fund a variety of public infrastructure projects. (Although this is not a grant, loan rates are largely determined by level of distress within a disadvantaged community).

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Attachment A- 6 Complete Streets

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Tulare County Complete Streets

Cutler/Orosi

Final

Prepared by:

Tulare County Resource Management Agency

Tulare County Complete Streets – Cutler/Orosi

Prepared for:

Tulare County RMA

5961 S. Mooney Boulevard

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December, 2016

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Appendix H – Orosi 30% Submittal Plan Set

Appendix I - Complete Streets Outreach

Introduction

Complete Streets Vision

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must "meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan."

The Tulare County Resource Management Agency (RMA) is committed to fully integrating modal options in its General Plan and various Community Plans within Tulare County. This includes supporting projects that enhance walking and bicycling infrastructure. Additionally, RMA will improve access to public transportation facilities and services. This includes supporting urban development patterns and Americans with Disabilities Act (ADA) infrastructure that allow for greater accessibility to transit stops and stations. Finally, RMA continues to improve safety for all users and encourages street connectivity to create a comprehensive, integrated and connected circulation network. This is particularly important for those who rely on transportation infrastructure to be physically active and for students who walk or bike to school.

Steady population growth in Tulare County has directly impacted transportation needs. In the past, many of Tulare County's federal, state, and local funding sources were used to develop new or improved traffic signals, interchanges, provide more travel lanes and to maintain existing roadway facilities. Historically, these funding sources have run well short of what is needed. The typical roadway transportation project that adds capacity and infrastructure is insufficient given these conditions. The RMA must adhere to its vision, which is to "provide a safe, convenient and effective County transportation system that enhances mobility and air quality for residents and visitors."

Recent RMA and RMA-supported projects have already fulfilled some of these desires. There are already expanded bus transit routes in the County and more are being constructed for implementation in the near future. New transit centers are being placed throughout the County and efforts are underway to add more bicycle lanes and routes. Recent planning studies are looking to improve roadway safety, pedestrian safety, and access management between roadways and building developments. These efforts are consistent with green house gas (GHG) emissions reductions efforts to reduce vehicle miles travelled (VMT) set forth under SB 375.

Promoting Complete Streets projects can offer Tulare County the ability to reduce traffic congestion, improve air quality, and increase the quality of life of residents by providing safe, convenient, and comfortable routes for walking, bicycling, and public transportation. Integration of Complete Streets into Tulare County's existing policies allows the potential to prevent chronic diseases, reduce motor vehicle related injury and deaths, improve environmental health, stimulate economic development, and ensure access of transportation options for all people in Tulare County.

Complete Streets Definition

Complete Streets are roadways designed to safely and comfortably accommodate all users, regardless of age, ability or mode of transportation. Users include motorists, cyclists, pedestrians and all vehicle types, including public transportation, emergency responders, and freight and delivery trucks among others. In addition to providing safety and access for all users, Complete Street design treatments take into account accommodations for disabled persons as required by the ADA. Design considerations for connectivity and access management are also taken into account for non-motorized users of the facility.

Implementation of Complete Street design treatments will be based on whether it connects the networks for all modes, whether it improves the functionality for all users, and whether it is appropriate given the surrounding context of the community. The final elements of a Complete Street roadway will be largely based on these factors. At a minimum, a Complete Street roadway includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

Complete Streets Attributes

While every street cannot be designed to serve all users equally, there are opportunities to enhance service for all users while maintaining its principal transportation function. Complete Streets incorporate community values and support adjacent land uses while ensuring safety and mobility. Proper applications of Complete Streets concepts support sustainable growth and preservation of scenic, aesthetic and historic resources.

Report Outcomes

As a part of the Circulation Element for the Community Plan Update, this Complete Streets Report (Implementation Work Plan) and the following Implementation and Policies Section achieved the following outcomes:

(i) *Addressed* congestion, climate change and oil dependence by shifting to lower-carbon modes;

(ii) *Improved* safety by addressing shoulders, sidewalks, better bus placement, traffic speed reduction, treatments for travelers with disabilities;

(iii) *Created* "livable communities" by encouraging walking and bicycling for health, and by providing a safe walking and bicycling environment as an essential part of improving transportation movement and safety within the roadways studied.

These outcomes are achieved by the following:

(a) *Included all users* namely, pedestrians, bicyclists, transit vehicles and users, and motorists. In drafting this report, all users were invited to comment on how the County could better serve the community. The implementation of complete streets directly shifts the emphasis to lower-carbon (using) modes of transportation. The shift from the gas using automobile to pedestrian and bicycle transport is achieved through the creation of sidewalks, improving sidewalks, and including bike lanes and/or bike routes for a wider ranger of people to use. The shift to transit is included in improving policies, programs and facilities in the operations of County's transit systems.

(b) *Created a comprehensive integrated and connected network* that supports "livable communities" that promote a safe interwoven fabric are provide for by the Policies Section using the transportation goals in the 2030 General Plan Circulation Element and by further defining complete streets network (see Appendix C).

(c) *Emphasized flexibility* recognizing that all streets with these communities are different, and thus, balancing user needs. No one standard was applied to all streets and the street designs were adjusted to existing conditions, differing jurisdictions and the desires of the community.

(d) *Considered both new and retrofit projects*, including design, planning, maintenance, and operation, for the entire right-of-way within these communities. In addition to the various sections discussed below Appendix A – D include plans that show the plans, designs, and existing and proposed maintenance plans and operations of the Complete Streets Plan.

(e) *Used the latest and best design standards*. By using newer design standards as represented in the preliminary design plans verses the County's Roadway Standards the County is able to provide wider sidewalks and include such amenities as traffic calming measures (bulbouts).

(f) *Conducted extensive public outreach* to ascertain the solutions that best fit within the context of these communities. This culminated in two meetings, wherein the Community provided final feedback on the preliminary designs.

Conclusions and Future Funding Opportunities

The intended effect of identifying the outcomes and reaching the conclusions in this report is that future funding opportunities will be enhanced because the Community will be supported by fully updated Community Plans. The conclusion to the report includes the Circulation Element of the Community Plan including the policies, and plans. The other conclusion to the report includes preliminary design drawings.

Specifically, the funding sources that are found in the Funding Section will be pursued actively by Tulare County to complete the work identified in the studies include, but are not limited to, the following:

- **California Safe Routes to Schools Funds**
- **Federal Safe Routes to Schools Funds**
- **Highway Safety Improvement Funds**
- **Federal Transportation Activity Program (TAP) Funds**
- **Federal Transit Funds**
- **Federal Communities Putting Prevention to Work Grant**
- **Federal Highway Administration Pedestrian Safety and Design**
- **Strategic Growth Council**
- **Walkable and Livable Communities Institute**
- **California's Local Public Health and Built Environment Program**
- **State Cap and Trade Funding**

Implementation

Selection of Communities

An effort is under way in Tulare County to implement Complete Streets Policies in the unincorporated communities within Tulare County's boundary. Just as the County updated its General Plan in 2012, many of the Community Plans are going through the update process. As a result of the Community Plan update process, several public meeting have been held in order to garner input from the local residents and business owners. Balancing the needs of what the people want while following local, state and federal policies and laws with a limited amount of available funding is the principal challenge in each community.

Transportation and related infrastructure costs tend to be exceedingly high may take years to implement. For purposes of this Study, four transportation corridors were selected within the community (see Appendix A), and two roadway segments in the community were selected to be evaluated for implementation of Complete Street standards. These roadway segments generally represent the highest volume roadways with a blend of residential and mixed land uses that also provide for regional access. Local streets and freeways were not selected, however tying into these facilities is considered.

General themes that were voiced from residents in each community related to transportation included the need for:

- Sidewalks
- Better road conditions
- Safe walking and biking areas
- Street lights
- Pedestrian crossings
- Safe (lower) vehicle speeds
- Improved drainage
- Increased transit stops
- Improved connectivity (railroad crossings)

Given the information provided by the residents and business owners, conceptual layouts and designs based upon the citizens concerns were presented to collect input. Based upon the community planning process, the following sections identify proposed projects for each community.

Avenue 416 – SR 63 to Road 140 (Orosi)

As selected as the highest priority in Orosi, Avenue 416, between SR 63 and Road 140, the project proposes to install new curb, gutter, sidewalk, at designated intersections, pedestrian ramps, relocate utilities and drainage. In the spirit of Safe Routes to School, land uses along this corridor include residential, highway commercial (mini-marts, service stations), the Elementary School and other Public and Quasi Public Land Uses, with the school and children as the main focus of the safety improvements. This project will include two travel lanes, a two-way turn lane, cross walks, parallel parking, street lights, improved/new bus stops, street signage and sidewalks with curb and gutter for drainage.

George Road/2nd Drive - Avenue 407 to SR 63 (Cutler)

The George Road/2nd Drive extends between Avenue 407 and SR63 near Cutler School. This project proposes to install new curb, gutter, sidewalk, pedestrian ramps and drainage facilities along portions of

the north and south sides of George Road/2nd Drive. Land uses affected by this project include commercial, residential and quasi-public (school).

Project Phasing

Tulare County RMA is proposing two types of projects coming from the community based upon the complexity of the project. The first types of projects could be built with limited improvement. They would be considered Phase 1 Projects and would have only minor needs for storm drain facilities, fence relocations, utility conflicts, etc. Phase 2 Projects are more inclusive and would be classified as medium to long range projects. These projects would need other infrastructure improvements such as storm water basins, major storm drain improvements, utilities to be undergrounded, Caltrans encroachment permits etc.

Phase 1 Projects	Phase 2 Projects
• Curb, gutter & sidewalk (storm drain water into existing system); pedestrian ramps; bulb outs (where appropriate) • Street lights • Bus shelters, benches, trash receptacles, etc. • Fence relocations • Street signage and striping • Minor utility conflicts • Minor land acquisition	• Curb, gutter & sidewalk (new drainage system) • Major storm drain facilities (new pipelines and storm water basins) • Utility relocations (undergrounding) • Major land acquisition • Railroad crossing improvements • Caltrans Bridge Improvements

Complete Street Policies

Complete Street Goals

The purpose of the RMA Complete Streets Policy is to create a comprehensive and uniform Complete Streets vision and policy for Tulare County. This will allow the implementing entities to incorporate Complete Streets guidelines and standards into both development and redevelopment actions. The County's goals are:

- Tulare County's transportation network will be supported through a variety of feasible transportation choices, which allows for sustainable growth.
- The livability of neighborhoods and commercial centers located along the County's transportation corridors will be enhanced by a safe and inviting pedestrian environment.
- The design of multimodal roadway facilities will not compromise the needs of larger vehicles such as transit vehicles, fire trucks and freight delivery trucks.
- Inclusion of Complete Streets design elements will allow for design flexibility on different street functions and neighborhood contexts.
- Inclusion of Complete Streets design elements will improve the integration of land use and transportation, while encouraging economic revitalization through infrastructure improvements.

Complete Streets Objectives

- To create an integrated and connected transportation network that supports transportation choices and sustainable growth.
- To ensure that all transportation modes are accommodated to the extent possible in all public roadway facilities in the County.
- To develop and use the latest design standards and guidelines in the design of Complete Streets.
- To provide flexibility in the implementation of this policy so that streets chosen for implementation of Complete Streets elements can be developed to fit within the context of their principal purpose and surroundings without compromising the safety of users and needs of larger vehicles.

Complete Streets Policies

Tulare County General Plan Policies

The Tulare County General Plan Update (2030) in complying with AB 1358 calls for 4 Complete Streets related principles including:

Principle 1: County-wide Collaboration

Support countywide transportation plans that provide choices in travel modes.

Principle 2: Connectivity

Emphasize connectivity among cities, communities, and hamlets to ensure County residents have access to jobs and services.

Principle 3: Community Circulation

Anticipate and provide transit, traffic, and roadway connections that support the interconnectivity of all communities.

Principle 4: Pedestrian and Bicycle Facilities

Plan for the development and expansion of pedestrian paths and bicycle facilities that provide residents, with alternative modes of travel.

These principles are expressed mainly in following policies including:

- TC-1.6 Intermodal Connectivity
- TC-1.7 Intermodal Freight Villages
- TC-5.1 Bicycle/Pedestrian Trail System
- TC-5.2 Non-motorized Modes in Planning and Development

Complete Street Policy Design Criteria

1. Tulare County promotes the incorporation of Complete Streets concepts and design standards in all appropriate new and retrofit County public streets (except State highways and freeways).

2. Tulare County will seek every opportunity to provide funding for the planning, design, and implementation of Complete Streets.

3. New Class I Multi-use Paths should be a minimum of eight (8) feet wide.

4. New Class II Bike Lanes should be a minimum of five (5) feet wide.

5. New sidewalks should be a minimum of five (5) feet wide.

6. Bulb-outs should be considered in areas of higher speed (35 mph or greater) where sufficient turning radii for trucks is available or as determined by the County Engineer.

7. As determined by the County Engineer, installation of posted speed limit vehicle activated traffic calming signs (VATCS) are encouraged in instances of high speed to promote safety.

8. Transit shelters and benches are encouraged at all County transit stops if FTA grants are available.

9. Street lighting and cross walk are encouraged to promote safety if considered feasible by the County Engineer.

10. Design policies should be consistent with the Tulare County Improvement Standards; other references include existing design guides, such as those issued by Caltrans, AASHTO and the ADA Accessibility Guidelines.

11. Public streets excluded from this policy include those where:
    - Complete streets concepts are in conflict with existing laws, codes, or ordinances.
    - Compliance with this policy would conflict with goals or physical conditions related to the unique aspects of the location.

12. Exceptions from Complete Street Policies:
    - Accommodation is not necessary where non-motorized use is prohibited, such as freeways.
    - Cost of accommodation is excessively disproportionate to the need or probable use as determined by the County Engineer.
    - A documented absence of current or future need.

Complete Street Mobility Plan

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must "meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan." Essentially, this bill requires a circulation element to plan for multimodal transportation accommodating all modes of transportation where appropriate, including walking, biking, car travel, and transit. The current functional classification system plan is shown in Appendix B (Circulation Plan).

The Complete Streets Act also requires circulation elements to consider the multiple users of the transportation system, including children, adults, seniors, and the disabled. For further clarity, AB 1358 tasks the Governor's Office of Planning and Research to release guidelines for compliance with this legislation by January 1, 2014. Implementation of complete streets principles should be tailored to the individual jurisdiction and the individual roadway. The Complete Streets Program for Tulare County focuses on a network-based approach that has been tailored to the needs of the Community of Cutler/Orosi. Another principle that is being applied is under SB 743, requiring a change to evaluating traffic using Vehicle Miles Traveled verses Level of Service under CEQA analysis, and under AB 32 in reducing Green House Gasses.

Complete Streets: According to the National Complete Streets Coalition, complete streets are a means by which, "… planners and engineers (can) build road networks that are safer, more livable, and welcoming to everyone…. Instituting a complete streets policy ensures that transportation planners and engineers consistently design and operate the entire roadway with all users in mind – including bicyclists, public transportation vehicles and riders, and pedestrians of all ages and abilities."

Network-Based Complete Streets: Combines individual travel mode networks into one multimodal transportation system, integrating infrastructure where appropriate, ultimately ensuring that all users can safely and efficiently access their destination.

Vehicle Miles Traveled (VMT): Vehicle miles traveled is the metric that identifies the total distance traveled in a car per driver. VMT drives roadway needs (the more people who drive, the more capacity and maintenance are needed on the roadway system). Under the Tulare County Climate Action Plan, in reducing VMT green house gas emissions are reduced and the County has an overall target of reducing 6% of its green house gas emissions through a reduction in VMT.

Community Plans adopt these principles, which are combined into the following mission statement:

> *The Community Complete Streets Network comprises four types of facilities—vehicular, pedestrian, bicycle, and public transit. This complete streets approach will enable residents to choose which travel mode best suits them. It also will ensure that streets are designed with the users in mind—accommodating for businesses, children, the elderly, bicyclists, and transit users.*

Caltrans and Complete Streets

Under Caltrans District Order 64-R1, Caltrans requires that a Complete Streets Implementation Action Plan be developed and implemented for Caltrans owned and maintained Streets. Their Implementation Action plan provides a background by which the Tulare County Completes Street Plan will be implemented.

TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS)

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Cutler/Orosi. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describe bicycle circulation patterns and Pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

Tulare County Climate Action Plan (CAP)

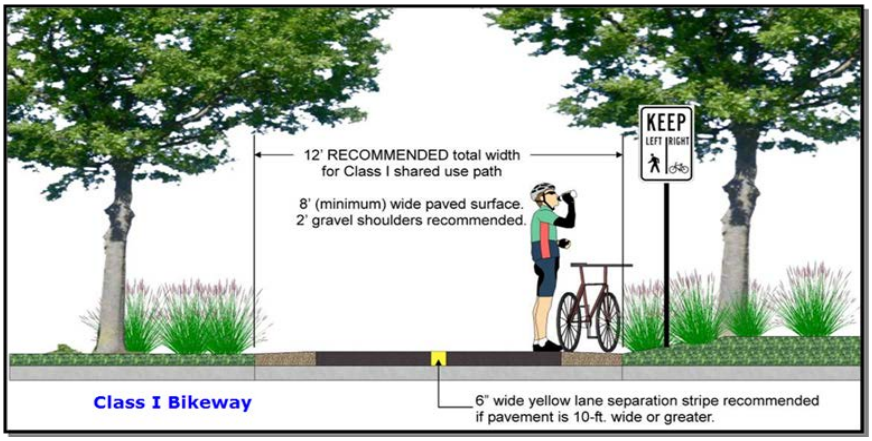
The Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% trough a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets.

Bicycle Facilities

Bicycle facilities consist of Class I, Class II, and Class III facilities as defined below. In Tulare County, this General Plan and the Bicycle Transportation Plan envision a system of bicycle lanes on roadways that will connect the activity centers of the communities to the residents. County has identified pedestrian corridors on the Community of Cutler/Orosi Bicycle, Bus and Pedestrian Plan (see Appendix C).

Class I

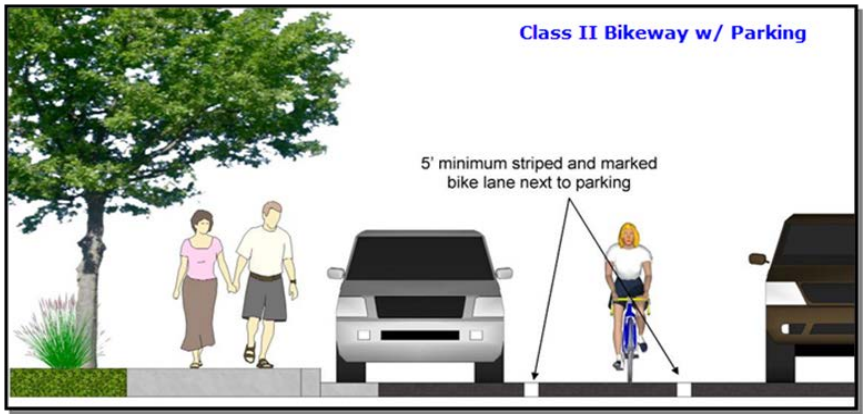
Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County, Class I facilities will primarily be implemented through TCAG. Future bicycle facilities have also been identified through the *Bicycle Transportation Plan* (TCAG - 2010). There is a proposed Class I on Ave. 416.



bicycle facilities in Cutler/Orosi.

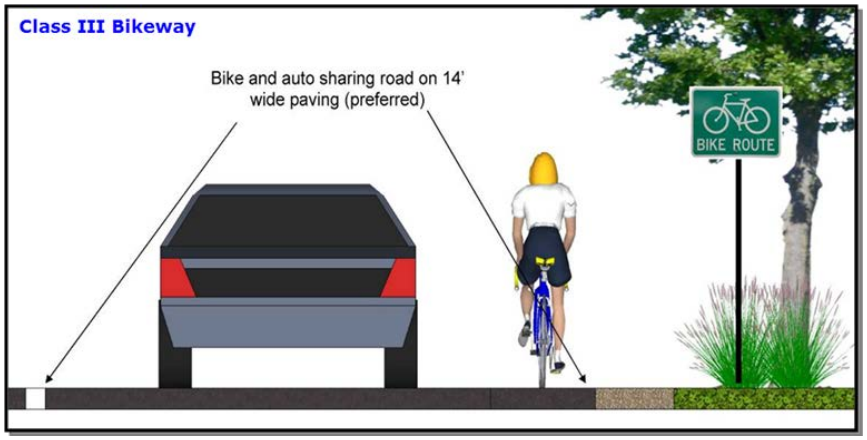
Class II

Bikeway that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. For the purposes of Complete Streets, the County is not proposing Class II bicycle facilities.



Class III

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III facilities are not proposed.



Although not signed on many local roads in Cutler/Orosi, bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways.

Pedestrian Facilities

Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

Multiuse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. No multiuse trails exist or are proposed in Cutler/Orosi.

Transit Facilities

Transit options give users the ability to get to a destination without relying on the automobile. This also provides other community benefits, including reduced vehicle miles traveled (VMT). Reducing VMT will help the County achieve their greenhouse gas reduction target,

Pass Sales Outlets

Dinuba Transit Center	189 Merced St., Dinuba
Porterville Transit Center	35 W. Oak St., Porterville
Tulare County Government Plaza	5961 S. Mooney Blvd., Visalia
Tulare Transit Center	360 N. 'K' St., Tulare
Visalia Transit Center	Oak & Santa Fe, Visalia

Tulare County Area Tranist (TCAT)

Public transportation services and facilities in Tulare County consist of public bus service, paratransit service, and could also consider park-and-ride locations.

Public Bus Service

Public bus service is provided by Tulare County Area Transit (TCAT) in rural areas such as Cutler/Orosi and by local City transit agencies in transitioning areas, which enables commuters to travel within the communities and adjacent cities with minimal transfers. Existing transit routes and designated bus stops are shown in the following figures.

Transit service is provided in Cutler/Orosi through the Tulare County Area Transit (TCAT). Additionally, Tulare County has provided guidance for including transit within facilities. These guidelines should be applied when considering new development to ensure appropriate connectivity and design features to support bus service.

Paratransit Service

Paratransit is an alternative mode of passenger transportation that does not follow fixed routes or schedules. Typically, vans or minibuses are used to provide paratransit service. Paratransit services vary considerably on the degree of flexibility they provide their customers. The most flexible systems offer on-demand, call-up, door to door service from any origin to any destination in a service area.

Park-and-Ride Lots

Park-and-ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the community's population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community along State Route 65.

Cost Benefits Analysis, Implementation, and Funding Mechanisms

Caltrans lists the following benefits of Complete Streets in their implementation plan. They include:

- Increased Transportation Choices: Streets that provide travel choices can give people the option to avoid traffic congestion, and increase the overall capacity of the transportation network.
- Economic Revitalization: Complete streets can reduce transportation costs and travel time while increasing property values and job growth in communities.
- Improved Return on Infrastructure Investments: Integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project spares the expense of retrofits later.

- Quality of Place: Increased bicycling and walking are indicative of vibrant and livable communities.
- Improved Safety: Design and accommodation for bicyclists and pedestrians reduces the incidence of crashes.
- More Walking and Bicycling: Public health experts are encouraging walking and bicycling as a response to the obesity epidemic. Streets that provide room for bicycling and walking help children get physical activity and gain independence.

Benefits of Complete Streets

The health benefits from walking and bicycle riding include increased overall health, and a reduction in air quality and green house emissions. According to the Caltrans accepted, Victoria Transport Policy Institute, walking has a $.25 per mile health benefit, while the cost of Greenhouse Gas (GHG) reductions is $23 per ton. According to the Federal Highway Administration, sidewalks reduce incidences to pedestrians over 80%.[1] According to Caltrans, the average costs of highway incidents are stated below.

Cost of Highway Accident	**Dollars Per Accident**
Fatal Accident	$4,800,000
Injury Accident	$67,400
Property Damage Only (PDO) Accident	$10,200
Average Cost per Accident	$52,500
Cost of an Event	**Dollars Per Event**
Cost of a Fatality	$4,400,000
Cost of an Injury	
Level A (Severe)	$221,400
Level B (Moderate)	$56,500
Level C (Minor)	$26,900
Cost of Property Damage	$2,500

Source: California Department of Transportation

The collision history within the Cutler/Orosi area can be traced using the Transportation Injury Management System hosted by UC Berkeley SafeTrec. When reviewing the 5 year (2009-2013) injury collision history, a total of 96 collisions have occurred involving a pedestrian or bicyclist and a motor vehicle. (See Figure below).

[1] http://www.dot.ca.gov/hq/tpp/offices/eab/benefit_cost/LCBCA-economic_parameters.html

Source: Transportation Injury Management System, UC Berkeley 2009-2013

Since there are currently no sidewalks many pedestrians walk in the street. Vehicles cannot distinguish roadway with walking paths when curbs do not exist. Any amount of collisions involving pedestrians or bicyclists should be of great concern, especially adjacent to a local school.

Factors such as speed, visibility and general pedestrian/bicyclist awareness are identified as contributing factors relating to the incidents. As a result, the project was developed with these specific factors in mind. The proposed design improvements have been proven to be effective and have also been proven to reduce speeds, increase visibility and increase overall awareness of pedestrian activity.

Community Specific Complete Street Implementation Measures

As part of a network-based approach, the County has identified (and will implement through pursuing further roadway studies and infrastructure design updates) a complete network for pedestrians. The County will also work to deliver infrastructure to support all modes of transportation. In addition to the General Plan Circulation Element Implementation Section, the key implementation measures include:

1. Evaluating Roadways as potential Bike/Pedestrian travel routes,
2. Completing pedestrian infrastructure, as appropriate,
3. Providing safe and accessible pedestrian facilities in high use areas,
4. Designing and building sidewalks for safer routes to school,
5. Designating roadways for bicycle routes that are aligned with the Tulare County comprehensive bicycle network,
6. Coordination with County Transit, and,
7. Submitting the following list of project and cost to TCAG and Caltrans for consideration under further grant funding opportunities.

Measure R

Bike/Transit/Environmental Projects (14% of Measure R Funding)

On November 7, 2006, the voters of Tulare County approved Measure R, imposing a ½ cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than $652 million over 30 years to Tulare County's transportation needs.

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

Active Transportation Program (ATP)

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation.

Citizen Feedback

Public Outreach Efforts

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues.

Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Agency Complete Streets and Community Plan Outreach (2015) – is located in Appendix I.

Cutler/Orosi Public Meetings

- Complete Streets Meeting February 2, 2015
- Complete Streets Meeting March 3, 2015

Community feedback was gathered at the February and March meetings and incorporated into the design of the Complete Street Plans and further discussed in the February and March, 2015 meetings to receive further community feedback. These designs were edited to include feasible improvements and cost estimates were assigned to each project within the respective community for each study roadway segment.

Design Facilities

Improvement Standards

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Transportation related facilities for public use are built within existing right of way (R/W) owned by a public agency, e.g., county, city or state. Within this R/W is a standard cross section, which is a term that is used to define the configuration of existing or proposed roadways at right angles to the centerline (CL). Typical sections show the width, thickness and descriptions of the pavement section, as well as the geometrics of the graded roadbed, side improvements and side slopes.

In Tulare County, the two most common cross sections are shown for two or four lane roads, varying in width based upon the number of lanes, parking, sidewalks, shoulders, bike lanes, etc. Figure 1 shows the cross section for two lane roads and Figure 2 identifies a typical four lane cross section.

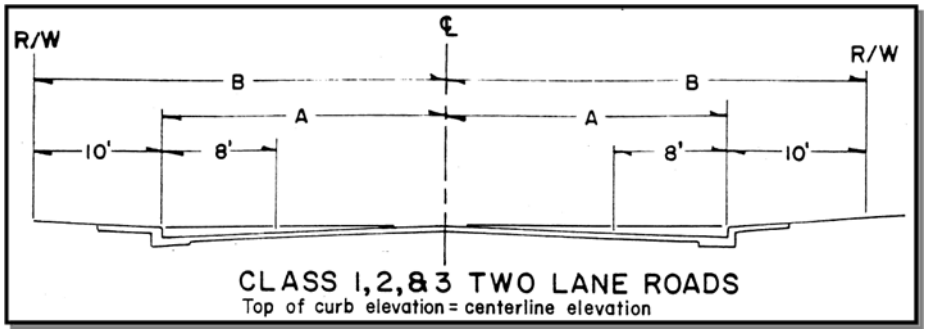


Figure 1 - Tulare County Class 1, 2 & 3 Two Lane Roads

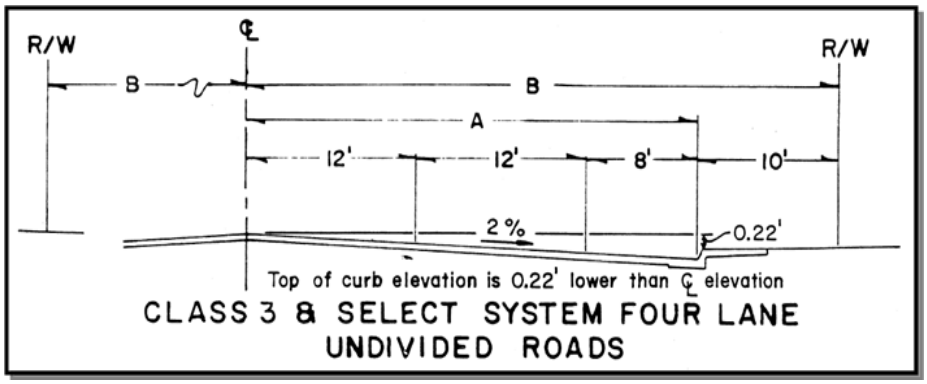


Figure 2 - Tulare County Class 3 Four Lane Road

Tulare County Pavement Management System

Pavement Management

Pavement management is the process of planning the maintenance and repair of a network of roadways or other paved facilities in order to optimize pavement conditions over the entire network. Pavement management incorporates life cycle costs into a more systematic approach to minor and major road maintenance and reconstruction projects. The needs of the entire network as well as budget projections are considered before projects are executed. Pavement management encompasses the many aspects and tasks needed to maintain a quality pavement inventory, and ensure that the overall condition of the road network can be sustained at desired levels.

Pavement Management System

The Tulare County Pavement Management System (PMS) is a planning tool used to aid pavement management decisions. PMS software programs model future pavement deterioration due to traffic and weather, and recommend maintenance and repairs to the road's pavement based on the type and age of the pavement and various measures of existing pavement quality. Measurements can be made by persons on the ground, visually from a moving vehicle, or using automated sensors mounted to a vehicle. PMS software assists RMA staff to create composite pavement quality rankings based on pavement quality measures on roads or road sections. Recommendations are usually biased towards preventive maintenance, rather than allowing a road to deteriorate until it needs more extensive reconstruction.

Typical tasks performed by Tulare County PMS include:

- Inventory pavement conditions, identifying good, fair and poor pavements;
- Assign importance ratings for road segments, based on traffic volumes, road functional class, and community demand;
- Schedule maintenance of good roads to keep them in good condition; and,
- Schedule repairs of poor and fair pavements as remaining available funding allows.

Research has shown that it is far less expensive to keep a road in good condition than it is to repair it once it has deteriorated. This is why pavement management systems place the priority on preventive maintenance of roads in good condition, rather than reconstructing roads in poor condition. In terms of lifetime cost and long term pavement conditions, this will result in better system performance.

The County is proposing a Road Maintenance Plan (see Appendix D) for the community of Cutler/Orosi that is a result of the PMS.

Projects

Complete Streets Project Plans

The plans and projects in the appendices are identified as part of the complete streets policy to identify corridors for various user types and to demonstrate examples of design policies. These plans and are the result of input obtained through the community outreach process, multiple Tulare County agencies and divisions and professional engineering consultants.

The six projects on Orosi and five projects in Cutler have been identified herein that represent the priority improvements to the backbone of the complete streets network within Cutler/Orosi. One project in Cutler and one project in Orosi will be developed to a 30% design stage and the remaining four projects have been preliminarily scoped and budgetary estimates have been prepared. These projects were developed to provide the County and various funding agencies with a list of projects to move toward funding, design, and ultimately construction.

Orosi

1. Avenue 416 – SR 63 to Road 140 (East Orosi)
2. Avenue 413 – Road 124 to SR 63
3. Avenue 419
4. Avenue 416 – SR-63 to Dinuba
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

General Road Concerns

1. Flooding: Intersection of Orosi Drive and 2nd Drive.
2. Stop Sign: Intersection of Road 124 at Aceves Avenue.
3. Traffic Speeding Concerns: Road 124 (Avenue 408 to Avenue 416).
4. Request for additional Crosswalks: Road 127 (El Monte School, Students cross mid-block at multiple locations primarily north of the school).
5. Sight Distance Problem with parked vehicles: Road 127 at Avenue 416 (Northbound left turn from Road 127 to westbound Ave 416).
6. Traffic Signal at Orosi High School: SR 63 (Road 128)
7. Traffic Speeding: Avenue 416 (There has been an accident along this road).
8. Lights & Sidewalks: Avenue 63.
9. Sidewalks: Avenue 63 and Ella Avenue.

Complete Streets Funding Opportunities

The following sections identify opinions of probable cost estimates for Complete Street transportation related improvements in Cutler/Orosi. As shown in the tables, the funding sources include local, state and federal programs. Typically, local matches are required for acquiring state and federal funds. Measure R, a Tulare County sales tax for transportation, is available for such matches.

Cost Estimates

Detailed cost estimates are included in Appendix E and G.

Appendix A –

Proposed Complete Streets Projects

Orosi

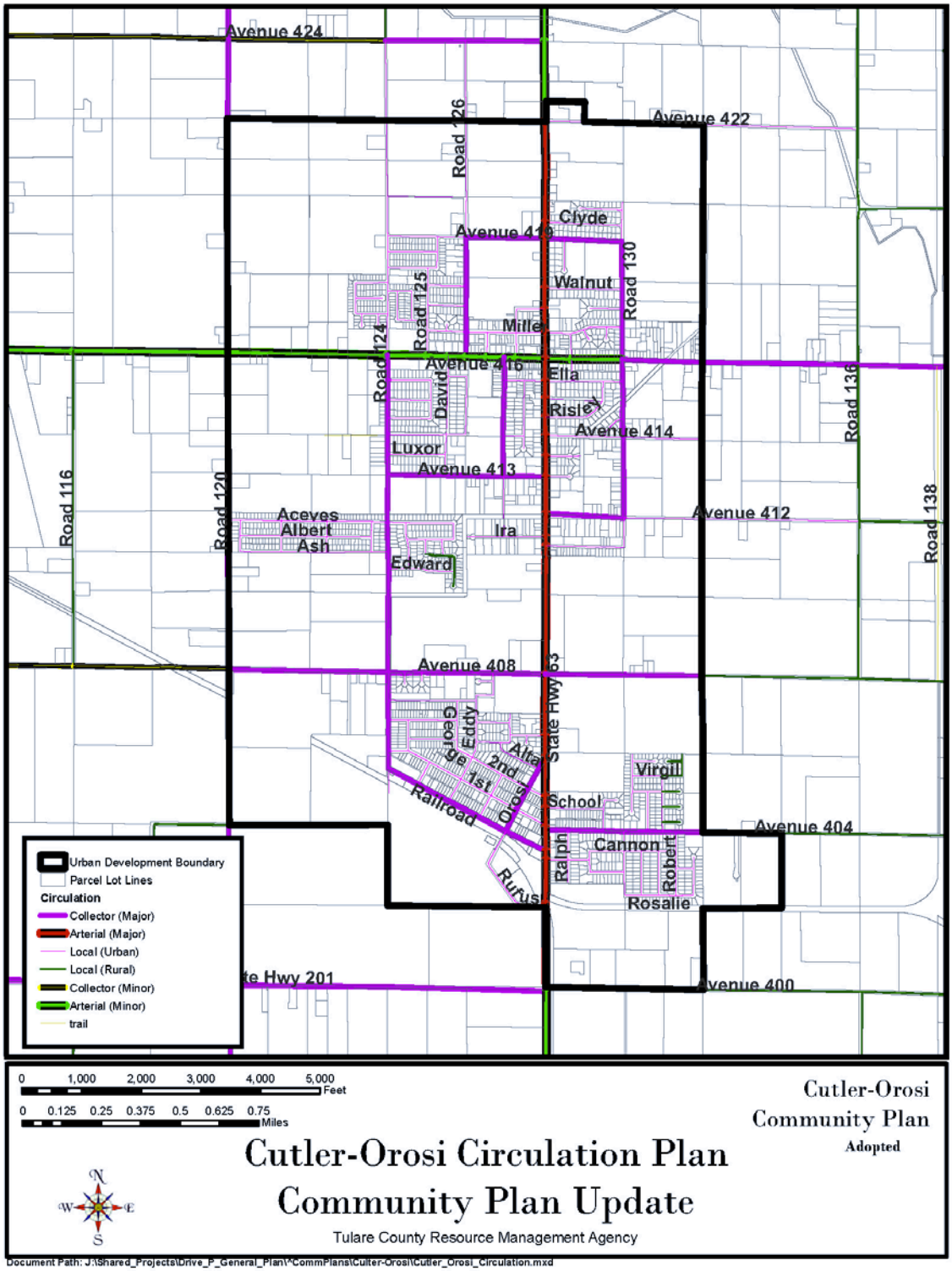
1. Avenue 416 – SR 63 to Road 140 (East Orosi)
2. Avenue 413 – Road 124 to SR 63
3. Avenue 419
4. Avenue 416 – SR-63 to Dinuba
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

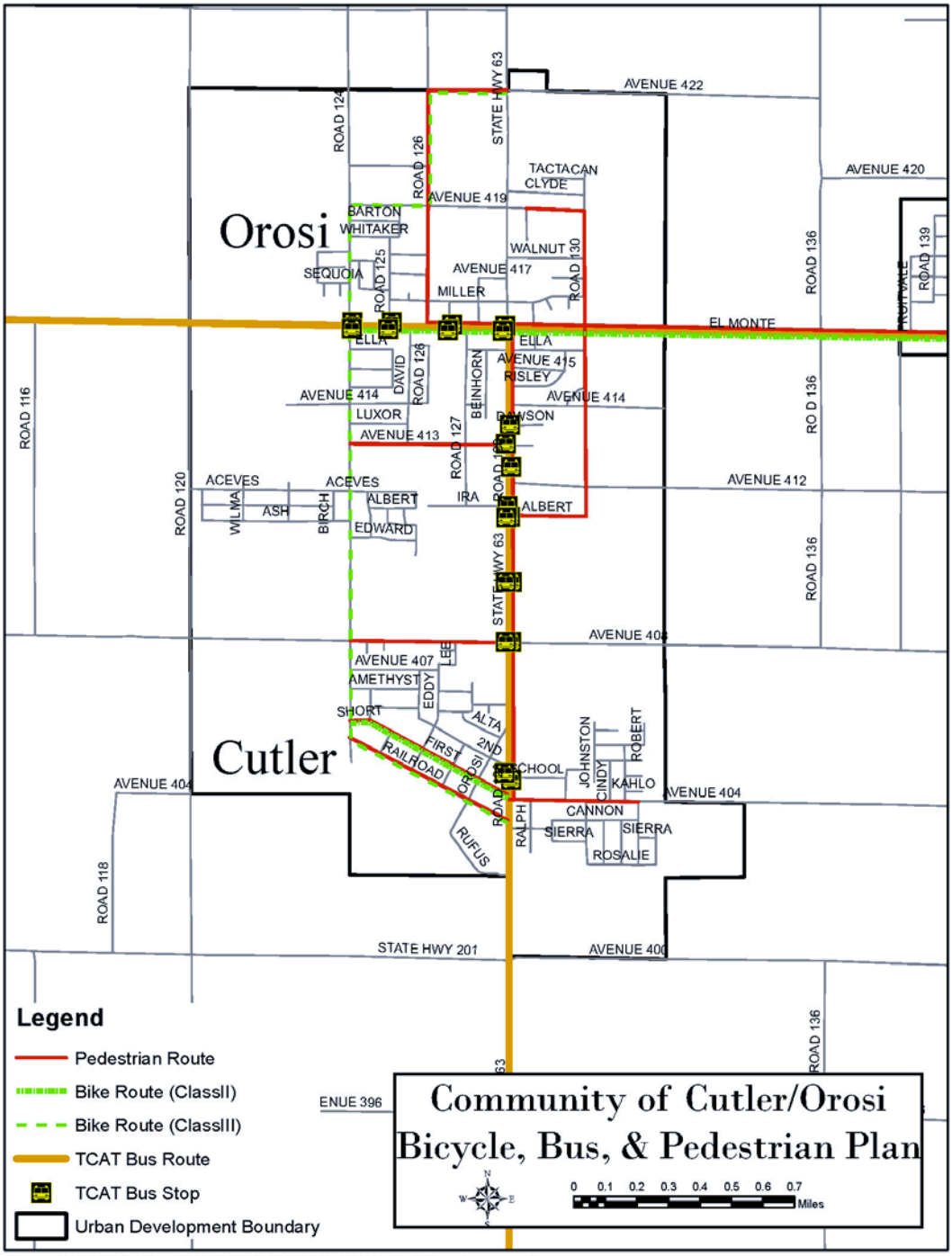
Appendix B –

Circulation Plan

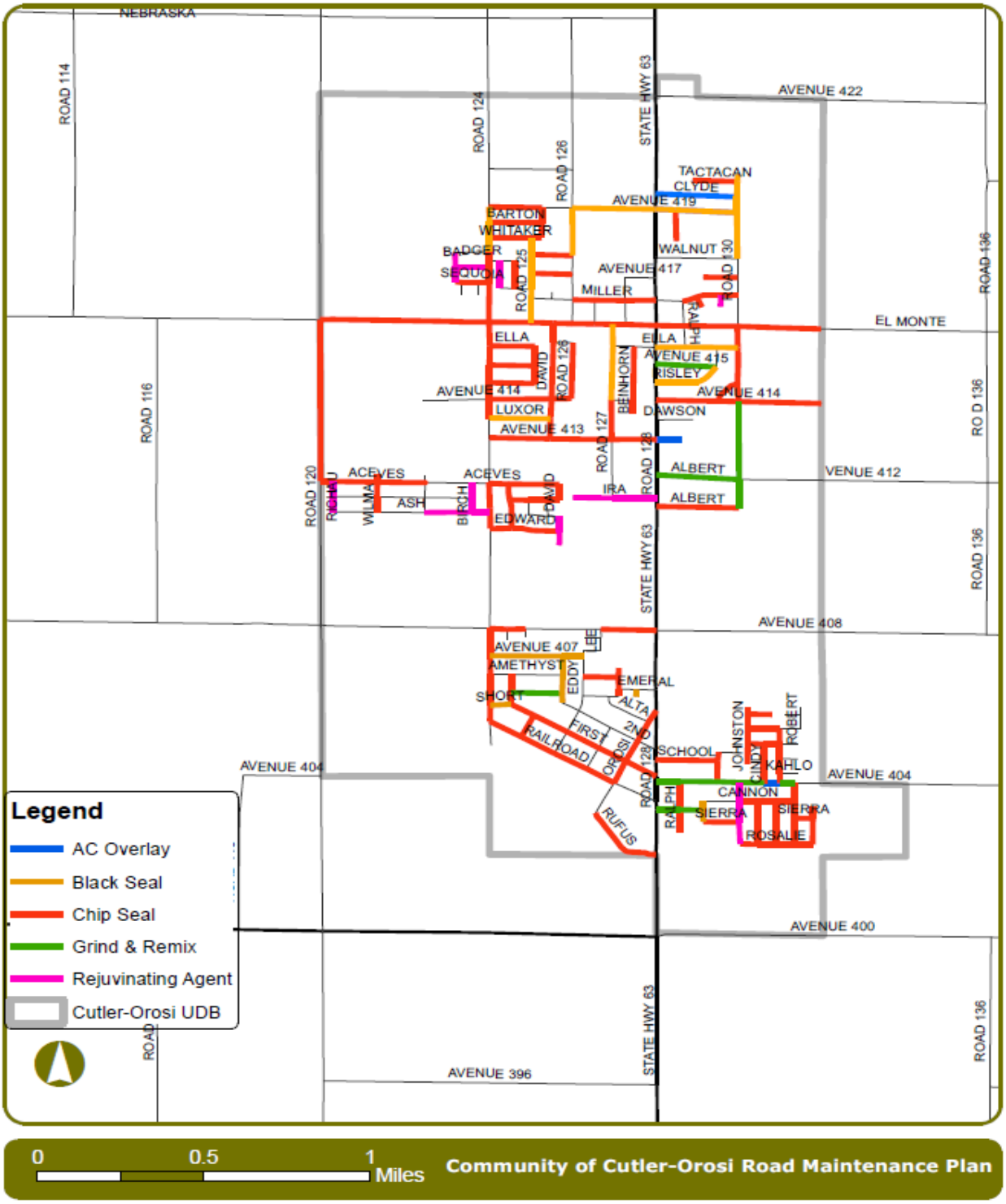


Appendix C –

Bicycle, Bus, and Pedestrian Plan



Appendix D
Road Maintenance Plan



Appendix E

Cost Estimates for Cutler

PRELIMINARY COST ESTIMATE (30-Percent)						
Cutler Complete Street Improvements				Date:	10/21/2015	
County of Tulare						
Construction						
ITEM		**ITEM DESCRIPTION**	**UNIT OF**	**ESTIMATED**	**ITEM**	**TOTAL**
NO	**(F)**		**MEASURE**	**QUANTITY**	**PRICE**	
1	-	MOBILIZATION	LS	1	$ 148 900.00	$ 148 900
2	-	JOB SITE MANAGEMENT	LS	1	$ 8 000.00	$ 8 000
3	-	WATER POLLUTION CONTROL PROGRAM	LS	1	$ 4 500.00	$ 4 500
4	-	CONSTRUCTION AREA SIGNS	LS	1	$ 7 500.00	$ 7 500
5	-	TRAFFIC CONTROL SYSTEM	LS	1	$ 30 000.00	$ 30 000
6	-	RELOCATE FENCE (CHAIN LINK)	LF	772	$ 50.00	$ 38 620
7	-	RELOCATE FENCE (IRON)	LF	896	$ 90.00	$ 80 675
8	-	RELOCATE FENCE (BRICK AND IRON)	LF	99	$ 150.00	$ 14 856
9	-	RELOCATE POWER POLE	EA	1	$ 40 000.00	$ 40 000
10	-	RELOCATE DRAIN INLET	EA	2	$ 1 500.00	$ 3 000
11	-	RELOCATE SIGN	EA	4	$ 300.00	$ 1 200
12	-	RELOCATE FIRE HYDRANT	EA	1	$ 6 000.00	$ 6 000
13	-	ADJUST WATER VALVE COVER TO GRADE	EA	9	$ 800.00	$ 7 200
14	-	ADJUST WATER METER BOX TO GRADE	EA	9	$ 3 500.00	$ 31 500
15	-	ADJUST FIRE HYDRANT TO GRADE	EA	3	$ 5 500.00	$ 16 500
16	-	ADJUST MANHOLE TO GRADE	EA	10	$ 1 200.00	$ 12 000
17	-	ADJUST DRAIN INLET TO GRADE	EA	1	$ 1 500.00	$ 1 500
18	-	RESET MAILBOX	EA	18	$ 300.00	$ 5 400
19	-	REMOVE TREE	EA	1	$ 800.00	$ 800
20	-	CLEARING AND GRUBBING	LS	1	$ 10 000.00	$ 10 000
21	(F)	ROADWAY EXCAVATION	CY	3 989	$ 60.00	$ 239 320
22	(F)	CLASS 2 AGGREGATE BASE	CY	2 384	$ 60.00	$ 143 048
23	-	HOT MIX ASPHALT (TYPE A)	TON	1 724	$ 110.00	$ 189 666
24	-	48" MANHOLE	EA	11	$ 6 000.00	$ 66 000
25	-	MINOR CONCRETE (CURB AND GUTTER)	LF	4 280	$ 25.00	$ 107 009
26	-	MINOR CONCRETE (SIDEWALK)	SQFT	17 056	$ 7.00	$ 119 393
27	-	MINOR CONCRETE (DRIVEWAYS AND CURB RAMPS)	SQFT	6 258	$ 15.00	$ 93 875
28	-	DETECTABLE WARNING SURFACE	SQFT	245	$ 45.00	$ 11 025
29	-	SIGNING & STRIPING	LS	1	$ 15 000.00	$ 15 000
30	-	EROSION CONTROL	LS	1	$ 5 000.00	$ 5 000
31	-	MISCELLANEOUS ITEMS	LS	1	$ 327 146.00	$ 327 146
		Total - Construction Items 1-31				**$1 784 631**
		Contingency (25%)				$ 446 158
		Recommended Total Construction Budget				**$2 230 789**
Non-Construction Related Costs						
ITEM		**ITEM DESCRIPTION**	**UNIT OF**	**ESTIMATED**	**ITEM**	**TOTAL**
NO	**(F)**		**MEASURE**	**QUANTITY**	**PRICE**	
32	-	Environmental Clearance	% of CON	5%	$1 784 631.36	$ 89 232
33	-	Right of Way Acquisition (Capital)	SQFT	1 081	$ 5.00	$ 5 403
34	-	Right of Way Acquisition (Support)	Parcel	5	$ 5 000.00	$ 25 000
35	-	Final Engineering Design	% of CON	15%	$1 784 631.36	$ 267 695
36	-	Construction Support	% of CON	2%	$1 784 631.36	$ 35 693
37	-	Construction Management	% of CON	15%	$1 784 631.36	$ 267 695
38	-	Utility Relocations	LS	1	$ 75 000.00	$ 75 000
		Total - Non-Construction Items 33-38				**$ 765 717**
		Total Construction & Non-Construction Items				**$2 996 506**

Preliminary Cost Estimate (30-Percent)

Avenue 408, Cutler

Tulare County Complete Streets

6/9/2016

55-7300-14/2144

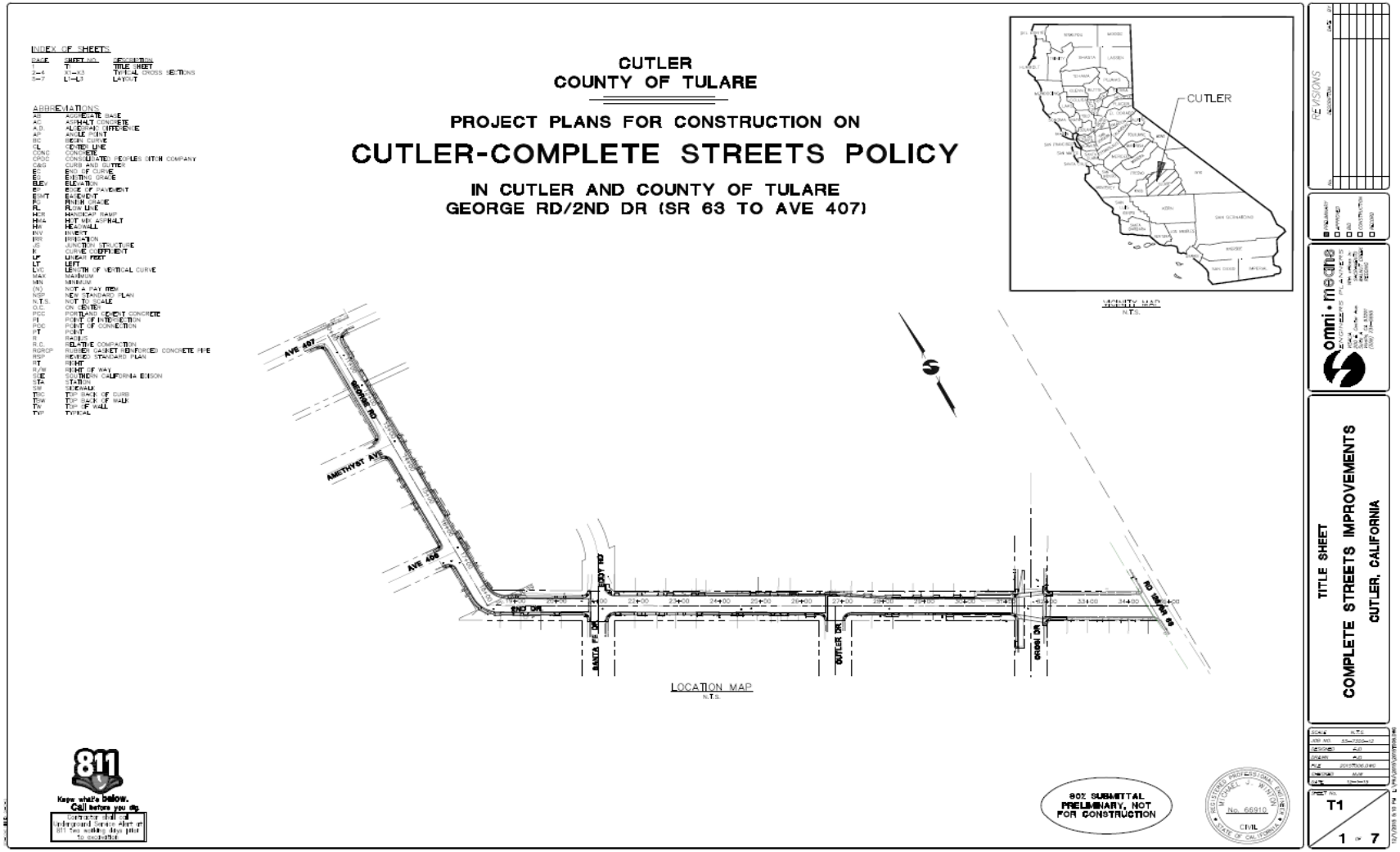
Construction Costs:

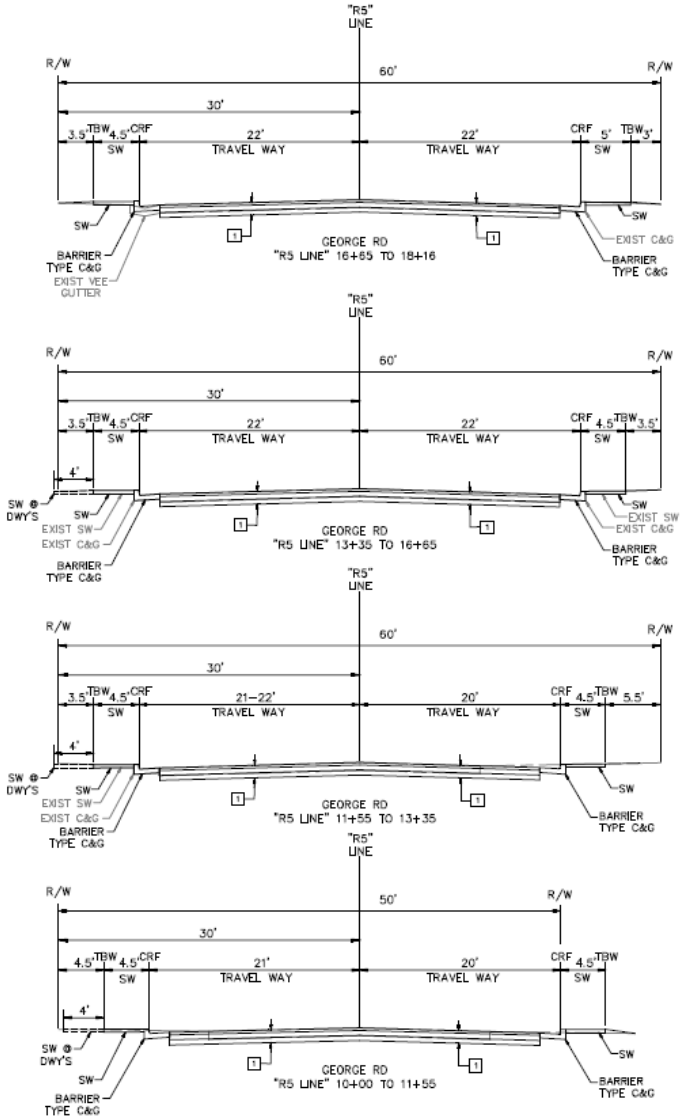
No.	Item Description	Units	Quantity	Unit Cost	Total
1	Job Site Management	LS	1	$8,000.00	$8,000.00
2	Water Pollution Control Program	LS	1	$4,500.00	$4,500.00
3	Construction Area Signs	LS	1	$7,500.00	$7,500.00
4	Traffic Control	LS	1	$30,000.00	$30,000.00
5	Remove Chain Link Fence	LF	31	$12.00	$372.00
6	Remove Wooden Fence	LF	12	$20.00	$240.00
7	Remove Traffic Stripe	LF	2992	$1.00	$2,992.00
8	Remove Pavement Marking	SQFT	456	$2.70	$1,231.20
9	Reconstruct Drain Inlet	EA	2	$3,500.00	$7,000.00
10	Remove Sign	EA	16	$130.00	$2,080.00
11	Remove Irrigation Standpipe	EA	1	$1,500.00	$1,500.00
12	Remove Metal Post	EA	3	$75.00	$225.00
13	Remove Concrete (Curb & Gutter)	LF	2307	$15.00	$34,605.00
14	Remove Concrete Sidewalk	SQFT	3787	$4.50	$17,041.50
15	Relocate Fence (Chain Link)	LF	33	$80.00	$2,640.00
16	Reset Mailbox	EA	1	$300.00	$300.00
17	Adjust Manhole to Grade	EA	6	$1,200.00	$7,200.00
18	Adjust Fire Hydrant to Grade	EA	4	$5,500.00	$22,000.00
19	Relocate Fire Hydrant	EA	2	$6,000.00	$12,000.00
20	Adjust Water Meter Box to Grade	EA	3	$800.00	$2,400.00
21	Adjust Water Valve Frame & Cover to Grade	EA	4	$1,200.00	$4,800.00
22	Clearing and Grubbing	LS	1	$10,000.00	$10,000.00
23	Remove Bollard	EA	3	$100.00	$300.00
24	Roadway Excavation (F)	CY	1150	$60.00	$69,000.00
25	Class 2 Aggregate Base (F)	CY	790	$60.00	$47,400.00
26	Hot Mix Asphalt (Type B)	TON	630	$110.00	$69,300.00
27	Roadside Sign - One Post	EA	15	$250.00	$3,750.00
28	Roadside Sign - Two Posts	EA	1	$400.00	$400.00
29	Paint Curb	SQFT	83	$2.25	$186.75
30	Storm Drain System	LS	1	$75,000.00	$75,000.00
31	Minor Concrete (Curb)	LF	156	$22.00	$3,432.00
32	Minor Concrete (Gutter)	SF	381	$10.00	$3,806.10
33	Detectable Warning Surface	SQFT	297	$45.00	$13,365.00
34	Minor Concrete (Curb and Gutter)	LF	5003	$25.00	$125,075.00
35	Minor Concrete (Sidewalk)	SQFT	34516	$7.00	$241,612.00
36	Minor Concrete (Driveway & Curb Ramps)	SQFT	2367	$15.00	$35,505.00
37	Thermoplastic Traffic Stripe	LF	3347	$1.25	$4,183.75
38	Thermoplastic Pavement Marking	SQFT	600	$6.00	$3,600.00
39	Erosion Control	LS	1	$5,000.00	$5,000.00
40	Mobilization	LS	1	$82,500.00	$82,500.00
41	Minor/ Supplemental Items	%	25%	$879,542.30	$219,885.58
	Subtotal (Construction Costs)				$1,161,927.88
	Construction Contingency			25%	$ 290,481.97
	Total Construction Costs				**$1,452,409.84**
	Total Construction Budget (Rounded)				**$1,452,500.00**

Non-Construction Related Costs:					
No.	***Item Description***	***Units***	***Quantity***	***Unit Cost***	***Total***
42	Environmental Clearance	% of CON	5%	$1,162,000.00	$58,100.00
43	Right of Way/TCE Acquisition (Capital)	SQFT	1630	$5.00	$8,150.00
44	Right of Way/TCE Acquisition (Support)	Parcel	2	$5,000.00	$10,000.00
45	Final Engineering Design	% of CON	15%	$1,162,000.00	$174,300.00
46	Construction Support	% of CON	2%	$1,162,000.00	$23,240.00
47	Construction Management	% of CON	15%	$1,162,000.00	$174,300.00
48	Utility Relocations	LS	1	$100,000.00	$100,000.00
	Total Non-Construction Related Costs				**$548,090.00**
	Total Project Capital Cost				**$ 2,000,590.00**
	Rounded				**$ 2,000,000.00**

Appendix F

Cutler 30% Submittal Plan Set





TYPICAL STRUCTURAL SECTIONS:

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0.25' HMA (TYPE B)
0.70' AB (CLASS 2)
0.50' COMPACTED NATIVE

307 SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

omni means
ENGINEERS PLANNERS

TYPICAL SECTIONS
COMPLETE STREETS IMPROVEMENTS
CUTLER, CALIFORNIA

SCALE 1" = 5'
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DRAWN F.O
CHECKED M.W
DATE 12-1-15

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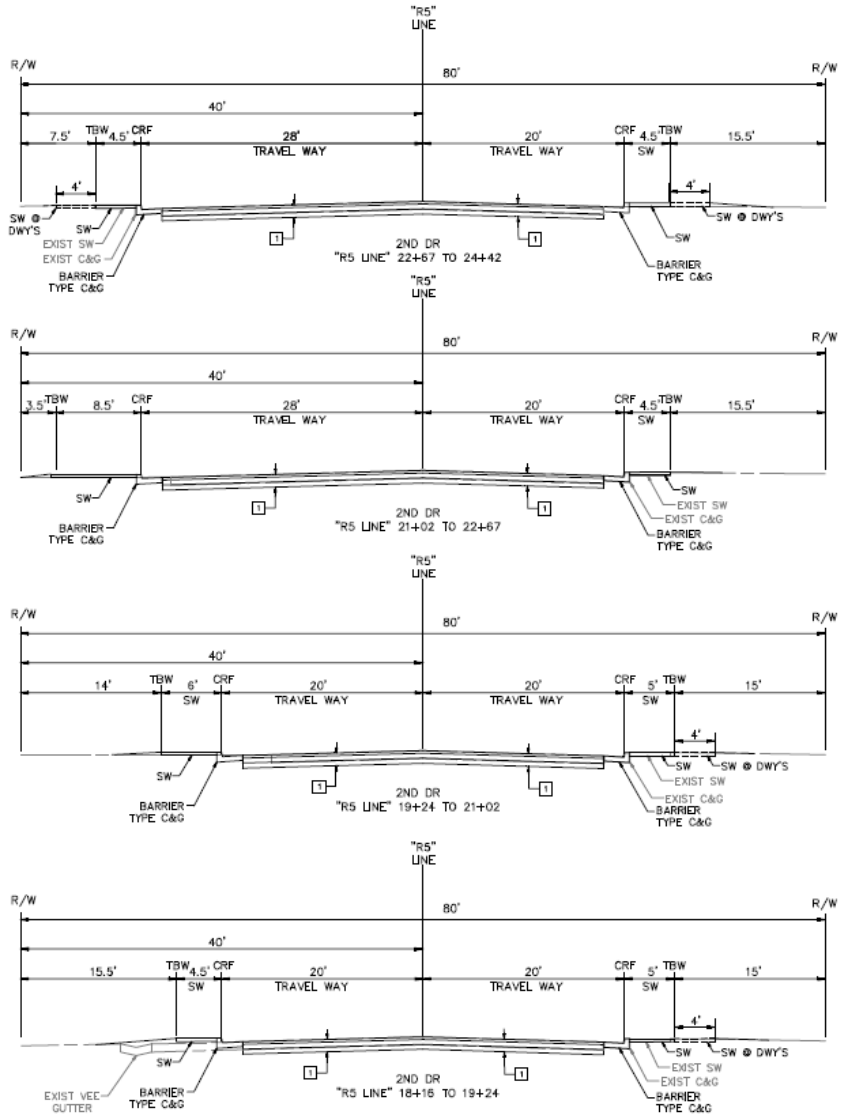
X1
2 OF 7



TYPICAL STRUCTURAL SECTIONS:

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0.70' AB (CLASS 2)
0.50' COMPACTED NATIVE

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WILTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

omni·means
ENGINEERS PLANNERS

TYPICAL SECTIONS
COMPLETE STREETS IMPROVEMENTS
CUTLER, CALIFORNIA

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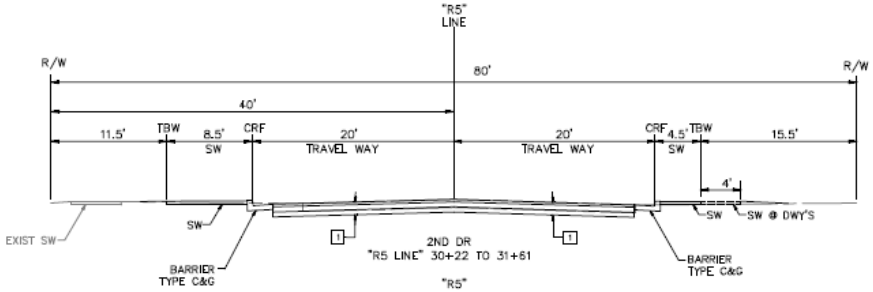

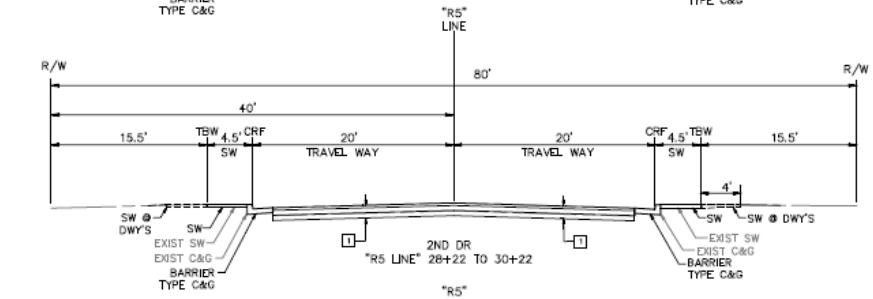

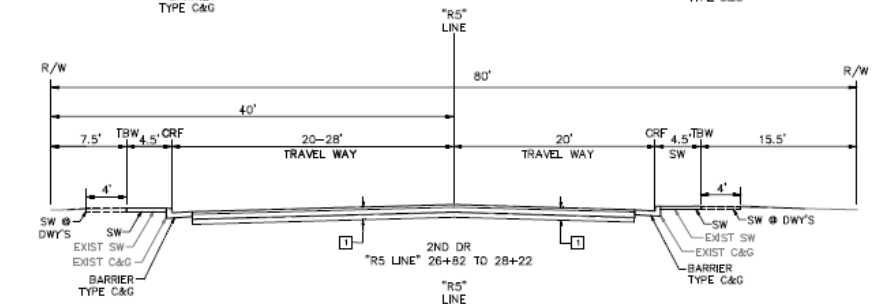

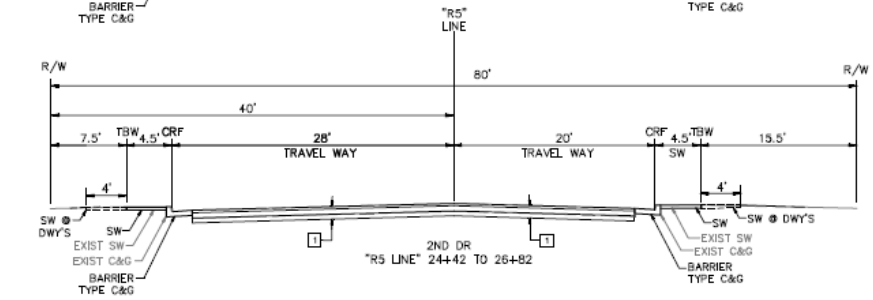

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30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WEITZEL
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CIVIL
STATE OF CALIFORNIA

REVISIONS

omni-means

TYPICAL SECTIONS
COMPLETE STREETS IMPROVEMENTS
CUTLER, CALIFORNIA

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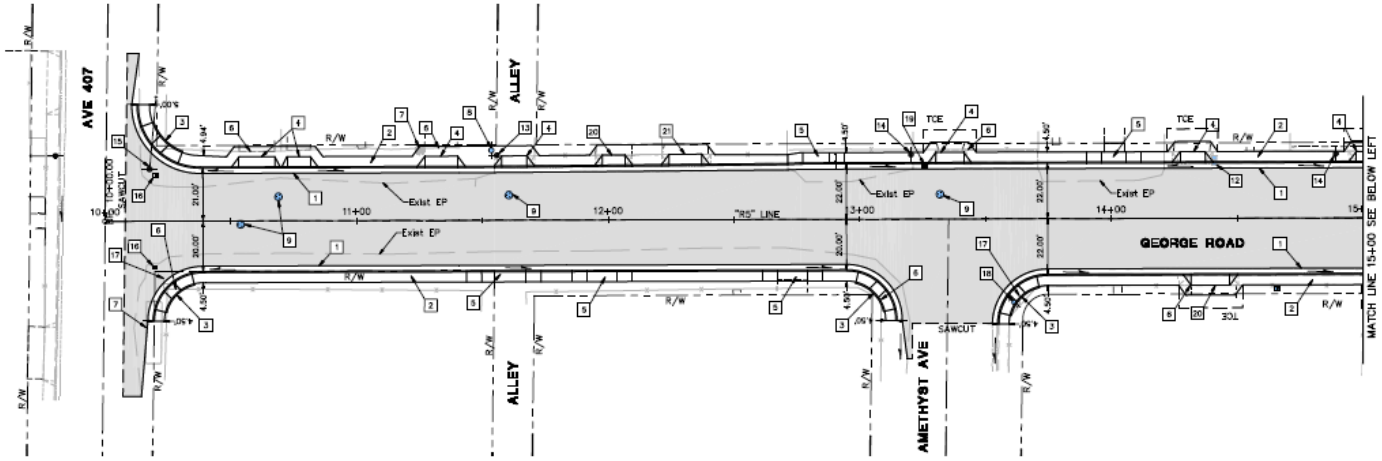
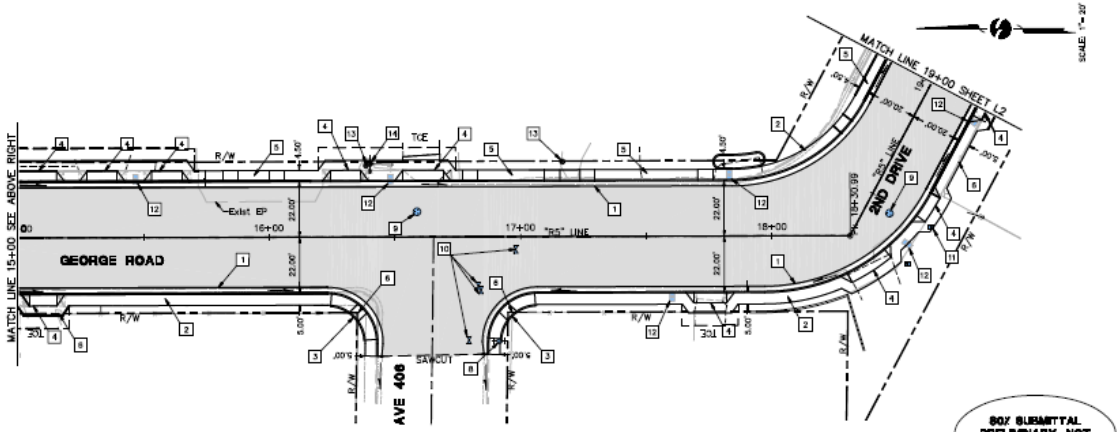




KEYED NOTES (THIS SHEET ONLY)

1. C&G.
2. SIDEWALK.
3. CASE C CURB RAMP.
4. DRIVEWAY.
5. DEPRESSED DRIVEWAY.
6. RELOCATE FENCE (CHAIN LINK).
7. REMOVE TREE.
8. ADJUST FIRE HYDRANT TO GRADE.
9. ADJUST MANHOLE TO GRADE.
10. ADJUST WATER VALVE COVER TO GRADE.
11. ADJUST WATER METER BOX TO GRADE.
12. RESET MAILBOX.
13. Exist UTILITY POLE (PROTECT IN PLACE).
14. Exist POWER POLE (PROTECT IN PLACE).
15. RELOCATE POWER POLE.
16. RELOCATE DRAINAGE INLET.
17. RELOCATE SIGN.
18. RELOCATE FIRE HYDRANT.
19. ADJUST DRAIN INLET TO GRADE.
20. RELOCATE FENCE (IRON).
21. RELOCATE FENCE (BRICK AND IRON).

LEGEND

ROADWAY CONST. SEE TYPICAL CROSS SECTIONS.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

80% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

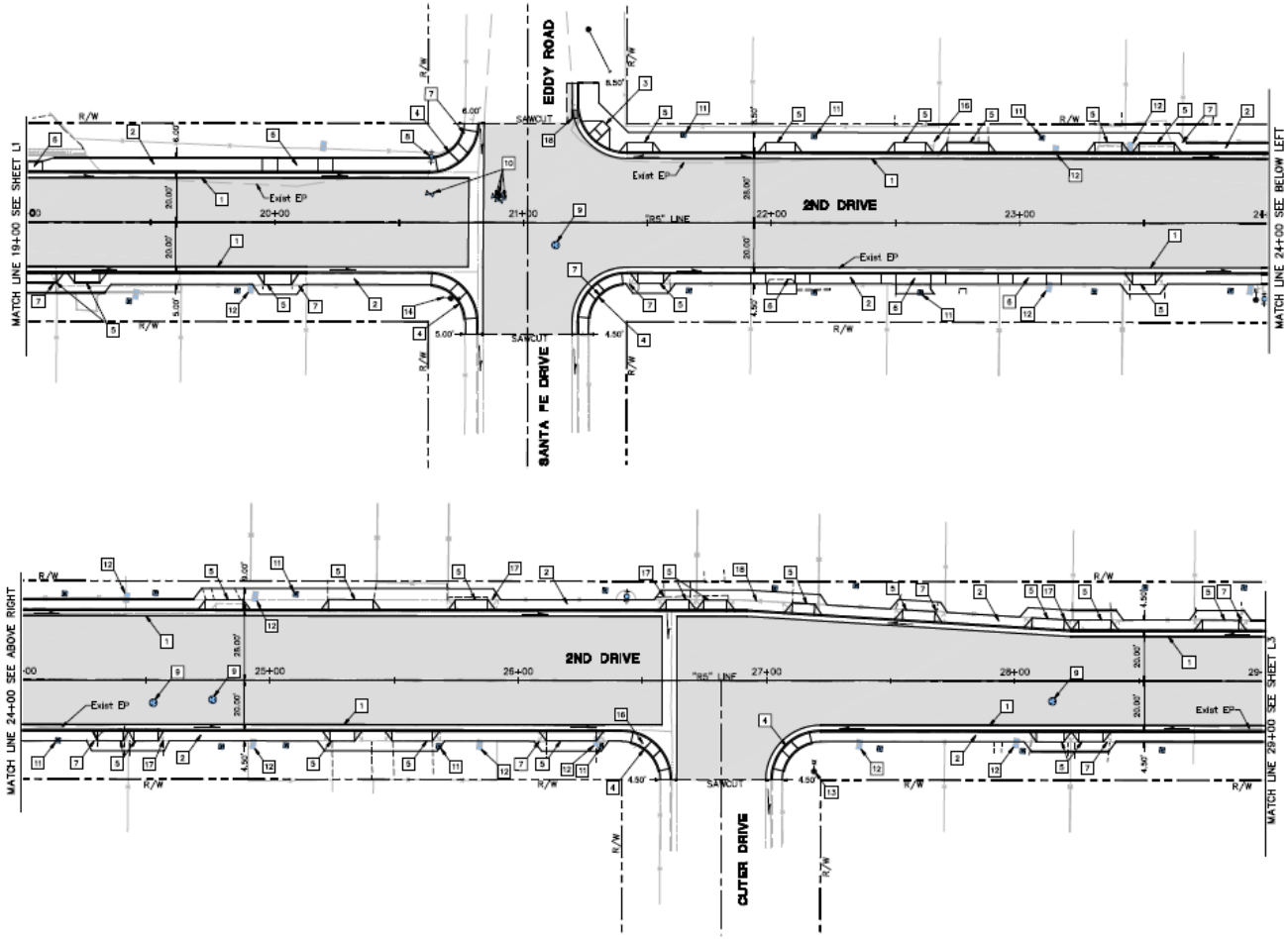
omni-means
ENGINEERS PLANNERS

LAYOUT
COMPLETE STREETS IMPROVEMENTS
CUTLER, CALIFORNIA

SCALE	1"=20'
JOB NO.	20-7200-12
DESIGNED	KJD
DRAWN	KJD
FILE	2015L006.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No. L1

5 of 7



KEYED NOTES (THIS SHEET ONLY)

1. C&G.
2. SIDEWALK.
3. CASE A CURB RAMP.
4. CASE C CURB RAMP.
5. DRIVEWAY.
6. DEPRESSED DRIVEWAY.
7. RELOCATE FENCE (CHAIN LINK).
8. ADJUST FIRE HYDRANT TO GRADE.
9. ADJUST MANHOLE TO GRADE.
10. ADJUST WATER VALVE COVER TO GRADE.
11. ADJUST WATER METER BOX TO GRADE.
12. RESET MAILBOX.
13. Exist UTILITY POLE (PROTECT IN PLACE).
14. Exist POWER POLE (PROTECT IN PLACE).
15. ADJUST DRAIN INLET TO GRADE.
16. RELOCATE SIGN.
17. RELOCATE FENCE (IRON).
18. RELOCATE FENCE (BRICK AND IRON).

LEGEND

- ROADWAY CONST. SEE TYPICAL CROSS SECTIONS.
- SLOPE ARROW. INDICATES DIRECTION OF FLOW.

BOY SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER MICHAEL J. WITKIN No. 66910 CIVIL STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

☐ PRELIMINARY ☐ APPROVED ☐ BID ☐ CONSTRUCTION ☐ RECORD

omni means ENGINEERS PLANNERS SURVEYORS ARCHITECTS 208 N. Center Ave. Suite 1 Rialto, CA 92376 (909) 820-0800

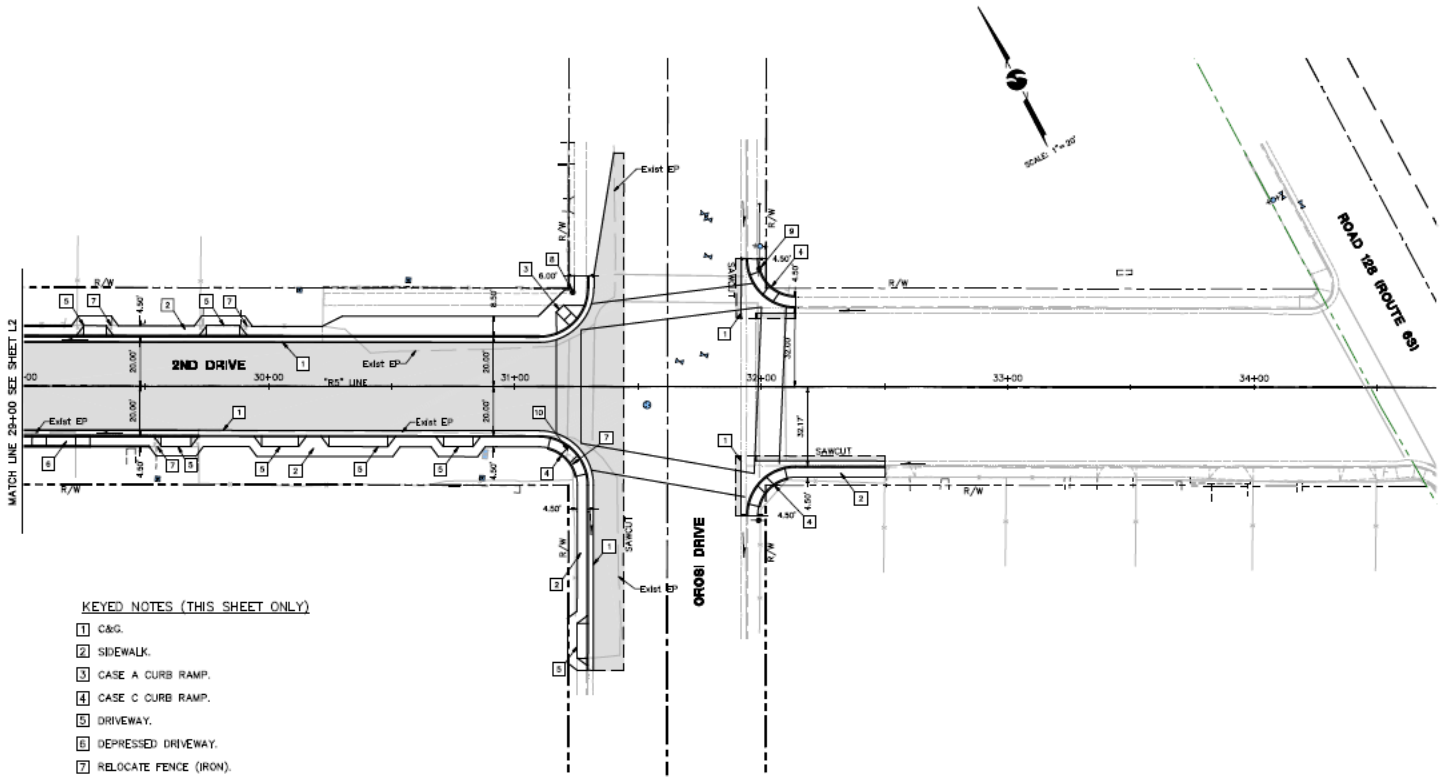
LAYOUT

COMPLETE STREETS IMPROVEMENTS

CUTLER, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7330-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L009.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No. L2



KEYED NOTES (THIS SHEET ONLY)

1. C&G.
2. SIDEWALK.
3. CASE A CURB RAMP.
4. CASE C CURB RAMP.
5. DRIVEWAY.
6. DEPRESSED DRIVEWAY.
7. RELOCATE FENCE (IRON).
8. Exist UTILITY POLE (PROTECT IN PLACE).
9. Exist POWER POLE (PROTECT IN PLACE).
10. RELOCATE SIGN.

LEGEND

- ROADWAY CONST. SEE TYPICAL CROSS SECTIONS.
- SLOPE ARROW. INDICATES DIRECTION OF FLOW.

60% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WRIGHT
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

PRELIMINARY
APPROVED
60
CONSTRUCTION
RECORD

omni means
ENGINEERS PLANNERS
1901 Lifeline Ln
Suite A, Lower Ln.
Bakersfield, CA 93307
(661) 325-5880

LAYOUT
COMPLETE STREETS IMPROVEMENTS
CUTLER, CALIFORNIA

SCALE	1"=20'
JOB NO.	50-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015LOCK.DWG
CHECKED	MJW
DATE	12-1-15

SHEET NO.
L3
7 OF 7

INDEX OF SHEETS

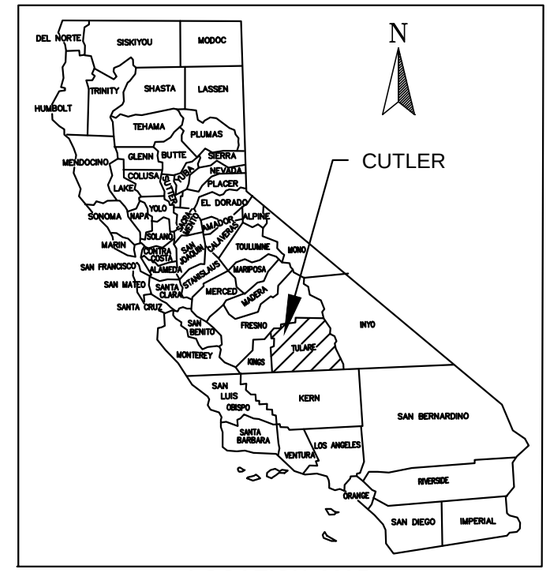
PAGE	SHEET NO.	DESCRIPTION
1	T1	COVER
2	X1	TYPICAL SECTIONS
3–5	L1–L3	LAYOUT
6–7	C1–C2	CONSTRUCTION DETAILS
8–10	PD1–PD3	SIGNING AND STRIPING PLAN

STATE OF CALIFORNIA
COUNTY OF TULARE

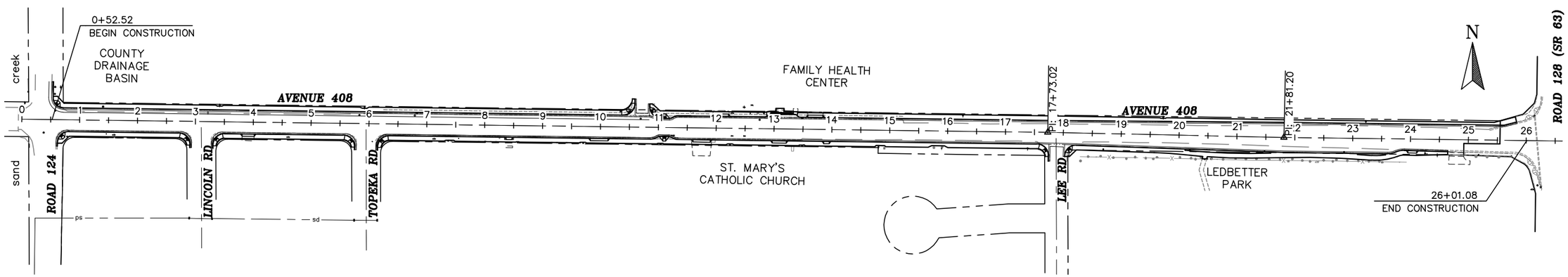
PROJECT PLANS FOR CONSTRUCTION OF

CUTLER-COMPLETE STREETS POLICY

IN CUTLER AND COUNTY OF TULARE
AVENUE 408 (FROM ROAD 124 TO SR 63)



LOCATION MAP
N.T.S.



VICINITY MAP
N.T.S.

Know what's **below.** **Call** before you dig.

Contractor shall call Underground Service Alert at 811 two working days prior to excavation

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

APPROVED BY:

JABED KHAN, P.E. — TULARE COUNTY RESOURCE MANAGEMENT AGENCY — DATE

APPROVED BY:

MICHAEL J. WINTON P.E. — PROJECT MANAGER OMNI-MEANS LTD. — DATE

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

omni•means ENGINEERS PLANNERS
VISALIA 200 E. Center Ave. Suite A Visalia, CA 93291 (559) 734-5895
With offices in: SACRAMENTO WALNUT CREEK REDDING SAN LUIS OBISPO

COVER
AVENUE 408 – CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	NO SCALE
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144T004.DWG
DATE	6-9-16
SHEET No.	T1

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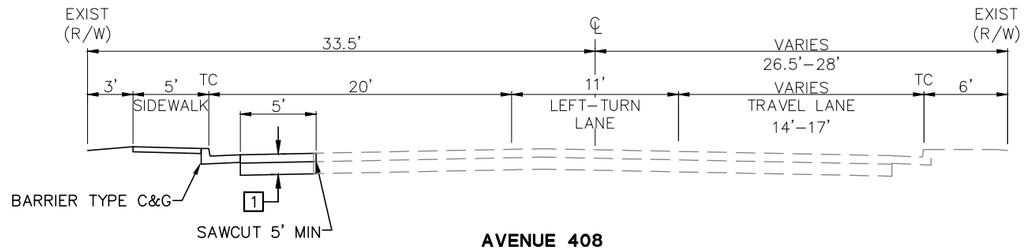
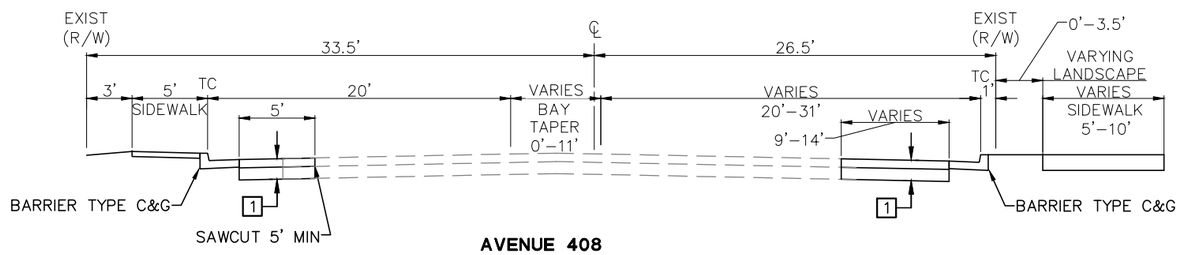
TYPICAL STRUCTURAL SECTIONS:

1 — 20-YEAR DESIGN
TI=5.5, R=25 (ASSUMED)
0.25' HMA (TYPE B)
0.70' AB (CLASS 2) (95% RC)
1.00' COMPACTED NATIVE (95% RC)

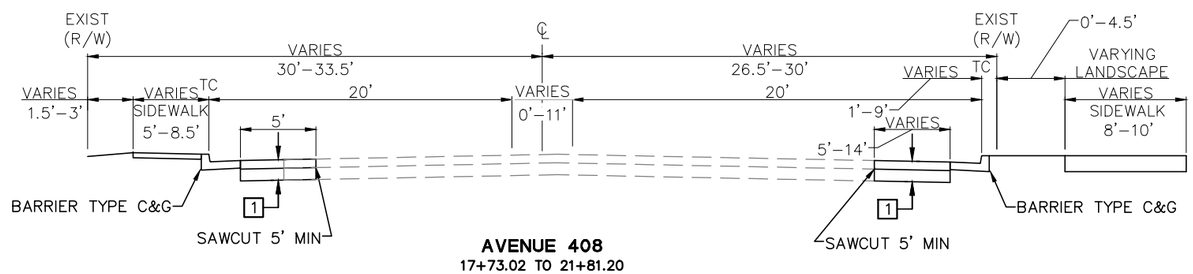
2 — LOADING ZONE TURNOUT
STA 12+84.81 TO STA 13+45.35
SEE CONSTRUCTION DETAILS.



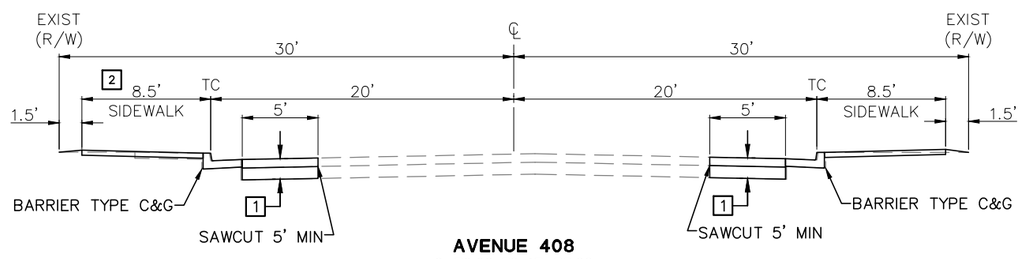
AVENUE 408
24+64.64 TO END



AVENUE 408
21+81.20 TO 24+64.64



AVENUE 408
17+73.02 TO 21+81.20



AVENUE 408
0+76.96 TO 17+73.02

30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS			
No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

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ENGINEERS PLANNERS
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

TYPICAL SECTIONS
AVENUE 408 – CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=5'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144X004.DWG
DATE	6-9-16
SHEET No.	X1

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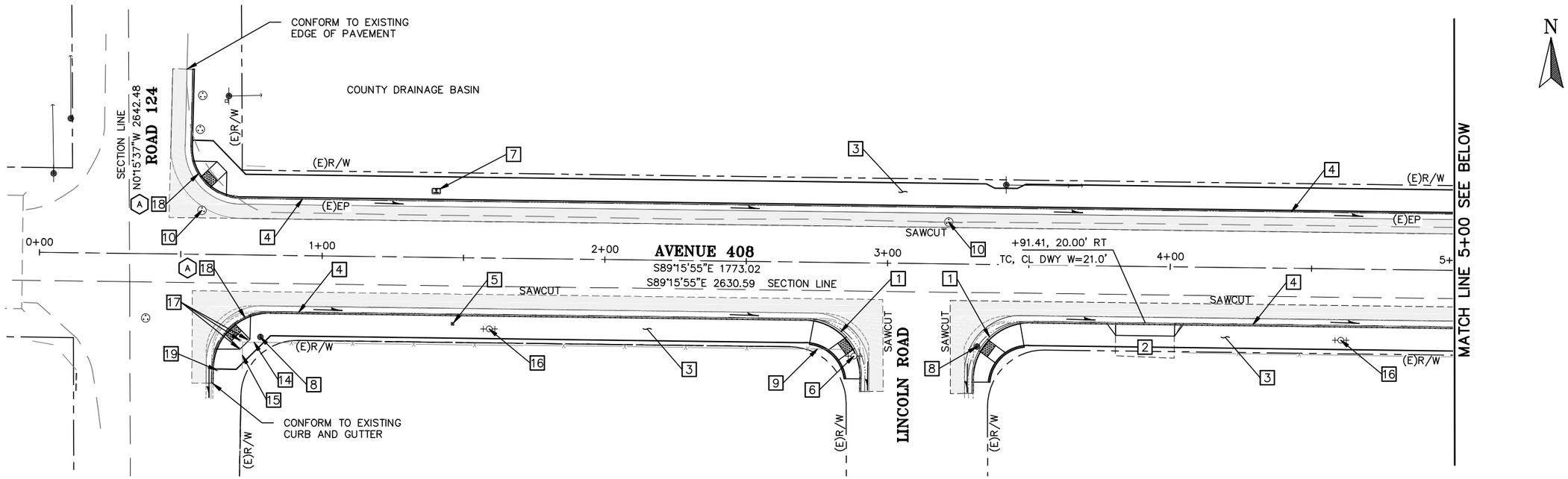

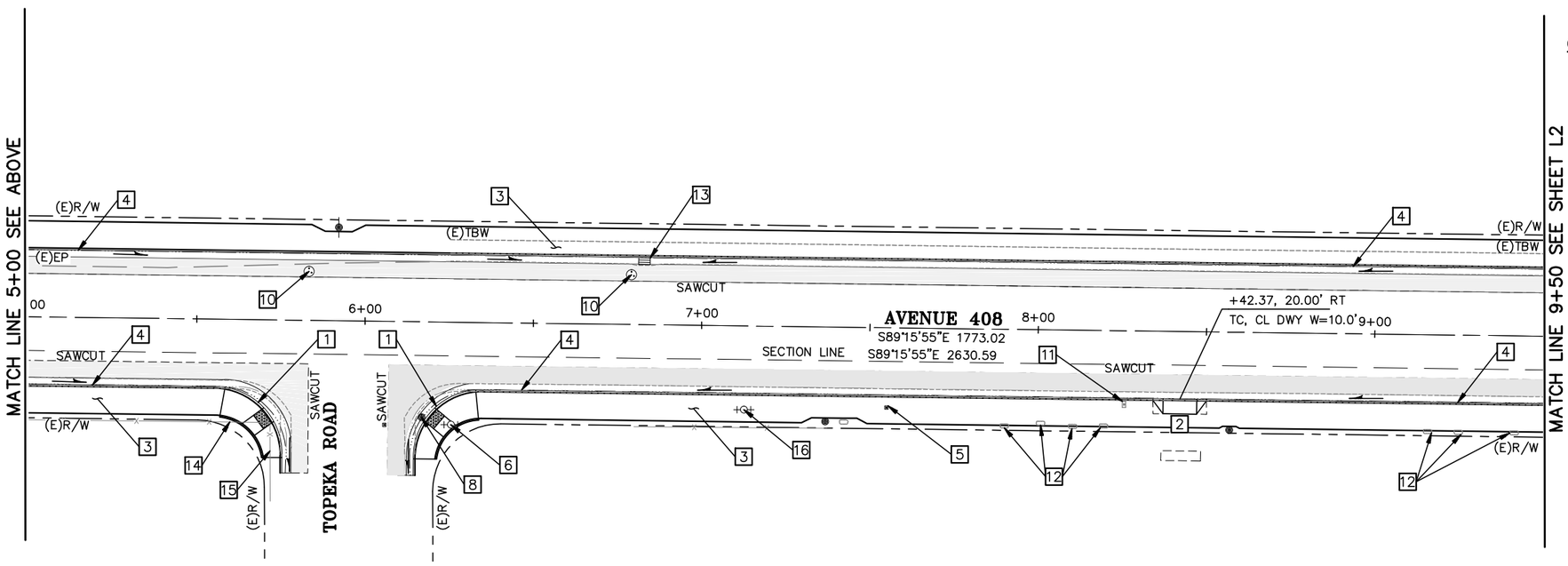

CONSTRUCTION NOTES (THIS SHEET ONLY):

1. MODIFIED CASE C CURB RAMP. SEE SHEET C1.
2. TULARE COUNTY URBAN DRIVEWAY. SEE SHEET C1.
3. SIDEWALK.
4. TULARE COUNTY BARRIER TYPE CURB AND GUTTER PER PLATE NO. A-19. SEE SHEET C1.
5. ADJUST WATER METER BOX TO GRADE.
6. RELOCATE FIRE HYDRANT.
7. ADJUST TELEPHONE PULL BOX TO GRADE (BY OTHERS).
8. RELOCATE UTILITY POLE (BY OTHERS).
9. REMOVE FENCE (WOODEN) TO R/W.
10. ADJUST MANHOLE TO GRADE.
11. RESET MAILBOX.
12. REMOVE BUSHES.
13. RECONSTRUCT DRAINAGE INLET.
14. REMOVE FENCE (CHAIN LINK).
15. INSTALL FENCE (CHAIN LINK) AT R/W.
16. ADJUST FIRE HYDRANT TO GRADE.
17. REMOVE METAL POSTS.
18. CURB RAMP PER DETAIL (A) ON SHEET C1.
19. REMOVE IRRIGATION STANDPIPE.

811 Know what's below. Call before you dig.

Contractor shall call Underground Service Alert at 811 two working days prior to excavation

30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

GENERAL NOTES (ALL L-SHEETS):

1. SEE SIGNING AND STRIPING PLAN FOR EXISTING SIGNS TO BE REMOVED OR RELOCATED.

LEGEND:

- ROADWAY CONSTRUCTION
- TEMPORARY CONSTRUCTION EASEMENT
- SLOPE ARROW, INDICATES DIRECTION OF FLOW

REGISTERED PROFESSIONAL ENGINEER MICHAEL J. WINTON No. 66910 CIVIL STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

omni•means ENGINEERS PLANNERS
VISALIA 200 E. Center Ave. Suite A Visalia, CA 93291 (559) 734-5895
With offices in: SACRAMENTO WALNUT CREEK REDDING SAN LUIS OBISPO

LAYOUT
AVENUE 408 – CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144L004.DWG
DATE	6-9-16
SHEET No.	L1

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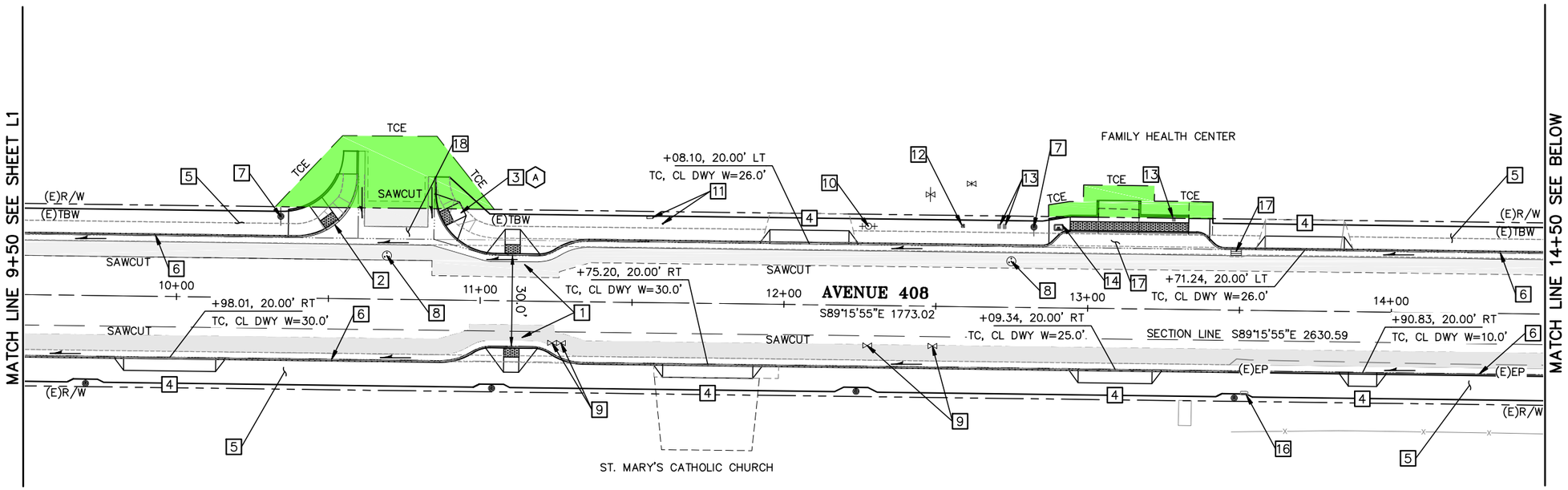

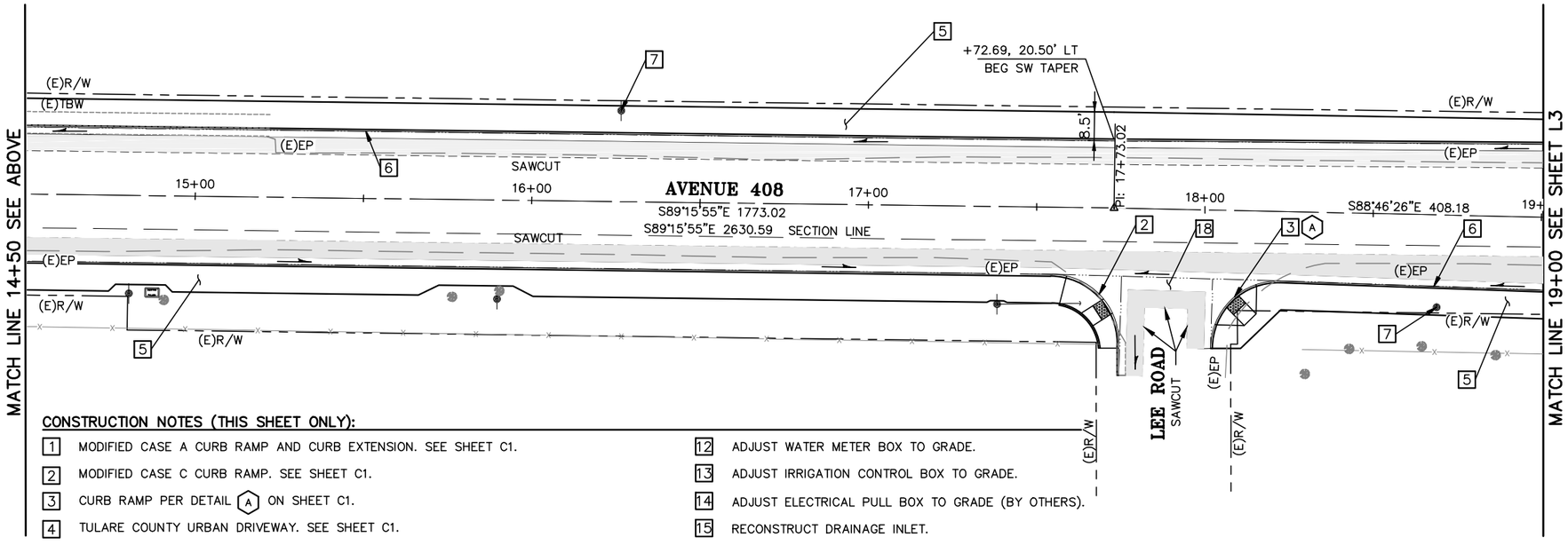

LEGEND:

- ROADWAY CONSTRUCTION
- TEMPORARY CONSTRUCTION EASEMENT
- SLOPE ARROW, INDICATES DIRECTION OF FLOW

CONSTRUCTION NOTES (THIS SHEET ONLY):

1. MODIFIED CASE A CURB RAMP AND CURB EXTENSION. SEE SHEET C1.
2. MODIFIED CASE C CURB RAMP. SEE SHEET C1.
3. CURB RAMP PER DETAIL (A) ON SHEET C1.
4. TULARE COUNTY URBAN DRIVEWAY. SEE SHEET C1.
5. SIDEWALK.
6. TULARE COUNTY BARRIER TYPE CURB AND GUTTER PER PLATE NO. A-19. SEE SHEET C1.
7. EXIST UTILITY POLE TO REMAIN (PROTECT IN PLACE).
8. ADJUST MANHOLE TO GRADE.
9. ADJUST WATER VALVE FRAME AND COVER TO GRADE.
10. ADJUST FIRE HYDRANT TO GRADE.
11. RELOCATE TELEPHONE PEDESTAL (BY OTHERS).
12. ADJUST WATER METER BOX TO GRADE.
13. ADJUST IRRIGATION CONTROL BOX TO GRADE.
14. ADJUST ELECTRICAL PULL BOX TO GRADE (BY OTHERS).
15. RECONSTRUCT DRAINAGE INLET.
16. REMOVE BUSH.
17. CONSTRUCT LOADING ZONE TURNOUT. SEE SHEET C2
18. CONSTRUCT CONTINUOUS GUTTER CURB RETURN. SEE SHEET C2.

811 Know what's **below.** **Call** before you dig.

Contractor shall call Underground Service Alert at 811 two working days prior to excavation

30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER MICHAEL J. WINTON No. 66910 CIVIL STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

omni·means ENGINEERS PLANNERS
VISALIA 200 E. Center Ave. Suite A Visalia, CA 93291 (559) 734-5895
With offices in: SACRAMENTO WALNUT CREEK REDDING SAN LUIS OBISPO

LAYOUT
AVENUE 408 – CUTLER
TULARE COUNTY COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144L004.DWG
DATE	6-9-16
SHEET No.	L2

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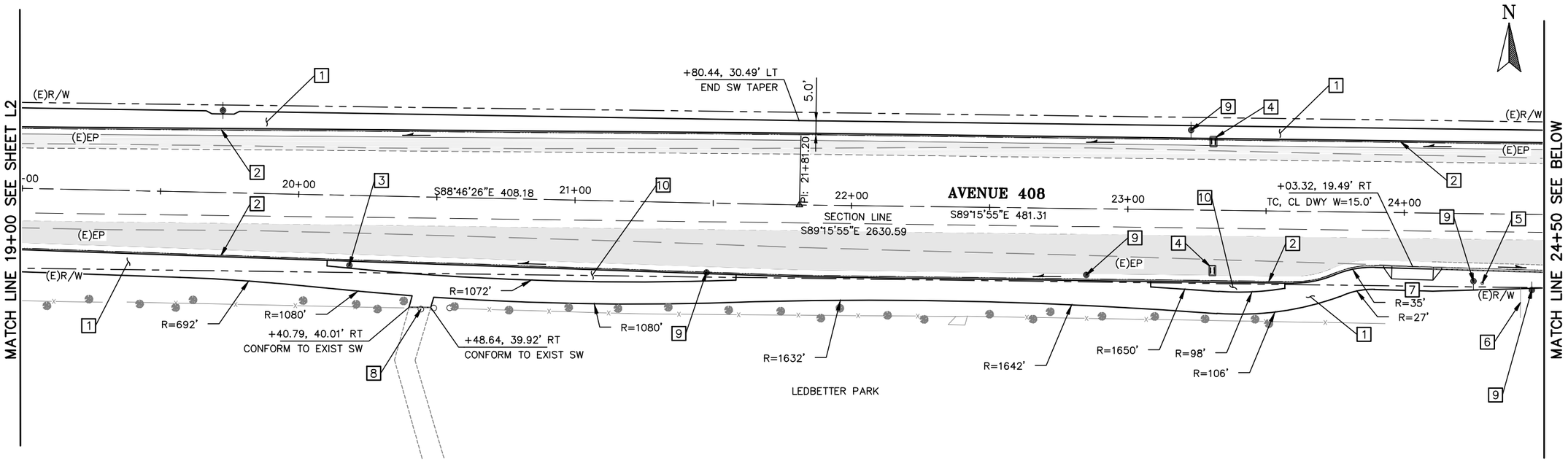

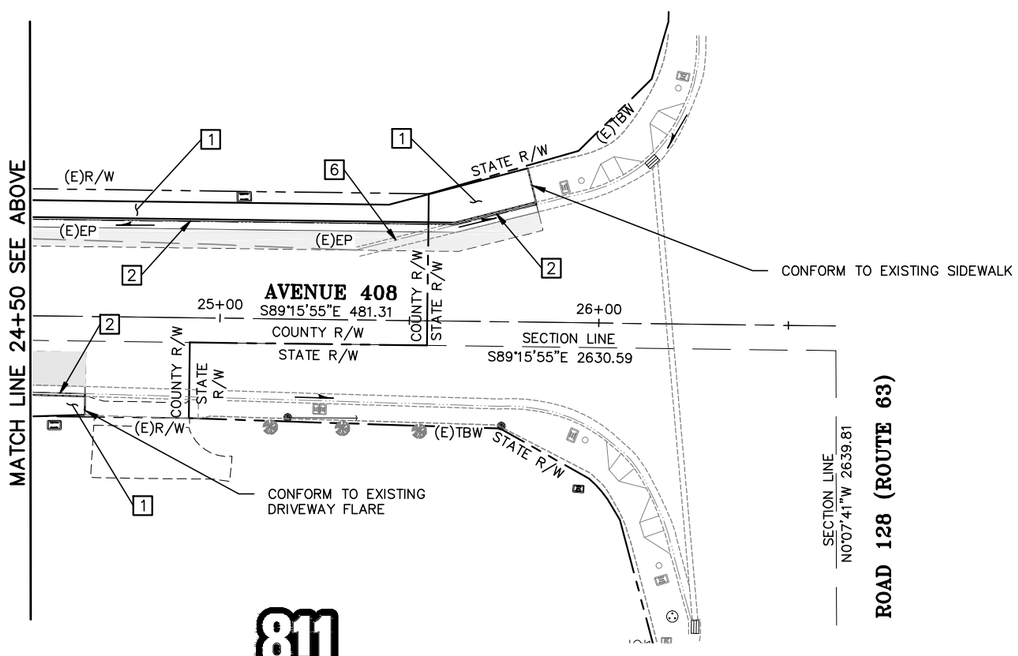

CONSTRUCTION NOTES (THIS SHEET ONLY):

1. SIDEWALK.
2. TULARE COUNTY BARRIER TYPE CURB AND GUTTER PER PLATE NO. A-19. SEE SHEET C1.
3. EXIST UTILITY POLE TO REMAIN (PROTECT IN PLACE).
4. RELOCATE ELECTRICAL PULL BOX (BY OTHERS).
5. RELOCATE TELEPHONE PEDESTAL (BY OTHERS).
6. REMOVE EXIST CURB AND GUTTER.
7. TULARE COUNTY URBAN DRIVEWAY. SEE SHEET C1.
8. REMOVE BOLLARD.
9. RELOCATE UTILITY POLE (BY OTHERS),
10. LANDSCAPE AREA.

LEGEND:

- ROADWAY CONSTRUCTION
- TEMPORARY CONSTRUCTION EASEMENT
- SLOPE ARROW, INDICATES DIRECTION OF FLOW

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

811
Know what's **below.**
Call before you dig.

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REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS			
No.	DESCRIPTION	DATE	BY

TULARE COUNTY
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

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ENGINEERS PLANNERS
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

LAYOUT
AVENUE 408 – CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144L004.DWG
DATE	6-9-16
SHEET No.	L3

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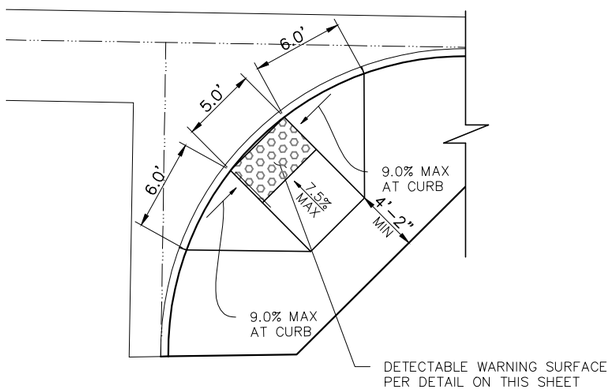
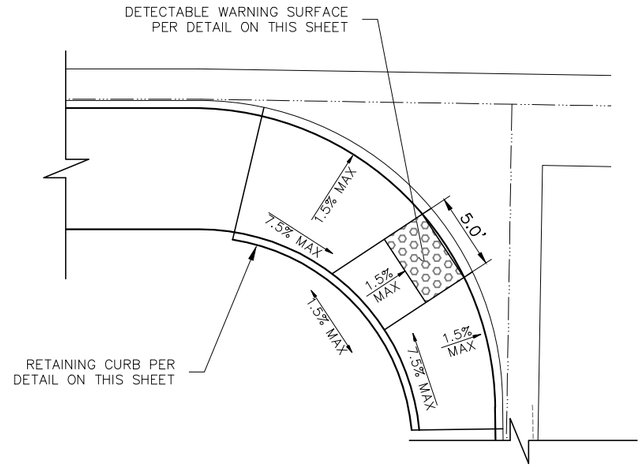


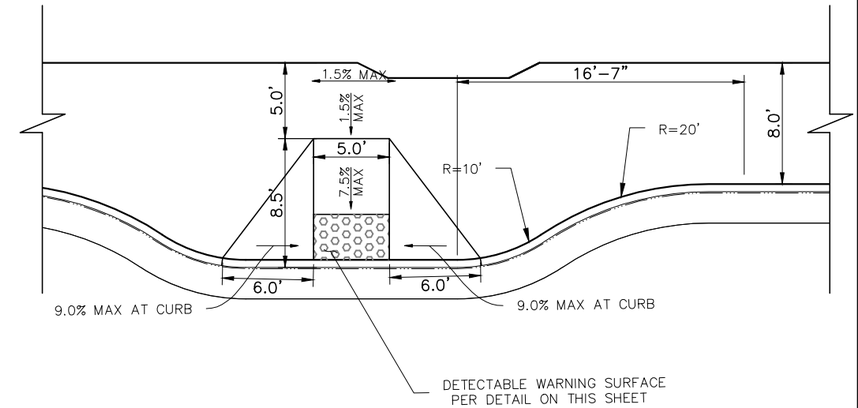
CURB RAMP DETAIL (A)

N.T.S



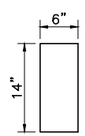
MODIFIED CASE C CURB RAMP DETAIL

N.T.S



MODIFIED CASE A CURB RAMP AND CURB EXTENSION DETAIL

N.T.S

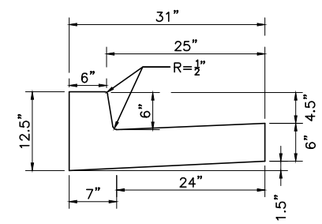


NOTES:

1. ALL CONCRETE SHALL BE MINOR CONCRETE.

RETAINING CURB DETAIL

SCALE: 1"=1'-0"

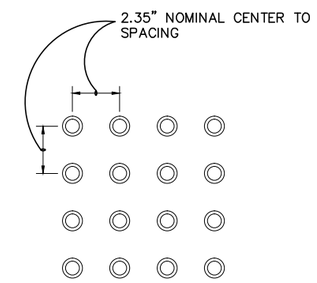


NOTES:

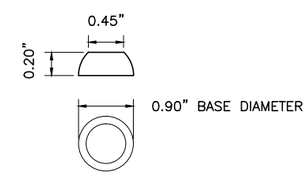
1. ALL CONCRETE SHALL BE MINOR CONCRETE.
2. BARRIER TYPE CURB AND GUTTER SHALL HAVE A MINIMUM OF 0.15 FEET PER 100 FEET.
3. AREA BETWEEN BACK OF CURB AND AND PROPERTY LINE SHALL BE BACK FILLED AND SLOPED TO DRAIN TO GUTTER.

COUNTY CURB & GUTTER (BARRIER TYPE) DETAIL

SCALE: 1"=1'-0"



RAISED TRUNCATED DOME PATTERN



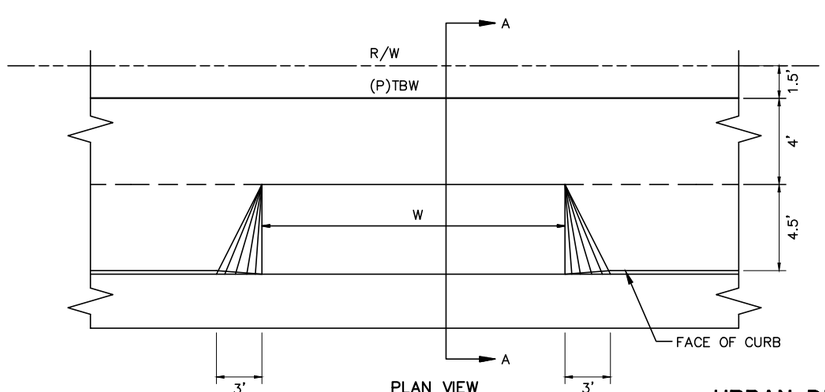
RAISED TRUNCATED DOME

DETECTABLE WARNING SURFACE

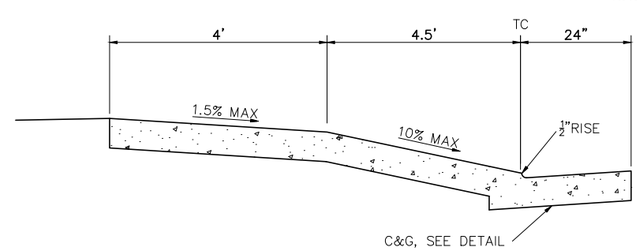
N.T.S

NOTES:

1. SHALL BE INSTALLED AT THE BOTTOM OF ALL CURB RAMPS.
2. SHALL BE THE FULL WIDTH OF RAMP AND SHALL BE A MIN. OF 36" IN DEPTH
3. SHALL BE PREMIXED FEDERAL YELLOW COLORED POLYMER CONCRETE MATERIAL.
4. ALL DETECTABLE WARNING PANELS INSTALLED WITH NEW IMPROVEMENTS SHALL BE WET SET TYPE/CAST IN PLACE TYPE PANELS.
5. A 4'-0" WIDE DETECTABLE WARNING SURFACE MAY BE USED ON A 4'-2" WIDE CURB RAMP.
6. DETECTABLE WARNING SURFACES SHALL CONFORM TO THE REQUIREMENTS IN THE STANDARDS SPECIFICATIONS.



PLAN VIEW



SECTION A–A

URBAN DRIVEWAY DETAIL

N.T.S

30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER MICHAEL J. WINTON No. 66910 CIVIL STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

omni • means ENGINEERS PLANNERS
VISALIA 200 E. Center Ave. Suite A Visalia, CA 93291 (559) 734-5895
With offices in: SACRAMENTO MAUMEE CREEK READING SAN LUIS OBISPO

CONSTRUCTION DETAILS
AVENUE 408 – CUTLER
TULARE COUNTY COMPLETE STREETS
TULARE COUNTY, CA

SCALE	AS SHOWN
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144C004.DWG
DATE	6-9-16

SHEET No. C1

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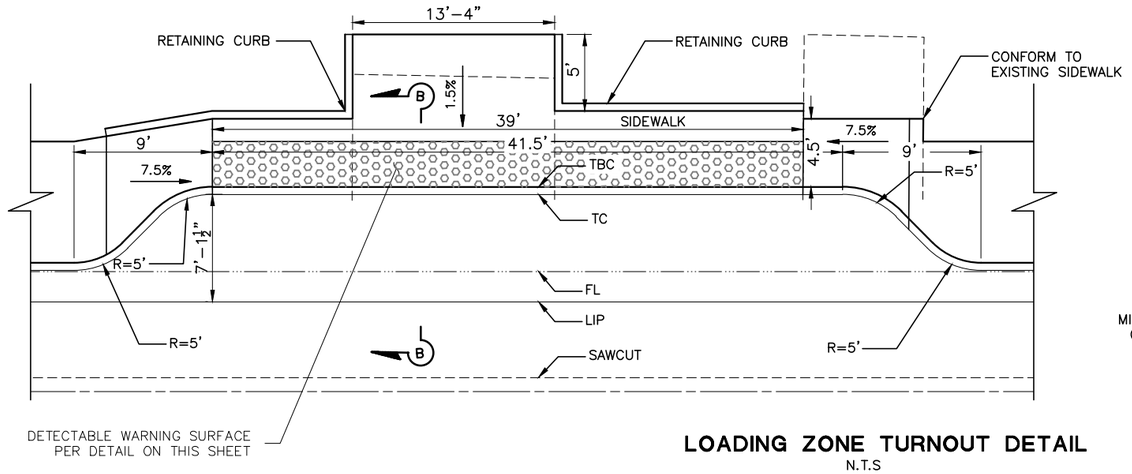
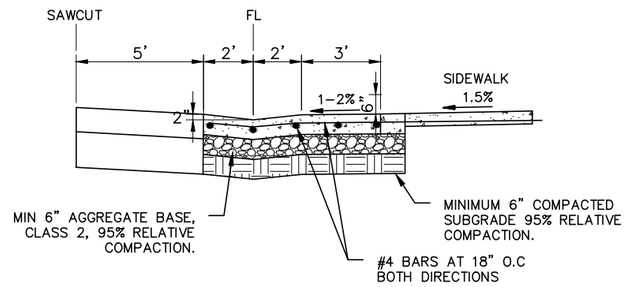
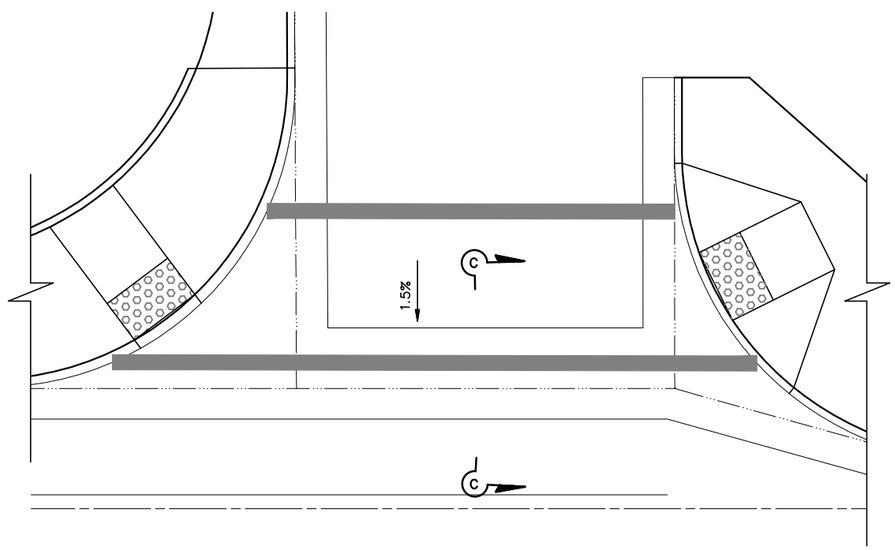


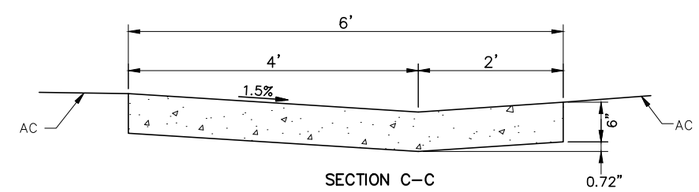
LOADING ZONE TURNOUT DETAIL

N.T.S



SECTION B-B





SECTION C-C

CONTINUOUS GUTTER CURB RETURN DETAIL

N.T.S

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

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SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

CONSTRUCTION DETAILS
AVENUE 408 - CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	NO SCALE
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144C004.DWG
DATE	6-9-16
SHEET No.	C2

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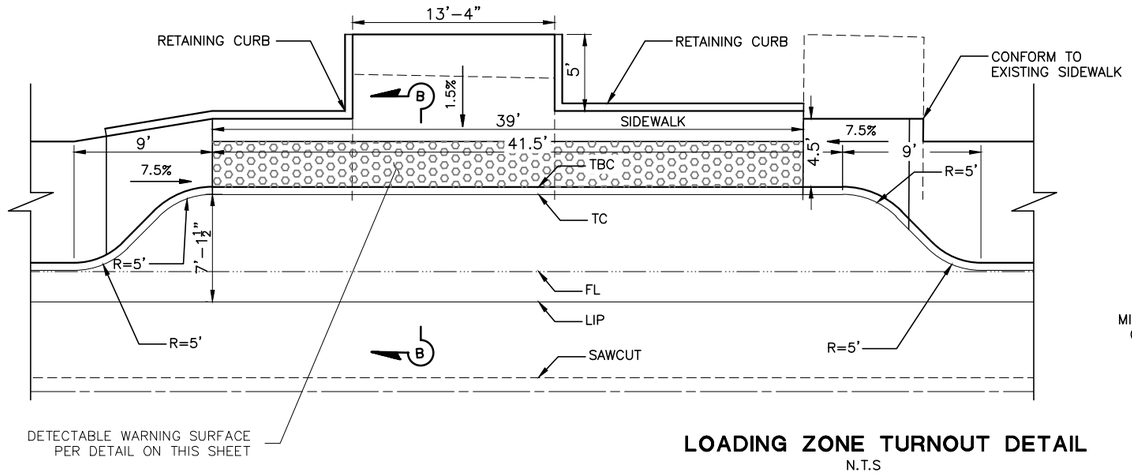
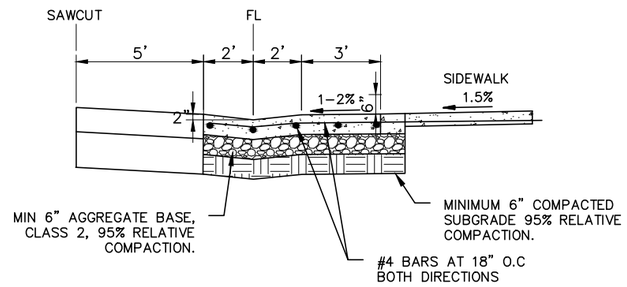
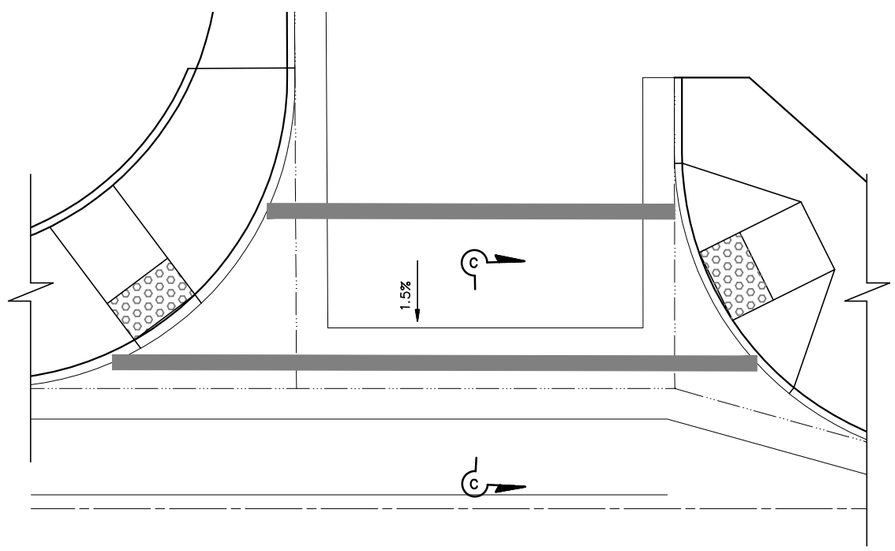


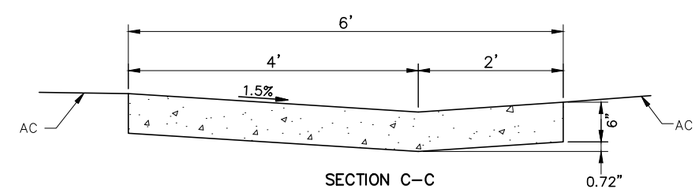
LOADING ZONE TURNOUT DETAIL

N.T.S



SECTION B-B





SECTION C-C

CONTINUOUS GUTTER CURB RETURN DETAIL

N.T.S

30% SUBMITTAL
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WALNUT CREEK
REDDING
SAN LUIS OBISPO

CONSTRUCTION DETAILS
AVENUE 408 - CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	NO SCALE
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
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SHEET No.	C2

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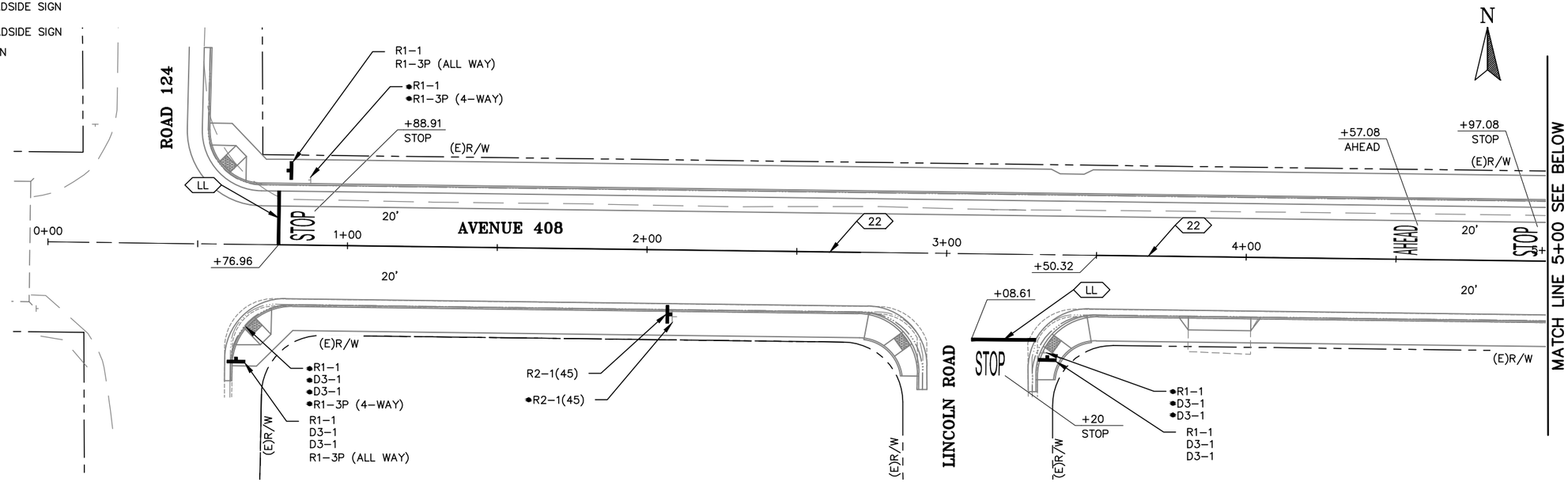
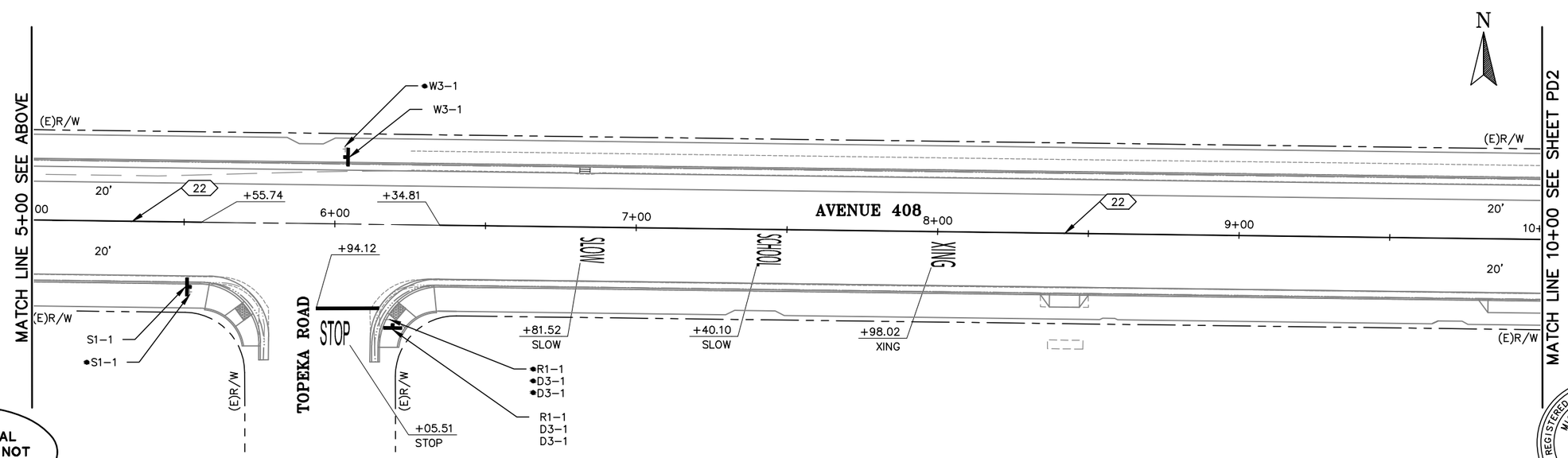
LEGEND: (SHEETS PD1–PD3)

- (#) STRIPING DETAIL NUMBER PER STATE STD PLANS
- (CW) THERMOPLASTIC CROSSWALK (CW) PER STATE STD PLANS
- (LL) THERMOPLASTIC LIMIT LINE (LL)
- STOP INSTALL PAVEMENT MARKING, WORD AS INDICATED, PER STATE STANDARD PLANS
- LIMIT OF STRIPING PATTERN
- EXISTING ONE POST ROADSIDE SIGN
- EXISTING TWO POST ROADSIDE SIGN
- PROPOSED ONE POST ROADSIDE SIGN
- PROPOSED TWO POST ROADSIDE SIGN
- LIMIT OF STRIPING PATTERN
- ANGLE POINT
- TANGENT POINT
- EXISTING SIGN TO REMAIN
- REMOVE SIGN
- (CA) CALIFORNIA SIGN CODE

GENERAL NOTES: (SHEETS PD1–PD3)

1. WORK SHALL BE DONE IN ACCORDANCE WITH THE 2015 EDITION OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND SPECIFICATIONS, THE 2014 EDITION OF THE CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) AND SIGN SPECIFICATION SHEETS, THE LATEST EDITIONS OF THE TULARE COUNTY STANDARD PLANS AND SPECIFICATIONS, AND THE SPECIAL PROVISIONS.
2. THESE PLANS ARE ACCURATE FOR SIGNING AND STRIPING ONLY.
3. ALL SIGNS SHALL BE AVERY DENNISON T6500 SERIES REFLECTIVE SHEETING AND SHALL BE COVERED WITH AVERY DENNISON OL1000 ANTI–GRAFFITI OVERLAY FILM.
4. ALL STRIPING SHALL BE THERMOPLASTIC PAINT.
5. ALL CROSSWALKS SHALL BE 10 FEET WIDE, MEASURED FROM INSIDE TO INSIDE OF MARKING.
6. REMOVE ALL STRIPING AND MARKINGS CONFLICTING WITH NEW PAVEMENT DELINEATION.
7. LANE WIDTHS ARE MEASURED TO TOP FACE OF CURB OR STRIPE, WHICH EVER COMES FIRST.





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SIGNING AND STRIPING PLAN
AVENUE 408 – CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55–7300–14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144PD004.DWG
DATE	6–9–16
SHEET No.	PD1

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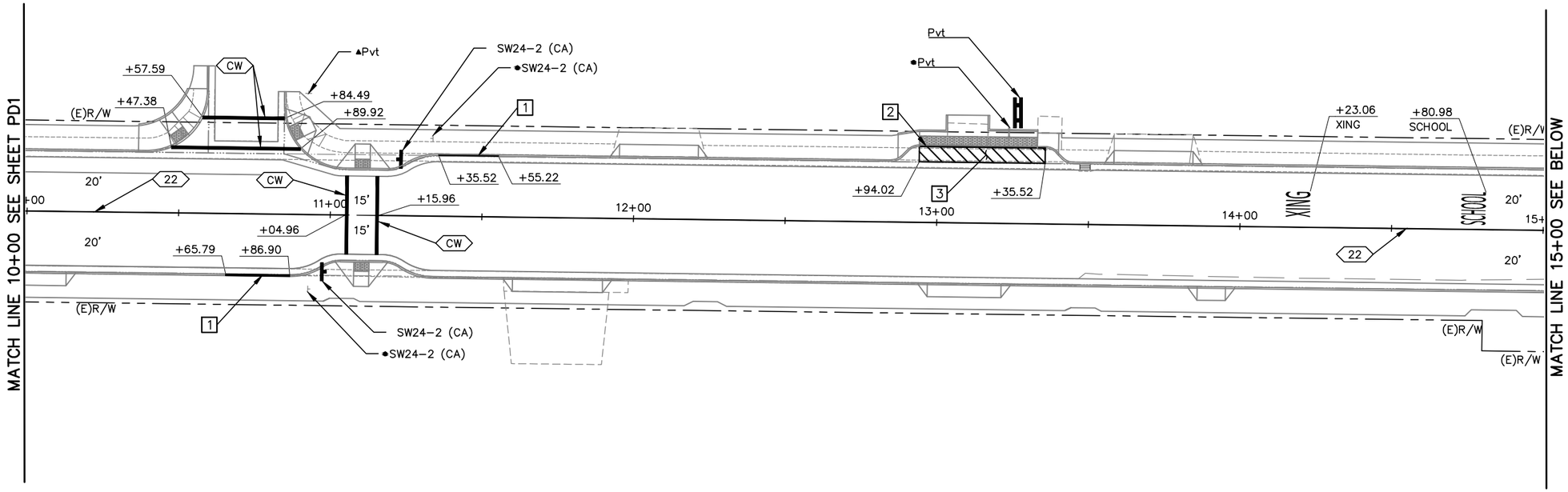

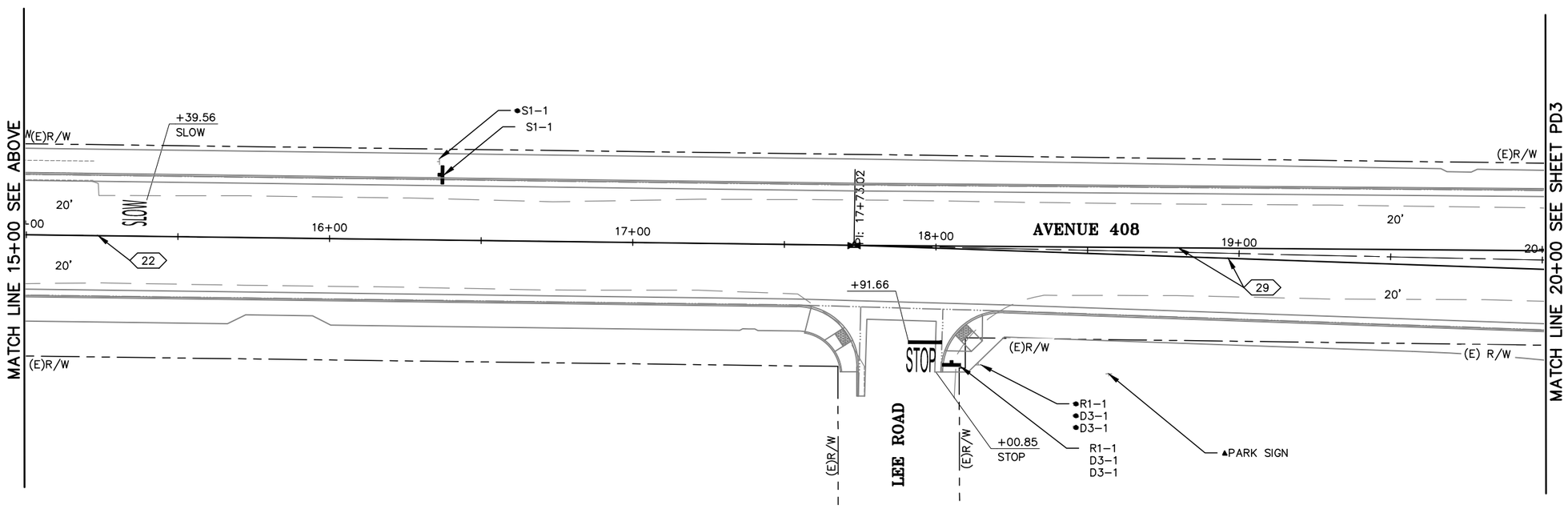

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

CONSTRUCTION NOTES (THIS SHEET ONLY):

1. PAINT CURB RED.
2. PAINT 41.5-FT OF CURB YELLOW WITH LOADING ZONE LEGEND.
3. ACCESS AISLE WILL HAVE A 4-IN WHITE BORDER STRIPE. DIAGONALS WILL BE 4-IN AND PAINTED WHITE, 3-FT O.C.

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
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REVISIONS			
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FILE	2144PD004.DWG
DATE	6-9-16
SHEET No.	PD2

9 OF 10

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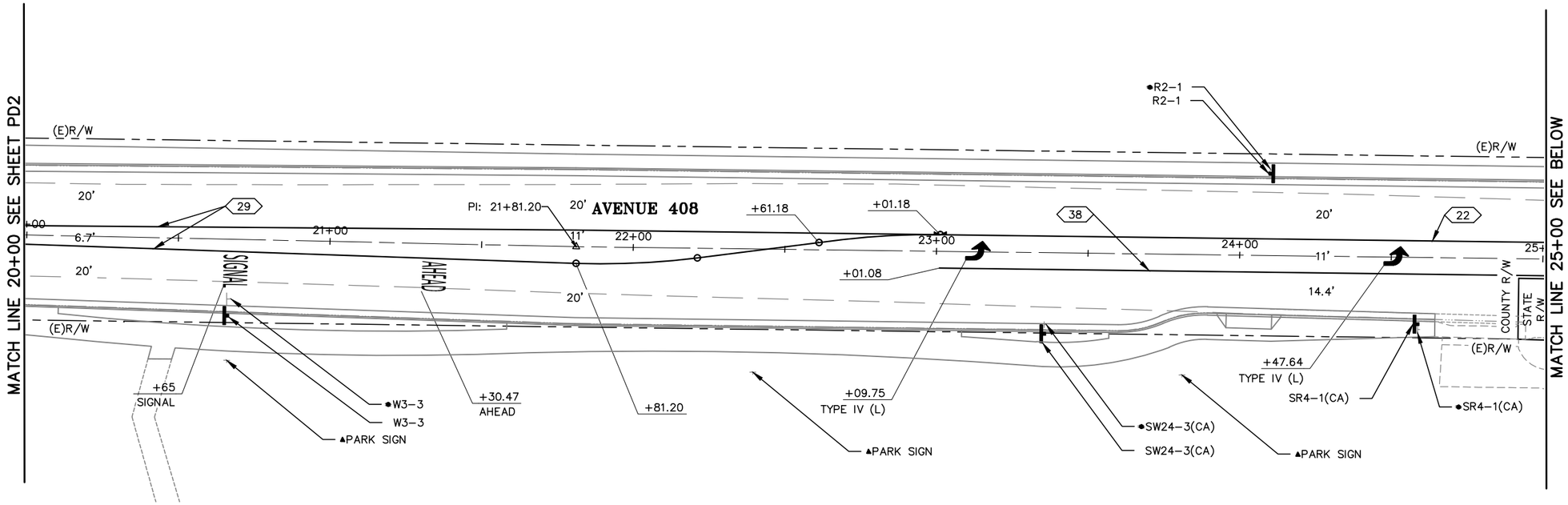

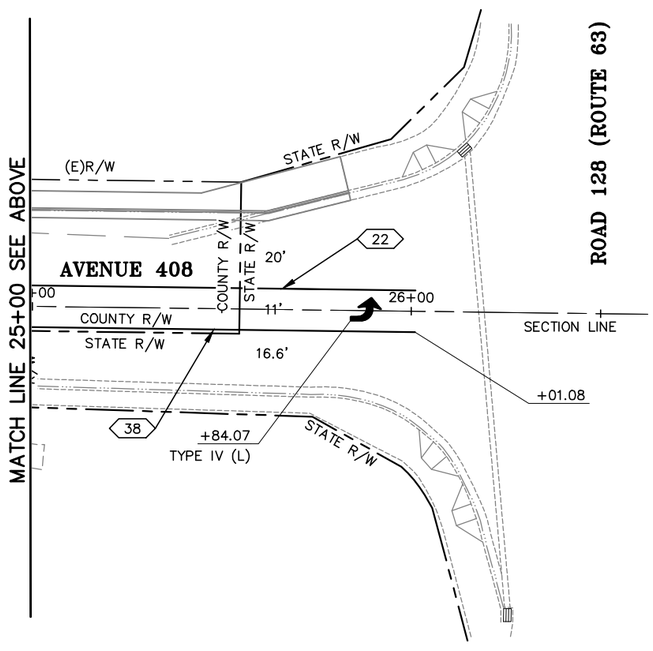

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SIGNING AND STRIPING PLAN
AVENUE 408 – CUTLER
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	MJW
FILE	2144PD004.DWG
DATE	6-9-16
SHEET No.	PD3

10 OF 10

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Appendix G

Cost Estimates for Orosi

PRELIMINARY COST ESTIMATE (30-Percent)						
Orosi Complete Street Improvements				Date:	10/21/2015	
County of Tulare						
Construction						
ITEM		**ITEM DESCRIPTION**	**UNIT OF**	**ESTIMATED**	**ITEM**	**TOTAL**
NO	**(F)**		**MEASURE**	**QUANTITY**	**PRICE**	
1		MOBILIZATION	LS	1	$ 91 500.00	$ 91 500
2	-	JOB SITE MANAGEMENT	LS	1	$ 8 000.00	$ 8 000
3	-	WATER POLLUTION CONTROL PROGRAM	LS	1	$ 4 500.00	$ 4 500
4	-	CONSTRUCTION AREA SIGNS	LS	1	$ 10 000.00	$ 10 000
5	-	TRAFFIC CONTROL SYSTEM	LS	1	$ 30 000.00	$ 30 000
6	-	RELOCATE FENCE (CHAIN LINK)	LF	286	$ 50.00	$ 14 300
7	-	RELOCATE FENCE (IRON)	LF	114	$ 90.00	$ 10 304
8	-	RELOCATE FENCE (BRICK AND IRON)	LF	105	$ 150.00	$ 15 695
9	-	RELOCATE FENCE (WOODEN)	LF	46	$ 40.00	$ 1 840
10	-	RESET MAILBOX	EA	54	$ 300.00	$ 16 200
11	-	RESET STREET SIGN	EA	8	$ 500.00	$ 4 000
12	-	REMOVE TREE	EA	15	$ 800.00	$ 12 000
13	-	BARRIER POST	EA	3	$ 500.00	$ 1 500
14	-	TYPE 3 MARKERS FOR BARRIER POSTS	EA	3	$ 25.00	$ 75
15	-	ADJUST DRAIN INLET TO GRADE	EA	1	$ 1 500.00	$ 1 500
16	-	ADJUST WATER VALVE COVER TO GRADE	EA	5	$ 800.00	$ 4 000
17	-	ADJUST FIRE HYDRANT TO GRADE	EA	1	$ 2 000.00	$ 2 000
18	-	AJUST GAS VALVE FRAME AND COVER TO GRADE	EA	3	$ 800.00	$ 2 400
19	-	ADJUST WATER METER BOX TO GRADE	EA	8	$ 3 500.00	$ 28 000
20	-	ROADSIDE DITCH	LF	5 374	$ 10.00	$ 53 740
21	-	CLEARING AND GRUBBING	LS	1	$ 10 000.00	$ 10 000
22	(F)	ROADWAY EXCAVATION	CY	2 617	$ 60.00	$ 157 000
23	(F)	CLASS 2 AGGREGATE BASE	CY	1 728	$ 60.00	$ 103 651
24	-	HOT MIX ASPHALT (TYPE A)	TON	1 475	$ 110.00	$ 162 250
25	-	12" REINFORCED CONCRETE PIPE	LF	1 024	$ 100.00	$ 102 400
26	-	18" REINFORCED CONCRETE PIPE	LF	82	$ 165.00	$ 13 530
27	-	MINOR CONCRETE (MINOR STRUCTURE-TYPE GO INLET)	EA	3	$ 3 500.00	$ 10 500
28	-	MINOR CONCRETE (CURB AND GUTTER/DIKE)	LF	717	$ 25.00	$ 17 928
29	-	MINOR CONCRETE (SIDEWALK)	SQFT	4 016	$ 7.00	$ 28 111
30	-	MINOR CONCRETE (DRIVEWAYS AND CURB RAMPS)	SQFT	2 140	$ 15.00	$ 32 094
31	-	DETECTABLE WARNING SURFACE	SQFT	15	$ 45.00	$ 675
32	-	SIGNING & STRIPING	LS	1	$ 8 000.00	$ 8 000
33	-	EROSION CONTROL	LS	1	$ 5 000.00	$ 5 000
34	-	MISCELLANEOUS ITEMS	LS	1	$ 217 800.00	$ 217 800
		Total - Construction Items 1-34				**$1 180 493**
		Contingency (25%)				$ 295 123
		Recommended Total Construction Budget				**$1 475 617**
Non-Construction Related Costs						
ITEM		**ITEM DESCRIPTION**	**UNIT OF**	**ESTIMATED**	**ITEM**	**TOTAL**
NO	**(F)**		**MEASURE**	**QUANTITY**	**PRICE**	
35	-	Environmental Clearance	% of CON	5%	$1 180 493.46	$ 59 025
36	-	Final Engineering Design	% of CON	15%	$1 180 493.46	$ 177 074
37	-	Construction Support	% of CON	2%	$1 180 493.46	$ 23 610
38	-	Construction Management	% of CON	15%	$1 180 493.46	$ 177 074
		Total - Non-Construction Items 35-38				**$ 436 783**
		Total Construction & Non-Construction Items				**$1 912 399**

omni · means
ENGINEERING SOLUTIONS

Preliminary Cost Estimate (30-Percent)

Avenue 413, Orosi

Tulare County Complete Streets

6/9/2016

55-7300-14/2144

Construction Costs:

No.	Item Description	Units	Quantity	Unit Cost	Total
1	Job Site Management	LS	1	$8,000.00	$8,000.00
2	Construction Area Signs	LS	1	$7,500.00	$7,500.00
3	Traffic Control	LS	1	$30,000.00	$30,000.00
4	Water Pollution Control Program	LS	1	$4,500.00	$4,500.00
5	Remove Roadside Sign	EA	8	$130.00	$1,040.00
6	Remove Pavement Markings	SQFT	228	$2.70	$615.60
7	Remove Concrete (Curb & Gutter)	LF	3750	$15.00	$56,250.00
8	Remove Concrete Sidewalk	SQFT	6400	$4.50	$28,800.00
9	Reconstruct Inlet	EA	4	$3,500.00	$14,000.00
10	Reset Mailbox	EA	21	$300.00	$6,300.00
11	Relocate Chain Link Fence	LF	400	$50.00	$20,000.00
12	Relocate Iron Fence	LF	340	$90.00	$30,600.00
13	Relocate Wooden Fence	LF	80	$40.00	$3,200.00
14	Relocate Brick Posts and Iron Fence	LF	80	$150.00	$12,000.00
15	Relocate Water Valve	EA	1	$1,400.00	$1,400.00
16	Relocate Light Pole	EA	1	$1,500.00	$1,500.00
17	Relocate Irrigation Structure	LS	1	$20,000.00	$20,000.00
18	Relocate Fire Hydrant	EA	2	$6,000.00	$12,000.00
19	Adjust Inlet	EA	1	$3,000.00	$3,000.00
20	Adjust Manhole to Grade	EA	5	$1,200.00	$6,000.00
21	Adjust Valve Frame and Cover to Grade	EA	9	$1,200.00	$10,800.00
22	Adjust W~~ater Meter Box to Grade~~	EA	11	$800.00	$8,800.00
23	Adjust Pullbox to Grade	EA	3	$850.00	$2,550.00
24	Adjust Fire Hydrant to Grade	EA	4	$5,500.00	$22,000.00
25	Clearing and Grubbing	LS	1	$11,000.00	$11,000.00
26	Remove Tree	EA	6	$1,400.00	$8,400.00
27	Roadway Excavation (F)	CY	720	$60.00	$43,200.00
28	Erosion Control	LS	1	$5,000.00	$5,000.00
29	Class 2 Aggregate Base (F)	CY	600	$60.00	$36,000.00
30	Hot Mix Asphalt (Type B)	TON	480	$110.00	$52,800.00
31	Roadside Sign - One Post	EA	24	$250.00	$6,000.00
32	Storm Drain System	LS	1	$75,000.00	$75,000.00
33	Minor Concrete (Curb)	LF	190	$22.00	$4,180.00
34	Minor Concrete (Gutter)	SQFT	590	$10.00	$5,900.00
35	Detectable Warning Surface	SQFT	155	$45.00	$6,975.00
36	Minor Concrete (Curb and Gutter)	LF	4390	$25.00	$109,750.00
37	Minor Concrete (Driveway & Curb Ramps)	SQFT	4970	$15.00	$74,550.00
38	Minor Concrete (Sidewalk)	SQFT	31270	$7.00	$218,890.00
39	Thermoplastic Traffic Stripe	LF	4830	$1.25	$6,037.50
40	Thermoplastic Pavement Marking	SQFT	496	$6.00	$2,976.00
41	Mobilization	LS	1	$92,800.00	$92,800.00
42	Minor/ Supplemental Items	%	0	$977,514.10	$244,378.53
	Subtotal (Construction Costs)				$1,314,692.63
	Construction Contingency			25%	$ 328,673.16
	Total Construction Costs				**$1,643,365.78**
	Total Construction Budget (Rounded)				**$1,643,400.00**

Non-Construction Related Costs:					
No.	***Item Description***	***Units***	***Quantity***	***Unit Cost***	***Total***
42	Environmental Clearance	% of CON	5%	$1,162,000.00	$58,100.00
43	Right of Way/TCE Acquisition (Capital)	SQFT	1630	$5.00	$8,150.00
44	Right of Way/TCE Acquisition (Support)	Parcel	2	$5,000.00	$10,000.00
45	Final Engineering Design	% of CON	15%	$1,162,000.00	$174,300.00
46	Construction Support	% of CON	2%	$1,162,000.00	$23,240.00
47	Construction Management	% of CON	15%	$1,162,000.00	$174,300.00
48	Utility Relocations	LS	1	$100,000.00	$100,000.00
	Total Non-Construction Related Costs				**$548,090.00**
	Total Project Capital Cost				**$ 2,000,590.00**
	Rounded				**$ 2,000,000.00**

OROSI
COUNTY OF TULARE

PROJECT PLANS FOR CONSTRUCTION ON

OROSI-COMPLETE STREETS POLICY

IN OROSI AND COUNTY OF TULARE
AVE 416 (FROM SR 63 TO RD 130)

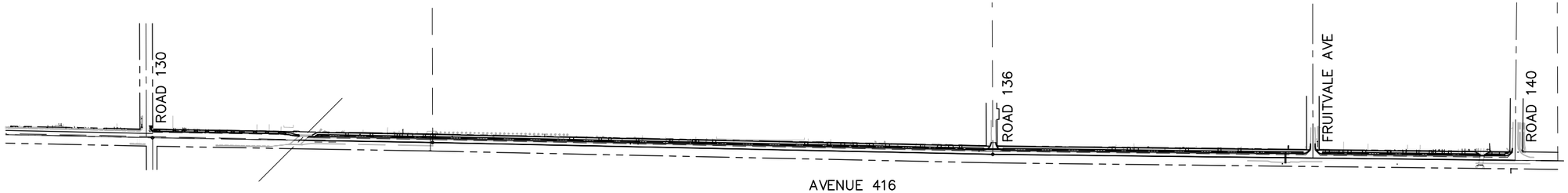
INDEX OF SHEETS

PAGE	SHEET NO.	DESCRIPTION
1	T1	TITLE SHEET
2–3	X1–X2	TYPICAL SECTIONS
4–12	L1–L9	LAYOUT

ABBREVIATIONS

AB	AGGREGATE BASE
AC	ASPHALT CONCRETE
A.D.	ALGEBRAIC DIFFERENCE
AP	ANGLE POINT
BC	BEGIN CURVE
CL	CENTER LINE
CONC	CONCRETE
CPDC	CONSOLIDATED PEOPLES DITCH COMPANY
C&G	CURB AND GUTTER
EC	END OF CURVE
EG	EXISTING GRADE
ELEV	ELEVATION
EP	EDGE OF PAVEMENT
ESMT	EASEMENT
FG	FINISH GRADE
FL	FLOW LINE
HCR	HANDICAP RAMP
HMA	HOT MIX ASPHALT
HW	HEADWALL
INV	INVERT
IRR	IRRIGATION
JS	JUNCTION STRUCTURE
K	CURVE COEFFICIENT
LF	LINEAR FEET
LT	LEFT
LVC	LENGTH OF VERTICAL CURVE
MAX	MAXIMUM
MIN	MINIMUM
(N)	NOT A PAY ITEM
NSP	NEW STANDARD PLAN
N.T.S.	NOT TO SCALE
O.C.	ON CENTER
PCC	PORTLAND CEMENT CONCRETE
PI	POINT OF INTERSECTION
POC	POINT OF CONNECTION
PT	POINT
R	RADIUS
R.C.	RELATIVE COMPACTION
RGRCP	RUBBER GASKET REINFORCED CONCRETE PIPE
RSP	REVISED STANDARD PLAN
RT	RIGHT
R/W	RIGHT OF WAY
SCE	SOUTHERN CALIFORNIA EDISON
STA	STATION
SW	SIDEWALK
TBC	TOP BACK OF CURB
TBW	TOP BACK OF WALK
TW	TOP OF WALL
TYP	TYPICAL

VICINITY MAP
N.T.S.



LOCATION MAP
N.T.S.

811
Know what's **below.**
Call before you dig.
Contractor shall call Underground Service Alert at 811 two working days prior to excavation

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

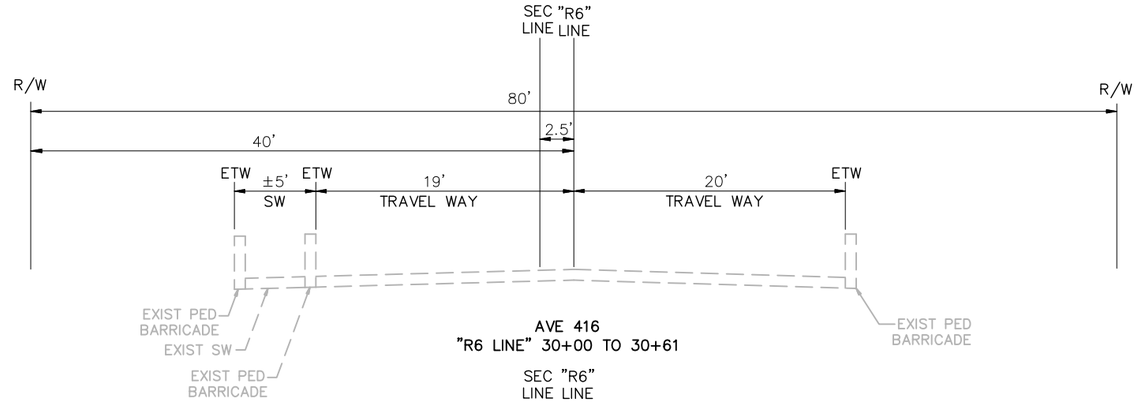
■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

omni · means
ENGINEERS PLANNERS
WILLIAMS, INC.
2050 Center Ave.
Suite A
Modesto, CA 93301
(559) 734-5895
WALNUT CREEK
REDDING

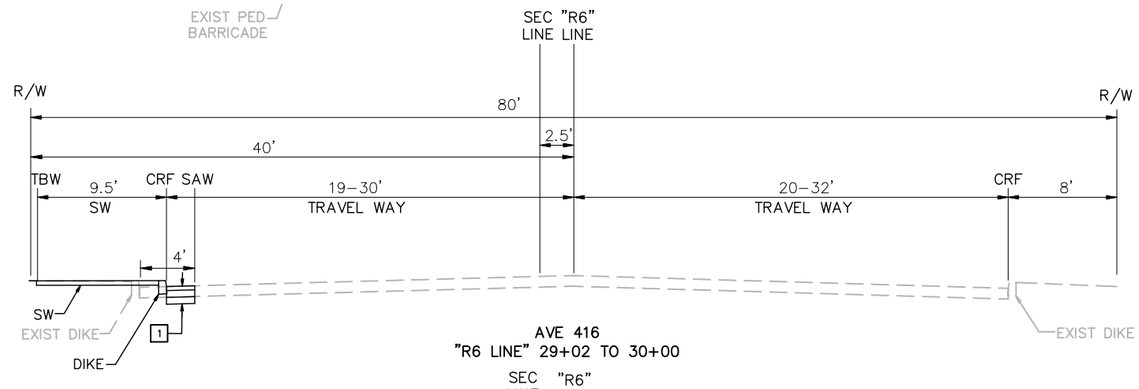
TITLE SHEET
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	N.T.S.
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	20157005.DWG
CHECKED	MJW
DATE	12-1-15

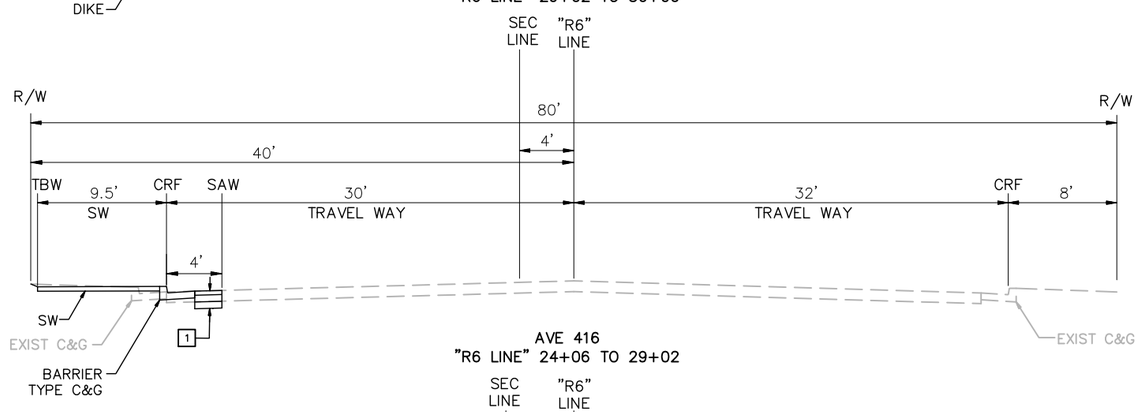
SHEET No.
T1
1 OF 12



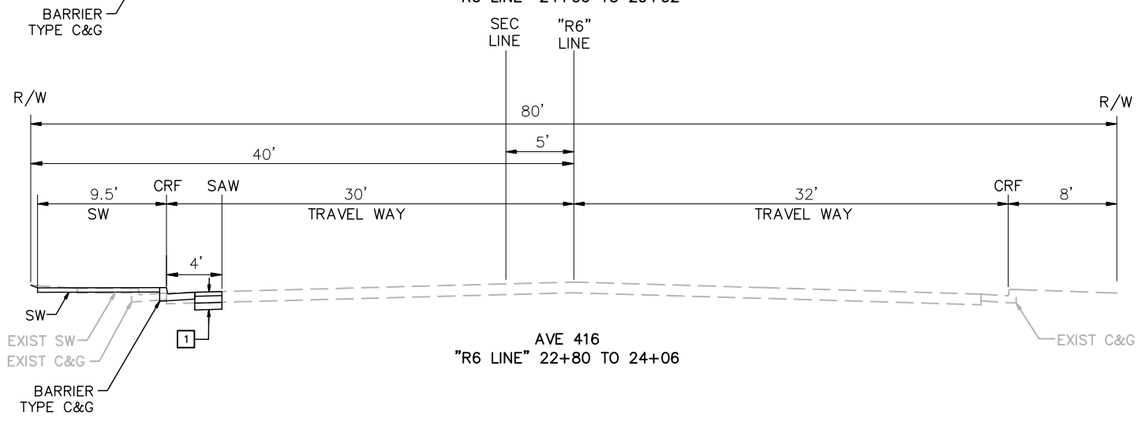
AVE 416
"R6 LINE" 30+00 TO 30+61



AVE 416
"R6 LINE" 29+02 TO 30+00



AVE 416
"R6 LINE" 24+06 TO 29+02



AVE 416
"R6 LINE" 22+80 TO 24+06

TYPICAL STRUCTURAL SECTIONS:

1
- 20–YEAR DESIGN
- TI=5.5, R=25
- 0.25' HMA (TYPE A)
- 0.70' AB (CLASS 2)
- 0.50' COMPACTED NATIVE

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
□ APPROVED
□ BID
□ CONSTRUCTION
□ RECORD

omni • means
ENGINEERS PLANNERS
2020 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895

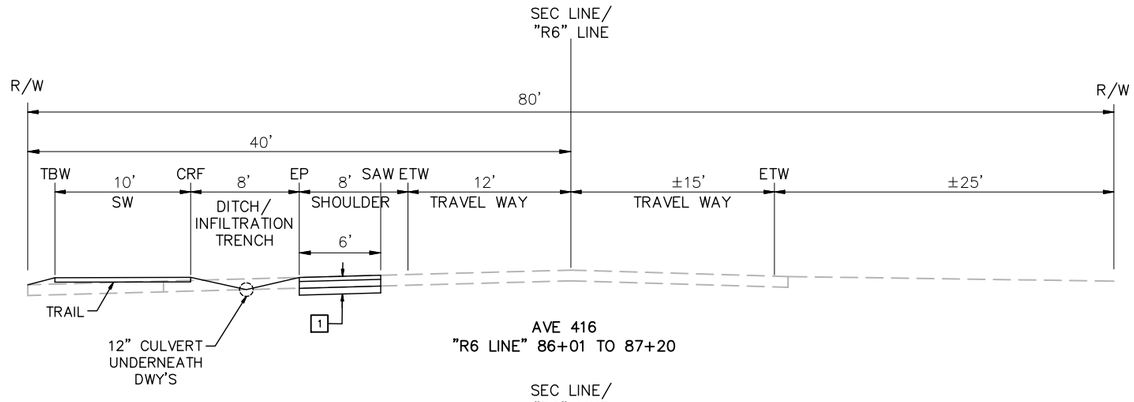
PREPARED FOR:
WALNUT CREEK
REDDING

TYPICAL SECTIONS

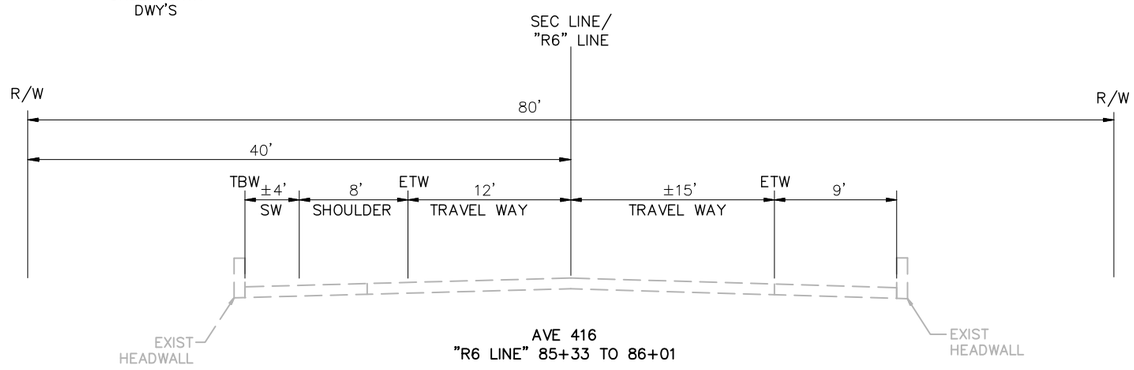
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1" = 5'
JOB NO.	55–7300–12
DESIGNED	FJD
DRAWN	FJD
FILE	2015X005.DWG
CHECKED	MJW
DATE	12–1–15

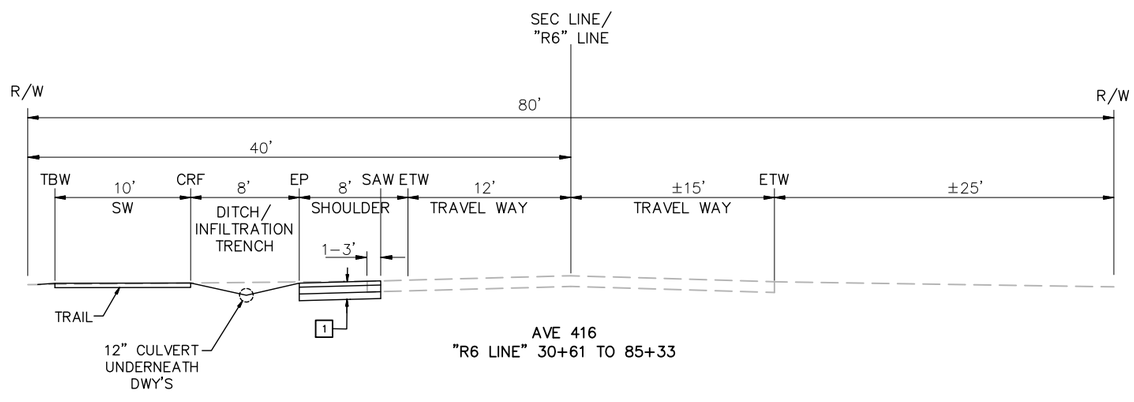
SHEET No.
X1



AVE 416
"R6 LINE" 86+01 TO 87+20



AVE 416
"R6 LINE" 85+33 TO 86+01



AVE 416
"R6 LINE" 30+61 TO 85+33

TYPICAL STRUCTURAL SECTIONS:

1
- 20-YEAR DESIGN
- TI=5.5, R=25
- 0.25' HMA (TYPE A)
- 0.70' AB (CLASS 2)
- 0.50' COMPACTED NATIVE

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS			
No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

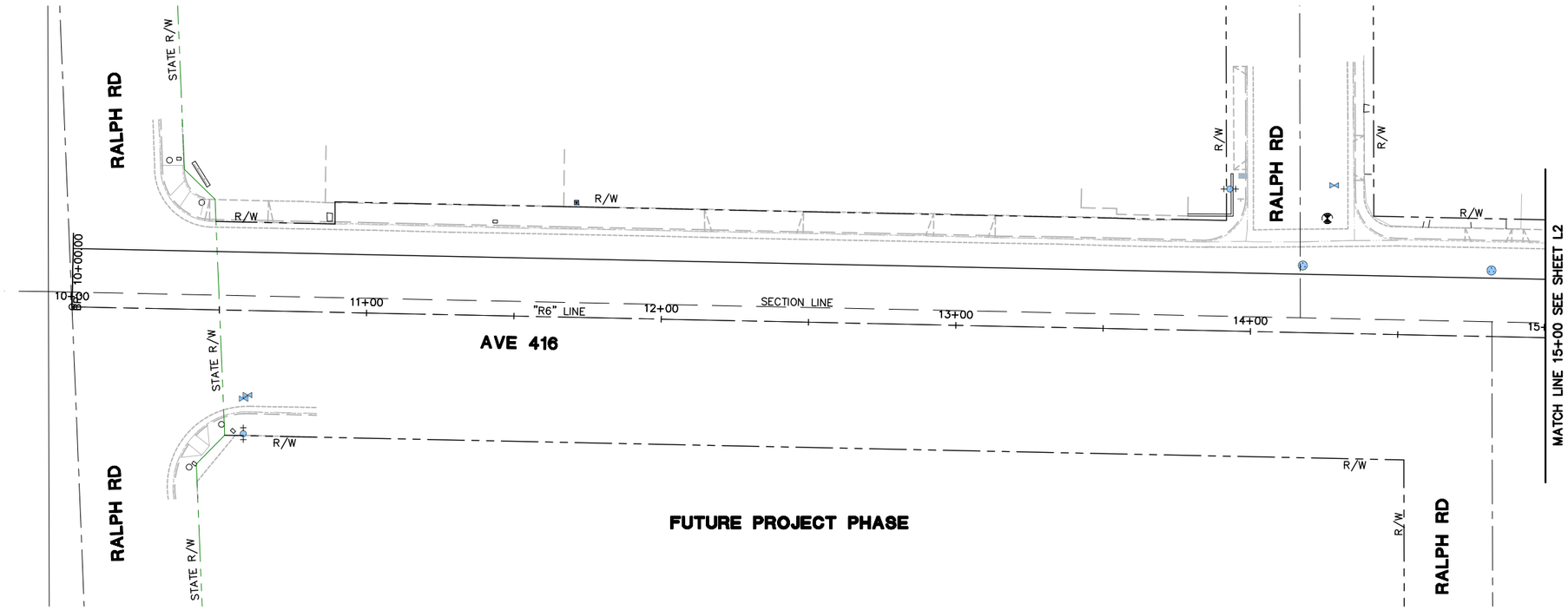
omni · means
ENGINEERS PLANNERS
LOCAL:
2000 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895
REGIONAL:
WALNUT CREEK
REDDING

TYPICAL SECTIONS

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1" = 5'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015X005.DWG
CHECKED	MJW
DATE	12-21-15

SHEET No.
X2



SCALE: 1"= 20'

REVISIONS			
No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

omni • means
ENGINEERS PLANNERS
WALNUT CREEK
2020 Center Ave.
Suite A
Walnut Creek, CA 93391
(559) 734-5895

LAYOUT

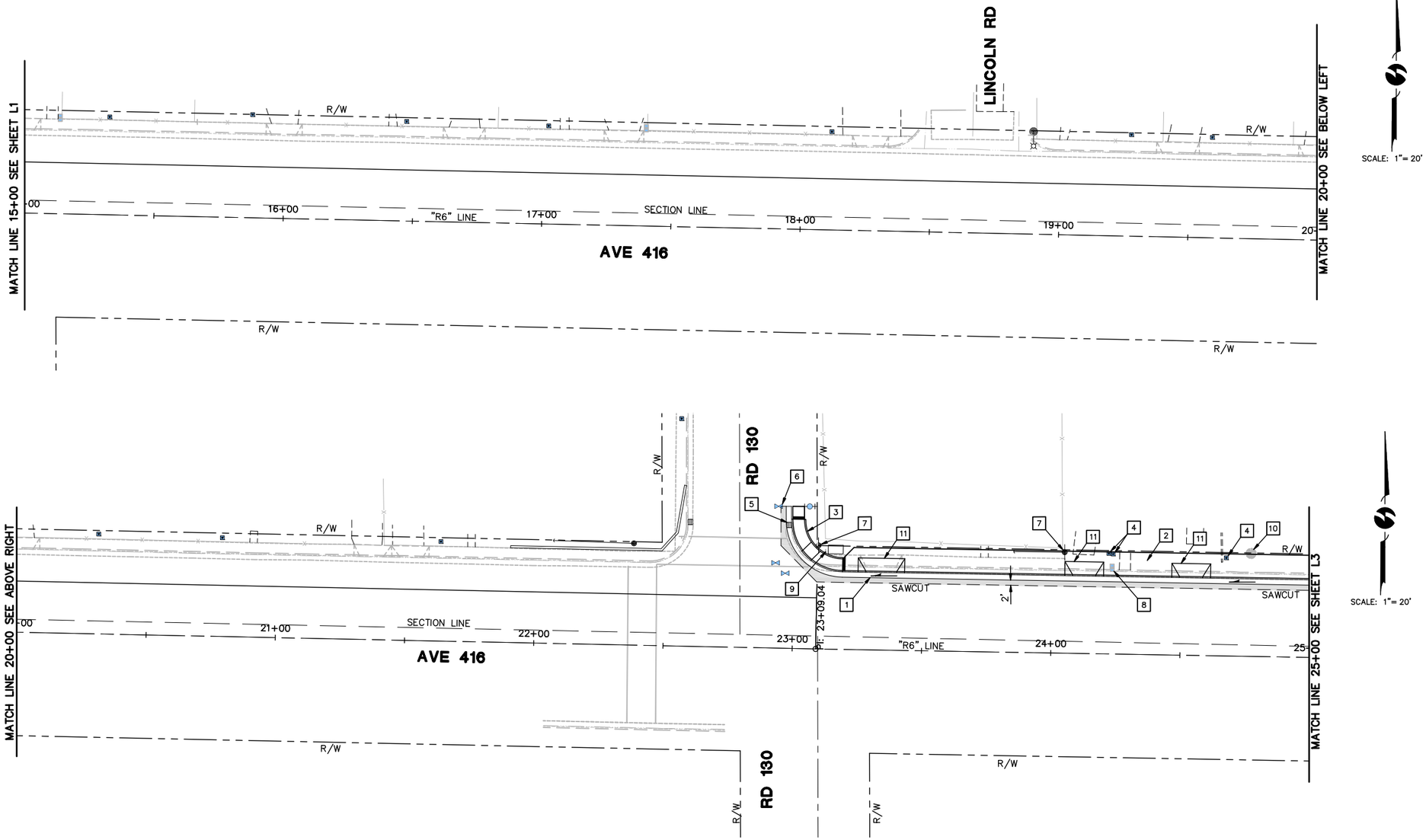
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L1

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA



KEYED NOTES (THIS SHEET ONLY)

1. C&G.
2. SIDEWALK.
3. CASE C CURB RAMP.
4. ADJUST WATER METER BOX TO GRADE.
5. ADJUST DRAIN INLET TO GRADE.
6. ADJUST WATER VALVE COVER TO GRADE.
7. Exist UTILITY POLE. PROTECT IN PLACE.
8. RESET MAILBOX.
9. RESET SIGN.
10. REMOVE TREE.
11. DRIVEWAY.

LEGEND

ROADWAY CONST.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

No.	DESCRIPTION	DATE	BY

REVISIONS

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

omni · means
ENGINEERS PLANNERS
2020 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895

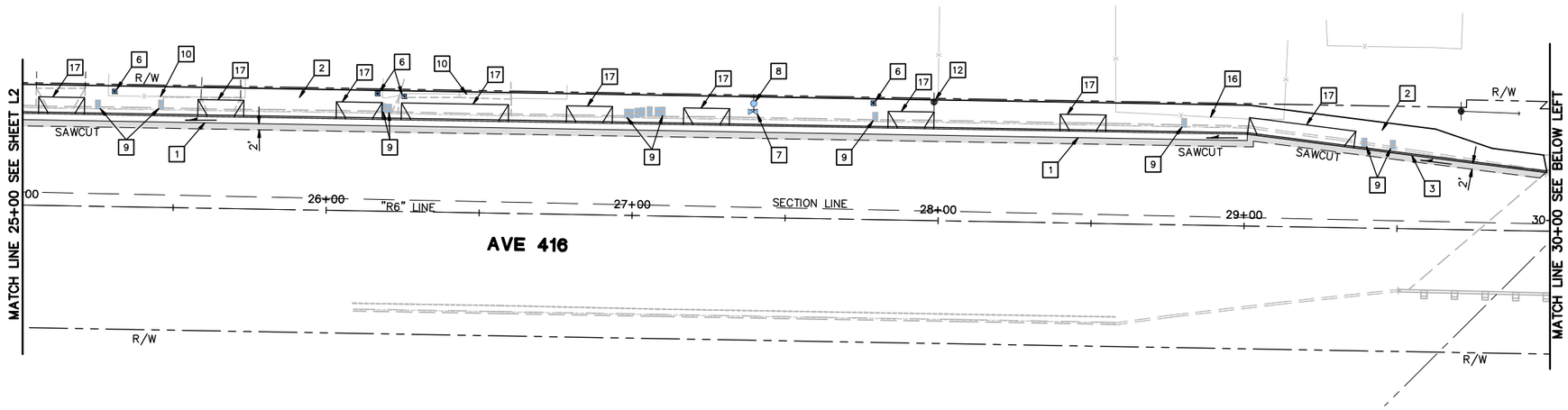
WORK IN:
RIVERMEAD
WALNUT CREEK
REDDING

LAYOUT

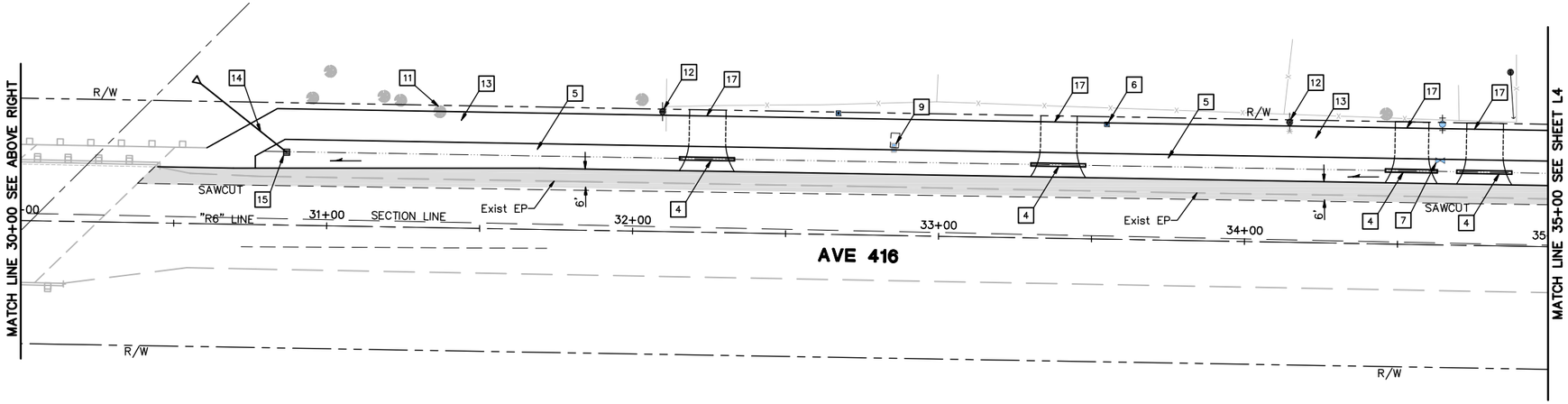
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L2
5 OF 12



SCALE: 1"= 20'



SCALE: 1"= 20'

KEYED NOTES (THIS SHEET ONLY)

1. C&G.
2. SIDEWALK.
3. HMA DIKE.
4. 12 IN. CULVERT.
5. DITCH/INFILTRATION TRENCH.
6. ADJUST WATER METER BOX TO GRADE.
7. ADJUST WATER VALVE COVER TO GRADE.
8. ADJUST FIRE HYDRANT TO GRADE.
9. RESET MAILBOX.
10. RELOCATE FENCE (BRICK AND IRON).
11. REMOVE TREE.
12. Exist UTILITY POLE. PROTECT IN PLACE.
13. CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
14. 18 IN. CULVERT.
15. DRAIN INLET.
16. RELOCATE FENCE (CHAIN LINK).
17. DRIVEWAY.

LEGEND

ROADWAY CONST.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS			
No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

omni • means
ENGINEERS PLANNERS
2020 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895

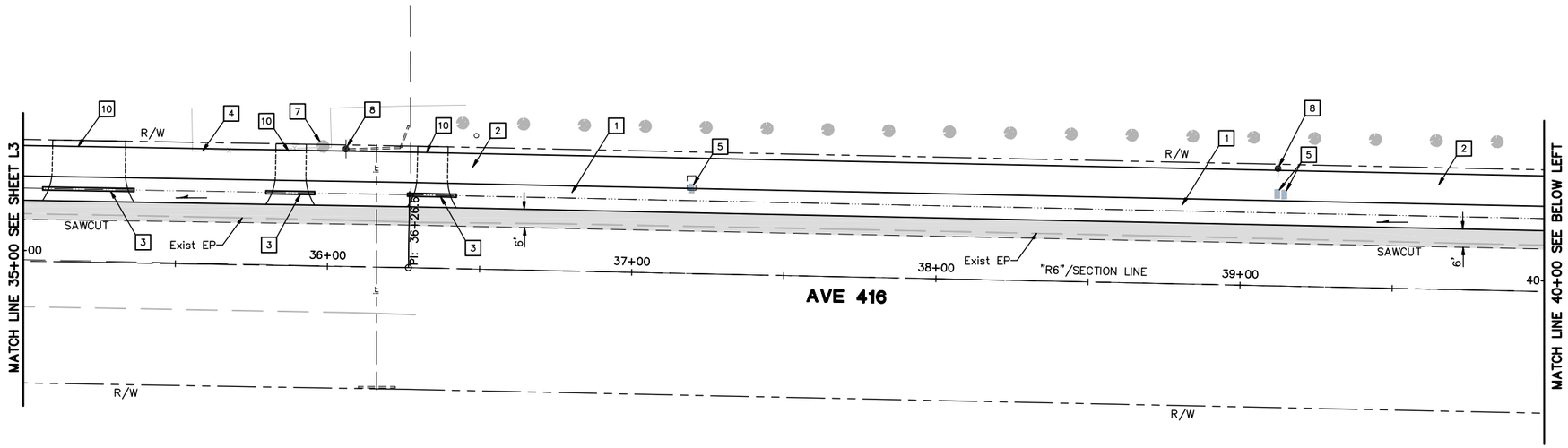
PREPARED FOR:
MOUNTAIN VIEW
WALNUT CREEK
REDDING

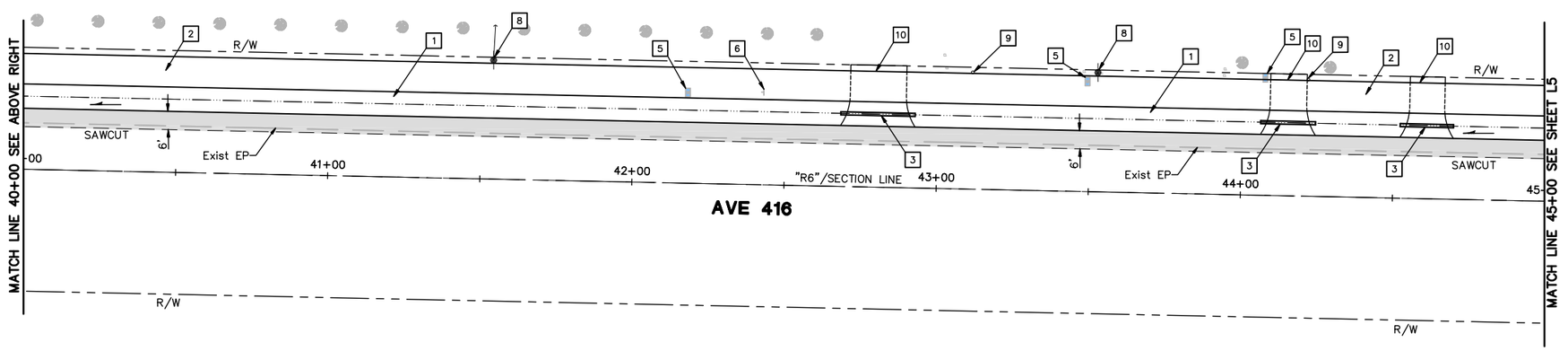
LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L3





KEYED NOTES (THIS SHEET ONLY)

1. DITCH/INFILTRATION TRENCH.
2. CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
3. 12 IN. CULVERT.
4. RELOCATE FENCE (WOODEN).
5. RESET MAILBOX.
6. RESET SIGN.
7. REMOVE TREE.
8. Exist UTILITY POLE. PROTECT IN PLACE.
9. Exist POST. PROTECT IN PLACE.
10. DRIVEWAY.

LEGEND

ROADWAY CONST.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

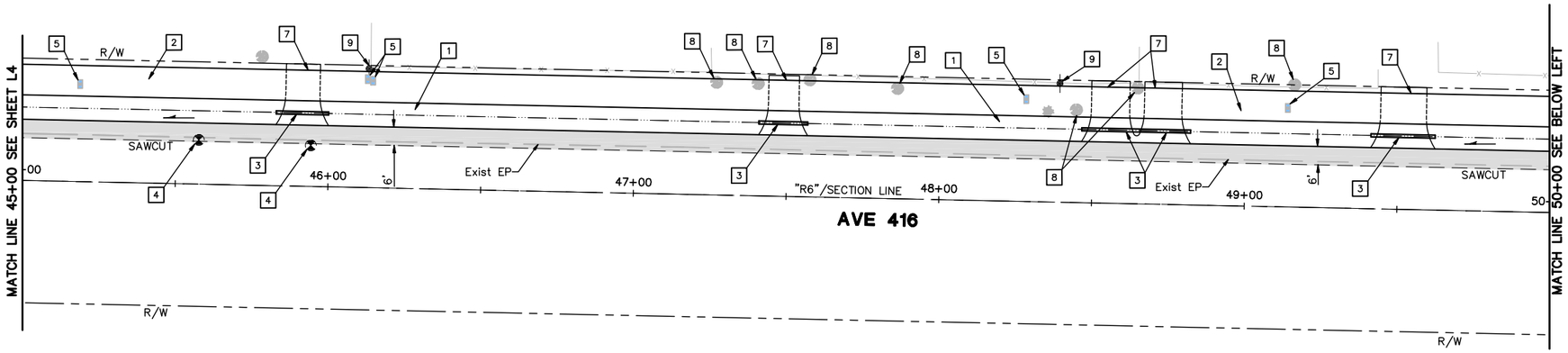
omni • means
ENGINEERS PLANNERS
LOCALLY OWNED
2020 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895
WILDWOOD
WALNUT CREEK
REDDING

LAYOUT

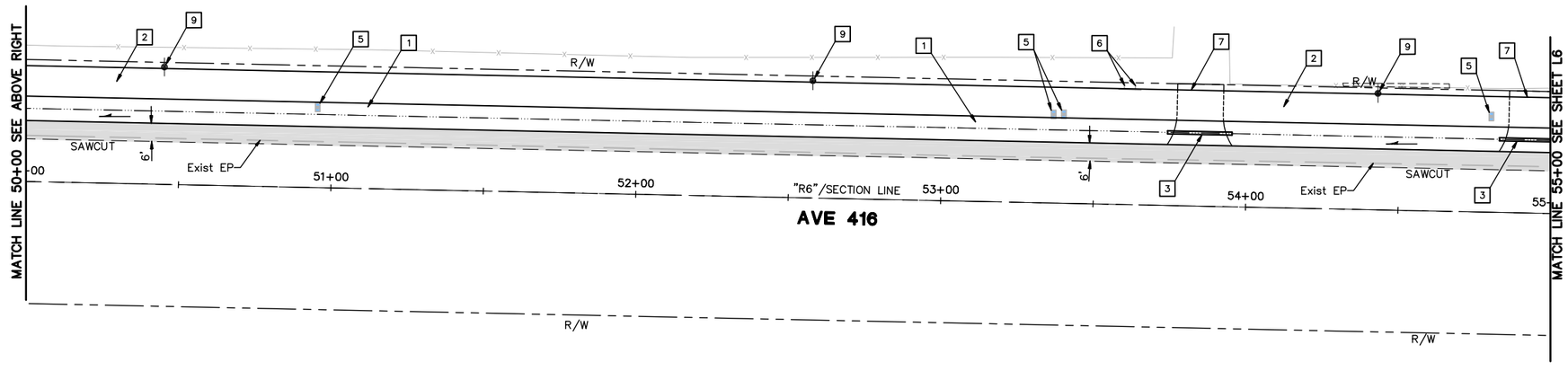
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No. L4



SCALE: 1"= 20'



SCALE: 1"= 20'

KEYED NOTES (THIS SHEET ONLY)

1. DITCH/INFILTRATION TRENCH.
2. CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
3. 12 IN. CULVERT.
4. ADJUST GAS VALVE COVER TO GRADE.
5. RESET MAILBOX.
6. RESET SIGN.
7. DRIVEWAY.
8. REMOVE TREE.
9. Exist UTILITY POLE. PROTECT IN PLACE.

LEGEND

ROADWAY CONST.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

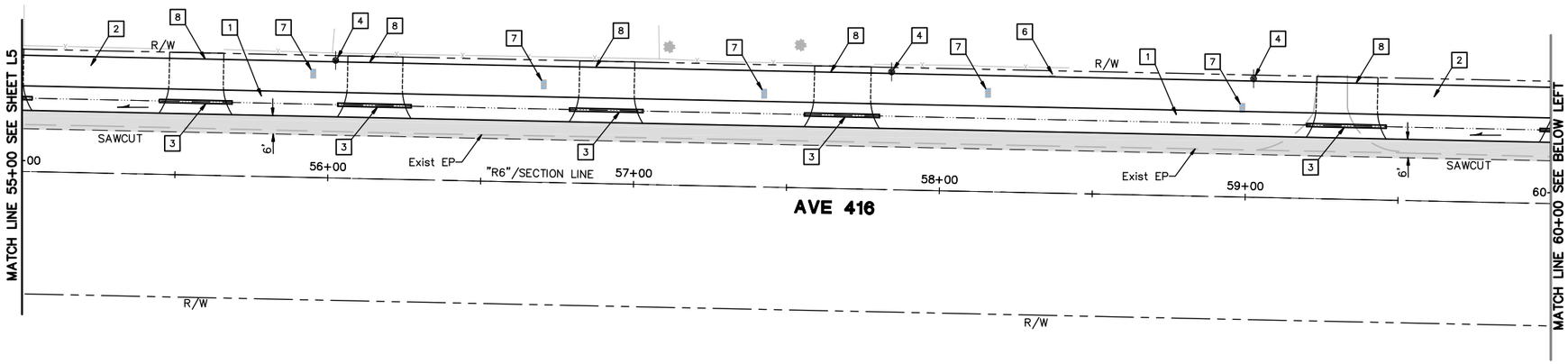
omni · means
ENGINEERS PLANNERS
Working in:
SILVERADO
WALNUT CREEK
REDDING
2020 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895

LAYOUT

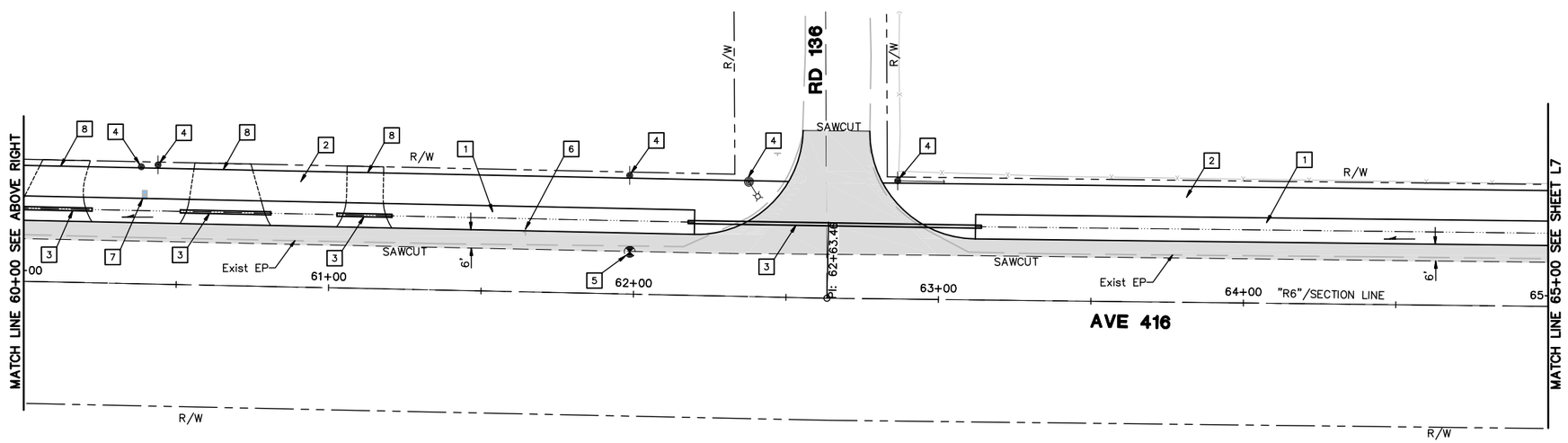
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L5



SCALE: 1"= 20'



SCALE: 1"= 20'

KEYED NOTES (THIS SHEET ONLY)

1. DITCH/INFILTRATION TRENCH.
2. CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
3. 12 IN. CULVERT.
4. Exist UTILITY POLE. PROTECT IN PLACE.
5. ADJUST GAS VALVE TO GRADE.
6. RESET SIGN.
7. RESET MAILBOX.
8. DRIVEWAY.

LEGEND

ROADWAY CONST.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS			
No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

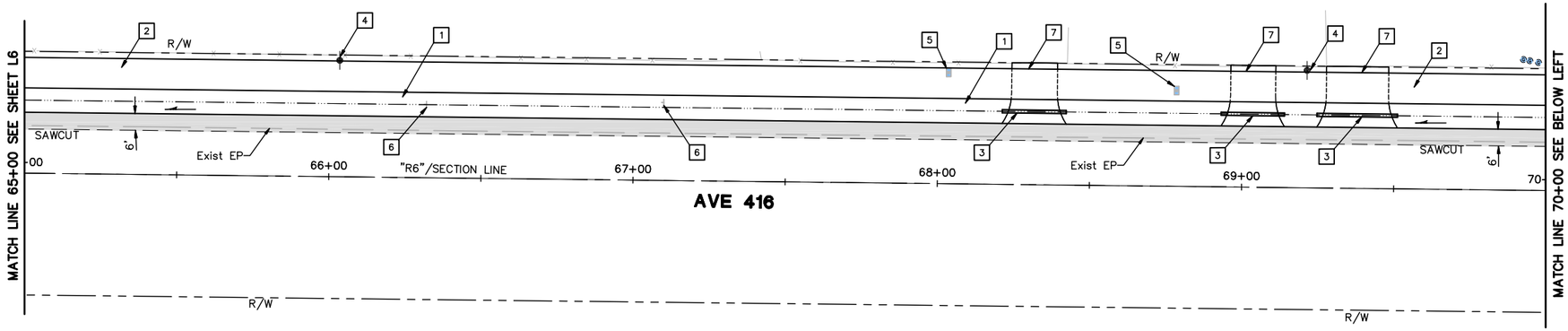
omni · means
ENGINEERS PLANNERS
Working in:
WILDWOOD
WALNUT CREEK
REDDING
Venue:
2020 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895

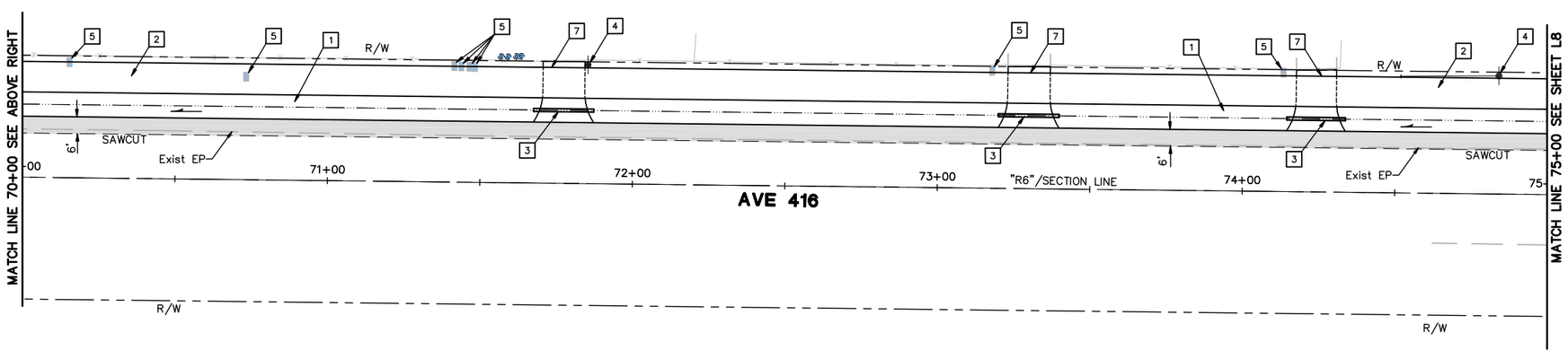
LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L6





KEYED NOTES (THIS SHEET ONLY)

1. DITCH/INFILTRATION TRENCH.
2. CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
3. 12 IN. CULVERT.
4. Exist UTILITY POLE. PROTECT IN PLACE.
5. RESET MAILBOX.
6. RESET SIGN.
7. DRIVEWAY.

LEGEND

ROADWAY CONST.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

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MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS			
No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

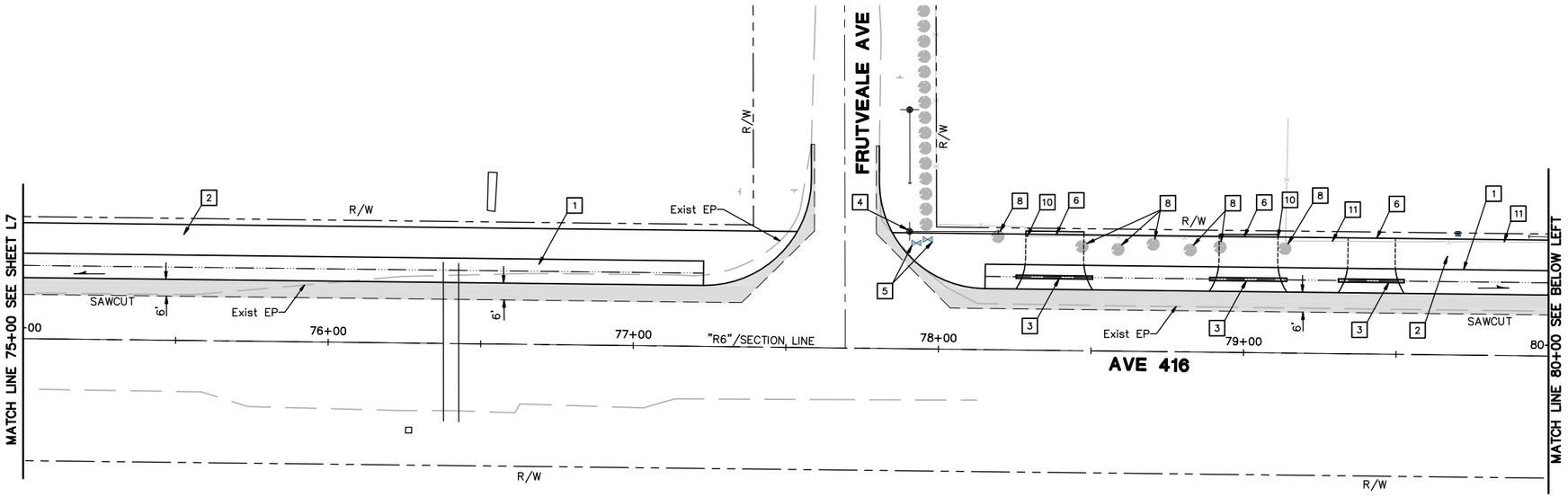
omni · means
ENGINEERS PLANNERS
Working in:
WILDWOOD
WALNUT CREEK
REDDING
2020 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895

LAYOUT

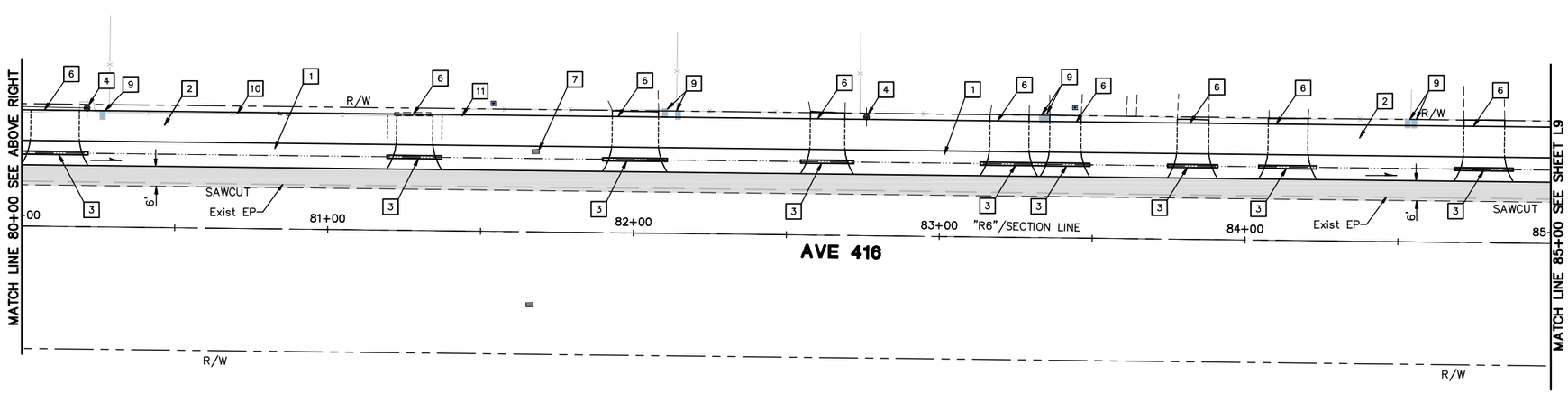
COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L7



SCALE: 1"= 20'



SCALE: 1"= 20'

KEYED NOTES (THIS SHEET ONLY)

1. DITCH/INFILTRATIN TRENCH.
2. CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
3. 12 IN. CULVERT.
4. Exist UTILITY POLE. PROTECT IN PLACE.
5. ADJUST WATER VALVE COVER TO GRADE.
6. DRIVEWAY.
7. REMOVE DRAIN INLET.
8. REMOVE TREE.
9. RESET MAILBOX.
10. RELOCATE FENCE (IRON).
11. RELOCATE FENCE (CHAIN LINK).

LEGEND

ROADWAY CONST.

SLOPE ARROW. INDICATES DIRECTION OF FLOW.

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

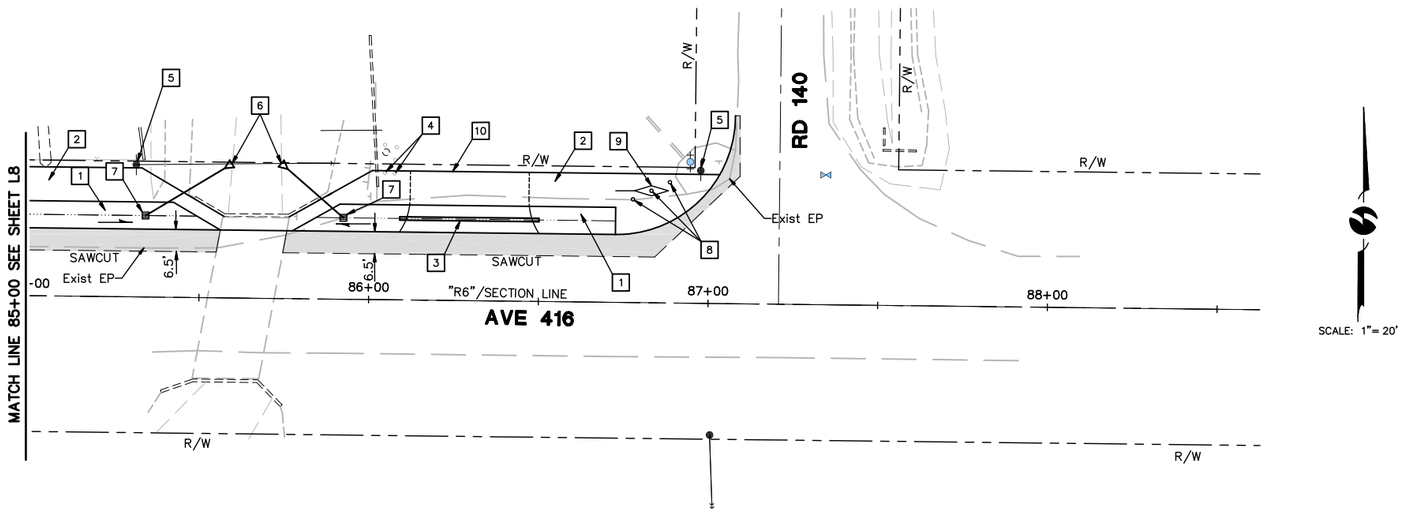
omni • means
ENGINEERS PLANNERS
VISALIA
2001 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895
WALNUT CREEK
REDDING

LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L8



KEYED NOTES (THIS SHEET ONLY)

1. DITCH/INFILTRATION TRENCH.
2. CLASS II BIKE PATH (2" HMA/4" CLASS 2 AB).
3. 12 IN. CULVERT.
4. REMOVE BOLLARD.
5. Exist UTILITY POLE. PROTECT IN PLACE.
6. 18 IN. CULVERT.
7. DRAIN INLET.
8. REMOVABLE BOLLARD POST WITH TYPE 3 MARKER.
9. ENVELOPE STRIPPING NORMAL SOLID YELLOW LINE (MUTCD FIG.9C-8).
10. DRIVEWAY.

LEGEND

ROADWAY CONST.

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MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

■ PRELIMINARY
☐ APPROVED
☐ BID
☐ CONSTRUCTION
☐ RECORD

omni • means
ENGINEERS PLANNERS
2020 L Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895
REVISIONS TO:
WALNUT CREEK
REDDING

LAYOUT

COMPLETE STREETS IMPROVEMENTS
PHASE 1 - RD 140 TO RD 130
OROSI, CALIFORNIA

SCALE	1"=20'
JOB NO.	55-7300-12
DESIGNED	FJD
DRAWN	FJD
FILE	2015L005.DWG
CHECKED	MJW
DATE	12-1-15

SHEET No.
L9
12 OF 12

INDEX OF SHEETS

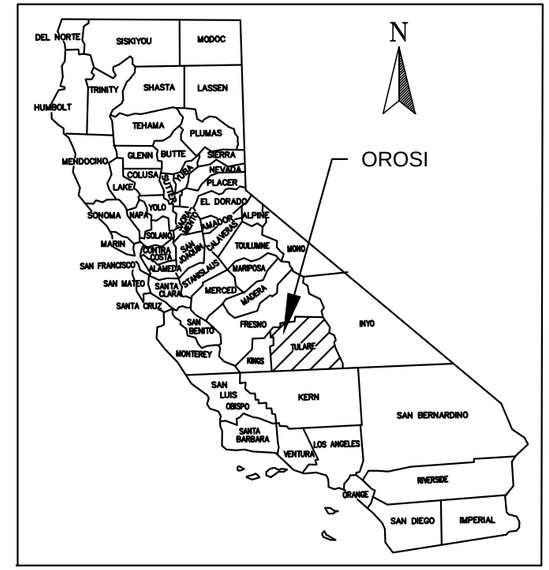
PAGE	SHEET NO.	DESCRIPTION
1	T1	COVER
2	X1	TYPICAL SECTIONS
3–5	L1–L3	LAYOUT SHEETS
6	C1	CONSTRUCTION DETAILS
7–9	PD1–PD3	SIGNING AND STRIPING PLAN

STATE OF CALIFORNIA
COUNTY OF TULARE

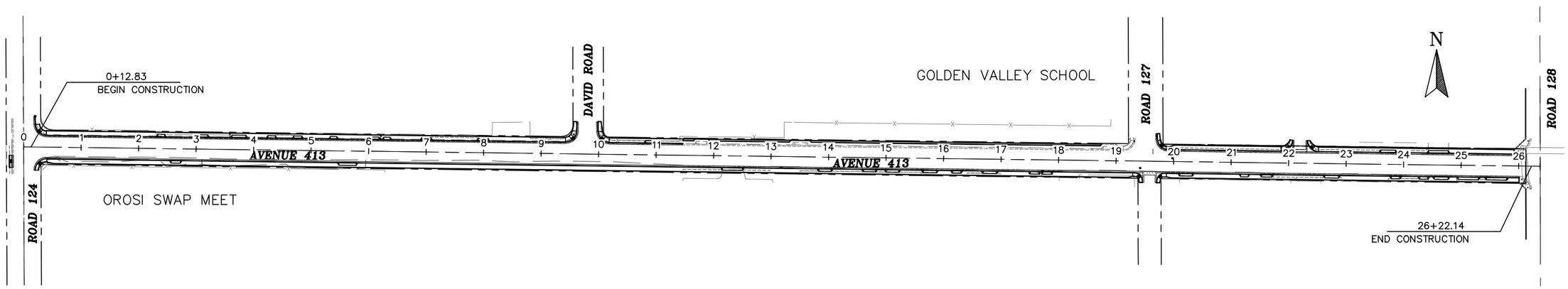
PROJECT PLANS FOR CONSTRUCTION OF

OROSI-COMPLETE STREETS POLICY

IN OROSI AND COUNTY OF TULARE
AVENUE 413 (FROM ROAD 124 TO STATE ROUTE 63)



LOCATION MAP
N.T.S.



VICINITY MAP
N.T.S.

Know what's **below.** **Call** before you dig.

Contractor shall call Underground Service Alert at 811 two working days prior to excavation

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

APPROVED BY:

JABED KHAN, P.E. — TULARE COUNTY RESOURCE MANAGEMENT AGENCY — DATE

APPROVED BY:

MICHAEL J. WINTON P.E. — PROJECT MANAGER OMNI–MEANS LTD. — DATE

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624–7000
WWW.TULARECOUNTY.CA.GOV/RMA

omni•means
ENGINEERS PLANNERS
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734–5895
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

COVER
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	NO SCALE
DIVISION	RMA
JOB NO.	55–7300–14
DESIGNED	JLR
DRAWN	JLR
CHECKED	SMH
FILE	2144T003.DWG
DATE	6–9–16

SHEET No.
T1

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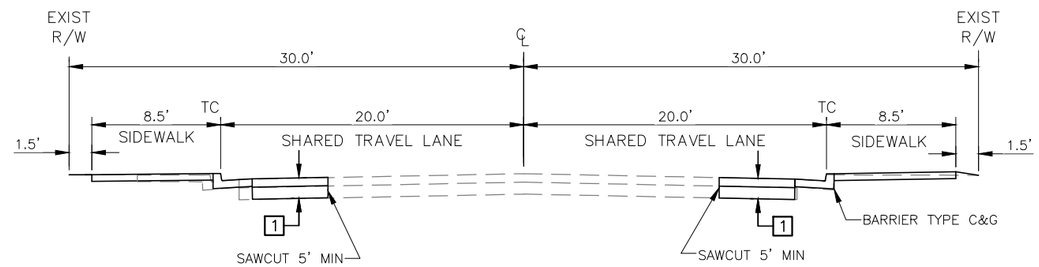
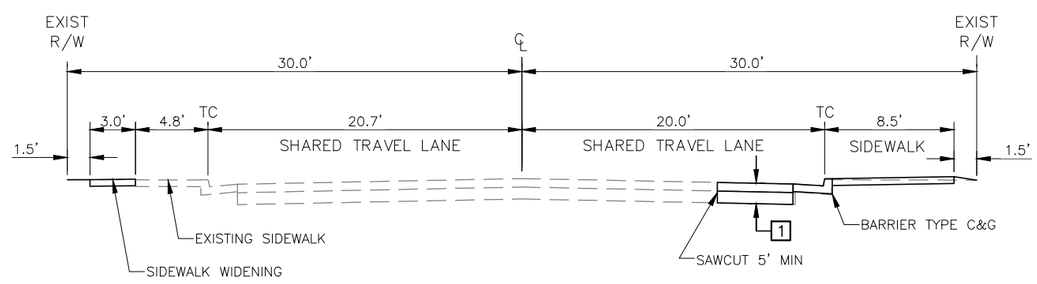
TYPICAL STRUCTURAL SECTIONS:

1 — 20-YEAR DESIGN
TI=5.5, R=25 (ASSUMED)
0.25' HMA (TYPE B)
0.70' AB (CLASS 2) (95% RC)
1.00' COMPACTED NATIVE (95% RC)



AVENUE 413
0+00 TO 11+46.51
18+51.85 TO 26+1.26



AVENUE 413
11+46.51 TO 18+51.85

30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
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ENGINEERS PLANNERS
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200 E. Center Ave.
Suite 1
Visalia, CA 93291
(559) 734-5895
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

TYPICAL SECTIONS
AVENUE 413 – OROSI
TULARE COUNTY COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=5'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JAV
CHECKED	SMH
FILE	2144X003.DWG
DATE	6-9-16
SHEET No.	X1

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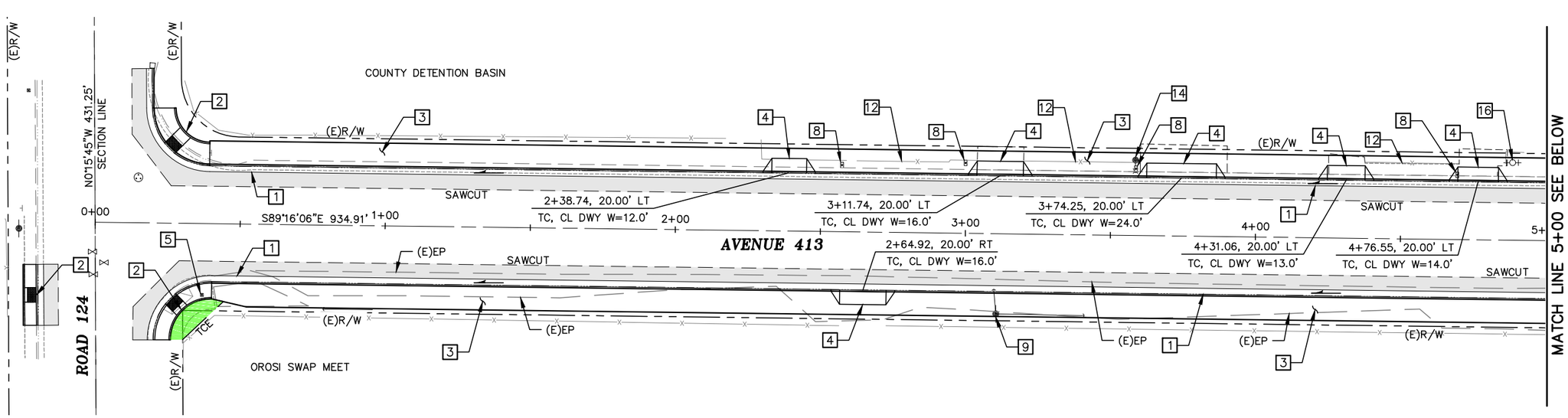

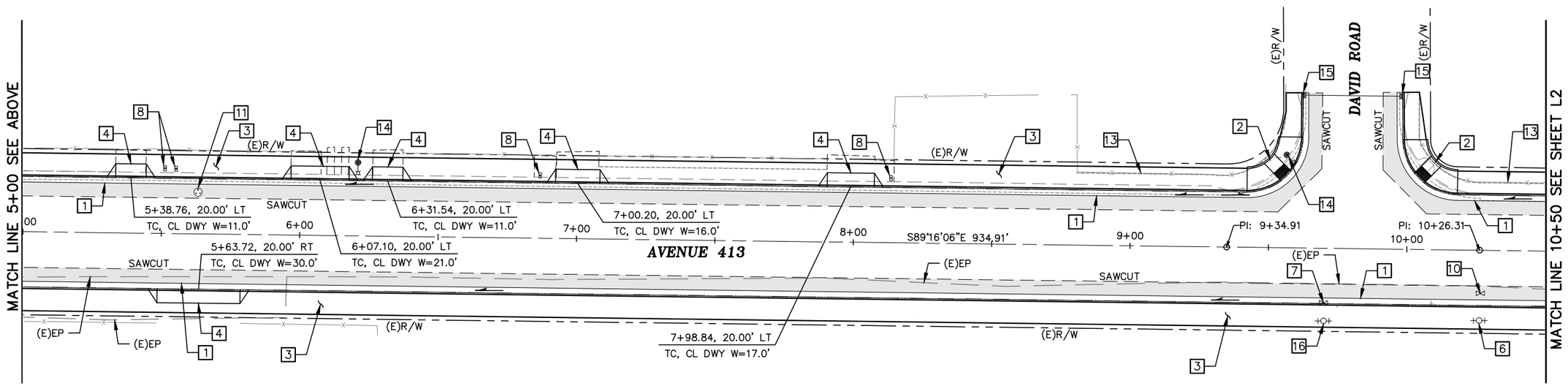

GENERAL NOTES (ALL L–SHEETS):

1. SEE SIGNING AND STRIPING PLAN FOR EXISTING SIGNS TO BE REMOVED OR RELOCATED.

CONSTRUCTION NOTES (THIS SHEET ONLY):

1. TULARE COUNTY STANDARD BARRIER TYPE CURB AND GUTTER PER PLATE NO. A–19. SEE SHEET C1.
2. CASE C RAMP. SEE SHEET C1.
3. SIDEWALK.
4. TULARE COUNTY STANDARD URBAN DRIVEWAY. SEE SHEET C1.
5. ADJUST WATER METER BOX TO GRADE.
6. RELOCATE FIRE HYDRANT TO BACK OF CURB.
7. RELOCATE WATER VALVE.
8. RESET MAILBOX.
9. RELOCATE UTILITY POLE. (BY OTHERS)
10. ADJUST WATER VALVE FRAME AND COVER TO GRADE.
11. ADJUST MANHOLE TO GRADE.
12. RELOCATE FENCE (CHAIN LINK).
13. RELOCATE FENCE (IRON).
14. RELOCATE LIGHT POLE.
15. RECONSTRUCT INLET.
16. ADJUST FIRE HYDRANT TO GRADE.

LEGEND:

ROADWAY CONSTRUCTION.

SLOPE ARROW, INDICATES DIRECTION OF FLOW.

811 Know what's **below.** **Call** before you dig.

Contractor shall call Underground Service Alert at 811 two working days prior to excavation

30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER MICHAEL J. WINTON No. 66910 CIVIL STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624–7000
WWW.TULARECOUNTY.CA.GOV/RMA

omni•means ENGINEERS PLANNERS
VISALIA 200 E. Center Ave. Suite 100 Visalia, CA 93291 (559) 734–5895
With offices in: SACRAMENTO WALNUT CREEK REDDING SAN LUIS OBISPO

LAYOUT
AVENUE 413 – OROSI
TULARE COUNTY COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55–7300–14
DESIGNED	JAV
DRAWN	JLR
CHECKED	SMH
FILE	2144L003.DWG
DATE	6–9–16
SHEET No.	L1

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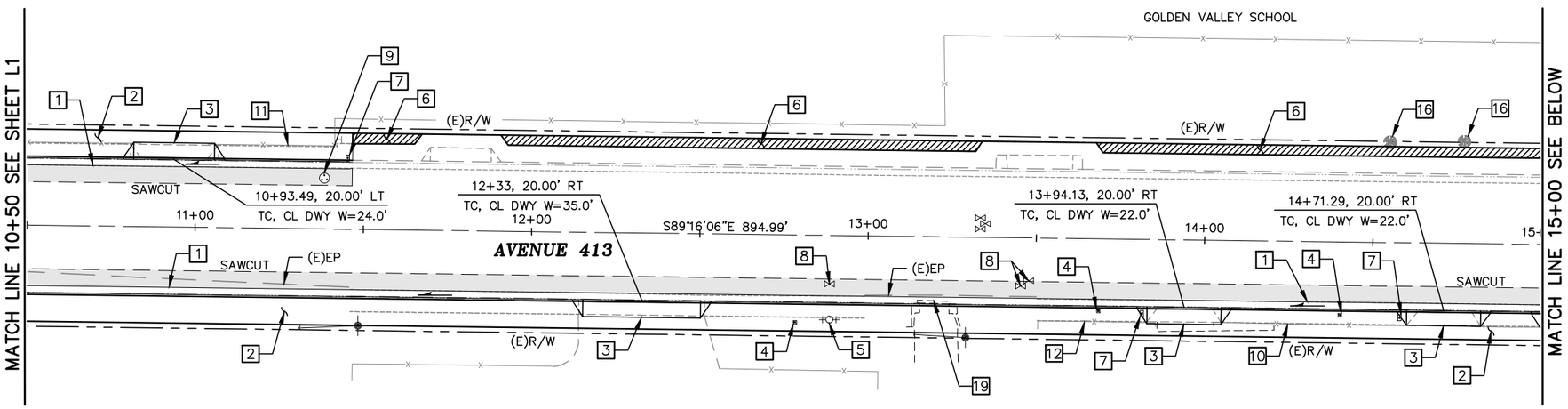

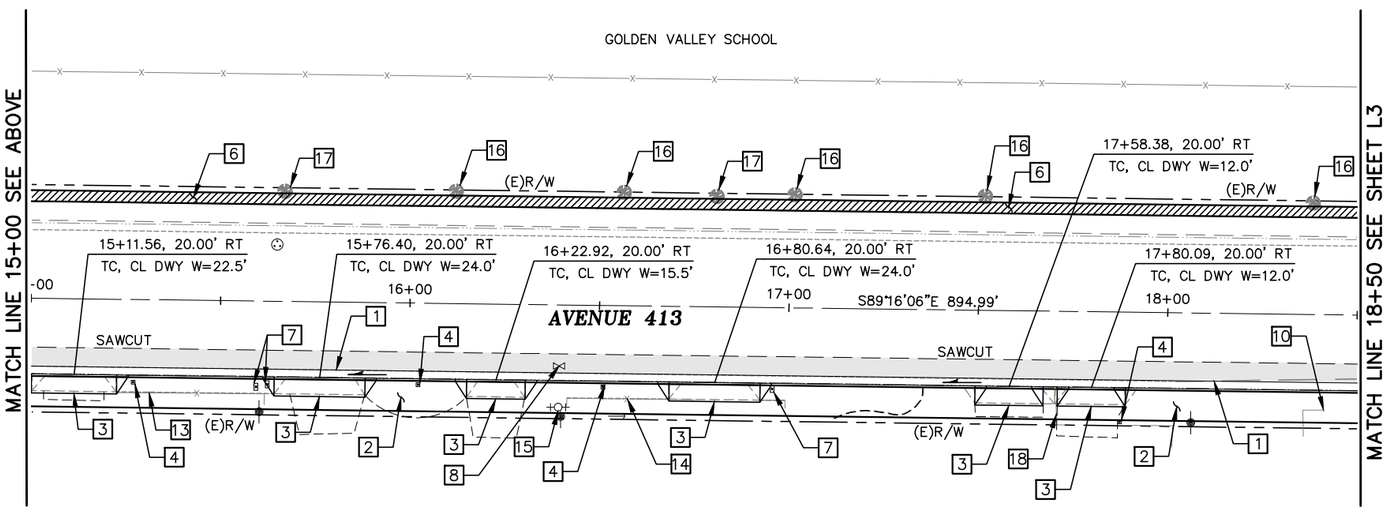

CONSTRUCTION NOTES (THIS SHEET ONLY):

1. TULARE COUNTY STANDARD BARRIER TYPE CURB AND GUTTER PER PLATE NO. A-19. SEE SHEET C1.
2. SIDEWALK.
3. TULARE COUNTY STANDARD URBAN DRIVEWAY. SEE SHEET C1.
4. ADJUST WATER METER BOX TO GRADE.
5. RELOCATE FIRE HYDRANT TO BACK OF CURB.
6. WIDEN SIDEWALK 3'.
7. RESET MAILBOX.
8. ADJUST WATER VALVE FRAME AND COVER TO GRADE.
9. ADJUST MANHOLE TO GRADE.
10. RELOCATE FENCE (CHAIN LINK).
11. RELOCATE FENCE (IRON).
12. RELOCATE FENCE (BRICK POST/IRON).
13. RELOCATE FENCE (WOODEN).
14. RELOCATE FENCE (BRICK).
15. ADJUST FIRE HYDRANT TO GRADE.
16. TREES, PROTECT IN PLACE.
17. REMOVE TREES.
18. REMOVE BRICK WALL/FENCE TO R/W.
19. RELOCATE IRRIGATION STRUCTURE.

LEGEND:

- ROADWAY CONSTRUCTION.
- 3' SIDEWALK WIDENING.
- SLOPE ARROW, INDICATES DIRECTION OF FLOW.

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30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER MICHAEL J. WINTON No. 66910 CIVIL STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

omni•means ENGINEERS PLANNERS
VISALIA 200 E. Center Ave. Suite A Visalia, CA 93291 (559) 734-5895
With offices in: SACRAMENTO WALNUT CREEK REDDING SAN LUIS OBISPO

LAYOUT
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JAV
DRAWN	JLR
CHECKED	SMH
FILE	2144L003.DWG
DATE	6-9-16
SHEET No.	L2

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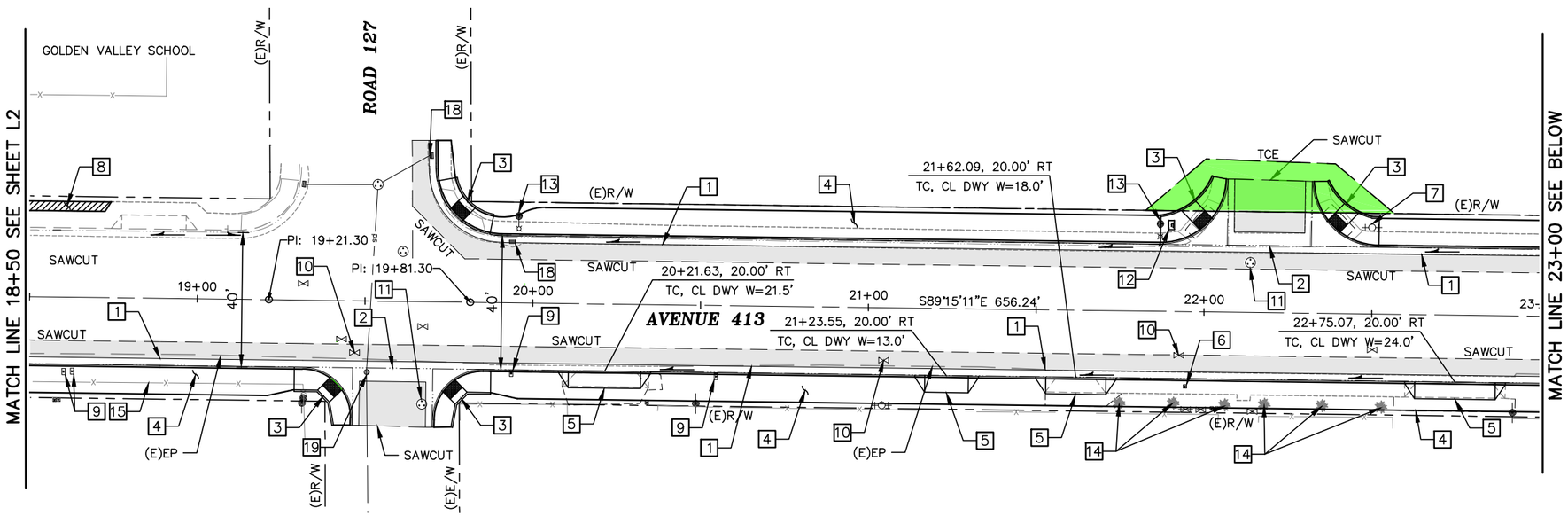

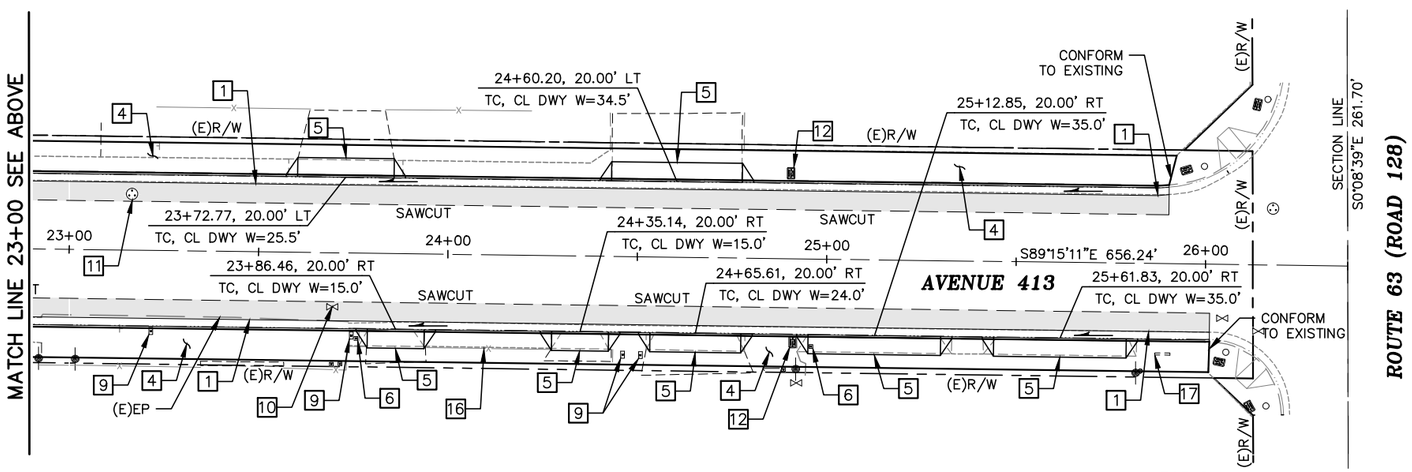

CONSTRUCTION NOTES (THIS SHEET ONLY):

1. TULARE COUNTY STANDARD BARRIER TYPE CURB AND GUTTER PER PLATE NO. A–19. SEE SHEET C1.
2. CONSTRUCT CONTINUOUS GUTTER CURB RETURN PER DETAIL ON SHEET C1.
3. CASE C RAMP (COUNTY STD). SEE SHEET C1.
4. SIDEWALK.
5. TULARE COUNTY STANDARD URBAN DRIVEWAY. SEE SHEET C1.
6. ADJUST WATER METER BOX TO GRADE.
7. ADJUST FIRE HYDRANT TO GRADE.
8. WIDEN SIDEWALK 3'.
9. RESET MAILBOX.
10. ADJUST WATER VALVE FRAME AND COVER TO GRADE.
11. ADJUST MANHOLE TO GRADE.
12. ADJUST PULLBOX TO GRADE.
13. EXIST LIGHT POLE TO REMAIN, PROTECT IN PLACE.
14. REMOVE TREES/SHRUBS.
15. RELOCATE FENCE (CHAIN LINK).
16. RELOCATE FENCE (IRON).
17. REMOVE CONCRETE WHEEL STOP.
18. RECONSTRUCT INLET.
19. ADJUST INLET TO GRADE.

LEGEND:

- ROADWAY CONSTRUCTION.
- 3' SIDEWALK WIDENING.
- TEMPORARY CONSTRUCTION EASEMENT (TCE).
- SLOPE ARROW, INDICATES DIRECTION OF FLOW.

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30% SUBMITTAL PRELIMINARY, NOT FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER MICHAEL J. WINTON No. 66910 CIVIL STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY RESOURCE MANAGEMENT AGENCY
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With offices in: SACRAMENTO WALNUT CREEK REDDING SAN LUIS OBISPO

LAYOUT
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55–7300–14
DESIGNED	JAV
DRAWN	JLR
CHECKED	SMH
FILE	2144L003.DWG
DATE	6–9–16
SHEET No.	L3

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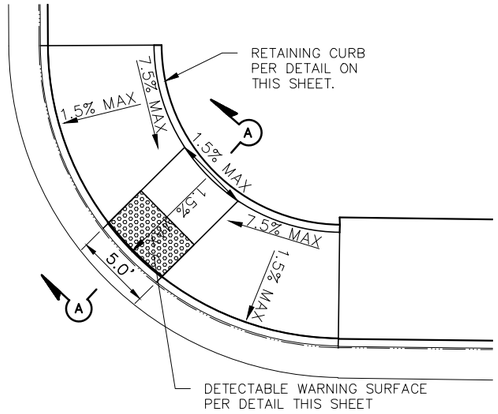
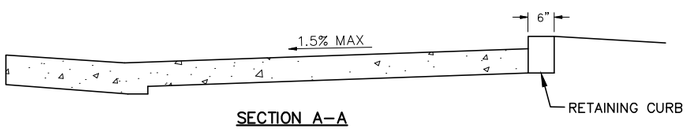
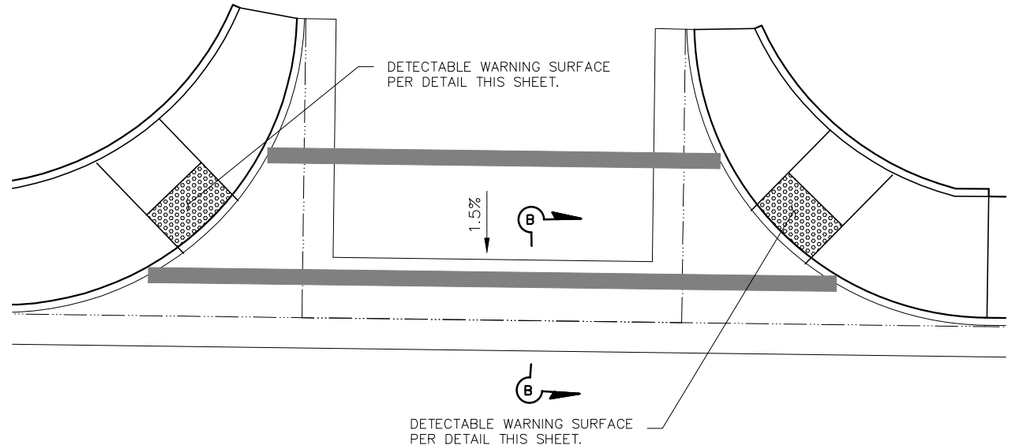


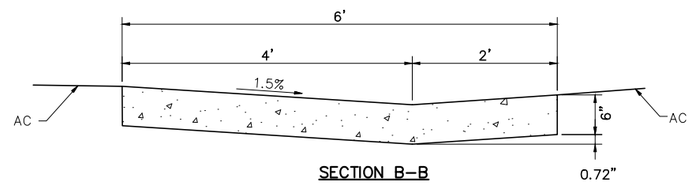


SECTION A–A

CASE C CURB RAMP DETAIL

N.T.S

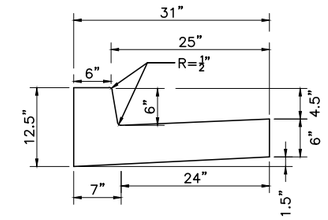




SECTION B–B

CONTINUOUS GUTTER CURB RETURN DETAIL

N.T.S.

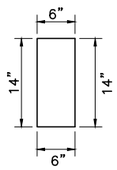


NOTES:

1. ALL CONCRETE SHALL BE MINOR CONCRETE.
2. BARRIER TYPE CURB AND GUTTER SHALL HAVE A MINIMUM OF 0.15 FEET PER 100 FEET.
3. AREA BETWEEN BACK OF CURB AND AND PROPERTY LINE SHALL BE BACK FILLED AND SLOPED TO DRAIN TO GUTTER.

CURB & GUTTER - COUNTY

SCALE: 1"=1'–0"

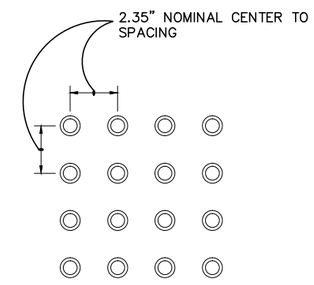


NOTES:

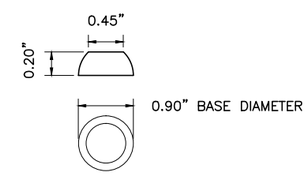
1. ALL CONCRETE SHALL BE MINOR CONCRETE.

RETAINING CURB DETAIL

SCALE: 1"=1'–0"



RAISED TRUNCATED DOME PATTERN



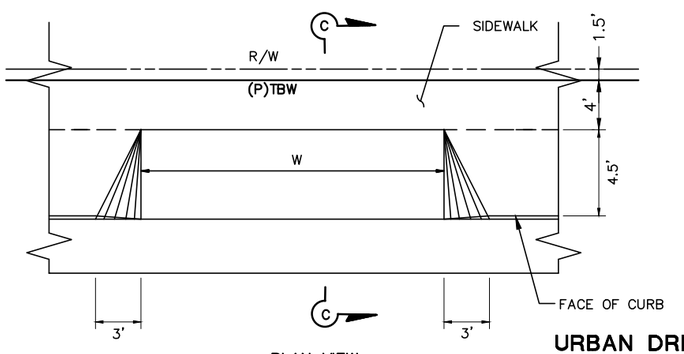
RAISED TRUNCATED DOME

NOTES:

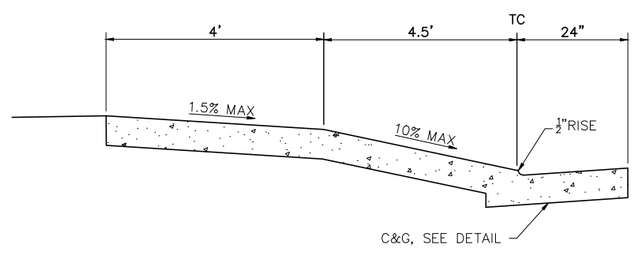
1. SHALL BE INSTALLED AT THE BOTTOM OF ALL CURB RAMPS.
2. SHALL BE THE FULL WIDTH OF RAMP AND SHALL BE A MIN. OF 36" IN DEPTH
3. SHALL BE PREMIXED FEDERAL YELLOW COLORED POLYMER CONCRETE MATERIAL.
4. ALL DETECTABLE WARNING PANELS INSTALLED WITH NEW IMPROVEMENTS SHALL BE WET SET TYPE/CAST IN PLACE TYPE PANELS.
5. A 4'–0" WIDE DETECTABLE WARNING SURFACE MAY BE USED ON A 4'–2" WIDE CURB RAMP.
6. DETECTABLE WARNING SURFACES SHALL CONFORM TO THE REQUIREMENTS IN THE STANDARDS SPECIFICATIONS.

DETECTABLE WARNING SURFACE

N.T.S



PLAN VIEW



SECTION C–C

URBAN DRIVEWAY DETAIL

N.T.S

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REGISTERED PROFESSIONAL ENGINEER
MICHAEL J. WINTON
No. 66910
CIVIL
STATE OF CALIFORNIA

REVISIONS

No.	DESCRIPTION	DATE	BY

TULARE COUNTY
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

COUNTY OF TULARE CALIFORNIA

omni•means
ENGINEERS PLANNERS
VISALIA
200 E. Center Ave.
Suite A
Visalia, CA 93291
(559) 734-5895
With offices in:
SACRAMENTO
WALNUT CREEK
REDDING
SAN LUIS OBISPO

CONSTRUCTION DETAILS
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	AS SHOWN
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JLR
DRAWN	JLR
CHECKED	SMH
FILE	2144C003.DWG
DATE	6-9-16
SHEET No.	C1

6 OF 9

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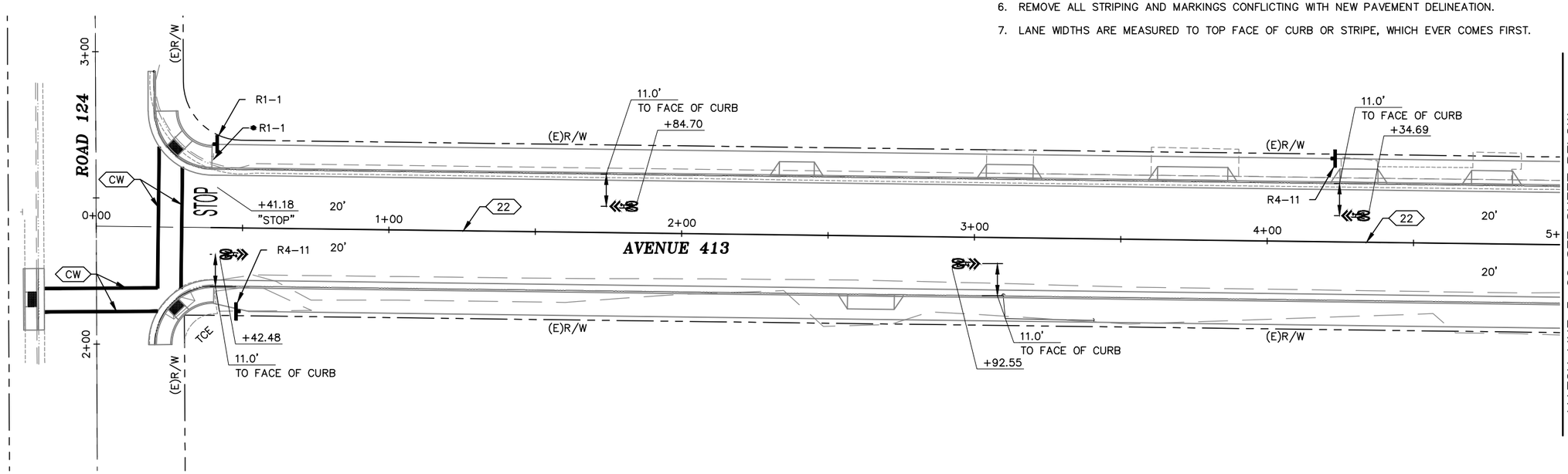
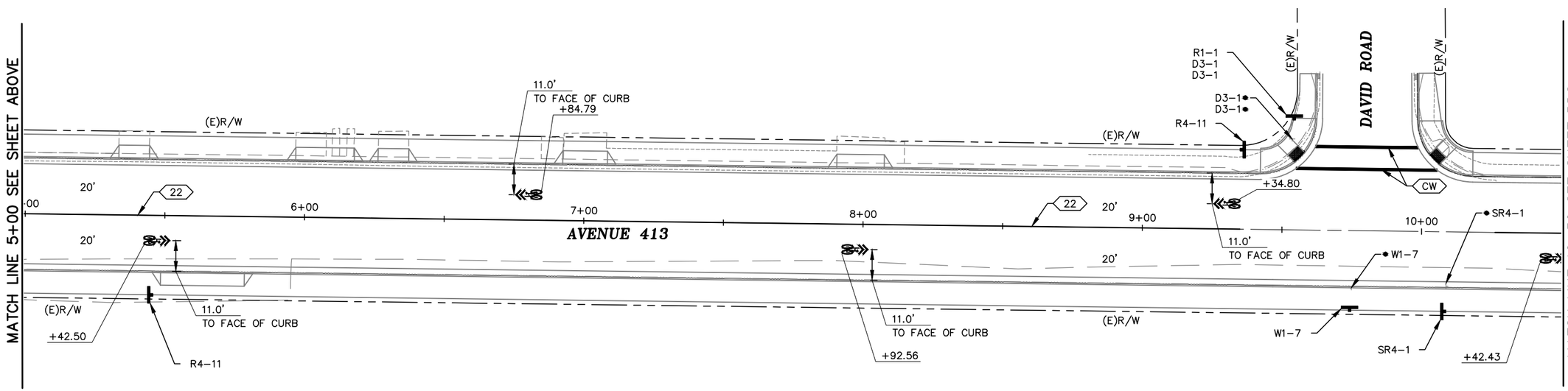
LEGEND (PD1–PD3)

- # — STRIPING DETAIL NUMBER PER STATE STD PLANS
- CW — THERMOPLASTIC CROSSWALK (CW) PER STATE STD PLANS; 10' WIDE FROM INSIDE TO INSIDE OF MARKINGS
- LL — THERMOPLASTIC LIMIT LINE (LL)
- SHARED ROADWAY BICYCLE MARKING PER CALTRANS RSP A24C
- STOP — STOP LEGEND MARKING PER STATE STD PLANS
- LIMIT OF STRIPING PATTERN
- ANGLE POINT
- TANGENT POINT
- EXISTING ONE POST ROADSIDE SIGN
- PROPOSED ONE POST ROADSIDE SIGN
- REMOVE SIGN
- (CA) CALIFORNIA SIGN CODE

NOTES: (SHEETS PD1–PD3)

1. WORK SHALL BE DONE IN ACCORDANCE WITH THE 2015 EDITION OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND SPECIFICATIONS, THE 2014 EDITION OF THE CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) AND SIGN SPECIFICATION SHEETS, THE LATEST EDITIONS OF THE TULARE COUNTY STANDARD PLANS AND SPECIFICATIONS, AND THE SPECIAL PROVISIONS.
2. THESE PLANS ARE ACCURATE FOR SIGNING AND STRIPING ONLY.
3. ALL SIGNS SHALL BE AVERY DENNISON T6500 SERIES REFLECTIVE SHEETING AND SHALL BE COVERED WITH AVERY DENNISON OL1000 ANTI–GRAFFITI OVERLAY FILM.
4. ALL STRIPING SHALL BE THERMOPLASTIC PAINT.
5. ALL CROSSWALKS SHALL BE 10 FEET WIDE, MEASURED FROM INSIDE TO INSIDE OF MARKING.
6. REMOVE ALL STRIPING AND MARKINGS CONFLICTING WITH NEW PAVEMENT DELINEATION.
7. LANE WIDTHS ARE MEASURED TO TOP FACE OF CURB OR STRIPE, WHICH EVER COMES FIRST.





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REDDING
SAN LUIS OBISPO

SIGNING AND STRIPING PLAN
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55–7300–14
DESIGNED	JLR
DRAWN	JLR
CHECKED	SMH
FILE	2144PD003.DWG
DATE	6–9–16
SHEET No.	PD1

7 OF 9

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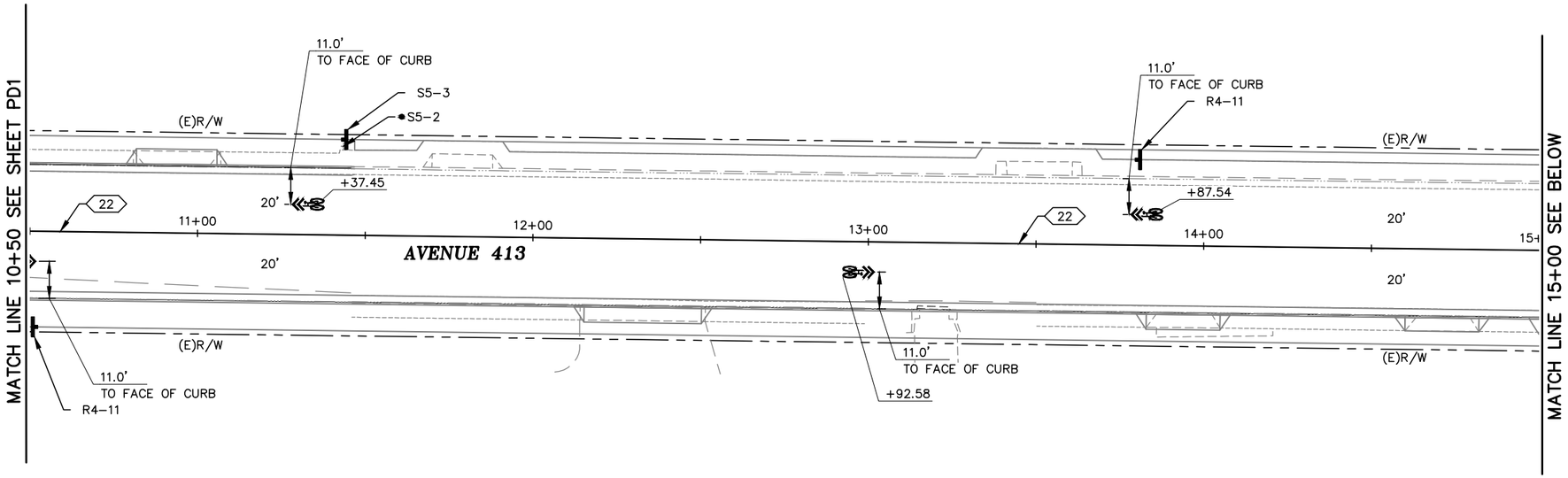

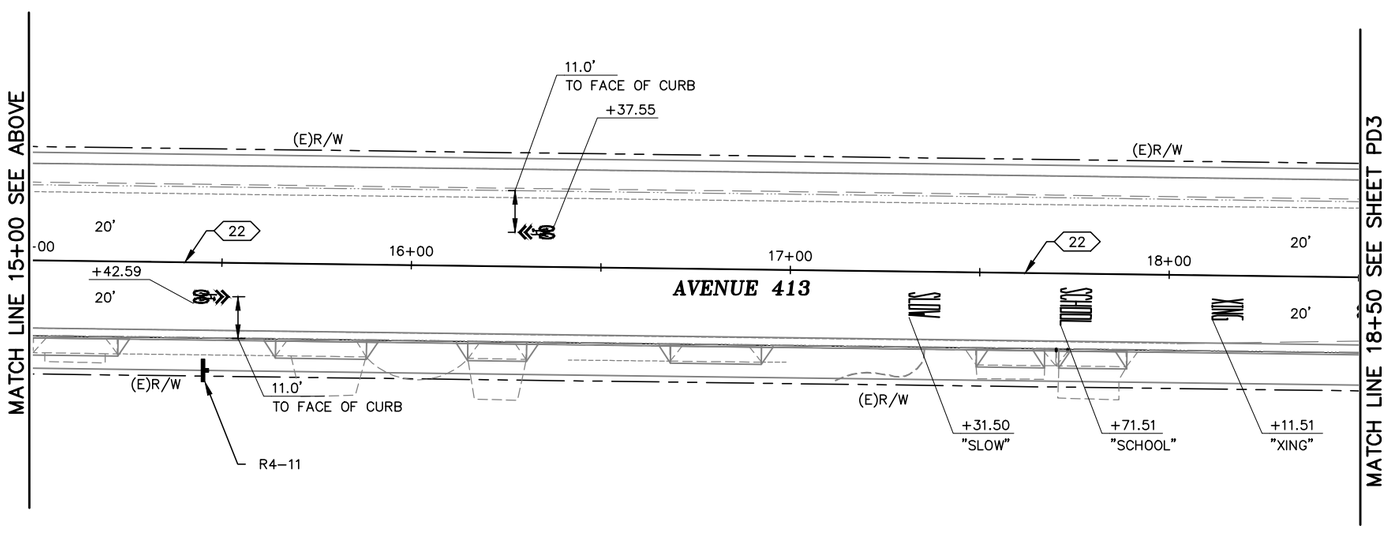

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SIGNING AND STRIPING PLAN
AVENUE 413 – OROSI
TULARE COUNTY
COMPLETE STREETS
TULARE COUNTY, CA

SCALE	1"=20'
DIVISION	RMA
JOB NO.	55-7300-14
DESIGNED	JLR
DRAWN	JLR
CHECKED	SMH
FILE	2144PD003.DWG
DATE	6-9-16
SHEET No.	PD2

8 OF 9

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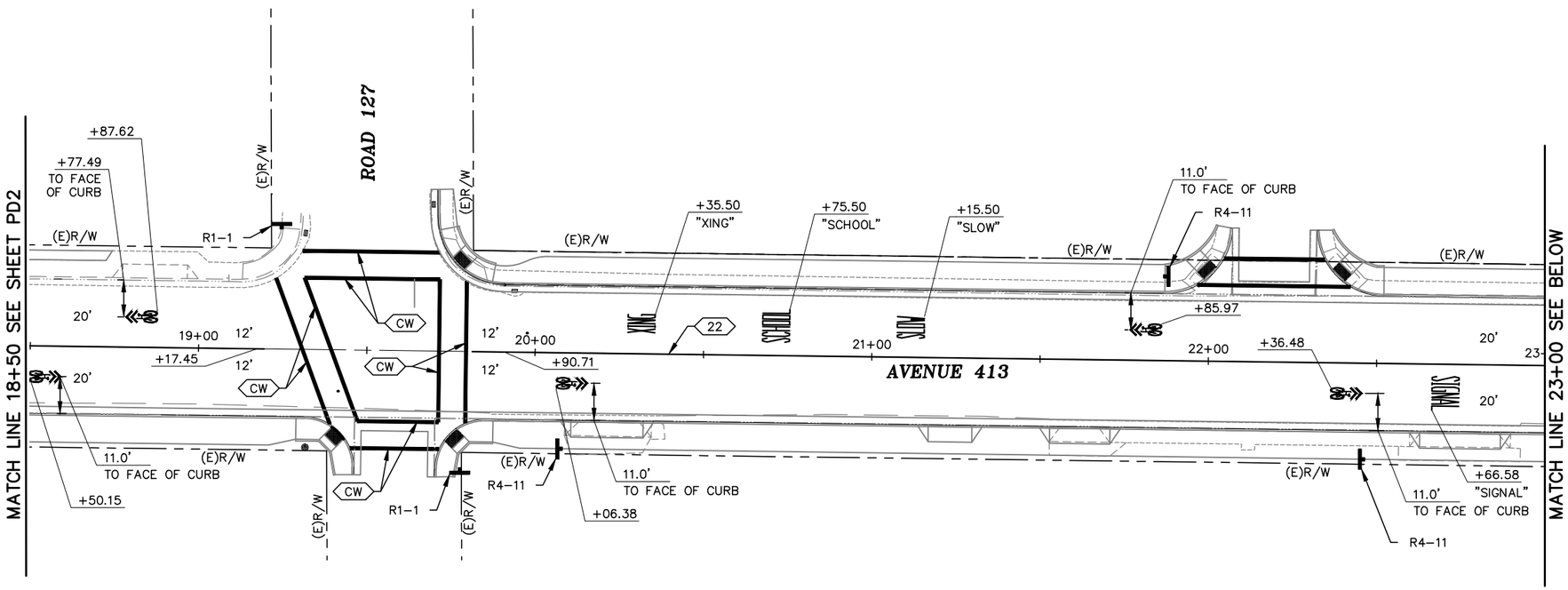

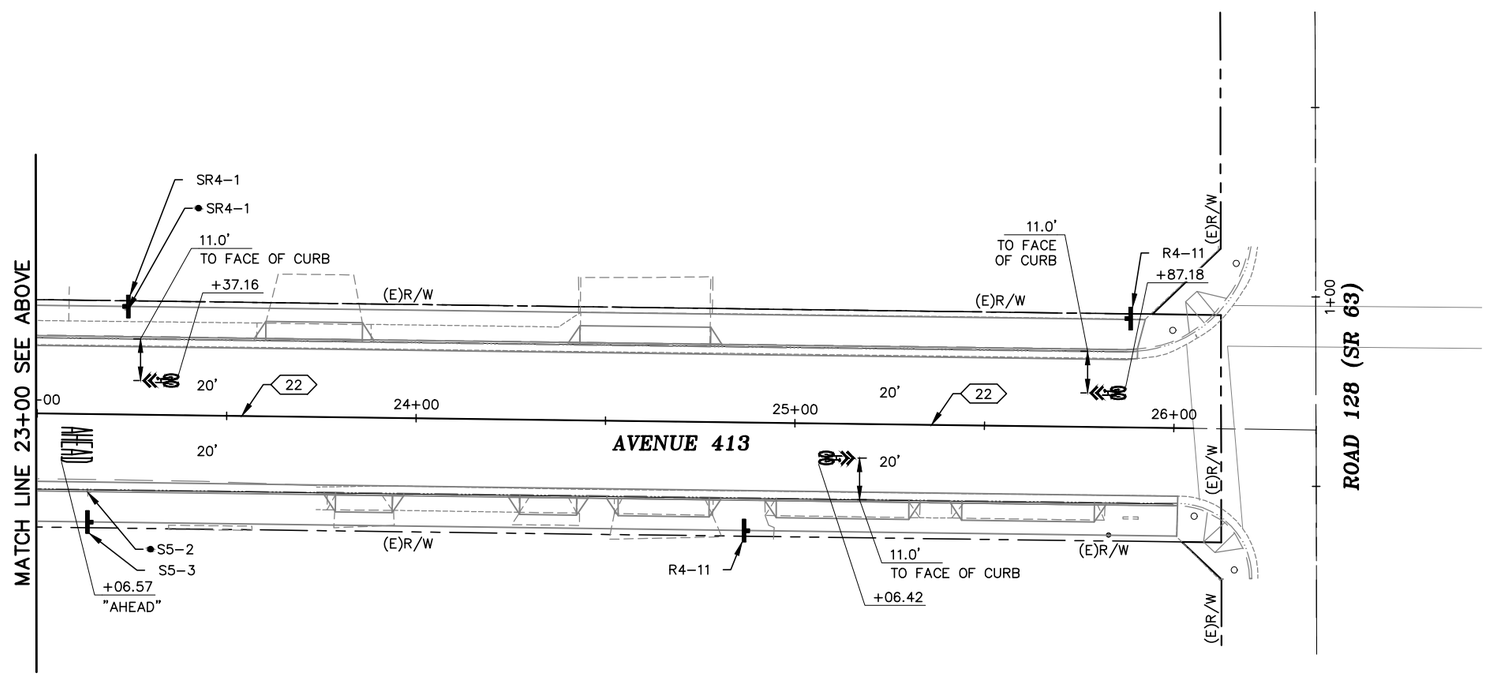

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AVENUE 413 – OROSI
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SCALE	1"=20'
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JOB NO.	55-7300-14
DESIGNED	JLR
DRAWN	JLR
CHECKED	SMH
FILE	2144PD003.DWG
DATE	6-9-16
SHEET No.	PD3

9 OF 9

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Appendix I –

Complete Streets Outreach

Cutler-Orosi Community Meeting hosted by the Tulare County Resource Management Agency
Complete Streets Meeting/ Community Plan Kick off Meeting
Tuesday, March 3, 2015
Golden Valley Elementary School- Cafeteria
41456 Rd. 127
Orosi, CA 93647
5:30p.m.

County staff Kyria Fierros, Dave Bryant and Aaron Bock from County Economic Development and Planning Branch from the Tulare County Resource Management Agency (RMA) attended the community meeting in Orosi, CA on March 3, 2015. This was the second meeting RMA held in Orosi.

On February 12, 2015, RMA gave an informational update about the Community Plan and Complete Streets program at the Cutler-Orosi Joint Unified School Board meeting.

At the meeting the Board suggested RMA hold a separate meeting with the community allowing all local residents the opportunity to come to a meeting. The Board also requested information on the past improvements completed from the previous Community Plan of 1988 and the amount of projects implemented since the passing of Measure R in 2006.

On March 3, 2015 RMA with the help from Roel Alvarado, Principal at Cutler Elementary, translated the meeting in English to Spanish.

RMA staff gave a PowerPoint explaining the Community Plan process and the Complete Streets program letting the public know we were seeking input on priority streets. Dave Bryant updated the community on infrastructure improvements from the previous community plan of 1988 and Kyria Fierros presented on the Measure R funds.

According to our Road Department there the total amount of funds used from Measure R funds since 2006:

Cutler $242K

Orosi $900K

Total: **$1.1 million**

The five Streets County identified for the complete streets by the community were:
Orosi
1. Avenue 416 – SR 63 to Rd 140 (East Orosi) **43 Votes**

2. Avenue 413 – Rd 124 to SR 63 **11 Votes**
3. Avenue 419 **3 Votes**
4. Avenue 416 – SR-63 to Dinuba **0 Votes**

Additional Considerations
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler
1. George Rd/2nd Drive - Ave 407 to SR 63 **36 Votes**
2. Avenue 408 – Rd 124 to SR 63 **14 Votes**
3. Railroad Drive – SR 63 to Rd 124 **9 Votes**
4. Avenue 404 – SR 63 to Robert Rd **6 Votes**
5. First Drive – SR 63 to Rd 124 **5 Votes**

General Road Concerns
-Orosi Drive at 2nd Drive-Flooding
-Road 124 at Aceves Ave-Stop Sign
-Road 124 (Ave 408 to Ave 416) Traffic Speeding Concerns
-El Monte School (Road 127) Request for additional Crosswalks(Students cross mid-block at multiple locations primarily north of the school).
-Road 127 at Ave 416 (Sight Distance Problem with parked vehicles, Northbound left turn from Road 127 to westbound Ave 416).
-SR 63 (Road 128) Request for Traffic Signal at Orosi High School(Walnut Ave?)
-Avenue 416 there is lots of traffic speeding. There has been accident there
-Avenue 63 there are no lights sidewalks
-Avenue 63 and Ella need for sidewalks.

Development:
Citizens expressed their concerns for a pharmacy. They stated the closest one is in Dinuba and even then there is very limited bus transportation. A pharmacy would be a great asset because local residents can walk to their local pharmacy than have to travel to another city.

Water Issue:
Superintendent Valdez voiced her concerned for the need of clean water for the children and families. Staff discussed the County- Wide bottle water program for the residents and let them know we qualify those who are on a dried well or show contamination in their water. Follow up information was made available.

Outreach:
The County provided 4,000 fliers to the school district where the fliers were distributed amongst the schools. A recorded voice message was made to the households of all the parents of the children Monday evening.

Conclusion:
We concluded the meeting by telling the superintendent we would host another meeting at their community parent meeting in the months to come. We are currently moving forward with updating Cutler-Orosi's community plan. Last time it was updated was 1988. We will hold 2-3 more meetings to receive community input.

County staff in attendance: Aaron Bock, Kyria Fierros, Dave Bryant

The meeting was held in English and Spanish.

The meeting started at 5:30p.m. And ended at 7:15 p.m.

Gold School

The Tulare County Resource Management Agency will be hosting a
Cutler-Orosi Community Plan Update and
Complete Streets Workshop on
***Tuesday, March 3rd, 2015, at 5:30p.m*. at the**
Golden Valley School-Cafeteria
41465 Road 127
Orosi, CA 93647

We will be discussing updating the existing Cutler-Orosi Community Plan which will include proposed Land Use, Zoning, and Circulation changes in the area. Our discussion will also include a new Complete Streets Program. We will be discussing proposed new changes in the area and are asking for community input on these important Community projects. This is the start of a series of meetings we will have with the community.
For more information call: Kyria at 559-624-7154

El condado de Tulare va dar una **Presentación Informativa de Planificación y Ingenieria** para la comunidad de Cutler-Orosi el dia
Marties, 3 de Marzo a las
5:30 p.m. en la
Cafeteria de Golden Valley School
41465 Road 127
Orosi, CA 93647

Por favor venga y únase a nosotros para un seguimiento de la presentación de Comunidades Saludables y discusión de usos de la tierra. Vamos a discutir propuestas de nuevos cambios en el área y pedir opiniones de la comunidad.Esta es parte de una serie de reuniones que el condado esta haciendo.
Si quiere mas informacion llame a: Kyria -559-624-7154

Attachment A- 7 Public Outreach

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Cutler-Orosi Joint Unified School District
District Office Board Room
12623 Avenue 416, Orosi, CA
(559) 528-4763

REGULAR BOARD MEETING AGENDA

March 14, 2019
Open Session: 5:00 p.m.
Closed Session: 6:00 p.m.

Roll call and establishment of quorum.

	Present	Absent
Rebecca Jimenez, Board President	_____	_____
Mary Helen Espino, Vice President	_____	_____
Sandra Williams, Board Clerk	_____	_____
Lloyd McClard, Trustee	_____	_____
Marisol Rubalcaba, Trustee	_____	_____
Sarah Herrera, Trustee	_____	_____
Delia Martinez, Trustee	_____	_____

Staff:

Yolanda Valdez	___	**Bryan Miller**	___	**Leanne Cerda**	___
Craig Drennan	___	**Antonio Quintanilla**	___	**Melissa Delgadillo**	___
Shevonne Swanson	___	**Lisa Castillo**	___	**Jayboy Camaquin**	___
Faith Price	___	**Ray Quintana**	___	**COUTA Representative**	___
Gabriela Guzman	___	**Roberto Vaca**	___	**CSEA Representative**	___
Leticia Trevino	___	**Marlena Celaya**	___	**Micaela Macareno**	___
Raffi Soghomonian	___	**Robert Gonzales**	___		

PLEASE BE COURTEOUS AND SILENCE YOUR CELL PHONES. – THANK YOU

Notice to the public: The meetings of the Board are scheduled for the purpose of conducting business of the school district and, therefore, are not public meetings, but rather meetings held in public. Questions regarding procedures and practices should be addressed to your school principal or to the District Superintendent. Members of the public will be given an opportunity to speak at every regular meeting of the Board—3 minutes per speaker and up to 20 minutes total on any given item. To speak members of the public must fill out a card requesting time. The cards are available at the Board meeting. To place an item on the agenda, arrangements must be made with the Superintendent ten (10) days prior to a regular Board meeting.

Meeting facilities are accessible to persons with disabilities. By request, alternative agenda document formats are available to persons with disabilities. To arrange for an alternative agenda document format or to arrange for aid or services to modify or to accommodate persons with a disability in order for them to participate in a public meeting, please provide a written request to the Superintendent at the District Office at least three (3) working days prior to any public meeting.

CALL TO ORDER- REGULAR OPEN SESSION at: _____
Time

Pledge of Allegiance Led by: _____

PUBLIC COMMENTS
Time is allocated for members of the public to comment on issues of concern related to District business. Action and/or discussion cannot be taken by the Board on items not already on the agenda. Persons wishing to address the Board must fill out a card which will indicate their names, addresses, group or organization they represent, and subject upon which they intend to speak. Individuals are requested to limit their comments to 3 minutes per speaker and up to 20 minutes total on any given item.

BOARD GOALS:

1. **Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
2. **Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
3. **Create efficient and effective systems that are innovative, accountable and proactive.**

PRESENTATIONS AND REPORTS (non-action items)

- **Superintendent's Report**
  - **Golden Valley Elementary School Presentation**
  - **Tulare County Resource Management Agency**
- **Student Board Representative Report**

RECESS

Motion for recess made by: _____
Motion Seconded by: _____
Recess called _____ p.m. Recess closed at _____p.m.

_____/_____/_____
Yes No Abstain

CONSENT
Action items prepared for the **CONSENT** section of the agenda are consistent with adopted policies and approved practices of the District and deemed routine in nature. They will be acted upon in one motion without discussion unless members of the Board request an item's removal from the **CONSENT** section. When this happens, the item will be removed and placed on the main agenda for action individually.

CONSENT ITEMS

Item #	Page(s)	Description	Board Goal
1	10-19	Minutes of the Regular Board Meeting Dated February 14th 2019	3
2	19-25	Minutes of the Special Board Meeting Dated February 21st 2019	3
3	26-32	Accounts Payable Total Payments – February 1, 2019 through February 28, 2019	3
4	33-34	February Cash Position Report	3
5	35-36	Interdistrict Transfer Requests	1, 3
6	37-39	Ratify Contract with Merced County Office of Education for Early Education Professional Development Services	3
7	40-48	Approve Memorandum of Understanding with Valley Health Team for Work Based Learning Student Internships	1, 2, 3
8	49-53	OHS Speech and Debate Overnight Trip to Long Beach, CA	2
9	54-56	Work Based Learning Development Skills USA State Leadership Conference for Law and Justice Pathway Overnight Trip to Ontario, CA	1, 2, 3
10	57-61	Update Board Policy 6173 – Education for Homeless Children	3
11	62-71	Update Administrative Regulation 6173 – Education for Homeless Children	3
12	72-76	Update Board Policy 6173.1 – Education for Foster Youth	3
13	77-85	Update Administrative Regulation 6173.1: Education for Foster Youth	3
14	86-102	Joint Powers Agreement for Regional Occupations Program	3
15	103-104	Memorandum of Understanding Between Valley Regional Occupations Program and Cutler-Orosi Joint Unified School District for Advanced Manufacturing & Welding Pathway program	1, 2, 3

<u>CONSENT ITEMS</u> – Continued

16	**105-106**	**Memorandum of Understanding Between Valley Regional Occupations Program and Cutler-Orosi Joint Unified School District for Nursing/ Healthcare Pathway Program**	**1, 2, 3**

Motion to approve Item #1- 16 made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

CURRICULUM/ INSTRUCTION ITEMS

Item #	Page(s)	Description	Board Goal
17	**107-111**	**March Curriculum/ Instruction Report** Motion to approve Item #17 made by: _____ Motion Seconded by: _____ _____/_____/_____ Yes No Abstain	**3**

ADMINISTRATIVE/ORGANIZATIONAL ITEMS

Item #	Page(s)	Description	Board Goal
18	**112**	**Approval of New Positions** Motion to approve Item #18 made by: _____ Motion Seconded by: _____ _____/_____/_____ Yes No Abstain	**3**
19	**113**	**2018-2019 Second Interim Budget Report** Motion to approve Item #19 made by: _____ Motion Seconded by: _____ _____/_____/_____ Yes No Abstain	**3**

Public Comments- Closed Session Items
Time is allowed for members of the public to comment on issues to be discussed in closed session.

CLOSING ACTIVITIES: The Governing Board members have an opportunity to comment.

Motion to adjourn to Closed Session at _____ p.m. made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

CLOSED SESSION **Board Goal**

Item A **Personnel (Gov. Code 54957): The Board will meet to consider Public Employee Appointment/Employment for the position(s) of:** 3

EMPLOYMENT/ADDITIONAL POSITIONS/HOURS:

1. Prevention & Intervention Specialist, OHS, effective February 19, 2019; paid at Range 38, Step 3.
2. K-5 Teacher, site to be determined, effective 2019-2020 school year; paid at Classification I, Step 1.
3. 9-12 English Teacher, Orosi High School, effective 2019-2020 school year; paid at Classification I, Step 1.
4. 9-12 Math Teacher, Orosi High School, effective 2019-2020 school year; paid at Classification III, Step 3 + MA pay.

LEAVES: (Statutory – For Informational Purposes)

1. Teacher, Palm School, requesting maternity leave from April 13, 2019, to approximately May 24, 2019. Her estimated date of delivery is April 13, 2019.
2. Library Aide, Golden Valley School, requesting maternity leave from March 15, 2019, to approximately May 24, 2019. Her estimated date of delivery is April 11, 2019.

RESIGNATIONS/RETIREMENTS:

1. 9-12 Social Sciences/AVID Teacher, Orosi High School, retiring/resigning effective June 30, 2019.
2. Kindergarten Teacher, Golden Valley School, resigning effective March 6, 2019.
3. 4-5 STEM Teacher, Golden Valley School, resigning effective June 7, 2019.
4. K-5 Special Education Instructional Aide, Golden Valley School, resigning effective March 8, 2019.\
5. R & E Aide, Palm School, resigned effective February 5, 2019.
6. K-5 P.E. Tech, Cutler School, resigning effective March 15, 2019.

Motion to approve Item A - Public Employee Appointment/Employment for the position(s) listed made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

<u>Item B</u> **Public Employee Discipline/Dismissal/Release, G.C. 54957** 3

Motion to approve Item B - Public Employee Discipline Dismissal Release made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

<u>Item C</u> **Conference with Labor Negotiators – Gov. Code Section 54957.6, 3549.1 – (Non Action discussion item only)** **3**

District Designated Representatives: Yolanda Valdez
Craig Drennan
Dr. Ken Caves

Employee Organization: COUTA
CSEA Chapter #253

Unrepresented Employees: Confidential/Management
(all positions in this group)

<u>Item D</u> **Student Expulsion, Suspension, or Disciplinary Action or Other Action Involving Pupil Records (Ed Code 35146; 48912(b); 48918(c) – The following cases are presented for discussion during closed session. Action will be taken during open session.**

- **2018-2019-15 Suspend Expulsion**
- **2018-2019-16 Suspend Expulsion**
- **2018-2019-17 Suspend Expulsion**

Adjournment to Open Session:

Motion to adjourn Closed Session at _____ p.m. and reconvene into Open Session made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

REPORTING OF CLOSED SESSION ITEMS

Item A **Personnel (Gov. Code 54957): The Board will meet to consider Public Employee Appointment/Employment for the position(s) of:** 3

EMPLOYMENT/ADDITIONAL POSITIONS/HOURS:

1. Prevention & Intervention Specialist, OHS, effective February 19, 2019; paid at Range 38, Step 3.
2. K-5 Teacher, site to be determined, effective 2019-2020 school year; paid at Classification I, Step 1.
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LEAVES: (Statutory – For Informational Purposes)

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6. K-5 P.E. Tech, Cutler School, resigning effective March 15, 2019.

Item B **Public Employee Discipline/Dismissal/Release, G.C. 54957** **3**

Item C **Conference with Labor Negotiators – Gov. Code Section 54957.6, 3549.1** **3**

Item D **Student Expulsion, Suspension, or Disciplinary Action or Other Action Involving Pupil Records (Ed Code 35146; 48912(b); 48918(c) – The following cases are presented for discussion during closed session. Action will be taken during open session.** **3**

- **2018-2019-15 Suspend Expulsion**
- **2018-2019-16 Suspend Expulsion**
- **2018-2019-17 Suspend Expulsion**

Motion to approve Item D Student Expulsion, Suspension, or Disciplinary Action or Other Action Involving Pupil Records (Ed Code 35146; 48912(b); 48918(c) made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

ADMINISTRATIVE/ ORGANIZATIONAL ITEMS – continued

Item #	Page(s)	Description	Board Goal
20	**117-118**	**Resolution No. 18-19- 09 In the Matter of Non Reelection of Probationary Certificated Employees**	**3**

Motion to approve Item #20 made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

CLOSING ACTIVITIES: The Governing Board members have an opportunity to comment.

The next Regular Meeting of the Board of Trustees will be held on Thursday, April 11, 2019 with Open Session beginning at 5:00 p.m. and Closed Session at 6:00 p.m. The Board Meeting will take place in the Cutler-Orosi Joint Unified School District Board Room located at 12623 Avenue 416, Orosi, California.

ADJOURNMENT: Motion to adjourn the meeting made by: _____
Motion Seconded by: _____

_____/_____/_____
Yes No Abstain

Meeting adjourned at: _____ p.m.

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **MINUTES OF THE REGULAR BOARD MEETING DATED FEBRUARY 14, 2019**

ATTACHMENTS: **MINUTES OF THE REGULAR MEETING OF THE BOARD DATED FEBRUARY 14, 2019**

DISCUSSION:
Attached for approval are the Minutes of the Regular Board Meeting dated February 14, 2019.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Yolanda Valdez, Superintendent***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the February 14, 2019 Regular Board Meeting Minutes.**

PROPOSED ACTION: **APPROVE**

Item #: 1

Cutler-Orosi Joint Unified School District

District Board Room – 12623 Avenue 416
Orosi, California

MINUTES OF THE BOARD OF TRUSTEES REGULAR MEETING

Regular	5:00 P.M.	Thursday	January 14, 2019
Type of Meeting	Time	Day	Date

Roster of Attendance

Trustee(s) Present
Rebecca Jimenez
Mary Helen Espino
Sandra Williams
Marisol Rubalcaba – Arrived at 5:25 p.m.
Delia Martinez
Sarah Herrera – Arrived at 5:10 p.m.
Lloyd McClard

District Office Staff:
Yolanda Valdez
Craig Drennan
Faith Price
Antonio Quintanilla
Gabriella Guzman
Shevonne Swanson
Lisa Castillo
Raffi Soghomonian
Micaela Macareno
Bryan Miller
Ray Quintana

Administrative Staff:
Jayboy Camaquin
Melissa Delgadillo
Robert Gonzales
Leanne Cerda
Roberto Vaca

CALL TO ORDER	Board President Mary Helen Espino called the Regular Board Meeting to order at 5:00 p.m.
Pledge of Allegiance	The Pledge of Allegiance was led by Trustee Sandra Williams.
Others Present	Sandra Cerda, Anabela Lopez, Yovana Castillo, Patricia Puentes, Anudre Garcia, Joshua Almaguer, Karina Mil ligan, Lisa Duval, Nora Celis, Liliana, Cisco Paredez, David Olias, Martha Juarez, Eduardo Martinez, Salvador Chavez, Gloria Huerta, Jesse Huerta Sr., Jesse Huerta
Public Comments	No Public Comments were presented.
Presentations and Report • **Superintendent's Report** • **Palm Elementary School Presentation**	Superintendent Valdez introduced Palm Elementary School Principal Jayboy Camaquin who presented the decorated bulletin board and thanked his staff for their hard work on putting it together. He then presented the following recognition awards: K - Aubrielle Garza 1 - Daniel Rocha 2 - Yuliana Perez Reyna 3 - Martin Hernandez 4 - Jayden Malagon 5 - Michelle Calixto Staff Certificated - Anavela Lopez Staff Certificated - Anna Marroquin Parent Volunteer - Patricia Puentes
• **Facilities Update Presentation**	Facilities Director Raffi Soghomonian, presented an update on the plans for the upcoming sports complex project. He stated before the Board Meeting the Ground Breaking Ceremony had taken place and the project would now be underway. He informed the program would include new softball field, soccer field, concession stand and restrooms along with a splash park for the community. He stated they were getting everything ready to begin the 2nd phase of the project.
• **Student Board Representative Report**	Student Board Representative Isamar Rodriguez presented a site presentation on past, current and upcoming events for Orosi High School. Superintendent Valdez then presented and award letter recognizing Lovell Continuation High School as a Model Continuation School for the State of

California. She congratulated Lovell School for this award and informed the Board a formal celebration would be held in April.

Board President Rebecca Jimenez thanked all of the COUTA and CSEA representation at the meeting. She stated she appreciated them taking the time to attend and be present.

No further comments were presented.

Consent Items:

1 Minutes of the Regular Board Meeting Dated January 10, 2019

2 Minutes of the Special Board Meeting Dated January 24, 2019

3 Accounts Payable Total Payments Dated December 21, 2018 through January 31, 2019

4 January Cash Position Report

5 Budget Revision #7

6 Update Board Policy 4140, 4240, 4340 - Bargaining Units

7 Interdistrict Transfer Requests

8 Central Valley Pipeline Doctor Academy Memorandum of Understanding

9 UC Merced Center for Educational Partnership (CEP) Data Sharing Agreement

10 Approval of Comprehensive Safe School Plans for 2019-2020

11 Orosi High School Student Overnight Trip to the 2019 FFA Sate Leadership Convention April 25-2018, 2019

12 Orosi High School Overnight Trip to the 2019 HOSA State Leadership Convention

13 Orosi High School Speech and Debate Overnight Trip to Bakersfield, CA

14 Ratify Approval of Attendance to the Out of State National Council for Community and Education Partnerships Gear Up – Capacity

Board President Rebecca Jimenez presented the Consent Items and entertained questions and/or comments.

Board Vice President Mary Helene Espino asked about the Disney Trip listed on Page 70 of Item #11. She asked if students needed to pay for their own ticket to the park. College and Career Readiness Director Lisa Castillo stated the admission ticket to Disneyland was included with their registration for the Leadership conference. She stated students were only asked to bring spending money should they want to purchase souvenirs and personal purchases.

Mrs. Espino also asked about the out of State trip listed in Item #14. She asked when they would be traveling to Texas. Superintendent Valdez informed the conference had already taken place. She stated the invitation to attend the conference was provided after the last board meeting and prior to the current meeting therefore there was no time to present the item to the Board for prior approval. She stated the Board received notice via the Friday letter and informed they would be receiving it for ratification. Superintendent Valdez informed that part of the UC Merced Gear Up Grant included the attendance to the conference. She stated the grant covered the expenses for it.

No further questions were added.

	Building Workshop in San Antonio, Texas	
15	Accept Quarterly Williams Uniform Complaint Report	
16	2018-2019 Consolidated Application	
17	Ratify Agreement Between Cutler-Orosi Joint Unified Schools District and the State of California Department of Healthcare Services for LEA Media-Cal Billing Program Services	A motion was made by Trustee Lloyd McClard, seconded by Marisol Rubalcaba to approve Items #1-18. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yeas Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
18	Ratify Agreement for Disclosure and Us of Med-Cal Date Between the Cutler-Orosi Joint Unified School District and the State of California Department of Healthcare Services	
Curriculum/ Instruction Items:		
19	February Curriculum/ Instruction Report	Curriculum and Instruction Director Shevonne Swanson presented the Curriculum/ Instruction Report for the month of February. A motion was made by Trustee Mary Helen Espino, seconded by Sandra Williams to approve Items #19. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
20	Low Performing Students Block Grant	Curriculum and Instruction Director Shevonne Swanson presented Item #20 and informed the Board the grant would allow the district to receive funding for intervention services for students needing extra help when they do not meet the income requirements for services. A motion was made by Trustee Mary Helen Espino, seconded by Sandra Williams to approve Items #20. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes

22	Orosi High School Advanced Digital Photography Course and Textbook Approval	Superintendent Valdez informed the Board the textbook material for Item 21 would arrive to the meeting shortly and asked that Item 22 be presented to for approval to allow time for the materials to arrive. Items 22 was presented for review. Board President Jimenez entertained questions and/or comments on the item. None were presented. A motion was made by Trustee Marisol Rubalcaba seconded by Trustee Sarah Herrera to approve Item #22. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
21	Orosi High School 2019-2020 Course of Study Guidebook	Curriculum/ Instruction Director Shevonne Swanson presented Item #21 and informed the new course would form part of the Arts, Media and Entertainment Pathway. She stated the department had already reviewed the course and materials and approved it. A motion was made by Trustee Sandra Williams seconded by Trustee Lloyd McClard to approve Item #21. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
Administrative/ Organizational Items:		
23	College of the Sequoias Dual Enrollment Course Offerings at Orosi High School	Superintendent Valdez presented Item #23 and informed the Board the course offerings would offer students college credits for classes taken at Orosi High School. Board Vice President Espino asked if students would have to pay for the course if attended at COS. Superintendent Valdez informed the Board the courses on the OHS campus for students would be free along with the textbook materials. She stated this would be great savings for students as they would already have college credits entering college. She informed that part of the Dashboard Indicators was to offer College and Career Readiness Courses. She informed these courses would help them prepare for college level courses. A motion was made by Trustee Lloyd McClard, seconded by Delia Martinez to approve Item #23. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes

		Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
24	Vote for 2019 CSBA Delegate Assembly Election	Superintendent Valdez presented Item #24 and informed this was an annual item. Board President Mary Helen Stated she know one of the incumbents and stated she was ok with electing the incumbents to continue in their positions. A motion was made by Trustee Mary Helen Espino, seconded by Lloyd McClard to approve Item #24. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
25	Award Contract for Sports Complex Bid #SP-04 for Fencing	Superintendent Valdez presented Item #25 and stated as previously mention by Mr. Soghomonian the Sports Complex had its groundbreaking ceremony and the project will begin construction. She stated the Item #25 was one of the many bids that are to come as the project takes place. Mr. Soghomonian informed the bids had been previously submitted and rejected because the bids were too high. Board Vice President Espino asked why the job could not just be done in house. Mr. Soghomonian stated that because of cost accounting guidelines, the law prevents the district from doing the work therefore the projects had to be outsourced. He informed Alta Fencing was the company that came in with the lowest bid. A motion was made by Trustee Sandra Williams, seconded by Marisol Rubalcaba to approve Items #25 and award the bid contract to Alta Fencing. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
26	Award Contract for Orosi High School Sports Complex Concession Stand Building Concrete Slab and Underground Plumbing	Superintendent Valdez presented Item #26. Mr. Soghomonian informed the cement slab was for the portable building for the sports complex concession stand. Superintendent Valdez informed the building was already approved by the board and already under construction. Mr. Soghomonian informed the lowest bid for this project came in from 99 Pipeline. A motion was made by Trustee Lloyd McClard, seconded by Mary Helen Espino to approve Items #26 and award the bid contract to 99 Pipeline. The motion was approved with a 7 to 0 vote. With the votes as follows:

		Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
27	Construction Cost Accounting Prequalification Lis for Projects Under $175,000	Mr. Soghomonian presented Item #27 and informed the Board a Prequalification List had to be created for projects under $175,000. Assistant Superintendent Craig Drennan informed that an informal bidding process was held to allow contractors to add their company to the list. A motion was made by Trustee Sandra Williams, seconded by Lloyd McClard to approve Items #27. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
28	Construction Cost Accounting Prequalification List for Projects Exceeding $1,000,000	Mr. Soghomonian presented Item #28 and informed the Board that similar to the Prequalification List previously approved a Prequalification List had to be created for projects Exceeding $1,000,000. A motion was made by Trustee Marisol Rubalcaba, seconded by Sarah Herrera to approve Items #27. The motion was approved with a 7 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes No comments were presented on closed session items.
Public Comments for Closed Session Items:		No public comments were presented on the Closed Session Items.
Closing Activities:		The Board individually thanked everyone for attending and congratulated Lovell School for their recent State recognition, thanked Mr. Camaquin for a great school presentation and congratulated each site for all the great things going on at the schools.
Adjourn to Closed Session:		A motion was made by Trustee Lloyd McClard, seconded by Trustee Sandra Williams to adjourn to Closed Session at 6:40 p.m. Motion was approved with a 7 to 0 vote. Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes

Trustee Lloyd McClard - Yes
Trustee Marisol Rubalcaba - Yes
Trustee Sarah Herrera – Yes
Trustee Delia Martinez – Yes

Reconvene to Open Session:

A motion was made by Trustee Marisol Rubalcaba, seconded by Trustee Lloyd McClard to Reconvene to Open Session at 7:40 p.m. Motion was approved with a 7 to 0 vote.

Board President Rebecca Jimenez – Yes
Board Vice President Mary Helen Espino - Yes
Board Clerk Sandra Williams – Yes
Trustee Lloyd McClard - Yes
Trustee Marisol Rubalcaba - Yes
Trustee Sarah Herrera – Yes
Trustee Delia Martinez – Yes

Reporting of Closed Session Items:

Item A – Personnel – Public Employee Appointment/ Employment for the Position(s) of:

During Closed Session the Board reviewed and discussed the following personnel matters.

EMPLOYMENT/ADDITIONAL POSITIONS/HOURS

1. Reinalda Arreguin-Dominguez, 3 Hour/183 Workday R & E Aide, El Monte Middle School, effective January 10, 2019; paid at Range 9, Step 1.
2. Angel Tamez, Part Time Avid Tutor, El Monte Middle School, effective January 28, 2019; paid at Range 17, Step 1.
3. Jose Jimenez-Sanchez, Part Time Avid Tutor, El Monte Middle School, pending clearances; paid at Range 17, Step 1.
4. Jacqueline Benavides, New Teacher for the 2019-2020 School Year, site and grade to be determined; paid at Classification I, Step 1.
5. Rosalia Lopez, New Teacher for the 2019-2020 School Year, site and grade to be determined; paid at Classification I, Step 1.
6. Erica Nunez, New Teacher for the 2019-2020 School Year, site and grade to be determined; paid at Classification I, Step 1.
7. Mayela Macias, New Teacher for the 2019-2020 School Year, site and grade to be determined; paid at Classification I, Step 1.
8. Andrew Baza, English Teacher for the 2019-2020 School Year, site and grade to be determined, paid at Classification I, Step 1.
9. Leandra Rojas, New Teacher for the 2019-2020 School Year, site and grade to be determined; paid at Classification I, Step 1.
10. Shon Larm, Science Teacher for the 2019-2020 School Year, site and grade to be determined; paid at Classification II, Step 15.
11. Samuel Hernandez, Music Teacher for the 2019-2020 School Year, site and grade to be determined; paid at Classification I, Step 1.
12. Areli Celia Quezada, 3 Hour/10 Month R & E Aide, Cutler School, effective date pending clearances and safety training; paid at Range 9, Step 1.
13. Elijah Guzman, 3 Hour/10 Month R & E Aide, Palm School, effective date pending clearances and safety training; paid at Range 9, Step 1.
14. Sherly Hernandez Alvarez, 8 Hour/10 Month LVN, District-wide, effective date pending clearances and safety training; paid at Range 39, Step 1.
15. **Orosi High School Spring Coaching Assignments:**

Softball	Varsity Head	Venisha Gonzalez
	Varsity Assistant	Manuel Villegas

	JV Head	Ramiro Gonzalez Jr.
Baseball	Varsity Head	Ramiro Gonzalez
	Varsity Assistant	Chris Rojas
	JV Head	Miguel Moreno
	JV Assistant	Jose Marroquin/Tyler Villarreal
Track & Field	Varsity Head	Ben White
	Varsity Assistant	Maria Ramirez/Bulfrido Martinez
Boys Tennis	Varsity Head	Nick Cabatutan
	Varsity Assistant	Anthony DeLaCerda
Boys Golf	Varsity Head	Josh Gonzalez

LEAVES: (Statutory – For Informational Purposes)

1. Ray Quintana, IT Director, District, took paternity leave from January 2, through January 18, 2019.
2. Daisy Hernandez, Special Ed Aide, Palm School, is requesting twelve weeks of baby bonding leave from January 14, 2019, through approximately April 5, 2019.
3. Lydia Natasha Gonzalez, Assistant Cook, Cutler School, is requesting twelve weeks of baby bonding leave beginning January 14, 2019 through April 8, 2019.
4. Maria Marin, Student/ Parent Advocate, El Monte Middle School, is requesting maternity leave from January 22, 2019, through approximately March 28, 2019. Her estimated date of delivery is February 7, 2019.
5. Mariah Mora, Campus Security/ Bus Driver, Orosi High School, is requesting maternity leave from January 21, 2019, through approximately April 15, 2019. Her estimated date of delivery is February 25, 2019.
6. David Lupian, Site Computer Technician, District, took paternity leave from January 7, 2019 through January 11, 2019.
7. Lupita Padilla Valdez, Outreach Aide, Family Ed. Center, is requesting maternity leave from January 31, 2019 through March 29, 2019. Her scheduled due date is on February 1, 2019.
8. David Jansson, Data Specialist, District Office, is requesting paternity leave from February 4, through February 18.

RESIGNATIONS/RETIREMENTS:

1. Karen Alvarez Saavedra, R & E Aide, Cutler School, resigned effective January 11, 2019.
2. Samantha Lopez-Moreno, LVN, District Wide, resigned effective last work day January 18, 2019.
3. Mary Rodriguez, Head Varsity Volleyball Coach, resigned effective January 15, 2019.
4. Cenovio Diaz Jr., 7-8 Math Teacher, El Monte Middle School, resigning effective his last workday, June 7, 2019.
5. Roland Rico, Campus Security, Lovell Alternative High School, resigning effective his last workday, February 14, 2019.
6. Leandra Rojas, AVID Tutor, OHS, resigning effective her last workday, February 8, 2019.

A Motion was made by Trustee Lloyd McClard, seconded by Trustee Sarah Herrera to approve the Personnel Letter as Presented. Motion was approved with a 6 to 0 to 1 vote.

Board President Rebecca Jimenez – Abstain
Board Vice President Mary Helen Espino - Yes

	Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
Item B - Public Employee/ Discipline Dismissal Release:	No action was taken on Item B.
Item C - Conference with Labor Negotiators:	No action was taken on Item C.
• **Item D – Student Expulsion, Suspension, or Disciplinary Action or Other Action Involving Pupil Records (Ed Code 35146; 48912(b); 48918(c) – The following Cases are presented for discussion during Closed Session. Action was taken during Open Session.** • **2018-2019-13 – Suspend Expulsion** • **2018-2019-14 – Suspend Expulsion**	Upon reconvening to Open Session a motion was made by Trustee Mary Helen Espino, seconded by Trustee Lloyd McClard to approve Item D for the cases presented. Motion was approved with a 7 to 0 vote. Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
Administrative/ Organizational Items: 29 Resolution No. 18-19-08: In the Matter of Non Reelection of Probationary Certificated Employees	A motion was made by Trustee Mary Helen Espino, seconded by Trustee Lloyd McClard to approve Item 29. Motion was approved with a 7 to 0 vote. Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
Comments/Adjournment	No additional comments were presented. A motion was made by Trustee Sandra Williams, seconded by Trustee Lloyd McClard to adjourn the meeting at 7:43 p.m. Motion was approved with a 7 to 0 vote. Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Yes Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Yes Trustee Delia Martinez – Yes
	Yolanda Valdez Superintendent (mm)

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **MINUTES OF THE SPECIAL BOARD MEETING DATED FEBRUARY 21, 2019**

ATTACHMENTS: **MINUTES OF THE SPECIAL MEETING OF THE BOARD DATED FEBRUARY 21, 2019**

DISCUSSION:
Attached for approval are the Minutes of the Special Board Meeting dated February 21, 2019.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Yolanda Valdez, Superintendent***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the February 21, 2019 Special Board Meeting Minutes.**

PROPOSED ACTION: **APPROVE**

Item #: 2

Cutler-Orosi Joint Unified School District
District Board Room – 12623 Avenue 416
Orosi, California

MINUTES OF THE BOARD OF TRUSTEES SPECIAL MEETING

Regular	5:00 P.M.	Thursday	February 21, 2019
Type of Meeting	Time	Day	Date

Roster of Attendance

Trustee(s) Present
Rebecca Jimenez – Arrived at 5:20 p.m.
Mary Helen Espino
Sandra Williams
Marisol Rubalcaba
Delia Martinez

Trustee(s) Absent
Sarah Herrera
Lloyd McClard

District Office Staff:
Yolanda Valdez
Craig Drennan
Faith Price

CALL TO ORDER	Board Vice President Mary Helen Espino called the Regular Board Meeting to order at 5:15 p.m.
Pledge of Allegiance	The Pledge of Allegiance was led by Mary Helen Espino.
Public Comments	No Public Comments were presented.
Administrative/ Organizational Items:	Superintendent Valdez presented Item # 1 and informed that most of the items to be presented were to start Phase-2 of the sport complex.
1 Contractor Prequalification List	She presented Item #1 and informed the list was updated after the previous submission at the Regular Board Meeting. A motion was made by Trustee Delia Martinez, seconded by Sandra Williams to approve Items #1 – Contractor Prequalification List. The motion was approved with a 4 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Absent Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
2 Additions to the Cost Accounting Prequalification List	She presented Item #2 and informed two new contractors were being added to the previous submission at the Regular Board Meeting. She informed the following companies were being added to the list: • King Khan Drilling and Construction • Community Play Ground A motion was made by Trustee Sandra Williams, seconded by Mary Helen Espino to approve Items #2: Additions to the Cost Accounting Prequalification List. The motion was approved with a 4 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Absent Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes

		Superintendent Valdez presented Item #3 and informed the lighting project was for the following sports complex fields: Varsity Baseball, Varsity Softball, youth football and one soccer field.
3	Purchase Approval of Lighting Equipment Using Sourcewell Cooperative Purchasing Agreement	A motion was made by Trustee Mary Helen Espino, seconded by Sandra Williams to approve Items #3: Purchase Approval of Lighting Equipment Using Sourcewell Cooperative Purchasing Agreement. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes Superintendent Valdez informed the district did not need to go out for bid for the shade coved as the district is a member of National Purchasing Partners and can therefore purchase without a bid.
4	Purchase Approval for Shade Cover Using National Purchasing Partners (NPP) for Sports Complex	A motion was made by Trustee Mary Helen Espino, seconded by Marisol Rubalcaba to approve Items #4 for the Purchase of Approval for Shade Coverings Using National Purchasing Partners for the Sport Complex. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes Superintendent Valdez informed the Board the bid for the landscape and irrigation work for Phase -2 of the sports complex had taken place earlier in the day. She presented the Bid Results. She informed Rainscape came in with the lowest bid in the amount of $530,171.
5	Award Contract for Sport Complex Phase-2 Landscape Irrigation	A motion was made by Trustee Sandra Williams, seconded by Mary Helen Espino to approve the Awarding of the Contract for Sport Complex Phase - 2 Landscape and Irrigation Work to Rainscape. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes Superintendent Valdez informed the bidding process for Item 6 had taken place earlier in the day for the earth, plumbing and concrete work of sports complex. She informed 99 Pipeline were the only bidders who presented a bid. She informed they bid was acceptable and asked that they be awarded the contact.

6	Award Contract for Sports Complex Phase – 2 Earth Work	A motion was made by Trustee Marisol Rubalcaba, seconded by Sandra Williams to approve Items #6 to award the Contract for Sports Complex Phase -2 Earth Work to 99 Pipeline. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
7	Award Contract for Sports Complex Phase – 2 Electrical Work	Superintendent Valdez stated the lowest bid for the electrical work came in from W&E Electric. A motion was made by Trustee Sandra Williams, seconded by Marisol Rubalcaba to approve Items #2 and award the bid to W&E Electric Co. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Absent Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
8	Award Contract for El Monte Middle School Modernization Project	Superintendent Valdez presented Item #8 and informed the lowest bid for the middle school modernization project came in from American Incorporated. A motion was made by Trustee Mary Helen Espino, seconded by Delia Martinez to approve Items #8 and award the bid contract to American Incorporated. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Absent Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
9	Award Contract for Orosi High School Modernization Project	Superintendent Valdez informed the lowest bid for the high school modernization project came in from American Incorporated. A motion was made by Trustee Delia Martinez, seconded by Marisol Rubalcaba to approve Items #9 and award the bid contract to American Incorporated. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent

		Trustee Marisol Rubalcaba - Absent Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
10	Award Contract for Sports Complex Bleachers	Superintendent Valdez informed the lowest bidder for the Sport Complex Bleachers came in from Southern Bleachers. A motion was made by Trustee Marisol Rubalcaba, seconded by Mary Helen Espino to approve Items #10 and award the bid contract to Southern Bleachers. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
11	Purchase Approval for Splash Park Equipment Using Sourcewell Cooperative Purchasing Agreement	Superintendent Valdez presented Item # 11 and informed the district was a member of Sourcewell Cooperative therefore no bidding process was necessary for Items 11 & 12. A motion was made by Trustee Mary Helen Espino, seconded by Delia Martinez to approve Items #11: Purchase Approval for Splash Park Equipment Using Sourcewell Cooperative Purchasing Agreement. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
12	Purchase Approval of Playground Equipment and Installation Using Sourcewell Cooperative Purchasing Agreement	A motion was made by Trustee Marisol Rubalcaba, seconded by Mary Helen Espino to approve Items #12 Purchase Approval of Playground Equipment and Installation Using Sourcewell Cooperative Purchasing Agreement. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes Superintendent Valdez informed the lowest bid for the OHS refrigerator/ freezer was from American Incorporated. A motion was made by Trustee Mary Helen Espino, seconded by Sandra Williams to approve Items #13 and award the bid contract to American

13	Award Contract for Walk-In Refrigerator/ Freezer for Orosi High School	Incorporated. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Absent Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
14	Approval of Piggy-Back Agreement for Modular Restroom Purchase	Superintendent Valdez presented Item #14 and explained that no bidding process was necessary for the project as they were using a piggy back agreement that had already gone out for bids. A motion was made by Trustee Sandra Williams, seconded by Marisol Rubalcaba to approve Items #14 to approve the Piggy Back Agreement with American Modular Systems for the Modular Restroom Purchase. The motion was approved with a 5 to 0 vote. With the votes as follows: Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes
15	Approval of Equipment Installation for Splash Park Using California Cost Accounting Act	Superintendent Valdez informed they would hold off on item #15 until further review. Item #15 was tabled until further notice.
Comments/Adjournment		No Additional comments were presented. A motion was made by Trustee Marisol Rubalcaba, seconded by Trustee Sandra Williams to adjourn the meeting at 5:43 p.m. Motion was approved with a 5 to 0 vote. Board President Rebecca Jimenez – Yes Board Vice President Mary Helen Espino - Yes Board Clerk Sandra Williams – Yes Trustee Lloyd McClard - Absent Trustee Marisol Rubalcaba - Yes Trustee Sarah Herrera – Absent Trustee Delia Martinez – Yes Yolanda Valdez Superintendent (mm)

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **ACCOUNTS PAYABLE TOTAL PAYMENTS FEBRUARY 1, 2019 THROUGH FEBRUARY 28, 2019**

ATTACHMENTS: **TOTAL PAYMENTS REPORTS**

DISCUSSION:

The attached Accounts Payable Total Payments Report dated February 1, 2019 through February 28, 2019 and is for expenditures paid during these periods.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Faith Price, Chief Financial Officer***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the Accounts Payable Total Payments Reports.**

PROPOSED ACTION: **APPROVE**

Item #: 3

DatePaid between 02/01/2019 and 02/28/2019

Vendor No.	Vendor Name	Ven. Type	1099		Amount
1407	CUTLER-OROSI UNIF. SCHOOL (P)	00	00	Total Payment Amount:	$10,433.27 *
2257	FRONTIER COMMUNICATIONS	00	00	Total Payment Amount:	$130.41 *
4237	OROSI PUBLIC UTILITY DIST. (0)	09	00	Total Payment Amount:	$6,178.27 *
4269	P G & E	00	00	Total Payment Amount:	$45,242.53 *
4325	CLINES BUSINESS EQUIPMENT INC.	03	00	Total Payment Amount:	$8,323.03 *
4372	PENAS DISPOSAL SERVICE INC.	03	00	Total Payment Amount:	$7,663.82 *
4672	R-N MARKET	00	00	Total Payment Amount:	$106.66 *
5308	SMITH AUTO PARTS	03	00	Total Payment Amount:	$29.36 *
5388	GAS COMPANY, THE	03	00	Total Payment Amount:	$6,389.29 *
10120	MIDWAY AUTO PARTS (9)	06	06	Total Payment Amount:	$424.46 *
11714	DINUBA LUMBER (6)	03	00	Total Payment Amount:	$1,940.21 *
11851	CUTLER PUBLIC UTILITY	09	00	Total Payment Amount:	$1,572.47 *
12284	CAVES & ASSOCIATES	02	09	Total Payment Amount:	$1,400.00 *
13225	MIDTOWN SPORTS	03	00	Total Payment Amount:	$2,986.85 *
13576	ABE-EL WHOLESALE, INC.	03	00	Total Payment Amount:	$14,883.05 *
14036	EWING IRRIGATION PRODUCTS	03	00	Total Payment Amount:	$6,676.01 *
14084	LESTER,CORRINE	00	00	Total Payment Amount:	$12.47 *
16246	EMPLOYMENT DEVELOPMENT DEPT.	00	00	Total Payment Amount:	$2,272.05 *
17821	YETTEM SEWER & WATER	00	00	Total Payment Amount:	$197.00 *
18532	CONSOLIDATED ELECTRICAL DISTR.	03	06	Total Payment Amount:	$710.93 *
18594	BATTERY SYSTEMS	00	00	Total Payment Amount:	$138.32 *
18686	KEENAN & ASSOCIATES/CPIC	03	00	Total Payment Amount:	$2,604.00 *
19008	OFFICE DEPOT	00	00	Total Payment Amount:	$11,934.47 *
19068	JIM MANNING DODGE	00	00	Total Payment Amount:	$807.24 *
19420	PRODUCERS	03	00	Total Payment Amount:	$15,321.17 *
19430	GOLD STAR FOODS	03	00	Total Payment Amount:	$52,987.71 *
19438	SYSCO FOODSERVICES OF	03	00	Total Payment Amount:	$39,066.05 *
19439	CULLIGAN	03	00	Total Payment Amount:	$2.00 *
19773	JENSEN & PILEGARD	03	00	Total Payment Amount:	$171.04 *
19903	HOME DEPOT CREDIT SERVICES	00	00	Total Payment Amount:	$395.93 *
20287	MEDICAL BILLING TECHNOLOGIES	09	00	Total Payment Amount:	$401.68 *
20632	ERNEST PACKAGING SOLUTIONS	03	00	Total Payment Amount:	$10,967.59 *

DatePaid between 02/01/2019 and 02/28/2019

Vendor No.	Vendor Name	Ven. Type	1099		Amount
20861	SISC III	09	00	Total Payment Amount:	$581,334.20 *
20966	CALIFORNIA STATE UNIVERSITY	00	00	Total Payment Amount:	$500.00 *
20969	QUINN POWER SYSTEMS	03	00	Total Payment Amount:	$756.26 *
20987	CALIFORNIA STATE UNIVERSITY	03	00	Total Payment Amount:	$19,622.97 *
21035	CHETS PLUMBING INC.	01	06	Total Payment Amount:	$157.50 *
21080	FRESNO OXYGEN	03	00	Total Payment Amount:	$2,560.95 *
21305	NORTH AMERICAN TECHNICAL SERV	03	00	Total Payment Amount:	$2,050.00 *
21584	BANKS & CO.	03	00	Total Payment Amount:	$100.00 *
21607	FERGUSON ENTERPRISES INC.#690	03	00	Total Payment Amount:	$878.67 *
21832	LAWRENCE TRACTOR CO.	03	00	Total Payment Amount:	$142.11 *
21891	VERIZON WIRELESS	00	00	Total Payment Amount:	$4,368.63 *
22028	SURTEC SYSTEM,THE	03	00	Total Payment Amount:	$224.21 *
22068	HOUGHTON MIFFLIN HARCOURT	03	00	Total Payment Amount:	$10,403.61 *
22069	TULARE COUNTY OFFICE OF ED.ERS	00	00	Total Payment Amount:	$600.00 *
22086	FRUIT GROWERS SUPPLY	00	00	Total Payment Amount:	$697.89 *
22122	RESERVE ACCOUNT	00	00	Total Payment Amount:	$6,800.00 *
22169	TULARE COUNTY REGISTRAR	00	00	Total Payment Amount:	$1,600.11 *
22193	DELL COMPUTER CORPORATION	00	00	Total Payment Amount:	$17,429.97 *
22252	TULARE COUNTY SHERIFFS DEPT.	09	00	Total Payment Amount:	$21,791.47 *
22441	CDW-G	03	00	Total Payment Amount:	$828.65 *
22442	GOLDEN VALLEY SCHOOL STUDENT B	00	00	Total Payment Amount:	$120.00 *
22455	VALLEY ELECTRICAL SUPPLIERS	03	00	Total Payment Amount:	$824.39 *
22610	SCHOOL SERVICES OF CA INC.	03	00	Total Payment Amount:	$450.00 *
22675	INTERNATIONAL AGRI-CENTER INC.	00	00	Total Payment Amount:	$1,680.00 *
22793	W & E ELECTRIC	03	00	Total Payment Amount:	$2,246.26 *
22894	UNITED REFRIGERATION INC.	03	00	Total Payment Amount:	$60.34 *
22917	POPES PROPANE SERVICE	03	00	Total Payment Amount:	$83.48 *
22963	TULARE COUNTY OFFICE OF EDUCAT	00	00	Total Payment Amount:	$6,200.00 *
23060	DELL COMPUTER CORP.	00	00	Total Payment Amount:	$100,832.16 *
23122	GRAINGER	00	00	Total Payment Amount:	$1,743.02 *
23146	DORA LOPEZ	00	00	Total Payment Amount:	$21.46 *
23173	VAL PRINT	03	00	Total Payment Amount:	$2,255.48 *

DatePaid between 02/01/2019 and 02/28/2019

Vendor No.	Vendor Name	Ven. Type	1099		Amount
23181	SCHOOLWORKS INC.	03	00	Total Payment Amount:	$2,000.00 *
23238	TETER	06	06	Total Payment Amount:	$68,797.92 *
23258	JTS MODULAR, INC.	03	00	Total Payment Amount:	$193,562.50 *
23325	ANAYA CONSTRUCTION	02	06	Total Payment Amount:	$6,250.00 *
23361	BSK & ASSOCIATES INC.	03	00	Total Payment Amount:	$761.00 *
23407	SACRAMENTO HYATT REGENCY	03	00	Total Payment Amount:	$4,371.04 *
23484	CENTRAL VALLEY LOCK & SAFE	03	00	Total Payment Amount:	$1,017.74 *
23802	MARVIN ARMSTRONG ARCHITECT	03	00	Total Payment Amount:	$4,142.86 *
23934	TERMINAL AIR BRAKE SUPPLY INC.	03	00	Total Payment Amount:	$373.35 *
24195	AWARDS AND SIGNS UNLIMITED	03	00	Total Payment Amount:	$244.13 *
24302	RITSCHEL, LEANNE	00	00	Total Payment Amount:	$79.99 *
24313	EXETER MERCANTILE CO.	03	00	Total Payment Amount:	$3,324.01 *
24417	SWANSON, SHEVONNE	00	00	Total Payment Amount:	$233.89 *
24430	JUAREZ, ADAM	00	00	Total Payment Amount:	$96.90 *
24436	BUSWEST - FRESNO	03	00	Total Payment Amount:	$1,917.02 *
24513	MCCLUNG, LINDA	00	00	Total Payment Amount:	$12.76 *
24637	SWRCB ACCOUNTING OFFICE	09	00	Total Payment Amount:	$387.20 *
24704	COMMERCIAL RADIATOR WORKS INC.	00	00	Total Payment Amount:	$1,550.00 *
24756	U. S. BANK	00	00	Total Payment Amount:	$9,550.13 *
24786	ANG, SUSAN T.	00	00	Total Payment Amount:	$48.73 *
24791	SUAREZ, ELLEN	00	00	Total Payment Amount:	$87.72 *
24839	SCOUT SPECIALTIES, INC.	03	00	Total Payment Amount:	$7.32 *
24909	LA TAPATIA TORTILLERIA, INC	00	00	Total Payment Amount:	$522.72 *
24929	SHERWIN-WILLIAMS CO., THE	03	00	Total Payment Amount:	$124.13 *
24987	MID VALLEY PUBLISHING INC.	03	00	Total Payment Amount:	$2,300.00 *
24988	KIMBALL MIDWEST	03	00	Total Payment Amount:	$1,275.91 *
25141	TRANSFINDER	03	00	Total Payment Amount:	$1,250.00 *
25205	AUTO ZONE	00	00	Total Payment Amount:	$3,158.18 *
25207	SAN JOAQUIN FAMILY HEALTHCARE	09	05	Total Payment Amount:	$115.00 *
25214	TINOS FENCE	01	06	Total Payment Amount:	$15,160.00 *
25229	LOWES -#98007297470	00	00	Total Payment Amount:	$930.14 *
25323	1ST QUALITY PRODUCE	03	00	Total Payment Amount:	$21,789.25 *

Vendor No.	Vendor Name	Ven. Type	1099		Amount
25358	U. S. BANK	03	00	Total Payment Amount:	$4,792.99 *
25364	AMERICAN INCORPORATED	03	00	Total Payment Amount:	$1,715.43 *
25428	CURRICULUM ASSOCIATES	06	06	Total Payment Amount:	$212.42 *
25430	QUINTANA, RAYMOND	00	00	Total Payment Amount:	$259.68 *
25449	DIGITECH INTEGRATION INC	03	00	Total Payment Amount:	$721.96 *
25488	RUSSELL SIGLER INC.	03	00	Total Payment Amount:	$467.58 *
25575	VALDEZ, YOLANDA	00	00	Total Payment Amount:	$462.16 *
25666	SY TECH SOLUTIONS, INC.	03	00	Total Payment Amount:	$4,200.00 *
25711	W.W. GRAINGER, INC.	03	00	Total Payment Amount:	$1,078.91 *
25741	FRESNO MOBILE RADIO	03	00	Total Payment Amount:	$300.00 *
25757	PROJECT LEAD THE WAY	03	00	Total Payment Amount:	$2,677.59 *
25778	LUSARDI MOTORSPORTS	01	06	Total Payment Amount:	$441.38 *
25843	ROCHA, VANESSA	00	00	Total Payment Amount:	$532.25 *
25937	CSNA	00	00	Total Payment Amount:	$70.00 *
25958	MADERA COUNTY OFFICE OF EDUCAT	00	00	Total Payment Amount:	$300.00 *
26013	CALIFORNIA FFA CENTER	09	00	Total Payment Amount:	$225.00 *
26016	O'REILLY AUTO PARTS	03	00	Total Payment Amount:	$1,717.50 *
26109	CVIN LLC	03	00	Total Payment Amount:	$5,977.09 *
26130	KOALA TREE SERVICE	01	06	Total Payment Amount:	$2,200.00 *
26136	NICOLAS, VICTOR	00	00	Total Payment Amount:	$2.08 *
26175	VACA, ROBERTO	00	00	Total Payment Amount:	$196.04 *
26207	SMART & FINAL - #384574	00	00	Total Payment Amount:	$195.86 *
26241	JASSO, ANA ELISA	00	00	Total Payment Amount:	$53.82 *
26289	DENNY MCCOWAN GENERAL ENGINEER	06	06	Total Payment Amount:	$8,585.00 *
26314	ARELLANO, ELISA	00	00	Total Payment Amount:	$425.54 *
26320	VALLEY SECURITY ALARM	03	00	Total Payment Amount:	$511.61 *
26326	SAN JOAQUIN REGION FFA	09	00	Total Payment Amount:	$60.00 *
26378	ISOM ADVISORS	03	00	Total Payment Amount:	$500.00 *
26388	UNIFIRST CORPORATION	03	00	Total Payment Amount:	$772.13 *
26395	MEDALLION SUPPLY	03	00	Total Payment Amount:	$428.31 *
26406	TRACTOR SUPPLY, CO.	03	00	Total Payment Amount:	$149.44 *
26415	ROSALINDA ESQUEDA	01	06	Total Payment Amount:	$2,092.70 *

Vendor No.	Vendor Name	Ven. Type	1099		Amount
26471	CENTRAL VALLEY EMBROIDERY	09	06	Total Payment Amount:	$1,388.80 *
26478	VALLEY ROP	09	00	Total Payment Amount:	$49,525.94 *
26523	ROBB ROSS FOODS INC	03	00	Total Payment Amount:	$630.50 *
26544	GONZALEZ, AMANDA	00	00	Total Payment Amount:	$2.78 *
26608	CONTRERAS, DANELLE	00	00	Total Payment Amount:	$302.99 *
26620	MEFFERD, JULIE	00	00	Total Payment Amount:	$6.62 *
26640	WALMART	00	00	Total Payment Amount:	$576.36 *
26650	SANTOS, COVINA	00	00	Total Payment Amount:	$4.17 *
26686	ANYWHERE CART	03	00	Total Payment Amount:	$4,037.39 *
26687	DOCTORS OCCUPAIONAL TESTING SO	01	00	Total Payment Amount:	$260.00 *
26693	FBC OF MODESTO LLC	00	00	Total Payment Amount:	$408.00 *
26718	NAF	03	00	Total Payment Amount:	$6,000.00 *
26721	REFRIGERATION SUPPLIES DISTRIB	03	00	Total Payment Amount:	$33.51 *
26776	CMEA CENTRAL SECTION	09	00	Total Payment Amount:	$925.00 *
26818	GALLS	03	00	Total Payment Amount:	$2,696.06 *
26833	SUPPLEMENTAL HEALTH CARE	03	00	Total Payment Amount:	$2,368.00 *
26912	AMAZON CAPITAL SERVICES	00	00	Total Payment Amount:	$2,092.82 *
26934	RONAN, ELIZABETH	00	00	Total Payment Amount:	$12.24 *
26980	APPLEBY & COMPANY, INC.	03	00	Total Payment Amount:	$20,943.86 *
26991	KAJEET INC.	03	00	Total Payment Amount:	$10,001.47 *
27020	INLAND INSPECTIONS & CONSULTIN	03	00	Total Payment Amount:	$2,500.00 *
27027	NATIONAL ASSOCIATION FOR MUSIC	03	00	Total Payment Amount:	$298.00 *
27038	BARRAGAN, LLUVIA	00	00	Total Payment Amount:	$438.84 *
27047	ROSENBALM ROCKERY	03	00	Total Payment Amount:	$1,074.95 *
27060	COTTON, ROSA	00	00	Total Payment Amount:	$28.42 *
27082	FAMAND, INC.	03	00	Total Payment Amount:	$994,915.39 *
27085	KAWEAH LIFT, INC	03	00	Total Payment Amount:	$225.41 *
27092	GOODYEAR TIRE & RUBBER CO.	03	00	Total Payment Amount:	$753.78 *
27100	VILLAGOMEZ, MARIA	00	00	Total Payment Amount:	$8.87 *
27128	NUNEZ, SOILA	00	00	Total Payment Amount:	$51.04 *
27184	SPEECH THERAPY LINK	01	06	Total Payment Amount:	$8,075.00 *
27211	CRG CLUB RESOURCE GROUP	00	00	Total Payment Amount:	$10,355.00 *

Summary

DatePaid between 02/01/2019 and 02/28/2019

Vendor No.	Vendor Name	Ven. Type	1099		Amount
27214	SA SERVING LINES, INC.	00	00	Total Payment Amount:	$8,102.64 *
27220	FRESNO NEON SIGN CO., INC.	03	00	Total Payment Amount:	$12,447.89 *
27231	DURABLE NORTH AMERICA	03	00	Total Payment Amount:	$15,263.33 *
27234	KYM KINGS INTERPRETING SERVICE	01	06	Total Payment Amount:	$3,549.70 *
27237	ARAUJO, ARIANA	00	00	Total Payment Amount:	$67.44 *
27238	VIA TRAILWAYS	03	00	Total Payment Amount:	$2,173.77 *
27246	CUSTOMLANYARDS4ALL.COM	03	00	Total Payment Amount:	$945.00 *
27248	SSICA	01	06	Total Payment Amount:	$8,766.54 *
27249	GONZALES, ROBERT	00	00	Total Payment Amount:	$90.00 *
27254	TULARE COUNTY CHARTER ACSA	00	00	Total Payment Amount:	$35.00 *
27255	MEJIA, CRYSTAL	00	00	Total Payment Amount:	$30.98 *
27259	VEGA, MONICA	00	00	Total Payment Amount:	$10.03 *
				Total Payment Amount:	$2,601,922.67 *

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **FEBRUARY CASH POSITION REPORT**

ATTACHMENTS: **FEBRUARY CASH POSITION REPORT**

DISCUSSION:

At the start of each month, the District Office reviews the District's cash position to ensure adequate cash reserves are on hand to pay for District operations. The February Cash Position Report is positive and submitted for review.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Faith Price, Chief Financial Officer***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe, supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the February Cash Position Report.**

PROPOSED ACTION: **APPROVE**

Item #: 4

County Fund	District Number	Fund		Current Cash 9110	Previous Total Payroll Objects	Percentage (%)
608	8	0100	General Fund	$19,721,193.19	$3,479,085.90	566.85
AAQ	8	1100	Adult Education Fund	$108,250.25	$22,986.71	470.93
AWL	8	1200	Child Development Fund	$266,525.57	$81,725.49	326.12
AUB	8	1300	Cafeteria Special Revenue Fund	$214,937.02	$124,320.10	172.89
AAT	8	1700	Special Reserve Fund for Other than Capital Outlay Pr	$0.60		
AZS	8	2100	Building Fund	$229,419.67		
AZT	8	2110	Building Fund #1	$1,554,576.08		
AJS	8	2120	Building Fund # 2	$2,395,836.91		
ACU	8	2130	Building Fund # 3	$2,439,386.29		
AJK	8	2510	Developer Fees Fund	$2,177.32		
ABF	8	3510	County School Facilities Fund - Modernization	$0.12		
AAV	8	4000	Special Reserve Fund for Capital Outlay Projects	$34.55		
689	8	5100	Bond Interest & Redemption Fund - #1	$905,884.04		
61C	8	5110	Bond Interest & Redemption Fund - #2	$851,119.97		
61D	8	5120	Bond Interest & Redemption Fund - #3	$0.00		
61P	8	5130	Bond Interest & Redemption Fund - #4	$200,506.20		
None	8	5610	Non-Treasury Debt Service COP/Revenue Bonds	$0.00		
			Report Total	**$28,889,847.78**		

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **INTERDISTRICT TRANSFER**

ATTACHMENTS: **INTERDISTRICT TRANSFER REQUESTS LIST**

DISCUSSION:
Attached is a list of Interdistrict transfer requests for students transferring in or out of the district that have been approved or denied.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Shevonne Swanson, Curriculum/ Instruction Director***

BOARD GOAL:

- [X] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [X] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the Interdistrict Requests.**

PROPOSED ACTION: **APPROVE**

Item #: 5

INTER-DISTRICT TRANSFER REQUESTS

March 14 2019

OUTGOING REQUESTS for 2018-19

Student	Grade	Parent	District Requested	Reason	Approved/ Denied
Angela Sanchez	5	Claudia Diaz	Kings Canyon Unified	Parent Employment	Approved

INCOMING REQUESTS for 2018-19

Student	Grade	Parent	District of Residence	Reason	Approved/ Denied
John Anthony Duncan	12	Rosa Ploneda	Dinuba Unified School	Continued Attendance	Approved
Damian Vidales	3	Elisa Arellano & Francisco Vidales	Visalia Unified School	Parent Employemnt	Approved
Mia Vidales	K	Elisa Arellano & Francisco Vidales	Visalia Unified School	Parent Employemnt	Approved
Adrian Banda	9	Jeanette Banda	Dinuba Unified School	Continued Attendance	Approved
Salvador Banda	7	Jeanette Banda	Dinuba Unified School	Continued Attendance	Approved
Adanely Banda	2	Jeanette Banda	Dinuba Unified School	Continued Attendance	Approved
Brandi Luevanos	11	Joanna Caballero	Visalia Unified School	Behavior	Denied

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **RATIFICATION OF CONTRACT WITH MERCED COUNTY OFFICE OF EDUCATION FOR EARLY EDUCATION PROFESSIONAL DEVELOPMENT SERVICES**

ATTACHMENTS: **CONTRACT FOR SERVICES**

DISCUSSION:

This contract is between Merced County Office of Education and Cutler-Orosi JUSD to provide Professional Development for early education staff on January 11, 2019. This contract is valid December 20, 2018 and will continue in effect until March 15, 2019.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Leticia Trevino, Early Childhood Education Director***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the ratification of the contract with Merced County Office of Education for professional development training services.**

PROPOSED ACTION: **RATIFY**

Item #: 6

CONTRACT FOR SERVICES

This AGREEMENT made this 20th day of December, 2018, between:

MERCED COUNTY OFFICE OF EDUCATION, (herein referred as contractor), having a principal place of business at 632 W. 13TH Street, Merced, California 95341,

And

Cutler-Orosi Joint Unified School District, having a principal place of business at 12623 Avenue 416, Orosi,CA

Term of Contract

This Agreement will become effective on December 20th, 2018, and will continue in effect until March 15, 2019 unless terminated in accordance with the provisions of this Agreement.

Services to be Performed by Contractor

Contractor agrees to: The Contractor will provide a 4 hour professional staff development for a maximum of 31 early childhood education professionals on the "SELF" from Social Emotional Domain of the Preschool Learning Foundations and Curriculum Frameworks to enhance the implementation of developmentally appropriate practices found in high quality State Preschool Program Practices. Materials provided will be purchased by Merced County Office of Education (CPIN). The training will be conducted on Friday, January 11, 2019 from 8:00am to 12:00pm

Compensation

In consideration for the services to be performed by Contractor, Cutler-Orosi Joint Unified School District agrees to pay Contractor $775.00

,Invoices

Contractor shall submit invoices for all services rendered as follows:

Invoice shall be submitted within 15 days of services rendered.

Contractor shall be responsible for all costs and expenses incident to the performance of services for Cutler-Orosi Joint Unified School District except as provided below:

Cutler-Orosi Joint Unified School District will provide an appropriate location for the training, food and beverages needed for either

Breakfast, lunch, dinner, and breaks, plus technology access and assistance.

Terms & Conditions

Confidentiality: Contractor shall comply with all laws, regulations, and professional standards pertaining to the confidentiality of Cutler-Orosi employment and student records and information which he or she may have access to in the course of performing services for Cutler-Orosi. **Term and Termination**: The term of this agreement shall be for a period commencing on 12/20/2018 and ending on 03/15/2019. It is specifically agreed by each party that this Agreement may be terminated by either party without cause or legal excuse provided that such party desiring termination gives 30 days written notice to the other party of said party's decision to terminate **Independent Contractor Status**: This Agreement is by and between independent contractors and is not intended to and shall not be construed to create a relationship of agent, servant, employee, partnership, joint venture, or association. **Exclusion of Benefits**: Contractor understands and agrees that he or she and all of his or her employees shall not be considered officers, employees, or agents of Cutler-Orosi, and are not entitled to benefits of any kind or nature normally provided employees of the Cutler-Orosi Joint Unified School District, including but not limited to, State Unemployment Compensation Insurance, or Worker's Compensation. Contractor shall assume full responsibility or payment of all federal, state, and local taxes or contributions, including unemployment insurance, social security, and income taxes with respect to Contractor's employees.

Fingerprint Clearance

Check Here: If Applicable _____ Not Applicable X

Contractor certifies that Contractor or any employees of Contractor performing services for TCOE have been cleared by a fingerprint check performed by the California Department of Justice and is not prohibited by law from being employed by TCOE or having contact with pupils pursuant to applicable State law.

Executed on 12-11-2018 at Merced, California.

CONTRACTOR:

Merced County Office of Education (MCOE)

By: _____ 12.12.18
(Signature)

Steve M. Tietjen

Merced County Superintendent of Schools or Designee

Cutler-Orosi Joint Unified School District

By: _____
(Signature)

Cutler-Orosi Joint Unified School District Superintendent or Designee

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **RATIFY MOU WITH VALLEY HEALTH TEAM**

ATTACHMENTS: **VALLEY HEALTH TEAM MOU**

DISCUSSION:

The Director of College and Career is requesting approval of the Valley Health Team MOU to establish a partnership that will provide Work Based Learning Student Internship and Job Opportunities in the clinical setting. In addition, the partnership can include industry partner participation, in other college and career events and activities. This partnership will provide students hands on learning experiences in various careers within the field of health sciences. This MOU will ensure a collaborative working partnership to support district wide student learning outcomes.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: District Director of College and Career Lisa Castillo***

BOARD GOAL:

- [X] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [X] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [X] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board.**

PROPOSED ACTION: **RATIFY**

Item #: 7

CUTLER-OROSI UNIFIED SCHOOL DISTRICT INTERNSHIP AGREEMENT

This Agreement ("**Agreement**") is entered into by and between Valley Health Team, Inc. a California non-profit corporation ("**VHT**") and Cutler-Orosi Unified School District ("**District**"), and is effective as of the date set forth below. VHT and The District shall be collectively referred to as the "**Parties**".

RECITALS

A. The District has curriculum in the following areas: "Medical Health Careers" and "Nursing Assistant" (the "**Program**"). "Classroom Theory Instruction Hours" are a required and integral component of the Hospital Community Health Curricula and "Classroom Theory Instructional Hours", clinical experience and practicum hours are required and an integral component of the Nursing Assistant curricula.

B. VHT operates as a Federally Qualified Health Center and has facilities and expertise suitable for providing field experience for the Program.

C. The District desires VHT's cooperation in implementing the clinical educational program of the Program and training students in the practical application of the Program.

D. The Parties believes that it is in their respective mutual benefit and for District students to have the opportunity to use VHT's facilities in connection with the work-based learning education portion of the Program, that Provider will benefit from the contributions of the District students' participation in the Program, and VHT wishes to assist the District in implementing the Program by providing a place and opportunity for work-based learning experiences for District students.

E. This Agreement is intended to satisfy the requirement for a formal training agreement between the District and VHT for the Program as required by Title 5 of the California Code of Regulations.

IN CONSIDERATION of the foregoing recitals, and other good and valuable consideration, receipt of which is hereby acknowledged, THE PARTIES HEREBY AGREE AS FOLLOWS:

AGREEMENT

1. **Term/Automatic Renewal.** The initial term of this Agreement shall be one year commencing January 28, 2019 (the "**Effective Date**") and will automatically be renewed on an annual basis unless terminated as provided in this Agreement.

2. **Program Schedule.** The Program will begin with formal training for the school year and will include time in the Provider's facility as mutually agreed-upon by and between VHT and the District (most likely 10-15 hours per week). Assignments to work-based learning experiences in VHT's facility will conform to the calendar adopted by the District and agreed-to by VHT as to holidays, vacations and similar events.

3. **Program Objectives.** The Parties hereby agree to the following Program objectives for District students to accomplish with VHT during the internship: (a) the application of skills and information learned in the curriculum to a "real-world" venue; (b) learn technical, professional, employability and interpersonal skills; and (c) observation of medical professionals in a typical work settings.

4. **Responsibilities of VHT.** In addition to the requirements set forth in Exhibit A (the "**Student Heath Careers Handbook**"), which is incorporated herein by reference, VHT shall have the following responsibilities:

a. **Tasks/Responsibilities.** VHT shall plan an appropriate variety of on-the-job training tasks/responsibilities for District students, allowing the District students to accomplish the objectives of the Program as set forth herein.

b. **Patient Care/Compensation/Proctoring.** VHT will at all times retain final responsibility for patient care and VHT staff will not be decreased due to the presence of District students on-site. VHT is not responsible for wages or other compensation for District students who participate in the Program. VHT staff will not be used to proctor, shadow or teach the District students.

c. **Adequate Facilities and Equipment.** VHT has and will continue to maintain adequate equipment, materials, facilities and other accommodations to provide appropriate learning opportunities for District students consistent with Program objectives.

d. **Safety.** VHT shall ensure that the work condition at its facility will be safe and secure and not endanger the health, safety, welfare or morals of District students.

e. **Records.** VHT shall maintain hourly records of District students' participation in the Program at VHT facilities and will cooperate in rating each District student's achievement at the VHT work site.

f. **Non-Discrimination.** As a recipient of federal financial assistance, VHT does not exclude, deny benefits to, or otherwise unlawfully discriminate against any person on the ground of race, color, or national origin, or on the basis of disability or age in admission to, participation in, or receipt of the services and benefits under any of its programs and activities.

g. **Adult Supervision.** VHT shall provide adequate adult supervision of District students at all times they are participating in the Program on VHT sites or using VHT facilities.

h. **Confer with District Instructor of Record.** VHT will confer with the District's Instructor of Record as designated below regarding the progress of District students and will immediately notify the Instructor of Record of any problems or concerns as well as of any indication that VHT may be considering the termination of a particular student's internship and/or participation in the Program on VHT's facilities.

5. **Responsibilities of District.** In addition to the responsibilities that may be set forth in Exhibit A, the District shall have the following responsibilities:

a. **Supervision by Instructor of Record.** The learning experiences for the students shall be planned and supervised by the Instructor of Record, which will be designated by the District and identified for VHT. The supervision of District students shall be the responsibility of the Instructor of Record and shall not exceed a 15:1 ratio at any time during the clinical schedule.

b. **Worksite Selection.** The selection and approval of worksites for each District student and the evaluation of the progress of each District student shall be the responsibility of the Instructor of Record.

c. **Progress Reports.** The District and Instructor of Record shall be responsible for all progress reports and student performance evaluation. The District shall forward evaluation and verification forms for VHT to complete and will discuss student performance with VHT and District students as appropriate.

d. **Site Visits.** The Instructor of Record will conduct a minimum of two (2) site visits at VHT facilities per school semester.

e. **Correlation with Classroom Instruction.** The District will correlate with VHT the classroom instruction with the work-based learning experience.

f. **District to Act as Liaison.** The District will act as a liaison and coordinate between the District, District students and VHT including, without limitation, assistance in the resolution of any concerns affecting the performance of District students.

g. **Health Clearance Records.** The District will be responsible for providing to VHT health clearance records for each District student participating in the Program including, without limitation, satisfactory documentation of physicals, influenza vaccination, TB clearance, CPR certification, Hepatitis B and up-to-date immunization records. The District shall submit to VHT the Affiliated School Roster with required immunizations and criminal background check clearance prior to any District student starting a clinical rotation at a VHT facility.

6. **Responsibilities of District Students.** In addition to the requirements set forth in Exhibit A, the District Student shall have the following responsibilities:

a. **Compliance with Rules and Regulations.** The District student shall be subject to and comply with VHT's rules and regulations applicable to the Program and the student's participation in the Program.

b. **District Remains Responsible for Disciplinary Actions.** District students participating in the Program shall remain at all times subject to the discipline and authority of the District and the Instructor of Record while participating in the Program.

c. **Uniforms and Identification Badge**. District students shall wear identification badges and uniform prescribed by VHT for Program participants as directed and instructed by VHT while on duty at VHT's facilities.

d. **Students to Provide Own Transportation**. District students who participate in the Program shall be responsible for providing their own transportation to and from VHT's facility unless such transportation is provided by the District. Neither the District nor VHT will assume responsibility for any activities or events that occur during the student's transportation/travel to and from VHT and the student's home or District premises except to the extent the District provides transportation to the student. VHT will not provide transportation for any student participating in the Program not will it assume any financial, operational or legal liability for such transportation.

e. **Work Permits and other Documentation**. District students shall complete any required documentation and obtain work permits, if required, prior to starting the internship and participating in the Program.

f. **Emergency Data/Authorization Consent Form**. District Students must complete the Emergency Data Sheet and Medical Authorization Consent form on file with the District prior to starting the internship and participating in the Program.

7. **Responsibilities of Parent/Guardian**. The parent/guardian of the participating District Students shall have the following responsibilities:

a. **Parent/Guardian Permission**. To the extent the District student is a minor (under the age of 18), the student's parent or guardian must provide permission for the District student to participate in the program prior to the student starting the internship and participating in the Program.

b. **Permission for Student to Be Off Campus**. To the extent the District student is a minor (under the age of 18), the student's parent or guardian must provide permission for the District student to leave school premises to participate in the Program.

c. **Student Travel the Responsibility of Parent/Guardian**. The parent/guardian of the District student hereby assumes responsibility for all student travel or transportation for the District student to participate in the Program.

d. **Parent/Guardian to Assist Student**. The parent/guardian of the District student participating in the Program shall assist student in the successful completion of the student's participation in the Program.

e. **Parent/Guardian Provide Off-Campus Supervision**. The parent/guardian of the District student participating in the program must assume responsibility for the student's supervision while off campus.

8. **Insurance**. The District shall secure, provide and maintain the following insurance coverage with at least the minimum amounts set forth below: (a) comprehensive general liability in the amount of $1,000,000.00; (b) worker's compensation liability which includes

medical benefits and liability insurance covering the activities of District students participating in the Program as provided under California Education Code Section 51769 and Tulare County Schools Insurance Group. The District will provide VHT with a Certificate of Insurance evidencing the above-referenced insurance coverage. VHT will secure, provide and maintain worker's compensation insurance that covers all VHT employees including coverage applicable to any employee-employer relationship created as a result of the relationship by and between VHT and any District student. VHT will forward to the District a Certificate of Insurance evidencing such coverage.

9. **Termination.** The Parties may terminate this agreement with or without "cause" on thirty (30) days' written notice to the other party. The right to terminate may be exercised without prejudice to any other right or remedy to which the terminating party may be entitled under California law or this Agreement. To the extent either party exercises the right to terminate this Agreement, the Parties agree to cooperate with one another so as to minimize any disruption to the District student or the Program.

10. **Indemnification**. The Parties each agree to hold each other harmless, defend and indemnify the other, its agents, officers, employees, contractors and students from and against any and all liability, claims, actions, costs, damages or losses of any kind including, without limitation, personal injuries or death to any person and/or proper damage arising from or in any way related to their performance under this Agreement. This indemnity provisions specifically applies to the Parties, their agents, officers, employees, contractors or students and specifically includes any claims that may be made against the District by any taxing authority asserting that an employer-employee relationship exists by reason of this Agreement, and any claims made against the District alleging civil rights violations by VHT under Government Code Section 12920 and 12940 et seq. (the California Fair Employment and Housing Act) and any related statutes or regulations. This indemnification provision shall survive the termination or expiration of this Agreement as to any acts or omissions occurring during the term of this Agreement or any extension thereof.

11. **Notice.** Whenever, under the terms of this Agreement written notice is required or permitted to be given by any other party, such notice shall be deemed to have been sufficiently given if personally delivered or deposited in the United States Mail, in a properly stamped envelope, certified or registered mail, return receipt requested, addressed to the party to whom it is to be given, at the party's last known address.

12. **Governing Law.** This Agreement shall be interpreted with and governed by the laws of the State of California, with venue and jurisdiction for any dispute in the County of Tulare.

13. **No Assignment**. Neither party shall assign or transfer any or all of its rights, burdens, duties or obligations under this Agreement without the prior written consent of the other party.

14. **Tuberculosis Certification.** VHT employees will have more than limited contact with District students during the term of this Agreement and, at no cost to District, will have received a tuberculosis test/screening in full compliance with California Education Code section 49406. VHT shall remain on file forms/certificates showing that VHT employees having contact with District students have been examined and found free of active tuberculosis. Such

forms/certificates are to be regularly maintained and updated by VHT and available for review/inspection by District upon request or as part of an audit.

15. **Binding upon Successors and Assigns.** All covenants, terms and provisions contained herein shall be binding upon and inure to the benefit of the permitted successors, executors, heirs, representatives, administrators and assigns of the Parties.

16. **Work Product**. The product of all work generated under this Agreement related to the performance of the District students under this Agreement including, without limitation, any reports, evaluations or other documentation shall be deemed District property and, to the fullest extent of the law, be intended and treated as confidential student records exempt from disclosure under the Family Educational Rights and Privacy Act (FERPA), the California Education Code and similar federal and state privacy laws. Upon the termination or expiration of this Agreement, VHT shall provide to District any records constituting "work product" as defined herein.

17. **Severability.** If any provision of this Agreement shall for any reason and to any extent be deemed invalid or unenforceable, the remainder of this Agreement and application of such provisions to other persons or circumstances shall remain valid and enforceable to the fullest extent of the law.

18. **Entire Agreement**. This Agreement constitutes the entire understanding and agreement of the Parties with respect to the subject matter of this Agreement, and supersedes all prior and contemporaneous Agreements, understandings, inducements or conditions, express or implied, written or oral between the Parties. The express terms hereof control and supersede any course of performance or usage of the trade inconsistent with any of the terms of this Agreement. Where there is a conflict between any provision in this Agreement and any VHT rule or regulation, the terms of this Agreement shall apply.

19. **No Modification.** This Agreement may be modified only by a writing signed by the party to be bound thereby.

20. **No Waiver.** The failure of any party to enforce any provision of this Agreement shall not be construed to a waiver of the right of such a party thereafter to enforce such provisions. No provision of this Agreement may be waived by oral or implied conduct, but only through a writing signed by The District and the Chief Executive Officer of VHT.

21. **Compliance with the Law**. VHT agrees comply with the applicable federal state and local laws, rules, regulations and ordinances applicable to the subject matter of this Agreement including, without limitation, confidentiality of records and California Education Code section 49406. VHT agrees that it shall comply with all legal requirement for the performance of its duties under this Agreement and that failure to do so shall constitute a material breach of this Agreement.

22. **HIPAA Compliance**. The Parties agree that District does not qualify as a business associate as it does not meet the requirements of 45 C.F.R. 160.103. However, the Instructor of Records and all District Students will be instructed as to the applicable provisions of the Health Insurance Portability and Accountability Act of 1996, and the requirements of any

regulations promulgated thereunder including, without limitation, the federal privacy regulations as (collectively, HIPAA). The Instructor of Record and District Students will be required to comply with the provision of HIPAA to the extent applicable and required and will not use or disclose any protected health information, other than as authorized by this Agreement and permitted under HIPAA. VHT will provide training to the Instructor of Record and District students participating in the Program on compliance with HIPAA.

23. **Independent Representation**. The Parties each recognize that in entering into this Agreement the Parties have relied upon the advice of their own attorneys and representatives, and that the terms of this Agreement have been completely read and explained to them by their attorneys or representatives, and that those terms are fully understood and voluntarily accepted.

24. **Execution of Other Documents/Completion of Other Actions**. The Parties shall fully cooperate in the execution of any and all other documents and in the completion of any additional actions that may be necessary or appropriate to give full force and effect to the terms and intent of this Agreement.

25. **Authority to Bind**. The Parties represent and agree that the person(s) signing on their behalf have the power and authority to execute this Agreement on behalf of that party.

26. **Governing Board**. This Agreement is contingent upon the approval of the District's governing board.

27. **Execution of Counterparts**. The Agreement may be executed by counterparts such as the signatures may appear on separate pages. A copy, facsimile or an original, with all the signatures appended together, shall be deemed a fully-executed agreement.

IN WITNESS WHEREOF, the Parties, having read and considered the provisions of this Agreement, indicate their agreement by their authorized signatures below.

VALLEY HEALTH TEAM, INC.

By: _____
Soyla A. Reyna-Griffin
Chief Executive Officer

Date: February 8, 2019

Address:

_____
_____
_____

CUTLER-OROSI UNIFIED SCHOOL DISTRICT

By: _____

Its: Superintendent

Date: 3/5/19

Address:

_____
_____
_____

(signatures continues on next page)

By: _____
Director of College and Career

Date:_____

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **OHS SPEECH & DEBATE OVERNIGHT TRIP TO LONG BEACH, CA.**

ATTACHMENTS: **SPEECH & DEBATE OVERNIGHT TRIP - SUPPORTING DOCUMENTS**

DISCUSSION:

The Orosi High School Speech & Debate team will be participating in the Speech and Debate State Championships Competition at California State University, Long Beach in Long Beach, CA. They will depart at 4:00PM on Thursday, May 2, 2019 and return home on May 6, 2019. Participation in this event requires overnight stay. The team will consist of students, Speech & Debate Coach Karson Kalashian, along with teachers Katherine Delgado and Destiny Warren will chaperone the students. The trip will be funded by the Speech and Debate budget and fundraising efforts.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Shevonne Swanson, Director of Curriculum, Instruction, and Professional Learning***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [x] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [ ] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the OHS Speech & Debate Overnight Trip to Long Beach, Ca.**

PROPOSED ACTION: **APPROVE**

Item #:8

Cutler-Orosi Joint Unified School District

12623 Avenue 416 Orosi, CA 93647

ACTIVITY REQUEST FORM

Teacher/Coach Mr. Kalashian / Mrs. Wade Date Submitted 2 / 27 / 2019

Type of Activity: Fundraiser [ ] Field Trip [X] Assembly/Parent Program [ ]
Other [X] Speech and Debate Competition

Please list approved fundraiser: N/A
Does this activity require an overnight stay? YES [X] NO [ ]

Please note that any activity requiring an overnight stay MUST be Board approved one month in advance! Please submit this paperwork at least 6 weeks before the overnight trip. For all other activities, please submit paperwork at least 4 weeks prior to activity.

Grades/Departments/Club/Team Involved: 9-12, Speech and Debate

Activity Date 5/2/19 – 5/6/19 **Activity Time** 4:00 P.M. 5/2/19 **to** Approx. 12:00 P.M. 5/6/19 **Location** Cal State University Long Beach

Activity Purpose To compete in speech and debate tournament (State Championships__)

Transportation Needed? YES [X] NO [ ]

If "YES" please complete and attach the Transportation Request Form to this Activity Request form.

If transportation is provided by a private vehicle the driver must be registered with the District and meet all requirements of fingerprinting and AB1025.

Name(s) of person(s) driving Mr. Kalashian / Mrs. Wade

What is the impact of this activity/trip on instructional time?
Students will miss Friday all day and Monday half day school to compete

How is this activity correlated to the standards or to topics currently under study?
Students will compete in speech and debate events that relate directly to units of study from their courses at OHS

If this is a fundraising activity, explain how and when money will be collected.
N/A

Who is responsible for collecting money from fundraisers or for field trip? Mr. Kalashian (student contribution)

Remember any field trip must have enough money collected to ensure that all children in the group/club/class can participate whether or not they choose to participate.

Prior to approval of this activity, the Administrator may require input from other staff members to ensure a balance between academic focus and maximization of instructional time. If the request is not approved, the staff should work cooperatively with the Principal to see possible compromises and/or solutions.

Kalashian Signature of Teacher/Coach	2/27/2019 Date	559-284-1780 Cell # - for field trips only
Signature of Department Chair/Grade Level Lead or Activity Director/Athletic Director	Date	
Signature of Administrator	2-28-19 Date	Approved ✓ Rejected ___

Reason for Rejection: Pending Board Approval

TRANSPORTATION REQUEST

№ 004419

ALL FIELDS MUST BE COMPLETED IN TOP BOX

SITE OR PROGRAM SECTION

DAY OF TRIP: (S) (M) T W (TH) (F) (S)

NUMBER OF STUDENTS ON TRIP: Approx 9

TRIP DATE: May 2nd - 6th, 2019

REQUESTED BY: Kalashian

TYPE OF VEHICLE REQUEST: BUS ☐ VAN 2 ☑ CAR ☐

ACTIVITY: State Championships (Speech/Debate) 5/3/19

DEPARTURE TIME: 4:00 AM/PM 5/2/19

RETURN TIME: Approx 12:00 AM/PM

DEPART FROM: Front of OHS

DESTINATION: State Champs. (Cal-State Long Beach)

ACCT CODE:

FUND	RESOURCE	PY	GOAL	FUNCTION	OBJECT	SITE	LOCAL	LCAP	MANAGER
010	07200	0	111 00	10000	57103	000	0000	1804	200

MEALS REQUESTED FOR TRIP: 2 Vans needed (8 people max each, driver + others); Gas Card needed.

MEALS NEEDED YES ☐ NO ☒ BREAKFAST ____ LUNCH ____ SNACK ____ SUPPER ____ ADULT MEALS ____ CHARGE TO ADULT

MEAL PICKUP LOCATION: CUTLER ☐ GV ☐ PALM ☐ EL MONTE ☐ OHS ☐ LOVELL ☐

SPECIAL MEAL ACCOMMODATION NEEDED? YES ☐ NO ☐ STUDENTS WITH ANY KNOWN ALLERGIES? YES ☐ NO ☐

NUMBER OF STUDENTS WITH ALLERGIES: ____ TYPE OF ALLERGIES: ____

→ TRIP APPROVAL BY SITE/PROGRAM ADMINISTRATOR (SIGNATURE): ____ DATE: 2/28/19

ABSOLUTELY NO EATING/DRINKING ON THE BUS. CLEATS MAY NOT BE WORN ON THE BUS.
BE AT THE SCHOOL 15 MINUTES PRIOR TO DEPARTURE.

TRANSPORTATION DEPARTMENT SECTION

DRIVER ASSIGNED: ____ BUS ASSIGNED: ____

DRIVER START TIME: ____ AM/PM DRIVER STOP TIME: ____ AM/PM TOTAL TIME: ____

PASSENGERS HAVE RECEIVED EVACUATION INSTRUCTIONS: (DRIVERS INITIALS) ____

DEPARTURE LOCATION	TIME	MILEAGE	ARRIVAL LOCATION	TIME	MILEAGE	PASSENGERS

STARTING ODOMETER: ____ ENDING ODOMETER: ____ TOTAL MILES: ____

TRIP DIRECTIONS: ____

TRIP COMMENTS: ____

DRIVER SIGNATURE: ____ DATE: ____

CHARGES: ____ MILES X CAR $0.75 ☐ VAN $1.00 ☐ BUS $5.00 ☐ TOTAL COST: $ ____

DISTRIBUTION: WHITE-TRANSPORTATION YELLOW-BUSINESS OFFICE PINK: SCHOOL SITE OR PROGRAM GOLDEN: FOOD SERVICES

The Speech and Debate team will be traveling to the State Championships in Long Beach on May 2nd at 4:00 P.M., and returning on May 6th at approx. 12:00 P.M. This will be a great opportunity that our students have qualified for to compete against roughly the top 150 schools in the state. They will also get to explore a large high college campus and bond further as a team.

This competition offers all speech and debate events, and is the hardest competition in the State. We plan to take approx. 9 students based on current qualifications. We will take two school district vans, and hotel arraignments are still TBD.

Thank you for the continued support of the program, especially as we continue to expose all freshman to the subject in hopes of making them more powerful communicators, helping them with their eventual senior exit interviews, and growing the core of the team for the future. Please approve our trip!

Respectfully,

Karson Kalashian

Head Speech and Debate Coach

Orosi High School, CA

Cell: (559) 284-1790

CALIFORNIA HIGH SCHOOL SPEECH ASSOCIATION

chssa.org

2019 CHSSA State Championship Schedule

California State University Long Beach | May 3-5, 2019

(Schedule subject to change)

Friday

10:00 AM	Registration (until noon)
12:00 PM	New Coach Meeting
1:00 PM	Welcome Coaches' Meeting (open to all coaches)
2:00 PM	Congress Meeting, Round 1 CX, Round 1 LD, Round 1 PF
3:45 PM	Round 1 Congress, Round 1 Pattern A IE
5:45 PM	Round 2 LD, Round 2 PF, Round 1 Parli
7:30 PM	Round 1 Pattern B IE

Saturday

8:00 AM	Round 2 CX, Round 2 Parli
9:45 AM	Round 2 Congress, Round 2 Pattern A IE
11:30 AM	Round 3 CX, Round 3 LD, Round 3 PF
1:15 PM	Round 3 Congress, Round 2 Pattern B IE
3:00 PM	Round 4 LD, Round 4 PF, Round 3 Parli
4:45 PM	Round 3 Pattern A IE
6:30 PM	Round 4 CX, Round 4 Parli
8:15 PM	Round 3 Pattern B IE

Sunday

8:00 AM	Octofinals CX, LD, PF, Parli
9:45 AM	Semis Congress, Semis Pattern A IEs
11:15 AM	Quarterfinals CX, LD, PF, Parli
1:00 PM	Semis Pattern B IE
2:00 PM	Finals Congress, Semifinals CX, LD, PF, Parli
3:30 PM	Finals All IE
4:30 PM	Finals CX, LD, PF Parli
6:00 PM	Awards at Marina High
8:00 PM	Dinner/Dance at Glowzone

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **WORK BASED LEARNING LEADERSHIP DEVELOPMENT SKILLS USA STATE LEADERSHIP CONFERENCE FOR LAW AND JUSTICE PATHWAY OVERNIGHT TRIP TO ONTARIO, CA.**

ATTACHMENTS: **SKILLS USA STATE LEADERSHIP CONFERENCE ITINERARY AND ACTIVITY REQUEST FORM**

DISCUSSION:
The Director of College and Career is requesting permission for Skills USA Advisor/Instructor Mr. Morgan, a Female Chaperone/Staff Member and 3 Law and Justice Pathway students to attend the Skills USA State Leadership Conference in Ontario California on April 24-28, 2019. This team competed in the Law and Justice- Crime Scene Investigations Event at the Regional Competition held in Paso Robles on February 23, 2019 and has advanced to the state level competition. Expenses for the trip will be covered through CCPT 2 & CTE Incentive $'s

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Lisa Castillo, College & Career Readiness Director***

BOARD GOAL:

- [X] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- **2.**
- [X] **3. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [X] **4. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the Law and Justice Pathway Overnight Trip to Ontario California.**

PROPOSED ACTION: **APPROVE**

Item #: 9

Cutler-Orosi Joint Unified School District

12623 Avenue 416 Orosi, CA 93647

ACTIVITY REQUEST FORM

Teacher/Coach : Morgan **Date Submitted** 3/4/2019

Type of Activity: Fundraiser [ ] Field Trip [ ] Assembly/Parent Program [ ]
Other [X] WBL: CTE Law and Justice Leadership Development - SKILLS USA STATE Leadership Conference
Please list approved fundraiser: n/a
Does this activity require an overnight stay? YES [X] NO [ ]

Please note that any activity requiring an overnight stay MUST be Board approved one month in advance! Please submit this paperwork at least 6 weeks before the overnight trip. For all other activities, please submit paperwork at least 4 weeks prior to activity.

Grades/Departments/Club/Team Involved: three students 12th gr. in CTE Courses

Activity Date 4/25/2019 @ 1 p.m. **Activity Time to** 4/28/19 @ 5:30 PM

Location:

Ontario Convention Center, 2000 E Convention Center Way, Ontario, CA 91764 Ontario, CA

Activity Purpose: Compete in Skills USA State Leadership Conference: CSI

Transportation Needed? YES [X] NO [ ]

If "YES" please complete and attach the Transportation Request Form to this Activity Request form. (Van)

If transportation is provided by a private vehicle the driver must be registered with the District and meet all requirements of fingerprinting and AB1025.

Name(s) of person(s) driving: Advisor/Instructor Bryan

What is the impact of this activity/trip on instructional time?

Students will be gone during instructional hours; however, each must meet academic eligibility requirements and make arrangements with other instructors to complete work missed ahead of time.

How is this activity correlated to the standards or to topics currently under study?

This activity correlates to course specific career technical education standards, college and career readiness standards and district student learning outcomes

If this is a fundraising activity, explain how and when money will be collected. n/a

Who is responsible for collecting money from fundraisers or for field trip? n/a
Remember any field trip must have enough money collected to ensure that all children in the group/club/class can participate whether or not they choose to participate.

Prior to approval of this activity, the Administrator may require input from other staff members to ensure a balance between academic focus and maximization of instructional time. If the request is not approved, the staff should work cooperatively with the Principal to see possible compromises and/or solutions.

Signature of Teacher/Coach	Date	Cell # - for field trips only
Signature of Department Chair/Grade Level Lead or Activity Director/Athletic Director	Date	
Signature of Administrator	Date	Approved ___ Rejected ___ (PENDING BOARD APPROVAL)

Reason for Rejection: _____

Orosi High School

52nd Annual Skills USA California State Leadership & Skills Conference

Itinerary for April 25-27, 2019

Leads: Morgan & Castillo
Destination: Ontario Convention Center
2000 E Convention Center Way, Ontario, CA 91764
Purpose: Students placed in top 8 teams at Regional competition advancing to Skills USA Ca. State Leadership and Skills Conference, where each will demonstrate competency and skills learned in Crime Scenes Investigations.

Depart OHS: 1:00 pm.
Estimated Arrival 5 p.m.

Thursday, April 25, 2019

- 12 p.m.-6 p.m. – Conference Registration OPEN Ontario Convention Center
- 6:30 p.m.-8:30 p.m. – 52nd Annual SkillsUSA State Conference Opening Ceremonies
- 8:45 p.m.-10 p.m. – Select Contest Orientations
- 11 p.m. – CURFEW

Friday, April 26, 2019

- 8 a.m.-4 p.m. – Leadership Competitive events (OCC); Skill Event Orientations, testing and pre-work (OCC); Teacher Professional Development sessions (OCC); Voting Delegate sessions (Doubletree Hotel)
- 5:30 p.m.-8:30 p.m. – Food Ruck Frenzy (OCC) 6 trucks this year plus entertainment!
- 10:00 p.m. – CURFEW

Saturday, April 27, 2019

- 7:00 a.m.-4 p.m. – Skill Competitive Events (OCC and Surrounding business and Community Colleges); Teacher Professional Development sessions (OCC); Voting Delegate sessions (Doubletree Hotel)
- 4:00 p.m.-4:45 p.m. – Skill Event Debrief (as determined by event judges and evaluators)
- 6:00-9:00 p.m. – Social Event
- 6:00-7:30 p.m. – Evening of Excellence Celebration
- 10:00 p.m. – CURFEW

Sunday, April 28, 2019

- 7:00-11:00 a.m. – Award Presentations and Closing Ceremonies (OCC Grand Hall A&B)
- 11:15 a.m.-1:00 p.m. – NLSC Debrief and Incoming State Officer Orientation

Estimated Arrival back to OHS: 5:30 pm.
**Students to bring $ for personal expenses & dinner meals.

Student Conduct: Students are expected to adhere to the same discipline rules/policies set in place at site of student enrollment. We ask that students be students of character representing themselves, school and community with respect and professionalism when away at school sponsored events and activities.

Student Dress: Students are expected to comply with school dress code policy and in a manner that will reflect professionalism. Recommend students should bring casual wear attire appropriate for weather. During competition and conference students will wear team apparel /Polos Uniforms supplied by OHS.

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **UPDATED BOARD POLICY 6173: EDUCATION FOR HOMELESS CHILDREN**

ATTACHMENTS: **UPDATED BOARD POLICY 6173: EDUCATION FOR HOMELESS CHILDREN**

DISCUSSION:

The following policy has been updated to reflect the need to incorporate the needs of homeless children into the local control and accountability plan if there are more than 15 homeless students in the District, to identify a District Liaison for homeless children and youth, and to identify students with a housing questionnaire. In addition, the updates indicated a need to provide homeless children and youth with comparable services and shall not be segregated based on their status. The current COJUSD BP 6173. was last updated on December 14, 2006 with the most recent recommended changes being dated 10/16.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Antonio Quintanilla; Director of Special Services:***

BOARD GOAL:

- ☐ **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- ☐ **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- ☐ **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve Update BP 6173: Education for Homeless Children.**

PROPOSED ACTION: **APPROVE**

Item #: 10

Cutler-Orosi Joint USD

Board Policy

Education For Homeless Children

BP 6173
Instruction

The Board of Education desires to ensure that homeless children have access to the same free and appropriate public education provided to other children within the district. The district shall provide homeless students with access to education and other services necessary them to meet the same challenging academic standards as other students.

(cf. 6011 - Academic Standards)

The Superintendent or designee shall identify and remove any barriers to the identification and enrollment of homeless students and to the retention of homeless students due to absences or outstanding fees or fines. (42 USC 11432)

(cf. 3250 - Transportation Fees)
(cf. 3260 - Fees and Charges)
(cf. 5113.1 - Chronic Absence and Truancy)

When there are at least 15 homeless students in the district or a district school, the district's local control and accountability plan (LCAP) shall include goals and specific actions to improve student achievement and other outcomes of homeless students. (Education Code 52052, 52060)

(cf. 0460 - Local Control and Accountability Plan)

The Superintendent or designee shall designate an appropriate staff person to serve as a liaison for homeless children and youths. The district liaison shall fulfill the duties specified in 42 USC 11432 to assist in identifying and supporting homeless students to succeed in school.

In order to identify district students who are homeless, the Superintendent or designee may give a housing questionnaire to all parents/guardians during school registration, make referral forms readily available, include the district liaison's contact information on the district and school web sites, provide materials in a language easily understood by families and students, provide school staff with professional development on the definition and signs of homelessness, and contact appropriate local agencies to coordinate referrals for homeless children and youth and unaccompanied youth.

(cf. 1113 - District and School Web Sites)
(cf. 4131 - Staff Development)
(cf. 4231 - Staff Development)
(cf. 4331 - Staff Development)

Information about a homeless student's living situation shall be considered part of a student's educational record, subject to the Family Educational Rights and Privacy Act and shall not be deemed to be directory information as defined in 20 USC 1232g. (42 USC 11432)

(cf. 5125 - Student Records)
(cf. 5125.1 - Release of Directory Information)

The Superintendent or designee shall ensure that placement decisions for homeless students are based on the student's best interest as defined in law and administrative regulation.

Each homeless student shall be provided services that are comparable to services offered to other students in the school, including, but not limited to, transportation, educational programs for which the student meets the eligibility criteria (such as federal Title I services or similar state or local programs, programs for students with disabilities, and educational programs for English learners), career and technical education programs, programs for gifted and talented students, and school nutrition programs. (42 USC 11432)

(cf. 3550 - Food Service/Child Nutrition Program)
(cf. 3553 - Free and Reduced Price Meals)
(cf. 5148.2 - Before/After School Programs)
(cf. 5148.3 - Preschool/Early Childhood Education)
(cf. 6159 - Individualized Education Program)
(cf. 6164.2 - Guidance/Counseling Services)
(cf. 6171 - Title I Programs)
(cf. 6172 - Gifted and Talented Student Program)
(cf. 6174 - Education for English Learners)
(cf. 6177 - Summer Learning Programs)
(cf. 6178 - Career and Technical Education)
(cf. 6179 - Supplemental Instruction)

Homeless students shall not be segregated into a separate school or program based on their status as homeless and shall not be stigmatized in any way. However, the Superintendent or designee may separate homeless students on school grounds as necessary for short periods of time for health and safety emergencies or to provide temporary, special, and supplementary services to meet the unique needs of homeless students. (42 USC 11432, 11433)

(cf. 0410 - Nondiscrimination in District Programs and Activities)
(cf. 3553 - Free and Reduced Price Meals)

The Superintendent or designee shall coordinate with other agencies and entities to ensure that homeless children and youth are promptly identified, ensure that homeless students have access to and are in reasonable proximity to available education and related support services, and raise the awareness of school personnel and service providers of the effects of short-term stays in a shelter and other challenges associated with homelessness. Toward these ends, the

Superintendent or designee shall collaborate with local social services agencies, other agencies or entities providing services to homeless children and youth, and, if applicable, transitional housing facilities. In addition, the Superintendent or designee shall coordinate transportation, transfer of school records, and other interdistrict activities with other local educational agencies. As necessary, the Superintendent or designee shall coordinate, within the district and with other involved local educational agencies, services for homeless students and services for students with disabilities. (42 USC 11432)

District liaisons and other appropriate staff shall participate in professional development and other technical assistance activities to assist them in identifying and meeting the needs of homeless students and to provide training on the definitions of terms related to homelessness. (42 USC 11432)

At least annually, the Superintendent or designee shall report to the Board on outcomes for homeless students, which may include, but are not limited to, school attendance, student achievement test results, promotion and retention rates by grade level, graduation rates, suspension/expulsion rates, and other outcomes related to any goals and specific actions identified in the LCAP. Based on the evaluation data, the district shall revise its strategies as needed to better support the education of homeless students.

(cf. 0500 - Accountability)
(cf. 6162.51 - State Academic Achievement Tests)
(cf. 6190 - Evaluation of the Instructional Program)

Legal Reference:
EDUCATION CODE
39807.5 Payment of transportation costs by parents
48850 Educational rights of homeless and foster youth
48852.5 Notice of educational rights of homeless students
48852.7 Enrollment of homeless students
48915.5 Recommended expulsion, homeless student with disabilities
48918.1 Notice of recommended expulsion
51225.1-51225.3 Graduation requirements
52052 Accountability; numerically significant student subgroups
52060-52077 Local control and accountability plan
CODE OF REGULATIONS, TITLE 5
4600-4687 Uniform complaint procedures
UNITED STATES CODE, TITLE 20
1087vv Free Application for Federal Student Aid; definitions
1232g Family Educational Rights and Privacy Act
6311 Title I state plan; state and local educational agency report cards
UNITED STATES CODE, TITLE 42
11431-11435 McKinney-Vento Homeless Assistance Act
12705 Cranston-Gonzalez National Affordable Housing Act; state and local strategies

Management Resources:
CALIFORNIA CHILD WELFARE COUNCIL PUBLICATIONS
Partial Credit Model Policy and Practice Recommendations
CALIFORNIA DEPARTMENT OF EDUCATION PUBLICATIONS
Homeless Education Dispute Resolution Process, January 30, 2007
NATIONAL CENTER FOR HOMELESS EDUCATION PUBLICATIONS
Homeless Liaison Toolkit, 2013
U.S. DEPARTMENT OF EDUCATION GUIDANCE
Dear Colleague Letter, July 27, 2016
Education for Homeless Children and Youths Program, Non-Regulatory Guidance, July 2016
WEB SITES
California Child Welfare Council: http://www.chhs.ca.gov/Pages/CAChildWelfareCouncil.aspx
California Department of Education, Homeless Children and Youth Education:
http://www.cde.ca.gov/sp/hs/cy
National Center for Homeless Education at SERVE: http://www.serve.org/nche
National Law Center on Homelessness and Poverty: http://www.nlchp.org
U.S. Department of Education: http://www.ed.gov/programs/homeless/index.html

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **UPDATED ADMINISTRATIVE REGULATION 6173: EDUCATION FOR HOMELESS CHILDREN**

ATTACHMENTS: **UPDATED ADMINISTRATIVE REGULATION 6173: EDUCATION FOR HOMELESS CHILDREN**

DISCUSSION:

The following documentation lists the types of changes and actions to be provided by the Cutler-Orosi Joint Unified School District in accordance to revisions in Homeless Children programs and supports. The current COJUSD AR 6173.1 was last updated on August 9, 2012 with the most recent recommended changes being dated 10/17.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Antonio Quintanilla; Director of Special Services:***

BOARD GOAL:

- ☐ **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- ☐ **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- ☐ **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approved Updated AR 6173: Education for Homeless Children**

PROPOSED ACTION: **APPROVE**

Item #: 11

Cutler-Orosi Joint USD

Administrative Regulation

Education For Homeless Children

AR 6173
Instruction

Definitions

Homeless means students who lack a fixed, regular and adequate nighttime residence and includes: (Education Code 48852.7; 42 USC 11434a)

1. Students who are sharing the housing of other persons due to loss of housing, economic hardship, or a similar reason; are living in motels, hotels, trailer parks or camping grounds due to the lack of alternative adequate accommodations; are living in emergency or transitional shelters; are abandoned in hospitals; or are awaiting foster care placement

2. Students who have a primary nighttime residence that is a public or private place not designed for or ordinarily used as regular sleeping accommodations for human beings

3. Students who are living in cars, parks, public spaces, abandoned buildings, substandard housing, bus or train stations, or similar settings

4. Migratory children who qualify as homeless because the children are living in conditions described in #1-3 above.

Unaccompanied youth includes youth who are not in the physical custody of a parent or guardian. (20 USC 11434a)

School of origin means the school that the student attended when permanently housed or the school in which the student was last enrolled, including a preschool. If the school the homeless student attended when permanently housed is different from the school in which he/she was last enrolled, or if there is some other school that he/she attended within the preceding 15 months and with which he/she is connected, the district liaison shall determine, in consultation with and with the agreement of the homeless student and the person holding the right to make educational decisions for the student, and in the best interests of the homeless student, which school shall be deemed the school of origin. (Education Code 48852.7; 42 USC 11432). (42 USC 11432)

Best interest means that, in making educational and school placement decisions for a homeless student, consideration is given to, among other factors, educational stability, the opportunity to be educated in the least restrictive educational setting necessary to achieve academic progress, and the student's access to academic resources, services, and extracurricular and enrichment activities that are available to all district students. (Education Code 48850, 48853; 42 USC 11432)

District Liaison

The Superintendent designates the following staff person as the district liaison for homeless students (42 USC 11432):

Director of Special Services
12623 Ave 416
Orosi, CA 93647
(559) 528-4075

The district's liaison for homeless students shall: (42 USC 11432)

1. Ensure that homeless students are identified by school personnel and through outreach and coordination activities with other entities and agencies.

(cf. 1400 - Relations between Other Governmental Agencies and the Schools)
(cf. 3553 - Free and Reduced-Price Meals)

2. Ensure that homeless students are enrolled in, and have a full and equal opportunity to succeed in, district schools

3. Ensure that homeless families, children, and youth have access to and receive educational services for which they are eligible, including services through Head Start and Early Head Start programs, early intervention services under Part C of the federal Individuals with Disabilities Education Act, and other preschool programs administered by the district.

(cf. 5148.3 - Preschool/Early Childhood Education)

4. Ensure that homeless families and students receive referrals to health care services, dental services, mental health and substance abuse services, housing services, and other appropriate services

(cf. 5141.6 - School Health Services)

5. Inform parents/guardians of the educational and related opportunities available to their children and ensure that they are provided with meaningful opportunities to participate in the education of their children.

(cf. 5145.6 - Parental Notifications)

6. Disseminate notice of the educational rights of homeless students in locations frequented by parents/guardians of homeless children and youth and by unaccompanied youth, including schools, family shelters, public libraries, and hunger relief agencies (soup kitchens). The rights shall be presented in a manner and form understandable to the parents/guardians of homeless students and unaccompanied youth.

7. Mediate enrollment disputes in accordance with law and the section "Resolving Enrollment Disputes" below.

8. Fully inform parents/guardians of homeless students and unaccompanied youth of all transportation services, including transportation to the school of origin, and assist them in accessing transportation to the school of choice.

(cf. 3541 - Transportation Routes and Services)

9. Ensure that school personnel providing services to homeless students receive professional development and other support.

(cf. 4131 - Staff Development)
(cf. 4231 - Staff Development)
(cf. 4331 - Staff Development)

10. Ensure that unaccompanied youth are enrolled in school, have opportunities to meet the same challenging state academic standards established for other students, and are informed of their status as independent students under 20 USC 1087vv and that they may receive assistance from the district liaison to receive verification of their independent student status for purposes of applying for federal student aid pursuant to 20 USC 1090.

11. Coordinate and collaborate with state coordinators and community and school personnel responsible for the provision of education and related services to homeless students, including the provision of comprehensive data to the state coordinator as required by law.

In addition, when notified pursuant to Education Code 48918.1, the district liaison shall assist, facilitate, or represent a homeless student who is undergoing a disciplinary proceeding that could result in his/her expulsion. When notified pursuant to Education Code 48915.5, the district liaison shall participate in an individualized education program team meeting to make a manifestation determination regarding the behavior of a student with a disability.

The Superintendent or designee shall inform homeless children and youth, their parents/guardians, school personnel, service providers, and advocates working with homeless families of the duties of the district's liaison. He/she shall also provide the name and contact information of the district's liaison to the California Department of Education (CDE) for publishing on the CDE's web site. (42 USC 11432)

Enrollment

The district shall make placement decisions for homeless students based on the student's best interest. (42 USC 11432)

In determining the best interest of the student, the district shall consider student-centered factors related to the student's best interest, including factors related to the impact of mobility on achievement, education, health, and safety, giving priority to the request of the student's parent/guardian or, in the case of an unaccompanied youth, the youth. (42 USC 11432)

Such factors may include, but are not limited to, the age of the student, the distance of the commute and the impact it may have on the student's education, personal safety issues, the student's need for special instruction, the length of anticipated stay in the temporary shelter or other temporary location, likely area of future housing, school placement of siblings, and the time remaining in the school year.

However, placement decisions shall not be based on whether a homeless student lives with his/her homeless parent/guardian or has been temporarily placed elsewhere. (42 USC 11432)

In the case of an unaccompanied youth, the liaison shall assist in placement or enrollment decisions, give priority to the views of the student, and provide notice to the student of his/her appeal rights. (42 USC 11432)

In determining a student's best interest, a homeless student shall, to the extent feasible, be placed in his/her school of origin, unless the student's parent/guardian or the unaccompanied youth requests otherwise. (Education Code 48852.7; 42 USC 11432)

Once a placement decision has been made, the principal or designee shall immediately enroll the student in the school of choice. The student shall be enrolled even if he/she: (Education Code 48852.7; 42 USC 11432)

1. Has outstanding fees, fines, textbooks, or other items or monies due to the school last attended

(cf. 5125.2 - Withholding Grades, Diploma or Transcripts)

2. Does not have clothing normally required by the school, such as school uniforms

(cf. 5132 - Dress and Grooming)

3. Is unable to produce records normally required for enrollment, such as previous academic records, proof of residency, and records of immunization and other required health records

(cf. 5111 - Admission)
(cf. 5111.1 - District Residency)
(cf. 5125 - Student Records)
(cf. 5141.26 - Tuberculosis Testing)
(cf. 5141.31 - Immunizations)
(cf. 5141.32 - Health Screening for School Entry)

4. Has missed application or enrollment deadlines during any period of homelessness

The principal or designee shall immediately contact the school last attended by the student to obtain the relevant records. If the student needs to obtain immunizations or does not possess immunization or other required health records, the principal or designee shall refer the parent/guardian to the district liaison for homeless students. The district liaison shall assist the parent/guardian or the student if he/she is an unaccompanied youth, in obtaining the necessary immunizations, screenings, or records for the student. (42 USC 11432)

If the student is placed at a school other than his/her school of origin or the school requested by his/her parent/guardian or an unaccompanied youth, the Superintendent or designee shall provide the parent/guardian or the unaccompanied youth with a written explanation of the decision along with a statement regarding the right to appeal the placement decision. (42 USC 11432)

The student may continue attending his/her school of origin for the duration of the homelessness. (Education Code 48852.7; 42 USC 11432)

To ensure that the homeless student has the benefit of matriculating with his/her peers in accordance with the established feeder patterns, the following shall apply: (Education Code 48852.7; 42 USC 11432)

1. If the student is transitioning between grade levels, he/she shall be allowed to continue in the same attendance area.

2. If the student is transitioning to a middle school or high school, and the school designated for matriculation is in another school district, he/she shall be allowed to continue to the school designated for matriculation in that district.

If the student's status changes before the end of the school year so that he/she is no longer homeless, he/she shall be allowed to stay in the school of origin: (Education Code 48852.7)

1. Through the duration of the school year if he/she is in grades K-8

2. Through graduation if he/she is in high school

Resolving Enrollment Disputes

If a dispute arises over student eligibility, school selection or enrollment in a particular school, the matter shall be referred to the district liaison, who shall carry out the dispute resolution process as expeditiously as possible. (42 USC 11432)

The parent/guardian or unaccompanied youth shall be provided with a written explanation of any decisions related to eligibility, school selection, or enrollment and of the right of the parent/guardian or unaccompanied youth to appeal such decisions. (42 USC 11432)

The parent/guardian shall be provided with a written explanation of the placement decision, including an explanation of the parent/guardian's right to appeal the decision. He/she shall also be referred to the district liaison. (42 USC 11432)

The written explanation shall include:

1. A description of the action proposed or refused by the district

2. An explanation of why the action is proposed or refused

3. A description of any other options the district considered and the reasons that any other options were rejected

4. A description of any other factors relevant to the district's decision and information related to the eligibility or best interest determination including the facts, witnesses, and evidence relied upon and their sources

5. Appropriate timelines to ensure any relevant deadlines are not missed

6. Contact information for the district liaison and state coordinator, and a brief description of their roles

The written explanation shall be complete, as brief as possible, simply stated, and provided in language that the parent/guardian or student can understand.

The district liaison may use an informal process as an alternative to formal dispute resolution procedures, provided that the parents/guardians or unaccompanied youth have access to the more formal process if informal resolution is not successful in resolving the matter.

In working with a student's parents/guardians or unaccompanied youth to resolve an enrollment dispute, the district liaison shall:

1. Inform them that they may provide written and/or oral documentation to support their position

2. Inform them that they may seek the assistance of social services, advocates, and/or service providers in having the dispute resolved

3. Provide them a simple form that they may use and turn in to the school to initiate the dispute resolution process

4. Provide them a copy of the dispute form they submit for their records

5. Provide them the outcome of the dispute for their records

If a parent/guardian or unaccompanied youth disagrees with the liaison's enrollment decision,

he/she may appeal the decision to the Superintendent. The Superintendent shall make a determination within five working days.

If the parent/guardian chooses to appeal the district's placement decision, the district liaison shall forward all written documentation and related paperwork to the homeless liaison at the county office of education.

Pending final resolution of the dispute, including all available appeals, the student shall be immediately enrolled in the school in which enrollment is sought and shall be allowed to attend classes and participate fully in school activities. (42 USC 11432, 11434a)

Transportation

The district shall provide transportation for a homeless student to and from his/her school of origin when the student is residing within the district and the parent/guardian, or the district liaison in the case of an unaccompanied youth, requests that such transportation be provided. If the student moves outside of district boundaries, but continues to attend his/her school of origin within this district, the Superintendent or designee shall consult with the superintendent of the district in which the student is now residing to agree upon a method to apportion the responsibility and costs of the transportation. (42 USC 11432)

(cf. 3250 - Transportation Fees)
(cf. 3541 - Transportation Routes and Services)

The district shall not be obligated to provide transportation to students who continue attending their school of origin after they cease to be homeless, unless the formerly homeless student has an individualized education program that includes transportation as a necessary related service for the student. (Education Code 48852.7)

Transfer of Coursework and Credits

When a homeless student transfers into a district school, the district shall accept and issue full credit for any coursework that the student has satisfactorily completed while attending another public school, a juvenile court school, or a nonpublic, nonsectarian school or agency and shall not require the student to retake the course. (Education Code 51225.2)

If the homeless student did not complete the entire course, he/she shall be issued partial credit for the coursework completed and shall be required to take the portion of the course that he/she did not complete at his/her previous school. However, the district may require the student to retake the portion of the course completed if, in consultation with the holder of educational rights for the student, the district finds that the student is reasonably able to complete the requirements in time to graduate from high school. Whenever partial credit is issued to a homeless student in any particular course, he/she shall be enrolled in the same or equivalent course, if applicable, so that he/she may continue and complete the entire course. (Education Code 51225.2)

Partial credits shall be awarded on the basis of 0.5 credits for every seven class periods attended

per subject. If the school is on a block schedule, each block schedule class period attended shall be equal to two regular class periods per subject. Partial credits and grades earned by a student shall be included on the student's official transcript within two business days of the district's notification of the student's transfer, as required under Education Code 49069.5.

In no event shall the district prevent a homeless student from taking or retaking a course to meet the eligibility requirements for admission to the California State University or the University of California. (Education Code 51225.2)

(cf. 6143 - Courses of Study)

Applicability of Graduation Requirements

To obtain a high school diploma, a homeless student shall complete all courses required by Education Code 51225.3 and fulfill any additional graduation requirements prescribed by the Governing Board.

(cf. 6146.1 - High School Graduation Requirements)

However, when a homeless student who has completed his/her second year of high school transfers into the district from another school district or transfers between high schools within the district, he/she shall be exempted from all district-adopted coursework and other district-established graduation requirements, unless the district makes a finding that the student is reasonably able to complete the additional requirements in time to graduate from high school by the end of his/her fourth year of high school. Within 30 calendar days of the homeless student's transfer, the Superintendent or designee shall notify the student, the person holding the right to make educational decisions for him/her, and the district liaison for homeless students of the availability of the exemption and whether the student qualifies for it. If the Superintendent or designee fails to provide this notification, the student shall be eligible for the exemption once notified, even if the notification occurs after the student is no longer homeless. (Education Code 51225.1)

To determine whether a homeless student is in his/her third or fourth year of high school, the district shall use either the number of credits he/she has earned as of the date of the transfer or the length of his/her school enrollment, whichever qualifies him/her for the exemption. (Education Code 51225.1)

The Superintendent or designee shall notify any homeless student who is granted an exemption and the person holding the right to make educational decisions for him/her how any requirements that are waived will affect the student's ability to gain admission to a postsecondary educational institution and shall provide information about transfer opportunities available through the California Community Colleges. (Education Code 51225.1)

The district shall not require or request a homeless student to transfer schools in order to qualify for an exemption and no request for a transfer solely to qualify for an exemption shall be made by a homeless student, the person holding the right to make educational decisions for the student,

or the district liaison on behalf of the student. (Education Code 51225.1)

If a homeless student is exempted from local graduation requirements, the exemption shall continue to apply after the student is no longer homeless or if he/she transfers to another school or school district. (Education Code 51225.1)

If the Superintendent or designee determines that a homeless student is reasonably able to complete district graduation requirements within his/her fifth year of high school, he/she shall: (Education Code 51225.1)

1. Inform the student and, if under 18 years of age, the person holding the right to make educational decisions for him/her, of the option available to the student to remain in school for a fifth year to complete the district's graduation requirements and how that will affect his/her ability to gain admission to a postsecondary educational institution

2. Provide information to the homeless student about transfer opportunities available through the California Community Colleges

3. Upon agreement with the homeless student or with the person holding the right to make educational decisions for him/her if he/she is under 18 years of age, permit the student to stay in school for a fifth year to complete the district's graduation requirements

Eligibility for Extracurricular Activities

Information regarding the educational rights of homeless students, as specified in Education Code 51225.1 and 51225.2, shall be included in the annual uniform complaint procedures notification distributed to students, parents/guardians, employees, and other interested parties pursuant to 5 CCR 4622. (Education Code 51225.1, 51225.2)

Any complaint that the district has not complied with requirements regarding the education of homeless students, as specified in Education Code 51225.1 or 51225.2, may be filed in accordance with the district's procedures in AR 1312.3 - Uniform Complaint Procedures.

(cf. 1312.3 - Uniform Complaint Procedures)

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **UPDATED BOARD POLICY 6173.1: EDUCATION FOR FOSTER YOUTH**

ATTACHMENTS: **UPDATED BOARD POLICY 6173.1: EDUCATION FOR FOSTER YOUTH**

DISCUSSION:

The following policy has been updated to reflect the need to incorporate the needs of foster youth into the local control and accountability plan, to identify a District Liaison for foster youth, and to provide a positive learning environment free from discrimination and harassment. The current COJUSD BP 6173.1 was last updated on December 14, 2006 with the most recent recommended changes being dated 12/13.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Antonio Quintanilla; Director of Special Services:***

BOARD GOAL:

- [X] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [X] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve Update BP 6173.1: Education for Foster Youth**

PROPOSED ACTION: **APPROVE**

Item #: 12

Cutler-Orosi Joint USD

Board Policy

Education For Foster Youth

BP 6173.1
Instruction

The Board of Education recognizes that foster youth may face significant barriers to achieving academic success due to their family circumstances, disruption to their educational program, and their emotional, social, and other health needs. To enable such students to achieve state and district academic standards, the Superintendent or designee shall provide them with full access to the district's educational program and implement strategies identified as necessary for the improvement of the academic achievement of foster youth in the district's local control and accountability plan (LCAP).

(cf. 0460 - Local Control and Accountability Plan)
(cf. 3100 - Budget)
(cf. 5131.6 - Alcohol and Other Drugs)
(cf. 5147 - Dropout Prevention)
(cf. 6011- Academic Standards)
(cf. 6145 - Extracurricular and Cocurricular Activities)
(cf. 6145.2 - Athletic Competition)
(cf. 6164.2 - Guidance/Counseling Services)
(cf. 6173 - Education for Homeless Children)
(cf. 6179 - Supplemental Instruction)

The Superintendent or designee shall ensure that placement decisions for foster youth are based on the students' best interests as defined in law and administrative regulation. To that end, he/she shall designate a staff person as the district liaison for foster youth to help facilitate the enrollment, placement, and transfer of foster youth.

The Superintendent or designee and district liaison shall ensure that all appropriate staff, including, but not limited to, each principal, school registrar, and attendance clerk, receive training on the enrollment, placement, and transfer of foster youth and other related rights.

(cf. 4131 - Staff Development)
(cf. 4231 - Staff Development)
(cf. 4331 - Staff Development)

The Board of Education desires to provide foster youth with a safe, positive learning environment that is free from discrimination and harassment and that promotes students' self-esteem and academic achievement. The Superintendent or designee shall develop strategies to build a foster youth's feeling of connectedness with his/her school, including, but not limited to, strategies that promote positive discipline and conflict resolution, the development of

resiliency and interpersonal skills, and the involvement of foster parents, group home administrators, and/or other caretakers in school programs and activities.

(cf. 0410 - Nondiscrimination in District Programs and Activities)
(cf. 0450 - Comprehensive Safety Plan)
(cf. 5131 - Conduct)
(cf. 5131.2 - Bullying)
(cf. 5137 - Positive School Climate)
(cf. 5138 - Conflict Resolution/Peer Mediation)
(cf. 5145.3 - Nondiscrimination/Harassment)
(cf. 5145.9 - Hate-Motivated Behavior)
(cf. 6020 - Parent Involvement)

To address the needs of foster youth and help ensure the maximum utilization of available funds, the Superintendent or designee shall collaborate with local agencies and officials including, but not limited to, the county placing agency, social services, probation officers, and juvenile court officers. The Superintendent or designee shall explore the feasibility of entering into agreements with these groups to coordinate services and protect the rights of foster youth.

(cf. 1400 - Relations Between Other Governmental Agencies and the Schools)

At least annually and in accordance with the established timelines, the Superintendent or designee shall report to the Board of Education on the outcomes for foster youth regarding the goals and specific actions identified in the LCAP, including, but not limited to, school attendance, student achievement test results, promotion and retention rates by grade level, graduation rates, and suspension/expulsion rates. As necessary, evaluation data shall be used to determine and recommend revisions to the LCAP for improving or increasing services for foster youth.

(cf. 0500 - Accountability)
(cf. 5123 - Promotion/Acceleration/Retention)
(cf. 5144.1 - Suspension and Expulsion/Due Process)
(cf. 5144.2 - Suspension and Expulsion/Due Process (Students with Disabilities))
(cf. 6162.51 - State Academic Achievement Tests)

Legal Reference:
EDUCATION CODE
32228-32228.5 Student safety and violence prevention
42238.01-42238.07 Local control funding formula
42920-42925 Foster children educational services
48645-48646 Juvenile court schools
48850-48859 Educational placement of students residing in licensed children's institutions
48915.5 Suspension and expulsion; students with disabilities, including foster youth
48918.1 Notice of expulsion hearing for foster youth
49061 Student records

49069.5 Foster care students, transfer of records
49076 Access to student records
51225.1 Exemption from district graduation requirements
51225.2 Pupil in foster care defined; acceptance of coursework, credits, retaking of course
51225.3 High school graduation
52060-52077 Local control and accountability plan
56055 Rights of foster parents in special education
HEALTH AND SAFETY CODE
1522.41 Training and certification of group home administrators
1529.2 Training of licensed foster parents
120341 Foster youth: school placement: immunization records
WELFARE AND INSTITUTIONS CODE
300 Children subject to jurisdiction
309 Investigation and release of child
317 Appointment of legal counsel
361 Limitations on parental or guardian control
366.27 Educational decision by relative providing living arrangements
602 Minors violating law; ward of court
726 Limitations on parental or guardian control
727 Order of care, ward of court
16000-16014 Foster care placement
CODE OF REGULATIONS, TITLE 5
4600-4687 Uniform complaint procedures
UNITED STATES CODE, TITLE 20
1415 Procedural safeguards; placement in alternative educational setting
6311 State plan
UNITED STATES CODE, TITLE 29
794 Rehabilitation Act of 1973, Section 504
UNITED STATES CODE, TITLE 42
670-679b Federal assistance for foster care programs
11431-11435 McKinney-Vento Homeless Assistance Act

Management Resources:
CSBA PUBLICATIONS
Foster Youth: Supports for Success, Governance Brief, May 2016
Our Foster Youth: What School Boards Can Do, May 2016
ALLIANCE FOR CHILDREN'S RIGHTS PUBLICATIONS
Foster Youth Education Toolkit, December 2016
CALIFORNIA CHILD WELFARE COUNCIL
Partial Credit Model Policy and Practice Recommendations
CITIES, COUNTIES AND SCHOOLS PARTNERSHIP PUBLICATIONS
Our Children: Emancipating Foster Youth, A Community Action Guide
U.S. DEPARTMENT OF EDUCATION PUBLICATIONS
Ensuring Educational Stability for Children in Foster Care, Non-Regulatory Guidance, June 2016
WEB SITES

CSBA: http://www.csba.org
Alliance for Children's Rights: http://www.kids-alliance.org
California Child Welfare Council: http://www.chhs.ca.gov/Pages/CAChildWelfareCouncil.aspx
California Department of Education, Foster Youth Services: http://www.cde.ca.gov/ls/pf/fy
California Department of Social Services, Foster Youth Ombudsman Office:
http://www.fosteryouthhelp.ca.gov
California Foster Youth Education Task Force: http://www.cfyetf.org
California Youth Connection: http://www.calyouthconn.org/site/cyc
Cities, Counties and Schools Partnership: http://www.ccspartnership.org
Foster Ed: http://foster-ed.org
National Center for Youth Law: https://youthlaw.org

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **UPDATED ADMINISTRATIVE REGULATION 6173.1: EDUCATION FOR FOSTER YOUTH**

ATTACHMENTS: **UPDATED ADMINISTRATIVE REGULATION 6173.1: EDUCATION FOR FOSTER YOUTH**

DISCUSSION:

The following documentation lists the types of changes and actions to be provided by the Cutler-Orosi Joint Unified School District in accordance to revisions in Foster Youth programs and supports. The current COJUSD AR 6173.1 was last updated on August 9, 2012 with the most recent recommended changes being dated 10/17.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Antonio Quintanilla; Director of Special Services***

BOARD GOAL:

- ☐ **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- ☐ **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- ☐ **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the Updated AR 6173.1.**

PROPOSED ACTION: **APPROVE**

Item #: 13

Cutler-Orosi Joint USD

Administrative Regulation

Education For Foster Youth

AR 6173.1
Instruction

Definitions

Foster youth means a child who has been removed from his/her home pursuant to Welfare and Institutions Code 309, is the subject of a petition filed under Welfare and Institutions Code 300 or 602, or has been removed from his/her home and is the subject of a petition filed under Welfare and Institutions Code 300 or 602 or is a nonminor who is under the transition jurisdiction of a juvenile court, as described in Welfare and Institutions Code 450, and satisfies the criteria specified in Education Code 42238.01. (Education Code 42238.01, 48853.5)

Person holding the right to make educational decisions means a responsible adult appointed by a court pursuant to Welfare and Institutions Code 361 or 727.

School of origin means the school that the foster youth attended when permanently housed or the school in which he/she was last enrolled. If the school the foster youth attended when permanently housed is different from the school in which he/she was last enrolled, or if there is another school that the foster youth attended within the preceding 15 months and with which the youth is connected, the district liaison for foster youth shall determine which school is the school of origin. This determination shall be made in consultation with and with the agreement of the foster youth and the person holding the right to make educational decisions for the youth, and shall be based on the best interests of the foster youth. (Education Code 48853.5)

Best interests means that, in making educational and school placement decisions for a foster youth, consideration is given to, among other factors, the proximity to the school at the time of placement, appropriateness of the educational setting, educational stability, the opportunity to be educated in the least restrictive educational setting necessary to achieve academic progress, and the foster youth's access to academic resources, services, and extracurricular and enrichment activities that are available to all district students. (Education Code 48850, 48853; 20 USC 6311)

District Liaison

The Superintendent designates the following position as the district's liaison for foster youth: (Education Code 48853.5)

Director of Special Services
12623 Ave 416
Orosi, CA 93647
(559) 528-4075

(cf. 6173 - Education for Homeless Children)

The liaison for foster youth shall:

1. Ensure and facilitate the proper educational placement, enrollment in school, and checkout from school of students in foster care (Education Code 48853.5)

2. Ensure proper transfer of credits, records, and grades when students in foster care transfer from one school to another or from one district to another (Education Code 48853.5, 48645.5)

When a student in foster care is enrolling in a district school, the liaison shall contact the school last attended by the student to obtain, within two business days, all academic and other records. When a foster youth is transferring to a new school, the liaison shall provide the student's records to the new school within two business days of receiving the new school's request. (Education Code 48853.5)

(cf. 5125 - Student Records)
(cf. 6146.3 - Reciprocity of Academic Credit)

3. When required by law, notify the foster youth's attorney and the representative of the appropriate county child welfare agency when the foster youth is undergoing any expulsion or other disciplinary proceeding, including a manifestation determination prior to a change in the foster youth's placement when he/she is a student with a disability. (Education Code 48853.5, 48911, 48915.5, 48918.1)

4. As needed, make appropriate referrals to ensure that students in foster care receive necessary special education services and services under Section 504 of the federal Rehabilitation Act of 1973

(cf. 6164.4 - Identification and Evaluation of Individuals for Special Education)
(cf. 6164.6 - Identification and Education Under Section 504)

5. As needed, ensure that students in foster care receive appropriate school-based services, such as counseling and health services, supplemental instruction, and after-school services.

(cf. 5141.6 - School Health Services)
(cf. 5148.2 - Before/After School Programs)
(cf. 6164.2 - Guidance/Counseling Services)
(cf. 6172 - Gifted and Talented Student Program)
(cf. 6174 - Education for English Language Learners)
(cf. 6177 - Summer School)
(cf. 6179 - Supplemental Instruction)

6. Develop protocols and procedures for creating awareness for district staff, including principals, school registrars, and attendance clerks, of the requirements for the proper enrollment, placement, and transfer of foster youth

(cf. 4131 - Staff Development)
(cf. 4231 - Staff Development)
(cf. 4331 - Staff Development)

7. Collaborate with the county office of education, county placing agency, social services, probation officers, juvenile court officers, nonprofit organizations, and advocates to help coordinate services for the district's foster youth

(cf. 1400 - Relations Between Other Governmental Agencies and the Schools)
(cf. 5113.1 - Chronic Absence and Truancy)

8. Monitor the educational progress of foster youth and provide reports to the Superintendent or designee and the Board of Education based on indicators identified in the district's local control and accountability plan.

(cf. 0460 - Local Control and Accountability Plan)

The Superintendent or designee shall regularly monitor the caseload of the liaison, as well as his/her additional duties outside of the foster youth program, to determine whether adequate time and resources are available to meet the needs of foster youth in the district.

Enrollment

A student placed in a licensed children's institution or foster family home within the district shall attend programs operated by the district unless one of the following circumstances applies: (Education Code 48853, 48853.5)

1. The student has an individualized education program requiring placement in a nonpublic, nonsectarian school or agency, or in another local educational agency.

(cf. 6159 - Individualized Education Program)
(cf. 6159.2 - Nonpublic, Nonsectarian School and Agency Services for Special Education)

2. The parent/guardian or other person holding the right to make educational decisions for the student determines that it is in the best interest of the student to be placed in another educational program submits a written statement to the district indicating that determination and that he/she is aware of the following:

a. The student has a right to attend a regular public school in the least restrictive environment.

b. The alternate education program is a special education program, if applicable.

c. The decision to unilaterally remove the student from the district school and to place him/her in an alternate education program may not be financed by the district.

d. Any attempt to seek reimbursement for the alternate education program may be at the expense of the parent/guardian or other person holding the right to make educational decisions for the student.

(cf. 5116.1 - Intradistrict Open Enrollment)
(cf. 5117 - Interdistrict Attendance)
(cf. 6159.3 - Appointment of Surrogate Parent for Special Education Students)

3. At the initial placement or any subsequent change in placement, the student exercises his/her right to continue in his/her school of origin, as defined above.

a. The student may continue in the school of origin for the duration of the court's jurisdiction.

b. If the court's jurisdiction over a grade K-8 student is terminated prior to the end of a school year, the student may continue in his/her school of origin for the remainder of the school year.

c. If the court's jurisdiction is terminated while the student is in high school, the student may continue in his/her school of origin until he/she graduates.

d. If the student is transitioning between school grade levels, he/she shall be allowed to continue in the district of origin in the same attendance area to provide him/her the benefit of matriculating with his/her peers in accordance with the established feeder patterns of school districts. A student who is transitioning to a middle school or high school shall be allowed to enroll in the school designated for matriculation in another school district.

The district liaison may, in consultation with and with the agreement of the foster youth and the person holding the right to make educational decisions for the youth, recommend that the youth's right to attend the school of origin be waived and he/she be enrolled in any school that students living in the attendance area in which the foster youth resides are eligible to attend. All decisions

shall be made in accordance with the foster youth's best interests. (Education Code 48853.5)

Prior to making any recommendation to move a foster youth from his/her school of origin, the liaison shall provide the youth and the person holding the right to make educational decisions for the youth with a written explanation of the basis for the recommendation and how this recommendation serves the youth's best interests. (Education Code 48853.5)

The role of the liaison shall be advisory with respect to placement decisions and determination of the school of origin. (Education Code 48853.5)

If the liaison, in consultation with the foster youth and the person holding the right to make educational decisions for the foster youth, agrees that the best interests of the youth would be served by his/her transfer to a school other than the school of origin, the principal or designee of the new school shall immediately enroll the foster youth. The foster youth shall be immediately enrolled even if he/she: (Education Code 48853.5)

1. Has outstanding fees, fines, textbooks, or other items or monies due to the school last attended

(cf. 5125.2 - Withholding Grades, Diploma or Transcripts)

2. Does not have clothing normally required by the school, such as school uniforms

(cf. 5132 - Dress and Grooming)

3. Is unable to produce records normally required for enrollment, such as previous academic records, proof of residency, and medical records, including, but not limited to, immunization records or other documentation.

(cf. 5111.1 - District Residency)
(cf. 5141.26 - Tuberculosis Testing)
(cf. 5141.31 - Immunizations)
(cf. 5141.32 - Health Screening for School Entry)

If the foster youth or a person holding the right to make educational decisions for the foster youth disagrees with the liaison's enrollment recommendation, he/she may appeal to the Superintendent. The Superintendent shall make a determination within 30 calendar days of receipt of the appeal. Within 30 calendar days of receipt of the Superintendent's decision, the parent/guardian or foster youth may appeal that decision to the Board. The Board shall consider the issue at its next regularly scheduled meeting. The Board's decision shall be final.

(cf. 9320 - Meetings and Notices)

If any dispute arises regarding the request of a foster youth to remain in the school of origin, the youth has the right to remain in the school of origin pending resolution of the dispute. (Education Code 48853.5)

Transportation

The Superintendent or designee shall collaborate with the local child welfare agency to determine how transportation will be provided, arranged, and funded in a cost-effective manner to enable foster youth to remain in their school of origin, for the duration of their time in foster care, when it is in their best interest to do so. Such transportation costs may be paid by either the child welfare agency or the district, or shared by both. (20 USC 6312)

(cf. 3540 - Transportation)
(cf. 3541 - Transportation Routes and Services)

Effect of Absences on Grades

The grades of a student in foster care shall not be lowered for any absence from school that is due to either of the following circumstances: (Education Code 49069.5)

1. A decision by a court or placement agency to change the student's placement, in which case the student's grades shall be calculated as of the date he/she left school

2. A verified court appearance or related court-ordered activity

(cf. 5121 - Grades/Evaluation of Student Achievement)

Transfer of Coursework and Applicability of Graduation Requirements

When a foster youth transfers into a district school, the district shall accept and issue full credit for any coursework that the foster youth has satisfactorily completed while attending another public school, a juvenile court school, or a nonpublic, nonsectarian school or agency and shall not require the foster youth to retake the course. (Education Code 51225.2)

If the foster youth did not complete the entire course, he/she shall be issued partial credit for the coursework completed and shall be required to take the portion of the course that he/she did not complete at his/her previous school. However, the district may require the foster youth to retake the portion of the course completed if, in consultation with the holder of educational rights for the foster youth, the district finds that the foster youth is reasonably able to complete the requirements in time to graduate from high school. Whenever partial credit is issued to a foster youth in any particular course, he/she shall be enrolled in the same or equivalent course, if applicable, so that he/she may continue and complete the entire course. (Education Code 51225.2)

Partial credits shall be awarded on the basis of 0.5 credits for every seven class periods attended per subject. If the school is on a block schedule, each block schedule class period attended shall be equal to two regular class periods per subject. Partial credits and grades earned by a student shall be included on the student's official transcript within two business days of the district's notification of the student's transfer, as required under Education Code 49069.5.

In no event shall the district prevent a foster youth from taking or retaking a course to meet the eligibility requirements for admission to the California State University or the University of California. (Education Code 51225.2)

Applicability of Graduation Requirements

To obtain a high school diploma, a foster youth shall complete all courses required by Education Code 51225.3 and fulfill any additional graduation requirements prescribed by the Board.

(cf. 6146.1 - High School Graduation Requirements)

However, when a foster youth who has completed his/her second year of high school transfers into the district from another school district or transfers between high schools within the district, he/she shall be exempted from all district-adopted coursework and other district-established graduation requirements, unless the district makes a finding that the student is reasonably able to complete the additional requirements in time to graduate from high school by the end of his/her fourth year of high school. Within 30 calendar days of the foster youth's transfer, the Superintendent or designee shall notify the foster youth, the person holding the right to make educational decisions for him/her, and the foster youth's social worker of the availability of the exemption and whether the foster youth qualifies for it. If the Superintendent or designee fails to provide this notification, the student shall be eligible for the exemption once notified, even if the notification occurs after the student is no longer a foster youth. (Education Code 51225.1)

To determine whether a foster youth is in his/her third or fourth year of high school, the district shall use either the number of credits the foster youth has earned as of the date of the transfer or the length of his/her school enrollment, whichever qualifies him/her for the exemption. (Education Code 51225.1)

The Superintendent or designee shall notify any foster youth who is granted an exemption and the person holding the right to make educational decisions for him/her how any requirements that are waived will affect the foster youth's ability to gain admission to a postsecondary educational institution and shall provide information about transfer opportunities available through the California Community Colleges. (Education Code 51225.1)

The district shall not require or request a foster youth to transfer schools in order to qualify for an exemption and no request for a transfer solely to qualify for an exemption shall be made by a foster youth or any person acting on behalf of a foster youth. (Education Code 51225.1)

If a foster youth is exempted from local graduation requirements, the exemption shall continue to apply after the termination of the court's jurisdiction over the student while he/she is still enrolled in school or if he/she transfers to another school or school district. (Education Code 51225.1)

Upon making a finding that a foster youth is reasonably able to complete district graduation requirements within his/her fifth year of high school, the Superintendent or designee shall:

(Education Code 51225.1)

1. Inform the foster youth and the person holding the right to make educational decisions for him/her of the foster youth's option to remain in school for a fifth year to complete the district's graduation requirements and how that will affect his/her ability to gain admission to a postsecondary educational institution

2. Provide information to the foster youth about transfer opportunities available through the California Community Colleges

3. Upon agreement with the foster youth or, if he/she is under 18 years of age, the person holding the right to make educational decisions for him/her, permit the foster youth to stay in school for a fifth year to complete the district's graduation requirements

Eligibility for Extracurricular Activities

A foster youth whose residence changes pursuant to a court order or decision of a child welfare worker shall be immediately deemed to meet all residency requirements for participation in interscholastic sports or other extracurricular activities. (Education Code 48850)

(cf. 6145 - Extracurricular and Cocurricular Activities)
(cf. 6145.2 - Athletic Competition)

Notification and Complaints

Information regarding the educational rights of foster youth shall be included in the annual uniform complaint procedures notification distributed to students, parents/guardians, employees, and other interested parties pursuant to 5 CCR 4622. (Education Code 48853, 48853.5, 49069.5, 51225.1, 51225.2)

(cf. 5145.6 - Parental Notifications)

Any complaint alleging that the district has not complied with requirements regarding the education of foster youth may be filed in accordance with the district's procedures in AR 1312.3 - Uniform Complaint Procedures. If the district finds merit in a complaint, the district shall provide a remedy to the affected student. A complainant not satisfied with the district's decision may appeal the decision to the California Department of Education (CDE) and shall receive a written decision regarding the appeal within 60 days of CDE's receipt of the appeal. If the CDE finds merit in an appeal, the district shall provide a remedy to the affected student. (Education Code 48853, 48853.5, 49069.5, 51225.1, 51225.2)

(cf. 1312.3 - Uniform Complaint Procedures)

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **JOINT POWERS AGREEMENT FOR REGIONAL OCCUPATIONAL PROGRAM**

ATTACHMENTS: **JOINT POWERS AGREEMENT**

DISCUSSION:

The District requests approval to continue the joint powers agreement for Valley Regional Occupational Program. The agreement shall provide career and technical training in support and augmentation of CTE programs offered by high schools, adult schools and community colleges serving the member districts.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Yolanda Valdez, Superintendent***

BOARD GOAL:

- ☐ **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- ☐ **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- ☐ **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends the Board approve the Joint Powers Agreement with Valley ROP.**

PROPOSED ACTION: **APPROVE**

Item #: 86

JOINT POWERS AGREEMENT
FOR A REGIONAL OCCUPATIONAL PROGRAM

PARTIES

This Joint Powers Agreement is entered into pursuant to the provisions of the Joint Exercise of Powers Act, Title I, Division 7, Chapter 5, Article 1, (section 6500, et seq.) of the California Government Code, and section 52300 et seq. of the Education Code relating to Regional Occupational Centers/Programs among the following public educational agencies which are signatory hereto for the purpose of creating the Valley Regional Occupational Program. The public educational agencies are referred to collectively herein as "member districts" or "parties" and include:

Cutler-Orosi Joint Unified School District
Dinuba Unified School District
Kings Canyon Unified School District
Kingsburg Joint Union High School District
Parlier Unified School District
Sanger Unified School District
Selma Unified School District

WITNESSETH

WHEREAS, it is to the mutual benefit of the parties herein subscribed and in the best public interest of said parties to join together to establish a joint powers agency to accomplish the purposes hereinafter set forth; and

WHEREAS, Kings Canyon Unified School District, Kingsburg Joint Union High School District, Parlier Unified School District, Sanger Unified School District and Selma Unified School District (hereinafter referred to as the "founding districts") entered into a joint exercise of powers agreement for a regional occupational program on May 15, 1971;

WHEREAS, the founding districts revised and updated the original Joint Powers Agreement on or about March 1, 2014 and on or about July 1, 2015; and on or about July 1, 2019;

WHEREAS, the founding districts desire, by this Joint Powers Agreement, to rescind said prior joint exercise of powers agreement, and establish a new Joint Powers Agreement (hereinafter "Agreement"); and

WHEREAS, the parties hereto intend to create and establish a public entity separate from the parties to this Agreement to be known as the "Valley Regional Occupational Program"; and

WHEREAS, it is not the purpose of the Valley Regional Occupational Program to provide a separate school of any kind, and in accordance with that purpose, the parties intend that Valley Regional Occupational Program shall not issue diplomas of any kind, but shall provide career and technical training in support and augmentation of programs offered by high schools, adult schools, and community colleges that serve the member districts.

NOW, THEREFORE, the parties hereto mutually agree as follows:

ESTABLISHMENT OF JOINT POWERS ENTITY

1. Creation of a Joint Powers Entity:

   A joint powers entity, separate and apart from the public educational agencies signatory hereto, shall be and is hereby created and shall hereafter be designated as the Valley Regional Occupational Program (hereinafter referred to as "Valley ROP").

2. Purpose of the Program:

   Valley ROP is established for the purposes of fulfilling this Agreement pursuant to the joint exercise of powers provisions of the Government Code, and for providing services necessary and appropriate for the establishment, operation, and maintenance of career and technical training programs for the parties hereto. The sole purpose of Valley ROP is to serve the member districts in the delivery of career and technical training programs.

   Although Valley ROP is a separate legal entity, it is created by the member districts for the purpose of providing a regional occupational program which extends, supports, and enhances the member districts' current career and technical training programs. Although legally autonomous, Valley ROP is not coequal to the member districts.

3. Terms of Agreement:

   This Agreement shall become effective as of its date of execution by all parties or on July 1, 2019 and shall continue in full force until lawfully terminated as provided herein.

ORGANIZATION

4. Valley ROP Board of Management:

   A. Members of the Valley ROP Board of Management

      Valley ROP shall be governed by the Valley ROP Board of Management which shall be comprised of one representative from the Governing Board of each of the member districts. The Governing Board of each member district shall elect one representative from its Board to serve on the Valley ROP Board of Management and to represent that member district on matters brought before

the Valley ROP Board of Management. The elected representative shall serve at the pleasure of the member district Governing Board electing the representative.

The Governing Board of each member district shall also elect another representative from its Governing Board as an alternate, with full voting rights on the Valley ROP Board of Management, whenever serving during the absence of the regularly elected representative to the Valley ROP Board of Management.

The Governing Board of each member district shall appoint the member district Superintendent or his/her designee to the Valley ROP Board of Management. Each member of the Valley ROP Board of Management shall be responsible to that member district's Governing Board.

B. Term of Office

The term of each member to the Valley ROP Board of Management shall be at the pleasure of the Governing Board of the member district which elected that member. The term of office shall commence no later than January 1st for all Board of Management members. A member district Governing Board may re-elect the same Valley ROP Board of Management member for an unlimited number of terms.

C. Valley ROP Board of Management Officers

The Valley ROP Board of Management shall elect a Board President and a Vice-President in December or January of each year during the term of this Agreement. An executed copy of this Agreement shall be furnished to said officials. They shall perform the duties specified in Government Code sections 6505 and 6505.5 and all other duties required by law and this Agreement.

The Valley ROP Board of Management may elect other officers, as it may deem necessary or appropriate. The Valley ROP Superintendent shall be appointed Executive Secretary to the Valley ROP Board of Management.

Rotation order of Officers is as follows:
Cutler/Orosi, Dinuba, Kings Canyon, Kingsburg, Selma, Parlier, Sanger

D. Voting Powers

Each member district shall have two votes on the Valley ROP Board of Management consisting of the representative from the member district's Governing Board or alternate representative and the member district's Superintendent or designee.

The alternate representative may vote at meetings of the Valley ROP Board of Management in the absence of the member district's representative. In the event that both the representative and the alternate representative are absent, an official alternate selected by the member district's Governing Board may serve as the representative with all normal voting privileges.

At least eight of the representatives and/or alternate representatives and/or Superintendents or designees must be present at a meeting of the Valley ROP Board of Management to constitute a quorum for transacting business.
A majority vote of the Valley ROP Board of Management shall be required to transact the business of the Valley ROP Board of Management.

Should additional member districts become parties to this Agreement, the majority vote and quorum determination shall be adjusted accordingly by the Valley ROP Board of Management.

At the end of the first year of operation, the voting procedure will be reviewed by the member districts and in the future may, at the discretion of the Valley ROP Board of Management, be reviewed annually.

E. Duties and Powers

The duties and powers of the Valley ROP Board of Management shall only be those specified in this Agreement and may be exercised only in the manner and methods provided in this Agreement.

The Valley ROP Board of Management shall have the power and authority:

i. To adopt annually a budget in accordance with Education Code requirements. Copies of proposed budgets shall be given to each member district prior to adoption. Copies of final budgets shall be given to each member district when adopted.

ii. To receive, accept, expend, or disburse funds by contract or otherwise for purposes consistent with the provisions of this Agreement and shall maintain at all times a complete and accurate system of accounting for said funds.

iii. To purchase, subject to the prior approval of all member districts' Governing Boards, any real property and to construct, purchase, or lease buildings.

iv. To purchase or lease furniture, equipment, fixtures, and other property.

v. To contract with any member district or Fresno County Superintendent of Schools for fiscal accounting services, payroll, credentials, reports, records, and other functions.

vi. To contract for specialized services of fiscal, legal, engineering, economic, administrative, and other services.

vii. To appoint and employ both classified and certificated employees; to determine the number of positions; to fix and pay the compensation of these employees; to establish personnel policies, evaluation processes, procedures, salary schedules, benefits; and other matters related to the Valley ROP's employees.

viii. To establish all other related policies to assure Valley ROP can function in matters of educational and business necessity.

ix. To provide transportation to pupils attending Valley ROP programs, where appropriate.

x. To initiate or defend legal actions on behalf of Valley ROP.

xi. To accept and expend grants from the Federal Government, State of California, or other public or private sources for the purposes of this Agreement.

xii. Provide a curriculum which includes skills training in occupational fields having current and future needs for such training.

xiii. The Valley ROP Board of Management may provide day (including Saturday) and evening, full-time and part-time career technical education programs for minors and adults throughout the calendar year. It shall be the goal of Valley ROP to offer as many career technical education opportunities to students as possible, whether that be during the regular academic school year/day or occurring after the normal school day and during weekends and summer months when typical schools may not be operating.

xiv. To perform such other functions as may be necessary or appropriate to carry out this Agreement, so long as such other functions so performed are not prohibited by any provisions of law.

xv. The Valley ROP Board of Management may delegate to the Valley ROP Superintendent responsibilities and duties that it determines necessary

for the administration, coordination and supervision of Valley ROP maintained hereunder.

F. Power and Authority Limitations

The Valley ROP Board of Management may amend, modify, add to, or delete from this Joint Powers Agreement or to engage in any action regarding a suggested or proposed amendment, modification, addition, or deletion to this Agreement upon approval by the member districts' Governing Boards.

Any question or disagreement regarding the interpretation of this Agreement shall be referred to the member districts' Governing Boards for resolution.

G. Meetings

The Valley ROP Board of Management shall meet regularly, but not fewer than four (4) times a year, to conduct Valley ROP's business. Any additional meeting of the Valley ROP Board of Management may be called by the President of the Valley ROP Governing Board or upon agreement by a majority of the members of the Valley ROP Board of Management.

All meetings of the Valley ROP Board of Management shall be called, held and conducted in accordance with the terms and provisions of the Ralph M. Brown Act, Section 54950 et seq., of the Government Code, as said Act may be modified by subsequent legislation, and as the same may be augmented by rules of the Valley ROP Board of Management not inconsistent therewith.

The date, time, and place for each of the regular meetings shall be fixed annually by the Valley ROP Board of Management.

The Valley ROP Board of Management shall keep the minutes of its meetings and shall promptly transmit to the Governing Board of each member district copies of the minutes when formally adopted by the Valley ROP Board of Management at a subsequent meeting.

5. Superintendent:

The Valley ROP's chief administrator, hereinafter designated as the "Superintendent" is responsible for managing, coordinating, and administering the Valley ROP's delivery of career and technical training programs and serving the member districts. The Superintendent works cooperatively with the staff of member districts, school sites and district offices in providing a regional occupational program which responds to the career and technical training needs of the member districts.

The specific duties of this position are further identified in the job description on file in the Valley ROP Personnel Department.

Whenever an opening exists for the Superintendent position, the Valley ROP Board of Management shall select the Superintendent. The Valley ROP Board of Management must agree, by majority vote, on a candidate to be selected for the Superintendent position.

The Superintendent shall attend all meetings of the Valley ROP Board of Management, except in closed session related to the Superintendent's contract or performance. The Superintendent shall be evaluated in writing annually by the Valley ROP Board of Management.

OPERATIONS

6. Employees:

   A. The Valley ROP Board of Management may appoint and employ employees to operate, maintain, conduct, and teach Valley ROP classes maintained within the territorial boundaries of each member district. Certificated employees of the Valley ROP shall be employed pursuant to Education Code sections 44910 and 44954. Classified employees of the Valley ROP shall be employed pursuant to Education Code section 45100 et seq. The Valley ROP Board of Management shall fix and pay the compensation of employees.

   B. All Valley ROP employees hired shall be employees of the Valley ROP unless other arrangements are mutually agreed upon by the Valley ROP Board of Management, the employee and the appropriate member district. Such arrangements shall be on a year-to-year basis with no obligation on the part of any party to continue the arrangement for subsequent years.

   C. Subject to the approval of the Valley ROP Board of Management, instructors currently employed by member districts on the effective date of this Agreement may provide instruction at any location in the Valley ROP attendance area. Such salary expenses incurred shall be reimbursed from funds of the Valley ROP. Instructors employed by member districts who provide such instruction with the approval of the Valley ROP Board of Management shall not be deemed employees of Valley ROP.

   D. Should occupational projections, student enrollment, or budget reductions require a reduction in instruction or services for the subsequent school year, the Valley ROP Board of Management shall notify the affected member district of the reduction in programs or services by January 15th of the current school year.

E. Subject to mutual agreement between the Valley ROP Superintendent and a member district's administration, each member district may operate Valley ROP programs on its properties at various site locations. Member districts have an option of using Valley ROP personnel to teach a program or to use district staff.

F. At the request of the member districts Valley ROP shall hire, supervise and evaluate credentialed teachers, and classified staff for the districts. Terms of the agreement are detailed in the TEACHER SERVICES AGREEMENT document signed by the Valley ROP Superintendent and member district Superintendent.

G. If additional funds are required from individual districts beyond the base funding allocation, the Valley ROP Board of Management shall notify the appropriate participating district as to the amount of additional support money required to meet the anticipated costs of educating students attending Valley ROP's classes in that district. If the additional support money is approved by the participating school district, the participating school district shall authorize the payment of additional funds to Valley ROP. If the additional funding request is denied, it may be necessary for Valley ROP to reduce offerings in that district.

   Beginning in the 2019-2020 school year, if additional funding for cost-of-living increases to Valley ROP base funding allocation is requested, the request will be subject to the approval of the Valley ROP Board of Management and the approval of all governing boards of the seven participating districts.

H. All Valley ROP funding (including but not limited to grant and contract funding) that is either distributed directly to or received directly by Valley ROP is the property of the JPA and is not the property of the individual member districts. These funds include but are not limited to, Adult Education Block Grant, CalWORKS, CTE Teach, Career Technical Education Incentive Grant, Dual Enrollment and Strong WorkForce funds.

7. Fiscal Matters:

   A. Funding

   The funding for the Valley ROP is through the California Budget Act of 2013-14. The Valley ROP shall receive annual apportionment, Lottery, Excess Property Taxes, etc., entitled to the Valley ROP from each of the original member districts of Kings Canyon USD, Kingsburg JUHSD, Parlier USD, Sanger USD and Selma USD for the established staffing and instructional materials provided by Valley ROP. Dinuba USD and Cutler/Orosi USD will provide 21% of the 2013-14 Regional Occupational Center and Programs apportionment for administrative oversight of the CTE ROP classes. These funds will be transferred to the Valley ROP in the most expedient manner, subject to approval by the member district and the Fresno County Office of Education. One-time funds or any other additional funds

beyond the above-mentioned funds that are identified for Valley ROP programs shall be distributed directly to the Valley ROP.

B. Notification of Potential Reductions

Should the annual Valley ROP student enrollment projections or the Valley ROP's budget require a reduction in programs or services for a subsequent school year as determined by the Valley ROP Board of Management, the Valley ROP Board of Management shall notify all member districts by May 15th of the current year that a reduction in programs and/or services will be required in the subsequent school year.

The Valley ROP Superintendent, working closely with the member district Superintendents, shall develop recommendations for the Valley ROP Board of Management as to which programs or services should be reduced. The Valley ROP Board of Management shall make the final determination as to which programs and/or services shall be reduced.

C. Annual Operating Funds

Valley ROP established and maintained hereunder shall receive the regional occupational program revenue limit from each member district as designated in the Agreement. All regional occupational program revenue limit funds will be electronically transferred from the County Treasurer. Payments to the member districts for the regional occupation programs provided by the member districts shall be paid by Valley ROP to the member districts on a monthly basis based upon when the regional occupational program revenue is allocated by the State of California.

The fiscal year hereunder shall commence on July 1st and end on June 30th of each year during the term of this Agreement.

D. Debts

The Valley ROP Board of Management shall have no power or authority to incur any obligations or debt in excess of the funds which are appropriated to Valley ROP. All debts, liabilities, and obligations of the Valley ROP Board of Management shall be debts, liabilities, and obligations of Valley ROP and shall not be in any way debts, liabilities, or obligations of any member district or combination of member districts.

E. Accounts and Records

Valley ROP is strictly accountable for all funds received and disbursed by it, and, to that end, shall establish and maintain such funds and accounts as may be required by good accounting practice or by provision of law or any resolution of Valley ROP. Books and records of Valley ROP shall be open to inspection at all reasonable times by representatives of the member districts.

F. Audit

The fiscal transactions of Valley ROP shall be audited annually by a firm of licensed certified public accountants to be selected by the Valley ROP Board of Management and paid by Valley ROP. Audits shall be conducted in line with the requirements of the State of California.

8. Insurance:

Valley ROP shall provide and maintain a comprehensive liability insurance policy and/or evidence of self-insurance which shall include general liability, acts or omissions arising out of negligence, bodily injury, death, injury to person, automobile liability, and property damage. This insurance policy shall name the Valley ROP and its employees, agents, officers, Governing Board members of the member districts, Valley ROP Board of Management members, and the member district Superintendents as the insured. Valley ROP shall provide and maintain in full force and effect a workers' compensation insurance and employer's liability policy.

9. Admission of Students:

Any person or student who resides in any of the member districts, who can profit from the instruction, and who is otherwise eligible to attend a high school or adult school may attend any Valley ROP program maintained hereunder.

After consultation with the Valley ROP Board of Management, the Valley ROP Superintendent may determine priorities for student admissions. A student may be admitted on a part-time or full-time basis. After consultation with the Valley ROP Board of Management, the Valley ROP Superintendent also may admit students residing in school districts other than the member districts.

MISCELLANEOUS PROVISIONS

10. Liability:

Pursuant to the provisions of Government Code section 895, et seq., the public school districts entering into this Agreement are jointly and severally liable upon any liability

which is imposed by any law upon any one of the parties or upon Valley ROP for injury caused by a negligent or wrongful act or omission occurring in the performance of this Agreement. If a party to this Agreement is held liable upon any judgment for damages caused by a wrongful act or omission occurring in the performance of this Agreement and makes payment in excess of its pro rata share on such judgment, such party or Valley ROP is entitled to contribution from each of the other parties to this Agreement. For purposes of this section, a party's pro rata share shall be determined in the same manner as set forth in section 14, Disposition of Property and Funds.

11. Indemnification:

Valley ROP, a joint powers entity separate and apart from the member districts, shall protect, indemnify, defend and hold harmless member school districts, their Governing Boards, officers, agents, representatives, and employees, as the same may be constituted now and from time to time hereafter, from and against any and all claims, suits, actions, liability, losses or damages or any expenses (including attorney's fees and court costs) for loss of life or bodily or personal injury to any person, damage or loss resulting, in any manner solely or partially, from any cause or condition or property, equipment, vehicles and/or facilities under the control of Valley ROP, or resulting from the use of such property, equipment, vehicles and/or facilities or from the negligent acts or omissions of Valley ROP's officers, employees, agents, contractors, or invitees arising out of or in connection with the Valley ROP. This obligation shall survive the expiration and/or termination of this Agreement.

12. Disputes:

Should a dispute arise from a member district(s) regarding this Agreement or regarding procedures, interpretation of, operations, changes, or policy, the Valley ROP Superintendent shall attempt to resolve the matter with the member district(s). If a resolution is not reached through the Superintendent, the dispute may be forwarded by the party raising the matter to the Valley ROP Board of Management for resolution. The Valley ROP Board of Management' decision will be final.

13. Withdrawal and/or Termination:

Any member district may withdraw from this Agreement at the end of any fiscal year, provided that written notice of intention to withdraw has been served upon the Valley ROP Board of Management and other member districts no later than twelve (12) months prior to the date of withdrawal, and provided further that at such time, the withdrawing member district has either discharged or arranged to discharge all pending obligations under this Agreement. Notice of withdrawal must include written authorization from the member district's Governing Board. If any party decides to withdraw, this Agreement shall remain in full force and effect between or amongst the remaining member districts.

This Agreement shall be terminated whenever six of the current member districts withdraw, leaving only one member district or whenever at least six of the member districts take action to terminate the Agreement. Should additional districts be added to this Agreement, the Valley ROP Board of Management shall determine, at the time of the addition, the number of member districts required to terminate this Agreement.

Following termination, Valley ROP and this Agreement shall continue to exist for the purpose of disposing of liabilities, distributing property and/or other assets, and all other functions necessary to conclude the business of Valley ROP.

14. Disposition of Property and Funds:

In the event of the dissolution of Valley ROP or other final termination of this Agreement by all member districts then a party hereto, any property interest remaining in the Valley ROP following a discharge of all obligations shall be disposed of as the Valley ROP Board of Management shall then determine with the objective of returning to each member district, which is then a party to this Agreement, a proportionate return on the contributions made to such properties by such parties.

In the event a member district withdraws from this Agreement and desires to keep, maintain and operate any equipment or structure provided by Valley ROP, the Valley ROP Board of Management shall determine the reasonable value of such equipment or structure and shall fix the amount of reimbursement to be paid by the withdrawing member district, subject to such offset or adjustment to be credited for the member district's share of the original acquisition cost of such equipment or structure as the Valley ROP Board of Management shall determine to be fair and reasonable.

At such time, the Valley ROP Board of Management shall decide whether it wishes to purchase any or all of the withdrawing member district's percentage share of the total tangible assets of the Valley ROP. If the Valley ROP Board of Management so decides, it shall pay the withdrawing member district's percentage share. If the Valley ROP Board of Management does not elect to purchase the withdrawing member district's percentage share of the total tangible assets, the withdrawing member district may accept tangible assets in any amount equal to its percentage share.

In the event a class is terminated in a given member district and there is no further need for the equipment in other programs of the Valley ROP, the member district in which the equipment is located shall be notified in writing. If the said member district desires to keep, maintain and operate any equipment or structure provided by the Valley ROP, the Valley ROP Board of Management shall determine the reasonable value of such equipment or structure and shall fix the amount of reimbursement to be credited for the member district's share of the original acquisition cost of such equipment or

structures as the Valley ROP Board of Management shall determine to be fair and reasonable.

In the event of termination of this Agreement, any remaining property acquired by the Valley ROP, following a discharge of all obligations, shall be disposed of in the same manner as for a withdrawing member district, with the objective of returning to each member district any property or surplus money on hand in proportion to the total generated Average Daily Attendance from the date each member district became a member of the Joint Powers Agreement until the last day of the last fiscal year prior to termination.

Equipment and supplies of the Valley ROP will be disposed of as described above. If a withdrawing member district disagrees with the determination of the current fair value of properties, as determined by the remaining members of the Valley ROP Board of Management, the current fair value of the Valley ROP's properties shall be determined by an independent appraiser. The appraiser shall be agreed upon by the remaining members of the Valley ROP Board of Management and the withdrawing district. In the event that the remaining members of the Valley ROP Board of Management and the withdrawing member district cannot agree upon the selection of an appraiser, the parties shall request that the Fresno County Superintendent of Schools to select the appraiser. Valley ROP shall pay all costs incurred in connection with the appraisal, unless otherwise agreed by the withdrawing member district or districts and the remaining members of the Valley ROP Board of Management.

15. Amendments:

This Agreement may be amended at any time with a subsequent written agreement and by the formal approval of the majority of the Valley ROP Board of Management and by the member districts' Governing Boards.

16. Addition of Participating Districts:

Additional school districts may enter into this Agreement subject to approval and conditions as agreed upon by the requesting school district's Governing Board, the Governing Board of each member district, the Valley ROP Board of Management. The inclusion of additional school districts to this Agreement shall not be deemed dissolution of the Valley ROP or a termination of this Agreement.

17. Severability:

Should any portion, term, condition, or provision of this Agreement be determined by a court of competent jurisdiction to be invalid or in conflict with any law of the State of California, or be otherwise rendered unenforceable or ineffectual, the validity of the remaining portions, terms conditions, and provisions shall be not affected thereby.

18. Location:

The program may be located at sites in each of the member districts, and/or other locations as determined by the Valley ROP Board of Management.

IN WITNESS WHEREOF, the parties hereto have caused this Agreement to be duly executed and signed by their authorized officers as of the date first mentioned.

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT BOARD OF TRUSTEES

Authorized Agent

3/7/19

Date of Board Authorization

ALICIA C. GALVAN
Notary Public – California
Tulare County
Commission # 2207707
My Comm. Expires Jul 29, 2021

APPROVED:

Fabrizio Lofaro
Superintendent
Valley Regional Occupational Program

By: _____ Date: _____

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CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **MEMORANDUM OF UNDERSTANDING BETWEEN VALLEY REGIONAL OCCUPATIONAL PROGRAM AND CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT FOR THE ADVANCED MANUFACTURING & WELDING PATHWAY**

ATTACHMENTS: **MEMORANDUM OF UNDERSTANDING WITH VALLEY ROP**

DISCUSSION:

The attached MOU is in support of the K12 Stronger Workforce Grant which supports the Advanced Manufacturing & Welding Pathway program at OHS and Reedley College. This Pathway will form part of the Career Technical Program that currently exists to empower students to be successful in any career by providing technical and professional skills.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Yolanda Valdez, Superintendent***

BOARD GOAL:

- ☐ **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- ☐ **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- ☐ **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends the Board approve the MOU with Valley ROP.**

PROPOSED ACTION: **APPROVE**

Item #: 15

Valley ROP
Valley Regional Occupational Program

1305 Q Street
Sanger, CA 93657
Phone 559-876-2122
Fax 559-876-2102

February 27, 2019

Memorandum of Understanding
Between
Valley Regional Occupational Program (Valley ROP)
And
Cutler-Orosi Joint Unified School District

This Memorandum of Understanding (MOU) between Valley Regional Occupational Program (**Valley ROP**) (LEAD INSTITUTION) and the **Cutler-Orosi Joint Unified School District** (PARTNER INSTITUTION) sets forth the terms and understanding between the parties herein to provide funding for the explicit purposes of providing leadership and facilitation of the K12 Strong Workforce Program (K12 SWP) to support the **Advanced Manufacturing & Welding pathway** program at Reedley College. The period of performance for this MOU is from July 1, 2019 through December 31, 2021 unless terminated earlier in accordance with this MOU or modified by mutual written agreement.

Cutler-Orosi Joint Unified School District's Career Technical Education programs exist to empower students to be successful in any career by providing opportunities to learn and practice career-related technical and professional skills. Students in these programs learn technical skills specific to their chosen industry sector by using up-to-date industry equipment and software, and they develop professional skills applicable to all careers and college success, including teamwork and collaboration, communication, critical thinking, perseverance, flexibility, integrity, and initiative.

To support the effort to improve the **Advanced Manufacturing & Welding pathway** through the K12 SWP program, **Cutler-Orosi Joint Unified School District** will support the school districts involved with Valley ROP in the K12 SWP by providing the following matching resources:

- Use of facilities for classroom
- School counselors
- JPA allocation to Valley ROP
- District LCAP funds

The total value of the matching resources identified above is **$50000**

Fabrizio Lofaro, Superintendent
Valley Regional Occupational Program

Yolanda Valdez
Cutler-Orosi JUSD Superintendent

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CONSENT**

AGENDA ITEM: **MEMORANDUM OF UNDERSTANDING BETWEEN VALLEY REGIONAL OCCUPATIONAL PROGRAM AND CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT FOR THE NURSING AND HEALTHCARE PATHWAY PROGRAM**

ATTACHMENTS: **MEMORANDUM OF UNDERSTANDING WITH VALLEY ROP**

DISCUSSION:

The attached MOU is in support of the K12 Workforce grant written in partnership with Valley ROP. A Nursing and Healthcare Pathway program is written into this grant. This grant will form part of the Career Technical Programs that currently exists to empower students to be successful in any career by providing technical and professional skills.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Yolanda Valdez, Superintendent***

BOARD GOAL:

- ☐ **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- ☐ **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- ☐ **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends the Board approve the MOU with Valley ROP.**

PROPOSED ACTION: **APPROVE**

Item #: 16

Valley ROP
Valley Regional Occupational Program

1305 Q Street
Sanger, CA 93657
Phone 559-876-2122
Fax 559-876-2102

February 27, 2019

Memorandum of Understanding
Between
Valley Regional Occupational Program (Valley ROP)
And
Cutler-Orosi Joint Unified School District

This Memorandum of Understanding (MOU) between Valley Regional Occupational Program (**Valley ROP**) (LEAD INSTITUTION) and the **Cutler-Orosi Joint Unified School District** (PARTNER INSTITUTION) sets forth the terms and understanding between the parties herein to provide funding for the explicit purposes of providing leadership and facilitation of the K12 Strong Workforce Program (K12 SWP) to support the **Nursing/Healthcare pathway** program at Fresno City College. The period of performance for this MOU is from July 1, 2019 through December 31, 2021 unless terminated earlier in accordance with this MOU or modified by mutual written agreement.

Cutler-Orosi Joint Unified School District's Career Technical Education programs exist to empower students to be successful in any career by providing opportunities to learn and practice career-related technical and professional skills. Students in these programs learn technical skills specific to their chosen industry sector by using up-to-date industry equipment and software, and they develop professional skills applicable to all careers and college success, including teamwork and collaboration, communication, critical thinking, perseverance, flexibility, integrity, and initiative.

To support the effort to improve the Nursing/Healthcare pathway through the K12 SWP program, **Cutler-Orosi Joint Unified School District** will support the school districts involved with Valley ROP in the K12 SWP by providing the following matching resources:

- Use of facilities for classroom
- School counselors
- JPA allocation to Valley ROP
- District LCAP funds

The total value of the matching resources identified above is **$100000**

Fabrizio Lofaro, Superintendent
Valley Regional Occupational Program

Yolanda Valdez
Cutler-Orosi JUSD Superintendent

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CURRICULUM/INSTRUCTION**

AGENDA ITEM: **MARCH CURRICULUM AND INSTUCTION REPORT**

ATTACHMENTS: **MARCH CURRICULUM AND INSTRUCTION REPORT**

DISCUSSION:
The curriculum report for the month of March is presented.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Shevonne Swanson, Curriculum/ Instruction Director***

BOARD GOAL:

[X] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**

[X] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**

[X] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board accept the Curriculum and Instruction Report.**

PROPOSED ACTION: **ACCEPT**

Item #: 17

March 2019
C & I report

Elementary Extended Professional Learning 2-5-19
During February's professional learning our k-5 teachers were trained to administer the ELPAC assessment. This training also included a calibration session. Teachers were required to demonstrate mastery of correctly scoring the speaking domain of the assessment.

Middle School Professional Learning 2-5-19
EMMS staff took the first step in becoming Google Certified. Teachers took a self-paced approach to acquiring new skills and knowledge of the Google Platform.

Orosi High School 2-25-19
OHS offered two break-out sessions during their February professional learning. Teachers rotated through the presentations and were allowed implementation time to apply their new knowledge in regards to the Flip Grid application and Sketchnotes.

Lovell 2-25-19
Lovell teachers spent the afternoon navigating the CAASPP interim assessment website. Teachers were provided support as they analyzed their Interim Assessment Block (IAB) data.

Curriculum and Instructional Services Steering Committee (CISC) Symposium Feb. 20- 22, 2019
Directors of: Student Services, Curriculum, Instruction, and Professional Learning, Early Education and English Language Learners, and an instructional coach attended the CISC Symposium. Breakout sessions were offered to target the needs of each attended. Our team took advantage of our size and attended a variety of sessions. Learning will be disseminated to the district to ensure large scale impact.

District Advisory Committee (DAC) 12-20-18
Attached are the minutes from the District Advisory Committee (DAC). This committee meets to provide input on district level programs and expenditures.

Teaching and Thriving in the PLC Life 2-25-19
Twenty five district level and site level leaders attended Tulare County Charter ACSA Annual Conference titled "Teaching and Thriving in the PLC Life". Throughout the conference Dr. Kanold focused on how a fully formed professional educator develops shares and transparent learning routines with their school team. He also focused on the inequities created by teaching in isolation.

Instructional Rounds
Instructional Rounds is based on the premise that collective observation and analysis of instructional practices, allows site teams to focus on a systematic approach to improve student learning. This year our sites are measuring our students' ability to demonstrate understanding of text through writing. Sites analyze data from Instructional Rounds to created "next steps." Student growth has been abundantly evident during our final visits.

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DELIA MARTINEZ, Board Member

YOLANDA VALDEZ
Superintendent
CRAIG DRENNAN
Assistant Superintendent

District Advisory Committee (DAC)
Lobo Room
December 20, 2018
4:15 PM

DAC: Members:	Site Representing:	In Attendance:
Alma Iglesias	Lovell High School	X
Omar Araujo	Golden Valley Elementary	--
Maria Trujillo	Palm Elementary	--
Sally Contreras	Cutler Elementary	X
Andrea Cuevas	El Monte Middle School	X
Meliza Zamora	Orosi High School	--

Other Attendees:
Amelia Fausto
Gladis Rodriguez

Call to Order:
Mrs. Guzman opened the meeting at 4:22 PM and welcomed committee members to today's meeting. Mrs. Guzman gave committee members a minute to introduce themselves and remind everyone the school they are representing. After the introductions, Mrs. Guzman reviewed the items on the agenda mentioning that in today's meeting we would be discussing the 2019-2020 LCAP and as a committee they would be giving their input on what services they would like implemented in the plan.

Review and Advise: Approval of Agenda and Previous Meeting Minutes (November 9, 2018):
Mrs. Guzman reviewed with committee members the minutes from the previous meeting held on November 9, 2018. She reminded committee members that during the last meeting as a committee they had learned about the rational for establishing a District Advisory Committee including the establishment of a quorum. Mrs. Guzman also reminded committee members that during the previous meeting as a committee they had elected officers; President, Vice-President, and Secretary and asked committee members if they remembered who had been elected for office. For President as a committee Mrs. Alma Iglesias was elected, Mrs. Meliza Zamora was elected Vice-President and Mr. Omar Araujo was elected Secretary. Mrs. Guzman also mentioned that as a committee they also approved the 2018-2019 Categorical Budgets, budgets that help supplant districts responsibilities. As minutes were reviewed Mrs. Guzman asked Mrs. Arellano to include funding allocated from Title V for the 2017-2018 school year, which $47,494. Mrs. Guzman asked committee members if they had any questions about the minutes being reviewed. Mrs. Sally Contreras gave the first motion to approve the minutes seconded by Mrs. Andrea Cuevas. All were in approval.

Review & Advise: LCAP Review & Data:
Mrs. Guzman mentioned to committee members that today they as a committee would be reviewing the LCAP, the district's plan that identifies the districts goals that meet State priorities. Mrs. Guzman shared with

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Superintendent
CRAIG DRENNAN
Assistant Superintendent

committee members the districts mission and vision, "Educating Minds, Inspiring Futures"; and that **ALL** students will be college, career, community ready and prepared to compete in a global economy. Mrs. Guzman shared the "Graduate Outcomes" with the committee members. She shared that as a district we strive for **ALL** students to be able to compete in a "Global Economy" stating that our students should be at the same level as students from other parts of the world such as Japan or Europe. She also shared that our focus is for students to be **Powerful Communicators**, that is, students effectively communicate verbally and in writing. She shared that we also prioritize the development of 21st Century Skills such as developing student's use of technology. She explained that in grades K-2 students use tablets while students in grades 3-12 use Chromebook to work on reports individually as well as collaboratively through Google.

Mrs. Guzman mentioned to committee members that LCAP funds equally amongst all school sites, and explained that this year students in grades K-3 accrued $1,408.44, grades 4-6 accrued $871.09, grades 7-8 $598.84 and students in grades 9-12 t $1,166.80. Mrs. Guzman also mentioned that our district also receives extra funding, supplemental and concentration, for students are identified as English Learners, Socially/Economically Disadvantaged or Homeless.

Mrs. Guzman reviewed the 8 State Priorities with committee members within **Engagement, Pupil Outcomes, and Conditions for Learning**. Mrs. Guzman reviewed Parent Involvement with committee members mentioning that our data is a reflection of 3 years' worth of parent participation at their school sites, she stressed that as a district we want parent to be involved in their student's education and parents are showing that they are attending trainings such as, Title I, Piqe, Parenting Partners, amongst others. Mrs. Guzman stressed that one area that we want to continue working on improving is decreasing chronic absenteeism; she reviewed a comparison between the 2016-2017 school year to 2017-2018 and mentioned that chronic absentee is students missing more than 10% or more school days (18 days). Mrs. Fausto was amazed in how many students miss in kindergarten. Mrs. Cuevas asked if the Men's Alliance Program was still offered at the secondary schools? Mrs. Guzman mentioned that they do still offer the program however but have had a turn-over with teaching staff for the program. Mrs. Guzman also reviewed Pupil Outcomes with committee members mentioning that Superintendent Yolanda Valdez has always stressed "Literacy is King!" Students reading at grade level has increased 2% in first grade and 7% in second grade, we hired Reading Intervention Specialists and provided technology along with supplemental programs to support students who are reading below grade level. Mrs. Guzman reminded committee members that students beginning in 3rd grade are expected to take the CAASPP test and overall our students in Cutler-Orosi have shown a growth in their CAASPP results. Mrs. Guzman stressed that recently, last school year, Cutler-Orosi Joint Unified hired 5 Parent/Student Advocates (4 at El Monte Middle School and 1 at Orosi High School) to assist school counselors and talking to students who are the lowest academically and need the support. Mrs. Guzman also discussed facilities that have been upgraded with LCAP funds, she reminded committee members that last school year the district was able to upgrade the science facilities at Orosi High School; and this year with LCAP we also renovate the George Millhorn Stadium at Orosi High School. Mrs. Guzman shared that we will soon begin to work on the sports complex between El Monte Middle School and Orosi High School which will include soccer and baseball fields.

After reviewing all the data for the Eight State Priorities, Mrs. Guzman provided the committee members an LCAP annual input form. She explained that this form was for them to provide input on what they would like

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Superintendent
CRAIG DRENNAN
Assistant Superintendent

to see implemented for next year. Mrs. Gladis Rodriguez mentioned that she would like to see more visual and performing arts offered to students, mentioning that these help stimulate our brains and students might be interested in something new. Mrs. Guzman mentioned that the District had recently been awarded a$1,000,000 grand by the Federal Government for visual and performing arts. Mrs. Guzman mentioned that this grant was written by Mrs. Tanya Goosev, Assistant Superintendent of Curriculum & Instruction before her retirement in October. Mrs. Guzman gave committee members time to fill out the LCAP annual input forms with their recommendations and collected LCAP input forms from members.

Public Comments:
Mrs. Guzman opened for public comments at 5:36 PM. There were no public comments.

Adjournment:
Mrs. Guzman closed the meeting at 5:36 PM. Mrs. Andrea Cuevas gave the first motion to close today's meeting seconded by Mrs. Sally Contreras. All were in approval.

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **ADMINISTRATIVE/ ORGANIZATIONAL**

AGENDA ITEM: **APPROVAL OF POSITIONS**

ATTACHMENTS: **NONE**

DISCUSSION:
In partnership with Central Valley Foundation, Cutler-Orosi Joint Unified School District is working on implementing practices to prevent the language/early foundation gap by providing a rich early education experience. The District proposes to provide a full day Transitional Kindergarten (TK) program for an additional 72 students, which includes Dual Immersions Instruction in TK. With the support of the Central Valley Foundation Pre-K Grant (Growing Strong), we are able to fund the following positions:

Three (3) TK Dual Teacher positions 7.5 Hours, 185 Days;
Eight (8) Instructional Assistants, four (4) English and four (4) Spanish, 5.5 Hours, 183 Days;
One (1) additional Bilingual Outreach Aide, 5.5 Hours, 183 Days;
One (1) Academic Coach/Teacher, with Pre-school experience (TK qualified); 8 Hours, 195 Days.

The staff will be placed at our three elementary schools. Our work has been significantly enhanced as a result of our partnership with the Central Valley Foundation. Through the support of CVF we have had additional resources and a year to plan the implementation of our Early Education Program for the 2019-2020 school year. All positions are funded through the grant.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Craig Drennan, Assistant Superintendent***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve these positions.**

PROPOSED ACTION: **APPROVE**

Item#: 18

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **ADMINISTRATIVE/ORGANIZATIONAL**

AGENDA ITEM: **2018-2019 SECOND INTERIM BUDGET REPORT**

ATTACHMENTS: **SECOND INTERIM REPORT PRESENTED AT BOARD MEETING**

DISCUSSION:

The Second Interim Report is due to Tulare County Office of Education on March 15th 2019. The report verifies that the Cutler-Orosi Joint Unified School District is maintaining a positive cash flow and adhering to all of the rules and regulations set by the California Department of Education. Cutler-Orosi Joint Unified School District will have a positive certification.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Faith Price, Chief Financial Officer***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe and supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the Second Interim Report.**

PROPOSED ACTION: **APPROVE**

Item #: 19

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **CLOSED**

AGENDA ITEM: **PERSONNEL**

ATTACHMENTS: **PERSONNEL LETTER**

DISCUSSION:

Employment/Additional Positions/Hours, Leaves, Resignations/Retirements, Employee Discipline/Dismissal/Release. (Personnel Letter will be presented at the meeting during closed session.)

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Craig Drennan, Assistant Superintendent***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the Personnel Letter.**

PROPOSED ACTION: **APPROVE**

Item#: A, B

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DELIA MARTINEZ, Board Member

YOLANDA VALDEZ
Superintendent

CRAIG DRENNAN
Assistant Superintendent

TO: Board of Trustees

FROM: Craig Drennan, Assistant Superintendent-Administrative Services

RE: Personnel Recommendations

DATE: March 14, 2019

EMPLOYMENT/ADDITIONAL POSITIONS/HOURS:

1. Prevention & Intervention Specialist, OHS, effective February 19, 2019; paid at Range 38, Step 3.

2. K-5 Teacher, site to be determined, effective 2019-2020 school year; paid at Classification I, Step 1.

3. 9-12 English Teacher, Orosi High School, effective 2019-2020 school year; paid at Classification I, Step 1.

4. 9-12 Math Teacher, Orosi High School, effective 2019-2020 school year; paid at Classification III, Step 3 + MA pay.

LEAVES: (Statutory – For Informational Purposes)

1. Teacher, Palm School, requesting maternity leave from April 13, 2019, to approximately May 24, 2019. Her estimated date of delivery is April 13, 2019.

2. Library Aide, Golden Valley School, requesting maternity leave from March 15, 2019, to approximately May 24, 2019. Her estimated date of delivery is April 11, 2019.

RESIGNATIONS/RETIREMENTS:

1. 9-12 Social Sciences/AVID Teacher, Orosi High School, retiring/resigning effective June 30, 2019.

2. Kindergarten Teacher, Golden Valley School, resigning effective March 6, 2019.

RESIGNATIONS/RETIREMENTS: continued

3. 4-5 STEM Teacher, Golden Valley School, resigning effective June 7, 2019.

4. K-5 Special Education Instructional Aide, Golden Valley School, resigning effective March 8, 2019.\

5. R & E Aide, Palm School, resigned effective February 5, 2019.

6. K-5 P.E. Tech, Cutler School, resigning effective March 15, 2019.

EMPLOYEE DISCIPLINE/DISMISSAL/RELEASE:

CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
Regular Board Meeting Agenda
March 14, 2019

AGENDA SECTION: **ADMINISTRATIVE/ORGANIZATIONAL**

AGENDA ITEM: **RESOLUTION IN THE MATTER OF NON REELECTION OF PROBATIONARY CERTIFICATED EMPLOYEES**

ATTACHMENTS: **RESOLUTION NO. 18-19-09**

DISCUSSION:

It is recommended that the Board approve the Resolution of the Non-Reelection of Probationary Certificated Employees.

ITEM SUBMITTED AND APPROVED BY: ***Name and title of administrator who reviewed and approved this item: Craig Drennan, Assistant Superintendent***

BOARD GOAL:

- [ ] **1. Achieve academic excellence and meet the needs for all students in a safe supportive environment.**
- [ ] **2. Build human capacity by investing in training, coaching, and setting expectations for students, parents, staff and the Board to support student achievement.**
- [x] **3. Create efficient and effective systems that are innovative, accountable and proactive.**

RECOMMENDATION: **The Superintendent recommends that the Board approve the Resolution.**

PROPOSED ACTION: **APPROVE**

Item#: 20

BEFORE THE BOARD OF TRUSTEES OF THE
CUTLER-OROSI JOINT UNIFIED SCHOOL DISTRICT
TULARE COUNTY, CALIFORNIA

RESOLUTION NO. 18-19-09

RESOLUTION OF NON-REELECTION OF PROBATIONARY CERTIFICATED EMPLOYEES

WHEREAS, Education Code section 44929.21(b) authorizes the Board of Trustees to give notice to a probationary certificated employee(s) of the Board's decision to non-reelect the employee for the next succeeding school year to such a position at any time prior to March 15th of the employee's second complete consecutive school year of employment by the District; and

WHEREAS, the employee(s) with SEID no. listed on Exhibit A to this Resolution is a probationary certificated employee who has been employed by the District during the 2018-2019 school year; and

WHEREAS, the Board of Trustees has received a recommendation from the District Superintendent or designee not to reelect the employee(s) with SEID no. listed on Exhibit A; and

WHEREAS, the Board of Trustees has decided not to reelect employee(s) with SEID no. listed on Exhibit A for the 2019-2020 school year.

NOW, THEREFORE, BE IT RESOLVED, that the Board of Trustees of the Cutler-Orosi Joint Unified School District hereby authorizes the District Superintendent or the Superintendent's designee, to notify the employee(s) with SEID no. listed on Exhibit A of the Board's decision not to reelect him/her for the 2019-2020 school year in accordance with the requirements of Education Code section 44929.21(b). The District Superintendent or the Superintendent's designee is further authorized to take any other actions necessary to carry out the intent of this Resolution.

The foregoing Resolution was PASSED and ADOPTED at a regular meeting of the Board of Trustees of the Cutler-Orosi Joint Unified School District on the 14th day of March, 2019, by the following votes:

AYES: ____
NOES: ____
ABSTAIN: ____
ABSENT: ____

_____
Rebecca Jimenez, President of the Governing Board
Cutler-Orosi Joint Unified School District

_____
Yolanda Valdez, District Superintendent
Cutler-Orosi Joint Unified School District

Cutler/Orosi Joint Unified School District

Located at the District Office Board Room

March 14, 2019, 5:00 pm

Who Attended:
Aaron R. Bock
David Bryant
Susan Simon

Approximately forty (40) members of the community

Materials:
- Community Presentation

Public Comments:

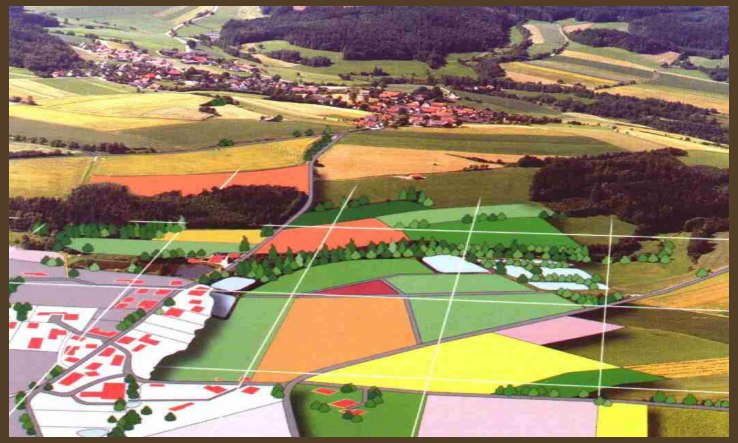
Educating Minds
Inspiring Futures

Educating Minds
Inspiring Futures

Cutler-Orosi Community Plan

2019 Update

Goals and Policies



Decision Making

Plan Framework

California State Law

Planning Principles

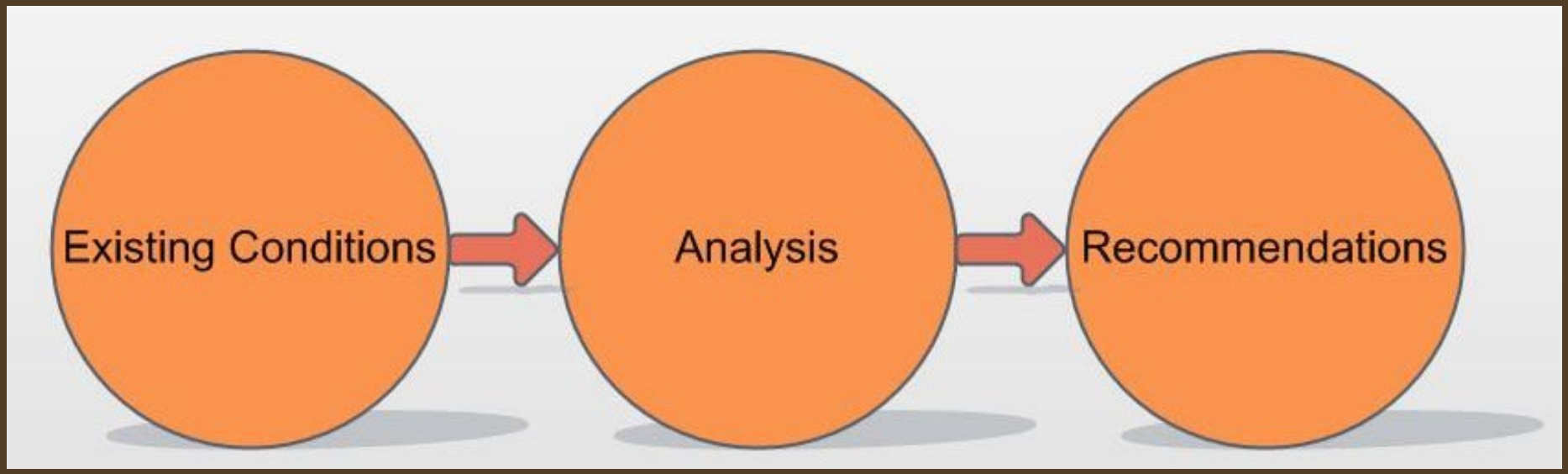
Tulare County General Plan Policies

Schedule

Cutler-Orosi Community Plan - Update and Environmental Impact Report (EIR)

	Tasks	Schedule and Milestones										
1	**Preliminary Work**											
1.1.	General Plan Initiation BOS September 30, 2014, Resolution No. 2014-0688											
1.2.	CEQA Consultant RFPs											
2	**Constraints Assessment**											
3	**Needs Assessment**											
4	**Prepare Policy Plan (0-5yrs, 5-10yrs, 10 to 17 yrs to GP)**											
5	**Environmental Impact Report (EIR)**											
6	**Planning Commission Hearing**											
7	**Board of Supervisors Hearing**											
8	**Project Completion**											

Community Plan Process



Community Conditions

Staff / Consultant Research

Citizen Participation

Sharing Concerns

Addressing Concerns

Analysis and Recommendations

One Major Purpose

1. Guide Future Decision Making

Two Primary Documents

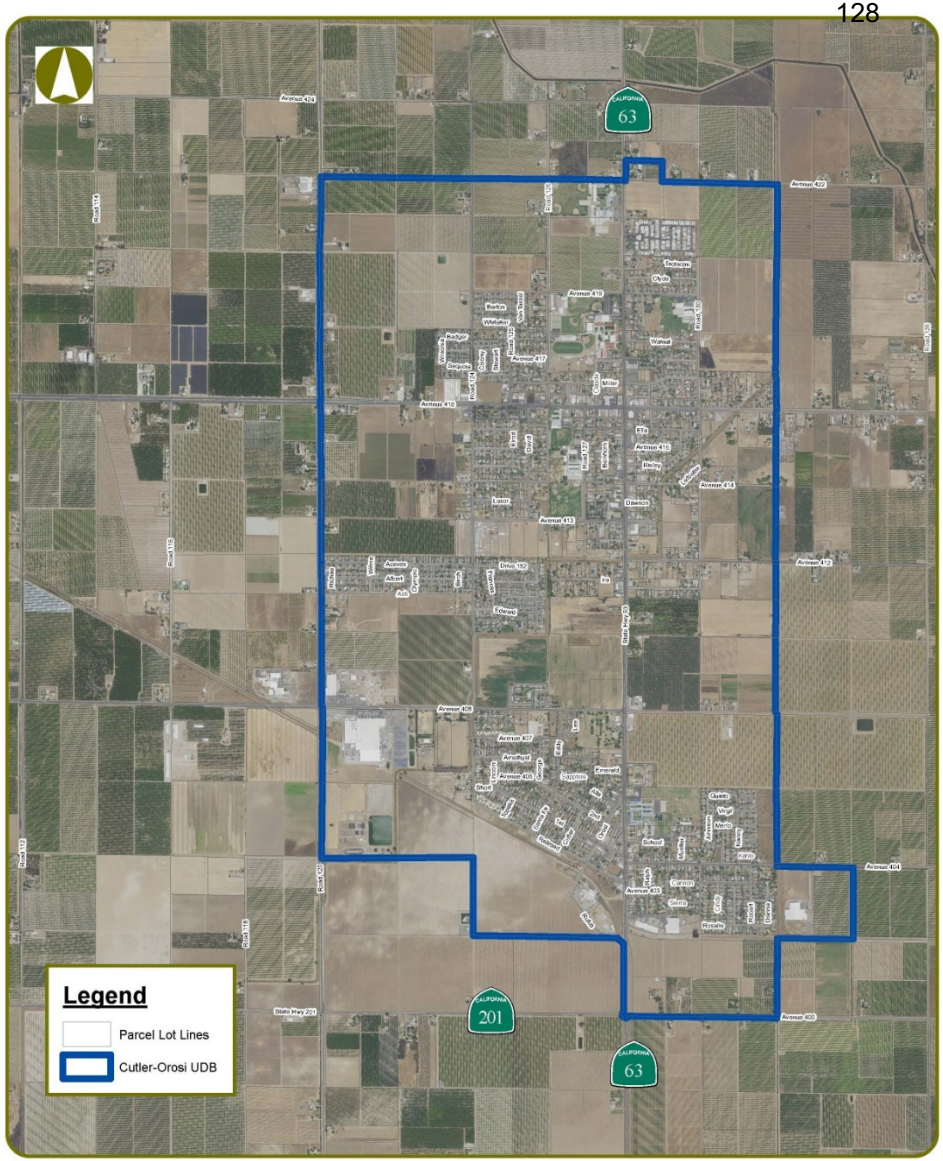
1. Cutler-Orosi Community Plan
2. Environmental Impact Report (EIR)

Three Primary Components

1. Urban Development Boundary

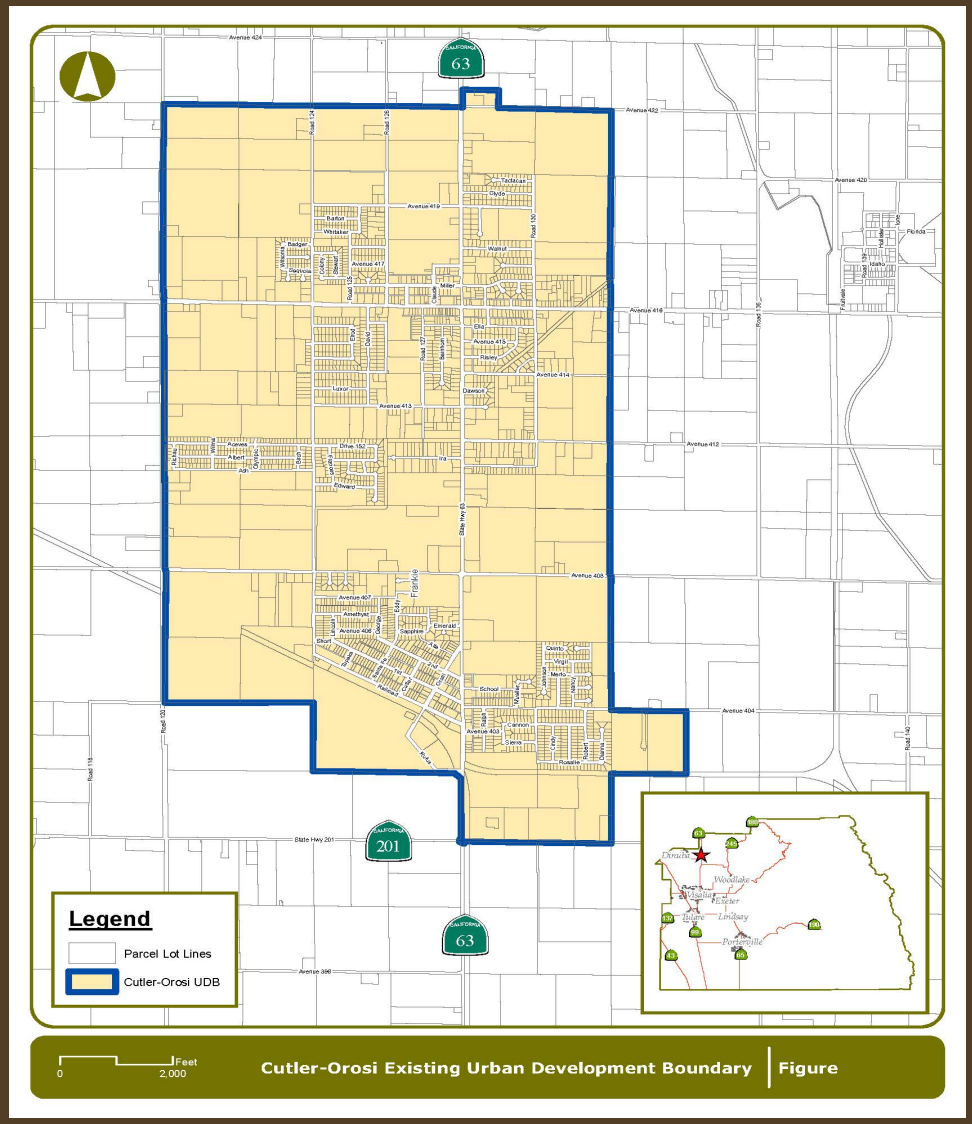
2. Land Use and Circulation

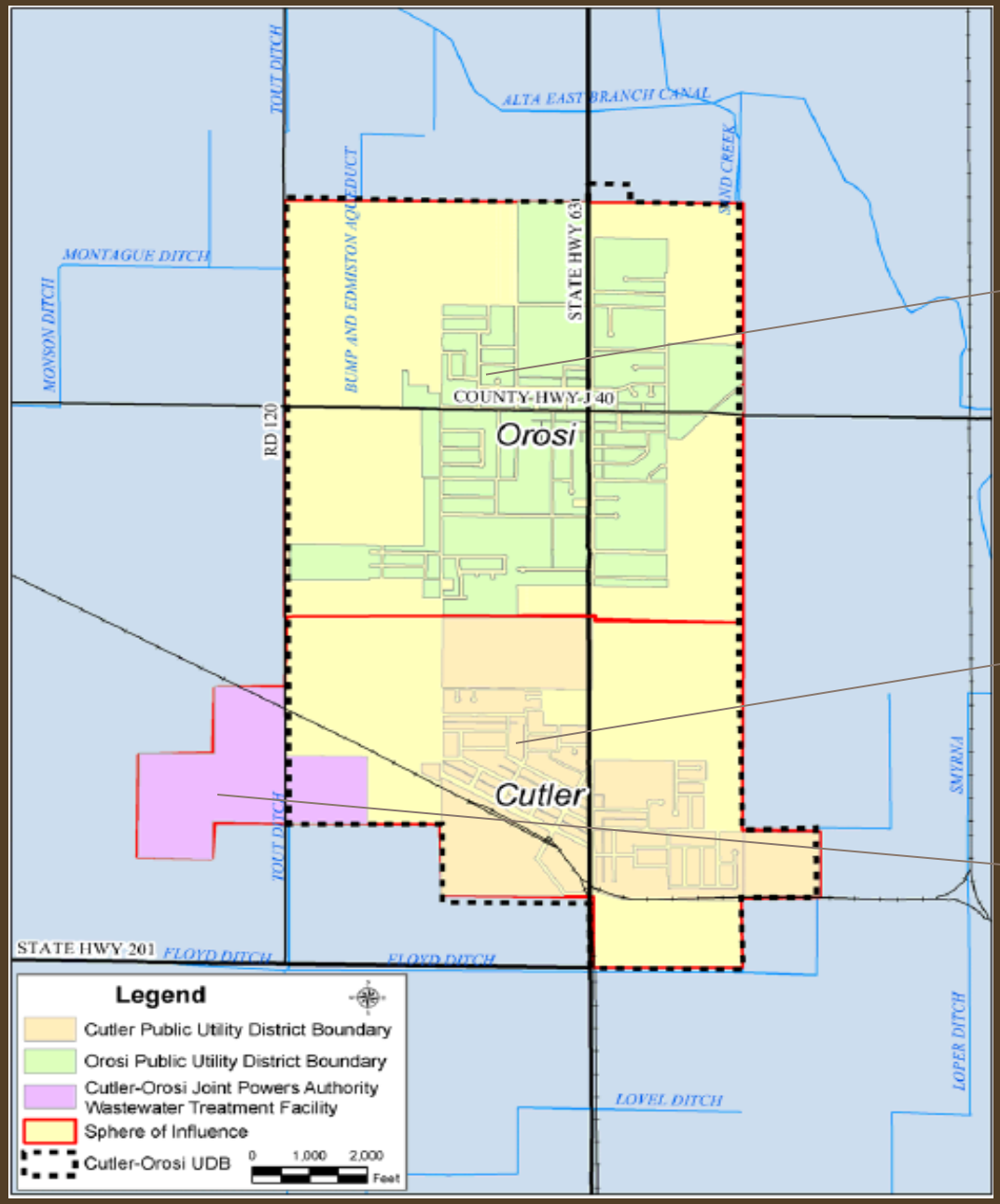
3. Policies to guide future decision making



Cutler-Orosi Aerial Map | Figure

Cutler/Orosi Existing Urban Development Boundary



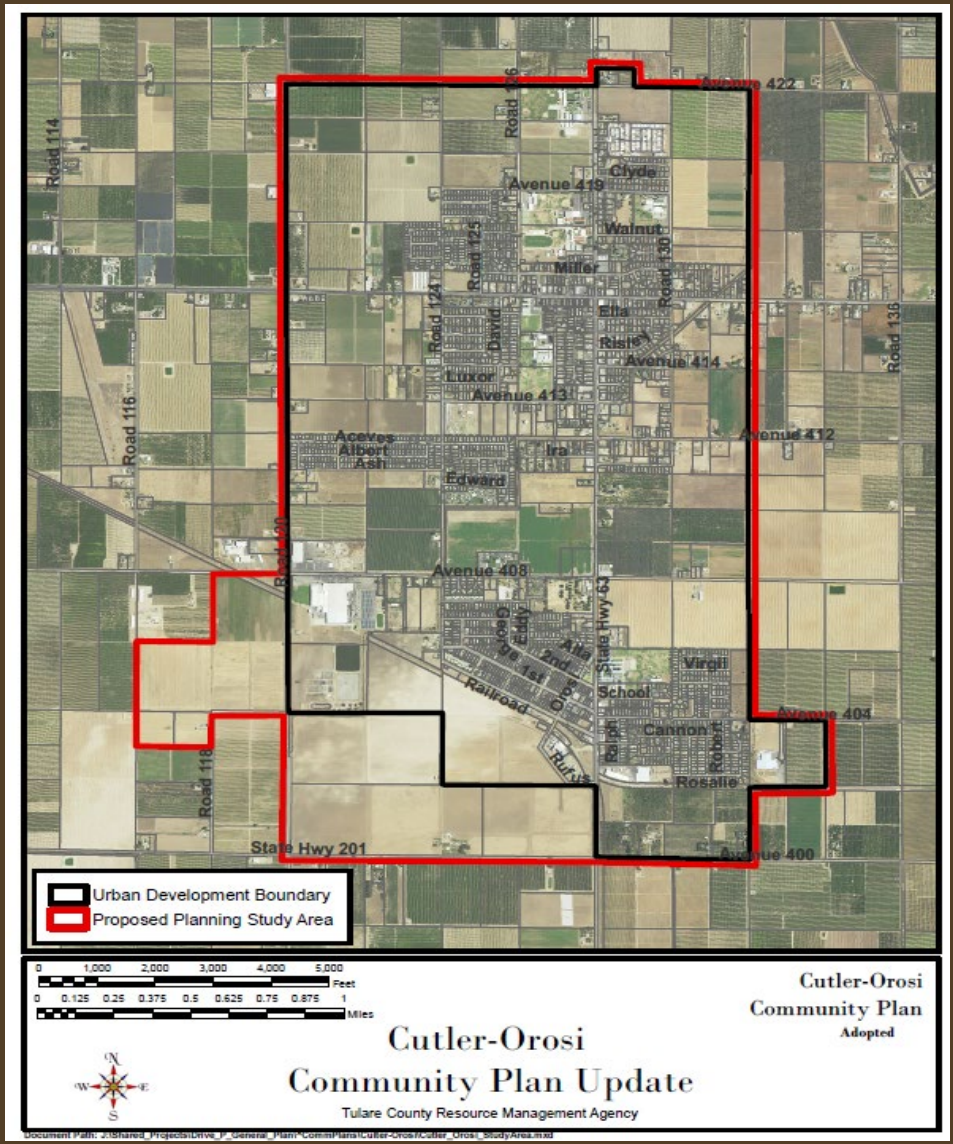


Orosi Public Utility District

Cutler Public Utility District

Cutler/Orosi Wastewater Treatment Facility

Cutler/Orosi Community Plan Update Study Area

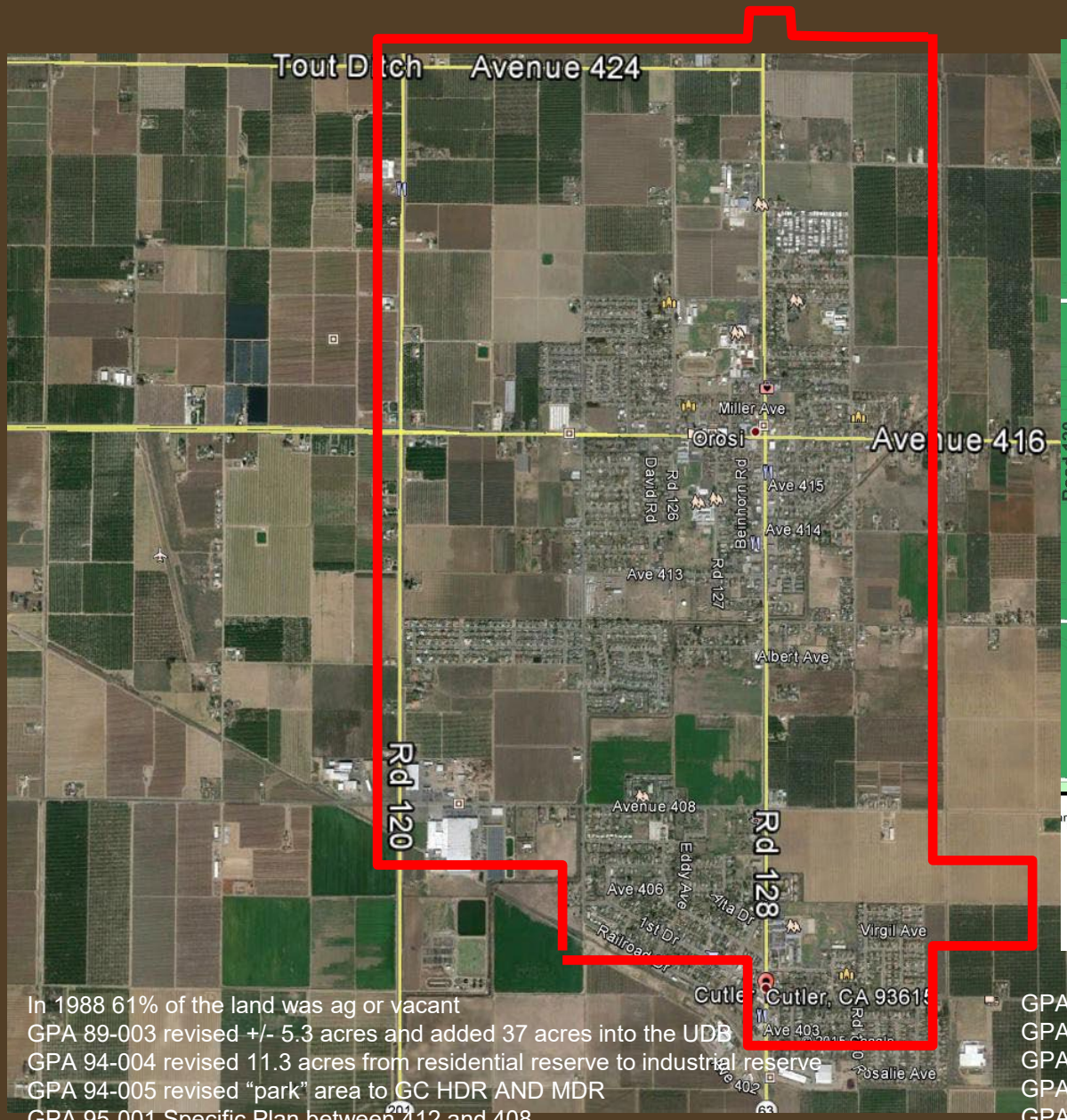


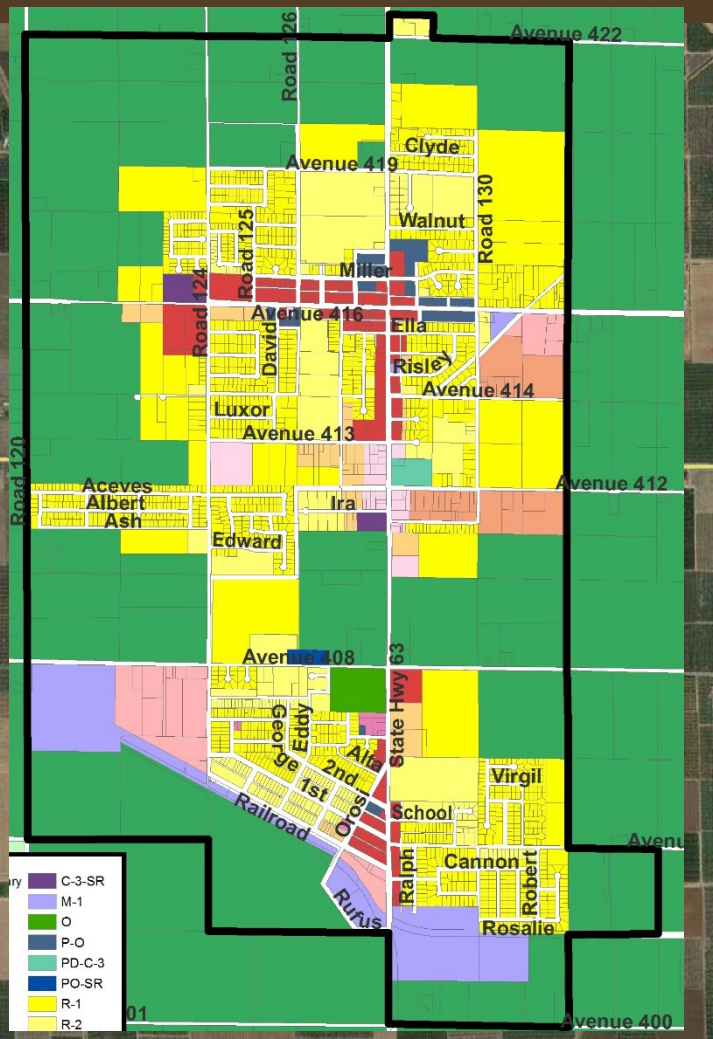
Cutler/Orosi Urban Development Boundary

Existing UDB area = 2,200 acres

UDB area with Study Area Additions = 2,600 acres

UDB increase in area = 400 acres



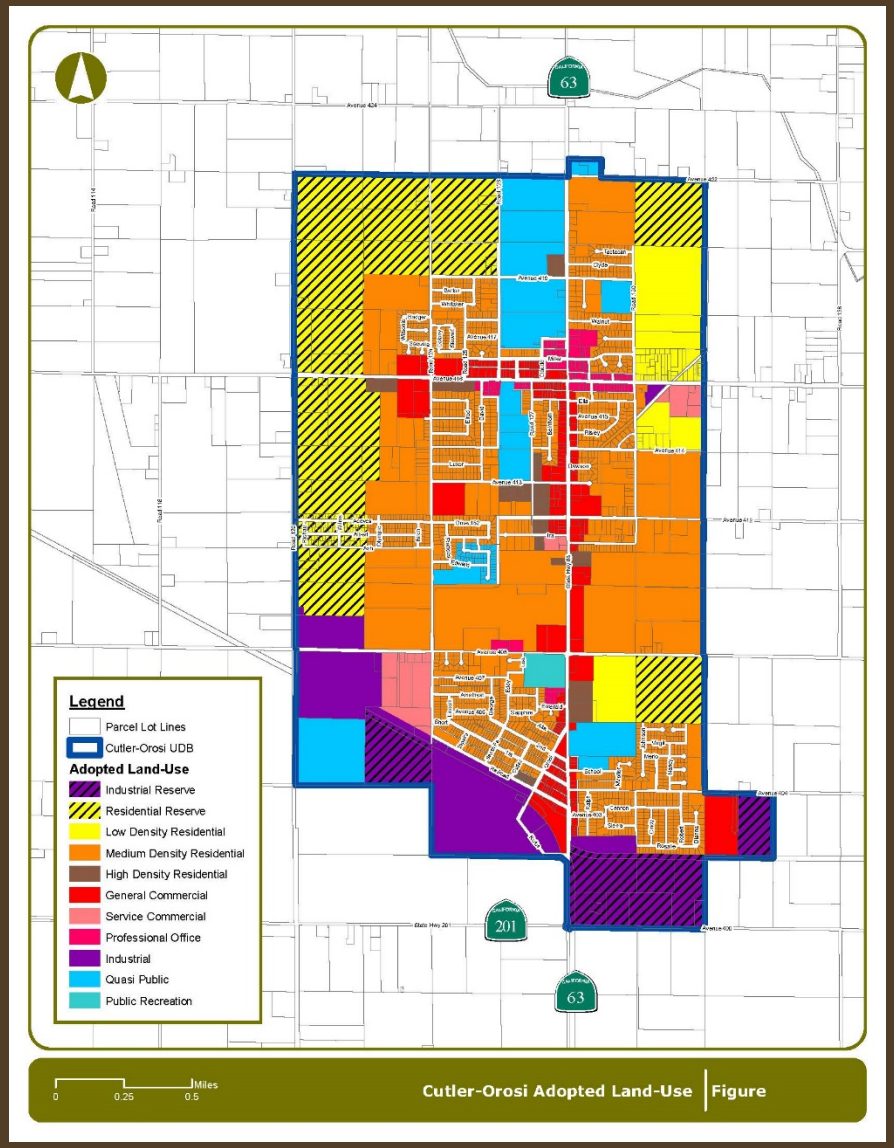


In 1988 61% of the land was ag or vacant
GPA 89-003 revised +/- 5.3 acres and added 37 acres into the UDB
GPA 94-004 revised 11.3 acres from residential reserve to industrial reserve
GPA 94-005 revised “park” area to GC HDR AND MDR
GPA 95-001 Specific Plan between 412 and 408

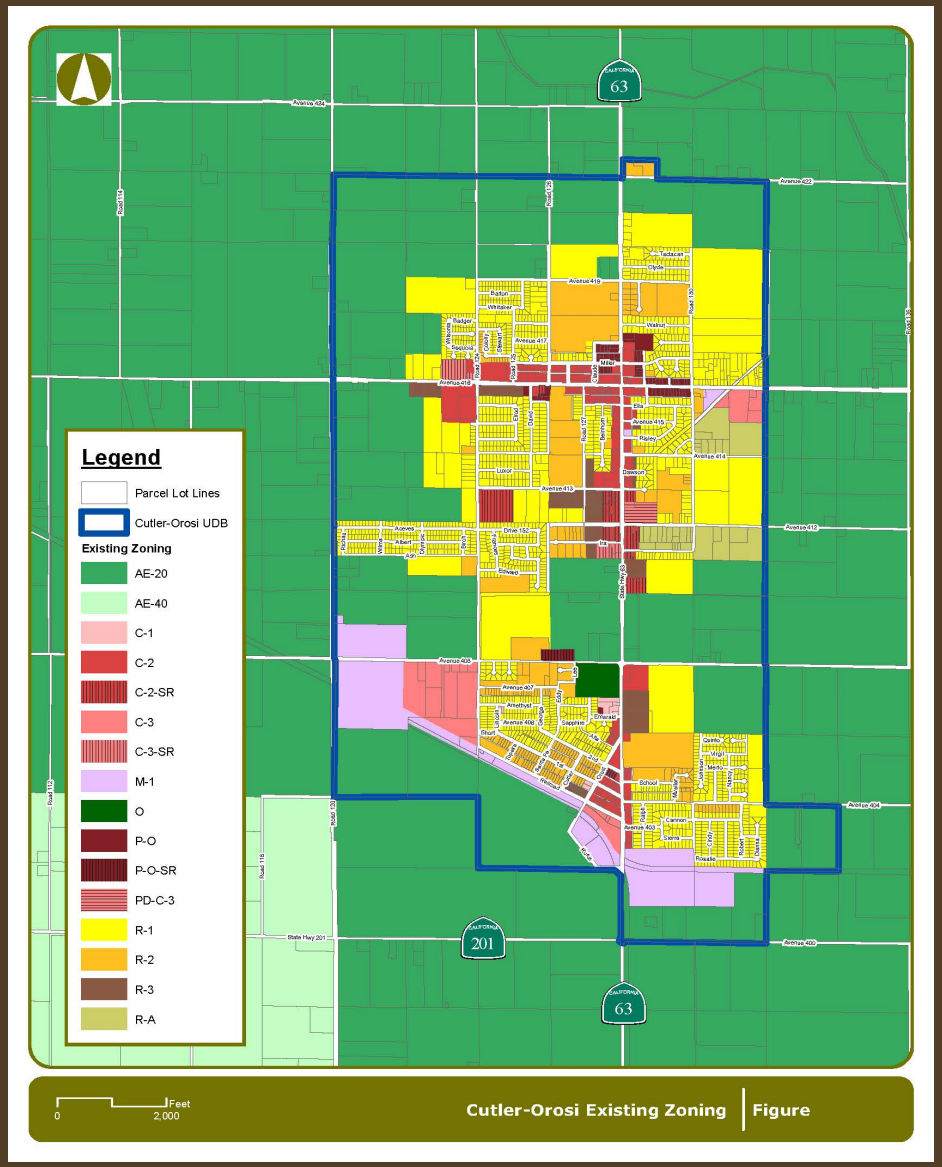
GPA 95-005 revised 9.1 acres MDR to GC
GPA 95-006 revised .61 acres
GPA 98-04 added 5 acres
GPA 00-001 revised 3 acres MDR to Professional Offices
GPA 00-005 revised 2.75 acres

Google earth

Cutler/Orosi Adopted Land Use



Cutler/Orosi Existing Zoning Districts



Community Infrastructure Improvements

$5 Million In Improvements to the Wastewater Treatment Facility

Orosi - Upgrades for new water and sewer lines

Orosi - Two New Water Wells

Orosi - New Flowmeters

Cutler - Upgrades for new water and sewer lines

Cutler - One new water well and one new test well

Cutler - New Water Blending Tank

Pending Grants to replace remaining water and sewer lines

Measure R Funds

- 2006 Voters approved Measure R ½ cent sales tax for transportation
- Since 2006 appx. $1.1 million – Cutler and Orosi
- Surface maintenance such as surface seal, oil seal, chip seal, road resurface.

Measure R Funds- Cutler

YEAR 2010 MEASURE R RESURFACING							
	ROUTE			LIMITS			LENGTH
R130		LA402	ROSALIE AVE.	-	A404		0.21
						TOTAL MILES	0.21
YEAR 2012 MEASURE R RESURFACING							
	ROUTE			LIMITS			LENGTH
LA403B	CANNON AVE.	R130		-	LR131C	ROBERT RD.	0.18
LA406C	EMERALD AVE.	R127		-	R128	SH 63	0.11
LA407B	HAZEL AVE.	R126*	END	-	LR126D	LEE RD.	0.05
LR124F	LINCOLN RD.	A407*		-	A408		0.05
LR124G	TOPEKA RD.	A407*		-	A408		0.05
LR127B	PEARL RD.	A406*	END	-	LA406C	EMERALD AVE.	0.03
						TOTAL MILES	0.47
YEAR 2013 MEASURE R RESURFACING							
	ROUTE			LIMITS			LENGTH
D125B	SANTA FE DR.	D128B	RAILROAD DR.	-	D129B	FIRST DR.	0.08
LA406A	AMETHYST AVE.	R124		-	R126	GEORGE RD.	0.22
						TOTAL MILES	0.30

2010-2013

Since 2006:
$242,000

Measure R Funds- Orosi

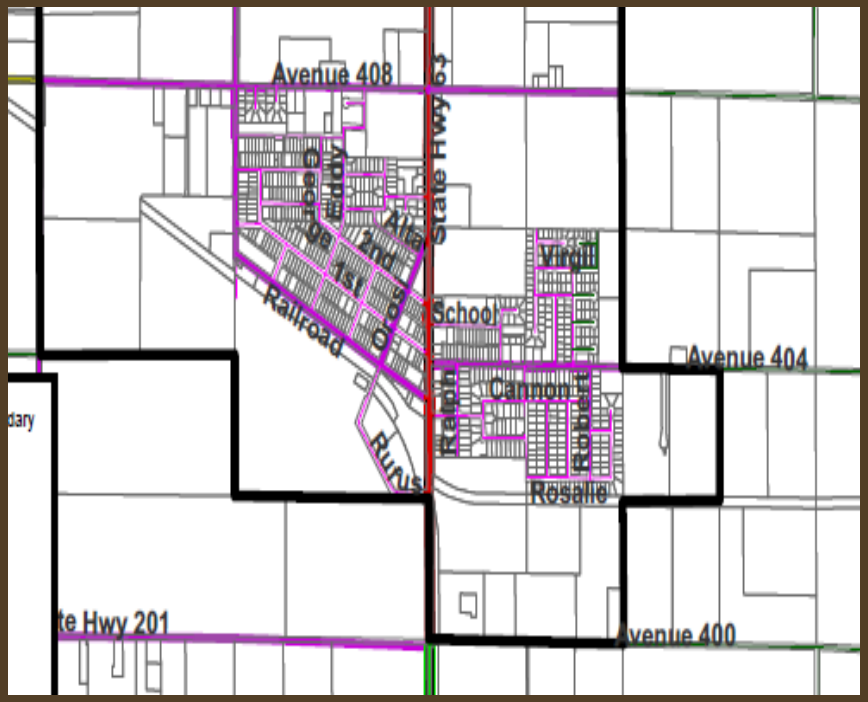
Since 2006- $900,000

YEAR 2011 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
D129D	LEDBETTER DR.	A414		-	R130		0.09
LA411B	IRA AVE.	R126*	END	-	R128	SH 63	0.23
LA416B	MILLER AVE.	R125	ELROD RD.	-	R126		0.13
LR125F	MILLER RD.	LA416B	MILLER AVE.	-	LA416*	END N/O MILLER	0.02
						MEASURE R MILES	0.47
						TOTAL MILES	0.72
YEAR 2012 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
A419		R124		-	R126		0.25
R125		A416	EL MONTE WAY	-	LA418C	WHITAKER AVE.	0.29
R126		A416	EL MONTE WAY	-	A419		0.39
						TOTAL MILES	0.93
YEAR 2013 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
A414		R127		-	LR127C	BEINHORN RD.	0.07
A415		R128	SH 63	-	LR129A	MUELLER RD.	0.19
LR128B	RALPH RD.	LA415C	ELLA AVE.	-	A416	EL MONTE WAY	0.08
						TOTAL MILES	0.34

2011-2013

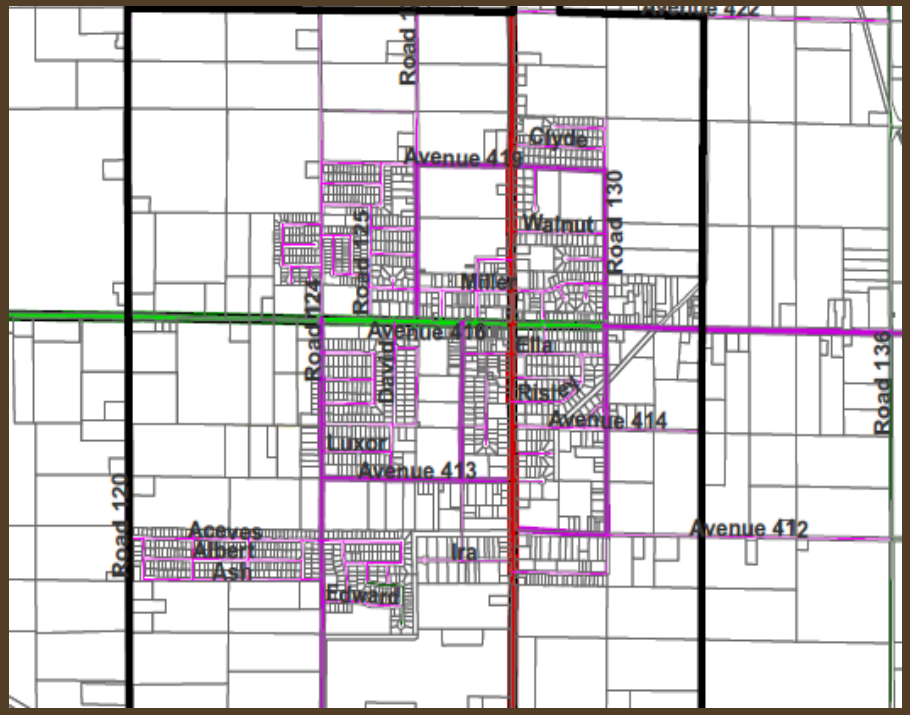
Complete Streets Cutler

- Avenue 404 – SR 63 to Robert Rd
  - Sidewalk, C&G, Drainage
- Avenue 408 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Railroad Drive – SR 63 to Rd 124
  - Sidewalk, C&G, Drainage
- First Drive – SR 63 to Rd 124
  - Striping, Bike Lane



Complete Streets Orosi

- Avenue 416 – SR 63 to Rd 140 (East Orosi)
  - Widening, Sidewalk, C&G, Drainage, Bridges over Sand Creek and Canal, Bike Lane, Transit
  - Multi Use Trail
- Avenue 413 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Avenue 416 – SR-63 to Dinuba
  - Class II bike lane
- Avenue 419



Proposed Complete Streets Projects

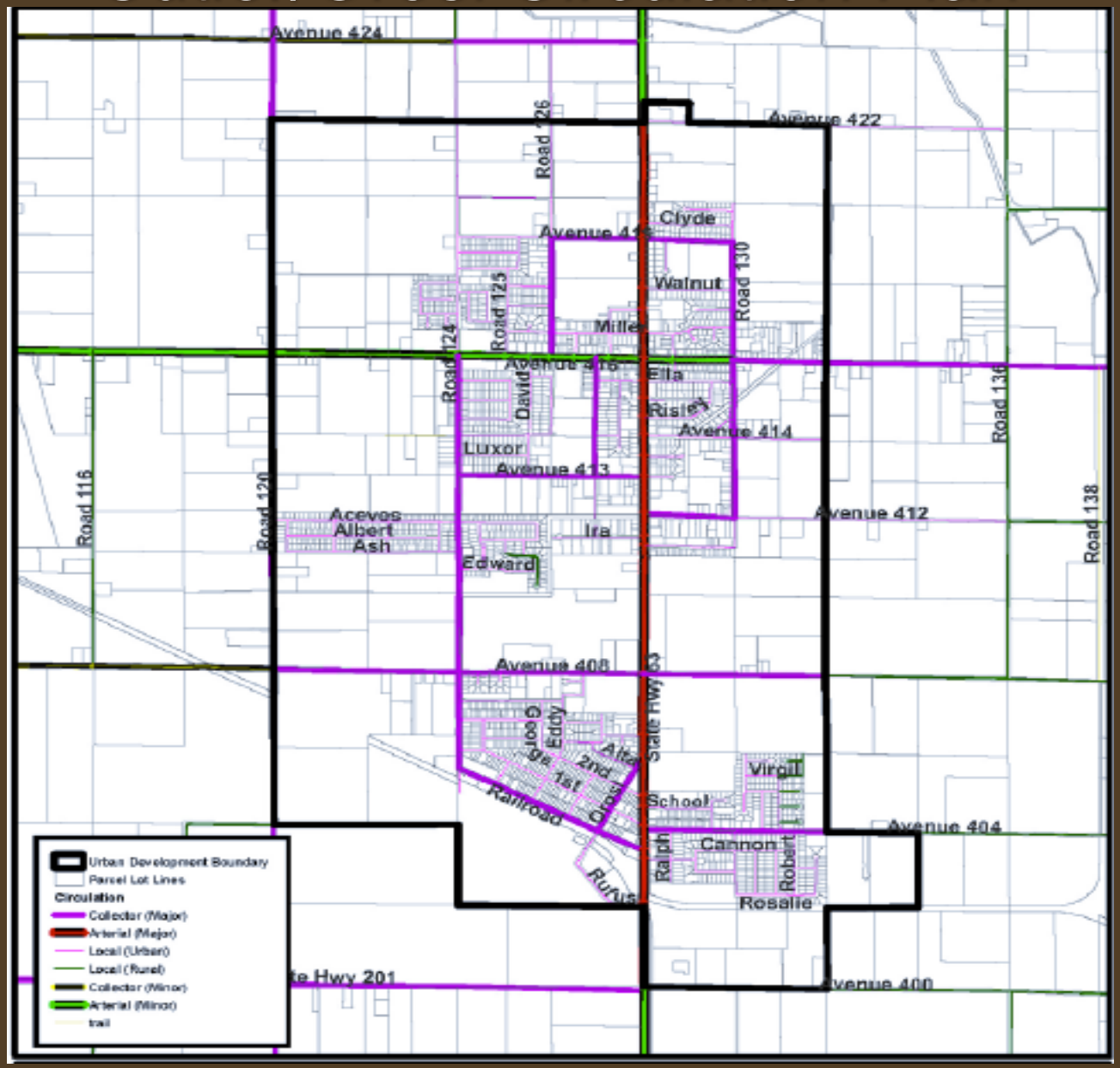
Orosi

1. Avenue 416 – SR 63 to Road 140 (East Orosi)
2. Avenue 413 – Road 124 to SR 63
3. Avenue 419
4. Avenue 416 – SR-63 to Dinuba
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

Cutler/Orosi Circulation Plan



Tulare County General Plan Update Guiding Policies

2. Planning Framework

PF-1	To provide a planning framework that promotes the viability of communities, hamlets, and cities while protecting the agricultural, open space, scenic, cultural, historic, and natural resource heritage of the County *[New Goal].*
PF-2	To provide a realistic planning area around each unincorporated community that clearly delineates the boundaries of each community and provides a framework for economic development, the provision of public services, and an outstanding quality of life *[Urban Boundaries Element; Goal 3, Goal 1UB.C][Urban Boundaries Element Amendment (88-01); 1988, Modified)].*

PF-2.4 **Community Plans** The County shall ensure that community plans are prepared, updated, and maintained for each of the communities.

PF-2.7 **Improvement Standards in Communities** The County shall require development within the designated UDBs to meet an urban standards for curbs, gutters, sidewalks, and community sewer and water systems.

4. Land Use

LU-1	To encourage the overall economic and social growth of the County while maintaining its quality of life standards and highly efficient land use *[New Goal].*
LU-7	To preserve and enhance the character and scale of Tulare County's communities, hamlets, and rural areas, including their design heritage and historic character *[New Goal].*

LU-1.1 **Smart Growth and Healthy Communities.** The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including: walkable neighborhoods, creating a strong sense of place, directing growth toward existing communities, preserving open space, and creating a range of housing opportunities and choices.

LU-4.1 **Neighborhood Commercial Uses.** The County shall encourage the development of small neighborhood convenience and grocery facilities to meet the everyday shopping and personal needs of immediately surrounding residential land uses in communities.

LU-6.3 **Schools in Neighborhoods.** The County shall encourage school districts to locate new schools in areas that allow students to safely walk or bike from their homes.

5. Economic Development

ED-1	To maintain a healthy and diverse local economy that meets the present and future employment, shopping, recreational, and service needs of Tulare County residents *[New Goal].*
ED-2	To promote business growth and industry diversification and maintain a favorable business climate and a supportive economic foundation *[New Goal].*
ED-3	To diversify the economic base of Tulare County through the expansion of non-agricultural industry clusters and through the development and expansion of recreation and visitor-serving attractions and accommodations *[New Goal].*

ED-1.6 **Develop Public/Private Partnerships.** The County shall pursue partnerships to encourage the development of public facilities and infrastructure improvements that benefit communities.

HS-1	To protect County residents and visitors from injury and damage resulting from natural catastrophes, man-made events, and hazardous conditions *[New Goal based on Five County Seismic Safety Element (1974); Goals 1, 2, 3, 6, 9, 12 & 13. Pg. 11].*

Health and Safety for all Neighborhoods

HS-9	To support healthy lifestyles among residents of Tulare County through the built environment and land use decisions that play an important role in shaping the pattern of community development, in either promoting or discouraging good health for its citizens *[New Goal]*.

14. Public Facilities and Services

PFS-1	To establish and maintain acceptable levels of service, minimize costs, and provide criteria for determining the location, capacity, and timing of existing and future public facilities and services *[New Goal].*

Adequate Quantity and Quality of Public Services

Achieving General Plan Goals

- Increasing uses by right
- Creating flexibility for uses through mixed use overlays

DEMOGRAPHIC AND ECONOMIC INFORMATION

- Population
- In 2010, Cutler's population was 5,000 and Orosi's population was 8,770.
- The population for Cutler increased to 5,850 and Orosi's decreased to 7,760 by 2017 (Total Population 13,610).
- Median Age
- The median age in Cutler stayed about the same 24.6 in 2010 to 24.5 in 2017. Orosi increased from 26.4 in 2010 to 28.8 in 2017.

- <u>Economic Conditions (2017)</u>
- Cutler had an unemployment rate of 13.1% and Orosi's unemployment rate was 20%, compared to 10.0% for Tulare County, and 7.7% for California
- Cutler's median household income was $30,760 and Orosi's median household income was $43,379, compared to $47,280 for Tulare County and $76,975 for State of California
- 39.7% of families in Cutler live below the poverty line and 24.3% of families in Orosi live below the poverty line compared to 23% for Tulare county and 11.1% for California.

- <u>Housing Units (2010-2017)</u>
- During the decade between 2010 and 2017, the number of housing units in Cutler increased from 1,136 to 1,293 Orosi increased from 2,070 to 2,076
- 2017 American Community Survey indicated that 69.6% of the housing units in Cutler were 1 unit detached and 80.0% in Orosi were 1 unit detached.
- 2017 American Community Survey indicated that 65.1% of both Cutler and Orosi housing units were owner occupied.
- In 2017, the vacancy rate in Cutler was 3.2% and 3.7% in Orosi.
- In 2015, approximately 15% of the housing units in Cutler and 87% in Orosi were sound.

FUTURE CONSIDERATIONS

- DEMOGRAPHIC AND ECONOMIC INFORMATION WILL ASSIST IN DETERMININING THE AMOUNT OF RESIDENTIAL, COMMERCIAL AND INDUSTRIAL OF FUTURE LAND USES

NEXT STEPS

- Future Community Plan Workshops.
  - April 11, 2019
  - May 9, 2019

Thank You for Attending

Aaron R Bock, MCRP, JD, LEED AP
Assistant RMA Director
Economic Development & Planning
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7050

David Bryant, MPA
Chief Planner, Special Projects
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7130

Susan Simon
Planner III
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7126

Cutler/Orosi Joint Unified School District

Located at the Cutler Elementary School

May 2, 2019, 6:00 pm

Who Attended:
Aaron R. Bock, RMA
David Bryant, RMA
Susan Simon, RMA
Jose Saenz, RMA

Yolanda Valdez, Superintendent, Cutler/Orosi Joint Unified School District

Approximately ten (10) members of the community attended the meeting at the Cutler Elementary School.

Materials:

- Community Presentation

Public Comments:

EXIT

Cutler Elementary School
Cutler/Orosi Community Plan
May 2, 2019

Name	Address & Email Address	Phone No.
Annabel Garcia	12441 Amethyst Ave Cutler annabelgarcia1987@yahoo.com	(559) 859-4110
Beatriz Lopez	40609 Road 128 Cutler CA	(559) 238-5098
Mary Andrade	7433 E. Conejo Ave Selma 93662	967-2080
Alicia Mendoza	40408 RD. 12 Apt.c.	559 476-95-97
Gabriela Garcia	40609 Road 128 Cutler CA	(559) 723-4336

(#1)

N

SCALE: 1"=30'
0 15 30 60
(SCALE ACCURATE FOR FULLSIZE PLANS)

State Route 63 (Road 128) Ella Avenue. Crosswalk Safety
Concerns: Traffic Speed, Sight Distance (on Street Parking) Road Crown

*General Note: Incorporate Caltrans Projects into Complete Streets Plan. Transit Shelters Not currently Utilized for Transit Stops.

REVISIONS

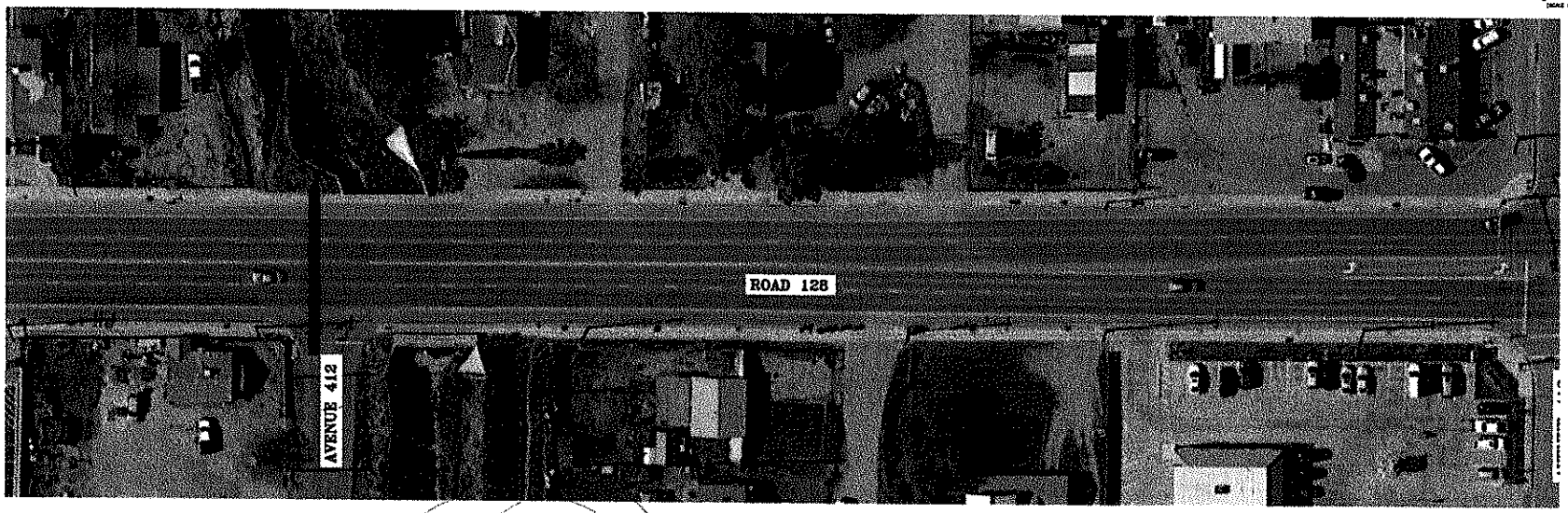
No.	Description	Date	By

COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

EXHIBIT
CUTLER–OROSI ROAD 128
TULARE COUNTY

SCALE	-
DIVISION	-
JOB NO.	-
DESIGNED	-
DRAWN	-
CHECKED	-
FILE	CUTLER-OROSI.DWG
DATE	-
SHEET No.	L5

1 of 158



State Route 63 (Road 128)/Avenue 412 No Crosswalk

PLAN

N

SCALE: 1"=50'

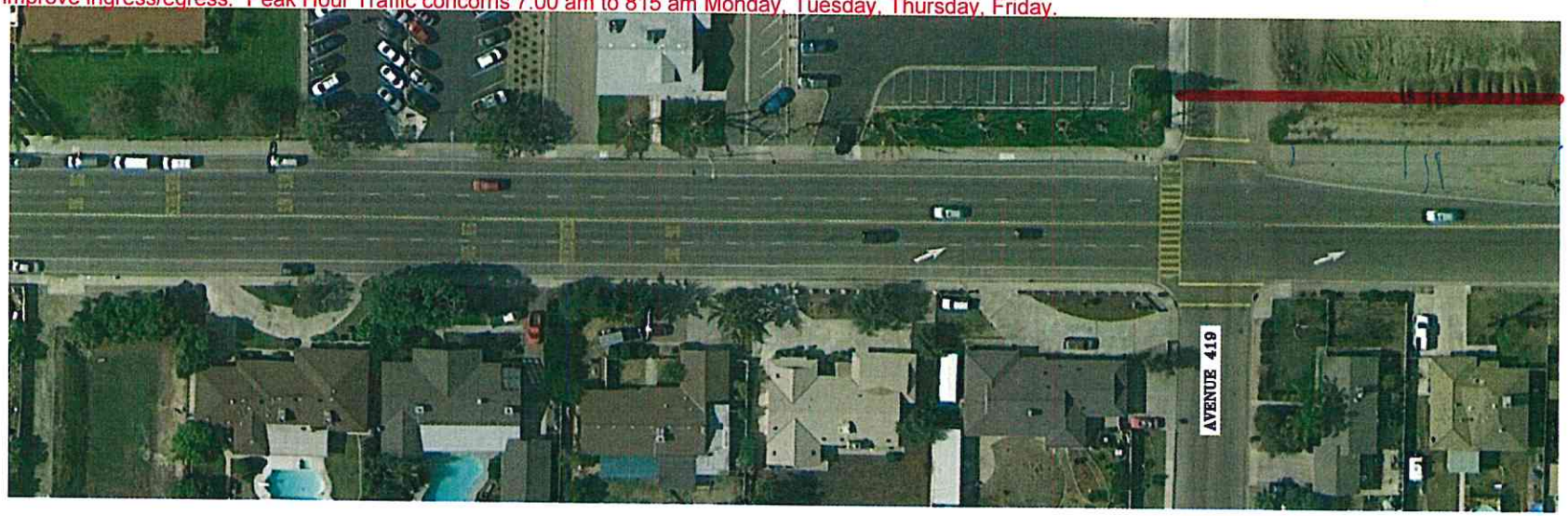
REVISIONS

COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

EXHIBIT
CUTLER–OROSI ROAD 128
TULARE COUNTY

SCALE
DIVISION
JOB NO.
DESIGNED
DRAWN
CHECKED
FILE CUTLER-OROSI.DWG
DATE
SHEET NO.
L4

State Route 63 (Road 128) North of Avenue 419 (El Monte Middle School) concerns: Tractor-Trailer Parking, Traffic Speed, Consider Traffic Parking Conmditions to improve ingress/egress. Peak Hour Traffic concorns 7:00 am to 815 am Monday, Tuesday, Thursday, Friday.



N

SCALE: 1"=30'
0 15 30 60
(SCALE ACCURATE FOR FULLSIZE PLANS)

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5961 SOUTH MOONEY BLVD.
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EXHIBIT
CUTLER-OROSI ROAD 128
TULARE COUNTY

SCALE	-
DIVISION	-
JOB NO.	-
DESIGNED	-
DRAWN	-
CHECKED	-
FILE	CUTLER-OROSI.DWG
DATE	-

SHEET No. L7

1 OF 16

Cutler-Orosi Community Plan

2019 Update

Plan Framework

California State Law

Planning Principles

Tulare County General Plan Policies

Schedule

Cutler-Orosi Community Plan - Update and Environmental Impact Report (EIR)

	Tasks	Schedule and Milestones										
1	**Preliminary Work**											
1.1.	General Plan Initiation BOS September 30, 2014, Resolution No. 2014-0688											
1.2.	CEQA Consultant RFPs											
2	**Constraints Assessment**											
3	**Needs Assessment**											
4	**Prepare Policy Plan (0-5yrs, 5-10yrs, 10 to 17 yrs to GP)**											
5	**Environmental Impact Report (EIR)**											
6	**Planning Commission Hearing**											
7	**Board of Supervisors Hearing**											
8	**Project Completion**											

Analysis and Recommendations

One Major Purpose

1. Guide Future Decision Making

Two Primary Documents

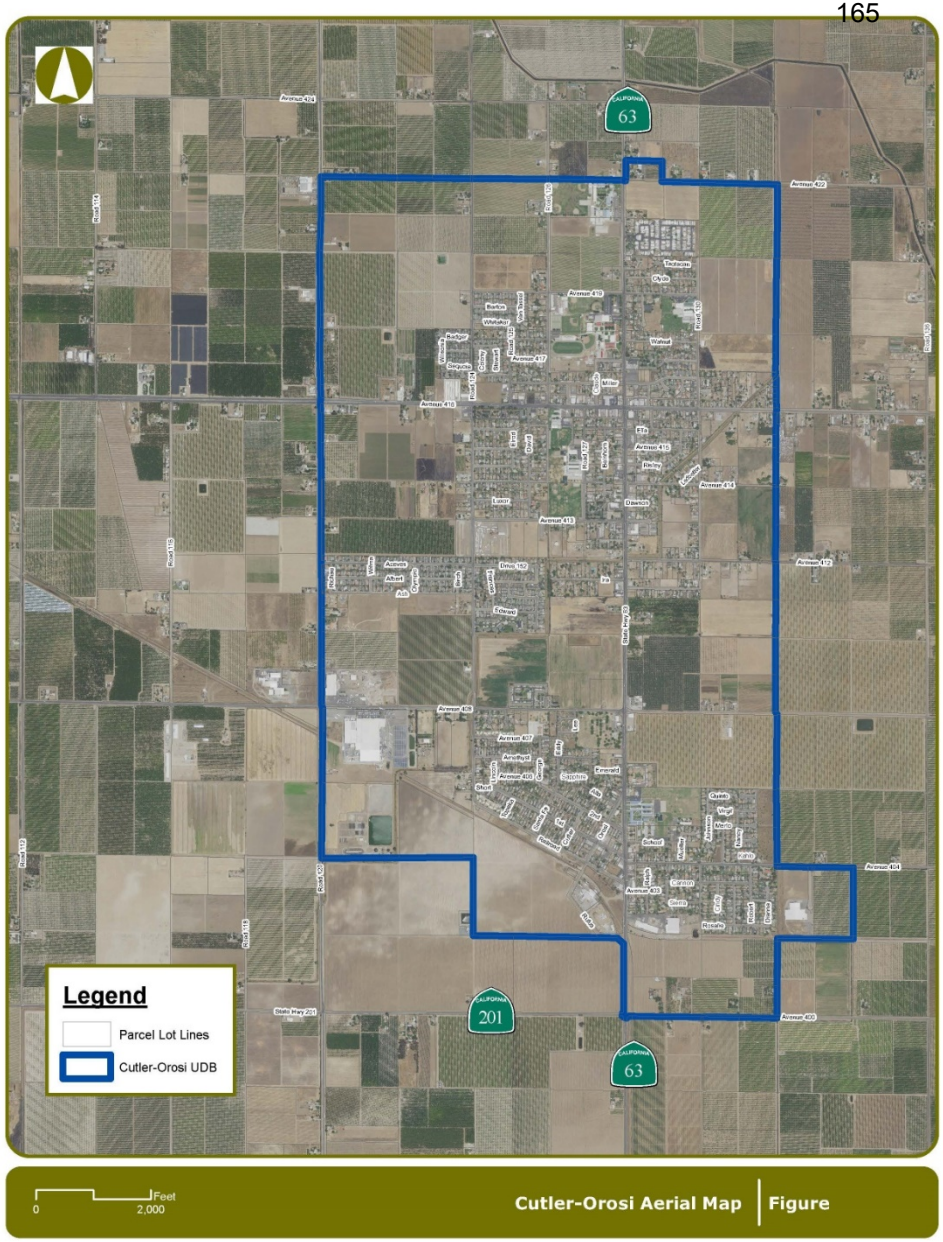
1. Cutler-Orosi Community Plan
2. Environmental Impact Report (EIR)

Three Primary Components

1. Urban Development Boundary

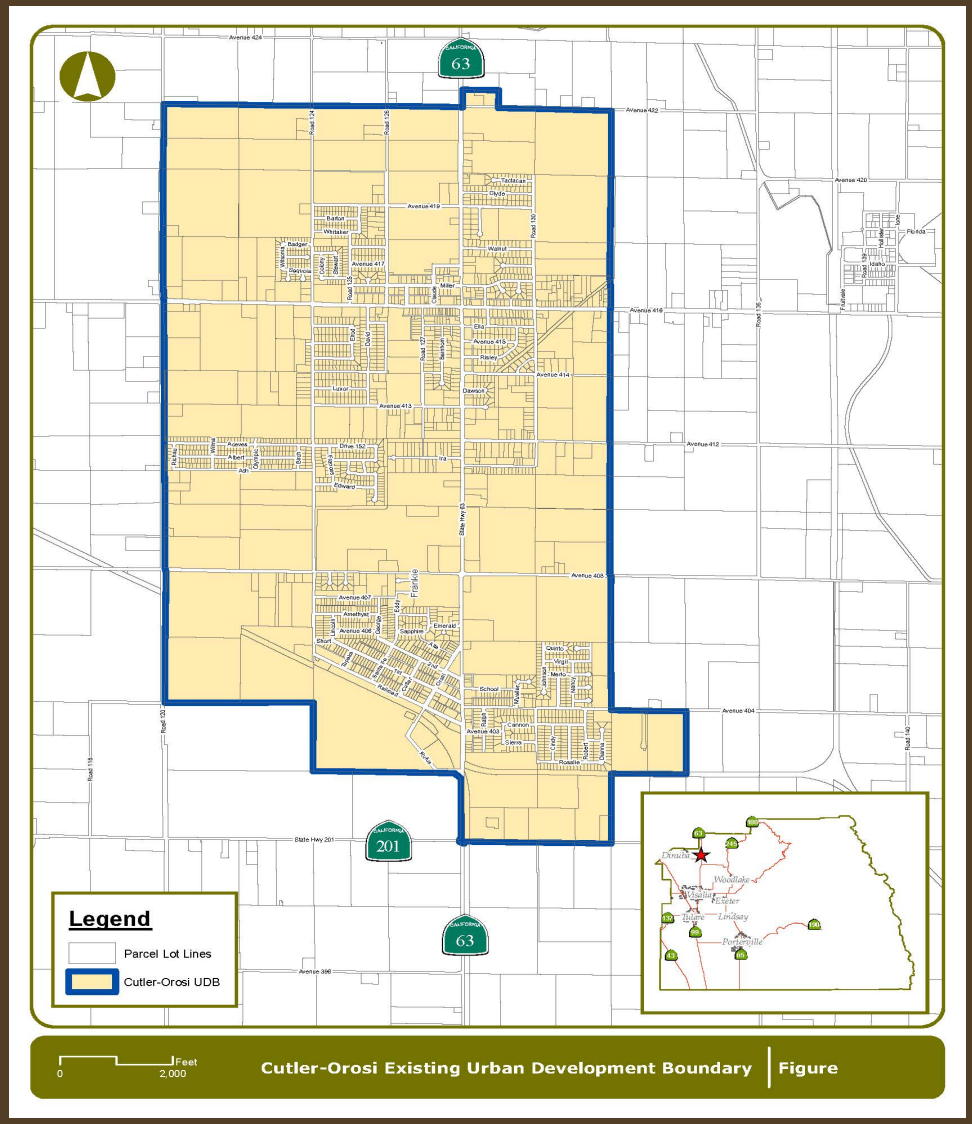
2. Land Use and Circulation

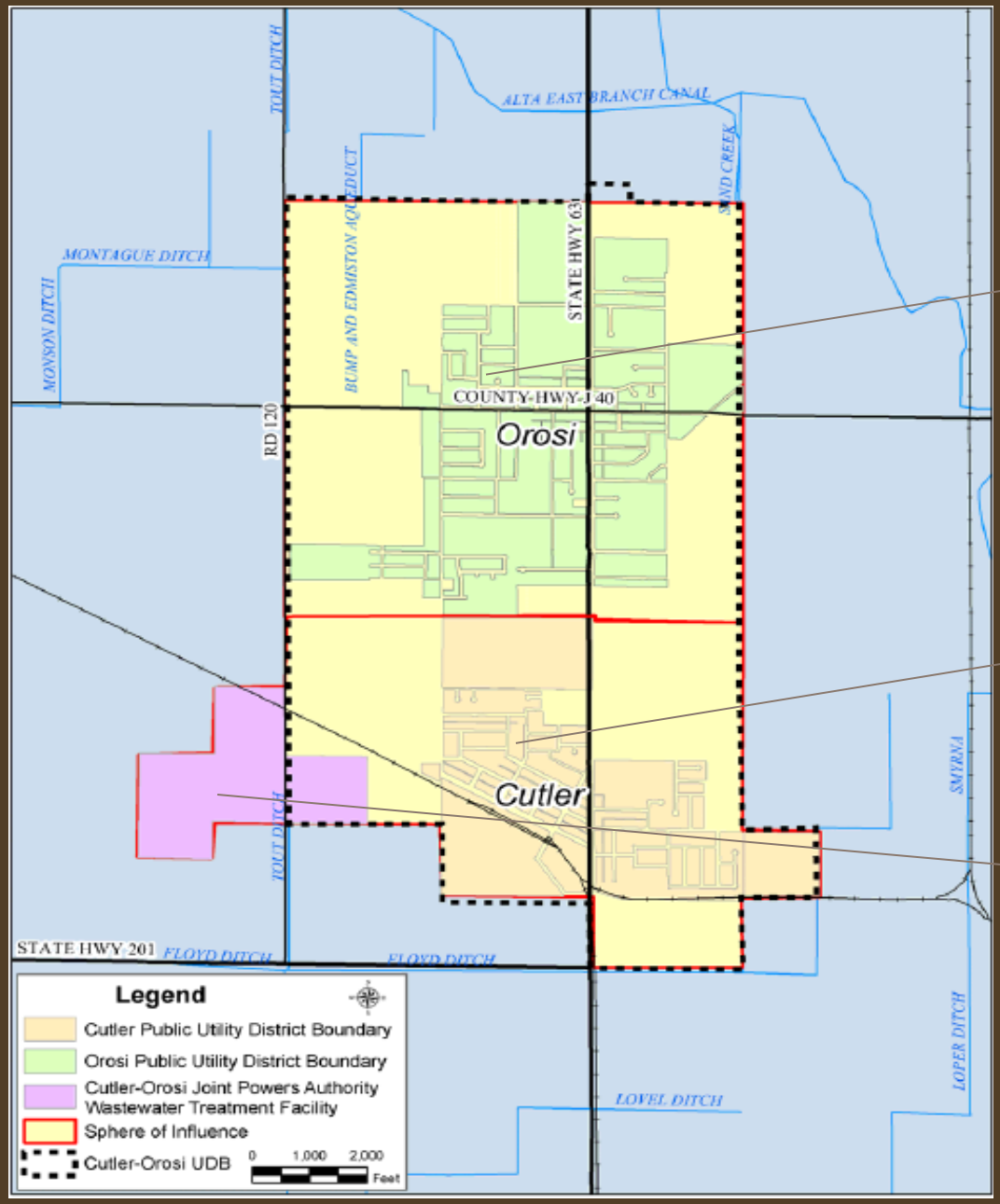
3. Policies to guide future decision making



Cutler-Orosi Aerial Map | Figure

Cutler/Orosi Existing Urban Development Boundary



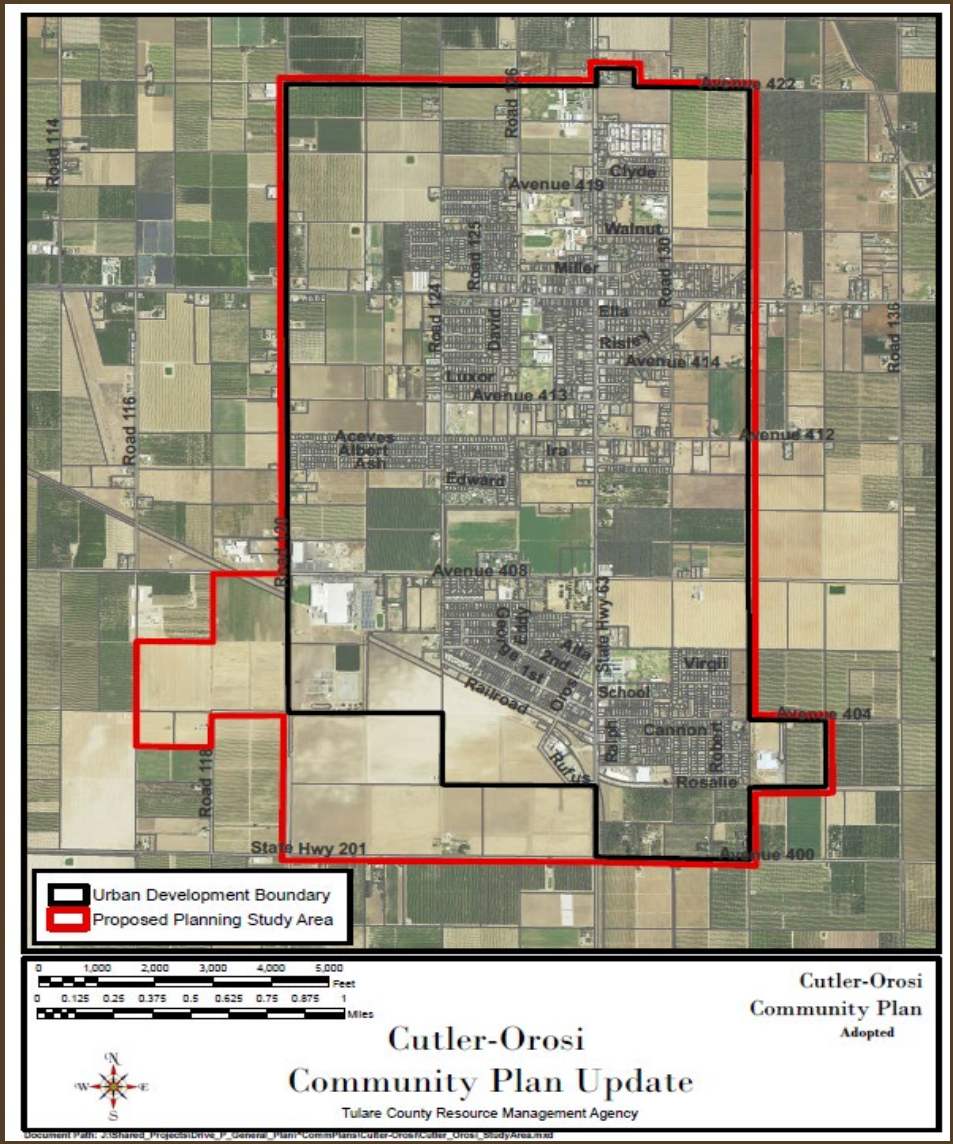


Orosi Public Utility District

Cutler Public Utility District

Cutler/Orosi Wastewater Treatment Facility

Cutler/Orosi Community Plan Update Study Area

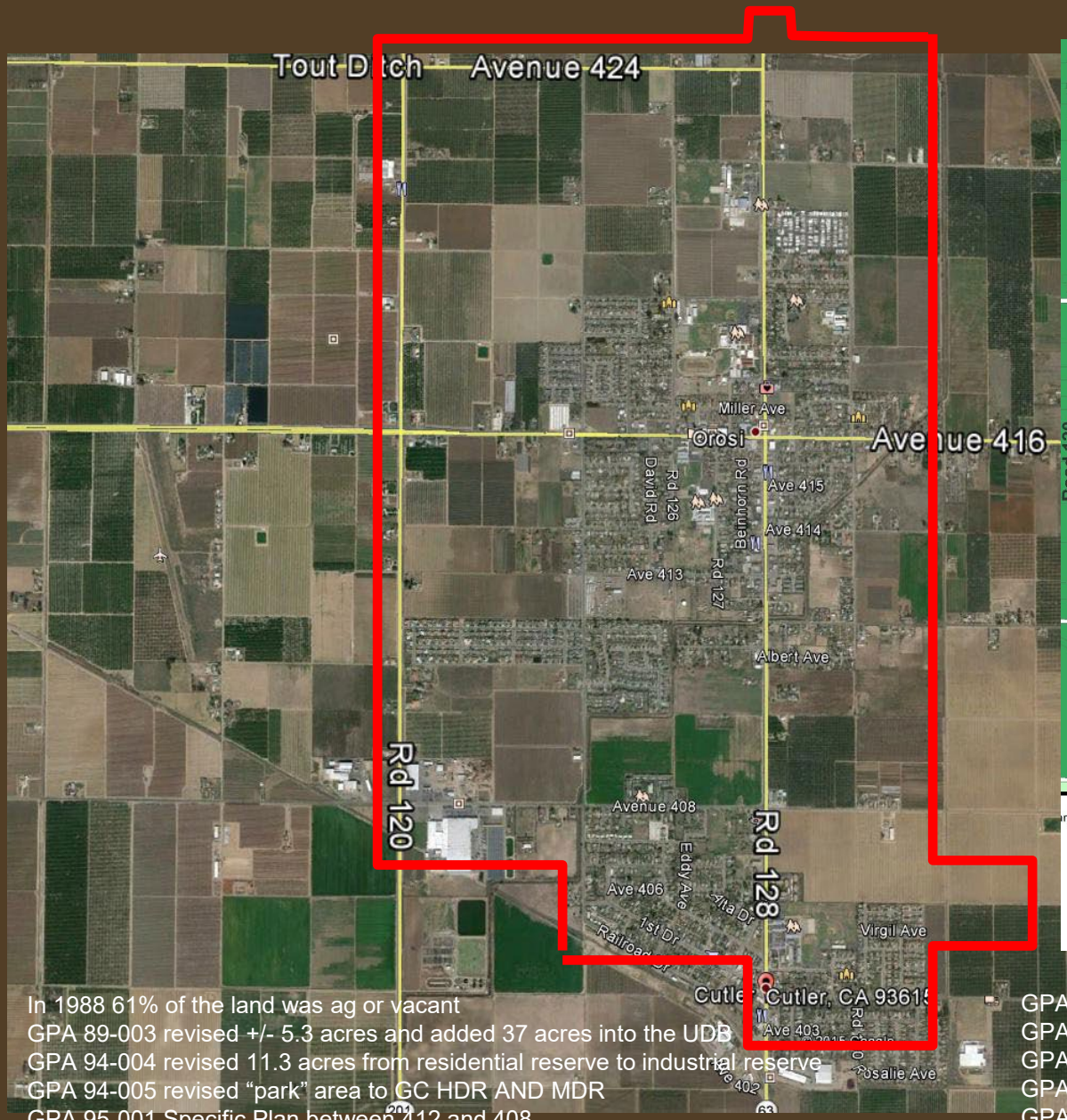


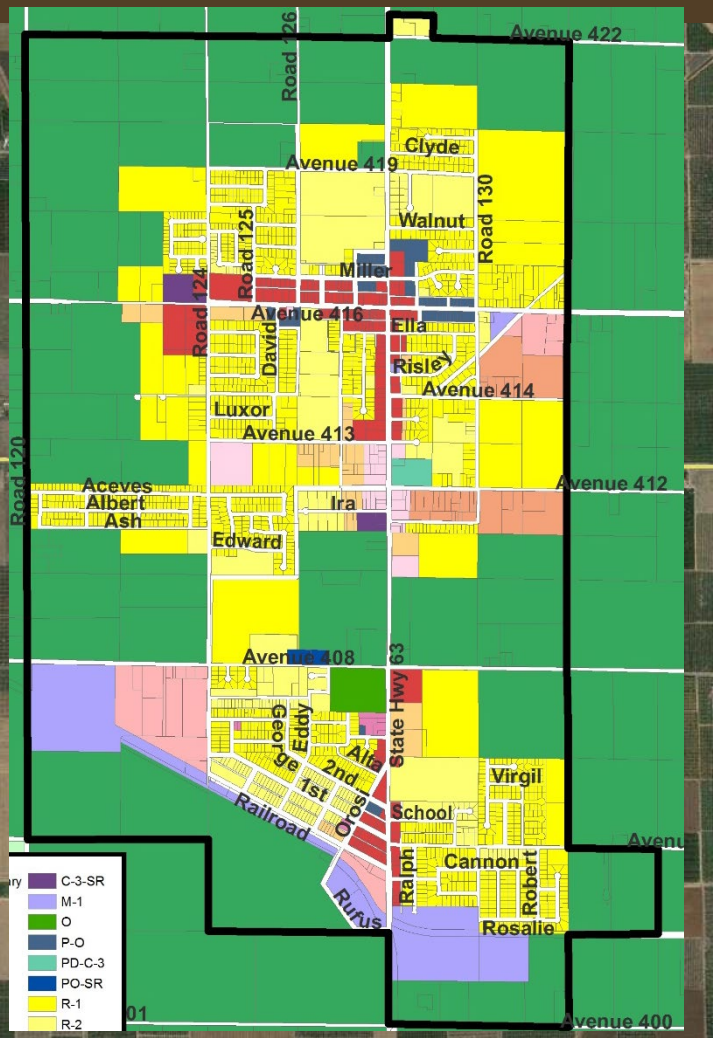
Cutler/Orosi Urban Development Boundary

Existing UDB area = 2,200 acres

UDB area with Study Area Additions = 2,600 acres

UDB increase in area = 400 acres



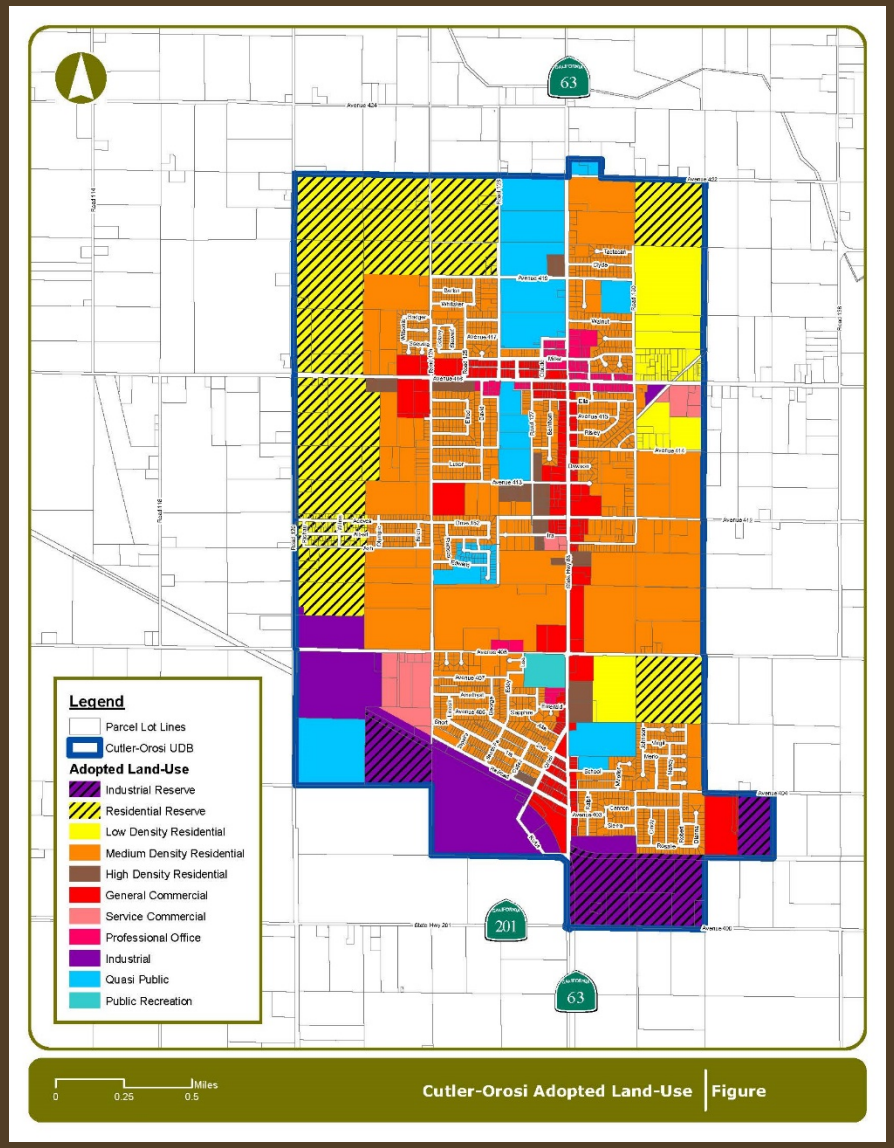


In 1988 61% of the land was ag or vacant
GPA 89-003 revised +/- 5.3 acres and added 37 acres into the UDB
GPA 94-004 revised 11.3 acres from residential reserve to industrial reserve
GPA 94-005 revised "park" area to GC HDR AND MDR
GPA 95-001 Specific Plan between 412 and 408

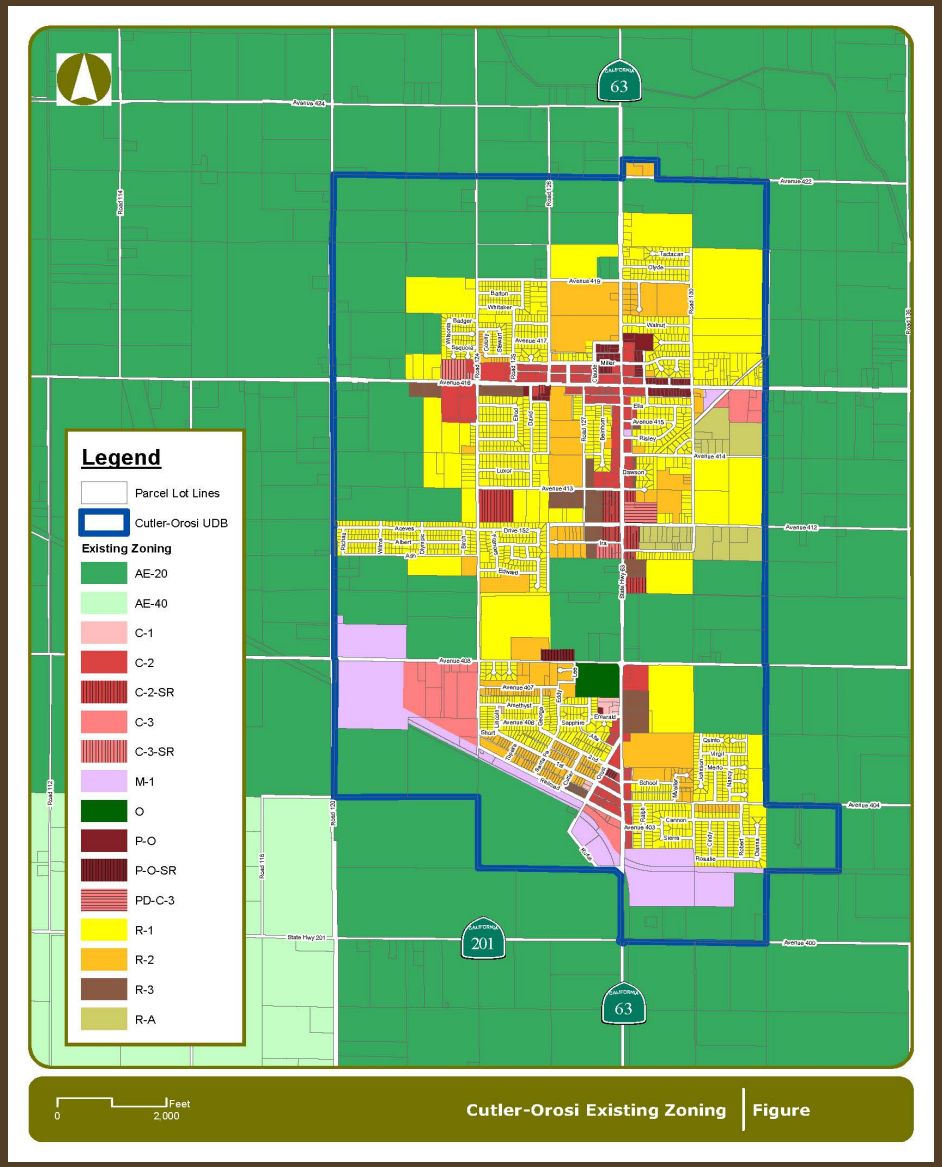
GPA 95-005 revised 9.1 acres MDR to GC
GPA 95-006 revised .61 acres
GPA 98-04 added 5 acres
GPA 00-001 revised 3 acres MDR to Professional Offices
GPA 00-005 revised 2.75 acres

Google earth

Cutler/Orosi Adopted Land Use



Cutler/Orosi Existing Zoning Districts



Complete Streets Cutler

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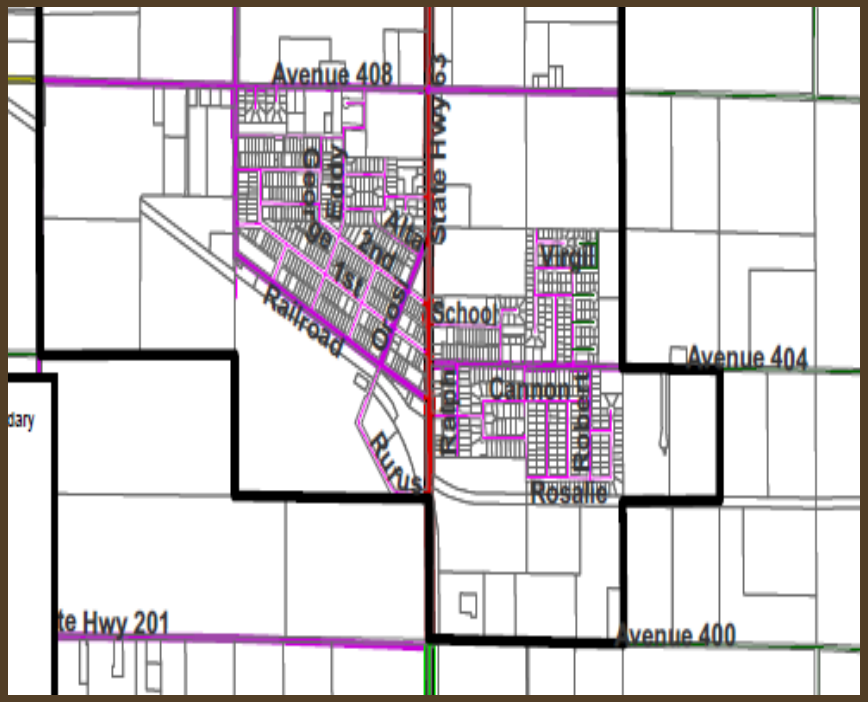

Complete Streets Orosi

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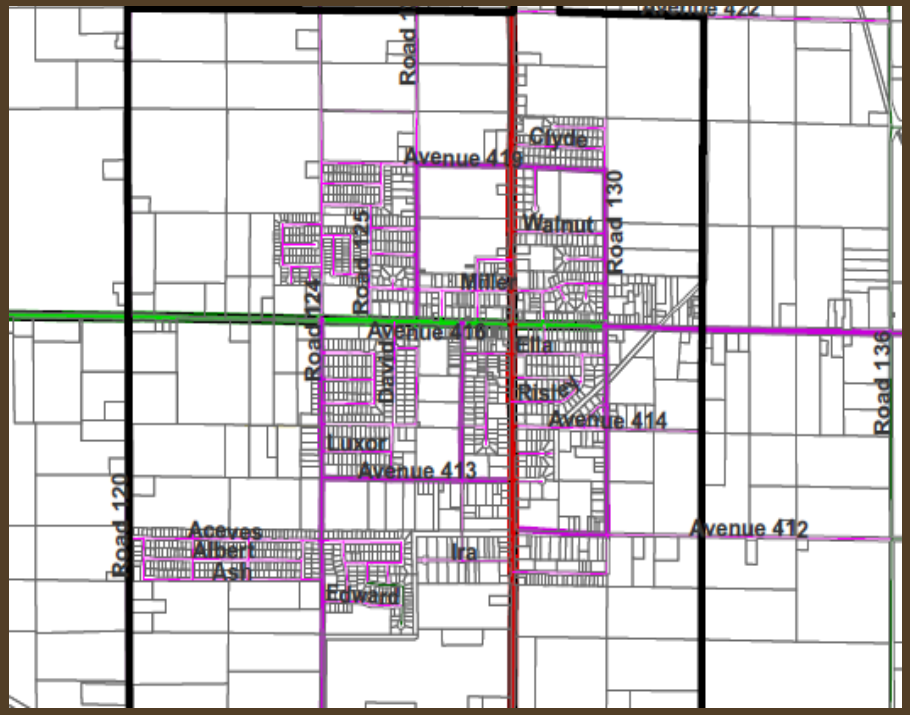

Proposed Complete Streets Projects

Orosi

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5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

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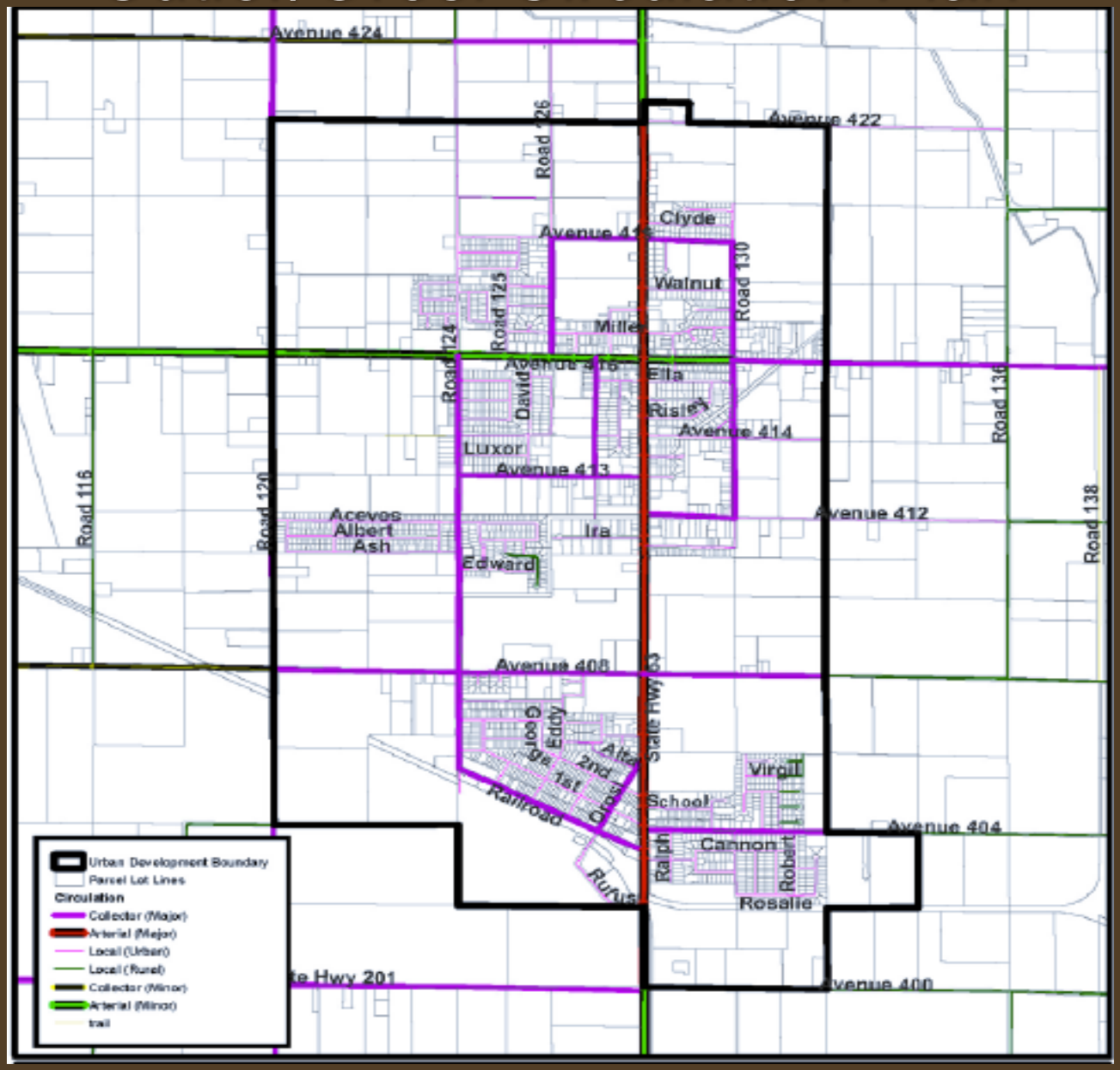
Cutler/Orosi Circulation Plan



Achieving General Plan Goals

- Increasing uses by right
- Creating flexibility for uses through mixed use overlays

NEXT STEPS

- Future Community Plan Workshops.

  May 7, 2019 Golden Valley School 6 P.M.

Thank You for Attending

Aaron R Bock, MCRP, JD, LEED AP
Assistant RMA Director
Economic Development & Planning
5961 South Mooney Blvd.
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(559) 624-7050

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Susan Simon
Planner III
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7126

Cutler/Orosi Joint Unified School District

Located at the District Office Board Room

May 7, 2019, 6:00 pm

Who Attended:
Aaron R. Bock, RMA
David Bryant, RMA
Susan Simon, RMA
Jose Senz, RMA
Johnson Vang, RMA

Pedro Ramirez, CalTrans

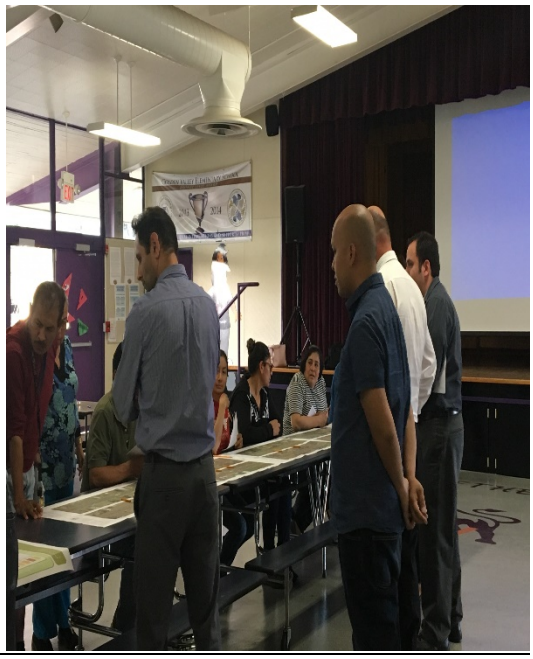
Yolanda Valdez, Superintendent, Cutler/Orosi Joint Unified School District

Approximately twenty (20) members of the community

Materials:

- Maps of Cutler/Orosi roadways
- Cutler/Orosi Land Use Map
- Culter/Orosi Zoning Map
- Cutler/Orosi Urban Development Boundary

Public Comments:



Golden Valley School
Cutler/Orosi Community Plan
May 7, 2019

Name	Address & Email Address	Phone No.
Maria Aguilar	41519 Rd 127 Orosi	559 3971818
Oralia Selix	12199 access ave orosi	559 859 9542
Yesenia Nunez	12847 Ave 412 Apt L orosi	559-723 8045
Rafaela Lucero	12428 Ellis Ave Orosi	559 397 1727
Maria A Ruiz		
Yolanda Valles		
Luis Zavala	12570 AHS AVE OROSI	

Cutler-Orosi Community Plan

2019 Update

Plan Framework

California State Law

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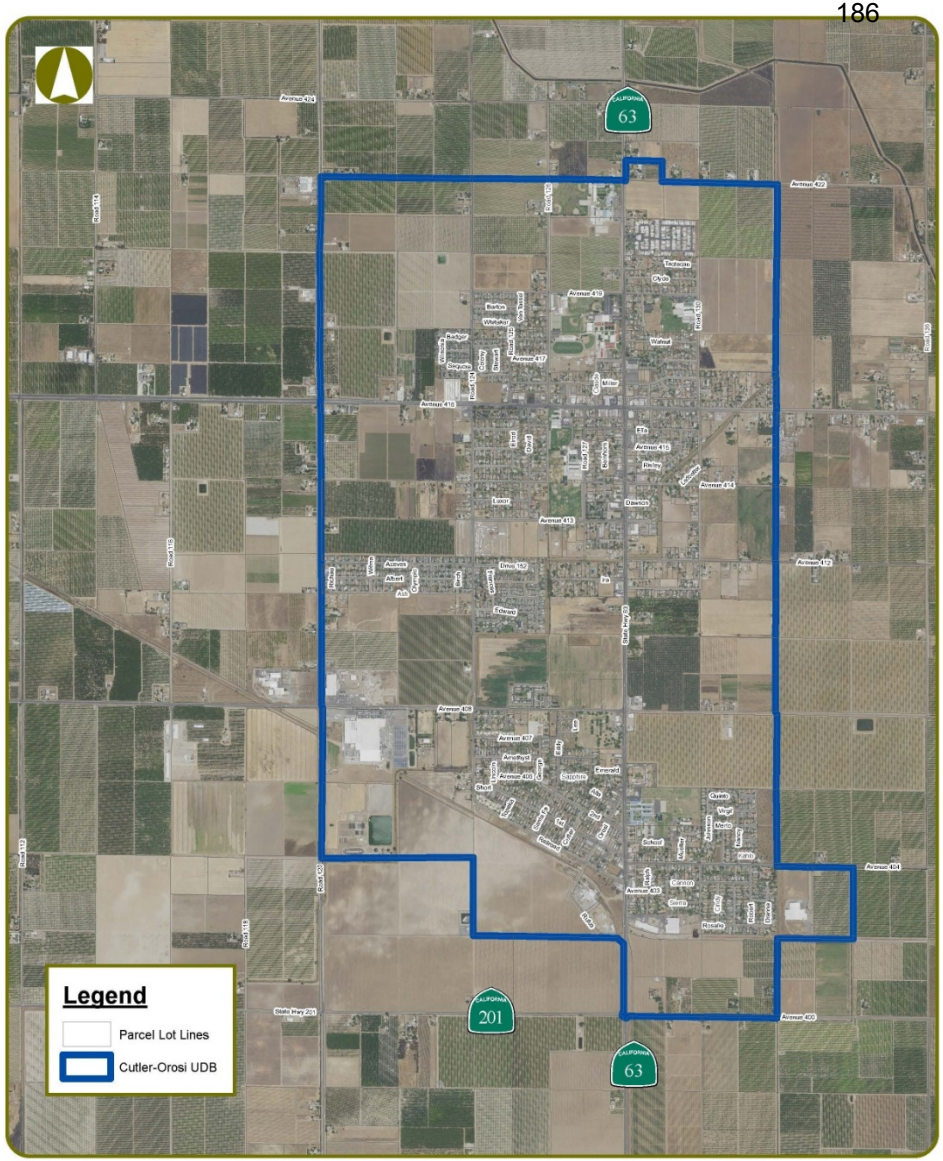
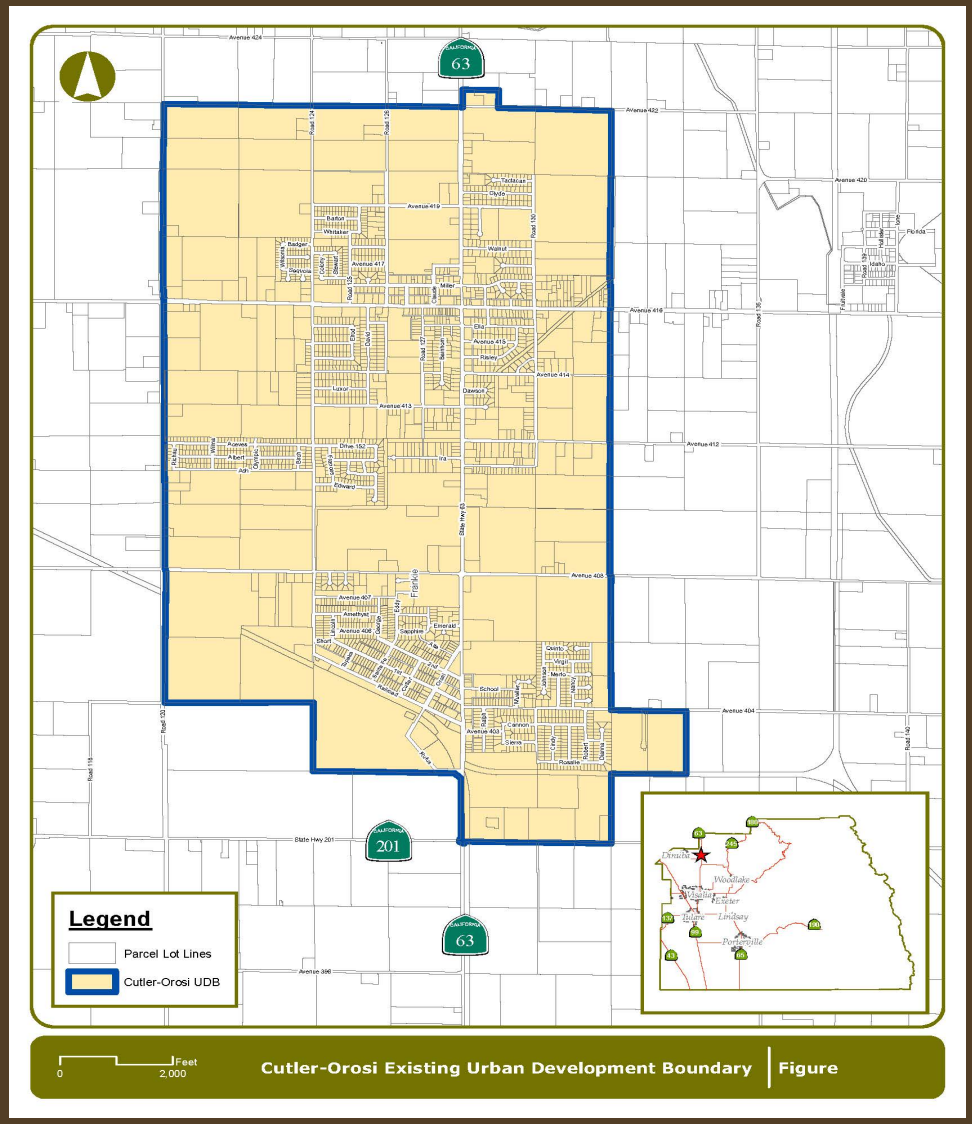
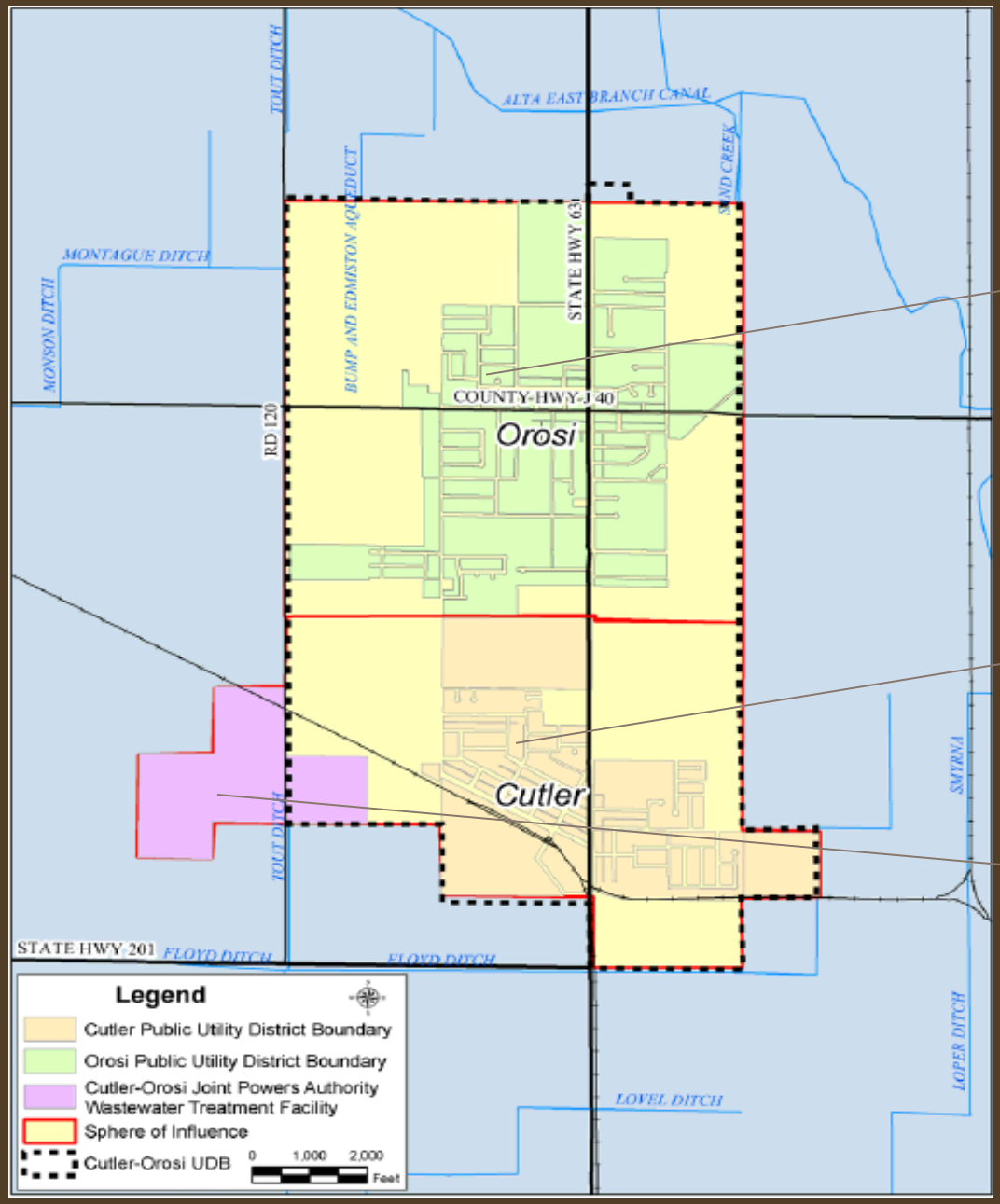


Cutler-Orosi Aerial Map | Figure

Cutler/Orosi Existing Urban Development Boundary



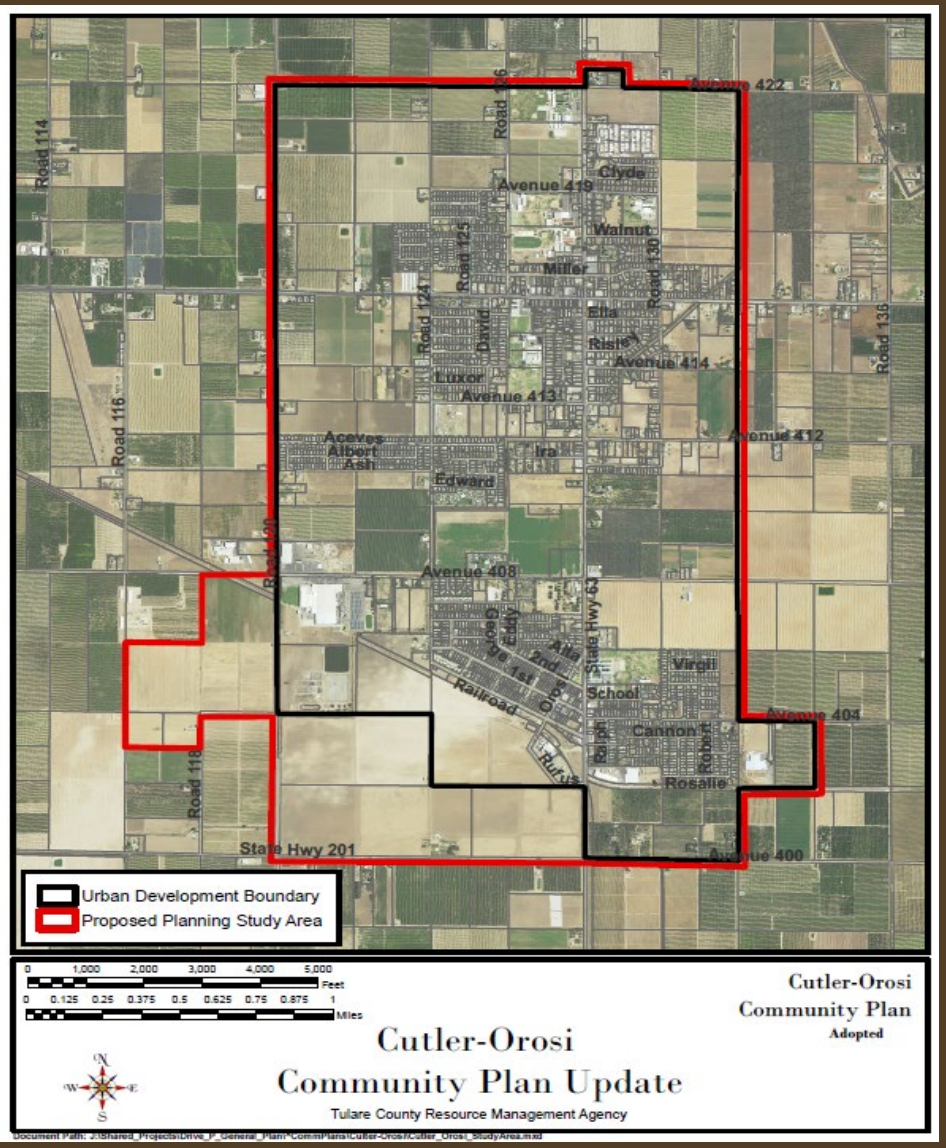


Orosi Public Utility District

Cutler Public Utility District

Cutler/Orosi Wastewater Treatment Facility

Cutler/Orosi Community Plan Update Study Area



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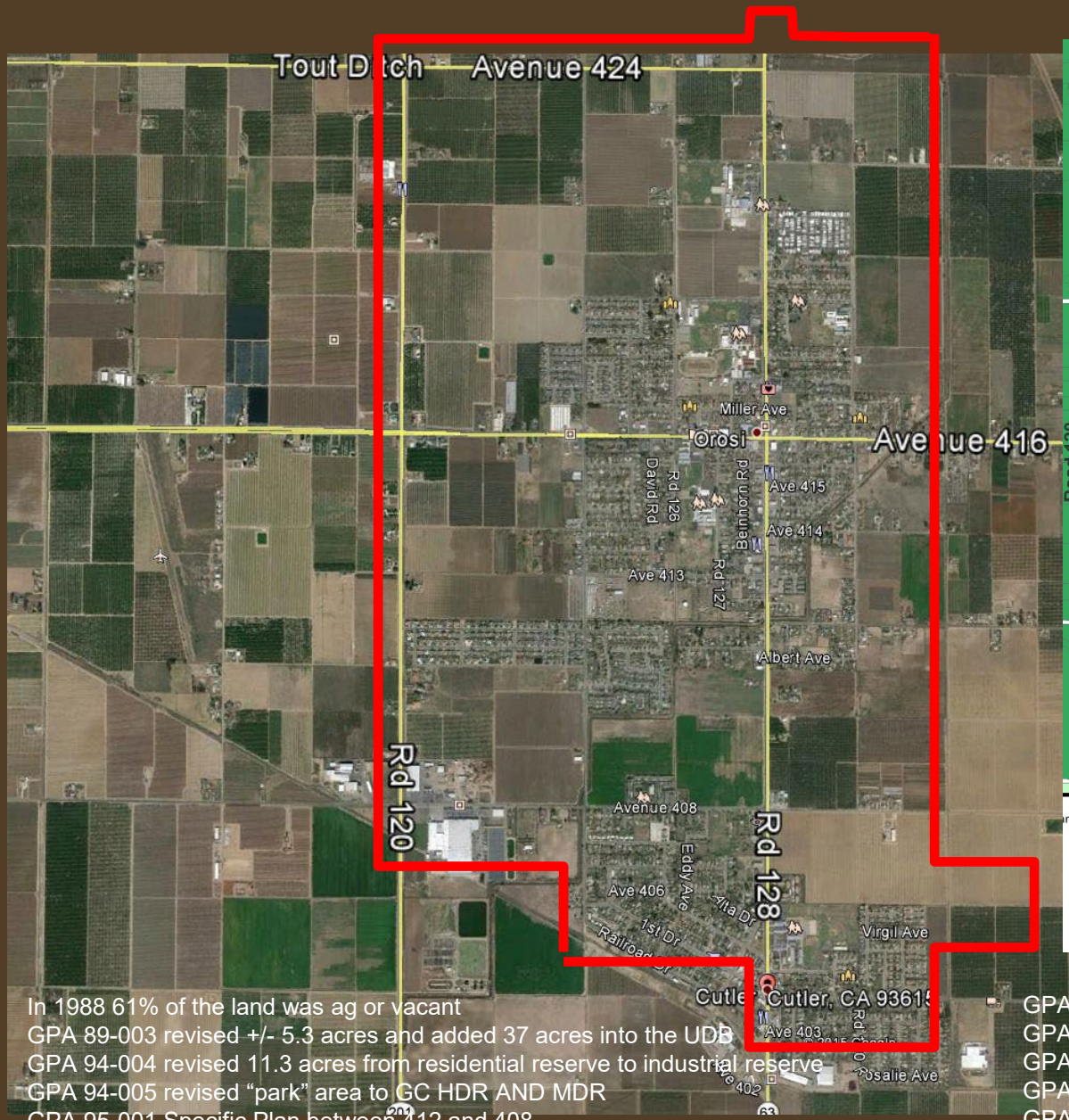

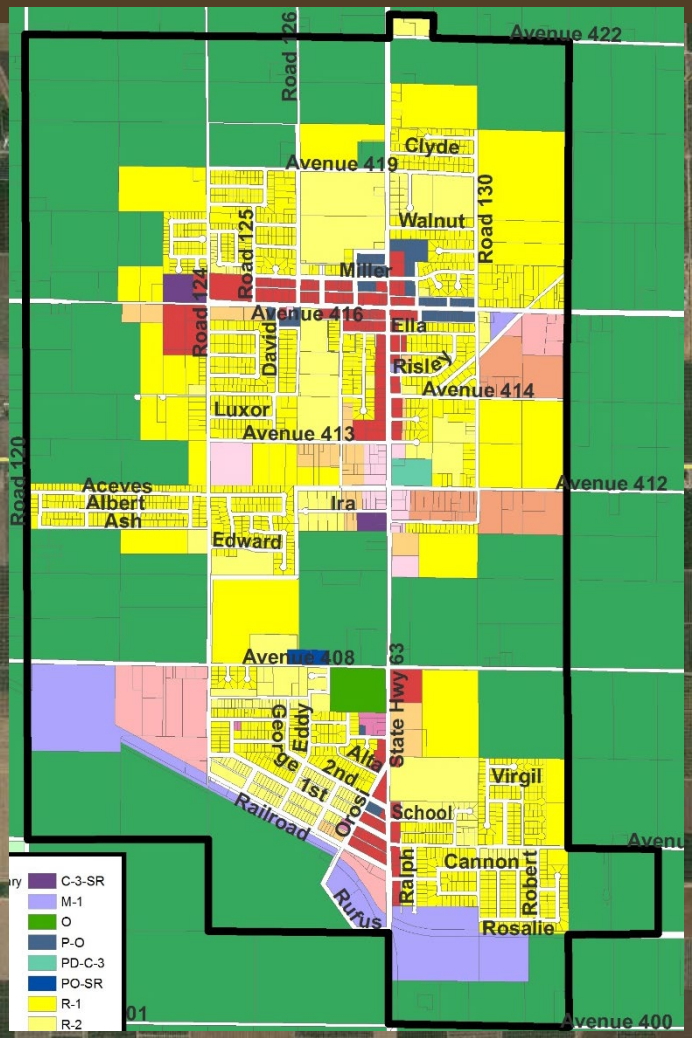

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Google earth

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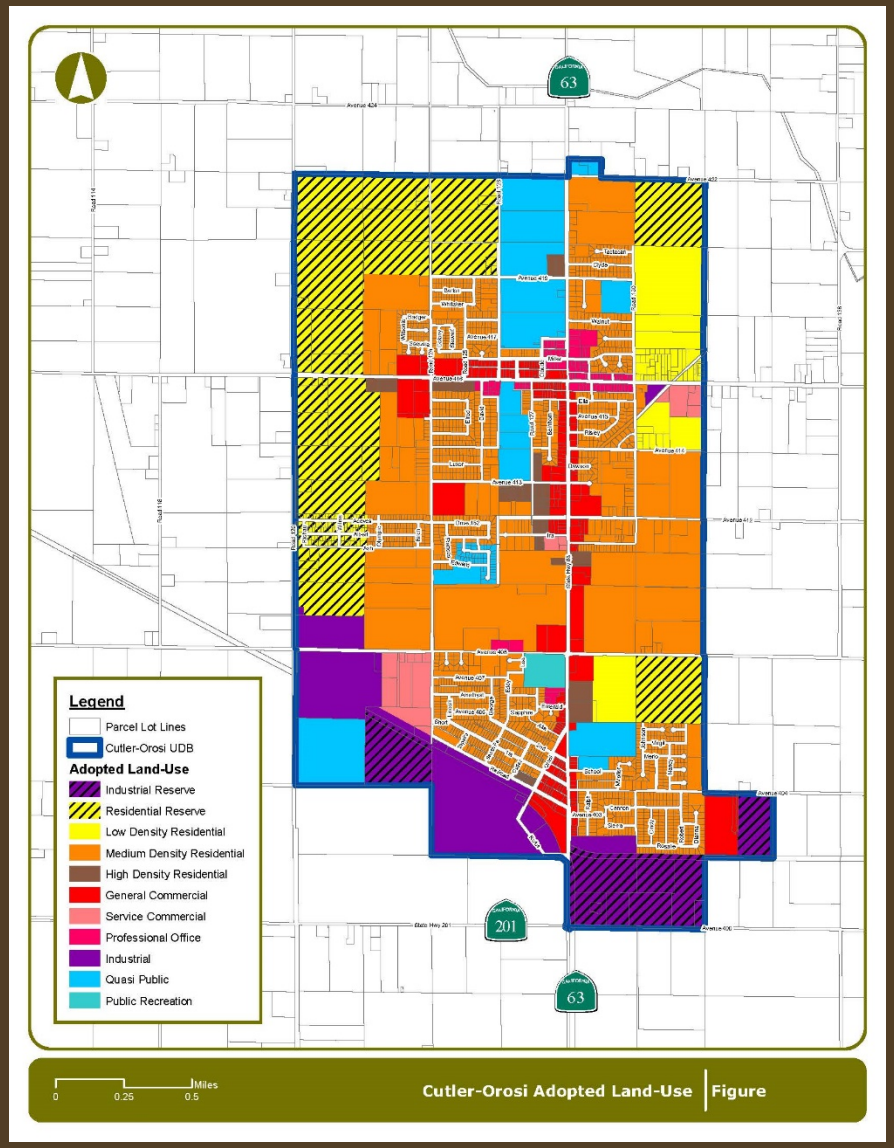
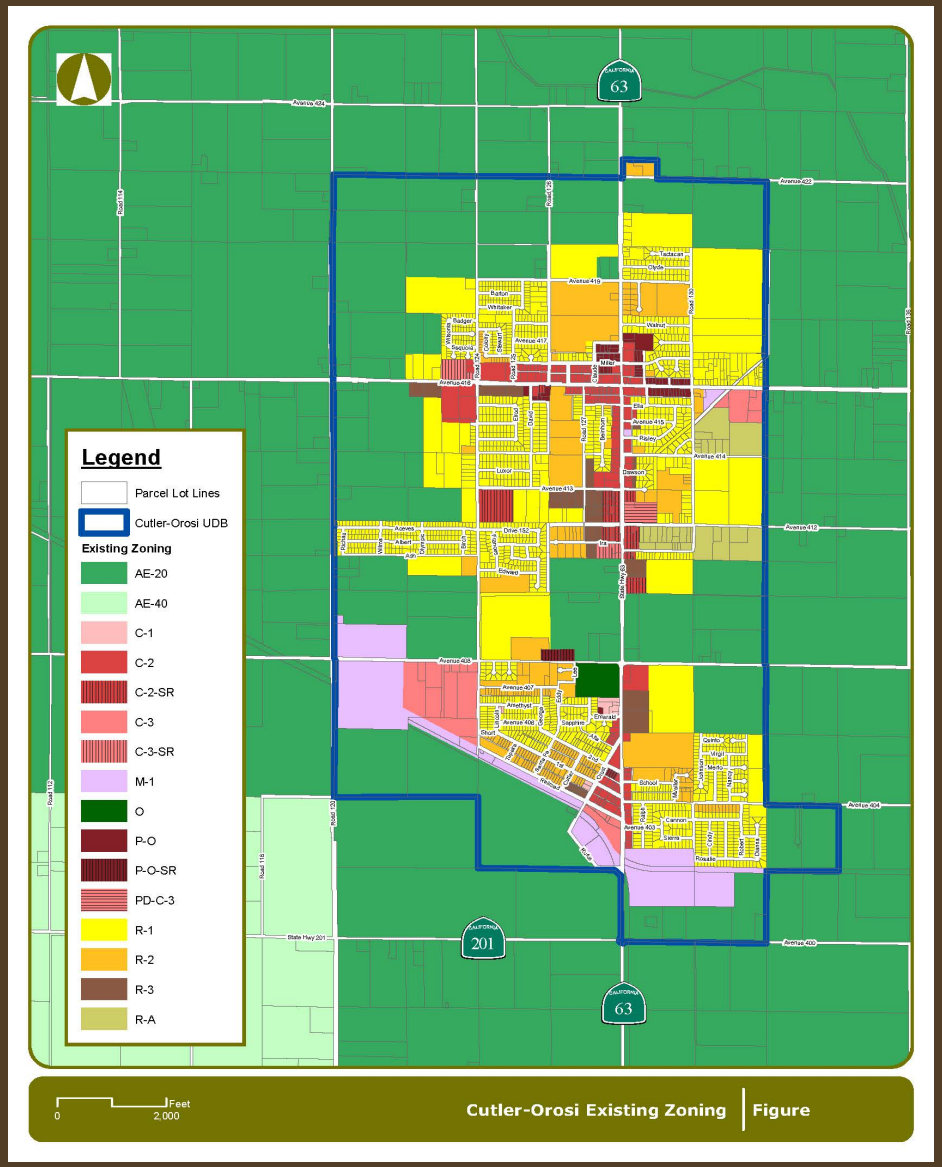


Cutler/Orosi Existing Zoning Districts



State Route 63 (Road 128)/Avenue 412 No Crosswalk

REVISIONS

No.	DESCRIPTION	DATE	BY

COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
559-733-6291
WWW.TULARECOUNTY.CA.GOV/RMA

EXHIBIT
CUTLER–OROSI ROAD 128
TULARE COUNTY

SCALE	–
DIVISION	–
JOB NO.	–
DESIGNED	–
DRAWN	–
CHECKED	–
FILE	CUTLER–OROSI.DWG
DATE	–
SHEET No.	L4
	– OF –

State Route 63 (Road 128) Ella Avenue. Crosswalk Safety
Concerns: Traffic Speed, Sight Distance (on Street Parking) Road Crown

*General Note: Incorporate Caltrans Projects into Complete Streets Plan. Transit Shelters Not currently Utilized for Transit Stops.

REVISIONS

No.	DESCRIPTION	DATE	BY

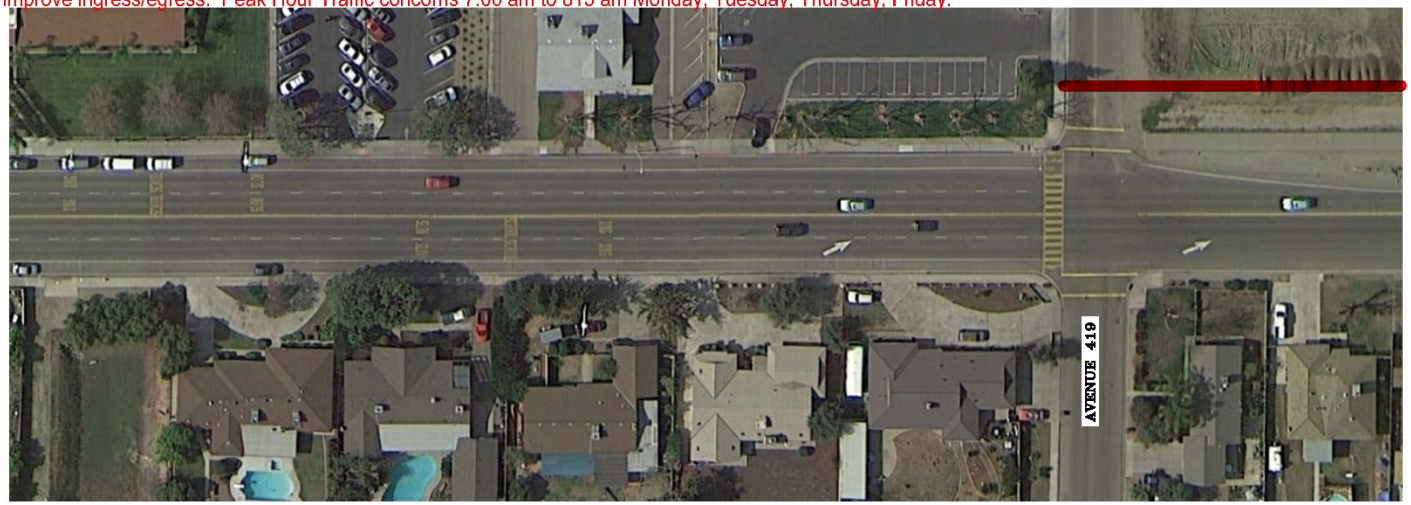
COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

EXHIBIT
CUTLER–OROSI ROAD 128
TULARE COUNTY

SCALE	–
DIVISION	–
JOB NO.	–
DESIGNED	–
DRAWN	–
CHECKED	
FILE	CUTLER–OROSI.DWG
DATE	–
SHEET No.	L5

— OF —

State Route 63 (Road 128) North of Avenue 419 (El Monte Middle School) concerns: Tractor-Trailer Parking, Traffic Speed, Consider Traffic Parking Conmditions to improve ingress/egress. Peak Hour Traffic concorns 7:00 am to 815 am Monday, Tuesday, Thursday, Friday.



N

SCALE: 1"=20'

0 10 20 40

(SCALE ASSUMES 8 FOOT LARGER PLANE)

REVISIONS

No.	DESCRIPTION	DATE	BY

COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
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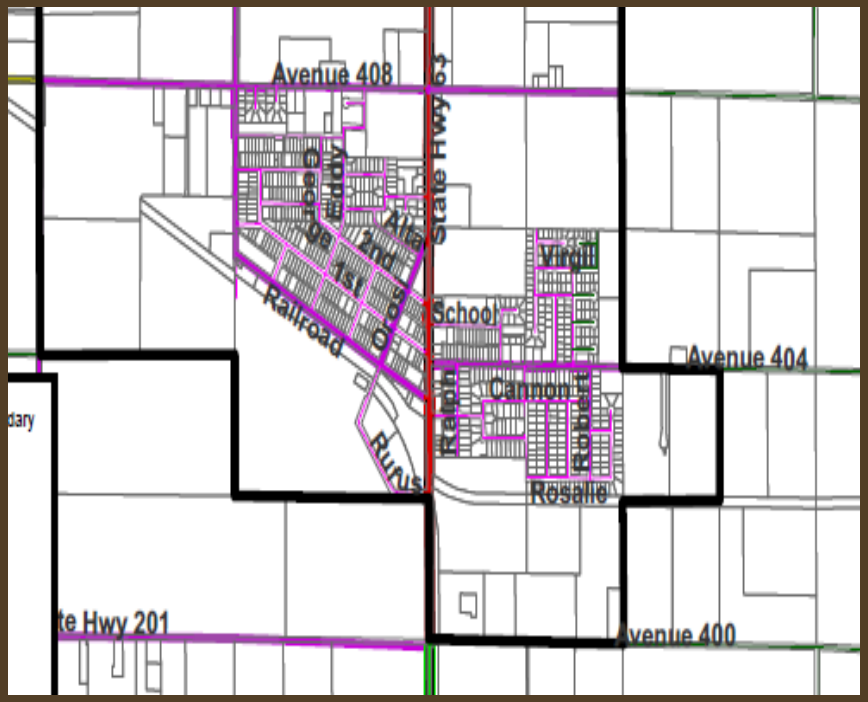
EXHIBIT
CUTLER–OROSI ROAD 128
TULARE COUNTY

SCALE	–
DIVISION	–
JOB NO.	–
DESIGNED	–
DRAWN	–
CHECKED	–
FILE	CUTLER–OROSI.DWG
DATE	–

SHEET No.
L7
– OF –

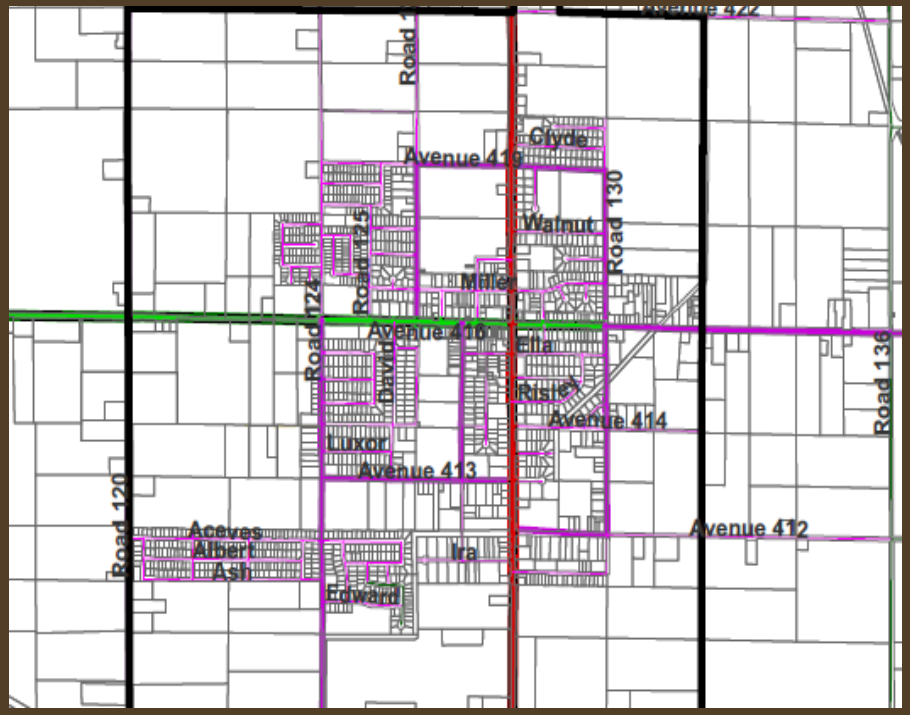
Complete Streets Cutler

- Avenue 404 – SR 63 to Robert Rd
  - Sidewalk, C&G, Drainage
- Avenue 408 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Railroad Drive – SR 63 to Rd 124
  - Sidewalk, C&G, Drainage
- First Drive – SR 63 to Rd 124
  - Striping, Bike Lane



Complete Streets Orosi

- Avenue 416 – SR 63 to Rd 140 (East Orosi)
  - Widening, Sidewalk, C&G, Drainage, Bridges over Sand Creek and Canal, Bike Lane, Transit
  - Multi Use Trail
- Avenue 413 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Avenue 416 – SR-63 to Dinuba
  - Class II bike lane
- Avenue 419



Proposed Complete Streets Projects

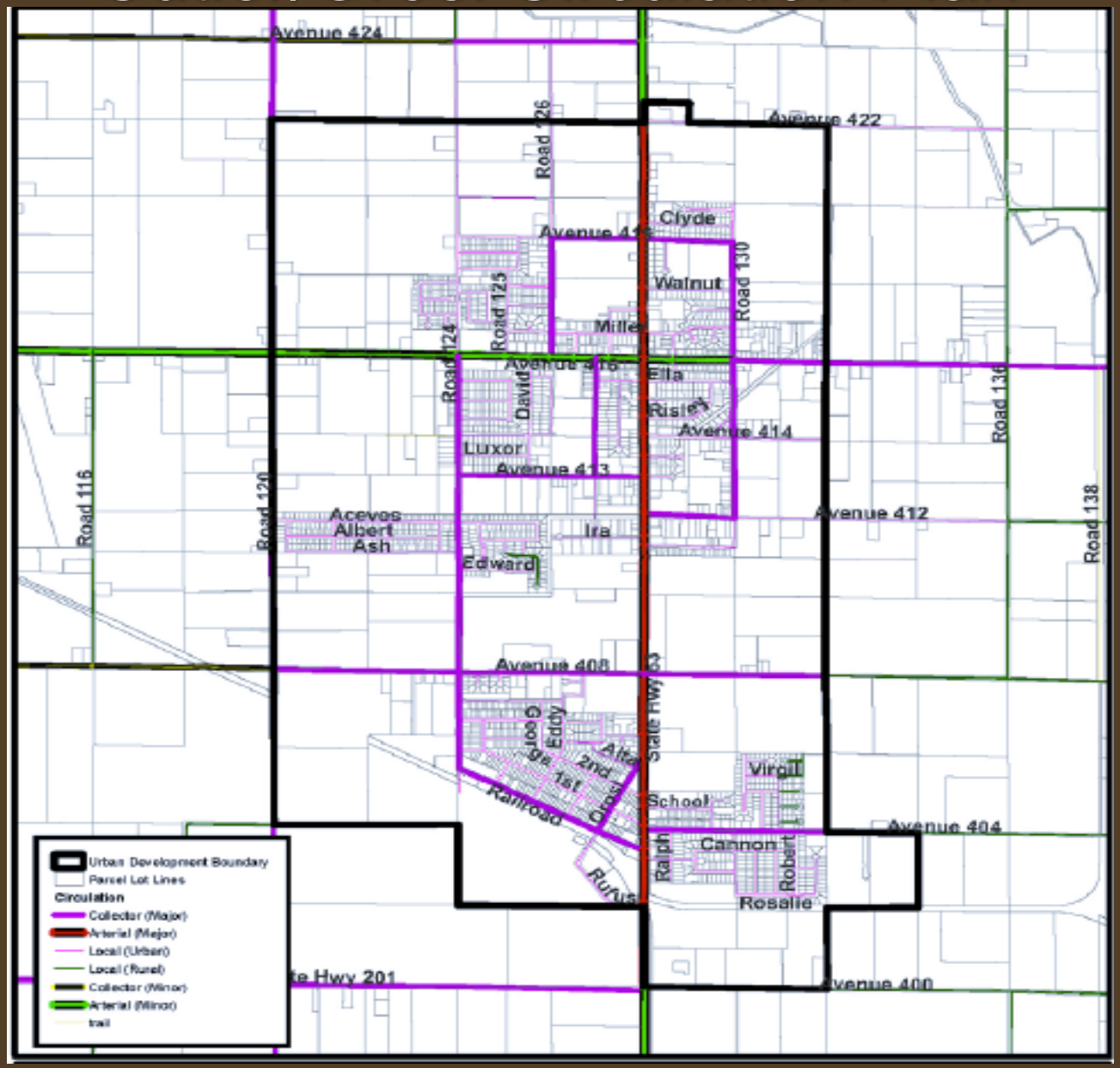
Orosi

1. Avenue 416 – SR 63 to Road 140 (East Orosi)
2. Avenue 413 – Road 124 to SR 63
3. Avenue 419
4. Avenue 416 – SR-63 to Dinuba
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

Cutler/Orosi Circulation Plan



Achieving General Plan Goals

- Increasing uses by right
- Creating flexibility for uses through mixed use overlays

Thank You for Attending

Aaron R Bock, MCRP, JD, LEED AP
Assistant RMA Director
Economic Development & Planning
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7050

David Bryant, MPA
Chief Planner, Special Projects
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7130

Susan Simon
Planner III
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7126

AGENDA

MEETING OF THE
BOARD OF DIRECTORS OF THE
OROSI PUBLIC UTILITY DISTRICT

For the meeting of: Orosi Public Utility District

Tuesday, April 9, 2019

Location: Orosi Public Utility District Office
12488 Avenue 416
Orosi, California

Time: 6:30 P.M.

President:	Alex Marroquin
Vice-President	Johnny Sandoval
Director:	Anthony Rubalcaba
Director:	María C. González
Director:	Romelia Castillo

As a courtesy to those in attendance, please turn off or place in alert mode all cell phones and pagers.

Public Comment Period: The Board has adopted guidelines related to the conduct of public meetings and related District business. An excerpt of the meeting rules, procedures and time limits is included at the end of this Agenda as Attachment 1. You may use a total of five minutes (including interpreter time) divided as you like to speak to the Board regarding items of interest during the public comment time for items that are not on the agenda, or before or during discussion of agenda items, or before closed session items on agenda for closed session. Sometimes the Board may find it necessary or helpful to talk longer with you. In that situation, the Board President may choose at his option to let you speak for more time.

NOTICE OF ADA COMPLIANCE: In accordance with the Americans with Disabilities Act (**ADA**), if you need assistance to participate in this meeting, please contact the District office at 559 528-4262 to request a reasonable accommodation.

DOCUMENTS: If distributed to the Board less than 72 hours before a regular meeting, any public records which are subject to public inspection and pertain to an open-session item on the regular meeting agenda shall be available at the following address at the time they are distributed to a majority of the Board: 12488 Avenue 416, Orosi, California 93647. Public records distributed to the Board at a public meeting will be available to the public at such meeting if they were prepared by the District. Exemptions and details in Government Code § 54957.5(a) shall apply.

PLEDGE OF ALLEGIANCE

I pledge allegiance to the flag of the United States of America, and to the republic for which it stands: one nation under God, indivisible, with liberty and justice for all.

Action taken: _____
Moved:_____; Seconded:_____
Ayes:_____Noes:_____Abstain:_____Absent:_____

B. Delinquent Accounts for April, 2019/Cuentas Morosas para el mes de April del 2019:

Staff will present a list of the delinquent accounts and requests for extension of time for the month of April, 2019.

Recommended Motion: I move to deny extensions of time to bring delinquent accounts current.

Action taken: _____
Moved:_____; Seconded:_____
Ayes:_____Noes:_____Abstain:_____Absent_____

C. Approval of Claims/Aprobación de Pagos:

Staff will present a list of claims to be approved and paid for the month of April, 2019.

Recommended Motion: I move to approve the claims as presented for the month of April, 2019.

Action taken: _____
Moved:_____; Seconded:_____
Ayes:_____Noes:_____Abstain:_____Absent:_____

D. Drinking Water Fill Station/Estacion de Llenado de Agua Potable:

This matter was tabled from the last meeting. The Kings River Water Quality Coalition had made a request for capacity service for a drinking water fill station. Information will be provided explaining the request, the reason for the request and the payment expectations of the Coalition relative to the service request.

Recommended Motion: Board discretion.

Action taken: _____
Moved:_____; Seconded:_____
Ayes:_____Noes:_____Abstain:_____Absent:_____

E. Car Wash fund raisers/Recaudador de fondos de lavado de coches:

Staff was asked regarding car wash fund raisers, if they will be permitted this year or not.

i. An update will be given as to what has taken place regarding the Reginal Facilities in the last month.

Recommended Motion: Board discretion.

Action taken: _____
Moved:_____; Seconded:_____
Ayes:_____Noes:_____Abstain:_____Absent:_____

6. **CLOSED SESSION/Sesion Cerrada**

The items to be considered by the Board in closed session are listed below. If you desire to comment on any of these items prior to the Board's adjournment to consider them, please step to the podium:

A. **PENDING LITIGATION** (Government Code § 54956.9). It is the intention of this governing body to meet in closed-session concerning: Es la intención de la mesa directiva de reunirse en sesión cerrada para:
Conference with legal Counsel – **EXISTING LITIGATION** (Government Code § 54956.9(d)(1).

Parties, case/claim no. Orosi Public Utility District v. The Dow Chemical Company et. al (San Francisco Superior Court Case No. CGC-12-522826)

B. **PENDING LITIGATION** (Government Code § 54956.9). It is the intention of this governing body to meet in closed-session concerning: Es la intención de la mesa directiva de reunirse en sesión cerrada para:
Conference with legal Counsel – **EXISTING LITIGATION** (Government Code § 54956.9(d)(1).

Parties, case/claim no. Orosi Public Utility District v. Shirley L. Dawson et. al (Tulare County Superior Court Case No. 185586)

C. **PENDING LITIGATION** (Government Code § 54956.9). It is the intention of this governing body to meet in closed-session concerning: Es la intención de la mesa directiva de reunirse en sesión cerrada para:
Conference with legal counsel – ANTICIPATED LITIGATION (Government Code § 54956.9(d)).

Initiation of litigation (Government Code § 54956.9(d)(4)).
Number of potential cases is: 2.

7. **CLOSED SESSION ANNOUNCEMENTS, IF ANY/Anuncios de Sesion Cerrada, si los hay**

Upcoming Board Meetings: Juntas Futuras

Regular Meeting: May 14, 2019
Regular Meeting: June 11, 2019
Regular Meeting: July 9, 2019

ATTACH 1 – Excerpt-Res.2007-02:5.CONDUCT OF MEETINGS:/Conduccion de las Asambleas (Juntas):

a. It is the objective of the Board to conduct the business of the District in a manner that strikes a balance between efficient, productive proceedings, and the desire of the public to observe and participate in same.

b. To further the goal stated in 5(a), the Board will request that each person wishing to comment on:

i. Non-Agendized Items: speak during the "unscheduled public comment" period;

ii. Public Hearing items: speak at the time the matter is opened to public hearing;

iii. Pursuant to Request to Appear Form: speak during the "scheduled public comment" period;

iv. Board Agendized Items: speak at the time such item is called.

v. Closed Session Agendized Items:speak when the Board announces it will adjourn to closed session.

c. When participating in any portion of the meeting, the Board requests the comments stay focused to the issue at hand, and that they be presented in a civil, courteous, constructive and non-duplicative manner.

d. To further the goals of this resolution and the goal stated in 5(a), the following time limits will apply subject to the Board's discretion to extend if the Board deems it helpful to its full and complete understanding:

i. Public Comment Period (Non-Agendized Items): Unscheduled public comments will be allotted a total of thirty (30) minutes.

ii. Public Hearings: Public hearings will be allotted approximately fifteen (15) minutes, or as much time as the Board deems necessary to conduct a thorough hearing on the subject matter.

iii. Time Allotted to Each Speaker: Each speaker shall be allocated five (5) minutes (including interpreter) for participation in the Board's meeting. This 5-minute per person allocation is not intended to extend the thirty minute unscheduled comment period. This is also not intended to authorize a public comment period at special meetings as such meetings are limited to the business agendized to be transacted at the meeting. However the public will be provided with an opportunity to address the Board on any item described in such notice of special meeting before or during consideration of such item(s).

iv. Use of Allotted Time for Each Speaker: A speaker may use his/her five minutes at one time or may divide his/her five minutes between or among the following: Public Comments – Unscheduled Items, Public Comments – Scheduled Items, Regular Agenda Items, Public Hearings, or to comment regarding agendized Closed Session Items. A person may exercise use of his/her time by stepping to the podium at the time the matter is called and waiting to be addressed by the Board's President.

v. Timer: An individual designated by the Board will perform the time keeping and will try to inform the speaker when he/she has thirty (30) seconds remaining and when five minutes are expired.

vi. Time May Not Be Transferred/Waived: A speaker has no authority to transfer or waive time to another speaker. Any time extensions will be in the discretion of the Board.

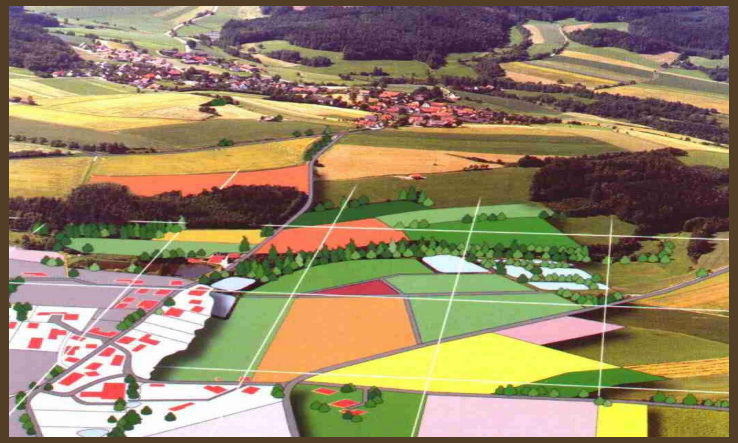
vii. Interpreter: To accommodate maximum meeting participation, the Board, in its discretion and in response to community requests, employs a Spanish speaking interpreter to attend and interpret Board meetings. To advance the objective of maximum participation and provide equal time to all speakers, speakers will not be allowed to pick and choose whether to have their comments interpreted and the interpreter's participation shall not extend the five minute time period allocated to each speaker. In the event the Board determines that remaining members of the public do not require interpretation services, the Board in its discretion may excuse the interpreter.

e. Disruptions and disorderly conduct deprive the community of their right to participate and the Board of its duty to conduct business. Therefore, disorderly participant(s) will initially be courteously reminded of the goals and processes established by the Board. If the disruption continues, the disorderly participant(s) will be asked to leave. If the disorderly participant(s) refuse to leave, the meeting room will be cleared and the meeting will continue in session. Representatives of the press or other news media, except those participating in the disturbance, shall be allowed to attend the "cleared meeting" session. If the Board is unable to remove the disorderly participant(s) or clear the room, it will contact local law enforcement for assistance. The process outlined here is intended to meet, and exceed the requirements of Government Code section 54957.9.

Cutler-Orosi Community Plan

2019 Update

Goals and Policies



Decision Making

Plan Framework

California State Law

Planning Principles

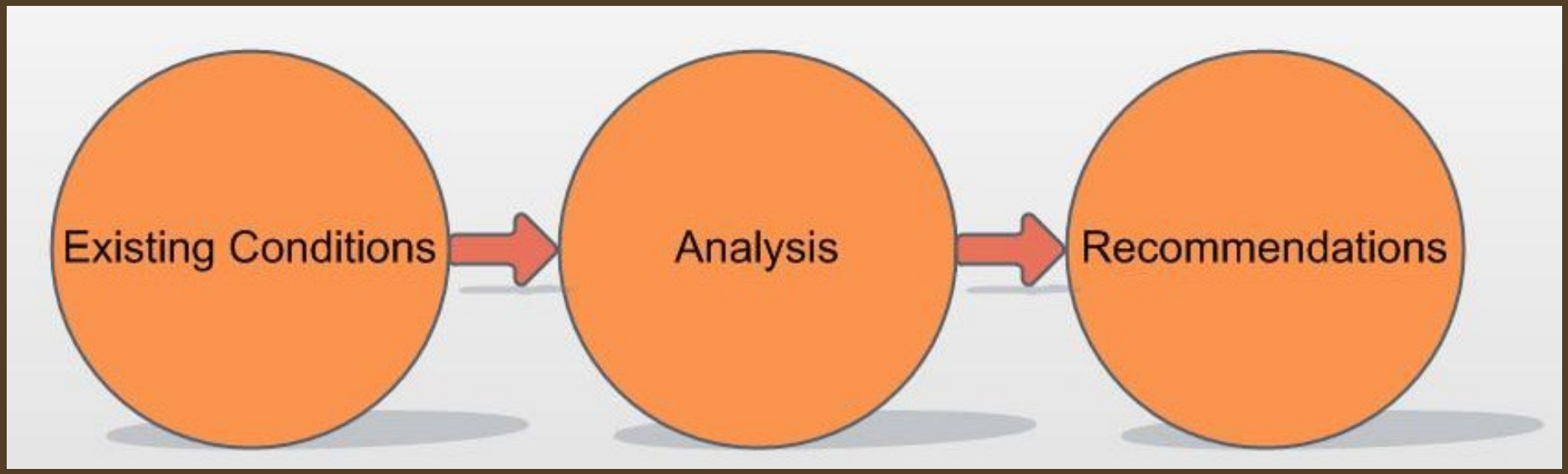
Tulare County General Plan Policies

Schedule

Cutler-Orosi Community Plan - Update and Environmental Impact Report (EIR)

	Tasks	Schedule and Milestones										
1	**Preliminary Work**											
1.1.	General Plan Initiation BOS September 30, 2014, Resolution No. 2014-0688											
1.2.	CEQA Consultant RFPs											
2	**Constraints Assessment**											
3	**Needs Assessment**											
4	**Prepare Policy Plan (0-5yrs, 5-10yrs, 10 to 17 yrs to GP)**											
5	**Environmental Impact Report (EIR)**											
6	**Planning Commission Hearing**											
7	**Board of Supervisors Hearing**											
8	**Project Completion**											

Community Plan Process



Community Conditions

Staff / Consultant Research

Citizen Participation

Sharing Concerns

Addressing Concerns

Analysis and Recommendations

One Major Purpose

1. Guide Future Decision Making

Two Primary Documents

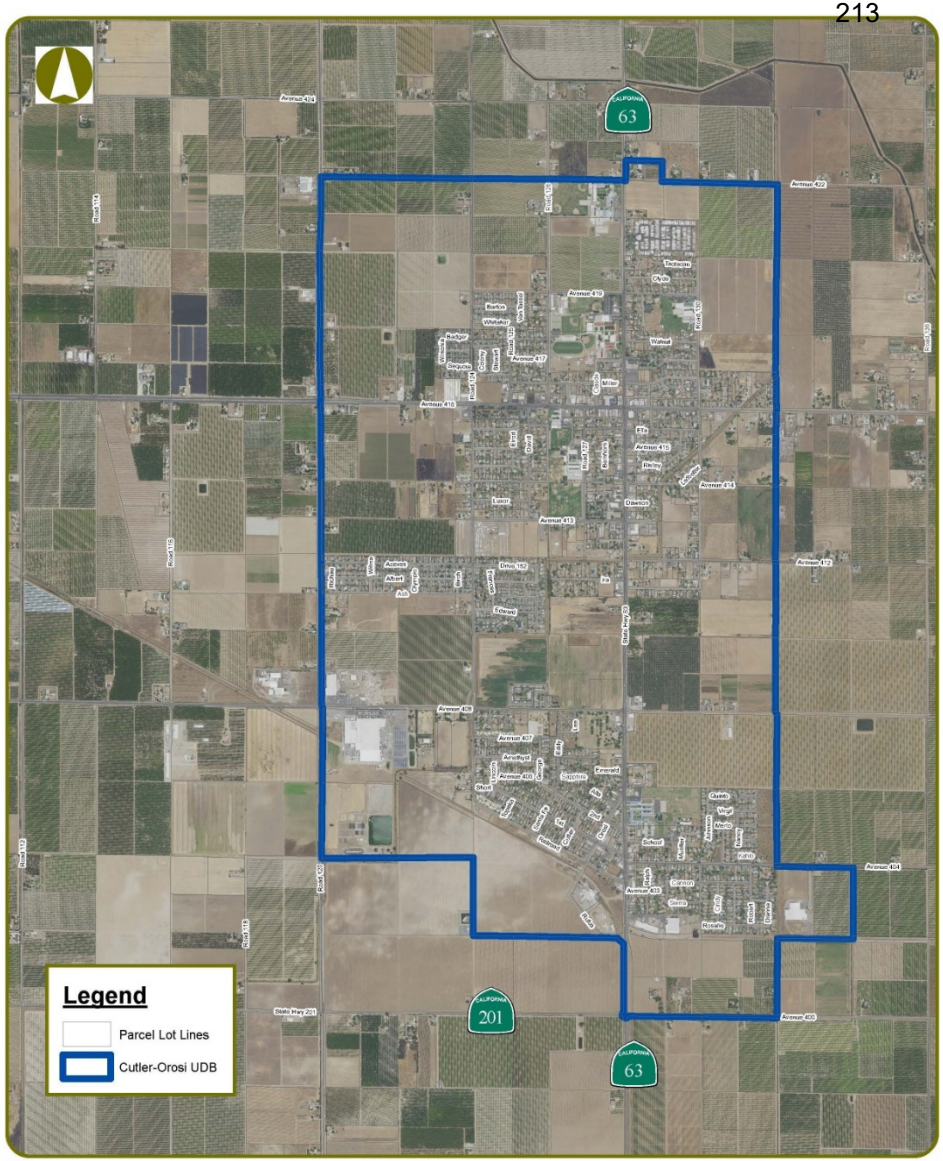
1. Cutler-Orosi Community Plan
2. Environmental Impact Report (EIR)

Three Primary Components

1. Urban Development Boundary

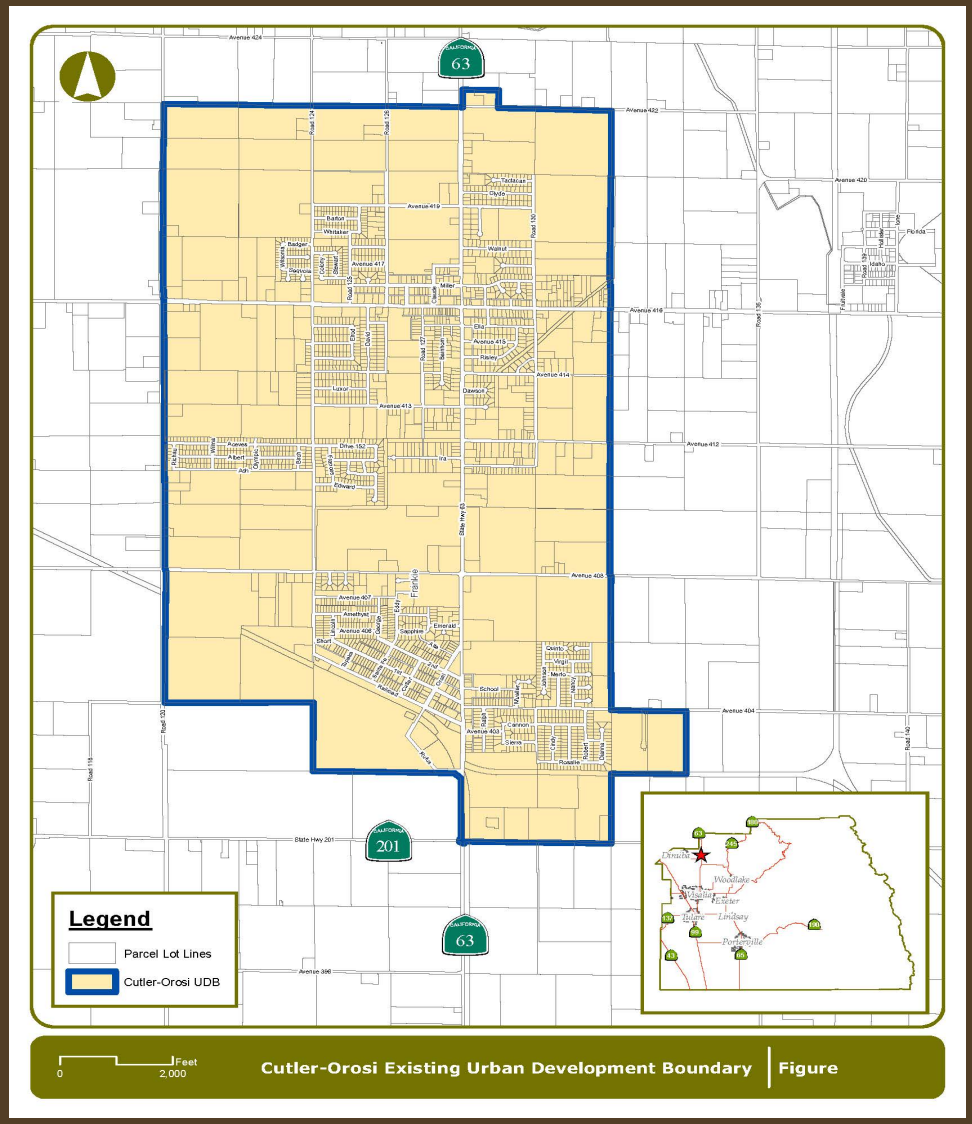
2. Land Use and Circulation

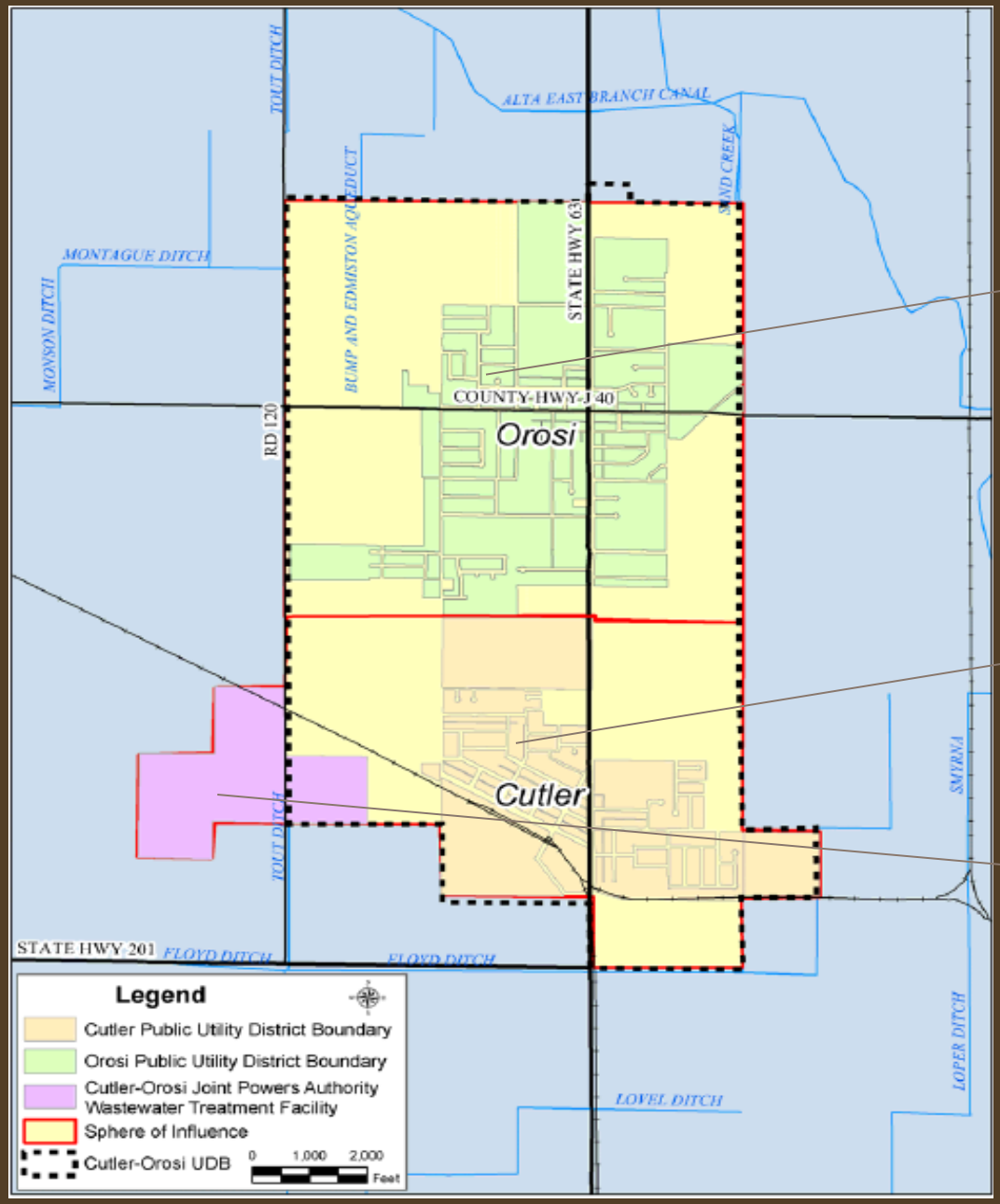
3. Policies to guide future decision making



Cutler-Orosi Aerial Map | Figure

Cutler/Orosi Existing Urban Development Boundary



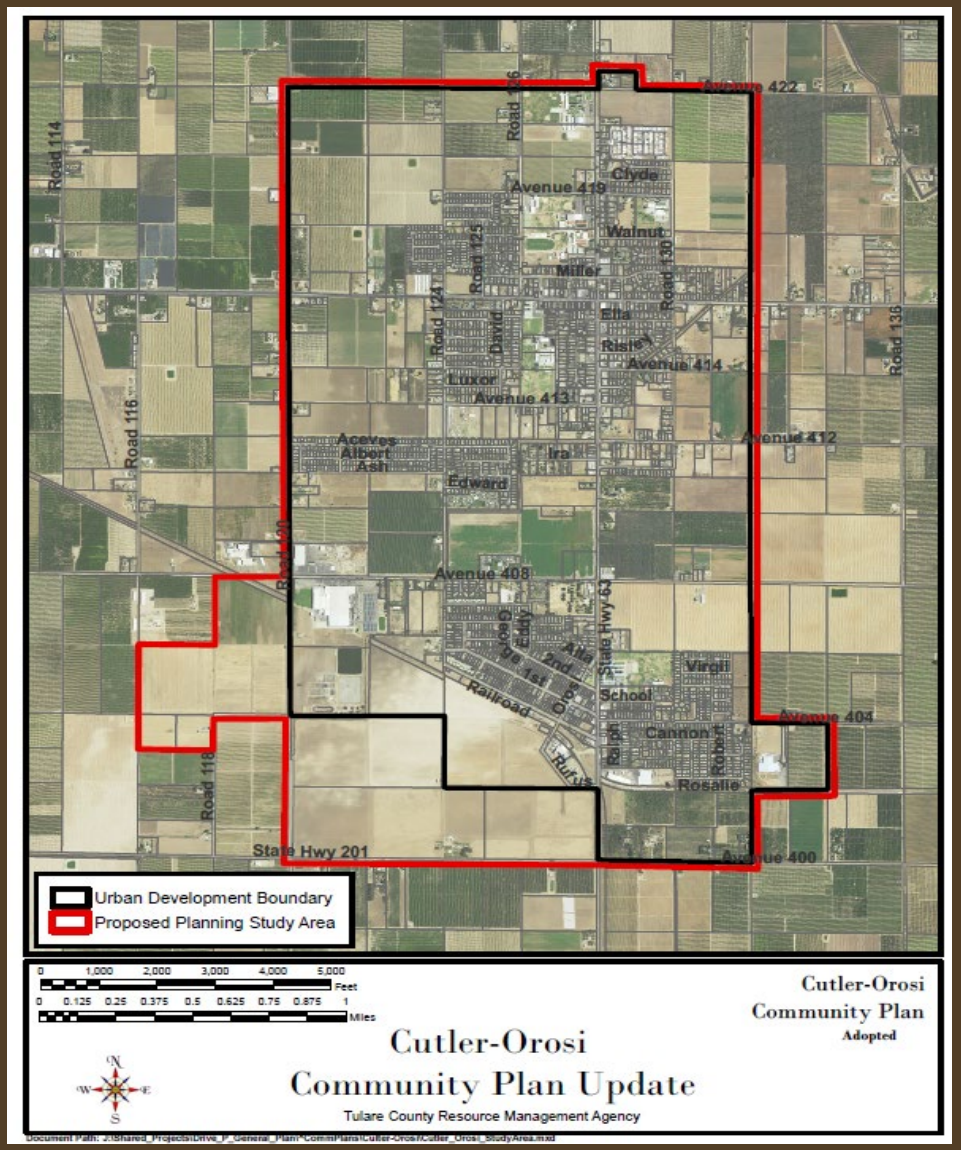


Orosi Public Utility District

Cutler Public Utility District

Cutler/Orosi Wastewater Treatment Facility

Cutler/Orosi Community Plan Update Study Area

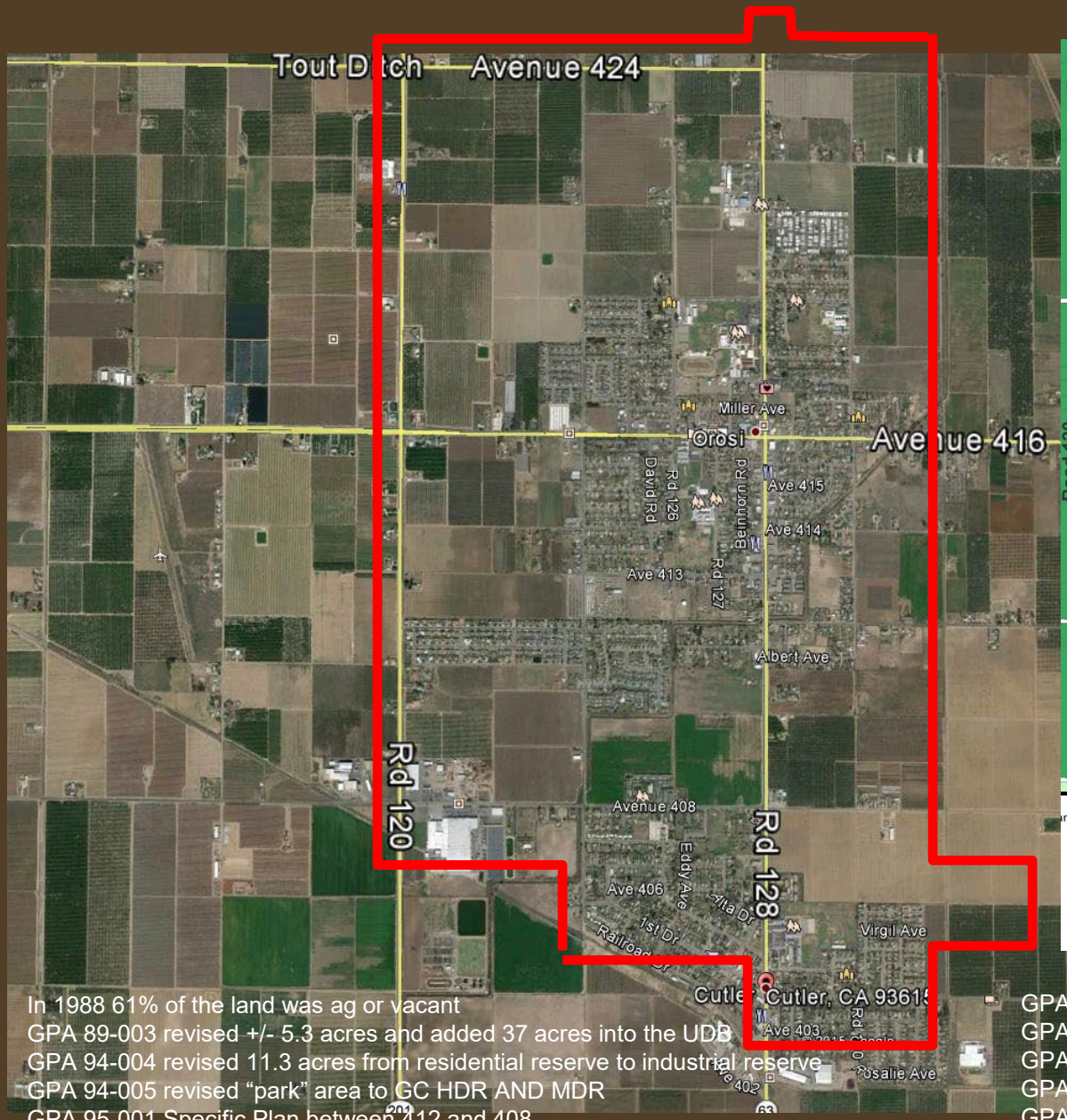


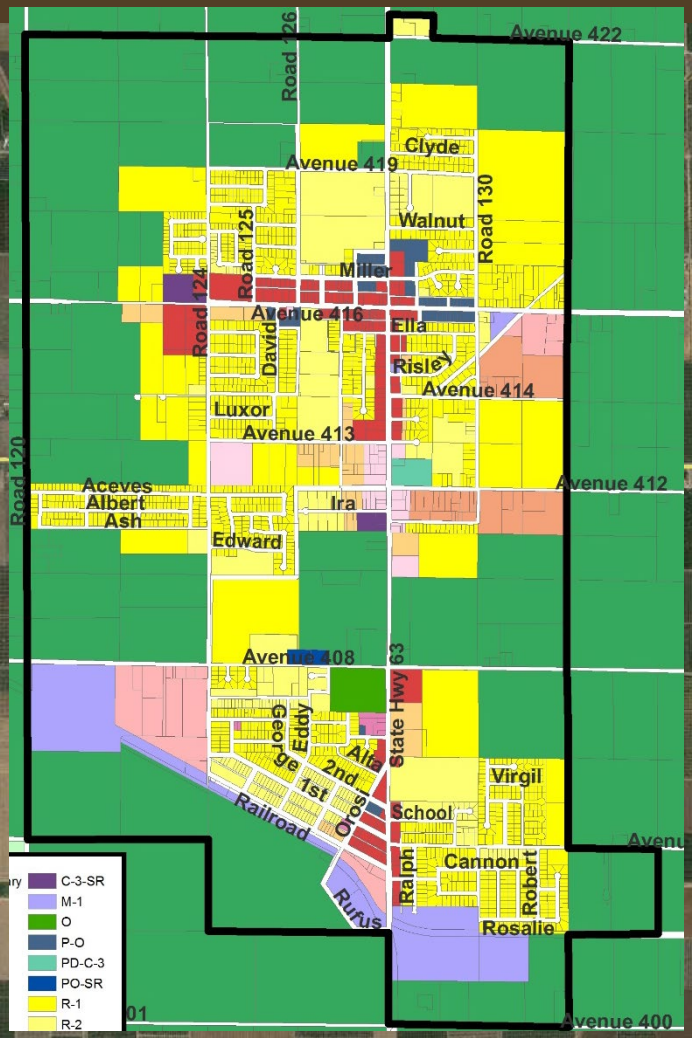
Cutler/Orosi Urban Development Boundary

Existing UDB area = 2,200 acres

UDB area with Study Area Additions = 2,600 acres

UDB increase in area = 400 acres





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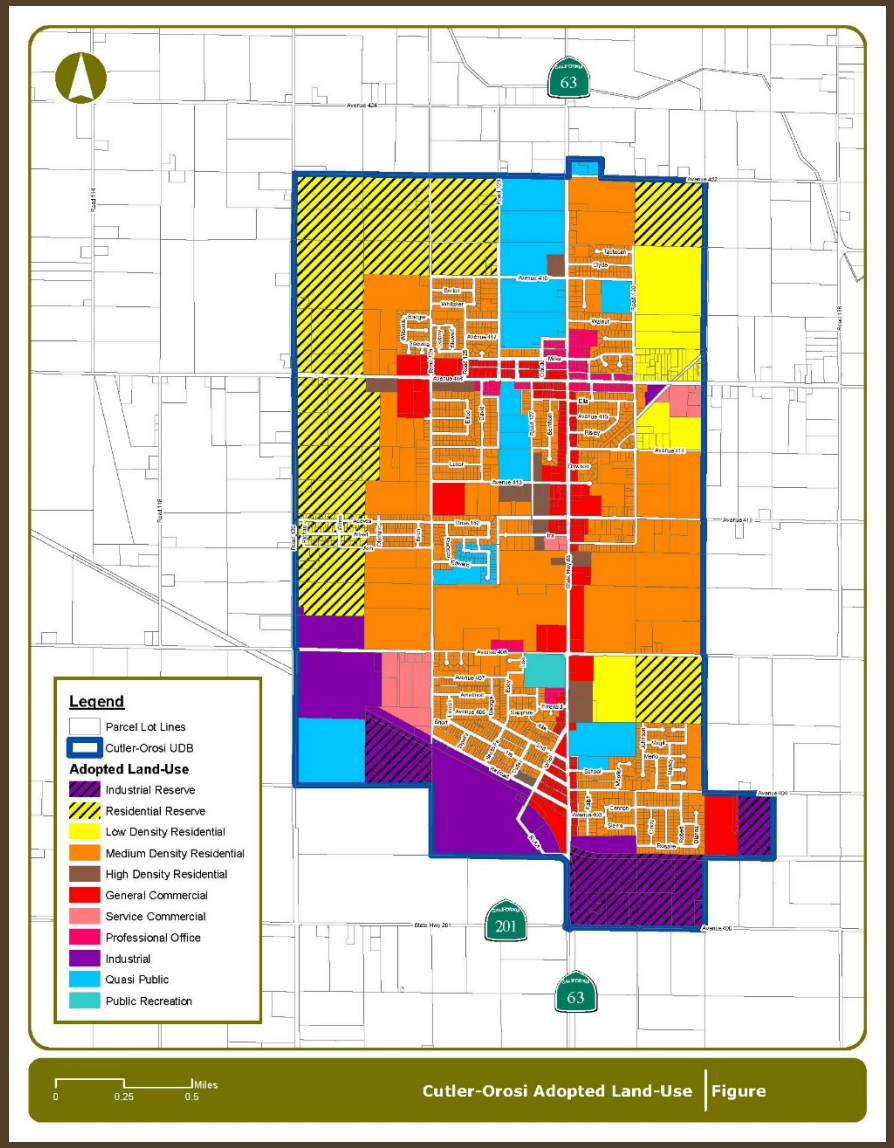
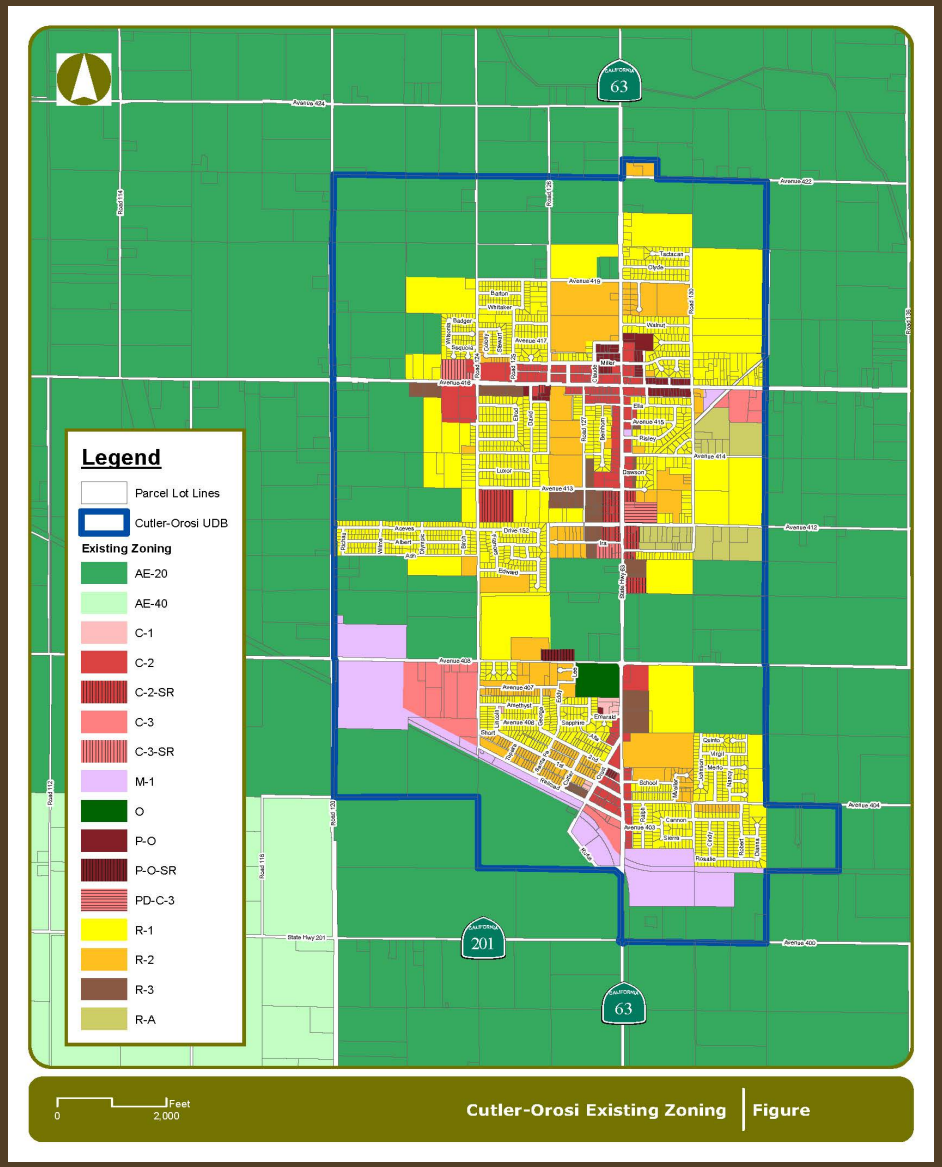
Google earth

Cutler/Orosi Adopted Land Use



Cutler/Orosi Existing Zoning Districts



Community Infrastructure Improvements

$5 Million In Improvements to the Wastewater Treatment Facility

Orosi - Upgrades for new water and sewer lines

Orosi - Two New Water Wells

Orosi - New Flowmeters

Cutler - Upgrades for new water and sewer lines

Cutler - One new water well and one new test well

Cutler - New Water Blending Tank

Pending Grants to replace remaining water and sewer lines

Measure R Funds

- 2006 Voters approved Measure R ½ cent sales tax for transportation
- Since 2006 appx. $1.1 million – Cutler and Orosi
- Surface maintenance such as surface seal, oil seal, chip seal, road resurface.

Measure R Funds- Cutler

YEAR 2010 MEASURE R RESURFACING							
	ROUTE				LIMITS		LENGTH
R130		LA402	ROSALIE AVE.	-	A404		0.21
						TOTAL MILES	0.21
YEAR 2012 MEASURE R RESURFACING							
	ROUTE				LIMITS		LENGTH
LA403B	CANNON AVE.	R130		-	LR131C	ROBERT RD.	0.18
LA406C	EMERALD AVE.	R127		-	R128	SH 63	0.11
LA407B	HAZEL AVE.	R126*	END	-	LR126D	LEE RD.	0.05
LR124F	LINCOLN RD.	A407*		-	A408		0.05
LR124G	TOPEKA RD.	A407*		-	A408		0.05
LR127B	PEARL RD.	A406*	END	-	LA406C	EMERALD AVE.	0.03
						TOTAL MILES	0.47
YEAR 2013 MEASURE R RESURFACING							
	ROUTE				LIMITS		LENGTH
D125B	SANTA FE DR.	D128B	RAILROAD DR.	-	D129B	FIRST DR.	0.08
LA406A	AMETHYST AVE.	R124		-	R126	GEORGE RD.	0.22
						TOTAL MILES	0.30

2010-2013

Since 2006:
$242,000

Measure R Funds- Orosi

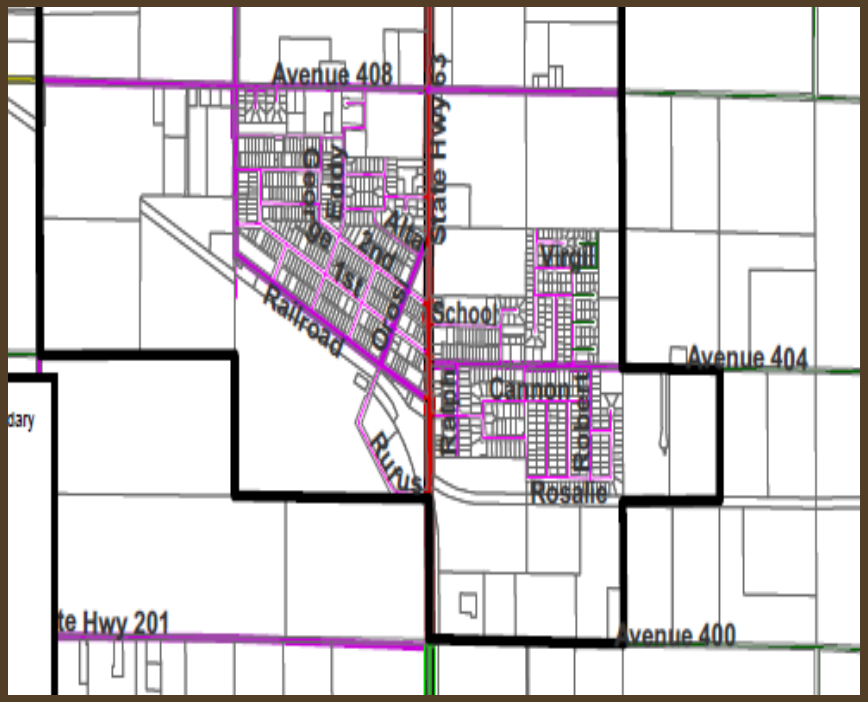
Since 2006- $900,000

YEAR 2011 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
D129D	LEDBETTER DR.	A414		-	R130		0.09
LA411B	IRA AVE.	R126*	END	-	R128	SH 63	0.23
LA416B	MILLER AVE.	R125	ELROD RD.	-	R126		0.13
LR125F	MILLER RD.	LA416B	MILLER AVE.	-	LA416*	END N/O MILLER	0.02
						MEASURE R MILES	0.47
						TOTAL MILES	0.72
YEAR 2012 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
A419		R124		-	R126		0.25
R125		A416	EL MONTE WAY	-	LA418C	WHITAKER AVE.	0.29
R126		A416	EL MONTE WAY	-	A419		0.39
						TOTAL MILES	0.93
YEAR 2013 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
A414		R127		-	LR127C	BEINHORN RD.	0.07
A415		R128	SH 63	-	LR129A	MUELLER RD.	0.19
LR128B	RALPH RD.	LA415C	ELLA AVE.	-	A416	EL MONTE WAY	0.08
						TOTAL MILES	0.34

2011-2013

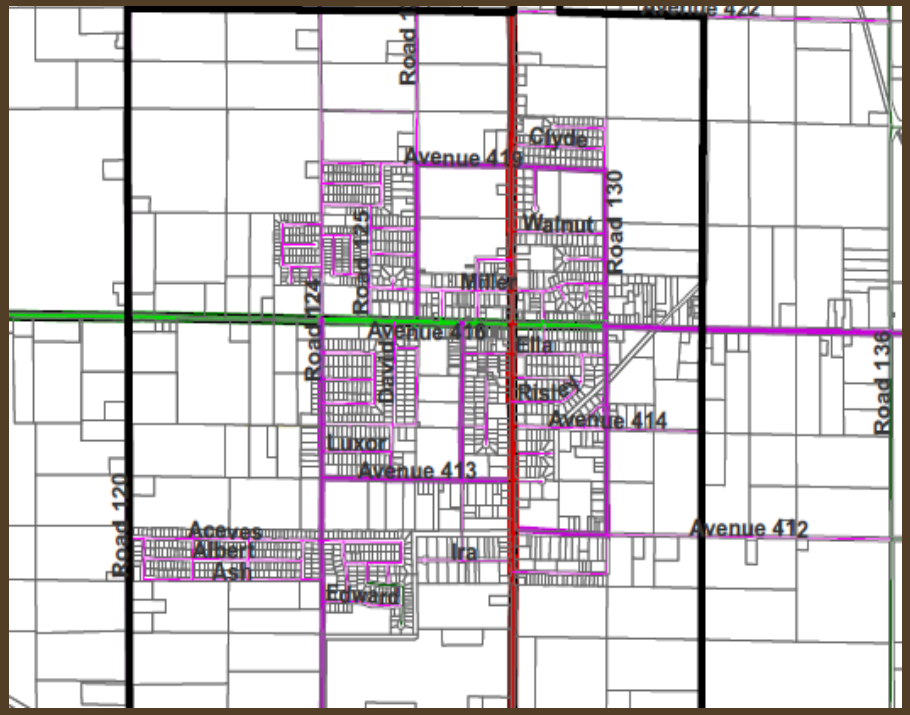
Complete Streets Cutler

- Avenue 404 – SR 63 to Robert Rd
  - Sidewalk, C&G, Drainage
- Avenue 408 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Railroad Drive – SR 63 to Rd 124
  - Sidewalk, C&G, Drainage
- First Drive – SR 63 to Rd 124
  - Striping, Bike Lane



Complete Streets Orosi

- Avenue 416 – SR 63 to Rd 140 (East Orosi)
  - Widening, Sidewalk, C&G, Drainage, Bridges over Sand Creek and Canal, Bike Lane, Transit
  - Multi Use Trail
- Avenue 413 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Avenue 416 – SR-63 to Dinuba
  - Class II bike lane
- Avenue 419



Proposed Complete Streets Projects

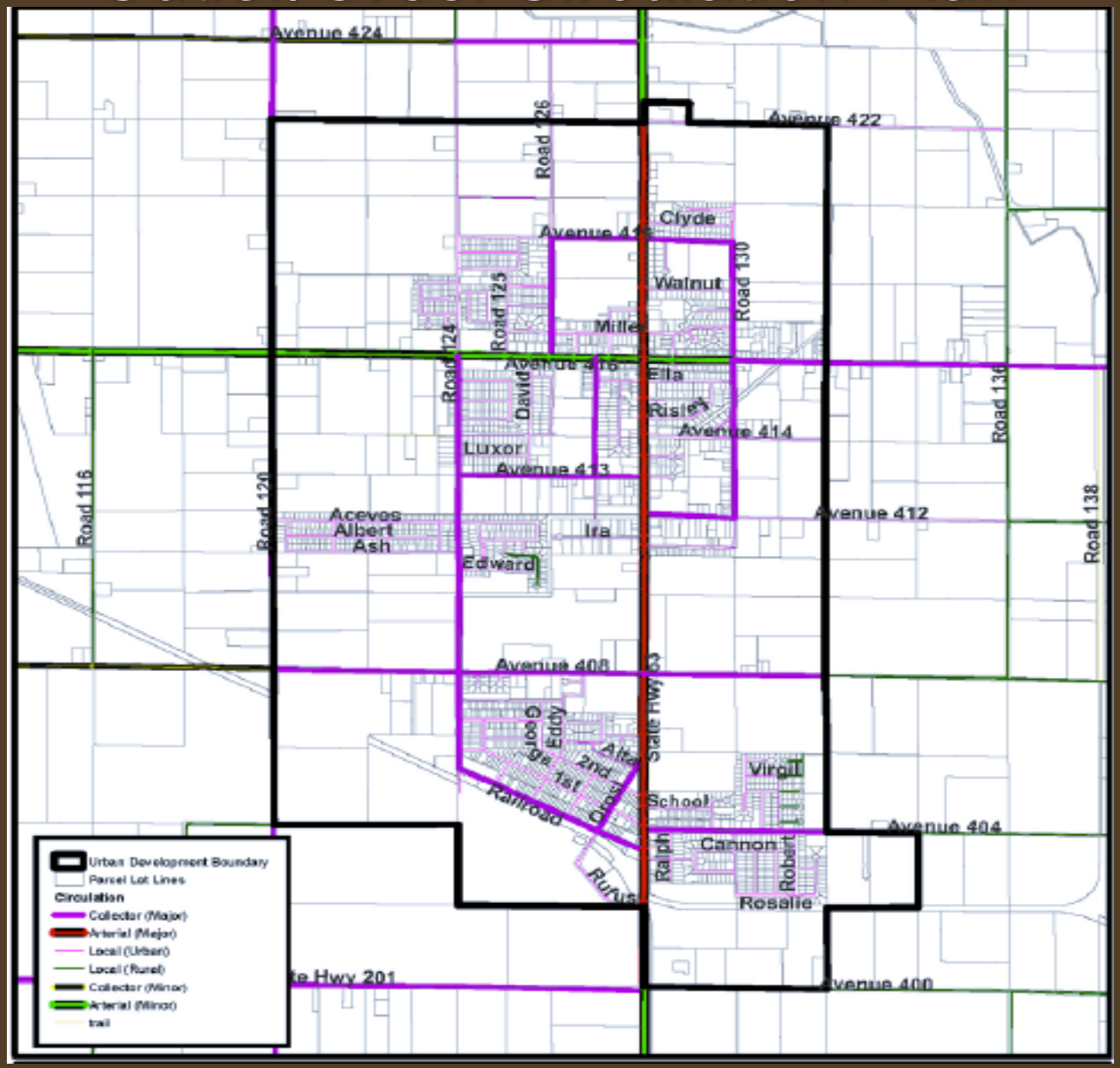
Orosi

1. Avenue 416 – SR 63 to Road 140 (East Orosi)
2. Avenue 413 – Road 124 to SR 63
3. Avenue 419
4. Avenue 416 – SR-63 to Dinuba
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

Cutler/Orosi Circulation Plan



Tulare County General Plan Update Guiding Policies

2. Planning Framework

PF-1	To provide a planning framework that promotes the viability of communities, hamlets, and cities while protecting the agricultural, open space, scenic, cultural, historic, and natural resource heritage of the County *[New Goal].*
PF-2	To provide a realistic planning area around each unincorporated community that clearly delineates the boundaries of each community and provides a framework for economic development, the provision of public services, and an outstanding quality of life *[Urban Boundaries Element; Goal 3, Goal 1UB.C][Urban Boundaries Element Amendment (88-01); 1988, Modified)].*

PF-2.4 **Community Plans** The County shall ensure that community plans are prepared, updated, and maintained for each of the communities.

PF-2.7 **Improvement Standards in Communities** The County shall require development within the designated UDBs to meet an urban standards for curbs, gutters, sidewalks, and community sewer and water systems.

4. Land Use

LU-1	To encourage the overall economic and social growth of the County while maintaining its quality of life standards and highly efficient land use *[New Goal].*
LU-7	To preserve and enhance the character and scale of Tulare County's communities, hamlets, and rural areas, including their design heritage and historic character *[New Goal].*

LU-1.1 **Smart Growth and Healthy Communities.** The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including: walkable neighborhoods, creating a strong sense of place, directing growth toward existing communities, preserving open space, and creating a range of housing opportunities and choices.

LU-4.1 **Neighborhood Commercial Uses.** The County shall encourage the development of small neighborhood convenience and grocery facilities to meet the everyday shopping and personal needs of immediately surrounding residential land uses in communities.

LU-6.3 **Schools in Neighborhoods.** The County shall encourage school districts to locate new schools in areas that allow students to safely walk or bike from their homes.

5. Economic Development

ED-1	To maintain a healthy and diverse local economy that meets the present and future employment, shopping, recreational, and service needs of Tulare County residents *[New Goal].*
ED-2	To promote business growth and industry diversification and maintain a favorable business climate and a supportive economic foundation *[New Goal].*
ED-3	To diversify the economic base of Tulare County through the expansion of non-agricultural industry clusters and through the development and expansion of recreation and visitor-serving attractions and accommodations *[New Goal].*

ED-1.6 **Develop Public/Private Partnerships.** The County shall pursue partnerships to encourage the development of public facilities and infrastructure improvements that benefit communities.

HS-1	To protect County residents and visitors from injury and damage resulting from natural catastrophes, man-made events, and hazardous conditions *[New Goal based on Five County Seismic Safety Element (1974); Goals 1, 2, 3, 6, 9, 12 & 13. Pg. 11].*

Health and Safety for all Neighborhoods

HS-9	To support healthy lifestyles among residents of Tulare County through the built environment and land use decisions that play an important role in shaping the pattern of community development, in either promoting or discouraging good health for its citizens *[New Goal]*.

PFS-1	To establish and maintain acceptable levels of service, minimize costs, and provide criteria for determining the location, capacity, and timing of existing and future public facilities and services *[New Goal].*

Adequate Quantity and Quality of Public Services

Achieving General Plan Goals

- Increasing uses by right
- Creating flexibility for uses through mixed use overlays

DEMOGRAPHIC AND ECONOMIC INFORMATION

- Population
- In 2010, Cutler's population was 5,000 and Orosi's population was 8,770.
- The population for Cutler increased to 5,850 and Orosi's decreased to 7,760 by 2017 (Total Population 13,610).
- Median Age
- The median age in Cutler stayed about the same 24.6 in 2010 to 24.5 in 2017. Orosi increased from 26.4 in 2010 to 28.8 in 2017.

- <u>Economic Conditions (2017)</u>
- Cutler had an unemployment rate of 13.1% and Orosi's unemployment rate was 20%, compared to 10.0% for Tulare County, and 7.7% for California
- Cutler's median household income was $30,760 and Orosi's median household income was $43,379, compared to $47,280 for Tulare County and $76,975 for State of California
- 39.7% of families in Cutler live below the poverty line and 24.3% of families in Orosi live below the poverty line compared to 23% for Tulare county and 11.1% for California.

- Housing Units (2010-2017)
- During the decade between 2010 and 2017, the number of housing units in Cutler increased from 1,136 to 1,293 Orosi increased from 2,070 to 2,076
- 2017 American Community Survey indicated that 69.6% of the housing units in Cutler were 1 unit detached and 80.0% in Orosi were 1 unit detached.
- 2017 American Community Survey indicated that 65.1% of both Cutler and Orosi housing units were owner occupied.
- In 2017, the vacancy rate in Cutler was 3.2% and 3.7% in Orosi.
- In 2015, approximately 15% of the housing units in Cutler and 87% in Orosi were sound.

FUTURE CONSIDERATIONS

- DEMOGRAPHIC AND ECONOMIC INFORMATION WILL ASSIST IN DETERMININING THE AMOUNT OF RESIDENTIAL, COMMERCIAL AND INDUSTRIAL OF FUTURE LAND USES

NEXT STEPS

- Future Community Plan Workshops.
  - April 11, 2019
  - May 9, 2019

Thank You for Attending

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Orosi Public Utility District Board Meeting

April 9, 2019

Who Attended:

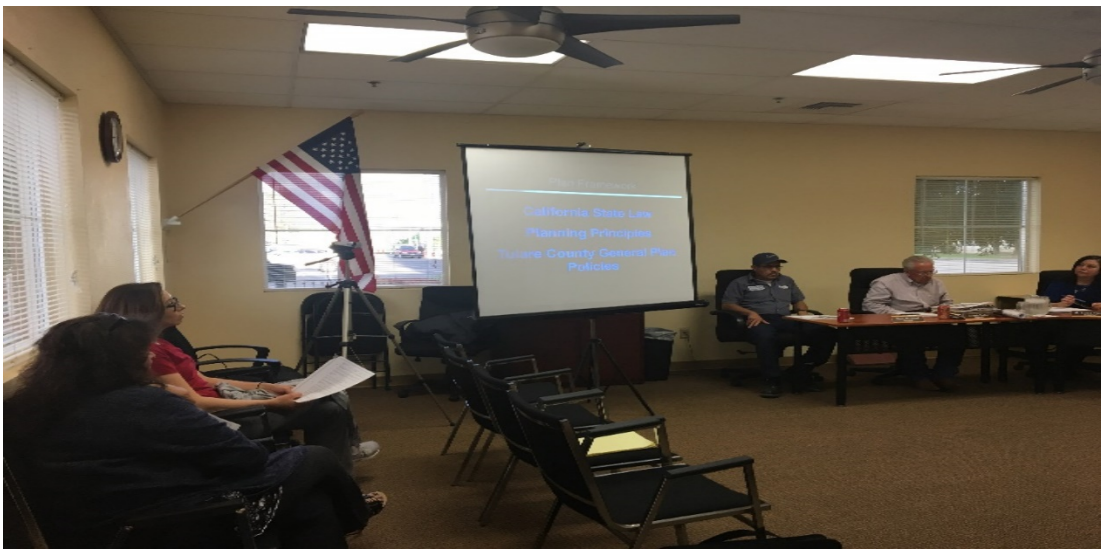
- Dennis Keller
- Aaron Bock
- Susan Simon
- Total of 10 Attendees

Materials:

- Community Presentation
- Handouts (Copy of presentation)
- Sign-In sheet

Public Comments:

Members of the Orosi Public Utility District (PUD) stated they had created their own plan years ago. *The Cutler-Orosi Charrette, was completed in November, 2001 (see attached).*


California State Law
Planning Principles
Tulare County General Plan
Policies

Orosi Public Utility District
Cutler/Orosi Community Plan
April 9, 2019

Name	Address & Email Address	Phone No.
Moses Tijerina		900-3500
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Johnny Saucedal		528-4262
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Cutler-Orosi Charrette

November, 2001

Cutler-Orosi Charrette Report

December, 2001

Prepared for:

County of Tulare Redevelopment Agency
and
Local Government Commission

WALKABLE COMMUNITIES, INC.
Dan Burden, Director
Michael Wallwork, P.E.

TRIAS & ASSOCIATES
Ramon Trias, AICP, Town Planner

COMMUNITY PARTNERS LLC
Sue Newberry, Principal

We are grateful for the assistance of many people and organizations who helped fund, plan and hold the event, including:

California Department of Transportation
Tulare County Resource Management Agency
Residents of Cutler and Orosi, California
Cutler Orosi Project Area Committee
CSET, Inc.
Catholic Charities
Cutler Orosi Unified School District
Family Health Care Network
Great Valley Center

This report was prepared for the County of Tulare by Walkable Communities, Inc. and the Local Government Commission through a CalTrans Community Based Transportation Planning grant. For background information on details found in this report contact Dan Burden, Director, 904-454-3304. Walkable Communities, Inc. provides more information at website: www.walkable.org

Disclaimer

The contents of this report represent the knowledge, experience, and expertise of the citizens and authors in providing ideas and concepts to improve safety, access, mobility and livability through streetscaping, transit, walking, bicycling enhancements and other traffic management strategies. This report does not constitute a standard, specification, or regulation, and is not intended to be used as a basis for establishing civil liability. The decision to use any particular measure should be made on the basis of engineering studies of the location. This report is not a substitute for sound engineering judgment. Adherence to the principles found in this report can lead to an overall improvement in neighborhood and community livability.

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Implementation strategies and funding possibilities

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Minutes - Evening Exercises - Notes - Comment Letter

Executive Summary

A charrette is a series of meetings, workshops and design sessions between the public and a skilled design team to create a community vision or plan. The Cutler-Orosi Community Design Charrette was conducted from November 1 – 5, 2001. Its goal was to create a conceptual plan for improvements for all roadway users along State Highway 63 and the intersecting main streets of Cutler-Orosi. These land use and transportation objectives are intertwined. The transportation decisions made on Highway 63 will have a profound impact on the type and quality of development in Cutler-Orosi. It was funded through a Caltrans Community-Based Transportation Planning Grant, and grew out of community visioning work initiated through a Great Valley Center Legaci grant. The design team included two transportation planners, a traffic engineer, a landscape architect, and an architect. All events were conducted in both English and Spanish.

Cutler-Orosi is in dire need of assistance. Cutler is now the poorest community in California. The poverty level for the two communities hovers around 75%. New industrial development is hindered by a lack of water and sewer capacity. Highway 63, which intersects and connects the two towns, and Avenue 416 (El Monte Way), the main street of Orosi, are not pedestrian-friendly. Five schools are located on or adjacent to these two roads and many people walk along these roads.

Community participation during the charrette was exceptional. 130 people attended the opening event, and approximately 300 people, including County and Caltrans District 6 staff took part during the five-day event. A multi-phased plan with several low-cost implementation steps to improve safety and help stimulate investment in Cutler-Orosi was developed.

Recommendations that could be implemented immediately include a series of community-based actions that require little cost and deliver a high impact. Their implementation will continue the momentum of the charrette. Recommendations that will require more time include interim safety and beautification measures such as sidewalk completion, curb extensions at critical crossings, the narrowing of SR 63 north of Avenue 416, and street tree plantings.

In the longer term vision, commuter traffic to and from Dinuba and heavy trucks are rerouted off of SR 63 through the construction of a truck bypass on County Road 120. By moving a significant portion of through traffic off of SR 63, it allows for significant enhancements and traffic calming which are appropriate for a pedestrian-friendly town center that the community envisions on the vacant land between the two towns. A median, on-street parking, bicycle lanes, and the removal of two traffic lanes are recommended for nearly the entire corridor in this later phase. Comments on the draft report were solicited and are included in the the appendix.

This report is one of three undertaken by the County for the Cutler-Orosi Project Area. Along with the concurrent water and sewer needs assessment study, the charrette report has informed the recommendations of the Cutler Orosi Commercial Development Plan. The action plan from the Commercial Development Plan is included in the Appendix.

Background

This report describes the results of a charrette conducted from November 1 through November 5, 2001 in the unincorporated communities of Cutler-Orosi in Tulare County, California. The charrette was a series of events in which the public is invited to work together to develop a shared vision for their community. The charrette was funded by a Caltrans Community Based Transportation Planning grant. The idea for the charrette grew out of community visioning workshops conducted in 2000 through a Great Valley Center Legaci grant. The charrette events are described in Section II.

Purpose

The purpose of the charrette was to create a broad, community-based vision for the design of future industrial, commercial and retail development along Highway 63 and its intersecting streets in downtown Cutler-Orosi and to improve pedestrian, bicycle and auto safety and access. Community consensus is a key element in obtaining support for change.

Community Planning

Cutler-Orosi area is the largest unincorporated area of Tulare County. The charrette was one of several efforts underway or planned to revitalize and develop this agricultural area. A Commercial Development Plan is being prepared by Applied Development Economics. This plan will provide a market analysis that will help guide private investment in the community and incorporates the recommendations of this report. There has also been a movement underway to incorporate the area as a city for several years. A citizens' committee has been formed and a consultant retained to evaluate the economic feasibility of incorporation.

Many previous planning efforts have incorporated the needs of the Cutler-Orosi area. A Community Plan was adopted by Tulare County in 1988 and has been revised to address specific issues several times. A Redevelopment Plan was adopted in July, 1989. An updated Implementation Plan for the Redevelopment Plan was adopted in 1999. Descriptions in these documents paint a bleak picture of this community of nearly 12,000 people. They describe deficiencies in the sewer and water systems that limit development opportunities, and problematic creek maintenance and storm drainage issues. High unemployment rates and poverty levels are cited. The area has a very high incidence of crime and obvious code violations throughout the two towns.

The Tulare County Redevelopment Agency sponsored a study of Cutler-Orosi Water and Sewer Needs for Economic Development. The study cites DBCP and nitrate contamination of drinking water and the need for a new wastewater treatment facility as key obstacles to development.

Tulare County is also developing a Circulation Element update for the General Plan that includes the Cutler-Orosi area. Circulation plans address all modes of transportation and incorporate issues of land use, noise, air quality, and safety. There is also a Regional Transportation Plan that includes the Cutler-Orosi area, but specific projects are not identified for the unincorporated areas of Tulare County.

Caltrans has a planning process for state highway corridors for highways such as SR 63 (Road 128). The Transportation Concept Report (TCR) describes current and projected operation of a state highway corridor over a 10 to 25 year period. This report establishes the planning concept for the corridor through assessment of the needs for various transportation modes. Participants in focus group sessions explained that Caltrans is in the process of updating all the TCR's for District 6 (Counties of Fresno, Kern, Kings, Madera and Tulare). A TCR takes about six months to prepare and serves as the first of many steps in developing a project that is funded with Caltrans transportation dollars.

Process

Outreach and Publicity

Caltrans message signs prompted passersby to attend the public meetings. The message was updated daily to advise of that day's event schedule.

Public participation is a fundamental ingredient of a charrette. The high level of participation from the community is partially the product of a diverse partnership of organizations conducting outreach with a variety of mediums. Community Services and Employment Training, Inc.(CSET) sent charrette invitation letters to over 500 people. Project Area Committee (PAC) members personally invited community and political leaders to the charrette through phone calls and face to face meetings. The Cutler-Orosi Unified School District agreed to send a charrette flyer home with every student (over 4,000). Catholic Charities gave flyers to many of the area's churches and asked the pastors to announce the charrette at Sunday services. Catholic Charities also sponsored a focus group at the Villa de Guadalupe housing complex for low income families. Caltrans agreed to post two of their message boards for one week on SR-63 promoting the charrette events. The project partners also heeded the recommendations of the PAC and provided music and food at the main charrette events.

Musicians entertained the audience before the meeting started.

All public events were conducted in both English and Spanish.
The Cutler-Orosi charrette provided several opportunities for the people who live, work or govern Cutler-Orosi to share their ideas, hopes and concerns. The schedule of the events was:

Thursday, Nov. 1:

7:00- 9:00 p.m.	Town Meeting and charrette kick-off. Cutler School.

Friday, Nov. 2: Focus Groups

9:00 – 10: 10 a.m.	Fifth grade class at Cutler School.
10:15 – 11:45 a.m.	Tulare County Resource Management and Caltrans District 6 staff.
11:45 – 1:00 p.m	Cutler-Orosi PAC, Chamber of Commerce, Community Service organizations and other local business people.
2:00 – 3:30 p.m	Church leaders and community service providers.
3:30 – 5:00 p.m.	Farm workers and other working families.

Saturday, Nov. 3:
Community Training, Pedestrian Audits and Design Tables

9:00 – 10:15 a.m.	Community Design Training
10:30 a.m - noon.	Pedestrian audits
1:00 – 3:00 p.m	Community design tables

Monday, Nov. 5:
Presentation to Government Transportation Staff, and Evening Community Presentation

1: 00 – 3:00 p.m.	Design concept presentation to Caltrans, County Roads, Redevelopment and Public Works staff.
7:00 – 8:30 p.m.	Presentation of charrette plan to community.

Some of the Thursday evening participants are shown at right.

The events kicked off with a town meeting at Cutler Elementary School on Thursday, November 1, 2001. Through a series of short, fun exercises, this kick-off event provided participants with the opportunity to share their hopes, ideas and concerns for Cutler-Orosi. Approximately 130 people enjoyed the music of a Mariachi band and food at the start of the event. See the appendix for a description of these events.

Envisioning the Future

Sharing a common vision builds community consensus. Cutler-Orosi citizens demonstrated their determination to improve their future when they aggressively pursued grant funding for the charrette event and the Commercial Development Plan. The charrette was an opportunity for everyone in the area to add their voice to that of the core group of grassroots community leaders. For the first charrette exercise on Thursday evening, members of the audience were invited to describe their dream of what they would like Cutler-Orosi to be in twenty years. Many members of the audience then read from their cards. Comments representative of those received include:

- Well-kept businesses and houses, self sufficient, with zoning properly done. Educational, training and recreational centers.
- Safe and healthy streets for our children, and decent playgrounds.

One succinct writer offered: "More jobs, more homes, more stores, more schools." In general, the dreams of people were for safe and clean streets, civic spaces such as libraries and recreation, a strong economy and more police enforcement. Many mentioned more opportunities for the young, especially those who offered their comments in Spanish.

Establishing Values

Community-based values should drive investment of public funds. By determining what is important to citizens, decision makers are able to set priorities that are compatible with those of the residents. To determine what is most important to participants, they were asked to list five of their top values for their community. These values were later sorted by category and counted:

Family, togetherness, friends	35
Community pride, culture, cooperation	30
Safety and security	26
Education, better schools	26
Jobs, employment	17
Street lights, trees, parks, and flowers	16
Better housing	9
Better Roads and sidewalks	8
Cleanliness	7

A presentation of photos of Cutler-Orosi and of similar places followed these activities. The program emphasized the many opportunities for reducing traffic speeds, providing comfortable and convenient public space.

Needs and Priorities

Cutler-Orosi has many, many needs. Charrette participants described the current environment as bleak, crime-filled, and unsafe. But they understand resources are limited, and that they must take a proactive role that complements the public and private investment in the community. Issues and needs were identified and voted on to determine which were most important. Streets and sidewalks emerged as the top concern. The desire for safety, an emphasis in the first two exercises, was again apparent as citizens identified better lighting, sidewalks, and streets as first priorities. Activity centers, beautification, the commercial environment, and housing were also priority areas. A complete listing of the voting is included in the Appendices.

Speaking Up: Focus Groups

The charrette offered many opportunities for people to provide input in different settings at varying times. Several focus group meetings were conducted at CSET headquarters in Orosi and at Villa de Guadalupe between the two communities on Friday. Focus groups varied in size from a dozen to several dozen attendees. The facilitator, Dan Burden of Walkable Communities, invited all participants to anonymously voice their concerns and tell what they what like the charrette to accomplish. Minutes of the proceedings are provided in the Appendices. Some primary points that emerged during the day were:

- Traffic is too fast. Street crossings are dangerous for people walking.
- Development is hindered by water and sewer issues.
- People must leave town to shop for many of their needs.
- Residents would like more public space for recreation and gatherings.
- There is momentum within the citizenry for improving quality of life.

Fifth grade at Cutler School. One idea is shown below.

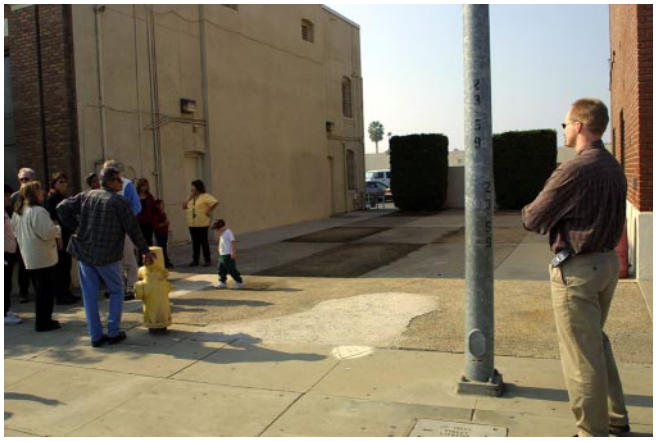
Youth

The youth of the community are a primary concern for those who participated in the charrette. Residents repeatedly stated the need for youth facilities such as a YMCA, recreation center, and better playgrounds. Their desire for better pedestrian crossings and sidewalks were often punctuated by remarks about their concerns for the children of the community.

On Friday, November 2, the Consultant team visited Mrs. Lloyd's fifth grade class at Cutler Elementary School on SR 63. The children were eager participants, with suggestions that ranged from completing the sidewalk system to building a shopping mall. A list of their ideas and the number of votes is included in the Appendices.

Field Tour

Over breakfast of Mexican pastries, Dan Burden provided a resident training to demonstrate what types of designs might be used to address many of their concerns. The training introduced people to concepts that address the issues that had been raised. After the training, residents took part in a walking audit of the area. Participants were assigned to analyze SR-63 and downtown Cutler-Orosi through the eyes of children, the elderly, the disabled and themselves. They pointed out issues and discussed possible solutions. There are many walkers of all ages in the community, but sidewalks are discontinuous and speeds are fast. Some of the vehicle traffic patterns, such as those at the grocery store parking lot, create chaotic situations.



During the field tour participants identified problems such as narrow or nonexistent sidewalks and opportunities, such as this potential pocket-park site next to the bank.

Citizen Design Teams

After lunch, participants took the information gathered during the morning and began sketching design ideas for a safer SR-63 and revitalized downtowns in Cutler-Orosi. Participants spent several hours in the afternoon illustrating their ideas on maps. For example, many showed roundabouts on their maps after learning how roundabouts function and improve safety. Landscaped medians, sidewalk table dining and other creative ideas were expressed. A listing of ideas presented is provided in the Appendix.

Bringing it all Together: Preliminary Design Concept Presentations

The consultant design team spent Sunday, November 4th and most of Monday, November 5th analyzing government documents, conducting further observation of the study area, and evaluating the values, needs, and suggestions made during the charrette events. From this process, the design team developed the preliminary design concept. The design team presented this concept during two presentations to obtain feedback from the general public and government agencies.

Government Presentation

In the afternoon, design team traffic engineer Michael Wallwork presented the design concept to Tulare County Resource Management and Caltrans District 6 engineering and planning staff, and Tulare County Supervisor Steve Worthley, who represents Cutler-Orosi. After his presentation, a spirited discussion followed on the plan. District 6 staff voiced concern that they have yet to be provided explicit technical direction on the State Director's policy on Context-Sensitive Design solutions. In addition, this staff noted that any design changes to SR-63 must be consistent with the long-term transportation needs of the community. Specific concerns included the proposed street trees and medians. Caltrans has sponsored a study of street trees within medians of state highways Tulare County should consult. Caltrans also suggested relinquishing SR 63 through Cutler-Orosi to Tulare County. This would failitate proposed improvements. Wallwork addressed the participant's questions and concerns regarding emergency access vehicles, street trees on SR-63 and medians compromising access. The participants were generally enthusiastic about the design team's plan to reroute commuter traffic through a truck bypass from SR-63 to Road 120. See the appendix for notes from this presentation.

On Monday evening, the design team presented the design concept to over 130 people at the final town meeting. Participants were invited to provide any additional input before the concepts were developed into a final vision. Audience members broke into spontaneous applause at several moments during the design presentation. Some expressed concern about how funding for construction and maintenance would be obtained. The consensus was that the conceptual plan represents the future the citizens envision for the community. At the end of the presentation, Esaul Guerrero, Cutler-Orosi Incorporation Committee chair, said that he was setting out paper for people to sign-up to help implement this vision. Three sheets of paper were filled with names.

Vision Plan

Street improvements, beautification, and civic and commercial development are needed for Cutler-Orosi to achieve their vision of a safe, sustainable, quality place to live and raise families. General principles are discussed first here, followed by site-specific street reconstruction recommendations that show how the principles can be implemented. The site-specific street recommendations are separated into two categories. Phase One recommendations primarily address immediate safety needs, but also include some low-cost beautification efforts. Phase Two recommendations, beginning with completion of the proposed truck bypass, include intermediate- and longer-term projects that can be undertaken in stages.

Streets

The two main streets in the community, State Route 63, also known as Road 128, and Avenue 416, also known as El Monte Drive, a County facility, have a profound impact on the community. A discussion of these streets is required to clarify their role, the perspective and responsibilities of agencies overseeing them, and the possibilities for the future.

Street Types

Engineers have labels for streets that explain their function primarily in terms of their function within a system-wide network. These terms allow engineers to quantify street design considerations such as vehicle speed, capacity, delay, lighting, signing, and others that impact driver safety and the regional movement of vehicles. For purposes of this discussion, streets will be defined as freeways or segments of highways identified by adjacent land use.

Freeways

The nation's freeway system was designed primarily for vehicles traveling through communities or neighborhoods to access surface streets in the vicinity of their destination. Most drivers expect freeways to offer limited access to destinations along the route, and they hope the freeway will provide a fast, continuous travel path to their destination. Freeways are often the safest place to drive. Engineers are careful to provide wide lanes, "clear zones" free of obstacles such as trees and poles in case a vehicle leaves the road, and other roadway features such as medians and barrier rails. Design elements such as these generally remain consistent through both rural and urban areas. These safety design measures have prevented many vehicle crashes and saved many lives over the years.

SR 63 (Road 128) looking toward Orosi at the intersection of Avenue 408 is typical of a suburban highway section that transitions to an urban highway. Even though this is a very short section of road, motorists feel comfortable driving fast here. Note the new sidewalk on the left side of the highway. There is no sidewalk on the right side of the highway, and no marked crosswalk in the vicinity.

Rural Highways

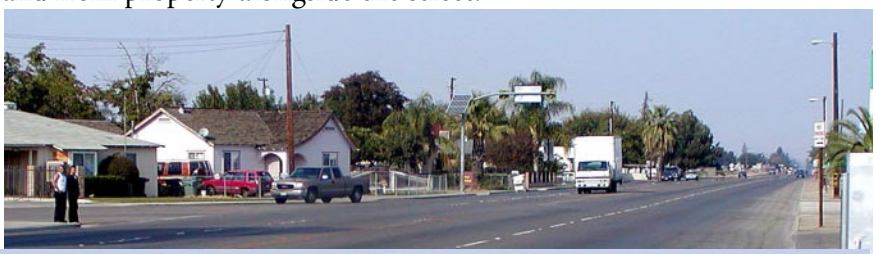
The definition of "rural" varies. In this report we define rural highways as those where few buildings are located adjacent to the street. Rural highways often have many of the features of a freeway. Avenue 416, El Monte Way, between the outskirts of Orosi and Dinuba, is a good example of a four lane rural highway. Lanes are wide and speeds are fast. SR 63 is a good example of a two lane rural highway. Because the land use adjacent to these highways does not require frequent access and crossings, and few people have reason to walk in these areas, rural highways are usually posted at speeds of 55 mph and higher, depending on the design features of the highway. The rural highway network is designed primarily to serve as a thoroughfare between destinations.

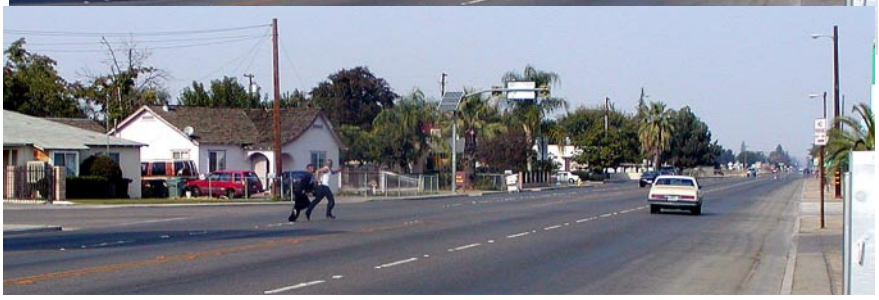
Suburban Highway Segments

As a rural highway nears the center of a community, buildings and cross streets occur more frequently. In this transition area between the center of a community and the sparsely inhabited rural area, more access to property is needed. There are more vehicle turning movements and more people walking. This activity is often occurring on streets that were built to serve as rural highways. Drivers usually behave as if they are still in rural areas unless the design features of the roadway have been significantly changed. A change in the posted speed limit is not likely to cause motorists to slow down unless it is accompanied by extensive enforcement.

Urban Highway Segments

Highways often run through the downtown area of rural communities. These rural downtown areas have higher concentrations of buildings, access points, and pedestrians than suburban areas. The purpose of the highway in these segments remains the same: to move traffic through the city and to provide access to the adjacent properties. However, the balance between these activities has changed because more people want access to and from property alongside the street.





These walkers waited at the corner for a gap in traffic, but obviously didn't feel comfortable once they started across the street. It is difficult to judge the gap in traffic when standing at the edge of a long, wide expanse of asphalt.

The Department uses "Context Sensitive Solutions" as an approach to plan, design, construct, maintain and operate its transportation system. These solutions use innovative and inclusive approaches that integrate and balance community, aesthetic, historic, and environmental values with transportation safety, maintenance and performance goals. Context sensitive solutions will be reached through a collaborative, interdisciplinary approach involving all stakeholders.
Director's Policy, Caltrans

Balanced Transportation

It is this balance between uses that is challenging. Streets that cater to through traffic have wide, fast lanes and limited stopping points to maximize driver convenience. This doesn't work well, however, for school children, shoppers, and others who are walking or bicycling to their destination. Cross traffic and turning vehicles must also be safely accommodated. As more local access traffic is generated, controls are required at intersections. These controls, signals or stop signs, impact the ability of the highway to handle larger volumes of traffic. Engineers must weigh the needs of the through traffic against the needs of the local traffic.

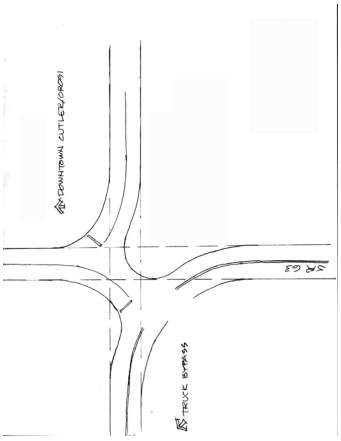
Other considerations, such as the desire of roadside businesses to capture commerce from passing vehicles, or the need to allow large trucks on the street further complicate the balancing process. For example, SR 63 is designated as a Surface Transportation Assistance Act (STAA) Truck route. This designation allows trucks longer than those normally legal in California to travel on the highway. Design features such as wide turning radii that are required by large trucks also impact driver behavior.

Context Sensitive Design

One approach to achieving the best possible balance between the users of streets is called context sensitive design. This term simply means that street designers evaluate the needs of the community impacted by the street or highway along with the need to provide fast, convenient, regional or statewide travel corridors for vehicles. The role of the community is to identify their priorities for the designers. In the case of Cutler-Orosi, Caltrans provided a Community-Based Transportation Planning (CBTP) grant to the community in order to learn what the community wants. This information will help them approach the planning, maintenance, and operation of SR63 with sensitivity to the needs of the community. The street redesign and development recommendations contained in this report reflect the vision of the charrette participants for an economically sustainable community and a safe, beautiful street system that serves all modes of surface transportation.

Tree wells, like those shown in the photo at left, are an effective way substitute for continuous planter strips to buffer the sidewalk from traffic, narrow the roadway, and to allow for parking. This location has two parking places between each tree, but there could be more or less.

The illustration shows how trucks and commuter traffic could bypass SR 63. Cutler-Orosi is in the upper left corner. Traffic would bypass SR 63 by taking the parallel Road 120. The drawing shows detail of the SR 63/201 intersection. The dotted circles show potential roundabout locations.



Truck Bypass

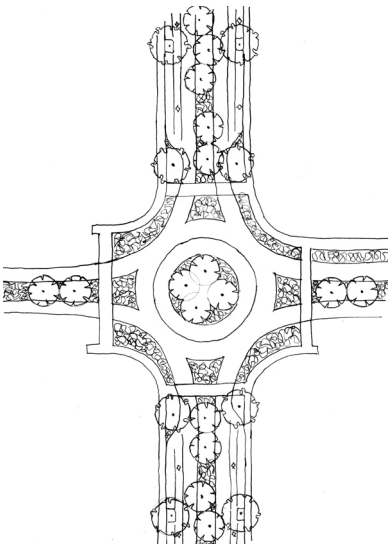
The community vision is to adjust the balance between through traffic and local needs to provide a more beautiful, walkable place. This can best be accomplished by reducing through traffic on SR 63.

A review of the 1998 county traffic counts and the projected counts for 2020 reveal that the basic traffic pattern through Cutler-Orosi is as shown in the diagram. People are traveling primarily from the southern area of SR 63 to and from Dinuba. This traffic pattern provides the opportunity to move a significant portion of the through traffic from Cuter-Orosi onto a truck bypass that starts at the intersection of SR 63 and SR 201, travels west along 201, and turns north onto Road 120 (J19).

This bypass will enable vehicles, especially trucks, that want to travel between the southern section of SR 63 and Avenue 416, to avoid Cuter and Orosi. The commuter traffic that travels between SR 63 and Dinuba could also use the truck bypass. It is important that the directional sign for northbound traffic on SR 63 be labeled “Downtown Cutler-Orosi (right arrow)”

A roundabout at the intersection of Road 120 (J19) and Avenue 416 (El Monte) is important as part of the truck bypass to provide a gateway to downtown Cutler-Orosi. This will redirect any tourists who may have missed the first sign to the downtown area. Tourists southbound from Avenue 416 can also be directed through the downtown through this roundabout. Signing to direct tourists and others to the downtown is important at this location. This rerouting will allow the community to take advantage of the economic opportunities that may be provided by tourist or commute traffic without requiring a wide, fast thoroughfare.

A roundabout design for the intersection of Road 120 and Avenue 416 is shown in this drawing.. This roundabout would serve as an alternate means of directing traffic to the center of Cutler-Orosi.



Street Design Elements

Roundabouts have been demonstrated to be generally safer for motor vehicles and pedestrians than other forms of at-grade intersections.
Roundabouts: An Informational Guide, U.S. Department of Transportation

The design of each of the elements of the street has an impact on the overall street environment. This section includes a brief discussion of roundabouts and other recommended components of the street.

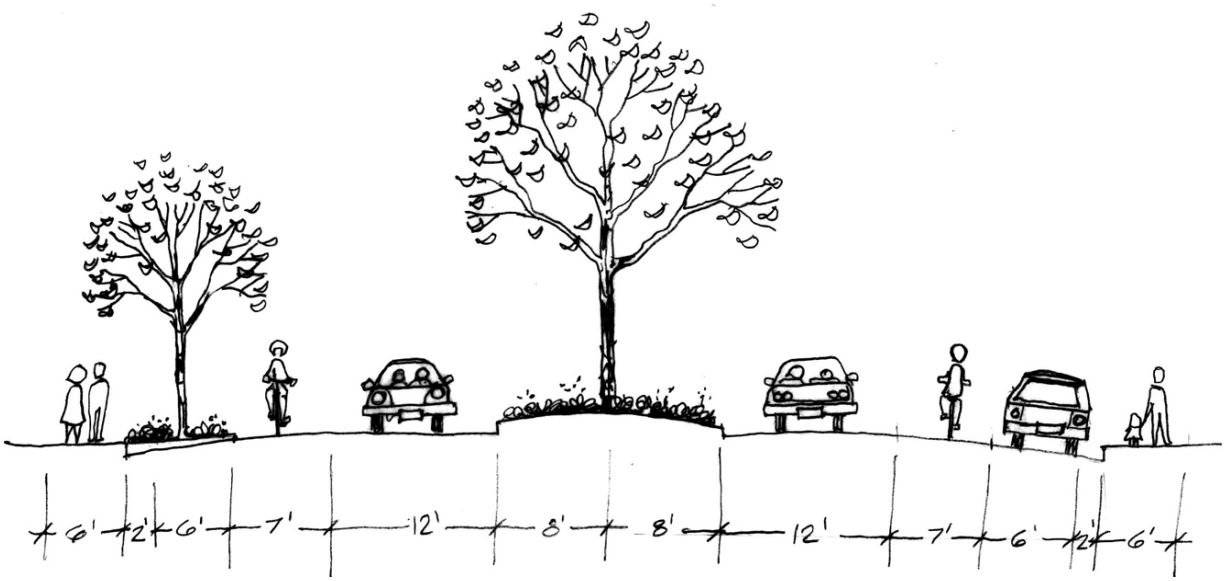
Roundabouts are designed to allow traffic to travel between 15 and 20 mph through intersections. Drivers circulating in roundabouts have right of way, so entering traffic yields. Drivers enter just as they would turn right from driveways – wait for gaps and enter. Roundabouts offer opportunities to provide attractive entries and focal points. Center islands and splitter islands are often landscaped. Roundabouts terminate long street views, and have an effect on calming all approaching streets. They should be designed to accomodate bicyclists and pedestrans.

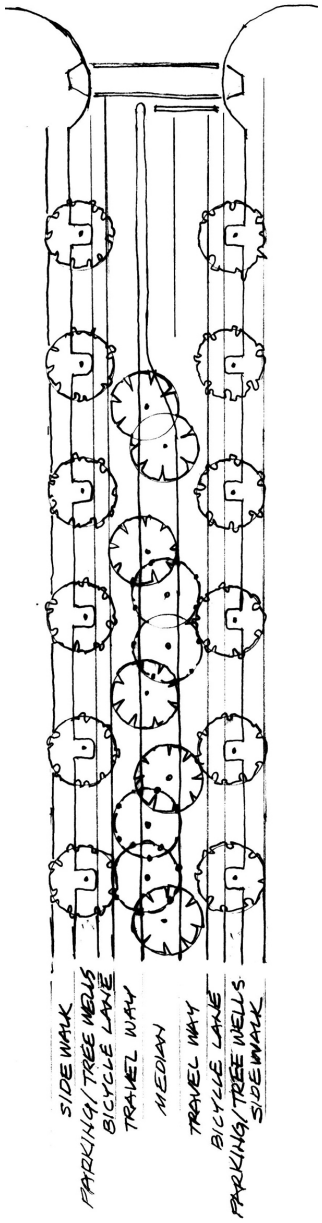
A Santa Barbara, CA roundabout

Pedestrian crossings are placed one car length away from circulating traffic to ensure drivers' attention is focused on pedestrians, not turning movements. This placement also encourages pedestrians to cross behind vehicles entering roundabouts. Splitter islands, triangular islands at entries to roundabouts, often create refuges for pedestrians waiting for crossing gaps. The consultants recommend the use of desert plants to landscape the roundabout because after the first year they will not require any watering. Santa Monica is now successfully using a range of desert plants to landscape their medians as they pedestrianize their main streets.Caltrans is not opposed to the use of roundabouts in general. However, each proposed location will need a detailed engineering study to determine its appropriateness. Caltrans Design Information Bulletin Number 80 (http://www.dot.ca.gov/hq/oppd/dib/dibprg.htm) contains guidelines and requirements that must be incorporated into roundabout designs.

Roundabouts offer many benefits to the Cutler-Orosi area, including smooth traffic flow at speeds appropriate for the area, landscape and gateway opportunities, and improved pedestrian crossings.

Often the issue of emergency service vehicles, especially fire trucks, using roundabouts is raised. It is an interesting question considering the significant danger firefighters face using a signalized intersection. Over the years many fire trucks have been hit in intersections by vehicles whose drivers did not see or hear them and hit the fire truck broadside. At a modern roundabout the speeds of all vehicles are kept low and the threat of a broadside, or "t-bone" crash is eliminated. An emergency vehicle must slow down to around 20 mph or so to pass through the roundabout. This is about the same speed at which an emergency vehicle proceeds through a signalized intersection because they must slow down to avert the threat of a broadside crash. Many fire chiefs recognize the increased safety a roundabout brings to their crews.





The illustrations show the street after all reconstruction in Phase II is completed. Sidewalks on both sides of the street are needed to accommodate the many people who walk in Cutler-Orosi. The six-foot width is needed so people can pass each other comfortably. On the left of the cross section is shown a planter strip between the sidewalk and the curb. This is a preferential design because it separates people walking from traffic, which makes the walking experience safer and more comfortable. Planter strips bring streets to life with the beauty of landscaping and space for attractive street furniture, lighting, and other amenities. Planter strips also provide places outside sidewalk space for traffic signs, fire hydrants, utility boxes and other operational elements of streets. In this design, tree wells are shown as an alternate to continuous planter strips. Trees can be planted between every two or three parking spaces.

On the right side of the drawing below is shown a sidewalk with no planter strip or tree well. Although this is less desirable, there may be space limitations that prevent provision of the preferred design.

Parallel parking spaces are set at 6-foot widths when there are bike lanes. This configuration encourages people to park close to the curb to reduce conflicts between people opening their car doors and bicyclists. The bike lanes and parking areas provide additional separation between pedestrians and vehicles.

Raised medians in the center of the streets can be landscaped with trees and low-water, low-maintenance plantings. Desert plants that only require watering in their first year are being used successfully in Santa Monica. Medians provide essential buffers between opposing lanes of traffic and can increase carrying capacity of individual lanes by 30%. They do this by moving left turn traffic out of the through lanes, and reducing the friction of crossover traffic. At major driveways left turn pockets are provided in the median.

A buffer, or planting strip, between the sidewalk and the curb provides separation between traffic and people who are walking. It also provides space for landscaping and street amenities.

Raised, landscaped medians provide a number of benefits. As the photo above shows, they can also help build community identity. The photo below shows the extension of curbs at corners that are often called bulb-outs.

This treatment enables management of access points along the road so that full access is provided at major generators and partial- to no-left turn access is provided at lesser driveways. Medians provide pedestrians with safe refuge as they cross streets. They allow space for gateway treatments and they help eliminate aggressive behaviors such as inappropriate passing.

Bulb-outs are extensions of curbing that reduce lane widths. Bulb-outs shorten crossing distances for pedestrians. Bulb-outs at intersections also slow turning traffic, provide added space for street amenities, protect parked vehicles, and improve sight lines.

In an effort to constrain costs, recommendations for the realignment of curbs and gutters are restricted to specific sites. If new construction occurs or additional funding becomes available, planter strips should be provided between sidewalks and the street by either moving the curbs and gutters or obtaining additional right-of-way from property owners.

Beautification

Trees, benches, flowers, clean alleys, and civic pride were top issues for charrette participants. Many spoke of the slow decline of the community, years of losing their youth and long-time residents to more vibrant places, and a strong desire to change.

Efforts need not be restricted to public spaces. Property owners and tenants will benefit as Cutler-Orosi becomes more attractive. And this will happen more quickly if everyone makes a contribution. Small investments reap a big return— a few flowers here, a donated bench there. The local refuse company has already embarked on a program of providing free trashcans and emptying them on a regular basis. Their efforts could be matched by other commercial enterprises.

The recommendations that follow vary from no cost to moderate cost. Within some of the suggestions are opportunities for entrepreneurial enterprise. The skills, knowledge, and resources of those living in the community must be utilized for the full vision to become reality.

Colorful displays create visual interest and can attract customers.

The computer simulations shown above illustrate how beautification can enhance marketability.

Community Cleanup

The community is sorely in need of a face-lift. While funding is required for many things, resources needed for a cleanup day are minimal. The local refuse company has already been a partner in a cleanup effort and will participate in additional cleanup efforts. The community should take advantage of such a willing partner. The cleanup day could include a ceremonial tree planting, a celebration, and possibly some planting of bulbs that will bloom in early spring.

The freshly painted facade above shows the pride of ownership that seems missing in the photo below.

Trees

Trees beautify a community, provide shade, reduce pollution, provide homes for wildlife, and improve storm drainage. The urban tree forest in San Jose, California, has the capacity to reduce the temperature as much as 11 degrees. One single mature tree provides the cooling equivalent of five average room air conditioners running 12 hours per day. The City of Modesto Public Works Director has found that by protecting the asphalt from the sun's rays, an extensive tree canopy extends the life of a street's use before it needs to be resurfaced by 10-20 years.

Tree wells are recommended for Phase Two of the street redesign. Trees should be planted as soon as possible following the addition of the tree wells. But even before the tree wells are in, planting can begin outside of the public right-of-way. Tree species, planting methods, water needs, and location are very important considerations. Improper species selection or improper planting methods threaten the life of a tree and the community's attempt to improve the community. Many resources are available to help California communities with planting issues. The Urban Tree Foundation in Visalia has worked extensively with the City of Visalia and other Valley and San Francisco Bay Area communities and has been extremely successful at securing grants for community urban forestry projects. The Foundation can be reached at (559) 713-0631. The Community Trees Organization website at www.communitytrees.org contains links to 48 California websites with tree information. Tree Guidelines for San Joaquin Valley Communities, written by the USDA Western Center for Urban Forest Research and Education, can be downloaded from the Local Government Commission's web site at http://www.lgc.org/bookstore/energy/publications/sjv_treeguidelines.html. For a hard copy, contact Dave Defanti at (916) 448-1198. The USDA Western Center for Urban Forest Research and Education can be reached at (530)752-7636.

Tree wells, like the one shown above, can serve as an effective substitute for continuous planter strips.

The simple act of planting and caring for flowers is a way for residents to significantly beautify their community.

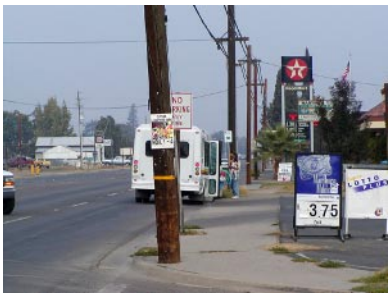
In Phase Two, medians can be planted with trees and other landscaping materials. A landscaping theme should be developed as part of the design work for the median. This will ensure that the plantings unify the streets and help identify the communities.

Flowers

Enterprising high school students in tiny Halfway, Oregon, raise funds for school programs by selling local merchants hanging pots of flowers each spring. The price includes seasonal maintenance and watering of the flowers. The effect of dozens of hanging planters filled with pink petunias is dramatic. This type of creative thinking takes advantage of skills, knowledge, and creativity within the community, rather than relying upon outside resources. Cuter Orosi could develop a unique planting program that would contribute to the unity and sense of identity in the community.

Community Gardens

Community gardens of produce, flowers, or other greenery bring people together and provide many opportunities. In fact, Seattle learned that crime is reduced in areas where community gardens are established. There is a bounty of information on community gardening available on websites, and the U.S. Department of Agriculture. Participants spoke of the desire for a Farmer's Market. In some areas, communities market their own produce. This is another project that can utilize the skills and knowledge of the residents of the community.



Benches

Benches placed outside of the walking path provide opportunities for gathering and resting. Benches help create a sense of welcome by inviting a person to rest, and contribute to safety by providing watchful "eyes on the street." Benches could be placed and maintained by local institutions or individuals. It is also possible that skilled craftspeople within the community could build unique benches.

Transit

Transit shelters are needed. Until funding is obtained, however, benches would at least provide minimal comfort to families waiting for the bus. The public transportation options offered to the community could include a Regional Transit Center. and additional routes.

Retention Ponds

Storm water retention ponds in Cutler-Orosi are unsightly areas that gather garbage. Future retention ponds should be designed for dual purposes. Some communities design retention areas so they can be used as practice fields for soccer or other recreational activities during dry periods. This would meet one of the needs defined by the community during the charrette. Public spaces are safest when they watched by neighbors. Orient play fields so that homes face them and provide edge treatments that encourage people to sit and watch.

Public art, like the examples shown below, is a source of community pride and identity.

The town of Exeter has transformed eyesores into assets by having local artists paint murals on blank walls in their downtowns.

Public Art

The roundabouts will offer delightful opportunities for public art and creative expression of the unique character of Cuter Orosi. However, these features will not materialize immediately. An assessment of the skills and knowledge of local residents is could reveal local talent willing to contribute. Talent and appropriateness of design can be assessed with a contest or some other method devised by the creative people of Cutler-Orosi. The wall on the grocery store at the southeast corner of the intersection of First Street and Orosi Drive was suggested as a site for a mural. The nearby town of Exeter has undergone a large mural program to beautify the blank walls in their downtown.

Civic and Commercial Development

Public space and activity centers were high on the list of priorities for Cutler-Orosi residents. A recreation center received the highest amount of votes during the wish list exercise at the town meeting on November 1st. This community of 12,000 has few civic areas and few activity centers for children. There is no public swimming pool and only a very limited library. People linger and socialize in public areas, but no benches attractive gathering places are provided. The recent market analysis showed that people must leave the community to purchase many of their needs.

The vision for a safe, vibrant community for residents to enjoy was very clearly expressed during the charrette. One very necessary component for this is for a sense of place and identity. Redesign of the street pro vides safety, and other elements help beautify. But a center is needed to bring Cutler-Orosi together and to develop the richness of everyday life in their hometown that is desired by residents.

Wide sidewalks separated from traffic with a planter strip should be provided as development occurs.

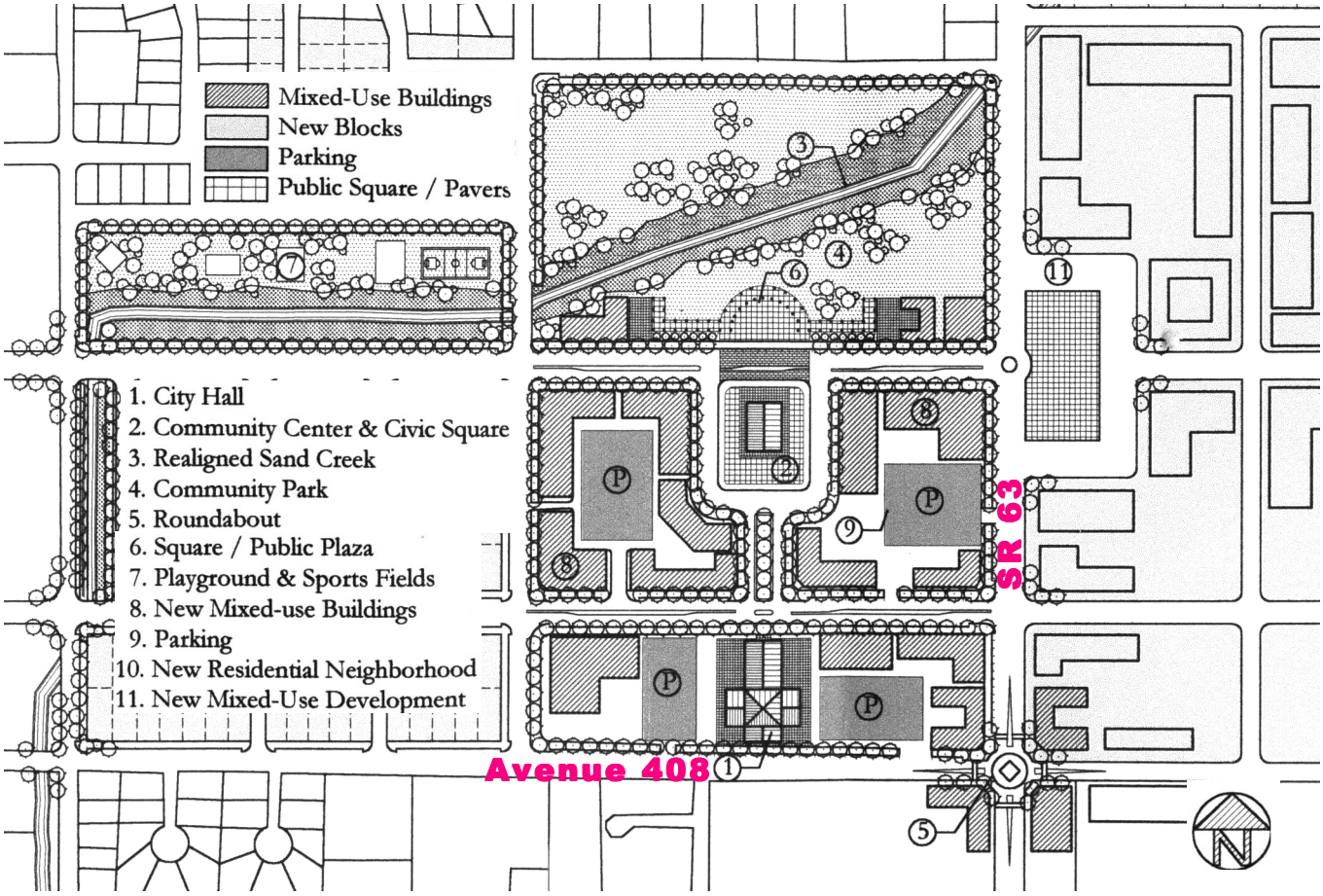
Avenue 408 Village

Participants identified several potential sites for civic and commercial development. Many have great potential. The northwest corner of Avenue 408 and SR 63 is recommended for a town center because of its location and size. This location is exceptional because it allows for a close connection with the existing fabric from either side of the vacant land and it is very visible along the two main thoroughfares. In addition, there is an opportunity to create a park or recreation center, the latter which residents selected as the most important asset missing in Cutler Orosi, at the existing drainage canal. Significant public buildings such as a future community center, town hall and transit center can be located here as well.

The proposed street system within the village is a modified grid that links to the existing street layout. The village area will have all the public and commercial uses that are lacking in both towns and will be accessible by foot, bicycle, or vehicle.

Photos of other communities suggest an architectural theme for the proposed Cutler-Orosi town center.

The site map below shows how Cutler-Orosi's town center could be layed out. This would be a potential site for a Regional Transit Center.



Special emphasis should be placed on building at least one, and preferably two, public buildings with their adjoining space such as a plaza or boulevard. Public buildings and public spaces are the most memorable areas of any town and have the potential to enhance the quality of life of the existing residents. A community center linked to the public spaces depicted in the site plan would be a very good first choice for the community to sponsor. The building does not have to be very expensive if placed in a prominent site and designed with some care as depicted in the rendering.

Buildings can be added over time as community and market needs may dictate. The implementation of commercial projects can be phased so the first layout resembles typical small-scale strip development, but with better design and pedestrian orientation. The developers of all parcels should be required to rebuild the sidewalk adjacent to their property as an eight-foot wide facility with a planter strip. As more buildings are constructed, a true town center on a civic plaza and a well-landscaped boulevard can emerge out of the partnership between public and private projects.

These photos suggest how the town center could look.

The planning and design strategies are based on the typical development of the central valley in California. Urban blocks with multi-story buildings along the sidewalks are seen in nearby towns that have successfully redeveloped. This plan depicts multi-story buildings with commercial and office space downstairs and residential or office space on the second floor. In the first phase parking lots may be adjacent to the roads with landscaped edges. As more buildings are added parking will be located at the center of the block behind the buildings.

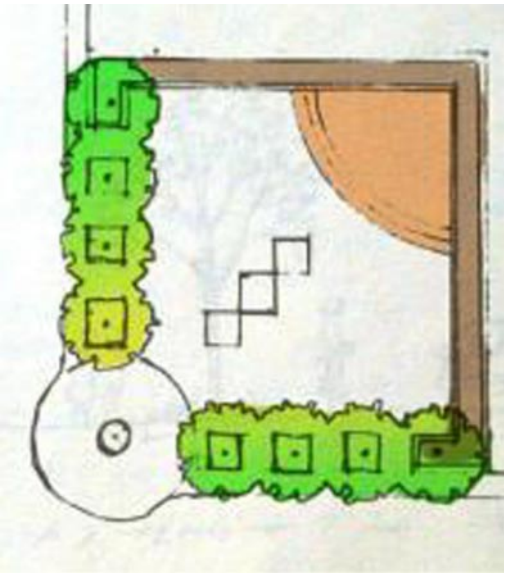
This rendering depicts a concept for the town

This lovely plaza was converted from a former parking lot.

When the towns incorporate the layout for the village area should be adopted as a Specific Plan or a Precise Plan. A Precise Plan can include the same level of detail as a Specific Plan but does not require a financial analysis be undertaken. The new City should adopt architectural guidelines to encourage appropriate buildings and amenities.

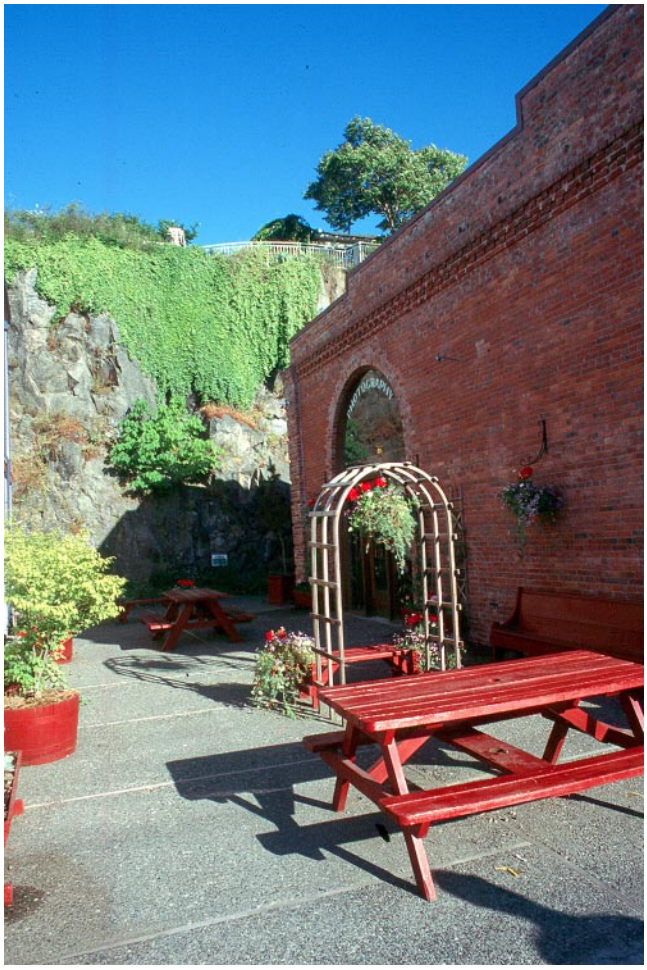
Avenue 416 (El Monte Way) and SR 63

Any recommendation for privately owned property is merely a suggestion of what might be welcomed by the community. With that said, the vacant, fenced lot on the southeast corner has great potential as a small public plaza, as suggested in the drawing below. This site could host public events or a market place. Another potential public space is described below.

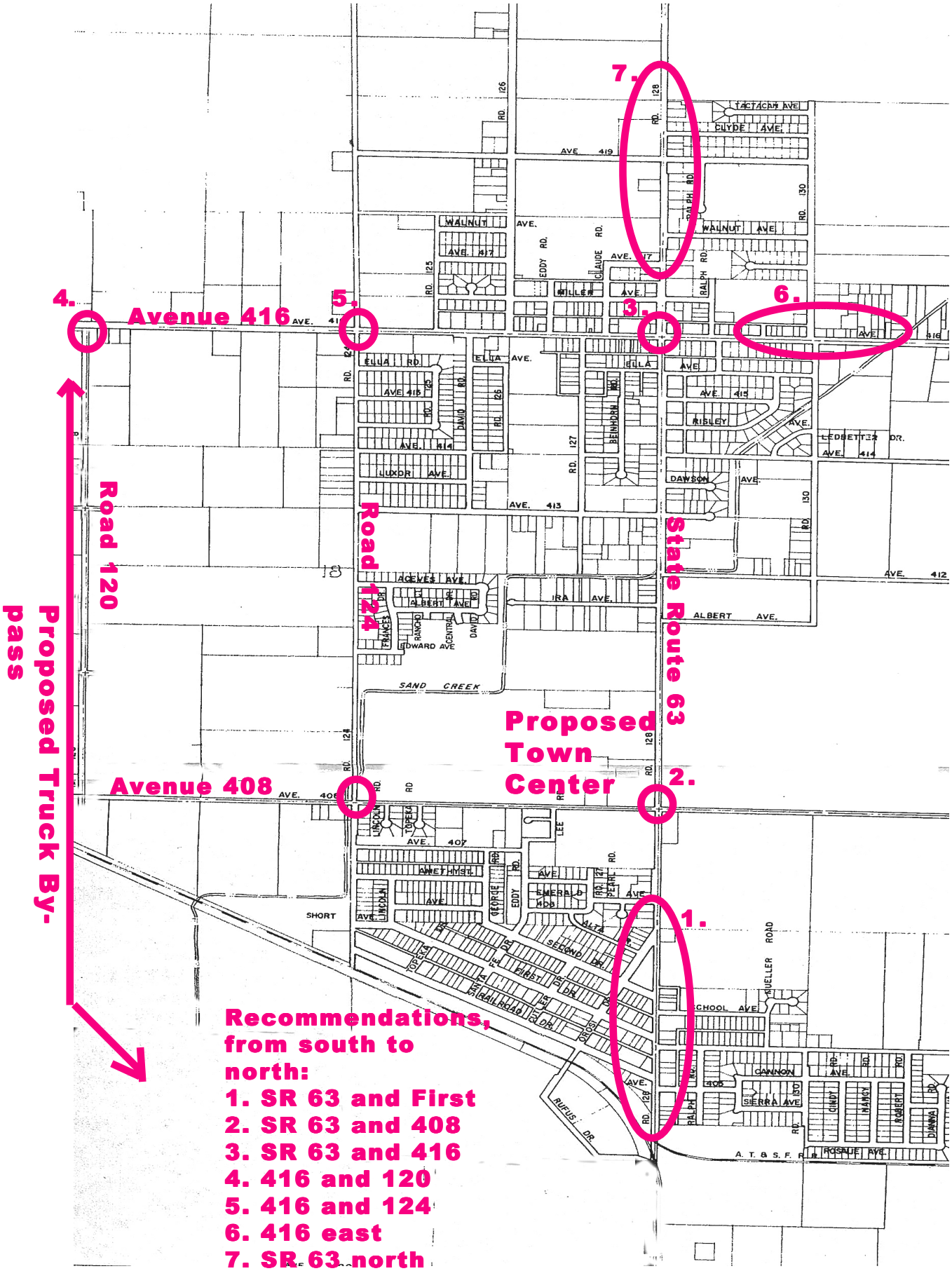


This rendering suggests how an existing vacant corner lot could be transformed into a small public plaza.

Quiet public spaces create a sense of place. This niche between buildings has the potential to become a delightful place for an afternoon snack or rest.



The photo at left shows one possibility for the development of a parcel of land between the Union Bank Building and the Mason's Hall into a public plaza. Youth are currently gathering in this area after school. This site was identified by young people as a possible park during the pedestrian audit conducted by Harrison Rue in December 2000.


7.
6.
4.
5.
3.
2.
1.
Avenue 416
Avenue 408
Road 120
Road 124
State Route 63
Proposed Truck By-pass
Proposed Town Center
Recommendations, from south to north:
1. SR 63 and First
2. SR 63 and 408
3. SR 63 and 416
4. 416 and 120
5. 416 and 124
6. 416 east
7. SR 63 north

Street Recommendations

The charrette team developed these recommendations after consulting the 20-year traffic projections of Caltrans and the Tulare Association of Governments for SR 63. Phase One recommendations should be implemented immediately to address serious safety concerns and to make some immediate aesthetic improvements at low cost. Phase Two recommendations begin with the truck bypass. Other Phase Two projects will incrementally enhance the community as resources permit.

SR 63 (Road 128) Corridor Recommendations

The top priority of the community was for changes to SR 63 to address their concerns for the safety, especially that of children walking. A safe, beautiful street will help the community in its efforts to improve commercial appeal and attract development. Irrespective of economic development, the safety issues observed during field tours and described by charrette participants must be addressed. Drivers on SR 63 frequently do not stop for pedestrians waiting to use the crosswalk and often do not yield to a person already in the crosswalk. The principals of Orosi High School and Cutler School, both of which are located on SR 63, are very concerned about students crossing the street. After spending two hours observing morning traffic throughout the study area, traffic engineer Michael Wallwork said improvements to the pedestrian environment are needed immediately.

Phase One:

Retain existing cross section with the following modifications:

- Complete sidewalk system on both sides of the street from therailroad crossing to Clyde Avenue north of the high school.
- Install lighting throughout the corridor.
- Install 6 foot square tree wells and trees along the corridor where feasible.
- Narrow the outside lane to 12’ by painting an edge line.
- North of 416, narrow the street to two lanes with a median, bike lanes and tree wells. Traffic volumes in this section can be accommodated without any additional changes.
- Improve pedestrian crossings at Cutler School, Avenue 413, and at Walnut as shown under Site Specific Recommendations.
- Provide basic shelter at existing transit stops.
- Implement Phase I treatments listed under each site.
- Complete bypass

Phase Two:

- Add a continuous, landscaped median, bike lanes and additional tree wells per drawings.
- Extend on-street parking by removing two through lanes.
- Add gateway elements in accordance with Caltrans policies.
- Implement site-specific treatments as recommended.

Gateway treatments announce arrival at a unique place and signal drivers that they are entering an urbanized area where slower speeds are appropriate.

SR 63 at Railroad Drive

Phase Two:

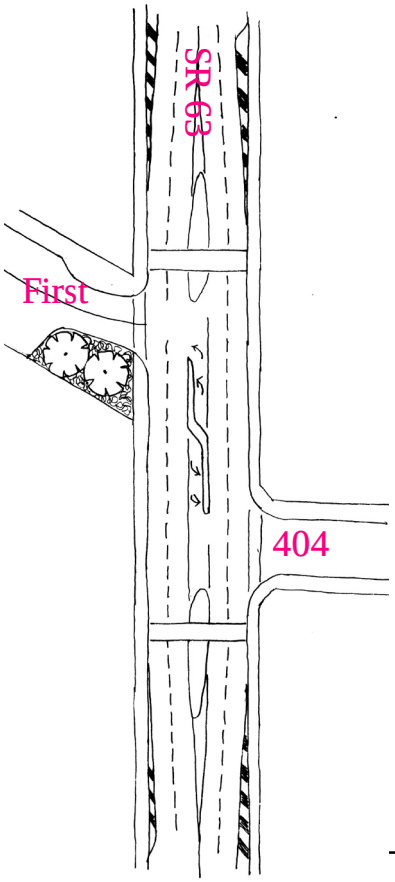
Construction of the median provides a great opportunity to create a gateway treatment in the median. The separation of the lanes creates ample space to put the gateway treatment in the driver's eye. All gateway designs should comply with Caltrans' policy on gateway monuments as it appears in its Traffic Manual and Highway Design Manual.

SR 63 at First Street and Avenue 404

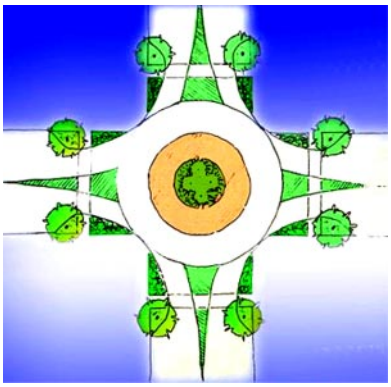
Participants stated many crashes occur at this intersection. Observations revealed high turning movements and vehicle movements from First Street to Avenue 404. The left turns must occur from through lanes because there are no turn pockets. This may be contributing to the high number of crashes. Between the two streets there is angled pedestrian crossing. The crossing area is within the area of the turning movement, creating many possible conflict points between walkers and vehicles. The crosswalk needs to be moved to a site with fewer turning movements.

Phase One:

The existing crossing that crosses SR 63 on an angle between First and 404 should be relocated south of Avenue 404 through the median or north of First Street through the median, or both, as shown in the drawing.



Restripe SR 63 in the vicinity to create the two short medians and two short left turn lanes as shown. Construct two landscaped medians. This design provides median refuge areas for the pedestrians in a reduced conflict location and left turn pockets to store four vehicles (two in each direction). Squaring and narrowing First Street as shown increases the distance between the two intersections, provide additional storage. It will also help to decrease the speed of vehicles turning into First Street.



Phase Two:

This configuration can be easily integrated into future phases of street reconstruction when the number of lanes on SR 63 is reduced.

SR 63 at First Street and Orosi Drive

Phase Two:

This area is the heart of the Cutler downtown. This intersection is very large because both streets have sixty feet of asphalt. This expanse creates a very wide, stark intersection. Construction of a round center island as part of a roundabout would significantly decrease the amount of asphalt and provide an opportunity for a central feature. Bulb-outs and splitter islands could be added as funds become available to enhance pedestrian crossings.

The audience broke into applause when a photo demonstrating the concept of a roundabout at First Street and Orosi Drive was shown.

The bakery on the southwest corner of the intersection has indented parking on Orosi Drive. This parking should be removed and replaced with on-street angled parking consistent with the other legs of the intersection. The space can then be converted to an outdoor eating area.

The marketing analysis shows an unmet demand for additional eating establishments. Sidewalk cafes are a popular addition to any downtown.

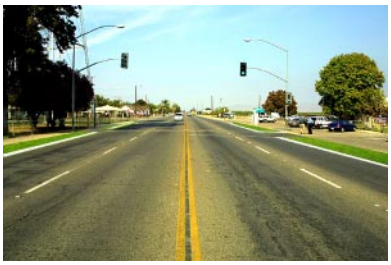
SR 63 at Orosi Drive at Public Utility District Office

Phase One:

This street is sixty feet wide, which is excessive for the environment. It should be narrowed to 24 feet between SR 63 and Second Street. The narrowing should occur adjacent to the Public Utility District Office to allow expansion of the park green space at this site. This space could provide the opportunity to conduct a small farmer's market or other community events. As part of the narrowing, the intersection of Orosi Drive with SR 63 should be squared to facilitate left turns and to slow down right turns from SR 63 onto Orosi Drive. The corner property on SR 63 and Orosi Drive needs to have the driveway realigned as part of this reconstruction.

The public space at the utility office could be used for community events.

This man is running across the street after leaving the video store. People afoot or in car seek the most convenient and direct path to and from their destination. A median would provide a safe refuge area for people crossing to access commercial areas or their homes.



The school crossing should have curb extensions as shown above, or a median refuge island as shown below. In both cases, highly visible marking are needed.

SR 63 at Cutler School Crossing

The signalized crossing at the school was provided to assist children accessing the facility. Charrette participants were still concerned about the crossing. Some even recommended a pedestrian overpass at this site. An overpass at this site is not practical because of costs and the fact that people often pick more direct routes to their destinations. There are, however, some simple and cost effective strategies that will improve the visibility of the crossing and shorten the time the pedestrian is exposed to traffic. There are two alternatives for Phase Two:

1. Construct a pedestrian refuge island between the travel lanes
2. Add bulb outs in the travel lanes on both sides of the road

Under either scenario, the crosswalk markings need to be upgraded and enhanced with the provision of a reflective thermal plastic high visibility stop bars forty feet from the signal mast arm. In Phase Two, both a median refuge island and bulb out can be provided.

SR 63 at Avenue 408

The photos above show existing conditions and a simulation of the proposed roundabout at Avenue 408.

Avenue 408 provides access to the Villa de Guadalupe multi-family housing complex and other residential areas at the edge of Cutler. It is the planned site for the addition of a civic and commercial area. It also connects the street system to areas envisioned as recreation centers by the community.

Phase Two:

A roundabout is recommended at this site to provide the adequate capacity while retaining the two through lanes on SR 63. A well-designed roundabout will also help fulfill the vision of the community by slowing traffic, enhancing pedestrian movements, and providing the opportunity for additional community identification.

SR 63 at Avenue 413

This is an unsignalized intersection with a pedestrian crossing on the north side that is substandard and extremely difficult to see. Two young girls lost their lives attempting to cross SR 63 here on Halloween a couple of years ago. The addition of a median would provide a refuge for people crossing the street. The markings should be upgraded to high visibility markings. Advance warning signs and pedestrian crossing signs in the medians should be added.

Phase One:

Restripe the approaches on SR 63 to create a 5’ median at this unsignalized intersection.

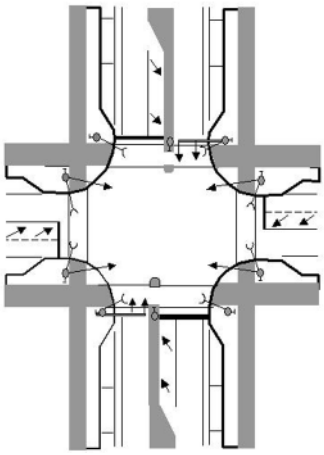
Phase Two:

Add bulb outs

Existing conditions at SR 63 and Avenue 416.

SR 63 and Avenue 416 (El Monte Way) Intersection

Limited right-of-way at this intersection eliminated the possibility of using a roundabout. However, a redesign of the intersection will shorten pedestrian crossings and help slow traffic. The redesigned intersection and the redesign of the approaching legs of the roadway are discussed under different headings, but implementation may be done concurrently.



Suggested final intersection design for SR 63 and Avenue 416.

Phase One:

As part of the installation of the median on the north leg, bulb outs can be added to all four corners and landscaping can be added. The two approach lanes on the south leg could be restriped to a left turn only lane and a through lane. The creation of a left turn only lane on the north side, the signal phasing for SR 63 can be changed so that the phasing would be:

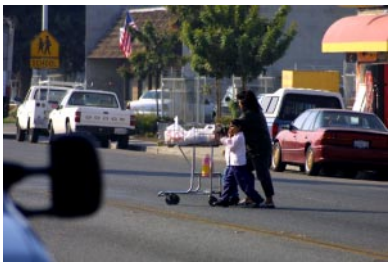
Signal Phase 1: concurrent left turns
Signal Phase 2: if needed, a left turn and through phase from the south
Signal Phase 3: north/south through phases

Bulb outs on each corner, to accommodate large turning vehicles.

Phase Two:

Add medians on both legs of Avenue 416 and the south leg of SR 63

Reducing the number of traffic lanes will allow installation of a median for pedestrian refuge.



Avenue 416 west of SR 63

Avenue 416 west of SR 63 is four lanes wide and carries 7,000 to 11,000 vehicles per day. The traffic volume increases the further west the traffic count is taken. The area has significant pedestrian volumes, including children who attend the elementary school. Many people must cross Avenue 416 to shop, to go to school and to meet friends. Therefore, we recommend the redesign of Avenue 416 to two lanes with on-street parking, a narrow median that increases in width as it extends westwards to Road 124. The downtown section of Avenue 416 should have parallel parking on both sides (7'), one lane each way, and a narrow median (4'). The travel lanes need to be wider to allow space for large truck and fire trucks to get through.

Extra landscaping and speed reduction treatments, such as tree wells, are also recommended on both sides of Avenue 416 in between every second or third parking space. Use bulb outs at intersections to protect parked vehicles, to narrow the road and help slow vehicles, to reduce the crossing distance for pedestrians, and to provide space for additional landscaping.

The driveway in these two photos are next to each other on Avenue 416. The landscaping in the photo on the right eliminates creates an attractive walking area.

Customers cannot enter the store from this side of the building, which makes this parking area less desirable.

Many people cross the street between their vehicles on the south side of El Monte and the grocery store. This behavior is a result of a lack of parking availability in the lot adjacent to the grocery store on the east side. There is a parking lot for the grocery store on the west side of the building that appears to be under used by customers. It appears that there may have been a west entrance to the store at one time. A doorway on this side of the building, even as an entrance only, would encourage people to utilize the parking on the west side. Another option to increase available parking for the market is to purchase the land adjacent to the market and expand the parking.

The bulb out and median in the photo above depict the type of design recommended on Avenue 416. The median provides a refuge for people crossing, whether at the school or near shopping. Crossing distance is greatly reduced by the median, the reduction in lanes, and the bulb out. Stop lines in advance of the crosswalk should also be used.

There is a signalized crossing at El Monte School, yet it was identified by the Cutler-Orosi Unified School District Transportation Manager as the second most dangerous place for children in Cutler-Orosi because of high levels of auto and pedestrian traffic. Four-lane roads with traffic signals typically reach saturation around 12,000 to 15,000 vehicles per day because of the need for the traffic signals to transfer capacity from the major road to the minor road.

Phase One:

Add bulb outs to both sides of the street at the crossing, move stop bars 40 feet back from the traffic signals, and add highly visibility markings

Phase Two:

Construction of a median on Avenue 416 between Road 120 and SR 63 would help people cross the road within the shopping area. The large number of people crossing at numerous locations in the vicinity of the market can be accommodated by providing gaps level with the street at intervals in the median.

The debris at 416 and 120 indicates this is a site of frequent crashes.

Roundabouts similar to the one depicted above are recommended at the intersections of Road 120 and Road 124 on Avenue 416.

Avenue 416 at Road 120

This is a fully signalized intersection on a four-lane road with a raised median. A single lane roundabout is more than adequate to accommodate traffic and provide for future growth. This roundabout should be constructed in conjunction with the development of the bypass. Later, if needed right, turn lanes could be added to provide increased capacity while retaining the single lane design. A single lane roundabout is safer and more convenient for pedestrians and bicyclists than either a two lane roundabout or traffic signals, although a two-lane roundabout can be made safe for pedestrians through the use of low speed design and special pedestrian crossings.

The roundabout at the truck bypass and Avenue 416 will provide a gateway to people who may be using the truck bypass to help draw them into Orosi. It will also be a gateway for people traveling east on Avenue 416. The only disadvantage of the roundabout, and in many people's eyes a major impediment is that it will force all drivers to slow down as they approach and enter Orosi. However, the exiting signalized intersection has ample evidence of many crashes occurring at this intersection and its replacement with a roundabout would certainly increase the safety of this intersection especially with increased truck and some vehicle traffic on the truck bypass crossing Avenue 416.

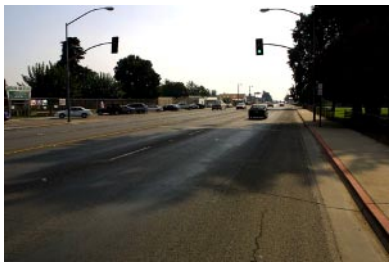
Avenue 416 at Road 124

Phase Two:

Vehicle speeds in this vicinity are relatively high. Debris at the site indicates there have been numerous crashes. A roundabout is recommended at this site to force slower speeds in the vicinity of the school and to reduce the number and severity of crashes. A single lane roundabout is more than adequate to accommodate the 2020 and 2030 projected traffic volumes.

Based on the existing traffic volumes at the intersection of Avenue 416 and Road 124, the proposed truck bypass with a single lane roundabout is adequate at this time and for the foreseeable future. It should operate at only about 60 percent of its capacity. The addition of a median will provide beauty to the street, a safe refuge for pedestrians who are crossing the street, and help slow the traffic.

SR 63 north from Avenue 416 to Clyde



Participants were concerned about safety in this four-lane roadway segment. They told stories of drivers racing to the end of the four-lane segment in a game of "chicken" and a recent fatal crash in the area. Orosi High School and Palm Valley Elementary are both located in this area. A third school is located north of this area on SR 63 where students are not allowed to walk or bike home because of traffic danger. The crossing guard for this area uses a red umbrella to supplement her yellow flag to try to alert the aggressive drivers she daily encounters. SR 63 and Clyde marks the beginning of urban development on the north end of the study area. Unfortunately, many drivers continue into town at unsafe speeds for a populated area.

Traffic volume north of Avenue 416 (El Monte Avenue) is approximately 2,600 vehicles per day. The projected volume in twenty years is 4,500 vehicles per day. This traffic volume is very light and well within the capacity of a two lane street, which can handle up to 12,000 vehicles per day. There are two pedestrian crossings within this area, one at Walnut Avenue and one at Avenue 419.

Phase One:

- Reduce the number of lanes from four to two
- Install a median.
- Install tree wells on both sides of the road.

Avenue 416 east of SR 63

Avenue 416 east of SR 63 carries approximately 3,000 vehicles per day, a very low figure for a four-lane road. East of Sand Creek, Ave 416 is only a two-lane road. It is recommended that between Sand Creek and SR 63 Ave 416 be narrowed to two lanes with parking, a median and bike lanes if possible.

Phase Two:

- Reduce the number of lanes from four to two
- Install a median.
- Install tree wells on both sides of the road.

Implementation

The following chapter outlines implementation strategies implementing the commnity's vision through potential partner organizations and funding opportunities. A more detailed action plan can be found in Chapter 7 of the Cutler-Orosi Commercial Devleopment Plan, included in the Appendix. Planning for the future is of limited value without some strategy for implementation. Cutler-Orosi has limited financial assets for implementation. They do, however, have the most valuable asset of all— the commitment, skills and knowledge of the citizens. These are the people who brought a charrette team to their small community. These are the people who turned out to express their views. And these are the people who have the power within to make their dreams become reality.

"Communities are never built from the top down, or from the outside in…Outside resources will be much more effectively used if the local community is itself fully mobilized and invested, and if it can define the agendas for which additional resources must be obtained." *Building Communities from the Inside Out.*

Community Vision Implementation Committee

There are several committees and groups currently formed by citizens to pursue specific issues, such as incorporation. These groups are all interested in improving the quality of life in Cutler-Orosi, but also have other issues to address. The consultants recommend that a grassroots citizen's group be formed with the specific task of seeking implementation of this vision plan. This group should work with county and CSET staff, the Project Area Committee, political leaders, service clubs, and schools, but remain independent of their jurisdiction. A brief overview of the initial function of the group is:

1. Develop a statement of purpose. Write a one-page overview that describes the group, its objectives, and its membership. rganize theThis becomes a handout for group members.
2. The group should request training from public agencies to become familiar with the processes and procedures that impact decision makers for Cutler-Orosi. Public sector staff generally welcomes thoughtful, well-informed input from citizens, and are eager to acquaint citizens with the system.
3. The group should request written notification of all public meetings that affect Cutler-Orosi. It is best if the notice can be faxed or emailed to one member of the group, who will then advise others. The group may also request an information packet, but not all public sector agencies make these available to the general public in advance of the public meeting.
4. A member of the group should be present at every meeting that affects Cutler-Orosi development and budgeting. They should be prepared to present pertinent facts that could impact funding opportunities or plan implementation. This duty can be divided up so that no member of the group is unduly burdened.

5. They should invite and escort Tulare County and Caltrans engineers on a field tour to allow them to acquaint them with the problems identified during the charrette. The observation period should include children walking to school in the morning.
6. As appropriate, the group should request to be included on public agendas to make a presentation explaining the vision plan to the agency.
7. This group should select activities from the vision plan that they feel they can achieve.

These are only a highlight of the possibilities, but it was clear to the consultants that the level of commitment within the community is more than enough to generate many creative approaches to implementation. This group can position itself as knowledgeable, articulate, and meaningful representation of the many residents in Cutler-Orosi who want a better place. A positive, proactive stance invites a similar response from political leaders.

Civic and Church Groups

The Cutler Latin-American Club has committed to maintaining a gateway on the south entrance to Cutler. They may also be willing to assume responsibility for maintaining landscaping in this vicinity. The Cutler-Orosi Chamber of Commerce has committed to maintaining the northern gateway entrance. In the Cutler downtown area, near Orosi Drive, all four businesses at the intersection of Orosi and First Street have committed to streetscape enhancements and cleanup. This would be done in Phase One. In Phase Two, a roundabout would be constructed here.

Outside Resources

Citizens need to be acquainted with the resources available to them and the role each public agency plays in the future of Cuter-Orosi. The following overview is a starting point, but residents will need to educate themselves fully in order to be effective with their efforts.

Tulare County

Although many in the community would like to incorporate as a city, this may or may not be economically feasible. Waiting for the outcome of the study being conducted to determine feasibility will delay implementation. The County's role in the implementation and funding of the charrette vision should be explored by the citizens and elected leaders. County staff demonstrated their interest and commitment to the community during the charrette. Citizens should request information regarding the allocation of general funds and how those funds could be made available to implement this vision plan.

Approval by the County Road Commissioner is an important step in obtaining approval for any of this report's transportation or street design recommendations. The Commissioner has complete jurisdiction over what improvements are made to any road or street by the County. The current Commissioner has made ongoing maintenance a top priority. When advocating for the adoption of improvements, the community should consider both the cost of installation and that of future ongoing maintenance.

Code enforcement and police patrols are also a county function until such time as the community incorporates. It will be difficult for citizens to maintain a cleaner community until the County supports their efforts through code enforcement. The cost of policing may prevent dramatically increasing patrols, but the Sheriff may be willing to work with residents to develop Neighborhood and/or Park Watch programs.

Tulare County Association of Governments

Whether or not the community becomes a city, their transportation projects will fall under the jurisdiction of this planning agency. The agency prepares and manages a region wide transportation plan that determines how money will be spent, the Regional Transportation Plan for 2001/02. Local Transportation Funds are apportioned to the unincorporated areas, such as Cutler-Orosi. Diverse public use of the public green and road right of way will benefit the entire community.

Funding Opportunities

Funding Sources

What are the possible funding sources for rebuilding the streets? Recent changes in federal and state law have made more transportation funds more available, pushed most decision-making power over those funds from the state highway agencies to the local and regional level, and made funding categories more flexible, so that funds once reserved for highway expansion may now be used for traffic calming, bike lanes and sidewalks. Meanwhile, many jurisdictions are finding creative ways to leverage local funds and thereby finance projects that make streets safer and more beautiful.

The funding soureces outlined below include:

Local County general fund (or possible future city general fund)
Capital Improvement Programs funded by developer fees
Redevelopment funds
Special Districts
State and federal transportatioin funds (TEA-21, STIP/RTIP, transit funds, etc.)
Grants and Loans to Communities
Community Development Block Grant (CDBG)
Economic Development Administration (EDA)
US Department of Agriculture – Rural Development Program (USDA –RD)
Parks bonds and other programs
Central Valley Empowerment Initiative

LOCAL FUNDS

Traffic Calming and Beautification

Several cities have successfully added traffic calming, sidewalks, curbs and similar elements to other projects which involve digging up or rebuilding portions of a street: storm drain and sewer improvements, utility undergrounding projects, and routine street repaving and reconstruction are all possibilities. The greater the extent of the reconstruction, the greater the possibilities are for adding elements such as bulbouts, medians and roundabouts at little cost. In some cities, combining traffic calming with other street work has allowed them to build traffic calming features for as little as 5% of the cost of a stand-alone project. In addition, communities avoid the disruption, noise and expense of repeatedly digging up a street and detouring traffic. However, such combination projects are not always feasible, and may require extensive coordination between departments and capital improvement projects whose schedules and budgets are normally separate.

Street Resurfacing or Reconstruction

Many cities have incorporated traffic calming into street reconstruction projects. In Venice, Florida, for example, officials brought new life to a dilapidated downtown by adding $80,000 to a main street resurfacing project: intersection bulb-outs, mid-block bulb-outs and median crossings, and crosswalks of colorful paver stones were all included. Also, county transportation sales tax measures can provide substantial funding for city street maintenance and rehabilitation.

Sewer and Storm Drain Projects

In Fort Pierce, Florida, three blocks of new sidewalks together with a new roundabout were added to a long-planned sewer project. Built as concurrent construction, the sidewalks and roundabout added just $15,000 to the cost of the project. Similarly, Seattle has added planted medians to several streets at little or no extra cost as part of sewer upgrade projects. Since the storm drain system is largely buried beneath the city's streets, any planned improvements might be combined with the implementation of traffic calming measures. Opportunities may arise on various residential arterial, collector and local streets where traffic calming is desired.

Utility Under-grounding

Utility under-grounding projects often require extensive digging, demolition and replacement. Utility under-grounding typically requires the installation of new conduit, bases and often poles for streetlights (when the lights are on luminaires mounted on the overhead utility poles). Substantial savings may be possible by installing pedestrian-scale lighting (if it is desired for a street) at the same time, rather than digging up the street a second time to provide the extra conduits and bases required for pedestrian-scale lighting.

Development Impact Fees

Some cities have required that developers install traffic calming measures and pedestrian features as part of overall specific development agreements. On a more systematic basis, however, it is feasible for cities to levy impact fees on new developments to fund bicycle, pedestrian and traffic calming infrastructure. Generally, development impact fees are "scheduled charges applied to new development to generate revenue for the construction or expansion of capital facilities located outside the boundaries of new development (i.e. off-site) that benefit the contributing development." As described in *A Practitioner's Guide to Development Impact Fees* (Nicholas, Nelson & Juergensmeyer, 1991), "Where capital facilities are not adequate, permitting development to occur is contrary to the responsibility of a local government to protect public health, safety and welfare." In order to protect public health and safety, then, the courts have held that cities may charge impact fees in order to recoup from each new development a proportionate share of the cost of providing adequate capital facilities — including the provision of safe streets.

In the specific case of traffic calming measures, bicycle and pedestrian facilities, a city may proceed as follows. As the *Practitioner's Guide* explains, "A properly created impact fee system begins with a good comprehensive plan backed up with an equally good capital improvements program (CIP)." City Comprehensive Plan policies should clearly establish the city's desire to protect public safety and welfare by providing bicycle and pedestrian facilities, and using traffic calming measures to create safe streets. Next, facilities services standards must be adopted. A city usually establishes measures of adequate roadway capacity for motor vehicles measured as Levels of Service. An example is, that no more than 40 seconds

average rush hour delay (i.e. Level of Service 'D') is acceptable for any intersection. A city may establish Level of Service standards for bicycling and walking. For example, the standards might specify that:

• 5' wide sidewalks are required on both sides of any roadway carrying more than 2500 vehicles per day; or it could require that walks are required on all streets belonging to the City's designated pedestrian network.
• all arterial and collector streets shall provide bicycle lanes;
• the capital facilities specified in the City's Bicycle and Pedestrian Plans must be completed;
• local residential streets shall operate at an 85th percentile speed of 20 m.p.h. or less; and collector and arterial streets shall operate at 30 m.p.h. or less.

Once the standards for a community have been set, the cost of achieving those standards must be determined (for example, the cost per mile of new sidewalks, multiplied by the miles of sidewalks needed). Finally, a proportionate share of that cost may be levied on new developments, in the form of an impact fee. This is likely to be the most difficult step. For roadway expansions, parks, public schools, parks, sewers and various other facilities, the methods of determining fair shares are well established. However, few if any cities have established regular impact fees for bicycle and pedestrian facilities or for traffic calming measures, since until recently many communities have largely overlooked bicycling and walking needs. Nonetheless, the same principles apply: a fee schedule may be developed to raise a proportionate share of the cost of the needed capital facilities from new development. For a sidewalk impact fee, for example, a proportionate fee might be based on the number of pedestrian trips expected to be generated by the new development. Or the fee could be proportionate to the number of automobile trips generated (since reaching a certain level of automobile traffic on a street may require the construction of a sidewalk in order to meet a city's standard for protecting pedestrian safety).

Tulare County Redevelopment Funds

The State of California originally approved the concept of redevelopment in 1945 in order to give cities and counties the authority to create redevelopment agencies and to give these agencies the authority and power necessary to attack and solve problems of urban decay and blight. Redevelopment agencies were (and are) able to apply for grants and loans from the federal government.

The economic base for redevelopment is tax increment financing (TIF). TIF allows the governing body of a city or county to adopt a redevelopment plan that provides for the redevelopment of a designated area and to use tax increment financing to fund the costs of redevelopment projects in the designated area. The method of financing involves the issuance of bonds or other obligations that are secured by a pledge of all or part of the funds raised by the redevelopment agency. The agency raises funds because of the increase in assessed valuation of taxable real property within its designated redevelopment area. The assumption being that the assessed value of real property within a redevelopment area will increase due to redevelopment improvements.

In addition to providing assistance with public improvements to facilitate redevelopment, a redevelopment agency can provide assistance to a variety of private development interests if the specific projects are in conformance with the agency's plan:

- Development of new industrial and commercial uses in the project area.
- Commercial building and property renovation and improvement projects.
- Rehabilitation and expansion of the community's existing private housing stock.

- "Writing Down" the cost of land when it is sold to a developer or owner participant (in accordance with an approved development agreement providing adequate assurances that the project would be completed).
- Provide low-interest loans or tax exempt financing to reduce financing costs through certificates of participation, lease revenue bonds, industrial development bonds or various forms of tax exempt notes.

The economic capability of a redevelopment agency is of utmost importance in deciding whether or not redevelopment is the "lead" or primary tool for accomplishing major physical changes in a local community. If tax increment income projections are lower than the community wants or expectations, redevelopment needs to be considered as one of the fiscal arrows in a community's financial quiver.

In the case of Tulare County Redevelopment Agency (RDA), the stagnant growth in Cutler-Orosi has resulted in limited growth in assessed valuation, which is important to the success of redevelopment as well as implementation of the recommendations in this report. Nevertheless, the RDA does have some funds to apply as appropriate to the project priorities in the community. The RDA earned about $226,000 in net tax increment in fiscal year 2001. This revenue must also cover existing and new debt service. The amount of annual tax increment revenue may increase or decrease in future years. The RDA develops an annual Implementation Plan for Cutler Orosi, in which its projects are named and prioritized. The RDA currently funds such projects as cleanup days, the Cutler Youth Center, sidewalk construction, and staff costs. Utility expansions are high on the list of RDA's priorities for Cutler-Orosi.

It should be noted that much of the land between Cutler-Orosi, including the land along Highway 63 between Albert Avenue and Avenue 408, is not in the redevelopment project area. This includes the proposed location of the new Village at Avenue 408. However, the RMA is allowed by law to participate in projects outside a project area as long as the project will clearly benefit residents and businesses within the project area. A new civic center/village in this area would clearly benefit the residents of both communities.

Special Districts

Special districts can be used by a jurisdiction to obtain up-front and on-going financing for projects benefiting defined areas or development. The County government could choose to develop a "special district" defined by: areas that receive "special benefits" from the development of infrastructure. Special assessment districts generally assess a charge levied upon parcels of real property within the District's boundaries to pay for "local improvements", such as infrastructure. In many cases they require a vote of the affected property owners to be created. Sewer districts are examples of special districts which primarily operate by virtue of collecting fees.

Typically, assessment bonds are sold to provide the capital needed to pay for immediate construction of a project and are secured by property liens. Landowners are given the option to pay off the assessment immediately, otherwise, the assessments become liens against the property and landowners pay them off in installments. Special assessment districts can assess annual assessments or one assessment at the transfer of property. Most special assessment acts provide for the issuance of bonds. In effect, bonds provide money that the local government is borrowing to construct the improvements authorized by the assessment district. These bonds are generally secured by the property within the district and the bonded indebtedness is repaid with the money generated by the assessments.

Several types of districts are possible, depending on the specific project goals and a host of legal considerations. Examples include Infrastructure Financing Districts, Community Service Districts, Facilities Benefit Assessments.

STATE AND FEDERAL TRANSPORTATION FUNDS

TEA- 21 and SB 45

The Transportation Equity Act for the 21st Century (TEA-21) increased federal funding for transportation by over 40%. In sharp contrast to previous years, TEA-21 and California's recent Senate Bill 45 (SB 45) make traffic calming projects eligible for most federal and state sources of transportation dollars. The specific programs that can fund traffic calming are described in greater detail below. Essentially, these funds are accessed in two ways:

1) A few programs, such as the Hazard Elimination/Safety program or the Transportation for Livable Communities fund are administered at the state level or regional level. Municipalities apply directly to the state (to the California Department of Transportation, or 'Caltrans') or regional transportation planning agency for these competitive funding pots. Those cities which have set clear priorities and have Council-approved plans in hand can often secure a larger share of funds.

2) For most transportation funding, the process begins at the local level, where city and county transportation and public works departments often draw up a list of project priorities. City staff then gathers at the county level to negotiate priorities.

Statewide Transportation Improvement Program

Funded at $8.3 billion over the 1999-2005 period, this program represents the lion's share of California's state and federal transportation dollars. Under California Senate Bill 45, 75% of the funds in this program are now controlled by the regional and county transportation agencies. This 75% share, called the Regional Transportation Improvement Program (RTIP), contains four years of funding worth billions of dollars, and is updated every two years. Senate Bill 45 declares that RTIP funds may be used for, among other things, improving local roads, pedestrian and bicycle facilities, and safety: in short, for construction of any of the alternatives suggested for the Cutler-Orosi projects.

In the case of the bypass discussed earlier, the county could negotiate with Caltrans to transfer ownership of the highway to the County in exchange for transferring Road 120 to Caltrans. Many of California's old highways that are now under local control (e.g. old Highway 99), it is important to include enough operations and maintenance funds to ensure "Old 63" does not fall into disrepair.

Transportation Enhancements

This federal program, amounting to $60 million per year for California, funds a wide variety of smaller community-oriented projects, including renovation of historic transportation facilities, bicycle and pedestrian facilities, and Main Street revitalization programs. Seventy-five percent is programmed by local regional transportation planning agencies, and twenty-five percent is programmed by Caltrans at the state level. For Highway 63, these funds could be used to help fund many of the suggested projects, including sidewalk and crosswalk improvements, medians, lighting and intersection safety improvements.

Surface Transportation Program

This federal fund is worth $500 million per year in California, and is usually programmed once every two to three years by the regional transportation planning agencies. STP funds are highly flexible funds for all sorts of capital projects, and could be used to fund any of the suggested street design projects.

Hazard Elimination and Safety Set-Aside

This federal safety funding provides $60 million per year for California, and is controlled by Caltrans. One-third of the funding is only available for state highways, one-third is locally-oriented and may be used for traffic calming, bicycle and pedestrian projects, and one-third is currently directed into the Safe Routes to School program.

Safe Routes to School: (Assembly Bill 1475)

This program currently directs $20 million per year in funding to projects providing safe passage to children walking or bicycling to school, including building bicycle paths and lanes, constructing sidewalks where none exist, and implementing "traffic calming" programs in neighborhoods around schools.

Bicycle Transportation Account (BTA)

This state fund, administered by the Caltrans Bicycle Facilities Unit, can be used for numerous items that aid cyclists, including median crossings, bicycle/pedestrian signals and bike lanes. SB 1772, which took effect July 2001, increased funding for the BTA to $7.2 million per year for Fiscal Years 2001/02 through 2005/06. After 2005/06, annual BTA funding will be $5 million. For Fiscal Year 2001/02, the maximum an applicant may receive is $1.8 million. BTA funds pay a maximum of 90 percent of the cost of an eligible project.

Environmental Enhancement & Mitigation

This state fund is worth $10 million annually. Applications are accepted yearly, usually in November, by the California State Resources Agency in Sacramento. Two categories could be appropriate for improving Highway 63: the Highway Landscaping and Urban Forestry category is designed to offset vehicular emissions of carbon dioxide by funding the planting of street trees and other suitable plants. The Roadside Recreation category has been used by cities to fund parks and trailheads along streets.

Congestion Mitigation & Air Quality

This federal fund, worth over $300 million per year for California, is distributed to regional transportation planning agencies in areas with air quality problems for projects that improve air quality and reduce congestion. Bicycle and pedestrian improvements are specifically eligible for these funds.

Federal Transit Funds

California receives close to $1 billion annually in federal transit funds, and these funds may now be used not just for buses and trains, but also for projects which improve transit stops and bicycle/pedestrian access to them.

GRANTS AND LOANS TO COMMUNITIES

US Department of Agriculture (USDA)

USDA has a number of grant and loan programs that can be utilized for the purposes of improving the economic and environmental climate of rural communities. USDA funding is open to agencies ranging from local and state government agencies to public and private non-profits, as well as for profit entities.

These programs include Business and Industry Direct and Guaranteed Loans, Rural Business Enterprise Grants, Rural Business Opportunity Grants, Rural Economic Development Loans/Grants, an intermediary lending program, and Community Facilities Loan/Grant Program.

USDA /US Forestry Economic Program provides grants ranging from $10,000 to $30,000 for projects that help communities organize and develop community action plans, and projects that will help diversify local economies and develop or enhance forest and natural resource based industries.

Community Development Block Grant (CDBG) Program

Under the State Small Cities Community Development Block Grant (CDGB) Program, rural cities and counties may apply for funding for a broad range of activities ranging from establishment and operation of revolving loan funds and construction of infrastructure improvements to construction of new housing and community facilities. Applicants may also apply for funding to conduct planning studies and to write grant applications relating to the above mentioned activities. Funding programs under the CDBG Economic Development Allocation include the Economic Enterprise Fund for small business loans, Over-the-Counter Grants for public infrastructure associated with private-sector job creation, and Planning and Technical Assistance Grants. Applications under the Economic Development Allocation will require a job creation/retention component. Potential projects include street and traffic improvements, water system expansion and improvements, and sewer system expansion and improvements.

Economic Development Administration (EDA)

Funding is available through the EDA for activities such as infrastructure improvements and planning projects. Programs under the EDA grants include the Public Works and Economic Development Program, Research and National Technical Assistance Program, and the Technical Assistance Program (Local). Potential projects for EDA grants include water system expansion and improvements, and sewer system expansion and improvements, and construction of publicly owned facilities (including demolition and renovation.)

California Department of Parks and Recreation

Through the Park Bond Act of 2000, the California Department of Parks and Recreation, as well as some other State agencies received funding to provide grants to local agencies. Through Parks and Recreation, the grants are being used for activities such as acquisition, development, and rehabilitation of park and open space land, as well as community centers and other facilities. A number of the programs have already stopped accepting applications, however, there a few programs left utilizing Park Bond 2000 funds. These programs are one-time monies, but there is the potential for future programs and recapitalization of these present programs through future bond funds. Potential projects in this area include community centers and seasonal fields.

Caltrans Planning Grants

Caltrans provides a number of grant opportunites that could be pursued in reviatlizing Cutler-Orosi. The deadline for the 2002-03 grant cycle has passed. For a detailed descrtiption of these grants, see the Caltrans Transportation Planning web page: http://www.dot.ca.gov/hq/tpp/grants.htm.

Community-Based Transportation Planning Grants

The source of Caltrans grant funding for the Cutler-Orosi Community Design Charrette, the funding program funds transportation/land use planning projects that support livable community concepts.

Projects may address a variety of topics, including long-term sustainable economic growth, mixed-use development, pedestrian, bycycle and transit linages, and restoring a jobs and housing balance.

Caltrans Environemental Justice Grants

This program funds one-time demonstration projects for environmental justice in order to create and highlight the most effective processes and policies. Public outreach efforts, identification of minority and low-income community needs and concerns in transportation are among possible project types.

Caltrans Transit Planning Grants

This grant funds public and intermodal transportation studies in rural transit areas of California (population 50,000 and less.) Eligible potential studies include short-range transit development plans, ridership surveys, and site selection studies.

Inter-Regional Partnership (IRP) Grants for Jobs Housing Balance

The California Department of Housing and Community Development has competitive grants available to Councils of Governments, two or more sub-regions within a Council of Governments (CoG), or to counties working in collaboration with the state and federal governments to address the impact of substantial jobs housing imbalances. For more information: http://www.hcd.ca.gov/ca/.

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Appendix

Focus Groups

Friday, November 2, 2001

A series of focus groups were conducted on Friday, November 2, 2001. Dan Burden of Walkable Communities, Inc., explained to each group that the Consultants wanted to hear the concerns and issues of the community. Comments of the facilitator are in italics.

10:15 Group 1
Tulare County and Caltrans Staff

What you most wish to get out of the charrette?

- What people have told me is they really want the sidewalk completed and the safety issues with SR63 resolved. It would be ideal to have something feasible that also allows the community to empower themselves.

- Being brutally honest, we maintain the street and storm drainage system. It is difficult to do with limited resources. He would like to see them incorporate so they can band together and use the resources to concentrate efforts. Sand Creek does carry a lot of storm water and the local storm water system ties into it. There is a lot of trash and weeds. We don't have manpower to adequately maintain the facilities. The flood control and storm drain facilities are not comprehensive.

If they were to incorporate do they qualify for funds?

- The priorities in maintenance are based on volume of traffic on roads. The minor roads here don't have a priority in terms of traffic volume. The county maintains very few subdivision roads.

People brought up the lack of lighting. Does the county cover that?

- The county has a policy regarding warrants based on the number of houses. But if the lights are on SR 63 they are paid by Caltrans. Some come out of mitigation. As a rule any added lights would become part of their responsibility.

- LEGACI grant portion of this has done a retail leakage analysis for the area. The road between the two only supports 10 or 11 thousand cars a day and is not a main road. It is too wide and not attractive. Ingress, egress is difficult. Where speed limit is posted 40 one cannot drive that slowly there. The highway between the two communities is one of the most significant to be looked at independently of whether or not the community incorporates.

- SR 63 is used to get to Kings Canyon National Park, but as you come from the west (Dinuba)

you are directed via signs to turn before you arrive in Cutler. Driving north from Visalia you do come through Cutler Orosi to access the park.
Some numbers are available for regional nature of traffic. (*Note: Follow up on this remark failed to reveal any traffic numbers that define the origin or destination of travelers using SR 63 through Cutler and Orosi.)*

- I would like to see how the visioning project fits into the long term planning concept within the community. We would like to see how the specifics of the design plan in terms of facilities and service fit into that picture.

- The local government association develops projects but it isn't necessarily driven by the community plans. Most community plans are not regional.

- Transportation Concept Reports: The system was formed a year ago to start renovating the reports. The reports now include multi-modal transportation, but there is not one for SR 63 – it may be a year off. It isn't in the pipeline now. Each state route transportation concept reports take into account land use, etc. We talk about the regional significance of things such as traffic calming.

- Clearly identify the project: site specific recommendations.

- Financing possibilities. Would like the land use interaction and how it fits into the vision and what about in between – what is the community thinking about for its future?

- This project is exciting. I would like to see more cohesiveness in the community, which has to do with the socioeconomic conditions but also to change the community to create ownership.

- The vision could serve as the foundation for the Transportation Concept Report. TCR's don't involve financing. They talk about traffic crash data, projected growth and how that facility can be modified to meet the needs in 2010 and 2025 for the facility. It is based on standard transportation models, which is sometimes a best guess.

- I have a store I considered tearing it down and put some other type of project. I know across the street someone purchased property and converted it to commercial zoning. There is interest, but the main problem is the sewer system. That whole area is a flood area and needs to be raised.

- Beautification is great and fine, but for long term development process we need to be able to maintain it. I hope we get some type of commerce thing going. The infrastructure and sewer system is maxed out and preventing development.

- Land use transportation linkage with SR 63 acting as a main street highway connecting these two communities to link the two places.

- Developers are interested in jump starting the community in a public/private partnership.

- I would like to come up with well defined conceptual ideas to provide a basis for the county, Caltrans, the Redevelopment Agency and community to complete a more detailed plan that might be acceptable to all parties. The idea of traffic calming, street unification, etc, is not a very common concept in rural communities in Tulare County, although some of the cities are doing it. Visalia is the only example where it was tried (in a residential area), but they haven't done it since.

- Very concerned that unrealistic expectations will be created. The process next Monday needs to reveal components that we can implement in the short term. We need to start with small things that are steps in the right direction, such as completing the sidewalks. Completing ADA ramps in commercial areas. Maybe doing street trees treatments in Orosi. In Cutler, some of the store owners have talked about streetscape and tables on sidewalks and umbrellas. I don't know how feasible that is in a community where the crime rate is so high. The redevelopment agency might look at cost sharing with low interest to improve store fronts. I think we should look at a twenty year vision and it will take an open mind and a willingness to think out of the box if we are to drastically change the appearance of Cutler Orosi and the function of SR63.

- Some curb and gutter alignments and light standards will have to be relocated to install ADA.

What is the possibility for building curb extensions instead of moving utilities?

- I don't think you can and maintain what you have for traffic. If you are at 413 and 63 in the morning when people are going to school and work you really need all the lanes you currently have. Peak month ADT 14,000. From an engineering perspective I don't understand the concept of reducing the number of lanes. People come to us and want streets widened.

Does the community have concerns about crossing the street?

- Yes.

- There is a TIP project on 416 which will make it 4 lanes from Dinuba to 99. It is partly for truck traffic.

Does that traffic pass through Cutler Orosi?

- Not all of it. Dinuba has a lot of industrial commercial activity.

- TCAG does the modeling.

- 1997 aerial photographic (aerial map displayed by participant). Software reconciles maps to streets. Red line is proposed city boundary.

Is Sand Creek all on the west side of 63?

- It crosses SR 63 just n or s of Avenue 413. Hard to maintain: difficult to get equipment. A lot is hand labor. Non-native growth is a problem. There was a plan in the late 80's early 90's for the soil conservation service to reroute it and install walkways and recreation facilities, but the funding wasn't there.

- Community identity is an issue. They need to establish their own destiny. Community cleanup, structure, important.

One of the ideas is for the community to strengthen its economic base. What is the engine here if the community could work together to make this a place that really works?

- Wes Ervin, Applied Development Economics: In the red area, 10 – 12 thousand people (16 m in Dinuba) There is a lot of buying power, although lower on average (3300 households) there are a lot of food and auto stores. Some clothing and personal services; not enough leakage here to support a K-mart or department store. So people do leave town to shop. There is potential for more of the same. The retailers here offer multiple lines of merchandise. A food store offers toys, sporting goods. What we've found is there is not a lot of potential for additional stores, big furniture stores, for example. But there is potential for small one. To revitalize commerce we recommend concentrating commerce: creating a sense of place where someone can go not just to shop but to pick up kids, go to a movie, do other things; linked activities. Spending is concentrated. That isn't here, especially with two centers. Basic services could be supported. More restaurants could also survive.

What other stores?

- Smaller furniture stores, variety stores, specialty food stores. Small truck farm and farmer's market could be supported by this area.

- There is a flea market held now on Road 124 near the intersection of 412.

- If it became a center I could develop an office complex that could be leased to the city. There was another entity that wanted to rent space but could not because it sold out.

Is mixed use development currently allowed, or do the rules needs to be changed?

- The major inhibitor to community development is the condition of the wastewater treatment plant. Unless the attitude of the two public utility district changes I don't see that changing. USDA offered a three year funding program of $12 million and no one followed up on it three years ago. These offers usually come with strings attached.

- Recently built apartments in Cutler have made the street less safe. Zoning changes prevent businesses from locating there.

- There is a real lack of sense of place when I come here. Pay attention to giving cultural context to the place. If there was some way to combine cultural and natural that would be great.

- Some successful markets combine with swap meets in some places.

- Mercado concept can be designed nicely. In Lindley, California they are doing an acre with a public plaza with housing and commercial.

- If Cutler Orosi were its own city it would have more control over its destiny. They are subject now to county planners, supervisors, and plans. There is a perspective of creating uniqueness

that it creates problems because planning staff members and engineers have to look at different stuff for each and every community, which is not desirable to them. In some respects, in order to achieve its goals the community has to incorporate and do especially long term stuff on their own. You can tell we are in a box here. If we can reach a point where we can at least cooperate on some of the major thoroughfare and funds some things that are not too dramatic we may have success. These are real problems with getting things changed.

Group 2
Chamber of Commerce, Community Service Agencies, Local Businessmen

Share with us what it will take to make Cutler Orosi a better place twenty years from now. What do you want as an outcome for this process?

- Interested in hearing your ideas to take them back to the county. My experience is limited, but we would like a plan that creates many partnerships between Caltrans, the community and the county. Empowerment with the citizens to work with agencies is ideal.

- The economic analysis piece of this: my impression is this is a rural crossroads that got big, but there is no sense of downtown or place to stop. There is no place to walk. It grew up around a factory that is no longer there. Neither place has a downtown or a place where you go for commerce without getting in a car and having to go 2 or 3 places.

- I would like to see an incorporated community with people working together. In Cutler there is Kiwana's Club, Latin-American Club, and Lions Club.

- One of my ideas would be to see us plan a city because we are working hard to achieve that goal.

- I would love to see a central place that reflects the cultural background of the area. A plaza with a fountain, a place to hang out, like in the Latino communities would be great on warm summer nights.

- Facilities that can be used by both the school and community such as gymnasium, swimming pool where the kids can go. We don't have proper walkways. Better walkways on some of the smaller streets, too. High school has a gymnasium. There is no pool except in Dinuba. An area where families can picnic, go to the pool, and share facilities with schools.

- Schools are a focal point in the community now. I would like to see the schools be more inviting and safer to get to, both during and after school. There are issues that need to be addressed. We need to be more of an asset to the community – something that looks nice and where there are activities occurring.

- Schools have adult education and sporting activities. We do have a couple of community centers, but there is no open gym, for example. I would like to see people use it that way. A place where kids can go, be safe and do something off the streets. A little cooler in the summer and warm in the winter.

- Kids can come after school, but the libraries close at 4:30. About 300 students at four sites do an after school program. Small county library here open a few days a week.

- In come communities the school and community go together to build joint facilities so school uses them during hour and the communities use them other times.

- My main concern in the long term picture is the commercial development. We need a viable business base to maintain any beautification. My main concern is the sewer system and the infrastructure for non-seasonal job creation.

- The streets here are not walkable. People coming to church don't have a place to walk. We need a placito. This town has beautiful people who want to better themselves. The street is dangerous for the children. The parents are involved here. The street needs to be improved.

- More flood control drainage and beautification of the entry to Cutler Orosi, sidewalks. Some residents want to put sidewalks in front of their homes, but it can't be done because of the flood control restrictions. They have beautiful homes but their sidewalks (Ralph and Second Streets) are dirt. <u>One person installed a sidewalk and was forced to remove it because the county flood control person said it wasn't compliant.</u>

- Beautify the sidewalk we started. It joined Cuter Orosi and many families walk. Last year you showed us a slide where someone put a bench that said please sit down. We could do that locally. We should also put some lighting there. We could get free trees and plant them. We planted 180 trees in a couple of weekends. We as residents of Cuter Orosi must take on some responsibility. If we start that people who are headed toward the Sequoias will want to stop here to eat. It is a small, inexpensive project. We are willing to do something.

- The roads in this area are terrible for refuse pickup. They fixed roads on 112 and 408 and they had 3 or 4 roads torn up at the same time instead of doing one and then moving on to the next one. Orange Cove is an example of a place where new things are being built. My wife has girl scouts, but there is no facility. We need a gathering place where people can use the facilities.

- In 1959 Orosi was pretty nice. I would to see nice residential areas with sidewalks. Paint alone would help. The only place I feel safe at night is in my neighborhood.

- Single men go to the area near 418 in the evening. Cutler has very high numbers of people in each home –mostly rental. More homes in Orosi are owned.

- Vacant lots and abandoned cars are a problem. Owners just leave them. Another person mentioned the county is helping with this now.

- In 43 years our two town have deteriorated. I want the school children safe. All these areas where we have such terrible traffic should be fixed. I want everything everyone said, but especially for the children.

Why hasn't the community come together until now? Why is now the time? Why has there been this devaluing of property and place? What happened?

- We have had a lot of local people, long term residents, move away instead of jumping in and saying let's do something about it.

- People turn their back on problems when they feel helpless. In fact we all have the power to act. What has brought it together is the belief that we will someday become our own city. There is a chance to be the architect of our future. The county prioritizes its need countywide. It comes back to us. What are WE going to do? We all want to take accountability for our actions. We've watched progress go elsewhere.

- The wastewater and drainage system is the limiting factor on growth in this community. What can we do together to improve the infrastructure? It's been discussed, but there has been no meeting of the minds.

- There are three water boards. Their engineer and their attorney say we need two water boards even though some communities that are ten times bigger than we are only have one. It doubles the spending on everything. Merging the water districts could help. There are 9 hookups (bathrooms) left in Orosi.

- A lot of homes are sold to two families to qualify for the mortgage.[so they use more sewer and water per household]

- In Orosi there are water meters. They are studying that now to get everyone metered. A meter system and being able measure dwelling flow would identify code violations. Otherwise it is very difficult. One reason they can't meter them is it would be too costly to move them.

- The migrant workers come here to work, but we don't want them. We need housing for them, but their illegal status is a problem. People rent rooms to single men. Many people live in home with only one restroom. You don't just find a place for a family of eleven.

- This is a statewide problem. There is money for investors, but the problem is if you don't get the paycheck you can't control the money. Most of them live and ride with the contractor.

- There are a few examples, one in Sonoma County, for single farm workers. The wine growers have put up buildings in the vineyards that are like barracks. This provides a clean, safe place for the men to live in. The farmers did it themselves because they saw a benefit in having the same people come work year after year. There isn't as much cohesion here. The labor camps will only take family units.

- Villa de Guadelupe started as a dream among 8 residents. The medical community is now taking credit.

- One person said the flashing road signs were very effective in getting people involved. Some community message board like that would be great.

- The local businessmen are angry when people set up illegal, unlicensed businesses along the roadside. How can we take those people who are entrepreneurs and make them contribute to the area?

San Louis Obisbo used to have a problem with kids drag racing so they closed the streets and held a farmers market. Now it is a regional event that attracts many. Why hasn't Cutler Orosi incorporated before?

- We lacked the commitment. But if the study comes back and says it isn't feasible it won't happen. The Board of Supervisors is giving us 1.6 million dollars to get started. It was tried in the early 60's. Other places have been successful in doing this.

- It will take a great city manager to make it survive. The state has established guidelines. We have to have a city manager, economic development officer, and building inspector.

- A consultant was hired to study it 3 ° months ago. Costs are being tabulated now. Late this year the preliminary results will be received. Phase 1, $27m will determine if it is feasible. Phase II, $22 m will provide more detail.

- Sarah Reyes didn't paint a very optimistic picture. The County will apply for $400,00, from AB 431 grant funding opportunity, but the most immediate benefit would be to assist a business locating in Goshen that is ready to go and will create 200 jobs.

- There is relatively little industrial property in the communities to create the value needed. One of the biggest costs is fire and police.

What haven't we discussed?

- Money. Construction and maintenance.

- Expansion of the sewer system. Freedom Homes does have 39 units of sewer capacity. But that should be the number one priority.

- One engineer is seen as stopping progress toward incorporation and expansion of the sewer and drainage system.

- USDA would love to consider a three year funding program for Cutler – Orosi and would set aside $12 million to do that. The PUC district has applied for $1 million to do something. A preliminary proposal needs to be submitted to the USDA office.

- Water Board is elected or appointed [conflicting viewpoints on this].

Focus Group Meeting with community residents
Villa de Guadalupe meeting room
November 2, 2001

36 personas, y seis ninos

- Youth center for children
- Clean up the downtown
- More safety
- Better lighting along highway
- Signal lights on 63
- Senior residence
- Day care center
- Safer crossings of 63 and other roadways
- Very poor drainage today
- School bus is needed when weather is messy
- Garbage and trash is not picked up
- Traffic light 63 and 408
- Better crosswalk markings
- Safety, security, slower traffic speeds
- Many people have been hit on 63
- Highway 63 is too wide, so people drive fast
- Gymnasium
- 25 mph is posted, people are doing much higher speeds
- Not enough police enforcement today
- 35 mph for main highway speed in town
- Improve the sidewalks
- No bicycle lane on the highway
- Beautification and gateway
- Gateway with a roundabout
- Trees
- Similar treatments to Orange Cove
- Wait for the bus for up to two hours
- There is no shade
- Para transit system
- There is on-demand transit for medical, but not for shopping
- No benches
- Red light cameras
- Supermarket — A larger supermarket
- Very clean in Dinuba
- Orange Cove, Selma
- Many events, there is a major, music, place to have a party
- We need somebody who can support us…this area is lost
- We need something that is better for our children
- Somebody in government needs to represent these people

- Farmers market
- Swap market charges to enter just to see what is inside
- No place to play soccer
- Kids need a skate park
- Too many taco stands
- No light in the park. It is so dark that someone could get killed, and no one would know
- Teenage activity center. Basketball and many other activities
- Plaza is needed. Where should it be?
- A large Mercado
- More housing
- Not enough housing that is affordable
- No houses to buy

Focus Group Meeting with church leaders and community service providers
Villa de Guadalupe meeting room
November 2, 2001

Roger Neufeld, Open Gate
Tim Rossiter
Jimena Ruiz Castillo
Lupe James, Cutler Orosi School District

Is focus on just physical or also the social, economic.

Many good people don't work here. They drive in. It's not a city, just a country town. LA Club is working on incorporation to get govt. funding and pull themselves out of the pit. County has limited resources and this is the tail of the county so they have to pull hard. Previous supervisor was from this area.

Community has been going downhill for many years, since 1970s. At same time Hispanic population has gotten more affluent, but there are drug and gang problems. About 50% of children aren't going to go far unless they leave. Only opportunities, in farming and farming has gone to the pits.

Very large haven for undocumented workers. About 10k.

Villa de Guadalupe has been biggest spark in last few years. New McDonalds also.

There are unique problems. Wealthy people are moving away, don't want to invest. Most of resources that have come in are from grants. Are becoming dependent on grants. Dangerous. If businesses would come in there is a large workforce. Older population doesn't have many skills but that's changing with younger population. Have some very good people here who want to make it go. Can't do it without outside money.

Local people have to want to make things happen. Work to make things happen. People who came into this area were farmworkers, not very educated. Hispanics who are making things happen are usually

second generation. Need opportunities to come through business. Kids ask what can we do in this community once we get an education.

Not sure what to do to change. People who live here have to want it to change. Undocumented workers don't have vested interest.

Latin American club is trying to make the biggest difference. Older population who were leaders don't want incorporation because they perceive that taxes will go up. If create local govt. then they can go after grants.

Overall property taxes will not change unless they decide to impose sales tax.

Many residents of this complex are below 50% median income for this county. Lot of high school dropouts. High percentage of teenage pregnancy. Kids don't see a lot of opportunity. Key to focus on economic development.

This community depends a lot on grants. Tried to get grant for after school activity. Had to partner with school district which has $. Catch 22. Create dependency for several years but when grant runs out then what? Need more economic development.

We do housing, not economic development. Tax credit project. Below market housing but no one is paying rent for you. Have to have income to live here. Stepping stone to buy a house because of savings from lower rents.

City needs to incorporate. May not happen soon. Need to open up shops and create more housing. Local shopping. Everyone who wants to shop has to go out of town. No store.

Need recreation for kids. Need YMCA type place where they can go and be in safe place. School is playing that role but are limited to number of students we can take and can only take at-risk children so end up punishing children who do well in school.

Save the Children is coming to town. National organization that saw study in which Cutler was listed as a very poor town and decided to focus on this area. Were here last week.

Feeling of hopelessness. Nothing to keep people here. Have to get interest in doing something.

Whole road between Cutler and Orosi should be 35 mph. People speed on that road, don't wait for pedestrians. Lots of people who don't have transportation. Very dangerous place. Lots of accidents. Lots of DUI and people who don't drive well. People will get aggravated with slowing down but should

Sidewalk should have been built further back from road. No trees. Should have been set back form road.

Older people don't walk much. Some walk across where the park in Cutler is. But many of them get around by car.

Transit. Bus goes to Dinuba and Visalia four times a day. County transit.

But still have to go to Dinuba to get WIC (milk) paperwork so some mothers miss out because they can't get to the office to get their coupons (?). Are trying to get them to locate in the community building that school district leases from County.

Why was road widened.

Not clear. May have been State Senator Rose _____ . But it is an unusual section.

Widening of 416. Best Buys has a large warehouse distribution center in West Dinuba so will widen the section of the road between 99 and Dinuba.

Water District. Scuttlebutt that folks who work there get paid by both so don't want to merge.

Kaweah container is limited in business they can do because access is very limited. Cul de sac built to industrial standards but problem is water. Quantity and quality. In early 90s redevelopment agency developed industrial park but well was contaminated by DBCP. Owners sued makers of DBCP. If well can be fixed then water problem can be cleared up. But also a problem of inadequate water in South side of Cutler. Only capacity for 9 new residential units.

Based on our experience we were able to build this project. Were successful because investors got tax credit. If there were other tax benefits for corporations to invest here that would help bring economic development into this area.

Cutler Orosi is part of Tulare tax increment area; companies in this area are eligible for tax credits. Also part of USDA champion community; no tax credits but does make area more eligible for federal funds.

Problem. Cost of living has increased. To purchase whatis needed minimum wage would need to be $10/hour which would destroy agricultural. Need more types of housing. Labor camps, e.g. A lot of folks live below poverty level. Need houses that could be rented for $2-300/month. Could be done if built houses for $20k. But today would cost $60k to meet all county standards. So either have to build more housing like this or build less expensive types of housing. But could build nice communities with less expensive housing. Need outside money for this area.

This is like a gated community. And there are all kinds of rules for people who live here. People watch who comes and goes.

Streets and Sidewalks	
Better lighting	50
Finish sidewalks	39
Fix existing roads	30
Traffic signal at SR 63 and Avenue 408	28
Overpass in front of Cutler School	28
Enforce Speed limits	20
Stop the “drag races” on SR 63	17
More Sidewalks on J419	15
Truck Bypass	12
Storm drainage curbs and gutters	10
Stop sign on Main Street	9
Narrow the roads	5
Traffic Signal at SR 63 and Avenue 419	5
Slow cars	4
Remove signs from sidewalk	4
Total Streets and Sidewalks	**276**
Activity Centers	
Recreation Center	72
Park in Orosi	28
Plaza in midtown w/water fountain	24
Soccer Field	23
More youth activities	23
Walking trails	8
Total Activity Centers	**178**
Beautification	
Trees, benches, lights, flowers	54
Clean Alleys	30
Pride in our community	15
Underground utilities	10
Better use of open space	4
More trash cans through the town	4
Total Beautification	**109**
Retail / Commercial	
Fix and paint buildings	32
More businesses and employment opportunities	29
Expand Sewer System	14
Total Retail / Commercial	**75**
Residential	
More affordable housing	22
Build houses in open lots	20
Total Residential	**42**
More police	6
MRS. LLOYD’S FIFTH GRADE CLASS:	
Fix the bumpy roads	12
Safety lights in street	3
Wider sidewalks	1
Fix broken sidewalks	1
Streets and Sidewalks	17
Make the walls clean from graffitti	7

Clean Place	0
Move fire hydrants	0
Beautification	7
Build Arcade and Mall	34
New park	18
Community center	8
Closer stores	1
Activity Centers	61
Affordable Housing	7

Public Design Table Presentations
Saturday, November 3, 2001

The citizens at design tables presented their maps at the close of the session. The following bullet points highlight their comments:

Table 1

- Paint
- Clean
- Improve vacant lots as homes
- Commercial center: capacity for many customers
- Incorporate
- Facilitate home ownership
- Create advisory council
- Enforce cleaning ordinance. What is the county responsible for?
- Trash pick up
- Vigilance of residents within the community
- Street lights
- Senior retirement home and recreation center
- Housing and health commitment – they will knock on doors and invite people to a meeting every two weeks
- Sports center behind Villa de Guadelupa
- Sports field; boxing club; karate instruction within community center.
- At the park, additional barbeque tables, grills.
- By water tower, expand area; more benches (across from school)
- Narrow streets: too wide
- School crosswalk improvements

Table 2

- Get rid of truck traffic: roundabout considered on south end of Cutler to encourage drivers to go onto 120 to access 180.
- Divert traffic into 120, then send them north toward Orange Cove, which would be a faster route.
- Roundabout on 416 and 120 is a proposed city boundary for the new city. This is a perfect place for an entrance.
- Cutler Latin American Club, the Chamber of Commerce, and a local church have committed in the past to maintain the gateways.

- 75 kids exit buses near the Cutler Market. They are willing to put the palm trees where they are best. They are going to put 15 tables in this area so the children can sit instead of standing as they wait. Medians and lighting will bring the town to life. In Cutler there are a large number of people who walk, but they are afraid because of the lack of lighting. They envisioned a Farmer's Market at Orosi Drive and First.
- Between 408 and 416 the piece of property will be a major source of commerce once the sewer capacity is improved. Zoned for commercial use.
- There are currently no sidewalks between Orosi High School and Golden Valley Elementary. [Add sidewalks]
- On El Monte, there is a crosswalk where speeds are too fast.
- Roundabout 400, 416, and 120. Slows traffic coming into town.

Table 3

- Property between Cutler and Orosi
- Zoned commercial; entire piece has to be raised a foot because of the flood plain. So when the sidewalks are added this should be considered.
- Corner of Ira currently being considered for commercial.
- Would like to develop something if sewer capacity is increased; no sidewalk on east side of the street; would add them during development.
- Install medians to slow traffic down
- Major problem: crossing street, Traffic too fast.
- Add angled crosswalks
- Add lights
- Add trees
- Underground utilities
- Need a crosswalk at Ira (bakery) Many people run across the street here.

Table 4

- Improve parking in Orosi
- Improve bleak landscape: trees
- Improve pedestrian crossings
- Add roundabouts
- Meandering sidewalk between two towns; very desolate now.
- Increase mix of stores in downtown; low-key thrift store needed.
- Add tree wells.
- Façade improvements: paint
- Parking lot on corner in downtown: widen sidewalk; add tree wells; improve appearance and safety for pedestrians
- Between the bank and next building: add tables, benches
- Across street, improve façade of the restaurant.
- Better lighting
- Better crosswalk signs

Presentation of Design Concept to Tulare County and Caltrans District 6 Staff
NOTES 11/5/2001

(The presentation began with Michael Wallwork giving a presentation of the design concept. Comments and responses are recorded)

- Michael Wallwork saw business owners sweeping in front of Orosi.

- Post Office in Orosi is not in operation.

- Steve Worthley: Improvements that have been made include:

  - 63 — 408-416 sidewalk./curb
  - 63 — north of 416 — sidewalks added seven years ago
  - El Monte Way — from El Monte School – sidewalk/curb

- Concern expressed by Caltrans staffperson over irrigation costs with tree irrigation.

  - Michael Wallwork: In Australia, Eucalyptus trees have preformed very well in Australia with regular watering at the onset and then have needed no irrigation

- Caltrans staff person expressed concern over : Emergency Vehicle Access, Especially at interstection. There is concern that traffic calming will block access.

Michael Wallwork:
  - Put optical sensors for fire trucks to change lights for trucks.
  - Ambulance, location of hospital. We need to find this out.
  - There are many more injuries and deaths from traffic accidents than fires, we need to come up with a road system that balances all of the communities' safety needs.

- Caltrans staff person: This is just a pedestrian design.

- Michael Wallwork Response: Bulbout will improve traffic flows in front of Cutler School.

- Caltrans Traffic Engineer: "I think you're solving one problem and creating another problem." Need more study. Planting trees next to roadways in a highway situation is against guidelines. Fixed roadway objects this is not allowed.

- Michael Wallwork: Those guidelines are for rural highways. This is an urban/suburban area, this is a common error.

  - 20 feet — Michael Wallwork — that study

- Caltrans engineer — "You call this a low cost improvement, but tree root damage can be signified".
  - 40 – 80 years oak trees
  - tree retention – maintenance and operations cost.
  - Must be careful in tree selection and siting.

- Jim Brown Tulare County RMA: — Cutler Orosi residents want an urban street concept.

- Michael Wallwork — We need to make a life-cycle analysis on trees. In this context, trees pay for themselves handsomely..

- Wes Ervin — it's time for these communities to be given a decent quality of life.

- *(Albert)* Caltrans Engineer — Not adverse to the concepts being presented for the most part:

  - Bulbout — no problem
  - 416 — no problem
  - Medians/trees — concerned at higher speeds.

•Michael Wallwork: You can design the median so that injuries from collisions are very infrequent. If you set back any fixed object (signs, gateway sculptures) 10 (yards/feet?) from the start of the median and put landscaping and a vertical lift you will absorb a large amount of the automobile's speed. These type of issues can be addressed through good design.

Caltrans Environmental Review Staff person: • "Recognize what you're doing. I think it's a good idea." Caltrans could give up SR-63 in trade for Rd 120. Rd 120 is a good quality road.

  - Caltrans make the connection

(*There was general agreement that Rd 120 was a very suitable highway option. Wallwork noted that there was one piece near it's meeting with SR 180 that would have to be fixed*).

- TCAG — I don't believe that you are going to subvert half of the traffic on SR 63 with a bypass.

  - Traffic Calming projections from the County are better those from Caltrans. DOT's are usually higher.
  - Caution before looking only at ADI numbers
  - SR-180 will absorb more traffic

- Caltrans *(Ed)* — Why are we talking about streets and traffic calming?

  - Why not put in a CHP to enforce speeds?
  - Michael Wallwork It would be much more expensive and unless the CHP is assigned to the area for a large portion of time it will not be effective..

- TCAG — This plan would give Cutler and Orosi and an identity.

- Michael Wallwork — We have heard that the sewer system is at near capacity. Natural grey-water systems that clean the water through bio-filtration are very common in Florida. This water could be used to irrigate the trees and landscaping.

- Caltrans — You should include responsibility of development.
  - Rails-to-Trails at roadway.
  - All costs need to be included.

OFFICE (559) 445-6027
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February 22, 2002
Cutler-Orosi Charrette Draft Report
Community Based Planning Grant

Mr. Jim Ochs
County of Tulare Resource Management Agency
5961 South Mooney Blvd.
Visalia, CA 93277

Dear Mr. Ochs:

Thank you for the opportunity to review the Draft Cutler-Orosi Charrette Report. Caltrans understands the Cutler-Orosi Charrette draft report is a community-visioning document, as opposed to an actual program or engineering study/plan for proposed improvement projects. However, it is our intent to identify potential concerns early in the process to help facilitate future discussions and the implementation of proposed improvements within the visioning draft report. Comments are the following:

General Comments:

The vision document needs to lead to work that addresses the long-term (10 and 20 year) needs within the land use and transportation elements of the Cutler-Orosi Community Plan. This work is necessary to evaluate whether the proposed improvements in the vision document can accommodate the land use and transportation requirements of the community. It is important to assure the community that long term needs will be addressed.

Caltrans highly recommends that Tulare County proceeds to develop a master/action plan for implementing the projects identified in the document. A collaborative effort should be comprised of a local, state and federal partnership.

Tulare County should be aware there are traffic design and safety issues to consider and work with Caltrans District Design, Traffic Engineering, Traffic Investigations, Traffic Operations, Project Development, and Traffic Liaison early in the scoping phase to address the related issues.

There is no mention of public transportation and the opportunity of constructing a regional transit center and additional routes within the communities. This should be addressed within the document.

North arrows should be placed on all maps.

Comments Related to the Document:

Page 2, Executive Summary: The use of words such as “unsafe”, “danger”, “dangerous” should be avoided, particularly if data and information is not provided to substantiate wording within the statements.

Page 2, last paragraph: States that "removal of two traffic lanes are recommended for nearly the entire corridor." A direct impact of this action is reduced capacity of the highway facility. This must be addressed in the report including what studies, local planning policies, and mitigation will be necessary. Removal of travel lanes should only be accomplished in conjunction with identifying and improving bypass route(s) and commensurate land use policy.

Page 4, third paragraph: Caltrans has a planning process for state highway corridors for highways such as SR 63 (Road 128). The Transportation Concept Report (TCR) describes current and ~~planned~~ projected operation of a state highway corridor over a 10 to ~~20~~ **25**-year period.

Page 4, Process, Outreach and Publicity:

Ø What is PAC?

Ø The outreach and public participation techniques used for the Cutler-Orosi Charrette are to be commended and can be used statewide as a model for future projects. The use of Caltrans "message boards" or changeable message signs and the "Mariachis" at the opening event were a great way to attract public participation. However, this section should offer more detailed description of the community meetings on Friday and Monday nights for the purpose of future successful public participation efforts (e.g. how the Friday night meeting was broken down into short sessions, so it would not be too long or too technical in nature). Tulare County should consider producing the document or the Executive Summary in Spanish.

Page 9, Government Presentation: This section should include that Caltrans clearly expressed concerns at the focus group meetings of being given direction within the Director's Policy on Context Sensitive Solutions and flexibility for design, but that technical policy has not been defined by Headquarters. Another important concern identified by Caltrans was that roadway decisions and improvements be addressed and is consistent with the long-term transportation needs of the community.

Page 9, Government Presentation: The participant's questions and concerns included street trees and medians on SR 63. Caltrans has sponsored a study of street trees within medians of state highways. Tulare County should consider the final results of this study.

Page 13, text box: The final version of the Director's Policy on Context Sensitive Solutions has been released and differs slightly from the excerpt quoted. In the first sentence, "Context Sensitive Solutions" has replaced "Context Sensitive Design", and in the last sentence, "are " is replaced by "will be".

Page 13, second paragraph: This states that SR 63 was recently designated an STAA truck route. SR 63 has actually been an STAA Terminal Access route for many years.

Page 13 (and other parts of the document that refer to Caltrans grant for this project), third paragraph: Refer specifically to the "Community Based Transportation Planning" (CBTP) grant so other communities can identify what grants to look for in their own activities.

Page 14, Comments related to the proposed truck bypass:

Ø It should be noted that Caltrans recommended the relinquishment of SR 63, through Cutler-Orosi to facilitate proposed improvements, at the focus group meeting.

Ø Tulare County should consider an alternative route for the truck bypass. Consideration should be given to an east/west route further south of SR 201. This would be a way to avoid addressing additional traffic volumes from the proposed truck bypass.

Ø The report should include a discussion on the process of implementing the truck bypass.

- Cost
- Right of way
- Operations and maintenance responsibilities of state and local agencies
- Funding available to implement the truck bypass

Ø The Vision Plan needs to identify what improvements will be required, to accommodate the truck bypass, at the SR 201/SR 63 intersection, SR 201 mainline between SR 63 and Road 120, and at the SR 201/Road 120 intersection.

Ø SR 63 is an STAA truck route and it should be maintained as such. If this cannot be done, an alternative parallel bypass route should be identified and brought up to STAA standards.

Ø Many of the recommendations are more appropriate for a local street rather than a State Highway and should only be considered with the truck bypass, which will be needed to accommodate future interregional traffic volumes.

Ø The references to SR 180 in the third and fourth paragraphs need to be corrected to reflect the correct route (Avenue 416).

Page 15, Street Design Elements:

Ø Caltrans is not opposed to the use of roundabouts in general. However, each proposed location will need a detailed engineering study to determine its appropriateness. Please refer to Design Information Bulletin Number 80 at http://www.dot.ca.gov/hq/oppd/dib/dibprg.htm and the Caltrans roundabout task force for further details.

Ø An example of a roundabout on a California State Highway is recommended here rather than one in Florida. The illustrations should be labeled.

Ø Roundabouts should be designed to accommodate bicycles.

Ø Roundabouts may be landscaped with groundcover or low shrubs, but trees and other fixed objects are not appropriate. There are drainage and maintenance issues related to tree wells that may make them impractical. For emergency vehicle access, at least 6 m (20') needs to be provided between curbs and raised medians.

Page 17, Trees: Tree planting along SR 63 will improve the appearance of the state facility. However, it increases the maintenance cost. Caltrans recommends that an assessment district be established to fund the maintenance of improvements along SR 63 such as trees and other landscaping plants.

Page 21, Avenue 408 Village, first paragraph, third sentence: Proposed development along SR 63 is “required” to provide sidewalks according to State standards.

Page 22, Avenue 416 (El Monte Way) and SR 63, first paragraph, second sentence: The sentence should specify which corner (SW, SE, NW, NE) is being discussed.

Page 23, Site Specific Recommendations: See previous comments related to improvements for long-term needs (Page 9, Government Presentation). The first and second phase improvements need to consider the long-term transportation needs on the State highway system.

Page 23, SR 63 Corridor Recommendation: See comments on the Executive Summary (p. 2) and the use of appropriate wording.

Page 23, second paragraph:

Ø The placement of tree wells and reduction of the number of lanes north of Avenue 416 will require additional studies to determine their safety and operational impacts. Edgelines are not typically placed where there are curbs and gutters. Its installation may be considered if its purpose is to channelize bicycles. Installing and maintaining street lighting is the responsibility of the local agency

Ø Any shoulder less than 2.4 m, such as at tree well and bulb-out locations, mandates a design exception.

Page 23, Phase One, first bullet: Identify which side (E, W) of SR 63 the sidewalk is to be completed.

Page 23, Phase One, seventh bullet: This bullet should identify the number and locations of transit stops. The identification of facilities will prove helpful for funding purposes.

Page 23, Phase One, ninth bullet: See previous comments related to Truck Bypass (Page 14).

Page 23, Phase Two, first and second bullets: Define the parameters for these improvements (from where to where on SR 63).

Page 23, Phase Two, third bullet: Identify locations of gateway elements. Refer to Caltrans' policy on gateway monuments in the Caltrans Traffic Manual and Highway Design Manual.

Page 23, Phase One, eighth bullet, and Phase Two, fourth bullet: What is meant by "Implement site-specific treatments as recommended?"

Page 24, Railroad Drive: Refer to Caltrans' policy on gateway monuments.

Page 28/29 – There is a detailed discussion of landscaping recommendations and emergency vehicle access for the proposed roundabout at Avenue 416. These details should either be included with the discussion of all five proposed roundabouts or discussed generically for all roundabouts.

Page 32, Implementation: The section on implementation simply offers a listing of potential funding sources. It should contain a funding plan to address cost, feasibility, strategies, etc., to implement the specific recommended improvements. The proposed improvements should be included within the Cutler-Orosi Community Plan Update and regional transportation planning documents and process in general.

Page 32, Implementation: The implementation plan should be developed as a collaboration of partners to find ways of implementing suggested improvements on SR 63.

Page 32-40 – The "Implementation" section should include a bulleted list of tasks and should assign responsibilities. This will increase the probability of implementation.

Page 40 – The "Grants and Loans" section should include information about the following Caltrans grants:

- Community Based Transportation Planning
- Environmental Justice
- Transit Planning
- Public Transportation (including Elderly and Disable Transportation Services and Job Access Reverse Commute grants)

Here's some California Department of Housing and Community Development grants for inclusion:

- Downtown Rebound
- Interregional Partnership

Caltrans looks forward to working with Tulare County and the Cutler-Orosi communities to find approaches toward implementing proposed improvements on SR 63. If you have any questions, please call me at (559) 445-6027.

Sincerely,

JAMES HEINRICH
Office of Transportation Planning

Opening Evening Exercises Described

Background

The intent of these opening evening exercises is to engage the community in envisioning an improved future for their community while maintaining their attention and interest. This is achieved through a series of quick, fun, relatively simple exercises that nonetheless provide the charrette team with an understanding of the values and priorities of the audience. Children, youth and seniors can all participate.

Values Exercise

-Charrette staff/volunteers hand out five Post-It notes to each member of the audience. Facilitator asks each participant to identify the five most important values that they would like to see embodied in their community in 20 years. Facilitator instructs participants to write down one of these values on each of the five Post-It notes. Charrette staff/volunteers collect notes. On the following day, the charrette staff then records the number of times each value was selected. Similar values are bundled together. For example, "safety, peace, and comfort" may be put together under "safety." These values are then written on butcher paper and posted at the Saturday and Monday public workshops, and are included in the closing presentation.

Priorities Exercise

Facilitator asks participants to brainstorm actions and changes that they would like to see in their community (e.g., better street lighting, completed sidewalks, a recreation center for kids). Participants raise their hands and tell the group their priority when chosen by the facilitator. Charrette staff/volunteers record each action that is expressed on paper on easels. (Easel paper with adhesive on the top is recommended). This is a fast and fun exercise that gets the audiences creative juices flowing. One staff/volunteer should be assigned to record the comments in each language spoken by participants. (In Cutler-Orosi, one person wrote the responses in English and the other Spanish).

As the easel paper is filled with ideas from the audience, additional staff/volunteers take the filed sheets and tape them to an adjacent blank wall. After the audience has exhausted itself of ideas (~5-10 minutes), volunteers/staff provide participants with five sticker dots to vote on which ideas are the most important to them. Facilitator explains that participants may only place one sticker on each of their five priorities (i.e., they cannot place two or more of their stickers on one item). At the end of the evening or the following day, charrette staff/volunteers count the votes of each idea/priority. The top ten priorities are included in the closing night presentation and inform the charrette design team.

7. A COMPREHENSIVE ACTION PLAN

This Chapter combines the strategies in Chapter 5 with the funding opportunities in Chapter 6 into a stand-alone Action Plan — a series of individual goals destined to become reality. The detailed information presented earlier has been condensed into a workable action agenda.

A successful implementation plan requires:

1. An approved prioritized action agenda
2. A responsible lead organization or set of organizations with complementary responsibilities;
3. A set of incentives or investment priorities; and,
4. Commitment and momentum.

This plan shows how to accomplish all four. It also includes proposed time lines, recommended responsibilities, and expected results. Table 15 assesses the applicability of the funding mechanisms described in Chapter 6 to the recommended projects. Table 16 is a graphic representation of suggested roles and responsibilities for the various organizations interested in economic development in Cutler-Orosi.

The following action agenda should be copied, modified, debated, improved, and used for action by the community and its supporters. This is a starting point. The reader should feel free to separate this chapter and/or the individual Goals from the rest of the report. The rest of this report, the Charrette Report, the Water and Sewer Needs Report and other documents should become supporting information and referred to as often as needed.

The dedicated and cooperative efforts of each and every participant identified in the plan will be needed if its goals are to be realized. It is important for the community, including groups and individuals, to realize that it will take several years, even decades, to realize all the actions outlined in the following pages.

recommended county actions

Encourage the County and the Tulare County Redevelopment Agency to participate as follows:

- Continue to support the community as it organizes itself for community development. It is recognized that this will primarily occur through the Redevelopment Agency, but assistance will be needed, in particular, from many agencies within the county government.
- The TCRA should plan and conduct additional community meetings to refine and to realistically prioritize the recommendations made in the Charrette Report. Agencies and organizations that can take the lead for particular projects can be identified. Once this process is completed, strategies can be addressed that would lead to the financing of selected improvements over a period of several years.
- Refer the Charrette Report to the Long Range Planning Division of the Resource Management Agency with the request that it consider the inclusion of appropriate projects in the next update of the Cutler-Orosi Community Plan, be they transportation, land use, or infrastructure related.
- Encourage CalTrans to work with the community, the County, and the Tulare County Association of Governments to incorporate appropriate recommendations from the Charrette Report into a Concept Report(s), which would facilitate the programming of project development and construction funds.
- In its next review of the Cutler-Orosi AB 1290 Implementation Plan, encourage the Redevelopment Agency to program its participation in priority projects and activities.

Goal 1: ORGANIZE FOR LOCAL SUCCESS

Create the non-profit Cutler-Orosi Vision Implementation Committee (COVIC) as an overall development coordinating committee. The sole purpose of this entity is to promote prosperity in the Cutler-Orosi community.

Strategy: **Create a Statement of Purpose** that ensures the committee will enhance and support the efforts of its member groups,

Projects/Actions:

A. Establish a unified vision for Cutler-Orosi that all stakeholders can support and to which all can contribute.

B. ***Create and maintain a joint prioritized action plan, taking advantage of the strengths and capabilities of each stakeholder group. Form project-specific subcommittees as needed.***

C. Coordinate and monitor all projects undertaken by each development group, removing roadblocks, encouraging collaboration, avoiding duplication, and helping find funding to ensure each project is successfully completed.

D. ***Promote business retention and expansion by working with individual businesses, by encouraging business expansions to optimal sites, by steering resources where they are most needed, and by arranging appropriate training opportunities. Emulate the Main Street Approach***.

E. Get written notice of all meetings at the local, county, regional and state level that affect Cutler-Orosi and its future. Ensure a local representative attends. Make presentations and testify as appropriate.

F. Publicize all successes as they occur.

Organizations: The recommended board of COVIC would include 1 representative each from the Planning Area Committee, the Cutler-Orosi Chamber of Commerce, the Incorporation Committee, the Joint Powers Agency, the Cutler-Orosi Unified School District, an Orosi business owner or manager or a land or building owner, a Cutler business owner or manager or land or building owner. However, the community should carefully consider membership issues and needs and include representation from as broad a cross-section of the community as possible while still allowing for effective and efficient operations.

Funding Source(s): Committee at first needs only minimal support, for basic activities and to reimburse its members for their time and travel. Each member organization should provide a share of the initial support. The Committee should, however, seek ongoing funding to support a full-time "Community Coordinator" position, and a budget of $100-200,000 per year.

Expected Results:
1 —Cutler-Orosi should become much better known to the outside world.
2 —The pace of completing projects should increase.
3 —Scarce resources will be better used, with little or no waste.

Timing: Creating this committee should be the first action of the local leadership

Goal 2: ENSURE SEWER AND WATER SYSTEM EXPANSION

Ensure ongoing expansion to keep ahead of the pace of development. Sewer plant and water system capacity are currently constraining all new development, and are keys to future development. The JWA should understand that the community expects action.

Strategy 2a: Maintain continuous positive interaction with the Cutler-Orosi Joint Wastewater Authority (JWA) to encourage immediate expansion of the sewer treatment plant, as well as periodic expansion of the plant to provide an opportunity for steady economic growth.

Projects/Actions: A. The engineer for the two public utility districts has indicated that a preliminary engineering study has been prepared, and that the districts are planning to apply for a USDA grant/loan that would allow them to begin construction in 2004. The Visioning committee should help coordinate community input into this process and aggressively encouraged the District and USDA to keep the proposed expansion on track. This should occur before incorporation, because after incorporation the community will no longer be eligible for USDA infrastructure funding.
B. Publicize each milestone as it is completed.

Organizations: JWA, Visioning Committee, Cutler-Orosi PAC, Incorporation Committee, RMA.

Funding Source(s): USDA combination grant/loan for $4.6 million, EPA line item for $1 million. Total project = $5.6 million.

Expected Results: Although 100,000 gpd of discharge capacity has just been allocated to Cutler and Orosi, the sewer expansion is necessary for housing, commercial and industrial development in Cutler-Orosi.

Timing: The districts are encouraged to submit an application for funding and a completed preliminary engineering report to USDA within the next few months.

Goal 3: IMPROVE HIGHWAY 63, EL MONTE, LOCAL STREETS and COMMUNITY FACILITIES

Complete Phase I and Phase II Street improvements, and pursue truck bypass. Either the Vision Coordinating Committee or a special street improvement subcommittee should oversee this work. See the Charrette Report for details of each project.

Strategy 3a: **Complete Phase I improvements. A-D are critical safety improvements for pedestrians and school children. Publicize each successful project.**

Projects/Actions: A. Complete sidewalk project with TCRA funding. In addition, where possible, the TCRA should leverage local funds against state and federal funding sources.
B. Signal, median, reflective markings @ Cutler School (Cutler)
C. Pedestrian Crossing, median refuge @ 63 & Ave. 413 (Orosi)
D. Reduce 63 to 2 lanes w/ medians & tree wells past Orosi High School. Improve pedestrian crossings (El Monte to Clyde) (Orosi)
E. Turn lanes, medians @ First Street & Ave. 404 (Cutler)
F. Street narrowing @ PUD office on Orosi Drive (Cutler)
G. . Left turn lanes, bulbouts @ 63 & El Monte (Orosi)
H. Build transit shelters at bus stops.

Organizations: The TCRA, Cutler-Orosi PAC, Caltrans, the Tulare County Road Commissioner, the Vision Committee and its members.

Organizations:	The TCRA, Cutler-Orosi PAC, Caltrans, the Tulare County Road Commissioner, the Vision Committee and its members.
Funding Source(s):	Pursue all possible and realistic sources of spending including but not limited to CalTrans, the County, TCRA, USDA, the federal Economic Development Administration, and the federal Highway Administration. Some CDBG funds could be available in any given year if it these projects involve private sector job creation.
Expected Results:	For each project that is completed, dramatic improvements in public safety, beautification, and economic vitality will occur. However, it should be noted that obtaining transportation funds is a long-term effort requiring years of advance planning and prioritizing competing projects.

Timing: As soon as funding is arranged for each improvement, the project should be undertaken.

Strategy 3b:	**Pursue Truck Bypass of Highway 63 if feasible**. (See the Charrette Report for details).
Projects/Actions:	A. Support a bypass study, which could help create a downtown without the need to maintain a wide, fast thoroughfare. There will be economic impacts from this action. Identify the economic impacts of fewer trucks passing through Cutler-Orosi. Also ensure there is enough funding to ensure long-term road maintenance, assuming Caltrans transfers ownership. B. If appropriate to support, arrange for the existing highway to be deeded to the County (or the new city, whichever is applicable).
Organizations:	TCRA, the Resource Management Agency, Cutler-Orosi PAC, County Public Works, Caltrans, Vision Committee and its members.
Funding Source(s):	Ask CalTrans to fund the bypass study, all traffic improvements and signage changes, and ongoing maintenance for the existing Highway once it becomes the property of the County.
Expected Results:	Under County ownership, traffic-calming and beautification road improvements should be easier to approve, but may be difficult to fund, (i.e. a smaller pool of money supporting county roads). A special fund should be established specifically for this road.

Timing: These actions can be taken immediately, with the expectation that the study and bypass construction process will take 3 years or more to complete from the time that participating agencies are able to begin a concerted effort.

Strategy 3c:	**Complete Phase II improvements. Publicize each successful project.** See the Charrette Report.
Projects/Actions:	A. Roundabout at First Street & Orosi Drive (Cutler) B. Signal, bulbouts, reflective markings on El Monte @ Palm School (Orosi) C. Gateway median @ 63 & Railroad Drive (Cutler) D. Roundabout for 2 traffic lanes @ 63 & Avenue 408 (Cutler) E. Street narrowing along First Street (Cutler) F. . Median @ 63 & El Monte (Orosi) G. Reduce El Monte to 2 lanes w/ medians and trees east of 63 to Sand Creek (Orosi). H. Continuous landscaped median along 63's entire length, bike lanes, tree .

I. Large capacity roundabout @ El Monte & Road 124.

J. Replace signal with large capacity roundabout @ El Monte & Road 120.

Organizations:	TCRA, the RMA, Cutler-Orosi PAC, Caltrans, Vision Committee and its members.
Funding Source(s):	Pursue Caltrans and TCRA funds, USDA funds, and other funds for sidewalk and community facilities improvements. Pursue CDBG funds if private-sector jobs will be created.
Expected Results:	For each project that is completed, dramatic improvements in public safety, beautification, and economic vitality will occur.

Timing: Begin Phase II after Phase I completed.

Strategy 3d:	**Specific Community Space Improvements, Business improvements, and Community Facilities**. See the Charrette Report.
Projects/Actions:	A. Improve building entrances to the bank and the R&N Market (assuming a move is not feasible—See Strategy #4a). B. Create a small community garden behind the bank building between the bank and Masonic Hall. C. Create a controlled weekend farmers market in Cutler in the park across from the PUD. D. Create a common design theme for Orosi's central shopping area. Encourage local businesses to follow the new theme with their future improvements. E. Create a new (future) civic center, commercial plaza and sports complex @ 63 & Avenue 408. F. Plan and build a community center with swimming pool at new civic center. G. Add public restrooms at key locations. H. Publicize each success as it occurs.
Organizations:	Vision Committee and its members. RMA, TCRA, private business owners and land and building owners.
Funding Source(s):	Encourage the TCRA to create a Façade Improvement Loan Program. Small business loans from SBA, CDBG. Bank should consider creating the small space behind its facility. USDA funds for community facilities. Approach Gongs, who own the site of the future civic center location. Park funds may apply to the community gardens and community facilities.
Expected Results:	These specific projects will create local beauty, places to sit and gather, and reduce traffic congestion.

Timing: Actions A-C should begin immediately as separate beautification projects.

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Timing: Actions A-C should begin immediately as separate beautification projects.

Goal 4: RETAIL REVITALIZATION

Capture as much additional retail spending as possible. Beautify the existing retail centers. Currently, over $21 million in household retail spending is currently spent outside Cutler-Orosi in a number of retail categories. Existing businesses can expand their lines and/or add new lines. Or the community can attract entirely new stores.

Strategy 4a:

Encourage R&N Market to either expand on site or relocate nearby. Use all available incentives and assistance, including possibly swapping land.

Projects/Actions: A. Work with the Market owners and other property owners to jointly investigate expanding to the southeast corner of 63 and El Monte. This is the ideal supermarket location for Cutler-Orosi, because of the proximity of the bank and other shops. 1-1/2 to 2 acres is needed.
B. If A above is not feasible, support expansion of the existing store and its parking on-site by helping the muffler shop relocate to Highway 63, and/or acquiring land behind the store. This is the second best location.
C. As a last resort, the R&N could in the future relocate near the corner of 63 and Avenue 408, which is owned by the market, as long as the store complements and does not interfere with the future civic center.
D. Publicize the outcome of this project.

Organizations: Vision committee, TCRA, landowners and R&N Market.

Funding Source(s): Private. TCRA can provide incentives via its property acquisition powers. CDBG grants may apply for infrastructure and/or loans if new jobs will be created.

Expected Results: While there is not enough retail leakage to support another supermarket, the R&N Market should be enlarged so it can support additional local household spending. Making the store larger will increase the sales tax and property tax increment that can be made available for future projects.

Timing: This action should be supported immediately.

Strategy 4b:	**Create a Façade Improvement Loan Program for Cutler-Orosi retailers. Steer other small business loans to the community. Create a single streetscape design theme for all businesses to follow.**
Projects/Actions:	A. Design and create the program to match the needs of local retailers. B. Market the program to existing retailers. C. Create a catalog of other available programs and promote the availability to local businesses. D. Create a common design theme for Orosi's central shopping area. Encourage local businesses to follow the new theme with their future improvements. E. Publicize each success. This will help the business and the community.
Organizations:	TCRA, Visioning Committee, CSET.
Funding Source(s):	TCRA should first create the façade program and provide its initial funding using existing resources, or a CDBG Enterprise Fund grant if the job creation requirements can be met. Accessing other small business assistance programs is a matter of publicizing their availability. The common theme streetscape can be pursued using existing RMA resources.
Expected Results:	Immediate beautification, increased retail patronage and increased local sales tax revenues.
Timing:	These actions can be taken immediately.

Strategy 4c:	**Encourage expansion of local retailers to fill retail leakage needs.**
Projects/Actions:	A Use the lists (from Chapter 3) of specific retail categories where additional store sales are needed. Compare the need against existing stores and their offerings. Create a comprehensive plan for filling specific needs, including identifying specific stores and their locations. Keep the plan flexible, since the private sector will actually be implementing the plan. B. Approach specific retailers in 'downtown' Orosi and 'downtown' Cutler. Encourage them to expand their lines, upgrade existing lines, or add new lines. Arrange training in merchandising from the local SBDC. Promote SBA, façade and other forms of assistance to those businesses. C. Jointly promote specialty retailers focused on products from Mexico (e.g. food, crafts, home furnishings, etc.) D. Find and help companies having trouble staying in business. Form a response team for each company. Depending on the problems, the team can include an expert in marketing, accounting, insurance, purchasing, management, etc. E. Publicize each success.
Organizations:	The Visioning Committee and the Chamber of Commerce should lead this activity, pulling in the SBDC, CSET, SCORE, College of the Sequoias, etc.
Funding Source(s):	Potential business assistance sources include the SBDC; the joint micro-enterprise program of the County, CSET and the SBDC; SBA; perhaps a TCRA façade improvement loan program; perhaps a CDBG Enterprise Fund revolving loan program; and the revolving loan program administered by the Tulare County Certified Development Corporation. Approach banks and to identify their local ending policies.
Expected Results:	Incremental –to-dramatic improvement in retail capture locally. This type of local planning will enhance the ability of local retailers to serve their clientele.
Timing:	These actions can be taken immediately.

Strategy 4d: Organize a local Farmers Market.

Projects/Actions: A. Organize an official community farmers market. Certification should be considered, but need not be pursued immediately. Insurance and other considerations should be addressed.
B. Set minimum standards for products such as local produce only; local produce, art and crafts only; produce and crafts and antiques only; etc. Discourage 'flea market' products such as used clothing and furniture, music, eyeglasses and other retail products freely available at local stores.
C. Secure an attractive temporary location, perhaps the southeast corner of 63 & El Monte, perhaps the Cutler Park across from Cutler School.
C. Schedule a market day (monthly, weekly during harvest season, etc.), publicize it, sign up vendors, and arrange security and gate staff.

Organizations: This should be a locally sponsored event. Chamber, Vision Committee, or the Incorporation committee should take the lead.

Funding Source(s): Vendor sign-up fees should subsidize each event. Initial startup subsidies may be needed.

Expected Results: Immediate beautification, an outlet for local truck farmers and small farmers, increased retail patronage and increased local sales tax revenues.

Timing: This action can be taken immediately. Starting small and building as momentum builds is a good strategy here.

Goal 5: CONTINUE TO PURSUE INCORPORATION

Pursue incorporation. **The community is large enough to incorporate, and incorporation would increase local control over the community's destiny.**

Strategy: Support movement toward cityhood.

Projects/Actions: A. Work with LAFCO, the Board of Supervisors and RMA to complete the incorporation study and release its results. Review it critically.
B. Take additional steps as appropriate that will eventually lead to a successful ballot measure. Seek additional funding for any necessary studies and other actions.
C. Promote the corner of 63 & Ave. 408 as the future civic center of the new city. Work with the owners to acquire and/or dedicate the land for that purpose.
D. Publicize all successful milestones along the way.

Organizations: Incorporation committee.

Funding Source(s): Various

Expected Results: Incorporation takes time, and the local public must believe incorporation and the increased local control and responsibilities are in their best interest.

Timing: Ongoing.

Goal 6: DEVELOP AND FILL THE INDUSTRIAL PARK

Designate the South Cutler Industrial Park as described in the Water and Sewer Report. Complete infrastructure to Phase I. Recruit new industries. **The area already includes a number of industrial users, and is close to the sewer plant for cost-effective delivery of infrastructure. The recommended 160-acre site would provide 20 years of land for new industry.**

Strategy: Designate and Develop the South Cutler Industrial Park.

Projects/Actions:
A. Officially designate the park.
B. Ensure the JWA and the Cutler PUD bring the necessary infrastructure to the site, starting with Phase I.
C. Encourage existing landowners within the Park boundaries to cancel their Williamson Act contracts, thus allowing the land to come on line in 10 years, when it will be needed.
D. Provide the Tulare EDC with data and maps so they can include the park in their marketing inventory.
E. Publicize all milestones as they are reached.

Organizations: RMA, Cutler-Orosi PAC, existing landowners, Tulare EDC. Vision Committee should support this action.

Funding Source(s): TCRA, RMA, the Cutler PUD and Orosi PUD could jointly provide up-front funding for sewer, water, and roads. Seek EDA or CDBG grant assistance. Connection fees, user fees and property taxes will provide payback.

Expected Results: A designated park will provide needed jobs for local residents, reducing the out-migration of workers. Additional tax benefits will also accrue to the future city.

Timing: Official Park designation can occur as soon as the County makes the necessary revisions to the Cutler-Orosi Community Plan. Williamson Act cancellations should occur immediately. Sewer and water are nearby and only need installation of new lines. Roads can be built as needed. Active recruitment by Tulare EDC can continue throughout this process, but recruitment will be easier once the necessary land use designations and infrastructure improvements have been made.

Goal 7: SUPPORT AGRI-BUSINESS DEVELOPMENT

Make Cutler-Orosi into an attractive place for current and future agri-business firms.

Strategy: Enhance employee training and create the facilities to support agri-business as it becomes more technologically dependent.

Projects/Actions:
A. Promote training of local residents at the College of the Sequoias agri-business program, at CSET and at other programs as they become available. Bring COS classes to the community.
B.. Encourage the formation of microenterprises and small businesses in the agri-business industry.
C. Encourage local developers to create facilities attractive to the industry, including industrial, research and office facilities. The future civic center would make an attractive site for laboratories, a trade center and other facilities.
D. Work with Tulare County EDC and the Tulare County BIZ to understand the needs of agri-business firms and recruit them to the community.

Organizations: CSET, College of the Sequoias, Tulare County EDC, and Tulare County BIZ. The Vision committee should help.

Funding Source(s): COS, CSET, local businesses.

Expected Results: The skill level of the local workforce will improve, creating a more attractive environment for technology-oriented agri-business firms.

Timing: Ongoing.

CUTLER PUBLIC UTILITY DISTRICT
BOARD OF DIRECTORS MEETING
40526 OROSI DRIVE
CUTLER, CA 93615

AGENDA FOR MARCH 19, 2019 – 6:00 P.M. AT THE DISTRICT OFFICE

1. CALL TO ORDER

2. ROLL CALL

President:	Fernie Rubalcaba
Vice-President:	Leonard R. Encinas
Treasurer:	Javier E. Hernandez
Director:	Jose F. Guerrero
Director:	Armando Porras

3. PUBLIC COMMENTS:
   A. The public may comment on any Agenda item;
   B. In addition, the public will be allowed to comment on each item presented; and
   C. The Board will not able to take action on any item not appearing on the Agenda.

4. CONSIDERATION OF ADDITIONAL ITEMS TO BE ADDED TO THE AGENDA:

5. MINUTES OF PREVIOUS MEETING:
   A. Action required: The Board will approve, amend or not approve the minutes of the previous meeting.

6. REVIEW OF DELINQUENT ACCOUNTS:
   A. The staff will present delinquent accounts and any requests for payment extension.

   B. Action required: The Board will, or will not, approve payment extension requests and authorize Staff to terminate service on delinquent accounts.

7. SUPERINTENDENT UPDATE:
   A. Operations staff will update the Board in regard to:
      1. The operational and water quality issues related to the wells and distribution system;
      2. Yettem/Seville Lift Station;
      3. DWR Meter Grant update; and
      4. Water leaks or other emergency call out.

   B. Action required: The Board will, or will not, instruct Staff accordingly.

8. 2017-2018 AUDIT:

A. Staff will review with the Board the 2017-2018 audit report.

B. Action required: The Board will or will not accept the audit report.

9. COMMUNITY PLAN: Information only

A. County Staff will be in attendance to make a presentation related to the update of the Cutler-Orosi Community Plan.

10. JOINT POWERS V WAWONA:

A. Staff will present to the Board a Resolution to conditionally authorize the President of the Board to execute deeds and any other required documents to complete the terms of the settlement agreement between the Cutler-Orosi Joint Powers Wastewater Authority and the Wawona Packing Company.

B. Action required: The Board will or will not consider adoption of the Resolution.

11. SRF #003: WATER PIPELINE:

A. Staff will update the Board with respect to status of construction activities.

B. Action required: None Information only.

12. SRF #001 BLENDING TANK PROJECT:

A. Staff will update the Board with respect to the status of construction activities. Status of permit activities will be reviewed. A pipeline contractor progress payment request and related reimbursement claim request will be presented for approval.

B. Action required: The Board will consider authorization of the execution and submittal of a reimbursement claim. The Board will consider approval of the submitted Contractor Progress Payment request.

13. DRINKING WATER FILL STATION:

A. The Kings River Water Quality Coalition has made a request for capacity service or a drinking water fill station. Information will be provided explaining the request, the reason for the request and the payment expectations of the Coalition relative to the service request.

B. Action required: The Board will determine to deny the request, approve the request or direct additional evaluation prior to final action.

14. CUTLER-OROSI JOINT POWERS AUTHORITY: Information only.

A. Board will be updated with regard to JPWA meeting discussion and actions.

15. CUTLER OROSI SURFACE WATER PLANT AUTHORITY:

A. As a part of the administration and management of the Cutler-Orosi Surface Water Plant Authority, a checking account is being established Directors representing the C.P.U.D. will complete bank required paperwork and sign the required documents.

B. Action required: None. Action pursuant to prior motion.

16. APPROVAL OF OVERTIME:

A. Staff will present to the Board Employees' overtime Dionicio Rodriguez Jr., Martha Lowrey, and Yolanda Perez 2 hours each for regular meeting on 2/19/2019, Martha Lowrey 2 hours special meeting on 2/28, Santiago Venegas 2 hours for 2/17 called out broken water service on School Avenue and 2 hours for 3/6 called out broken water service on Amethyst Avenue.

B. Action required: The Board will or will not approve overtime.

17. PROCESS BILLS FOR PAYMENT:

A. Action required: The Board will or will not approve bills for payment.

18. MEETING ADJOURNMENT:

A. Action required: The Board will adjourn the meeting.

I, Martha Lowrey, certify that on March 15, 2019 I posted the Agenda for the Cutler Public Utility District, meeting of March 19, 2019 at the Cutler Public Utility District office, 40526 Orosi Drive, Cutler, California. I declare under penalty of perjury the foregoing is true and correct. The disclosable public records related to agendas are available for public inspection at Cutler Public Utility District office, 40526 Orosi Drive, Cutler, CA 93615 for all meetings.

Martha Lowrey

Martha Lowrey
Ex- Officio Clerk of the Board of Directors

A person with a qualifying disability under the Americans With disabilities Act of 1990 may request the District to provide a disability-related modification or accommodation in order to participate in any public meeting of the District. Such assistance includes appropriate alternative formats for the agenda and agenda packets used for any public meetings of the District. Requests for such assistance and for agendas and agenda packets shall be made in person, by telephone, facsimile, or written correspondence to the District office, at least 48 hours before a public District meeting.

Cutler Public Utility District Board Meeting

March 19, 2019

Who Attended:

- Board members
- Dennis Keller (son)
- Aaron Bock
- Dave Bryant
- Susan Simon
- Total of 10 Attendees

Materials:

- Community Presentation
- Handouts (copy of presentation)

Public Comments:

1. Who should the public contact at the County now that Steve Worthley has retired?

Answer:

- Eddie Valero, Supervisor, District 4
- Reed Schenke, RMA Director
- Michael Walsh, RMA Associate Director
- Aaron Bock, RMA Assistant Director
- Johnny Wong, RMA, Road Maintenance

2. Intersections of Avenue 408 and Avenue 416 at Road 120, regarding brining this area into the Urban Development Boundary (UDB)?

Answer:
Dave Bryant reviewed this area and created a revised UDB map including this area in the expansion of the UDB

3. What road improvements were completed over the last three years?

Answer:
Project: Ella Avenue from SR 63 to Ralph Road (0.09 miles), Blade Patch, completed on 12/10/2014

County Transportation Improvement Project (CTIP)

2017 CTIP:

- Avenue 416 Crosswalk and ADA Ramps budgeted at $200,000 (Construction summer 2019)

2018 CTIP:

- Orosi- ADA Improvement & Blade Patching budgeted at $500,000 (Construction in Summer 2019)
- Cutler- ADA Improvement & Blade Patching budgeted at $300,000 (Construction in Summer 2019)
- Cutler- Road 144 and Avenue 384 to Avenue 416 budgeted at $1,600,000 (Construction in Progress)

2019 CTIP (Proposed projects):

- Orosi- ADA Improvement & Blade Patching budgeted at $200,000 Construction in Summer 2020
- Cutler- ADA Improvement & Blade Patching budgeted at $200,000 Construction in Progress

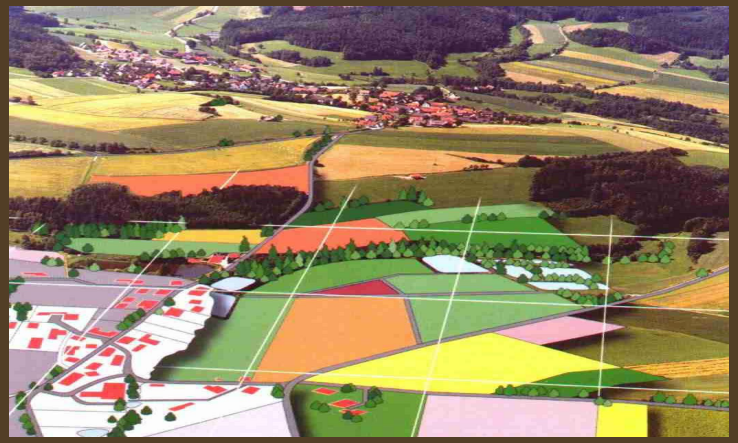
Summer 2020

- George Road and 2nd Drive in Cutler. Pedestrian improvements (sidewalk and drainage improvements to be specific) with an estimated cost of $2,500,000.

Cutler-Orosi Community Plan

2019 Update

Goals and Policies



Decision Making

Plan Framework

California State Law

Planning Principles

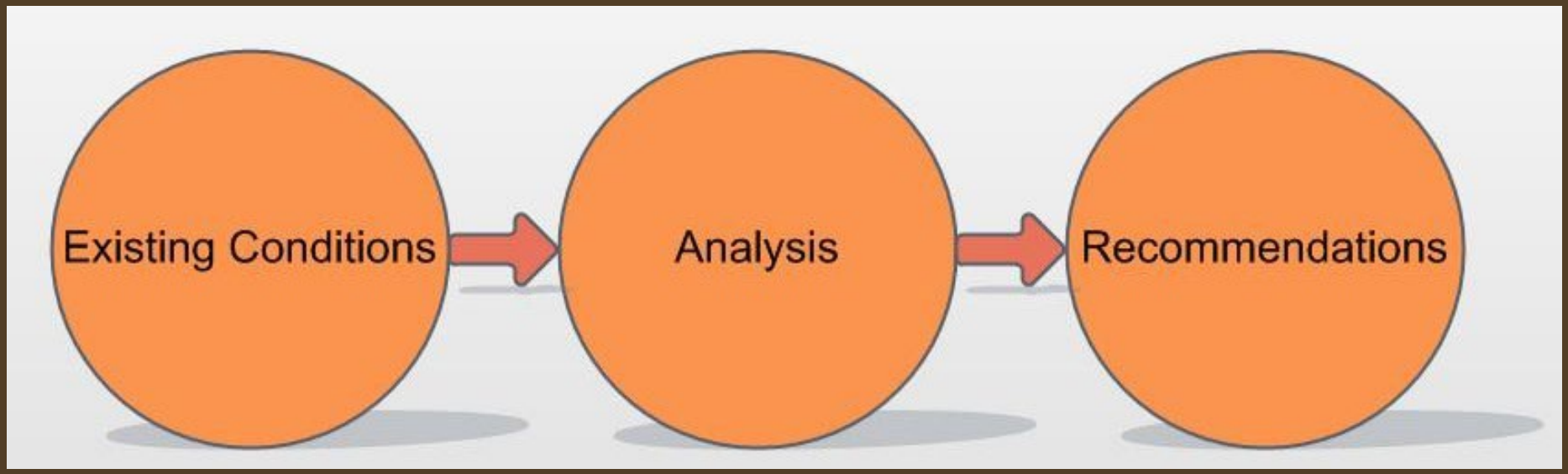
Tulare County General Plan Policies

Schedule

Cutler-Orosi Community Plan - Update and Environmental Impact Report (EIR)

	Tasks	Schedule and Milestones										
1	**Preliminary Work**											
1.1.	General Plan Initiation BOS September 30, 2014, Resolution No. 2014-0688											
1.2.	CEQA Consultant RFPs											
2	**Constraints Assessment**											
3	**Needs Assessment**											
4	**Prepare Policy Plan (0-5yrs, 5-10yrs, 10 to 17 yrs to GP)**											
5	**Environmental Impact Report (EIR)**											
6	**Planning Commission Hearing**											
7	**Board of Supervisors Hearing**											
8	**Project Completion**											

Community Plan Process



Community Conditions

Staff / Consultant Research

Citizen Participation

Sharing Concerns

Addressing Concerns

Analysis and Recommendations

One Major Purpose

1. Guide Future Decision Making

Two Primary Documents

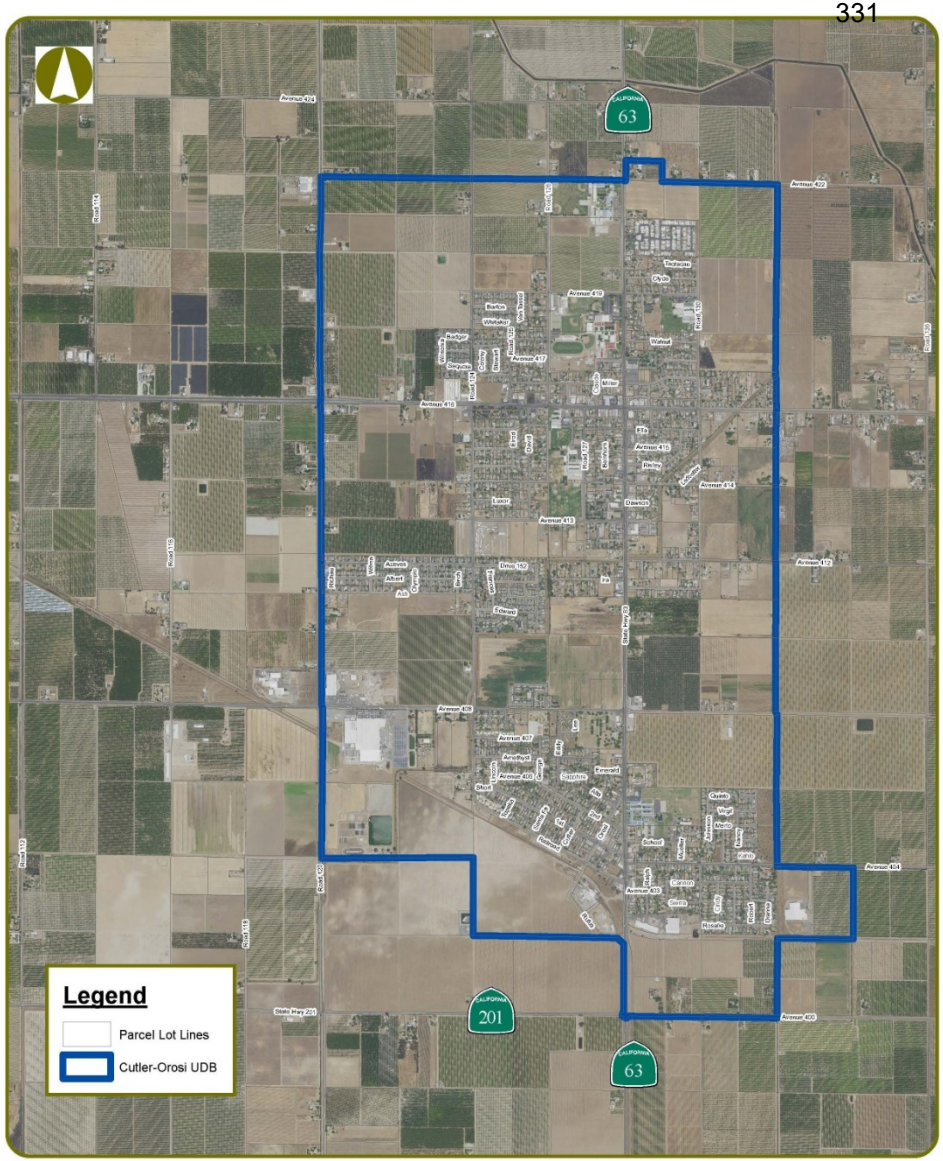
1. Cutler-Orosi Community Plan
2. Environmental Impact Report (EIR)

Three Primary Components

1. Urban Development Boundary

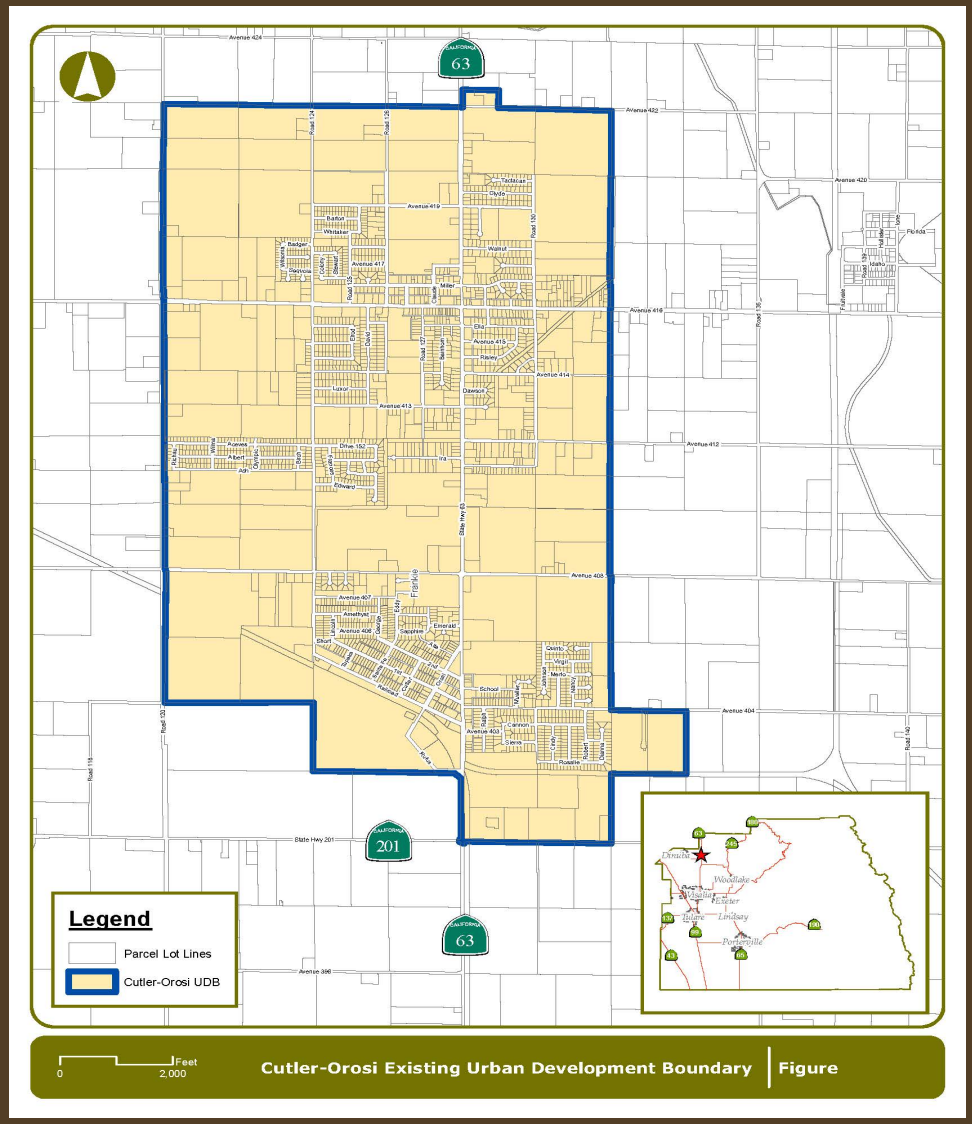
2. Land Use and Circulation

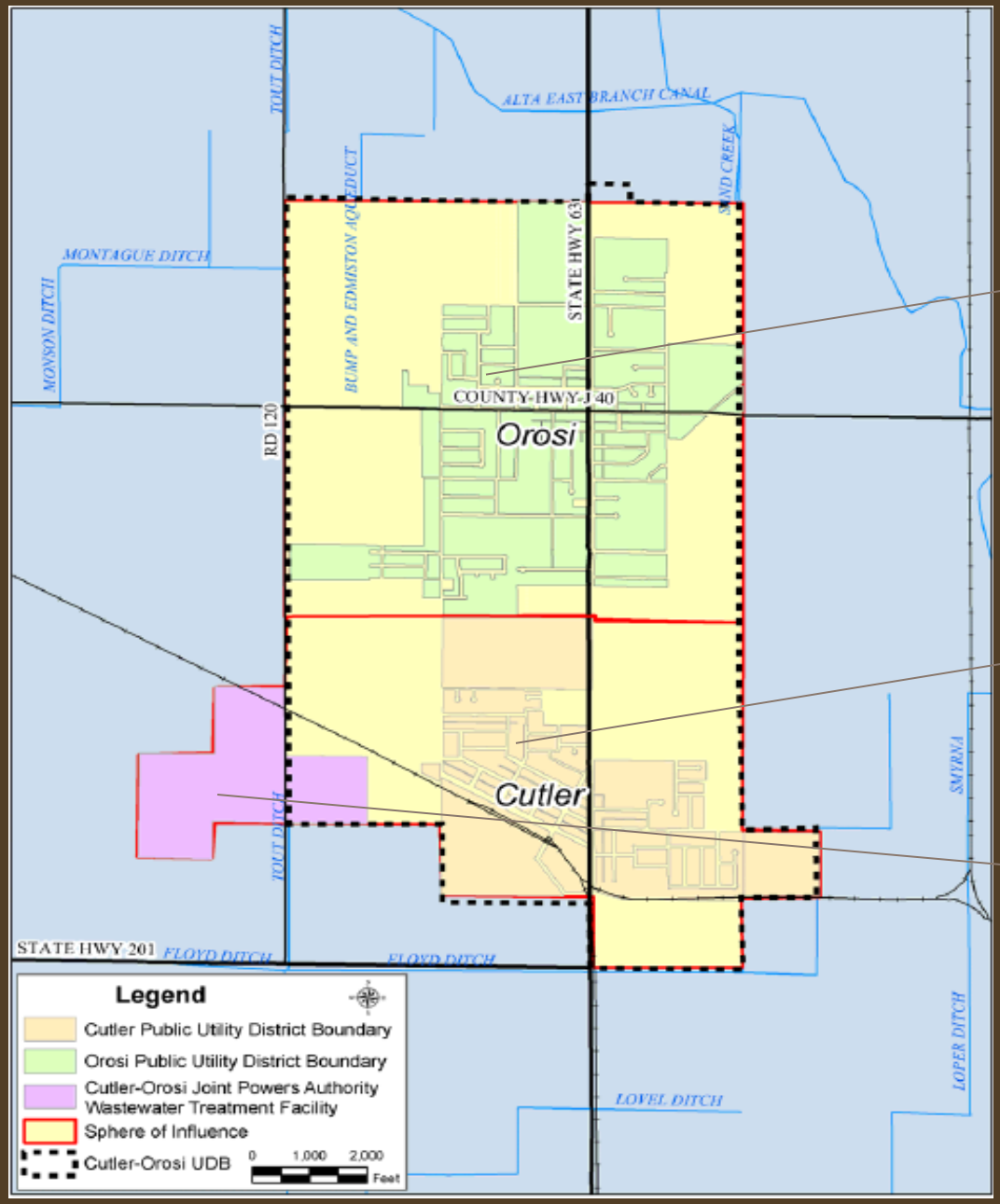
3. Policies to guide future decision making



Cutler-Orosi Aerial Map | Figure

Cutler/Orosi Existing Urban Development Boundary



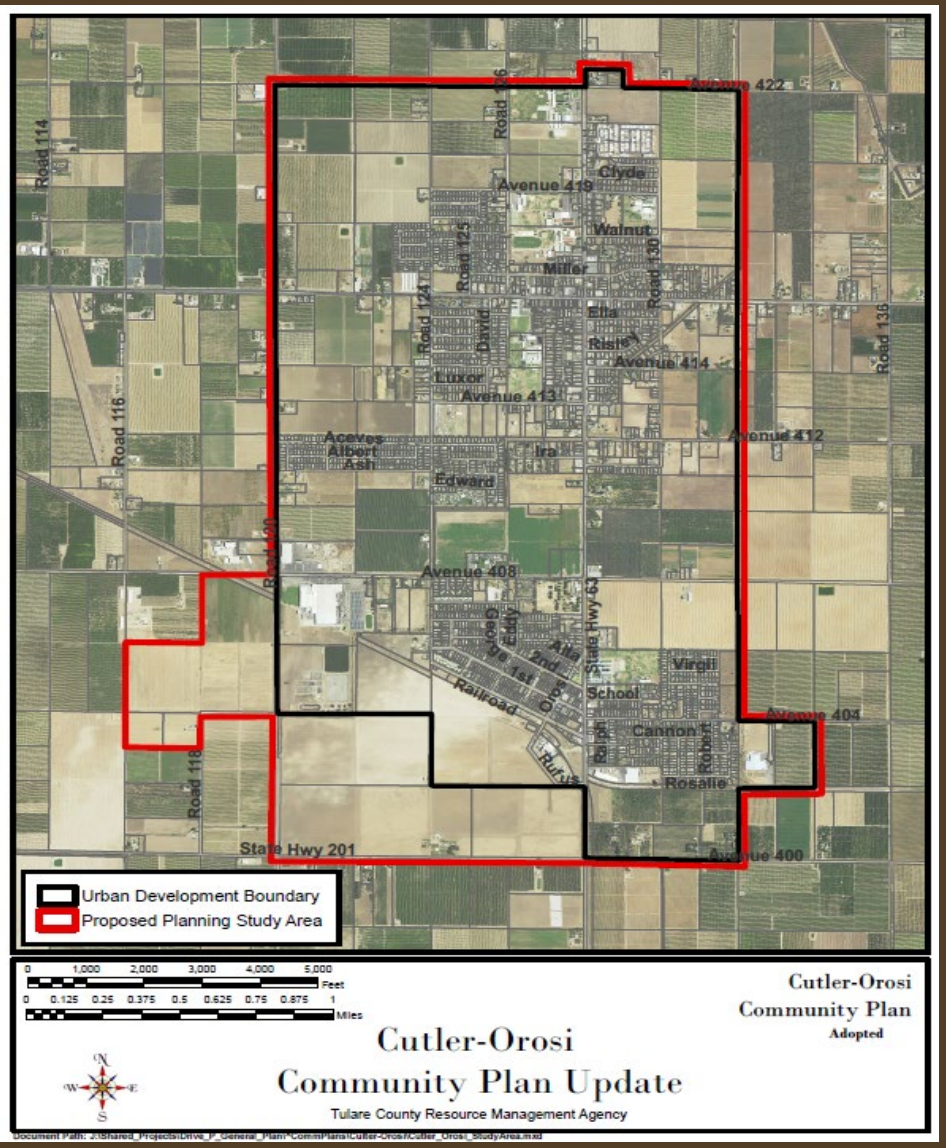


Orosi Public Utility District

Cutler Public Utility District

Cutler/Orosi Wastewater Treatment Facility

Cutler/Orosi Community Plan Update Study Area

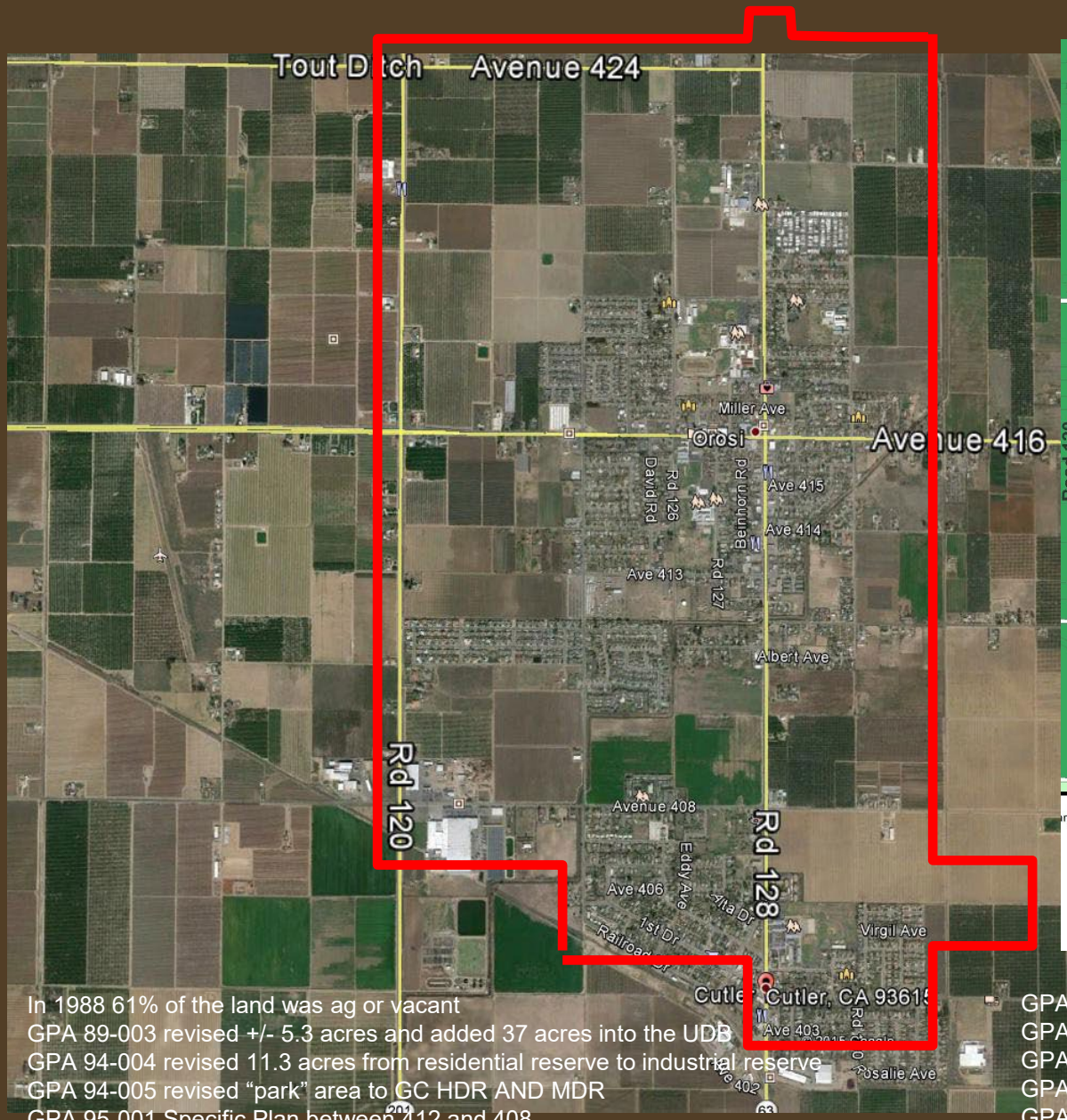


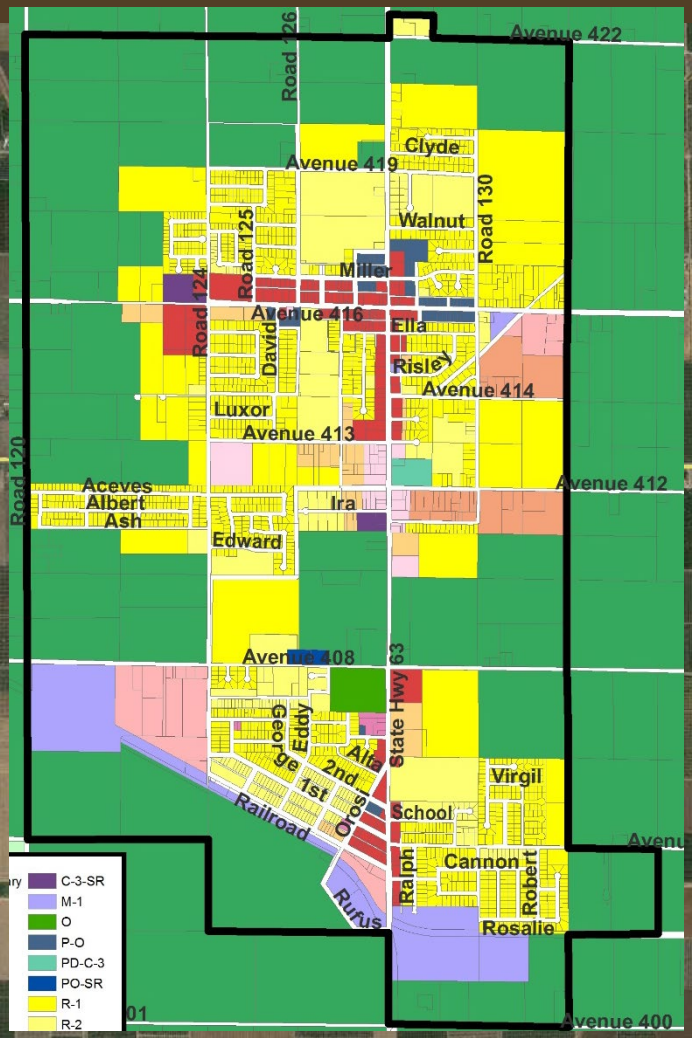
Cutler/Orosi Urban Development Boundary

Existing UDB area = 2,200 acres

UDB area with Study Area Additions = 2,600 acres

UDB increase in area = 400 acres



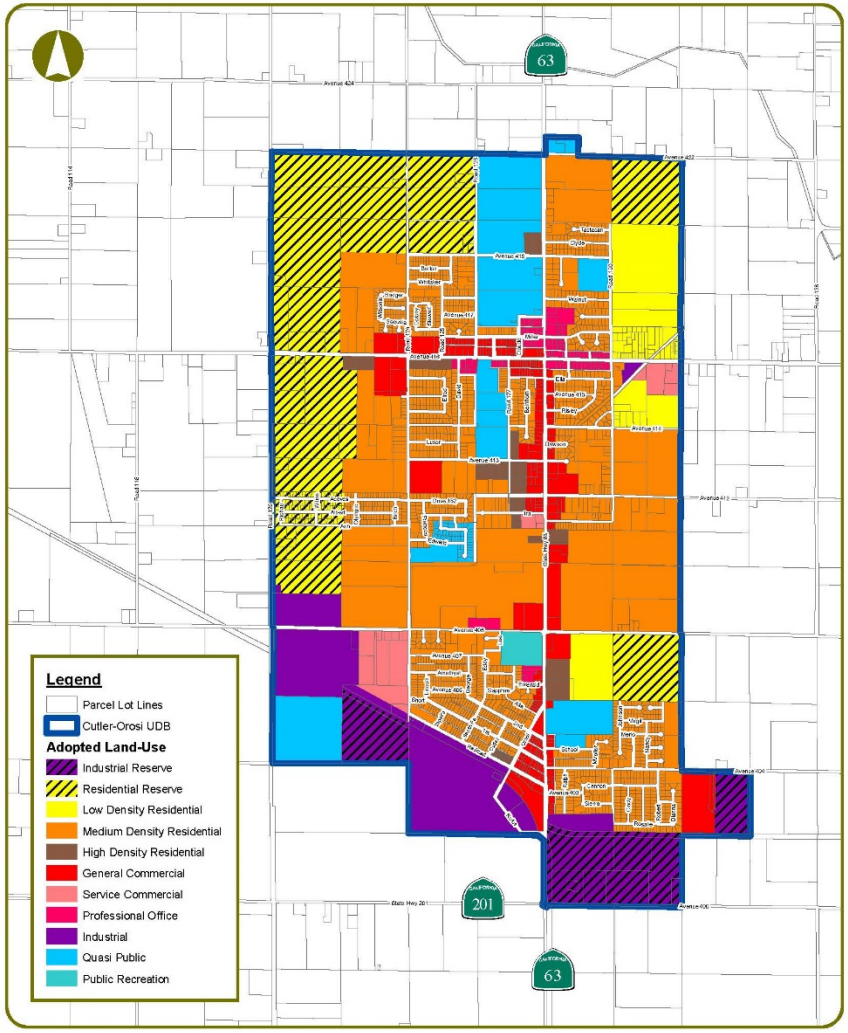


In 1988 61% of the land was ag or vacant
GPA 89-003 revised +/- 5.3 acres and added 37 acres into the UDB
GPA 94-004 revised 11.3 acres from residential reserve to industrial reserve
GPA 94-005 revised “park” area to GC HDR AND MDR
GPA 95-001 Specific Plan between 412 and 408

GPA 95-005 revised 9.1 acres MDR to GC
GPA 95-006 revised .61 acres
GPA 98-04 added 5 acres
GPA 00-001 revised 3 acres MDR to Professional Offices
GPA 00-005 revised 2.75 acres

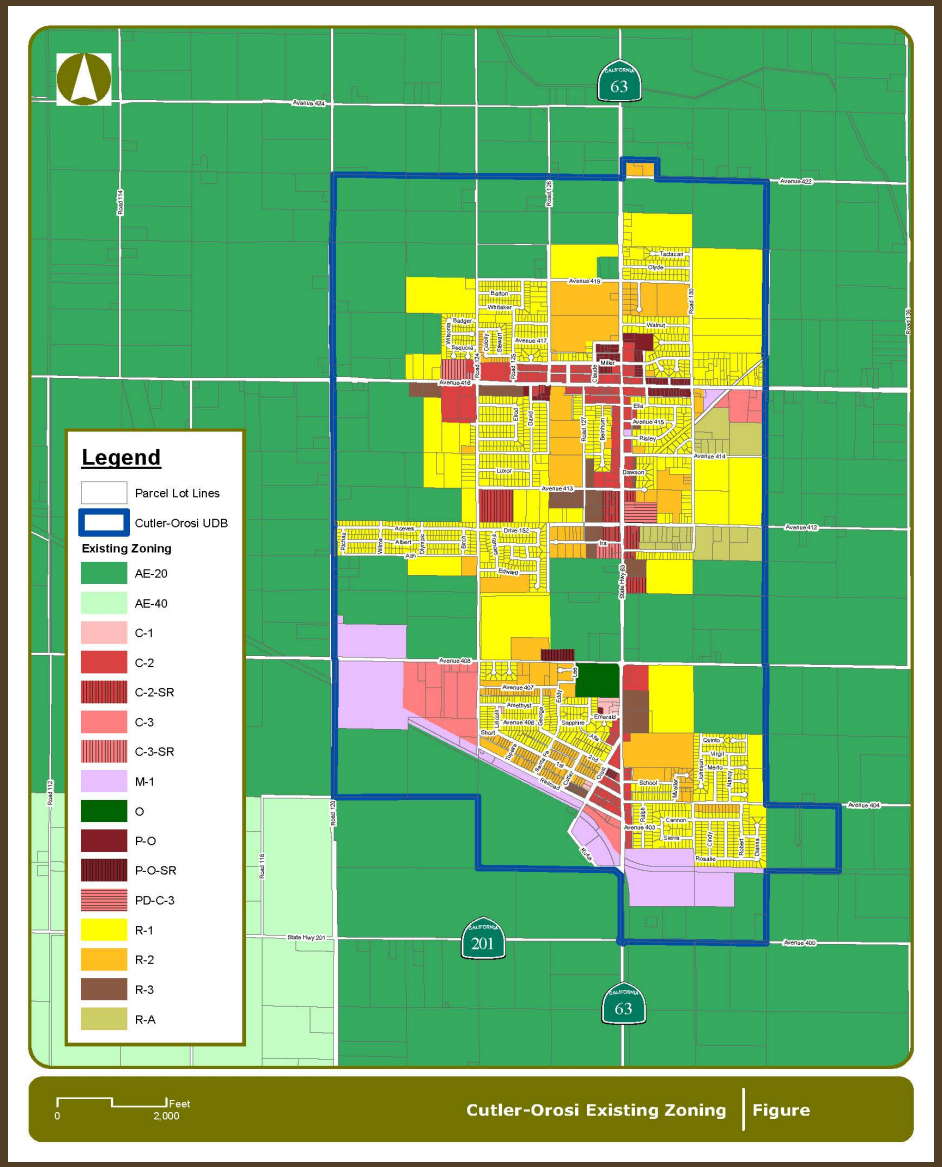
Google earth

Cutler/Orosi Adopted Land Use



Cutler-Orosi Adopted Land-Use | Figure

Cutler/Orosi Existing Zoning Districts



Community Infrastructure Improvements

$5 Million In Improvements to the Wastewater Treatment Facility

Orosi - Upgrades for new water and sewer lines

Orosi - Two New Water Wells

Orosi - New Flowmeters

Cutler - Upgrades for new water and sewer lines

Cutler - One new water well and one new test well

Cutler - New Water Blending Tank

Pending Grants to replace remaining water and sewer lines

Measure R Funds

- 2006 Voters approved Measure R ½ cent sales tax for transportation
- Since 2006 appx. $1.1 million – Cutler and Orosi
- Surface maintenance such as surface seal, oil seal, chip seal, road resurface.

Measure R Funds- Cutler

YEAR 2010 MEASURE R RESURFACING							
	ROUTE			LIMITS			LENGTH
R130		LA402	ROSALIE AVE.	-	A404		0.21
						TOTAL MILES	0.21
YEAR 2012 MEASURE R RESURFACING							
	ROUTE			LIMITS			LENGTH
LA403B	CANNON AVE.	R130		-	LR131C	ROBERT RD.	0.18
LA406C	EMERALD AVE.	R127		-	R128	SH 63	0.11
LA407B	HAZEL AVE.	R126*	END	-	LR126D	LEE RD.	0.05
LR124F	LINCOLN RD.	A407*		-	A408		0.05
LR124G	TOPEKA RD.	A407*		-	A408		0.05
LR127B	PEARL RD.	A406*	END	-	LA406C	EMERALD AVE.	0.03
						TOTAL MILES	0.47
YEAR 2013 MEASURE R RESURFACING							
	ROUTE			LIMITS			LENGTH
D125B	SANTA FE DR.	D128B	RAILROAD DR.	-	D129B	FIRST DR.	0.08
LA406A	AMETHYST AVE.	R124		-	R126	GEORGE RD.	0.22
						TOTAL MILES	0.30

2010-2013

Since 2006:
$242,000

Measure R Funds- Orosi

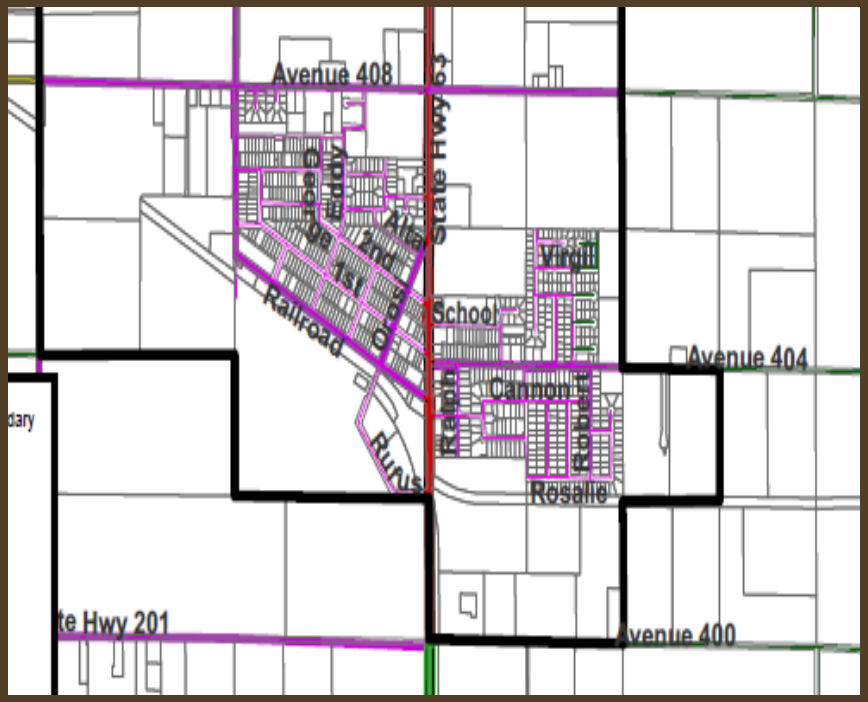
Since 2006- $900,000

YEAR 2011 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
D129D	LEDBETTER DR.	A414		-	R130		0.09
LA411B	IRA AVE.	R126*	END	-	R128	SH 63	0.23
LA416B	MILLER AVE.	R125	ELROD RD.	-	R126		0.13
LR125F	MILLER RD.	LA416B	MILLER AVE.	-	LA416*	END N/O MILLER	0.02
						MEASURE R MILES	0.47
						TOTAL MILES	0.72
YEAR 2012 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
A419		R124		-	R126		0.25
R125		A416	EL MONTE WAY	-	LA418C	WHITAKER AVE.	0.29
R126		A416	EL MONTE WAY	-	A419		0.39
						TOTAL MILES	0.93
YEAR 2013 MEASURE R RESURFACING							
	ROUTE		LIMITS				LENGTH
A414		R127		-	LR127C	BEINHORN RD.	0.07
A415		R128	SH 63	-	LR129A	MUELLER RD.	0.19
LR128B	RALPH RD.	LA415C	ELLA AVE.	-	A416	EL MONTE WAY	0.08
						TOTAL MILES	0.34

2011-2013

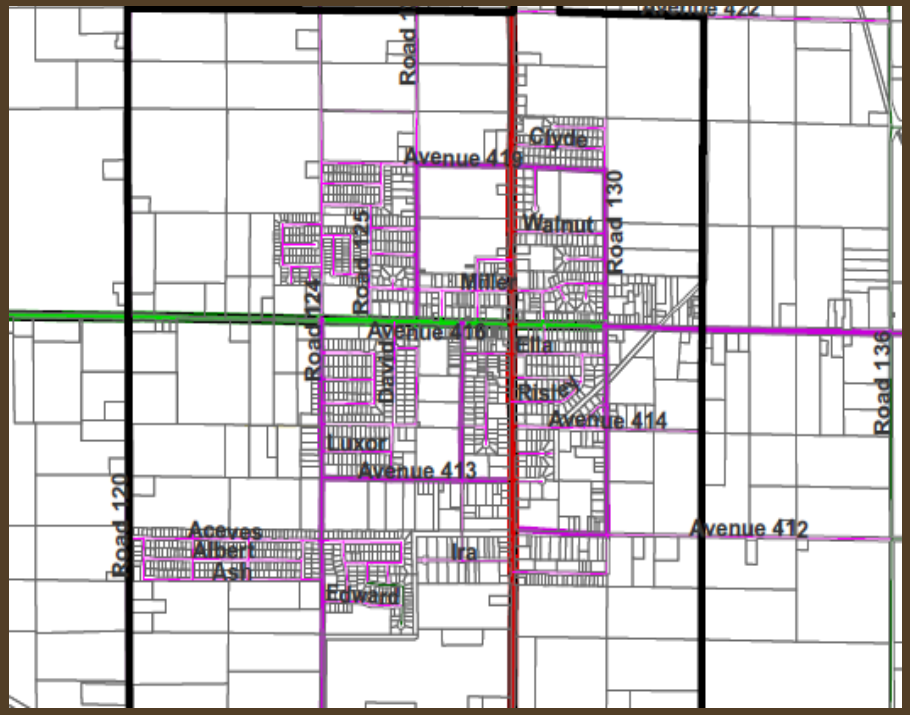
Complete Streets Cutler

- Avenue 404 – SR 63 to Robert Rd
  - Sidewalk, C&G, Drainage
- Avenue 408 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Railroad Drive – SR 63 to Rd 124
  - Sidewalk, C&G, Drainage
- First Drive – SR 63 to Rd 124
  - Striping, Bike Lane



Complete Streets Orosi

- Avenue 416 – SR 63 to Rd 140 (East Orosi)
  - Widening, Sidewalk, C&G, Drainage, Bridges over Sand Creek and Canal, Bike Lane, Transit
  - Multi Use Trail
- Avenue 413 – Rd 124 to SR 63
  - Sidewalk, C&G, Drainage
- Avenue 416 – SR-63 to Dinuba
  - Class II bike lane
- Avenue 419



Proposed Complete Streets Projects

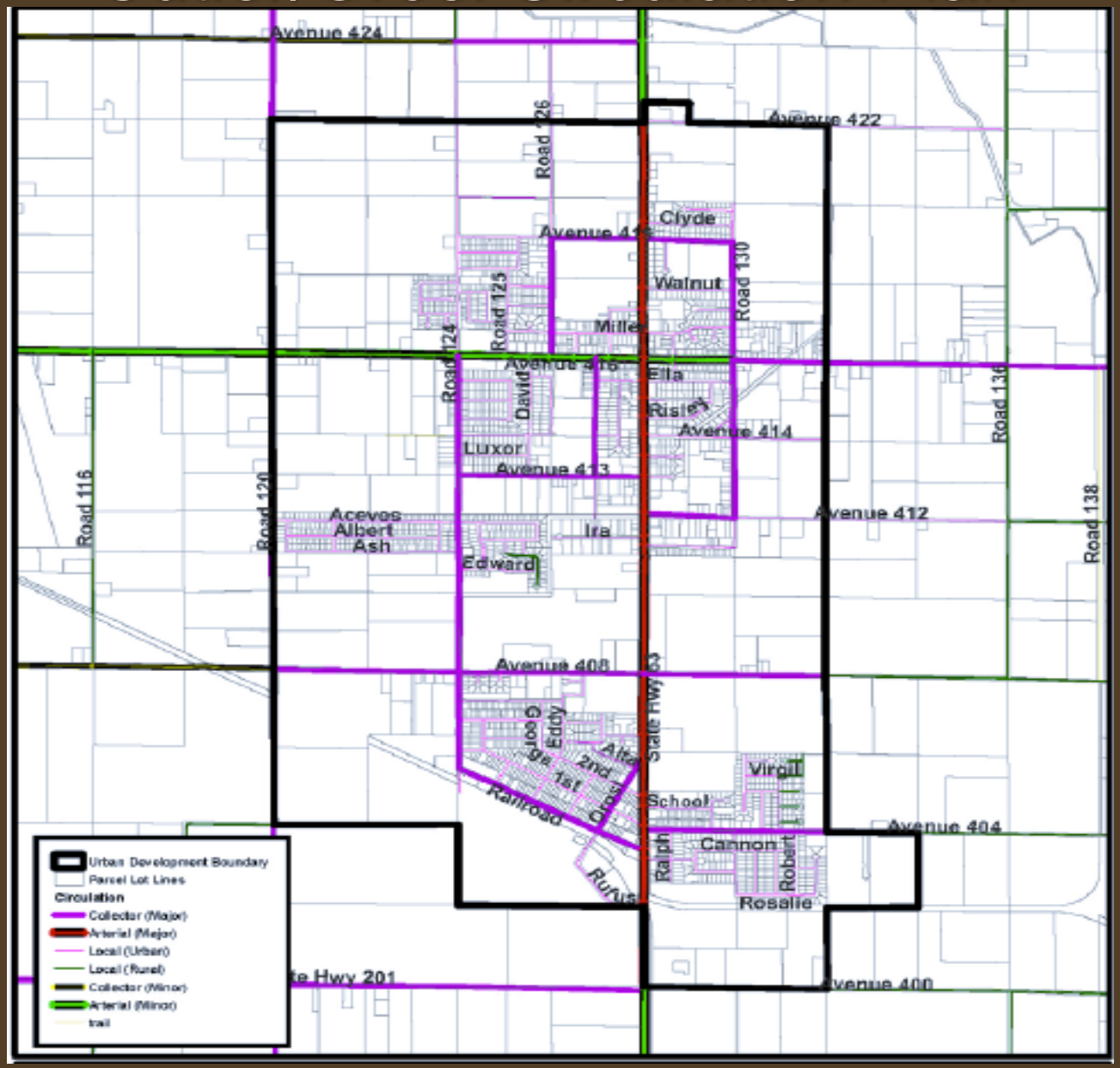
Orosi

1. Avenue 416 – SR 63 to Road 140 (East Orosi)
2. Avenue 413 – Road 124 to SR 63
3. Avenue 419
4. Avenue 416 – SR-63 to Dinuba
5. Road 130 (Strong interest from the school district)
6. Road 124

Cutler

1. George Road/2nd Drive – Avenue 407 to SR 63
2. Avenue 408 – Road 124 to SR 63
3. Railroad Drive – SR 63 to Road 124
4. Avenue 404 – SR 63 to Robert Rd
5. First Drive – SR 63 to Road 124

Cutler/Orosi Circulation Plan



Tulare County General Plan Update Guiding Policies

2. Planning Framework

PF-1	To provide a planning framework that promotes the viability of communities, hamlets, and cities while protecting the agricultural, open space, scenic, cultural, historic, and natural resource heritage of the County *[New Goal].*
PF-2	To provide a realistic planning area around each unincorporated community that clearly delineates the boundaries of each community and provides a framework for economic development, the provision of public services, and an outstanding quality of life *[Urban Boundaries Element; Goal 3, Goal 1UB.C][Urban Boundaries Element Amendment (88-01); 1988, Modified)].*

PF-2.4 **Community Plans** The County shall ensure that community plans are prepared, updated, and maintained for each of the communities.

PF-2.7 **Improvement Standards in Communities** The County shall require development within the designated UDBs to meet an urban standards for curbs, gutters, sidewalks, and community sewer and water systems.

4. Land Use

LU-1	To encourage the overall economic and social growth of the County while maintaining its quality of life standards and highly efficient land use *[New Goal].*
LU-7	To preserve and enhance the character and scale of Tulare County's communities, hamlets, and rural areas, including their design heritage and historic character *[New Goal].*

LU-1.1 **Smart Growth and Healthy Communities.** The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including: walkable neighborhoods, creating a strong sense of place, directing growth toward existing communities, preserving open space, and creating a range of housing opportunities and choices.

LU-4.1 **Neighborhood Commercial Uses.** The County shall encourage the development of small neighborhood convenience and grocery facilities to meet the everyday shopping and personal needs of immediately surrounding residential land uses in communities.

LU-6.3 **Schools in Neighborhoods.** The County shall encourage school districts to locate new schools in areas that allow students to safely walk or bike from their homes.

5. Economic Development

ED-1	To maintain a healthy and diverse local economy that meets the present and future employment, shopping, recreational, and service needs of Tulare County residents *[New Goal].*
ED-2	To promote business growth and industry diversification and maintain a favorable business climate and a supportive economic foundation *[New Goal].*
ED-3	To diversify the economic base of Tulare County through the expansion of non-agricultural industry clusters and through the development and expansion of recreation and visitor-serving attractions and accommodations *[New Goal].*

ED-1.6 **Develop Public/Private Partnerships.** The County shall pursue partnerships to encourage the development of public facilities and infrastructure improvements that benefit communities.

HS-1	To protect County residents and visitors from injury and damage resulting from natural catastrophes, man-made events, and hazardous conditions *[New Goal based on Five County Seismic Safety Element (1974); Goals 1, 2, 3, 6, 9, 12 & 13. Pg. 11].*

Health and Safety for all Neighborhoods

HS-9	To support healthy lifestyles among residents of Tulare County through the built environment and land use decisions that play an important role in shaping the pattern of community development, in either promoting or discouraging good health for its citizens *[New Goal]*.

PFS-1	To establish and maintain acceptable levels of service, minimize costs, and provide criteria for determining the location, capacity, and timing of existing and future public facilities and services *[New Goal].*

Adequate Quantity and Quality of Public Services

Achieving General Plan Goals

- Increasing uses by right
- Creating flexibility for uses through mixed use overlays

DEMOGRAPHIC AND ECONOMIC INFORMATION

- Population
- In 2010, Cutler's population was 5,000 and Orosi's population was 8,770.
- The population for Cutler increased to 5,850 and Orosi's decreased to 7,760 by 2017 (Total Population 13,610).
- Median Age
- The median age in Cutler stayed about the same 24.6 in 2010 to 24.5 in 2017. Orosi increased from 26.4 in 2010 to 28.8 in 2017.

- <u>Economic Conditions (2017)</u>
- Cutler had an unemployment rate of 13.1% and Orosi's unemployment rate was 20%, compared to 10.0% for Tulare County, and 7.7% for California
- Cutler's median household income was $30,760 and Orosi's median household income was $43,379, compared to $47,280 for Tulare County and $76,975 for State of California
- 39.7% of families in Cutler live below the poverty line and 24.3% of families in Orosi live below the poverty line compared to 23% for Tulare county and 11.1% for California.

- <u>Housing Units (2010-2017)</u>
- During the decade between 2010 and 2017, the number of housing units in Cutler increased from 1,136 to 1,293 Orosi increased from 2,070 to 2,076
- 2017 American Community Survey indicated that 69.6% of the housing units in Cutler were 1 unit detached and 80.0% in Orosi were 1 unit detached.
- 2017 American Community Survey indicated that 65.1% of both Cutler and Orosi housing units were owner occupied.
- In 2017, the vacancy rate in Cutler was 3.2% and 3.7% in Orosi.
- In 2015, approximately 15% of the housing units in Cutler and 87% in Orosi were sound.

FUTURE CONSIDERATIONS

- DEMOGRAPHIC AND ECONOMIC INFORMATION WILL ASSIST IN DETERMININING THE AMOUNT OF RESIDENTIAL, COMMERCIAL AND INDUSTRIAL OF FUTURE LAND USES

NEXT STEPS

- Future Community Plan Workshops.
  - April 11, 2019
  - May 9, 2019

Thank You for Attending

Aaron R Bock, MCRP, JD, LEED AP
Assistant RMA Director
Economic Development & Planning
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7050

David Bryant, MPA
Chief Planner, Special Projects
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7130

Susan Simon
Planner III
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7126

CUTLER PUBLIC UTILITY DISTRICT
BOARD OF DIRECTORS MEETING
40526 OROSI DRIVE
CUTLER, CA 93615

AGENDA FOR APRIL 16, 2019 – 6:00 P.M. AT THE DISTRICT OFFICE

1. CALL TO ORDER

2. ROLL CALL

President:	Fernie Rubalcaba
Vice-President:	Leonard R. Encinas
Treasurer:	Javier E. Hernandez
Director:	Jose F. Guerrero
Director:	Armando Porras

3. PUBLIC COMMENTS:
   A. The public may comment on any Agenda item;
   B. In addition, the public will be allowed to comment on each item presented; and
   C. The Board will not able to take action on any item not appearing on the Agenda.

4. CONSIDERATION OF ADDITIONAL ITEMS TO BE ADDED TO THE AGENDA:

5. MINUTES OF PREVIOUS MEETING:
   A. Action required: The Board will approve, amend or not approve the minutes of the previous meeting.

6. REVIEW OF DELINQUENT ACCOUNTS:
   A. The staff will present delinquent accounts and any requests for payment extension.

   B. Action required: The Board will, or will not, approve payment extension requests and authorize Staff to terminate service on delinquent accounts.

7. FERNANDO MENDOZA:
   A. Customer Fernando Mendoza will present to the Board a request to have the $9.00 Certified Letter charge credited to his account.

   B. Action Required: The Board will or will not approve request.

8. COMMUNITY PLAN: Information only
   C. A. County Staff will be in attendance to make a presentation related to the update of the Cutler-Orosi Community Plan.

9. SUPERINTENDENT UPDATE:

A. Operations staff will update the Board in regard to:

1. The operational and water quality issues related to the wells and distribution system;
2. Yettem/Seville Lift Station;
3. DWR Meter Grant update; and
4. Water leaks or other emergency call out.

B. Action required: The Board will, or will not, instruct Staff accordingly.

10. SRF #003: WATER PIPELINE:

A. Staff will update the Board with respect to status of construction activities. The Board will review and consider electrical changes to address a new well on the site.

B. Action required: The Board will consider approval of a contract change order.

11. SRF #001 BLENDING TANK PROJECT:

A. Staff will update the Board with respect to the status of construction activities. Status of permit activities will be reviewed. A pipeline contractor progress payment request and related reimbursement claim request will be presented for approval. Multiple contract change order requests will be reviewed. Discussion will occur with respect to permit and schedule issues.

B. Action required: The Board will consider authorization of the execution and submittal of a reimbursement claim. The Board will consider approval of the submitted Contractor Progress Payment request. The Board will consider separate actions related to contract change order requests.

12. DRINKING WATER FILL STATION:

A. The Kings River Water Quality Coalition has made a request for capacity service for a drinking water fill station. Information will be provided explaining the request, the reason for the request and the payment expectations of the Coalition relative to the service request.

B. Action required: The Board will determine to deny the request, approve the request or direct additional evaluation prior to final action.

13. CUTLER-OROSI JOINT POWERS AUTHORITY: Information only.

A. Board will be updated with regard to JPWA meeting discussion and actions.

14. APPROVAL OF OVERTIME:

A. Staff will present to the Board Employees' overtime Dionicio Rodriguez Jr., Martha Lowrey, and Yolanda Perez 2 hours each for regular meeting on 3/19/2019.

B. Action required: The Board will or will not approve overtime.

15. PROCESS BILLS FOR PAYMENT:

A. Action required: The Board will or will not approve bills for payment.

16. MEETING ADJOURNMENT:

A. Action required: The Board will adjourn the meeting.

I, Martha Lowrey, certify that on April 12, 2019 I posted the Agenda for the Cutler Public Utility District, meeting of April 16, 2019 at the Cutler Public Utility District office, 40526 Orosi Drive, Cutler, California. I declare under penalty of perjury the foregoing is true and correct. The disclosable public records related to agendas are available for public inspection at Cutler Public Utility District office, 40526 Orosi Drive, Cutler, CA 93615 for all meetings.

Martha Lowrey

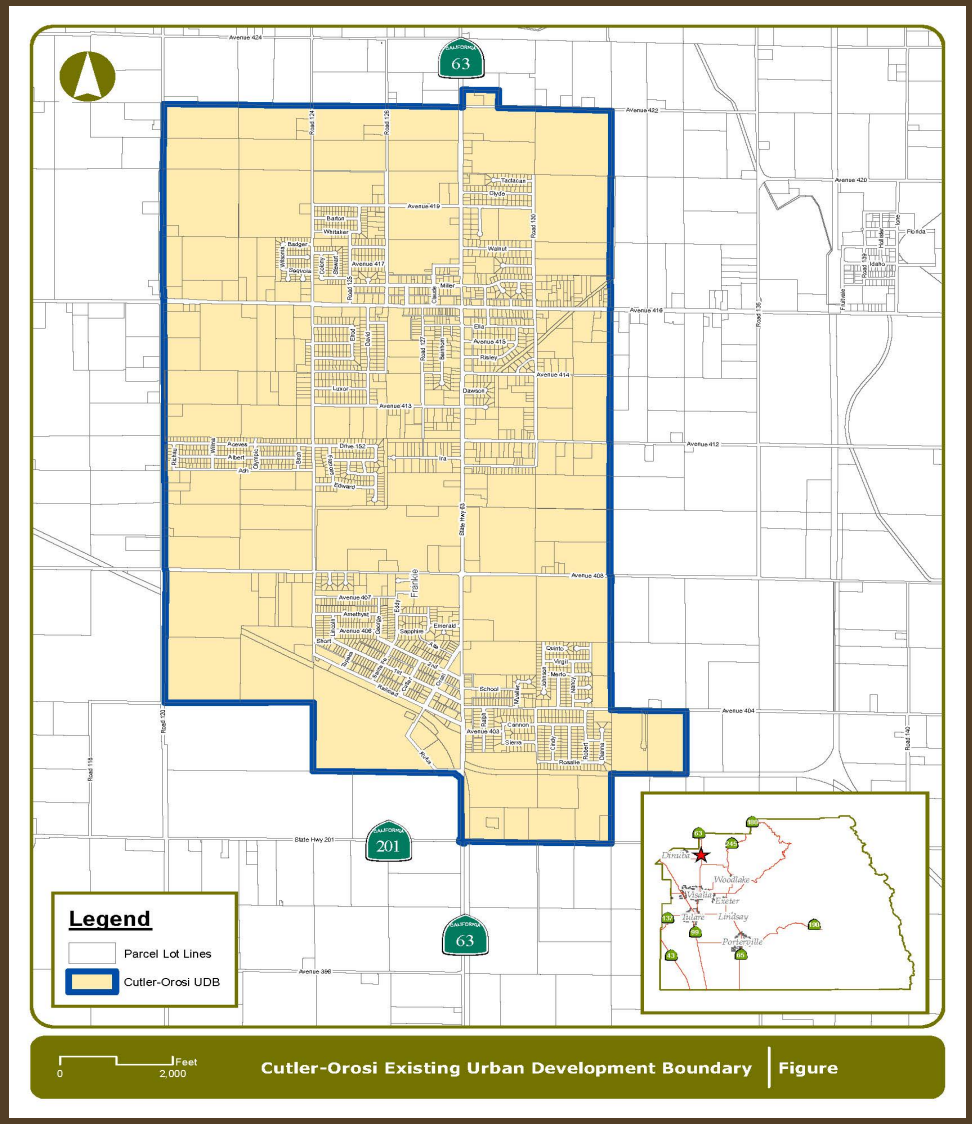
Martha Lowrey
Ex- Officio Clerk of the Board of Directors

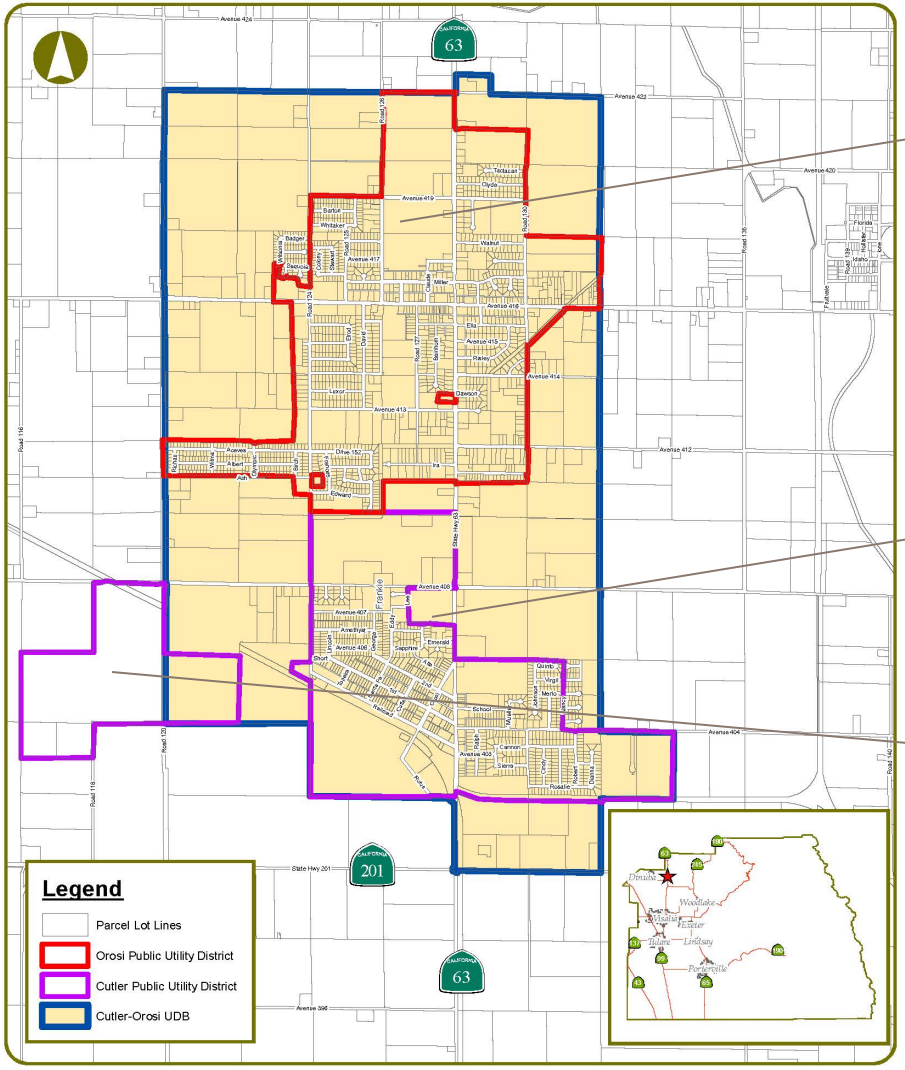
A person with a qualifying disability under the Americans With disabilities Act of 1990 may request the District to provide a disability-related modification or accommodation in order to participate in any public meeting of the District. Such assistance includes appropriate alternative formats for the agenda and agenda packets used for any public meetings of the District. Requests for such assistance and for agendas and agenda packets shall be made in person, by telephone, facsimile, or written correspondence to the District office, at least 48 hours before a public District meeting.

Cutler-Orosi Community Plan

2019 Update

Cutler/Orosi Existing Urban Development Boundary



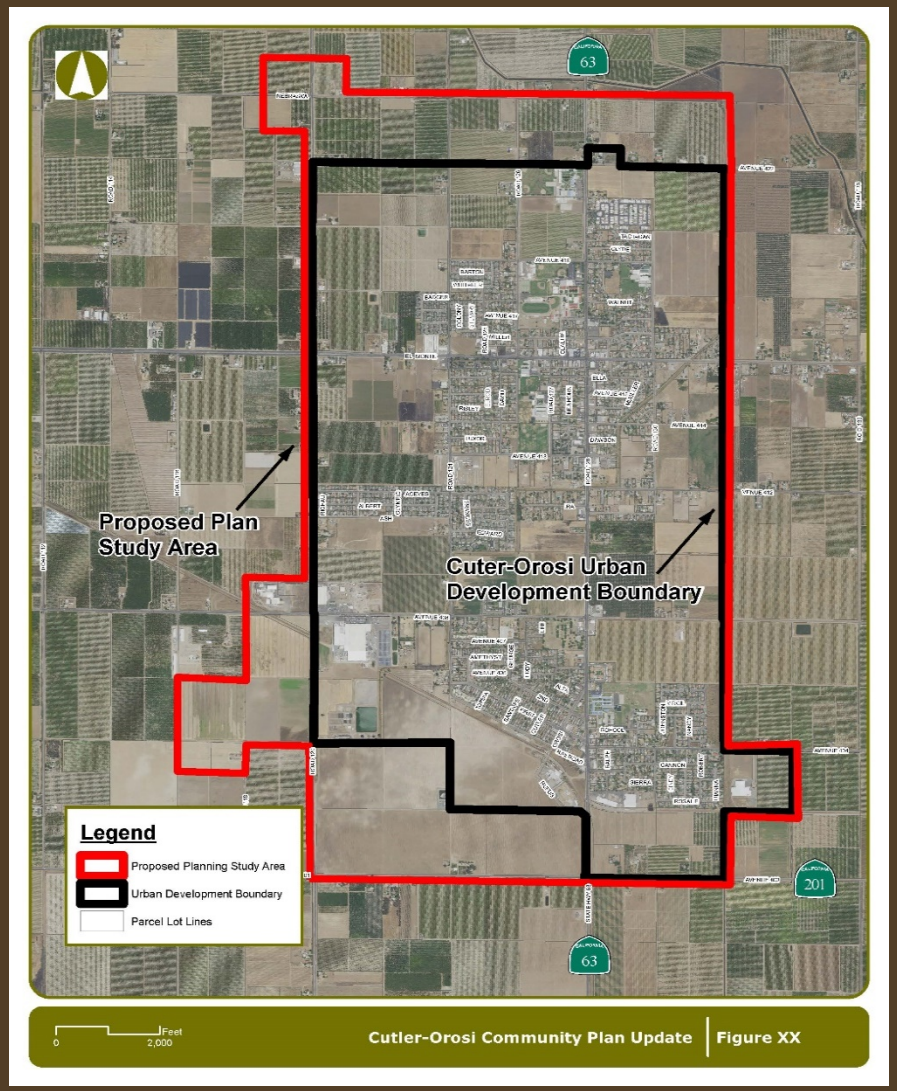


Orosi Public Utility District

Cutler Public Utility District

Cutler/Orosi Wastewater Treatment Facility

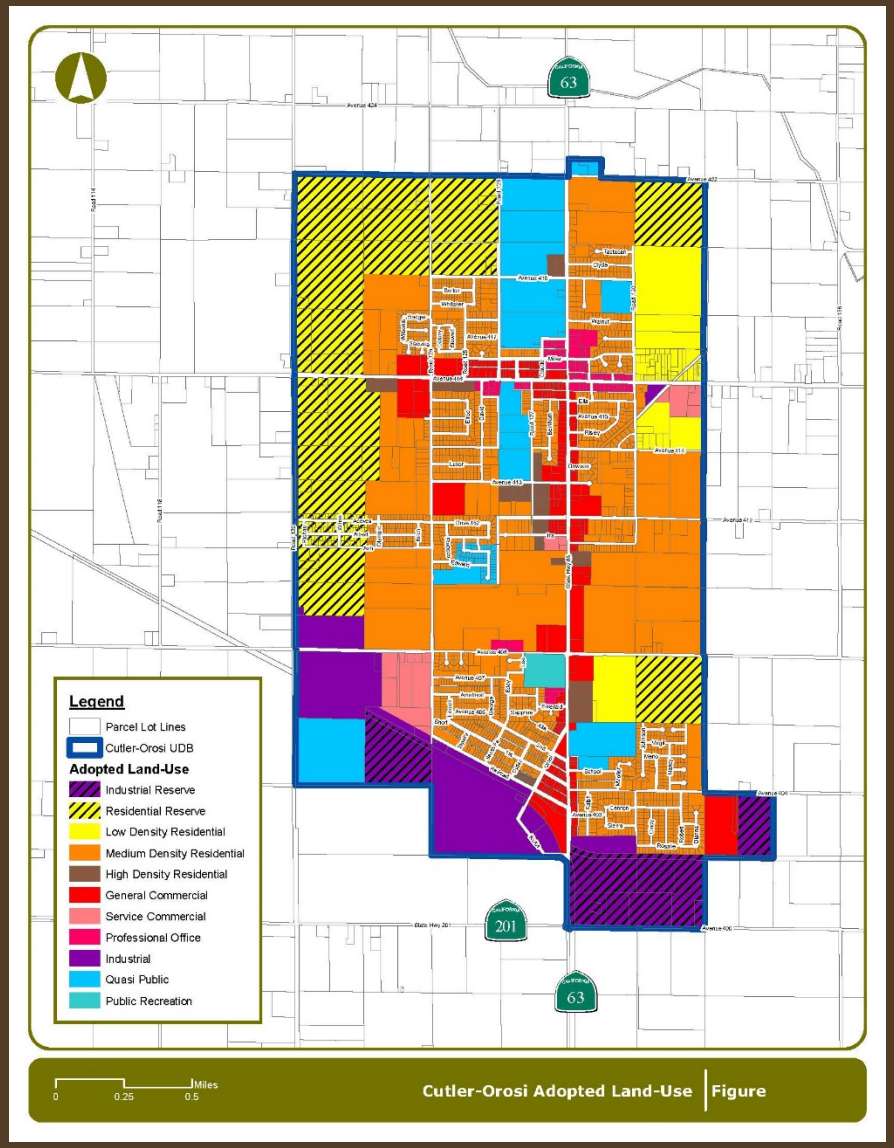
Cutler/Orosi Community Plan Update Study Area



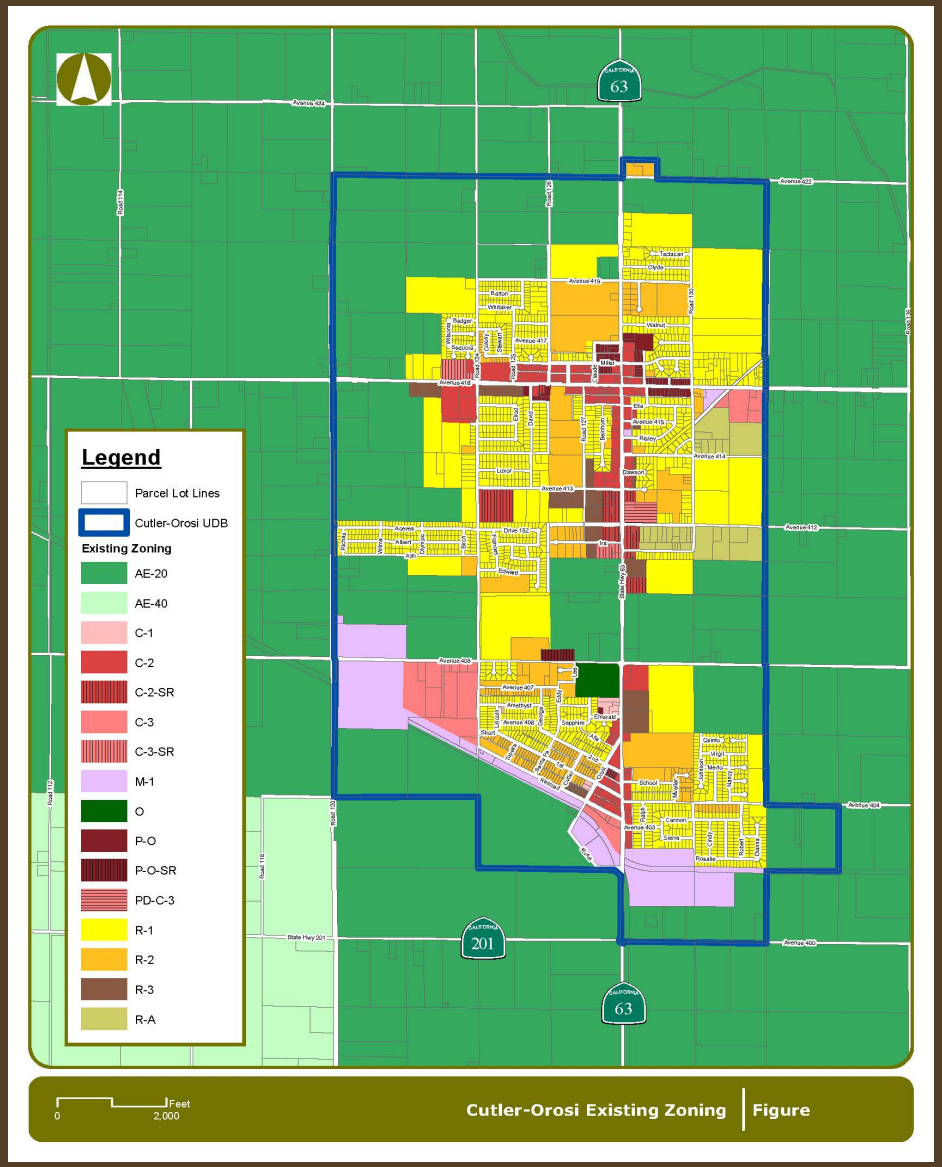
Cutler/Orosi Urban Development Boundary

Existing UDB area = 2,200 acres

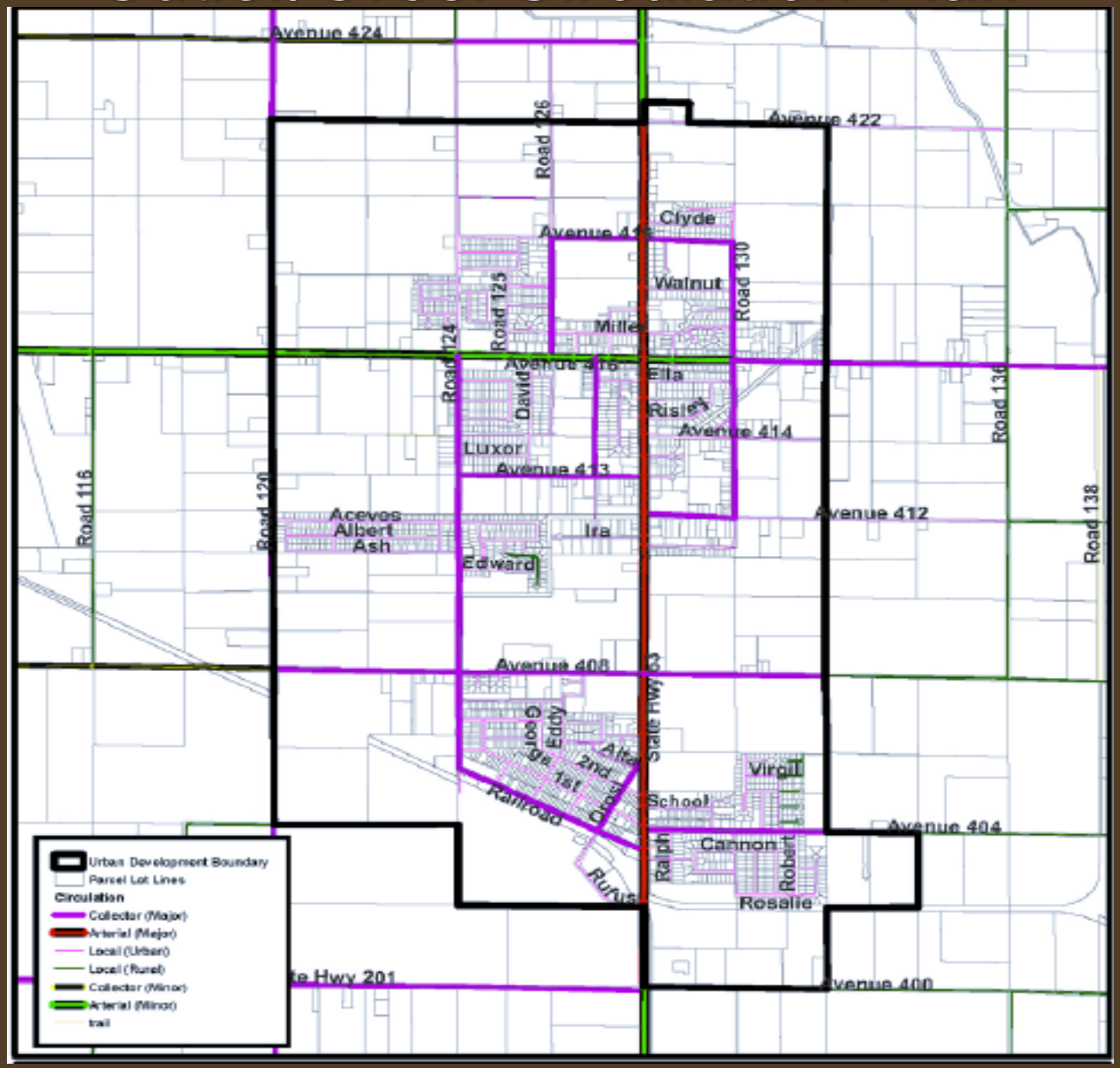
Cutler/Orosi Adopted Land Use



Cutler/Orosi Existing Zoning Districts



Cutler/Orosi Circulation Plan



County Transportation Improvement Project (CTIP)

2017 CTIP:

- Avenue 416 Crosswalk and ADA Ramps budgeted at $200,000 (Construction summer 2019) .

2018 CTIP:

- Orosi- ADA Improvement & Blade Patching budgeted at $500,000 (Construction in Summer 2019)
- Cutler- ADA Improvement & Blade Patching budgeted at $300,000 (Construction in Summer 2019)
- Cutler- Road 144 and Avenue 384 to Avenue 416 budgeted at $1,600,000 (Construction in Progress)

2019 CTIP (Proposed projects):

- Orosi- ADA Improvement & Blade Patching budgeted at $200,000 Construction in Summer 2020
- Cutler- ADA Improvement & Blade Patching budgeted at $200,000 Construction in Progress

Summer 2020

- George Road and 2nd Drive in Cutler. Pedestrian improvements (sidewalk and drainage improvements to be specific) with an estimated cost of $2,500,000.

Thank You for Attending

Aaron R Bock, MCRP, JD, LEED AP
Assistant RMA Director
Economic Development & Planning
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7050

David Bryant, MPA
Chief Planner, Special Projects
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7130

Susan Simon
Planner III
5961 South Mooney Blvd.
Visalia, CA 93277
(559) 624-7126

Cutler Public Utility District Board Meeting

April 16, 2019

Who Attended:

- Fernie Rubalcabe
- Leonard R. Encinas
- Javier E. Hernandez
- Jose F. Guerrero
- Dennis Keller
- Aaron Bock
- Susan Simon
- Total of 10 Attendees

Materials:

- Community Presentation
- Handouts (Copy of presentation)
- Sign-In sheet

Public Comments:

County Transportation Improvement Project (CTIP) - Improvement and maintenance strategy of the County's transportation infrastructure for Fiscal year's: 2017, 2018/2019, and proposed for 2019/2020

"Mandatory Consolidation" was Noticed by the State Water Resources Control Board for the East Orosi Community Services District (CSD) water system to connect to the Orosi Public Utilities District (PUD) water system. East Orosi CSD has consistently failed to provide safe drinking water due to high nitrate levels. State lacks authority to require East Orosi to connect to Cutler/Orosi wastewater treatment facility for sewer service.

Cutler Public Utility District
Cutler/Orosi Community Plan
April 16, 2019

Name	Address & Email Address	Phone No.
Fernie Rubalcaba	12650 2nd Dr. Cutler Fernierubalcaba@gmail.com	
Jose F. Guerrero	12540 1st. Dr. Cutler CA. 93615 jose-n-silvia@sbcglobal.net	559-385-0928
Javier E Hernandez	40238 Nancy Rd Cutler Ca. 93615	
[illegible]	505 N. West Vis.	
Dennis Keller	P.O. Box 911 Visalia, CA 93279	559/732-7988

2019 LCAP Community Input Forums

HEY!

WE NEED YOUR INPUT

¡Necesitamos sus sugerencias!

Your input will be used to help prepare the 2020-2021 LCAP and the Tulare County Community Plan for Cutler Orosi

Sus sugerencias se usarán para preparar el LCAP del 2020-2021 y el Plan Para la Comunidad de Cutler-Orosi de 2019

School/ Escuela:	Date/ Fecha:	Time/ Hora:
Cutler Elementary School	August 20th/ 20 de agosto	6:30 p.m.
Orosi High School	August 21st / 21 de agosto	6:30 p.m.
Golden Valley Elementary School	August 22nd / 22 de agosto	6:30 p.m.
Palm Elementary School	August 27th / 27 de agosto	6:30 p.m.
Lovell High School	August 28th /28 de agosto	6:30 p.m.
El Monte Middle School	September 3rd/ 3 de septiembre	6:30 p.m.
Family Education Center	September 4th/ 4 de septiembre	6:00 p.m.
Family Education Center	September 10th/10 de septiembre	9:00 a.m.

OUR MISSION

"Educating Minds, Inspiring Futures"

OUR VISION FOR ALL STUDENTS

All students will be college, career, community ready and prepared to compete in a global economy.

NUESTRA MISIÓN

"Educando Mentes, Inspirando Futuros"

NUESTRA VISIÓN PARA CADA ESTUDIANTE

Todo alumno estará preparado para la universidad, carrera, comunidad y para competir en una economía global.

Cutler/Orosi Community Plan Update

Cutler Elementary School

August 20, 2019

Aaron Bock, RMA
Hector Ramos, RMA
Susan Simon, RMA
Jose Saenz, RMA

Yolanda Valdez, Cutler/Orosi School District

Approximately 50 Cutler/Orosi community members took part.

Presentation by Yolanda Valdez, Superintendent Cutler/Orosi School District, informing the community Tulare County Resource Management Agency (RMA) staff was there for the communities to address concerns and answer questions.

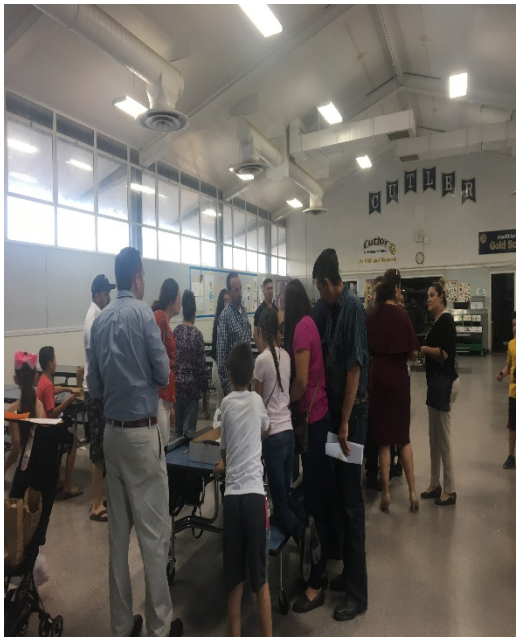
Introduction of Tulare County Resource Management Agency (RMA) staff by Jose Saenz (Spanish).

Communities concerns:
SR63/1st Drive No lights at crosswalk, no flashing lights, High traffic area, requesting reflector lights.
SR63/School Avenue Fix crosswalk
SR 63/Avenue 404 Fix crosswalk
SR 63/1st Drive Alley behind Fiesta Market floods and has potholes
SR 63/2nd Drive
SR 63/Orosi Drive
Cutler Elementary Drainage problem
Emerald Avenue/Road 127 flood, needs sidewalks, need lights, big dogs are out
Orosi Drive PG&E connected electronic for crosswalk end of park.
SR 63/Avenue 408 (Ledbetter Park) No benches to sit on, Playground , need bathroom, no lights, no water fountains, no swings
SR 63/Ella Avenue very dark, heavy traffic area, crosswalk needed.
Walnut Avenue cars not stopping for students at crosswalk
SR 63/Avenue 419 Traffic concerns
Avenue 416/Road 123
Avenue 406/School Drive Flooding
SR 63 Repaint all cross walks
Avenue 419/Walnut Avenue (north of sports complex) want merged arrow repainted, it's faded
El Monte School traffic is backed up in the morning and in the afternoon.
SR 63/Orosi status of ADA ramps
SR/63/2nd status of ADA ramps
Avenue 404/Muller Road dark area, need streetlights and crosswalk lights
SR 63/Avenue 419 traffic is backed up and you cannot get onto SR 63 during school hours.
Road 127 ends just after Golden Valley School.

CUTLER
Gold School

Distrito Escolar Unificado de
Cutler-Orosi

CUTLER


CUTLER

Cutler/Orosi Community Plan Update
Cutler Elementary School (Back to School)
August 20, 2019

Name	Address & Email Address	Phone No.
Camille Montanez	14010 Ave 400 Cutler camille2584@yahoo	407 · 989 · 8478
Trinidad Valencia	41020 RD 124 Trinidad.Valencia51@gmail.com	(559) 393-7324
Juan Lopez	13115 Antonia Ave Cutler	528-0188
Gonzalo Galvan	12937 404 Av. Cutler	559-800-5133
Jacqueline Martinez	39437 Road 128 jacquelinemartinez@yahoo.com	(714) 818-0178
Raul Sanchez	12554 AVE 408 Orosi	559-217-5223
Leanne Cerda	12483 Risley Ave Orosi	559 528-3512
Salvador Rubio	12671 Amethyst Ave Cutler CA	559-799-57-49
RAUL ACEVEDO	12730 Second DR. Cutler. mda245@yahoo.com	(559) 303-3053
Delia Martinez	12936 walnut Ave Orosi, CA	559-590-9782

Cutler/Orosi Community Plan Update

Orosi High School

August 21, 2019

Dave Bryant, RMA
Hector Ramos, RMA
Susan Simon, RMA
Roxana Bran, RMA

Yolanda Valdez, Cutler/Orosi School District

Approximately 50 Cutler/Orosi .community members took part

Presentation by Yolanda Valdez, Superintendent Cutler/Orosi School District, informing the community Tulare County Resource Management Agency (RMA) staff was there for the communities to address concerns and answer questions.

Introduction of Tulare County Resource Management Agency (RMA) staff by Roxana Bran (Spanish).

Community Concerns:
Street light is out on Edward (12463 Edward Ave)

Easement is fenced off south of Avenue 412 (41235 Road 128)

Ira and Road 127 foot bridge ready to fall

Existing crosswalk not seen at night (fog) Walnut/SR 63 would like flashing lights

Palm School and Road 130 on the east side turn out to drop off students.

Road 130/Avenue 419 need cross walk and flashing lights

Palm School along Road 130 (no sidewalks and the road needs to be repaired) from Avenue 416 and Avenue 419 needs to be repaired.

Ella/SR 63 need Flashing Lights and streetlights (area is dark).

Avenue 404/SR 63 need streetlights and flashing lights.

Golden Valley School need crossing signal by Claude/Avenue 416 also need cross walk.

Claude Road in need of repair, sidewalks, and it floods near sports park.

Walnut/Avenue 416 need flashing light.

Avenue 419/SR 63 near Orosi High need flashing lights.

Orosi/SR63 need signal

First/SR 63 walking light

Railroad/SR 63 need cross walk and walking lights.

Ira/SR63 easement blocked

Avenue 413/SR 63 easement issue

Want a Starbucks

Traffic signals and flashing pedestrian warning lights on Highway 63 and Avenue 404 and Orosi Drive in Cutler.

Flooding on Eddie and Avenue 407 and also on Amethyst in Cutler.

Improvements to The Pedestrian Footbridge across Sand Creek near the intersection of Ira and Road 127 in Orosi.

Road 136 improvements between Avenue 416 and Avenue 412 in Orosi.

Pocket Parks recommended on Albert Avenue East of Highway 63, Miller Avenue East of Highway 63, and on Road 124 East of the Sequoia Ave cul-de-sac alignment.

Road improvements on Road 136 between Avenue 412 and Avenue 416.

Recommended shade structures utilizing trees or awnings on buildings in the downtown area of Orosi between the Orosi High School and Avenue 416.

Rails to Trails along Railroad Avenue between Highway 63 and Road 120 in Cutler with connectivity North on Road 122 Avenue 416 in Orosi.

Bike and pedestrian path along Sand Creek in Orosi.

Traffic signal and pedestrian flashing lights crosswalk at the intersection of Avenue 416 and Claude in Orosi.

Pedestrian safety concerns at School Ave and Highway 63 in Cutler.

General repairs to Ledbetter Park in Cutler including concerns regarding broken drinking fountains.

Street lights needed on Ira Avenue West of Highway 63 in Orosi.

Flooding concerns on Road 130 between Avenue 416 and Clyde Avenue in Orosi.

Road repairs @Ella Avenue and Highway 63.

Traffic control devices recommended on Road 124 and Road 125 in Orosi to address speeding and safety concerns.

Cutler/Orosi Community Plan Update

Golden Valley Elementary School

August 21, 2019

Jessica Willis, RMA
Susan Simon, RMA
Jose Saenz, RMA
Kevin Sullivan, RMA

Yolanda Valdez, Cutler/Orosi School District

Approximately 40 community members took part

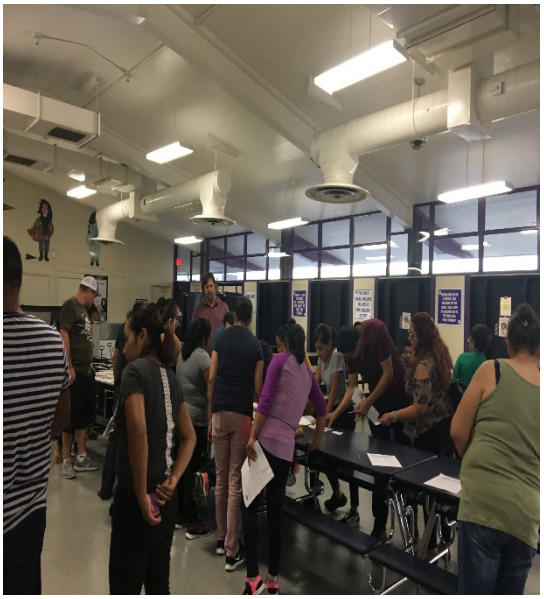
Presentation by Yolanda Valdez, Superintendent Cutler/Orosi School District
Informing the community RMA staff was there for the communities to address concerns and answer questions.

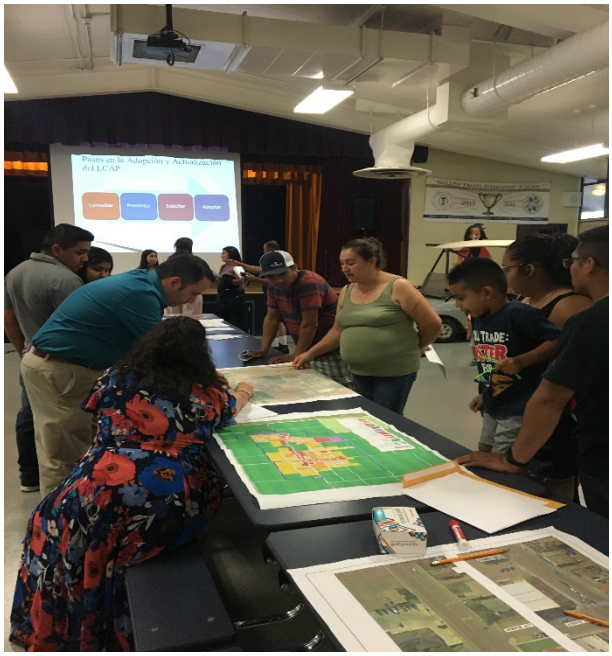
Introduction of County staff by Jose Saenz, RMA staff (Spanish)

Community Concerns:

Cars do not slow down at Cutler School SR 63.
Walnut/SR63 Cars do not slow down
Road 130/Avenue 416 cars do not slow down (cars do not stop after crossing guard has left and children are still walking home.
Road 130/Avenue 416 Lose dogs chase kids
Road 130 between Avenue 416 no cross walks or sidewalks.
Luxor Street between SR 63 and David floods every time it rains hard
SR 63 between Ella and Avenue 416 need flashing light (two children have been hit).
David and Ella need stop signs
Avenue 414 and David need cross walks
Avenue 414 and David drive too fast.
David/Avenue 414 need stop sign
SR 63/Avenue 416 turn arrows north and south, but no arrows going east/west.
SR 63 between Avenue 413 and Avenue 419 needs streetlights.
Safety issues around the preschool at David around preschool. Also, need sidewalks along David (fences almost straight up to roadway).
Avenue 404/SR 63 dark at night needs lights, cars do not stop
SR 63 between Alta and Railroad dark at night, need lights, cars do not stop.
Avenue 416 along the school (need lights
Orosi Sports Park contains basketball, baseball, football, soccer fields. Water area, snack bar, and restrooms.

FAMILY
Connect to your child's learning
Conéctese con el aprendizaje de su hijo/a




Pasos en la Adopción y Actualización del LCAP
Comunicar
Promover
Seleccion
Adoptar

Cutler/Orosi Community Plan Update

Palm Elementary School

August 27, 2019

Aaron Bock, RMA
Hector Ramos, RMA
Jason Garcia-Lobue, RMA
Sandra Sabin, RMA
Roxana Bran, RMA

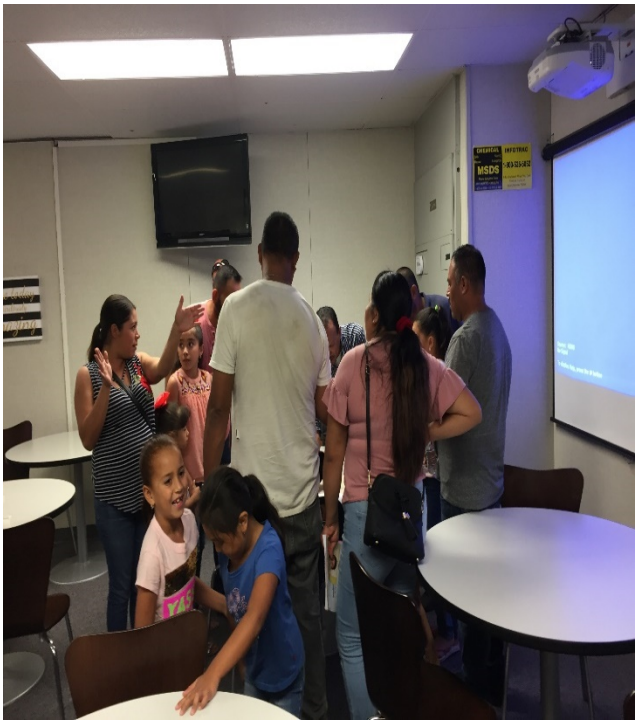
Yolanda Valdez, Cutler/Orosi School District

Approximately __ Cutler/Orosi community members took part.

Introduction of Tulare County Resource Management Agency (RMA) staff by Roxana Bran (Spanish).

Communities concerns:

Avenue 417/Road 136 need stop sign
Avenue 418/Road 140 need stop sign
Avenue 416 east/west bound need green turn light
SR 63/Avenue 419 Flashing Crossing Lights
Road 136 has potholes on N & S side of Avenue 416
Flashing crosswalk lights in front of Orosi High School
Intersections between Avenue 416 and SR 63 could use left turn signals on Avenue 416 in both directions.
Miller and Eddy needs Sidewalk and road repair and crosswalk from Memorial Hall to parking lot.



One of the special members of an Extraordinary family

Cutler/Orosi Community Plan Update
Palm Elementary (Bake to School)
August 27, 2019

Name	Address & Email Address	Phone No.
Brenda Dominguez	42387 Rd 144, Orosi, CA	(559) 397-5621
Iris Lopez	41640 Vista ct orosi ca lopezmails@yahoo.com	(559) 902-8169
Noelia Rodriguez	41659 Rd 164 Orosi	(559) 723-1754
Amanda Figueroa	42042 Rd 128 Sp 74 Orosi	397-1651
Silvia Carreon	41578 RD 130 Orosi	406-0990
Rebeca Martinez	12915 Avenue 419 Orosi	5284757
Mirna Tenango	13498 Ave 416 Apt C	559 391 7972
Julio Quezada	12468 Ripley Ave. Orosi Ca.	559 622 4013
Manuel Canchola		559 406 5401
Mayra Manzo	41427 Rd 136 Orosi CA 93647	559 223-50-18

Cutler/Orosi Community Plan Update

Lovell High School (continuation school)

August 28, 2019

Dave Bryant, RMA
Hector Ramos, RMA
Susan Simon, RMA
Jose Saenz, RMA

Yolanda Valdez, Cutler/Orosi School District

Approximately 10 Cutler/Orosi community members took part.

Presentation by Yolanda Valdez, Superintendent Cutler/Orosi School District, informing the community Tulare County Resource Management Agency (RMA) staff was there for the communities to address concerns and answer questions.

Introduction of Tulare County Resource Management Agency (RMA) staff by Jose Saenz (Spanish).

Communities concerns:
Flooding behind Fiesta Market in alley way need better drainage (neighbor dug up ditch)
Along Road 130 between Avenue 416 and Avenue 419, pot holes
Miller/Road 130 Pot Holes
SR 63 and Avenue 419 lots of congestion
Road 136 between Avenue 412 and Avenue 414 dip in road
SR 63 Pizza Planet (no cross walk and people J-walk)
Eddy safety study

Cutler-Orosi Community Plan Meeting Notes-Lovell High School 8-28-19 (Dave's comments).

Property owner requested rezoning from R-3 and R-1 to R-3 or R-3 mixed-use on APN 023-160-049.
Concern over needing improvements on Road 127 South of Avenue 413 (Ped crossing issues over sand creek and non-maintained road).

General comments:
Supporting larger commercial businesses to promote jobs and consumer access.
Commercial in-fill between Cutler and Orosi OK.
Recommendations for shade and beautification in downtown Orosi on Avenue 416 and shade, pedestrian, and bike improvements on State Route 63.

Cutler/Orosi Community Plan Update
El Monte Middle School

September 3, 2019

Dave Bryant, RMA
Hector Ramos, RMA
Susan Simon, RMA
Juan Carmona, RMA

Yolanda Valdez, Cutler/Orosi School District

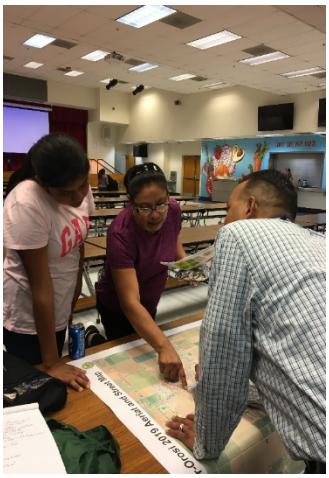
Approximately thirty (30) community members took part.

Presentation by Yolanda Valdez, Superintendent Cutler/Orosi School District, informing the community Tulare County Resource Management Agency (RMA) staff was there for the communities to address concerns and answer questions.

Introduction of Tulare County Resource Management Agency (RMA) staff by Juan Carmona (Spanish).

Communities concerns:

- Avenue 417/Road 136 need stop sign
- Avenue 418/Road 140 need stop sign
- Avenue 416 east/west bound need green turn light
- SR 63/Avenue 419 Flashing Crossing Lights
- Road 136 has potholes on N & S side of Avenue 416
- Flashing crosswalk lights in front of Orosi High School
- Intersections between Avenue 416 and SR 63 could use left turn signals on Avenue 416 in both directions.
- Miller and Eddy needs Sidewalk and road repair and crosswalk from Memorial Hall to parking lot.



Cutler/Orosi Community Plan Update

Family Education Center

September 4, 2019

Dave Bryant, RMA
Hector Ramos, RMA
Jessica Willis, RMA
Juan Carmona, RMA

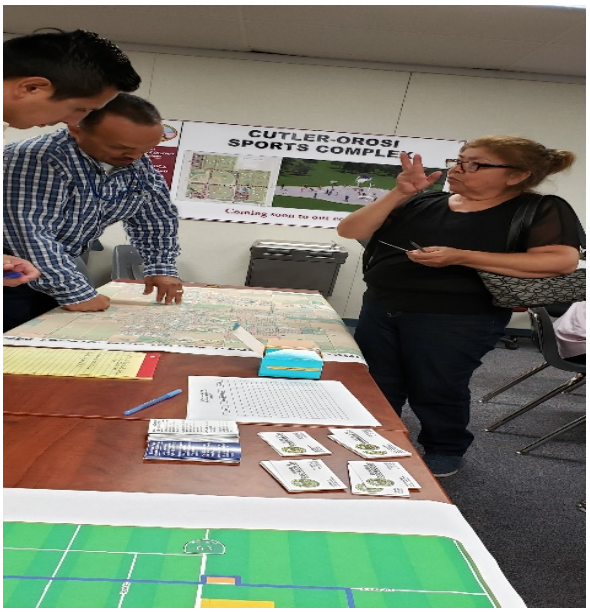
Presentation by Yolanda Valdez, Superintendent Cutler/Orosi School District, informing the community Tulare County Resource Management Agency (RMA) staff was there for the communities to address concerns and answer questions.

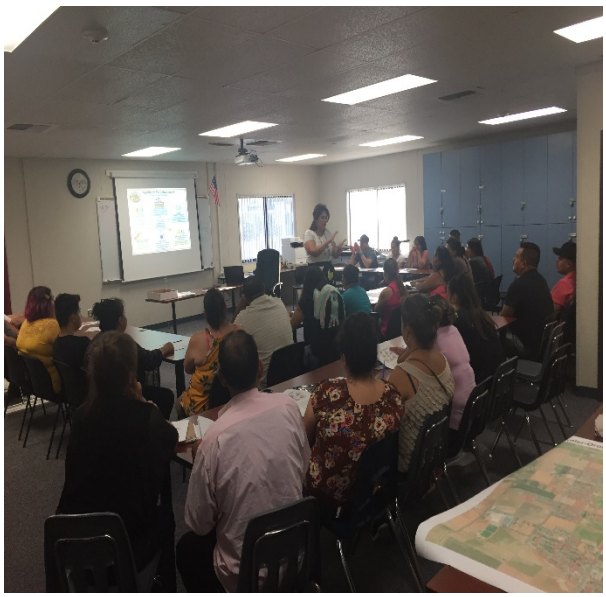
Approximately twenty (20) community members took part.

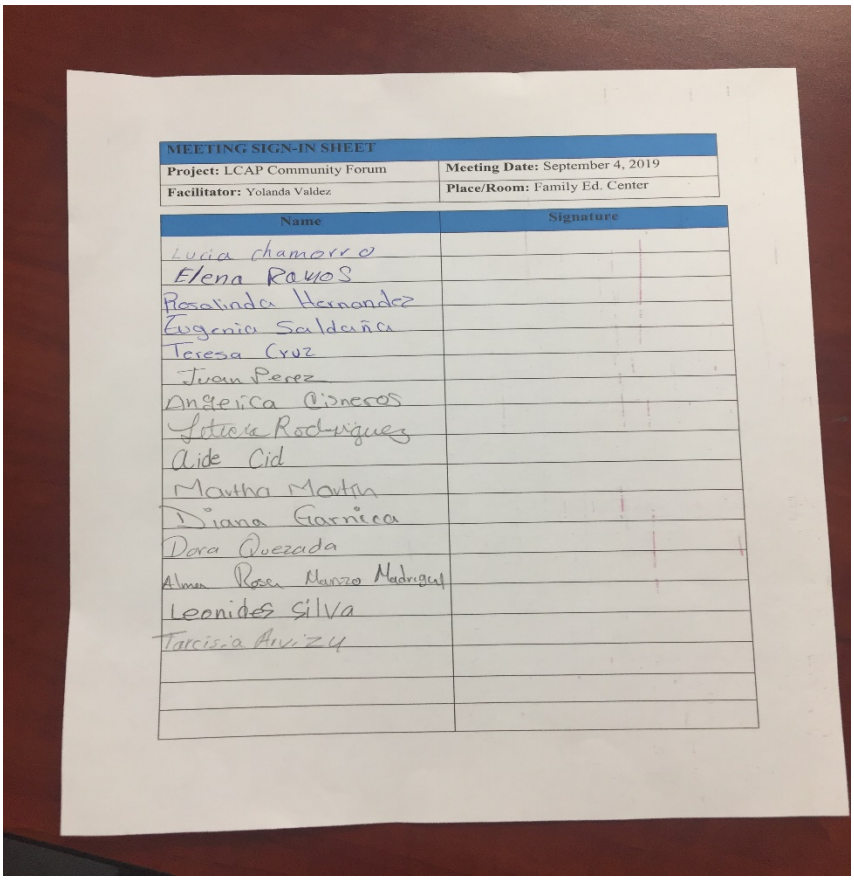
Introduction of Tulare County Resource Management Agency (RMA) staff by Juan Carmona (Spanish).

Communities concerns:

- El Monte Middle School – semis parking by construction area
- Accidents by the memorial hall
- Avenue 4121 between SR 63/Road 130 need lights and sidewalks
- Road 128 Mouth of mobilehome park needs stop sign or crosswalk with flashing lights by El Monte Middle School
- Avenue 419/Road 126 needs a 4-way stop and sidewalks on Avenue 418 by the two parcels on road 126.







MEETING SIGN-IN SHEET

Project: LCAP Community Forum	**Meeting Date:** September 4, 2019
Facilitator: Yolanda Valdez	**Place/Room:** Family Ed. Center

Name	Signature
Lucia Chamorro	
Elena Ramos	
Rosalinda Hernandez	
Eugenia Saldaña	
Teresa Cruz	
Juan Perez	
Angelica Cisneros	
Lilia Rodriguez	
Aide Cid	
Martha Martin	
Diana Garnica	
Dora Quezada	
Almas Rosa Munoz Madrigal	
Leonides Silva	
Tarcisia Arvizu	

Cutler/Orosi Community Plan Update

Family Education Center

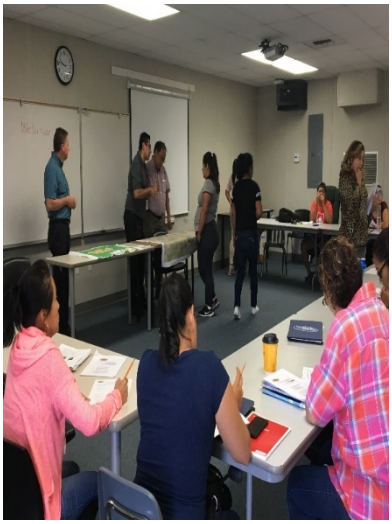
September 10, 2019

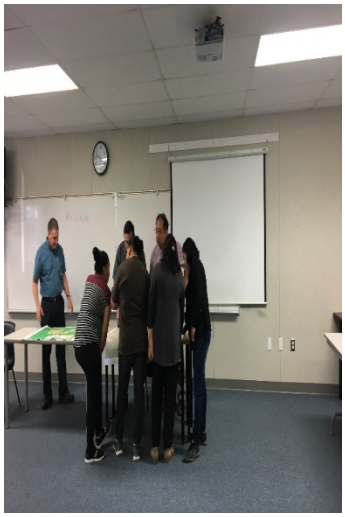
Dave Bryant, RMA
Susan Simon, RMA
Johnny Wong, RMA
Juan Carmona, RMA
Shawn
Amer Razaq

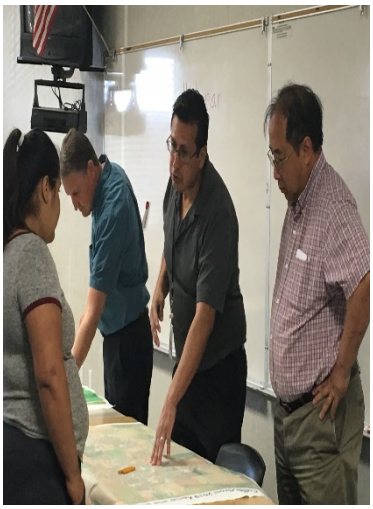
Approximately seventeen (17) members of the Cutler/Orosi community took part.

Introduction of Tulare County Resource Management Agency (RMA) staff by Juan Carmona (Spanish).

Communities concerns:
Community would like to have a streetlight installed on the corner of Ave 419 / State Highway 63 (Next to the Sports Complex).
Community would like a crosswalk installed in the vicinity of El Monte Middle School.
Community would like a flashing beacon installed for the existing crosswalk on the corner of Ave 404 / State Route 63.
Community would like a new crosswalk installed on the corner of School Ave / State Route 63.
Community would like streetlights to be installed on Railroad / Cutler Street.







Cutler Orosi Town Hall Meeting

COJUSD Board Chambers
Tuesday, October 1, 2019
6:30 pm

Welcome & Introductions – Supervisor Eddie Valero

Presentation: Cutler-Orosi After School Program

Community Updates

- Tulare County Water Resource Program
- Tulare County Fire Department
- Tulare County Sheriff's Department
- Resource Management Agency – Code Enforcement
- Resource Management Agency – Roads
- Resource Management Agency - Transit
- Tulare County Animal Services
- Tulare County Parks and Recreation

Question and Answer

Adjourn

CUTLER-OROSI / EAST OROSI BUS TOUR

County staff, please join Superintendent Yolanda Valdez and Supervisor Eddie Valero for a bus tour around the community prior to our scheduled Cutler-Orosi Town Hall. Superintendent Yolanda will share infrastructure needs via a guided tour.

WHEN: TUESDAY, OCTOBER 1, 2019

TIME: 5:30 PM

MEET IN FRONT OF DISTRICT OFFICE

(12623 AVENUE 416, OROSI, CA)

Cutler/Orosi Community Plan Update

Cutler-Orosi/East Orosi Town Hall meeting

October 1, 2019

Michael Washam, RMA
Aaron Bock, RMA
Hector Ramos, RMA
Johnny Wong, RMA
Susan Simon, RMA

Eddie Valero, Tulare County Board of Supervisors

Yolanda Valdez, Cutler/Orosi School District

County employees were given a guided bus tour around Cutler, Orosi, and east Orosi informing them of infrastructure needs within these communities. After the bus tour, a Town Hall meeting met in the school Board meeting room.

Approximately 20 Cutler/Orosi community members took part

Approximately 10 students from the Orosi High School presented their concerns around

Community Concerns:
Street light is out on Edward (12463 Edward Ave)

Easement is fenced off south of Avenue 412 (41235 Road 128)

Ira and Road 127 foot bridge ready to fall

Existing crosswalk not seen at night (fog) Walnut/SR 63 would like flashing lights

Palm School and Road 130 on the east side turn out to drop off students.

Road 130/Avenue 419 need cross walk and flashing lights

Palm School along Road 130 (no sidewalks and the road needs to be repaired) from Avenue 416 and Avenue 419 needs to be repaired.

Ella/SR 63 need Flashing Lights and streetlights (area is dark).

Avenue 404/SR 63 need streetlights and flashing lights.

Golden Valley School need crossing signal by Claude/Avenue 416 also need cross walk.

Claude Road in need of repair, sidewalks, and it floods near Sports Park.

Walnut/Avenue 416 need flashing light.

Avenue 419/SR 63 near Orosi High need flashing lights.

Orosi/SR63 need signal

First/SR 63 walking light

Railroad/SR 63 need cross walk and walking lights.

Ira/SR63 easement blocked

Avenue 413/SR 63 easement issue

Want a Starbucks

Traffic signals and flashing pedestrian warning lights on Highway 63 and Avenue 404 and Orosi Drive in Cutler.

Flooding on Eddie and Avenue 407 and also on Amethyst in Cutler.

Improvements to The Pedestrian Footbridge across Sand Creek near the intersection of Ira and Road 127 in Orosi.

Road 136 improvements between Avenue 416 and Avenue 412 in Orosi.

Pocket Parks recommended on Albert Avenue East of Highway 63, Miller Avenue East of Highway 63, and on Road 124 East of the Sequoia Ave cul-de-sac alignment.

Road improvements on Road 136 between Avenue 412 and Avenue 416.

Recommended shade structures utilizing trees or awnings on buildings in the downtown area of Orosi between the Orosi High School and Avenue 416.

Rails to Trails along Railroad Avenue between Highway 63 and Road 120 in Cutler with connectivity North on Road 122 Avenue 416 in Orosi.

Bike and pedestrian path along Sand Creek in Orosi.

Traffic signal and pedestrian flashing lights crosswalk at the intersection of Avenue 416 and Claude in Orosi.

Pedestrian safety concerns at School Ave and Highway 63 in Cutler.

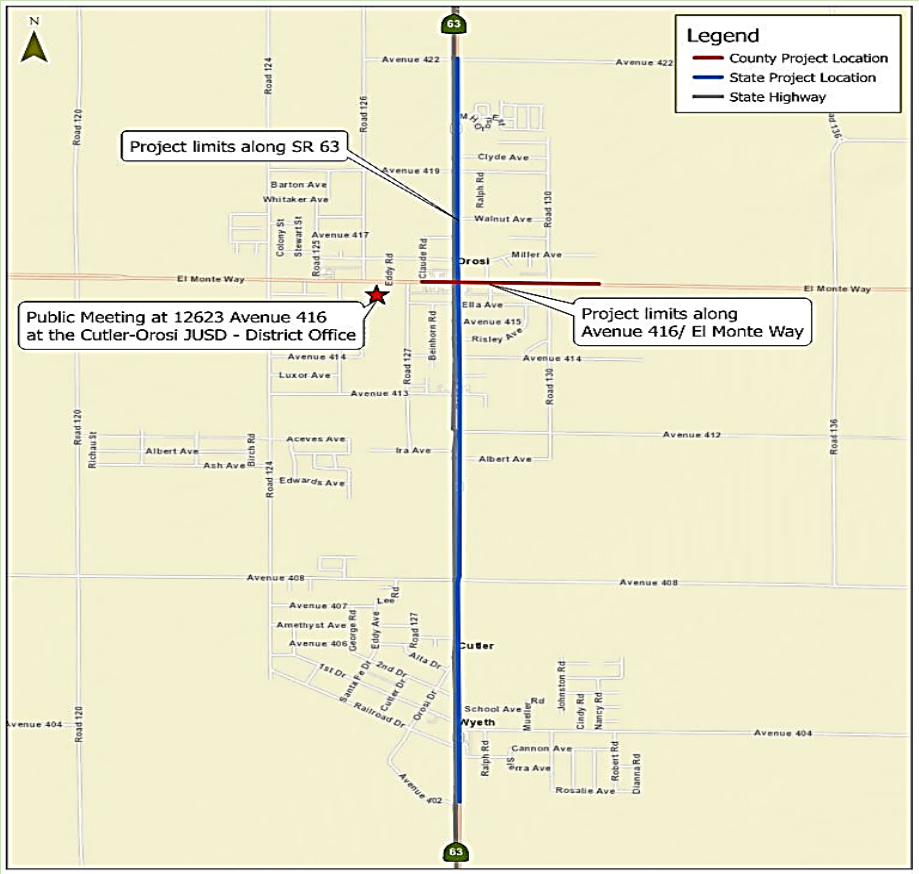
General repairs to Ledbetter Park in Cutler including concerns regarding broken drinking fountains.

Street lights needed on Ira Avenue West of Highway 63 in Orosi.

Flooding concerns on Road 130 between Avenue 416 and Clyde Avenue in Orosi.

Road repairs @Ella Avenue and Highway 63.

Traffic control devices recommended on Road 124 and Road 125 in Orosi to address speeding and safety concerns.



STATE ROUTE 63 (ROAD 128) SAFETY IMPROVEMENTS IN CUTLER AND OROSI AND AVENUE 416 SAFETY IMPROVEMENTS PROJECTS

Caltrans District 6, in conjunction with County of Tulare Resource Management Agency (County) and Tulare County Association of Governments (TCAG), propose improvements to enhance pedestrian and bicycling safety on State Route (SR) 63 in the communities of Cutler and Orosi. The intersection of SR 63 and Avenue 416 will also be reconfigured to provide left-turn lanes with left-turn signals.

SR 63 improvements (Caltrans Project) include:

- New marked crosswalks
- Flashing Beacons at marked crosswalk
- Bike Lanes
- Lighting

Ave 416 improvements (County Projects) include:

- ADA Improvements
- Enhanced pedestrian crossings
- Road Diet (Bike Lines)

Caltrans and County staff will be giving brief presentations to provide an overview of the projects and the opportunity for you to provide feedback.

Please join us in person at the Cutler-Orosi JUSD-District Office or via Zoom to learn more about the projects.

RESOURCE MANAGEMENT AGENCY

Caltrans

DISTRICT 6

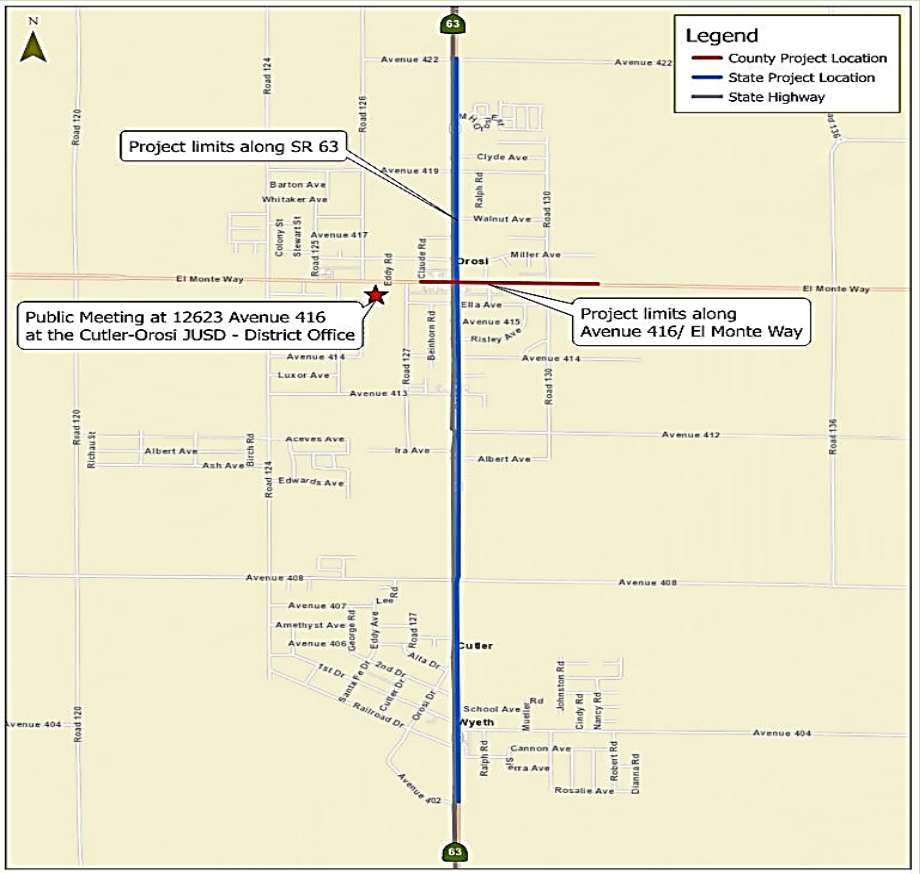
MONDAY
JUNE 7, 2021
5:00 PM (ENGLISH)
6:00 PM (SPANISH)

CUTLER-OROSI JUSD OFFICE
12623 AVENUE 416

ZOOM MEETING ID:
931-5426-2507
PASSCODE: 337432

CONTACT INFORMATION

Juan Carmona
Property Specialist
Office: (559)624-7149
Email:
JCarmona@tularecounty.ca.gov



PROYECTOS DE MEJORAS DE SEGURIDAD DE RUTA ESTATAL 63 (CARRETERA 128) EN CUTLER Y OROSI Y AVENIDA 416

Caltrans Distrito 6, en conjunto con la Agencia de Administración de Recursos del Condado de Tulare (Condado) y la Asociación de Gobiernos del Condado de Tulare (TCAG), proponen mejoras en Ruta Estatal 63 en las comunidades de Cutler y Orosi a fin de aumentar la seguridad peatonal y ciclista. La intersección de Ruta Estatal 63 y Avenida 416 también será reconfigurada para proveer carriles de giro a la izquierda con modificación de semáforo.

Mejoras a Ruta Estatal 63 (Proyecto de Caltrans) incluyen:

- Nuevo paso de peatones señalizado
- Balizas intermitentes en paso de peatones señalizado
- Carriles de bicicletas
- Farolas

Mejoras a Avenida 416 (Proyecto de Condado) incluyen:

- Mejoras para personas discapacitadas (ADA)
- Pasos de peatones mejorados
- Modificación de circulación (carriles de bicicletas)

Personal de Caltrans y el Condado darán presentaciones breves para proveer información general de los proyectos y la oportunidad para comentarios públicos.

Reúnase con nosotros <u>en persona</u> en la Oficina de Cutler-Orosi JUSD o <u>por Zoom</u> para aprender más sobre los proyectos.

AGENCIA DE ADMINISTRACIÓN DE RECURSOS

Caltrans

DISTRITO 6

LUNES
JUNIO 7, 2021
5:00 PM (INGLES)
6:00 PM (ESPAÑOL)

OFICINA DE CUTLER-OROSI JUSD
12623 AVENUE 416

CODIGO DE REUNION DE ZOOM:
931-5426-2507
CLAVE: 337432

INFORMACIÓN DE CONTACTO

<u>Juan Carmona</u>
Especialista en Propiedad
Teléfono: (559)624-7149
Correo electrónico:
JCarmona@tularecounty.ca.gov

	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T
1	**Cutler/Orosi Public Outreach Impacts**	Curb, Gutter, sidewalk	Road Improvements	Street Lights	Flashing Lights	Light Signals	Crosswalk	Flooding	Easement	Safety	Pedestrian Foot Bridge	Park Repairs	Pedestrian/Bike path	Shade Structures	Stop Sign	Turn Arrows	Congestion	Speed Bumps		
2	1st and Railroad (between)									Trucks										
3	2nd/Orosi		x							x										
4	Along Sand Creek in Orosi		x							x							x			
5	Amethyst/Road 129		x							x										
6	Ave 416/Road 130									Trucks										
7	Ave 419/Miller		x							x										
8	Avenue 412 between SR 63 and Road 130		x							x										
9	Avenue 412 /(Sand Creek)									x					x					
10	Avenue 416/David		x		x		x			x					x					
11	Avenue 416/Claude									x										
12	Avenue 416/Road 127				x			x		x										
13	Calude/Miller									x										
14	David/Ave 414				x		x			x										
15	David/Ella			x		x				x										
16	Eddie/Avenue 407			x						x										
17	Edward Ave									x								x		
18	Edward Ave (12463 Edward)				x					x										
19	Emerald/Road 127	x	x	x	x	x	x	x		x										
20	Ira/Road 127		x	x						x										
21	Ira/Road 127 (Sand Creek Bridge)			x						x										
22	Ledbetter Park									x		x								
23	Luxar between Road 124/David					x				x										
24	Orosi Dr between First and Second						x			x										
25	Raukriad Avenue between SR 63 and Road 120 to Road 122/Avenue 416						x			x						x				

	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T
1	**Cutler/Orosi Public Outreach Impacts**	Curb, Gutter, sidewalk	Road Improvements	Street Lights	Flashing Lights	Light Signals	Crosswalk	Flooding	Easement	Safety	Pedestrian Foot Bridge	Park Repairs	Pedestrian/Bike path	Shade Structures	Stop Sign	Turn Arrows	Congestion	Speed Bumps		
26	**Road 124 and Road 125**							x		x										
27	**Road 127/south of Ave 413**							x		x										
28	**Road 127**	x			x					x										
29	**Road 130/Ave 416**					x		x		x										
30	**Road 130 between Ave 416 and Ave 419**			x				x		x										
31	**Road 130 between Ave 416 and Walnut**									x										
32	**Road 130 between Avenue 416 and Clyde Avenue**							x`		x										
33	**Road 136 between Ave 412 and Avenue 416**		x					x`		x										
34	**SR 63 between 413 and 419**			x																
35	**SR 63 north of Ave 419**									x										
36	**SR 63/1st**			x	x	x	x	x		x	x		x	x						
37	**SR 63/2nd**						x			x										
38	**SR 63/Alta**						x			x										
39	**SR 63/Avenue 404**			x	x		x			x										
40	**SR 63/Avenue 408**									x										
41	**SR 63/Avenue 412**									x										
42	**SR 63/Avenue 413**									x										
43	**SR 63/Avenue 414**									x										
44	**SR 63/Avenue 415**									x										
45	**SR 63/Avenue 416**							x		x										
46	**SR 63/Avenue 417**									x										
47	**SR 63/Avenue 419**					x				x							x			
48	**SR 63/Ella Avenue**			x					x	x										
49	**SR 63/Ira Avenue**								x	x										
50	**SR 63/Miller**			x						x										

	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T
1	**Cutler/Orosi Public Outreach Impacts**	Curb, Gutter, sidewalk	Road Improvements	Street Lights	Flashing Lights	Light Signals	Crosswalk	Flooding	Easement	Safety	Pedestrian Foot Bridge	Park Repairs	Pedestrian/Bike path	Shade Structures	Stop Sign	Turn Arrows	Congestion	Speed Bumps		
51	**SR 63/Orosi**			x	X	x	x	x		x										
52	**SR 63/Railroad**						x			x										
53	**SR 63/School Avenue**			x	x	x				x										
54	**SR 63/Walnut**			x	x	x		x		x										
55	**SR 63/Orosi High School**			x	x	x	x			x							x			
56	**Miller/Eddy**	x	x				x			x										
57	**El Monte Middle School**	Truck Parking								x							x			
58	**Cutler School**									x							x			
59	**Golden Valley Elementary**									x							x			
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Appendices

Appendix A: Planning Commission Resolutions
Appendix B: Board of Supervisors Resolution

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Appendix A: Planning Commission Resolutions

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Appendix B: Board of Supervisors Resolution

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Appendices

Appendix A: Planning Commission Resolutions
Appendix B: Board of Supervisors Resolution

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Appendix A: Planning Commission Resolutions

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Appendix B: Board of Supervisors Resolution

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