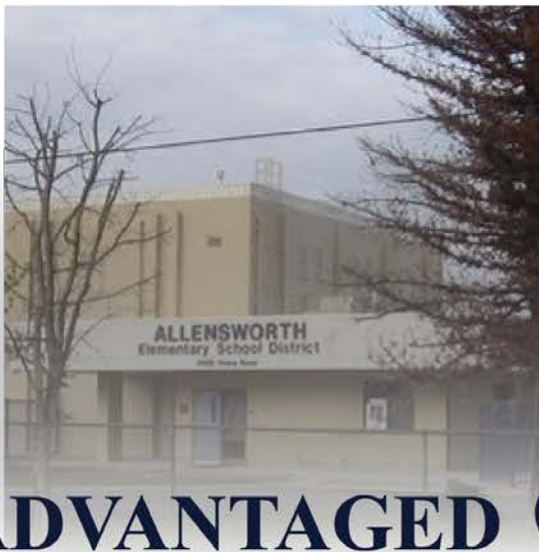
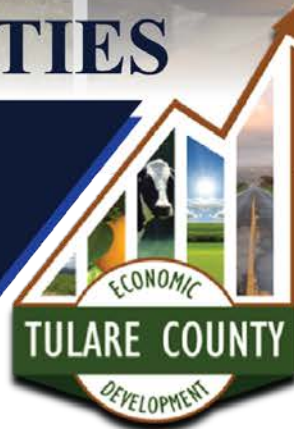


# 2017



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE & PLANNING POLICY STUDY

COUNTY OF TULARE RESOURCE MANAGEMENT AGENCY  
PLANNING BRANCH  
5961 SOUTH MOONEY BOULEVARD  
VISALIA, CA 93277



# **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

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# **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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## **COUNTY OF TULARE SUMMARY AND CONCLUSIONS REPORT: DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

Board of Supervisors

Resolution No. 2017-0976

Adopted December 5, 2017

Community Plans 2017 Update (GPA 17-035)

Hamlet Plans 2017 Update (GPA 17-035)

Board of Supervisors

Resolution No. 2017-0868

Adopted: October 17, 2017

Legacy Plans (GPA 17-033)



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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## **Tulare County Board of Supervisors**

**Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
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Steve Worthley – District 4 (Vice Chairman)  
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Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)**

# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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## **INTRODUCTION**

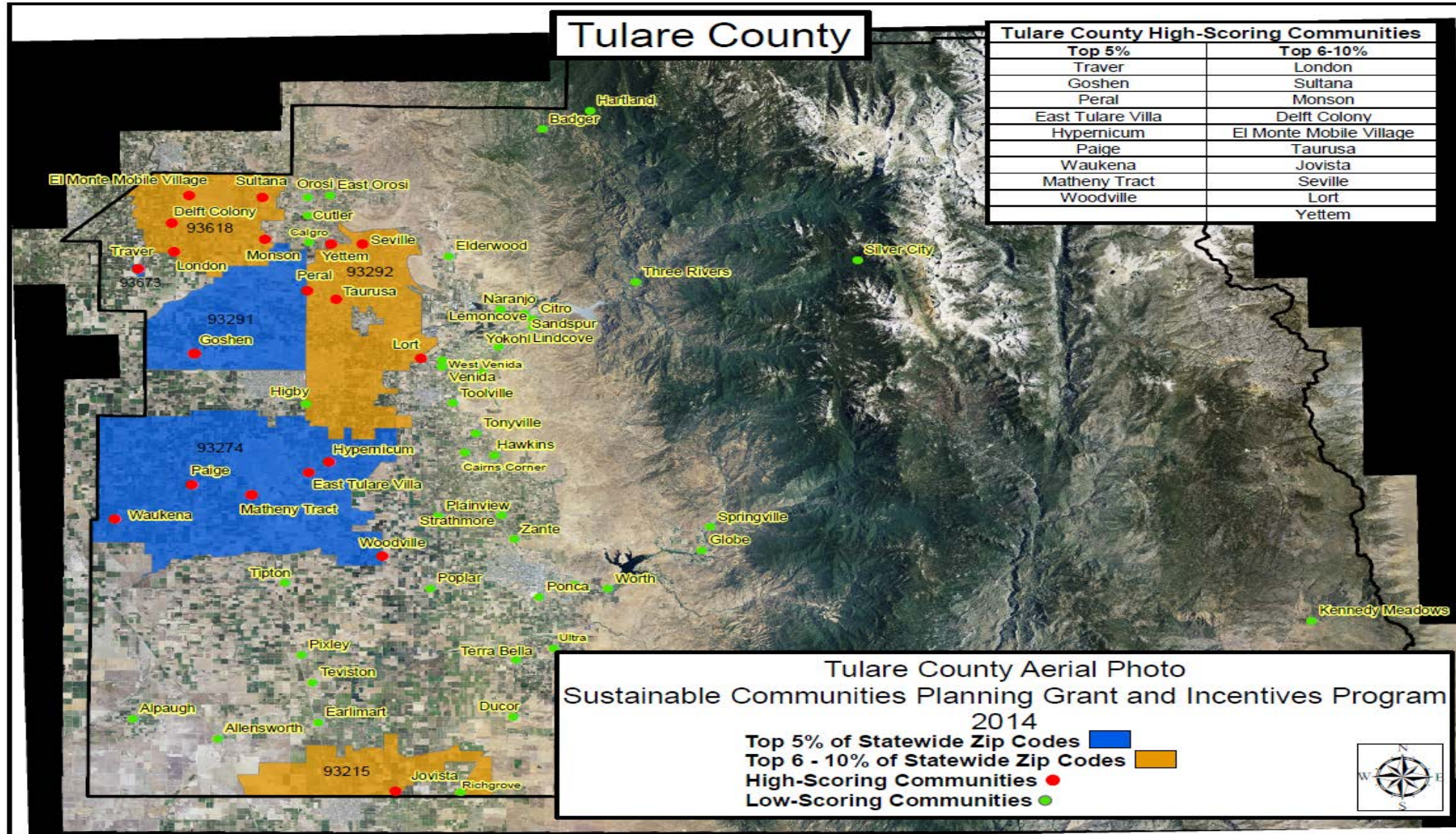
The *County of Tulare: Disadvantaged Communities Infrastructure and Planning Policy Analysis Grant* (“the Project”) is a three-year study and report, resulting in General Plan Amendment (GPA 17-033 and GPA 17-035) representing twenty-one (21) “Community Plans” and over twenty-seven (27) Changes to Zoning Code and Zoning District Boundary Maps, throughout the “Planning Areas,” or roughly 48 entitlements in all. These were approved through an Addendum to the General Plan Environmental Impact Report based studies required for this grant. The Planning Areas are inclusive areas within Tulare County that were established by the California Environmental Protection Agency’s (CalEPA) EnviroScreen, and the location of Disadvantaged Unincorporated Communities (DUC) within Tulare County.

Overlaying SB 535’s “EnviroScreen” tiered areas within Tulare County shows the communities with the highest level of poverty and poor health based on their placement within tier 1 areas, within the top 5%, and tier 2 areas, within the top 10%. (See Figure 1). The tiers are based on Census Designated Places (CDP) with the highest correlation between poverty and poorer health (meaning a statistical corollary relationship between Census Designated Places, poverty and poor health cohorts) in California, as mapped on CalEPA’s “EnviroScreen” software. (See Figure 1). Tulare County had some of the highest rates of CDP’s within tier 1 in the Central Valley, and larger areas covered by the United States Census Designated Places (based on lower population density) than the rest of California. Hence, Tulare County received the funding for this grant. The mapping of “disadvantaged (legacy) communities” had been conducted previously through the 2014 Housing Element and SB 244 Study, and overlaid on the EnviroScreen map for purposes of applying for this grant.



# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

Figure 1 - Study Areas



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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Figure 1 shows the original 19 communities within the study area. During project identification / description phased of the project, as many as 25 communities were decided to receive outreach. However, also through the community plan updating process, only twenty-one (21) of the original 25 identified “Communities” within the planning area were fully reviewed with infrastructure and environmental analysis related to land use.

For this Project, the term “Community” generally refers to those “Areas” contained within Urban Development Boundaries (UDB’s), Hamlets (or Hamlet Development Boundaries or HDB’s, and Legacy Development Boundaries (LDB’s). Typically, in Tulare County, Community has a General Plan definition that is more specific to UDB’s, but in general for this report’s purposes Community can refer to any of the named places described as HDB’s or UDB’s. For further clarification, LDB’s are areas defined within the SB 244 Disadvantaged Area Communities Plan, which is defined under SB 244 as holding 10 registered voters within a one-mile area (mostly subdivision level “communities” that do not have established Community Service Districts).

Furthermore, the rationale for the creation of Legacy Development Boundaries and creation of these plans for the Disadvantaged Communities of Tulare County is under SB 244 as defined in the Housing Element as follows:

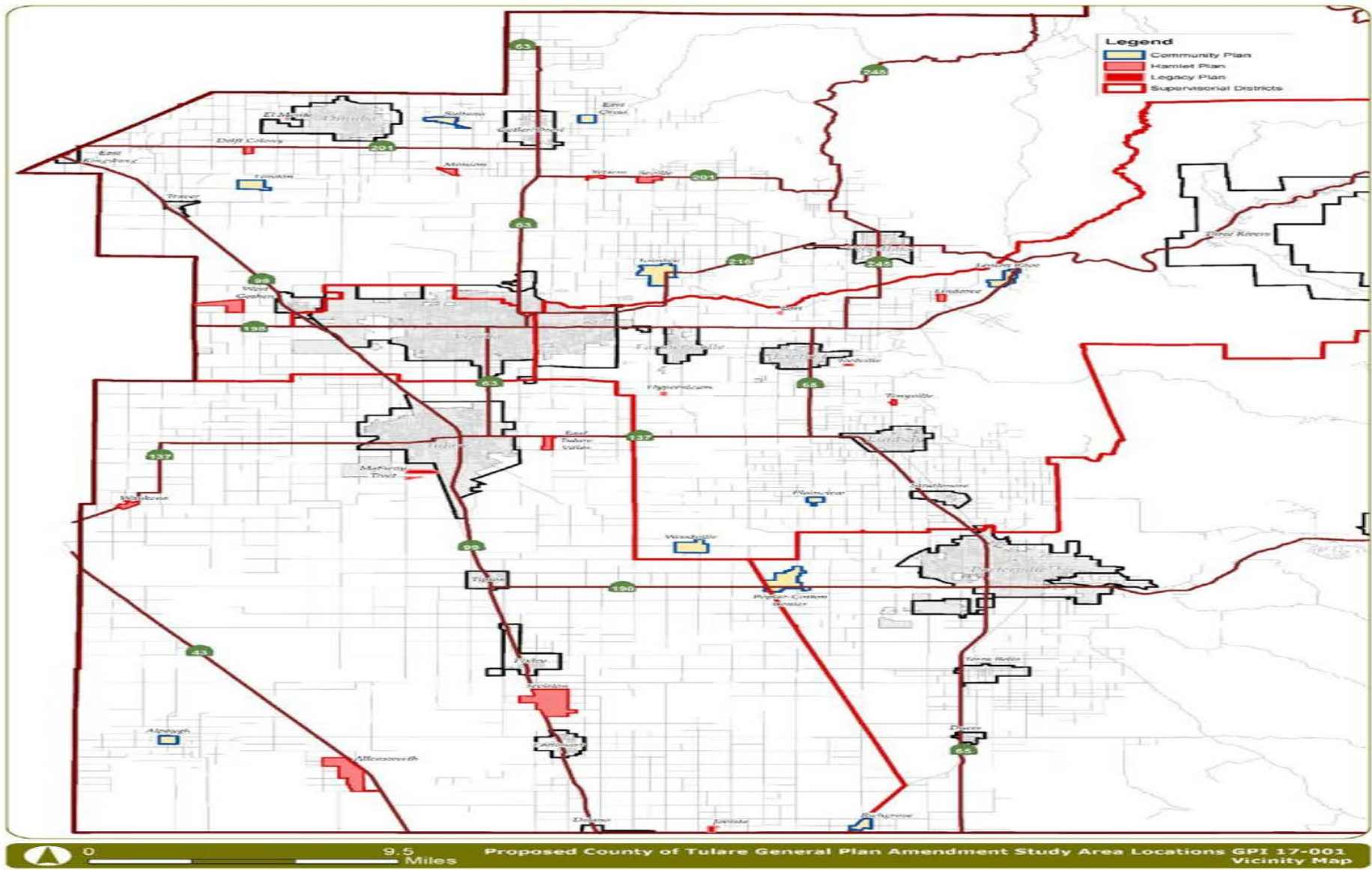
“Under SB 244 (Wolk 2011), the County is required to address “island, fringe, or legacy” unincorporated communities. The County is embarking on looking at the existing infrastructure in disadvantaged communities not discussed in the Action Program 9 Document. This program addresses most of the remaining communities’ existing infrastructure throughout the County. The other communities not addressed in SB 244 will be addressed in Sustainable Growth Council grants the County has pursued and through grants that have been awarded. The SB 244 document also addresses infrastructure needs for all communities, hamlets and legacy communities within the County and describes available grants that can be used to address these needs” (2015 Housing Element page 1-6).

The Tulare County SB 244 Study completed in December 2015 concurrently with the 2015 Housing Element (and Housing Element 2009, Action Program 9) successfully reported on the deficient infrastructure status of the existing communities and proposed “legacy” communities within the General Plan. The Housing Element shows that 25,618 units would be immediately available for housing under existing City (fringe development) and County Zoning. There are over 46,000 units currently built out in the Unincorporated County of Tulare, with just over 3,200 available through approved projects or tentative maps (paper lots) over the last 2 decades, but lack of infrastructure is stymieing developer interest and the ability to build.



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### Figure 2 - Study Area Locations



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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Currently there is three (3) times the amount of zoned land available for commercial purpose within existing larger communities (25,617 unit's worth), to meet the Regional Housing Needs Assessment required number of 7,081 units allocated on non-project or mapped land. (See Table 1 - Housing Element Adequate Site Inventory). Only 3,231 (half of the RHNA number) have adequate infrastructure, and that is only true if each study in Action Program 9 and the SB 244 Study stands true under existing capacity issues and moratorium problems associated with the drought and water quality issues and inadequate waste water systems.

The projected General Plan growth rate (1.3% till 2030) when applied to the existing 25,116-unit adequate sites inventory indicates that there will only be a net increase of just over 3,000 homes built out within the Urban Development Boundaries over the next 13 years (if there is adequate infrastructure). However, since 2005 there has only been a little over 250 units built, year over year, on average in Tulare County. Therefore, the reality is there is not enough / adequate housing growth projected to meet the California Department of Finance, RHNA projection.

Given some of the land projected to sustain *the adequate sites inventory* had non-residential housing, the zoning prescribed in the inventory included commercial and industrial. This suggests that even with this "zoning allowance" housing developments will still grow at its own rates, regardless of flexibility within the Tulare County Zoning Code. Therefore, the point of this study delves into how to further create incentives for economic development through expanding mixed-use zones, removing conditional permitting requirements and creating a job to housing balance thereby reducing VMT.

# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

Table 1 - Housing Element Adequate Site Inventory

Adequate Sites Inventory				
Community	Single Family	Multi-Family	Approved/Pending Subdivisions`	Total Units
<b>Communities</b>				
Alpaugh	185	22	0	207
Cutler-Orosi	710	660	40	1,410
Ducor	400	212	0	612
Earlimart	274	780	385	1,439
East Orosi	16	0	0	16
East Porterville	894	188	113	1,195
Goshen	263	515	526	1,304
Ivanhoe	63	735	80	878
Lemon Cove	61	4	0	65
London	358	0	0	358
Patterson Tract	177	70	55	302
Pixley	712	478	558	1,748
Plainview	3	0	0	3
Poplar-Cotton Center	577	372	406	1,355
Richgrove	0	133	0	133
Springville	797	389	206	1,392
Strathmore	38	201	47	286
Sultana	724	4	0	728
Terra Bella	817	401	0	1,218
Three Rivers	2,409	1,204	0	3,613
Tipton	837	530	172	1,539
Traver	212	259	0	471
Woodville	<b>259</b>	<b>103</b>	<b>247</b>	<b>609</b>
<b>Hamlet Development Boundaries</b>				
Allensworth	723	0	0	723
Delft Colony	7	0	0	7
East Tulare	27	0	0	27
Lindcove	56	0	0	56
Monson	195	0	0	195
Seville	71	55	0	126
Teviston	0	0	0	0
Tonyville	7	0	0	7
Waukena	0	0	0	0
West Goshen	27	0	0	27
Yetter	4	0	0	4
<b>City Fringe</b>				
Dinuba	546	0	0	546
Exeter	0	0	0	0
Tooleville	2	0	0	2
Farmersville	352	0	0	352
Lindsay	52	0	0	52
El Rancho Tract	16	0	0	16
Porterville	355	80	113	548
Tulare	135	17	0	152
Visalia	491	11	178	680
Tract 92	2	0	0	2



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

Adequate Sites Inventory				
Community	Single Family	Multi-Family	Approved/Pending Subdivisions`	Total Units
Woodlake	5	332	46	383
Kingsburg	761	0	70	831
Total	14,620	7,755	3,242	25,617

### 1.1 Reports & Studies

The reports contained within this document are generally broken into Outreach, Infrastructure, Environmental, and Planning. The following is a summary of these sections of the report, as related to the tasks and scopes of works required to fulfill the requirements of the grant.

For the purposes of this grant, the Infrastructure Needs and Constraints Analysis of the Disadvantaged Communities within the planning area, includes also the following reports, and/or analysis:

- (1) The Outreach Process (Chapter 2)
- (2) Assessment Report of Infrastructure Availability: Needs and Constraints Report (Chapter 3),
- (3) Mapping and Diagram of Existing Infrastructure Analysis & Strategies for Improving Infrastructure (Chapter 4)
- (4) Environmental Report on Reducing Vehicle Miles Traveled and Green House Gas Emissions (Chapter 5)
- (5) Planning Report for Changes to Zoning Policy to Incentivize Economic Development (Chapter 6)
- (6) Assessment of Housing Conditions and Housing Conditions Survey (Chapter 7)

The Plans approved, as part of this grant include the *Urban Development Boundaries Update*, which includes Final Community Plans that are approved concurrently with this report (“Community Plans Report”). These are discussed in Chapter 6 and in Appendix B. The analysis includes existing conditions, including demographics and environmental characteristics. The infrastructure analysis includes both qualitative and quantitative analysis for increased potential for grants. The focus is on the policy changes required to match the 2012 General Plan Policy Framework Plan, Rezones, and to stimulate Economic Development. Namely these include:

1. Alpaugh
2. East Oroshi
3. London
4. Richgrove
5. Sultana

The following chapter is the *Hamlet Development Boundaries Plan Update*, and includes a report and study on the Hamlets, and includes a General Plan Amendment and Zoning Changes to officially update

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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the Hamlets in the General Plan. These are discussed in Chapter 6 and in Appendix C. The Hamlets include:

6. Allensworth
7. Delft Colony
8. East Tulare Villa
9. Lindcove
10. Monson
11. Seville
12. Teviston
13. Tonyville
14. West Goshen
15. Yettem
16. Waukena

In Chapter 7, the *Legacy Communities Plans* includes a report and study similar to the Communities and Hamlets, but also requires the creation of these “legacy areas” under the General Plan. These are discussed in Chapter 7 and in Appendix D. The Legacy Communities include:

17. El Monte Mobile Home
18. Hypericum
19. Jovista
20. Matheny Tract
21. Tooleville

### **1.2 Sustainable Growth Commission (SGC) Infrastructure and Planning Policy Analysis Grant (“the Grant”) Report**

The original intent of this grant was to:

“perform a Disadvantaged Communities Infrastructure and Planning Policy Analysis. We intend to integrate infrastructure analysis with the needs of our individual rural disadvantaged communities. A thorough examination and cataloguing of existing land uses will suggest infrastructure improvements, land use changes and possibly zoning changes, where feasible, in order to fulfill the goals of Senate Bill 244. The resultant Disadvantaged Community Infrastructure Maps will provide the measurement tools needed to identify infrastructure solutions and appropriate improvement projects. The intent is to reduce greenhouse gas emissions, promote equity, provide economic stability and thus enhance sustainability, protect the environment and promote healthy and safe communities. These are communities with nonexistent, aged or failing infrastructure that face serious contamination challenges, economic inequities and a severe lack of resources”(Grant Application Summary (2013)).

The County in fulfillment of this Grant seized upon the opportunity to not only fulfill the obligation for the SGC Grant, but also to fulfill the obligations of the 2012 General Plan, and that is to create official Hamlet Plans and Legacy Plans. Under the Housing Element / SB 244 Report (General Plan Amendment (GPA 2015-C)), the County is to provide direction for Legacy Communities through the

# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

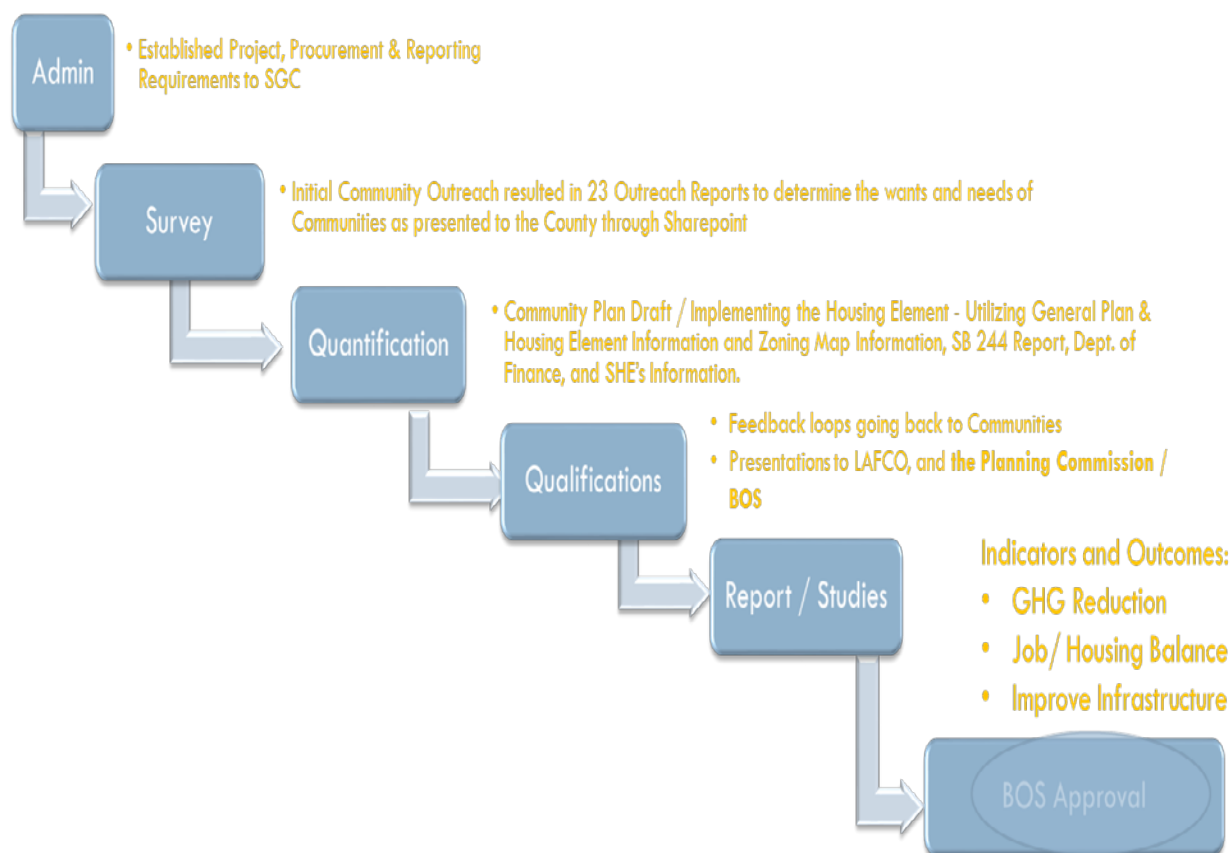
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use of an even smaller increment of development boundary in the form of “Legacy Plans.” Therefore, the County instead of merely making a finding to the SGC that the next and/or final steps for a future planning exercise would be creating these plans, the County took the “next step” and have completed a wide range of documentation from environmental, to infrastructure, to general services, and land use in addition to the infrastructure related plans and policies within each “Community Plan.”

“The goal of this report is first and foremost, as stated in the Housing Element, is regarding the status of infrastructure in each of the communities within the study area. These studies initially only looked at the sustainable infrastructure in the top 10% of the areas suggested by Cal Environmental Protection Agency’s California Environmental Scan (EnviroScan). Ultimately, these studies looked at how to address land use related to available infrastructure, including areas designated for housing. *The result will be a study that shows how much of the “adequate sites” can be serviced by available infrastructure*” (2015 Housing Element page 6-26).

All of this was in a real attempt to provide the state and federal agencies a glimpse into the demographics, health issues and infrastructure needs to help provide the background information for grant funding and to increase opportunities for economic development. It will also provide the State of California with a model to handle the unique nature of Tulare County being the largest agricultural economy in California, while dealing with issues of poverty, health, and limited infrastructure.

## 1.3 Tasks and Sub-tasks



# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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## Project Development.

Through *Task 1 Project Development*, this Project has morphed into a broader application of the Tulare County General Plan and Zoning Policies. As this project was being developed, concurrently, the General Plan Housing Element and Disadvantaged Communities SB 244 Countywide studies were being completed, updated and approved by the Board of Supervisors, as General Plan Amendment (GPA 2015-D). The Housing Element and SB 244 Studies have a significant amount of infrastructure services and general services studied throughout the County. With the bulk of the work done on the qualitative analysis of infrastructure, the focus began to look at the real difference between the “on the ground” experiences of the neighborhoods, as was discovered in workshops, versus the infrastructure data and research / analysis previously conducted in the Housing Element. Therefore, the County proceeded to adjust the focus on how to create opportunities in these Communities through policy changes. These policies include the application of alternative Zoning policies and consequently bringing properties into General Plan compliance. This was to match the need for economic development, creating jobs to housing balances, have land owners bring forward building permits, and to support much needed infrastructure that cannot be fostered only by limited state and federal grant opportunities, wholly, but will require efforts by the private side of development in order to match the infrastructure needs.

In the Second Round of Project Development, EnviroScreen 2.0 was used. The original list of 19 communities was paired down and expanded or limited based on the communities’ ability to receive outreach and data collection. These were both in the “EnviroScreen” tiered areas with the highest level of poverty and poor health (tier 1 within the top 5% and tier 2 within the top 10%) but were also based on outreach criteria (accessibility) presented by Self Help Enterprises and the Leadership Foundation; or whether a community plan had already been done for the Community (i.e. Ducor & Strathmore.)

## Top 5%

1. East Tulare Villa
2. West Goshen
3. Hypericum
4. Matheny Tract
5. Traver
6. Waukena
7. Woodville

## Top 6-10%

8. Delft Colony
9. El Monte Mobile Village
10. London
11. Monson
12. Seville
13. Sultana

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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### 14. Yettem

#### Low Scoring

### 15. Allensworth

### 16. Alpaugh

### 17. Ducor

### 18. Earlimart

### 19. East Orosi

### 20. Lindcove

### 21. Richgrove

### 22. Tonyville

### 23. Tooleville

### 24. Strathmore

**Total: 24 communities**

**In addition, Lemon Cove was discussed (so 25 communities), but no additional studies were conducted for Lemon Cove due to inability to get the community to participate, but Lemon Cove is proposed to be revisited in the future.**

#### Data Gathering.

The Second task, *Task 2 Data Gathering*, was accomplished through data gathering of demographic and environmental information from both the consultants, through their community survey documentation, and by staff. The first part of the analysis compared the demographic information to the Department of Finance's, *American Survey Estimates (2010 to 2014)*, and existing environmental information from the Public Works Department, and existing agency information from FEMA, California Department of Fish and Wildlife, and United States Geographic Survey. The main analysis is in regard to demographic studies and were followed with focused data procurement for environmental considerations of relative environmental constraints and information to the location of the community such as flood, biology, and soils. The bulk of the infrastructure analysis came from two documents, the 2014, *Housing Element Action Program 9*, and the *Senate Bill 244: Disadvantaged Unincorporated Communities Assessment* (May 2015). The remaining documents were garnered by the consultant, Self Help Enterprises, and on a more limited basis, the Leadership Foundation Counsel, who have historically helped procure and develop many feasibility studies for infrastructure improvements throughout the County.

Initial formal Workshops were held and conducted within as many of the twenty-three (23) communities between 2014 and 2016. These were followed up with an additional 24 workshops regarding desired street improvements related to the Circulation Element of the General Plan, "Complete Streets" policies, and to review the *Infrastructure Needs and Constraints Report* within the Communities. These last two opportunities led to additional information gathered that was also included in this report.



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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The consultant provided information through surveys, and based on the surveys, created a “SharePoint” file database to hold the information they gathered. The information was posted online in various formats (excel, word, and pdf), including draft documents and summary write ups of the workshops including critical statistics and graphs on modes and distances traveled, infrastructure deficiencies, and the overall needs of the communities. These statistics were ranked, and further analyzed in Task 3, and discussed in the workshops (stakeholder review).

### Identification and Description of Communities.

*Task Three, Identification and Description of the Communities* represented in this Report are based on several factors. Highest priority was given based on the qualifications required by the grant. These were normalized by three factors, (1) availability of the community to hold general workshops, and (2) whether the County had completed Community Plans (i.e. Goshen, Traver, and Pixley), or was in the process of completing Community Plans (i.e. Earlimart / Woodville), and (3) whether the community had sufficient available persons to hold a meeting or in the case of the El Monte Mobile Home Park would not allow the County to hold a workshop. Each community is described by the Census Designated Place (CDP) for workshop purposes but in some cases, may have a different Development Boundary name, and is further detailed or replaced by other communities through agreement of the County and Consultant accordingly. This led to the resultant twenty-one (21) Communities recommended for approval to the Board.

Each of the twenty-one (21) communities is further introduced and described textually in general terms, at the introduction to each Community Plan (See Chapter 6 and Appendix B-D).

### Analysis of Infrastructure Needs and Constraints.

*Task Four, Assessing Infrastructure and Constraints* was achieved by reviewing the quantitative data against the qualitative data found through workshops. The information was mapped in GIS and where there was a difference in the data sets, the analysis indicated where the differences initiated from. A final report summary and conclusion on the *Infrastructure Needs and Constraints* is a sub-section of this Chapter.

### Reviewing the Land Use and Zoning Policies.

*Task Five, Reviewing the Land Use and Zoning Policies* was conducted by staff. During the workshops the existing traditional land use and zoning were discussed, and it was suggested there are alternative forms of land use and zoning that may incentivize economic development. Simultaneously, the discussion centered on bringing hundreds of properties that are not in conformance into compliance with the General Plan and Zoning Code. The Communities were favorable to this approach, and staff has already updated seven (7) communities with this approach.

We also reviewed the requirements under the California Environmental Quality Act (CEQA) and found that the Community Plans are consistent (Finding of Consistency and Addendum to the Tulare County General Plan Environmental Impact Report (EIR)). Moreover, the Project we found to be self-mitigating in its approach to reduced vehicle miles traveled, and reduced greenhouse gasses

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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through the “mixed use” concept allowing multiple uses and other reduced entitlements on one property.

The General Plan land use for most of these communities is “Mixed Use.” The County has strong interest in separating agricultural uses from urban uses in the Urban Boundaries, as an example under A-1 Zone (Agriculture 1-Acre Minimum Zone), to a more formal residential use such as R-A or R-12,500 or R-43,500 Zone, as A-1 Zoning is overly broad in its approach and does not limit uses by design and was considered a “holding zone.” Adding commercial and industrial opportunities including a “Mixed Use” Overlay Zone and reducing the uses that require a “Special Use Permit” is a more viable, progressive, and sustainable approach to reducing regulation and increasing development opportunities, increase job to housing balances, and reducing Green House Gas Emissions.

### Public Hearings.

*Task 6 and 7 consists of Public Hearings* that were conducted in front of LAFCO, Tulare County Planning Commission and the Board of Supervisors. These were required for Amending the General Plan, Zoning Code, and under CEQA. The resolutions in the attachments include:

(Attachment 1 & 2, Public Hearings and Approvals) in this Chapter are certifications and approvals of the information and of the Community Plans, the CEQA documentation, overall studies and recommendations.

### Report Conclusions.

According to the SGC, the Report should indicate how each of the Project’s three selected primary objectives, as identified in the grant application, has been incorporated into the planning process. The Report should also address how the Project has addressed any additional objectives or co-benefits. This Report addresses the following objectives of the SGC and the State of California:

- Improve air and water quality
  - Promote public health
  - Promote equity (primary objective 1)
  - Increase housing affordability
  - Increase infill and compact development
  - Revitalize urban and community centers
  - Protect natural resources and agricultural lands
  - Reduce automobile usage and fuel consumption
  - Improve infrastructure systems (primary objective 2)
  - Promote water conservation

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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- Promote energy efficiency and conservation
  - Strengthen the economy (primary objective 3)
1. **Promoting Equity:** The purpose of this study is to analyze how to strengthen the economy and improve public health in these disadvantaged communities. The disadvantaged nature of the communities is documented in the Community Plans. Environmental Justice is achieved through the workshops and everyone having an opportunity to discuss the issues and respond to them in both Spanish and English at every workshop. Fairness is being promoted by giving the disadvantaged communities a leg up in improving their communities through creating plans and policies. The County has increased this real possibility of a citizen, of one of these communities, increasing their chances of success by not only studying the community plan's planning policies, but moving forward to changing the zoning and limiting use permit requirements, without them having to pay for these changes (\$10,000 to \$4,000 savings). Thereby giving each citizen of these communities a better chance to create economic development and a better job to housing balance. This effort also required an analysis of existing infrastructure needs and constraints and this information was created with the help of the people within the communities.
  2. **Improve Infrastructure:** By gathering data and providing it to the Community, the County has been able to evaluate what's occurring based on its infrastructure studies and present the information to the communities to gain the citizens feedback based on their perception and expectations. The Community Plans discuss how to improve the infrastructure systems.
  3. **Strengthen Economy:** Economic opportunities have been presented by business interests within the County; however, the severe lack of adequate infrastructure and facilities prove infeasible to overcome. As discussed above, the promotion of the mixed-use concepts in land use and zoning will increase job/housing balances, increase infill and compact development, reduced automobile dependence; and thereby, reducing fuel consumption, and we believe strengthening the overall economies within the disadvantaged communities.

### Sustainable Indicators and Outcomes

The three spreadsheets below are the original grant indicators that have been updated with a right-hand column discussing the report's findings for each indicator described. Qualitative Analysis through workshops in tasks 1 and 2 indicates that citizens are exhibiting concerns over inadequate infrastructure, especially during the drought and flooding conditions from 2013-2017. They are commuting 25 plus miles for shopping and employment, which is greater than the 23-minute avg. commute time for Tulare County. They also have concerns over road maintenance, road improvements, and policing / lighting/ sidewalks (safety). But they are interestingly equally concerned about lack of Wifi / Community Centers and parks. The County and Special District are moving rapidly to start water and sewer projects in the highest priority areas; however, there is a lag time of at least a couple of years in most instances, and since the rains of 2017 has taken some of the pressure off the water conservation / drought issues. This expresses a great need for more economic development to increase chances for improvements, as the County and CSD's both rely on grants to fund improvements, and there are few fee mechanisms to serve these purposes.

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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Tasks 1, 2, and 3, (Chapter 3 & 4) helped develop Qualitative and Mapping Studies indicating through Infrastructure studies that every community is deficient in either water or sewer, and sometimes both. However, some communities have a surplus of either water or sewer capacity. However, once improvements are made, they will need to increase population density, through zoning, to allow for development to meet the capacity necessary, without expanding into new areas; but through upsizing piping, hence the cost of entitlements would be a greater detriment to these disadvantaged communities.

In Task 5 (and Chapter 5), the greenhouse gas studies (GHG) indicated what the total Green House Gas emission reduction is in total Carbon equivalents (CO<sub>2</sub>e), and established the metrics for the previous indicators of future modeling of the infrastructure / jobs / housing cohorts on the spectrum of future potential CO<sub>2</sub>e source criteria (i.e. can changing zoning thereby increase infrastructure or infrastructure increase zoning, with a resultant of reduced Vehicle Miles Traveled and thereby reduce GHG). The County did an addendum to the General Plan EIR for this project.

Planning and Zoning studies in Task 5 and in Chapter 6 indicates that there is an adequate supply of mixed use land uses in the County to support economic development needs, and the Housing Element indicates there is enough zoned land for residential development and more commercial development, but that land is not necessarily owned by persons interested in developing, or in areas that have adequate infrastructure. Through outreach, the County could reach even more persons that were interested in rezoning their properties, without the citizens funding their own zoning changes. Outreach also indicated the need for the County to rezone properties, and remove further regulatory burdens, such as the use permit requirements to allow for greater economic development within these communities.

Summary of Indicator Measured (see Table 2-4):

1. Participation level in Public Engagement Process – developed new standard of engagement as utilized by Self Help Enterprises to go door to door to notice meetings, involved surveys to get information and published information to the County through SharePoint.
2. Existing access to infrastructure – gathered both qualitative and quantitative information for each community – prioritized community projects through Spreadsheet suggested in 2015 Housing Element.
3. Population with no healthy food outlet – community plans focus on requirements for grocery store type zoning.
4. Residents at Poverty Rate – Land use to mixed use documented in Community Plans, conducting rezones currently to change land use policies to see if stabilizes poverty and travel times.
5. Lack of Affordable Housing – Housing Condition Survey comparing 2009 housing survey to 2014 housing survey and information presented by citizens as to their perception of housing within their communities. Currently being studied as part of overall project report.
6. Improve Infrastructure – have background data for storm drain, water and sewer surveys and quantitative data.

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7. Complete Streets – have been completed for some of the communities where warranted. To increase mobility and walkability. These were mapped and identified as needing improvements. Already applied for ATP grants for some of these roadways.
  - a. Complete Streets Grant funded programs were completed for:
    - i. Allensworth, Alpaugh, Earlimart, East Orovi, and Woodville
8. Improve fire protection. SB 244 study is utilized to locate (map) fire stations in relation to communities.
9. Land Use and Zoning Maps currently adopted in General Plan was adopted for these communities, on a community by community basis, through a larger General Plan Amendments and rezoned through rezoning process – we can track real change through PALMS process system through the County’s Annual General Plan Report.

**Table 2 - Summary of Indicator Measured**

Indicator Measured	Data Source (Include date compiled)	Indicator Baseline	grant-term milestones (1-3 years)	implementation milestones (3+ years)	Expected Outcome and/or CHANGE from the baseline	End OF PROJECT Report
Participation level in Public Engagement Process	Inventory of Public Participation with CAL/EPA 2005	Existing public outreach methods – flyers through schools and community service districts.	Involve stakeholders in meaningful participation, access to analysis of infrastructure and text changes <i>(Completed)</i>	Develop new standards for engagement (completed door to door flyers and noticing protocols have been established)	Utilization of community surveys, teleconference meeting opportunities and expanded distribution methods	SHE completed community surveys and draft outreach studies, with results reported in SharePoint, and we are now reporting results back to communities. Distribution achieved by door to door delivery of brochures. Presentations in power point: teleconferencing not an option for lack of available technology & services provided, so staff and consultant directly presented at over 50 meetings.
Existing access to infrastructure	Analyze existing needs and constraints <i>(Completed)</i> .	Existing conditions based on data gathered <i>(Completed)</i> .	Map existing conditions and identify areas of need <i>(achieved)</i>	Prioritize projects, devise project descriptions and seek funding <i>(within General Plan Horizon (2030))</i>	Increased applications for infrastructure projects	Utilizing Housing Element and SB 244 studies have completed mapping for the majority of communities in GIS / Corral Draw. Creating infrastructure status spreadsheet (Tables 5&6), as required by the Housing Element,



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

						showing levels of infrastructure and whether the needs be met at the time of the Gen. Plan (2030) or after based on population projections.
Population with no healthy food outlet	CDC, Division of Nutrition, Physical Activity, and Obesity, 2011	Currently 20.5% of population	Plan reflecting increased focus on economic development opportunities ( <i>achieved</i> )	Use economic development opportunities to bring in healthy food outlets to reduce travel by 30%	Increase in food outlets within disadvantaged communities by 65%	Analysis shows that residents are looking for “big box” stores. County rezoned properties to commercial.
Residents at Poverty Rate	The DataWeb Hot Report 2 2010 Census	Currently 23.8% Poverty Rate	Plan reflecting better jobs/housing balance	Actual changes to land use and zoning policies	Increased jobs/housing balance by 150%	American Survey (DOF) and Census show all the communities are disadvantaged. Zoning changes away from Ag zoning to urban zoning may reduce the severely disadvantaged status.
Lack of Affordable Housing	Tulare County Housing Element 2009 / 2015 Update (completed).	32.1% of households in unincorporated Tulare County are overpaying for housing	Complete Housing Conditions Survey by 2015. (Completed - achieved).	Plan reflecting promotion of affordable housing – shown in 2015 Housing Element.	Increase affordable housing opportunities by 40%	Updated Housing Element in 2015 / created SB 244 Report as update to General Plan, and utilized Conditions survey which was completed by 2015. In addition, SHE’s Community surveys did self-reporting on housing status. Housing Element / community surveys shows high demand, high amounts of zoned property, relatively light regulatory hurdles, but no supply being created by developers. Affordable housing being increased through manufactured and mobile homes in 2 <sup>nd</sup> & 3 <sup>rd</sup> residents and employee housing. Rezoned Properties to higher density. A housing conditions study was conducted as part of this grant and is found in Chapter 7.

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**Notes:** This section should include limitations of data, assumptions, uncertainties, time lags in collection, method for tracking progress, and other necessary information about this indicator.

**Table 3 - Summary of Indicator Measured**

Indicator Measured	Data Source (Include date compiled)	Indicator Baseline	grant-term milestones (1-3 years)	implementation milestones (3+ years)	Expected Outcome and/or change from the baseline	End OF PROJECT Report
Reduce water runoff, increase biofiltration, detention and drainage	US EPA Green Long Term Control Plan EZ template 2011	Existing conditions based on Flood Zone maps (unknown condition)	Map and calculate retention and detention based on needs analysis, identify existing stormwater drainage master plans. <i>(Complete identifying existing stormwater and drainage master plans)</i>	Design and seek funding for filtration/detention/recharge and funding for stormwater drainage	Known flooding conditions, increase water quality, increase ground water recharge, improve drainage	We have downloaded and shown the FEMA flood zones / soil maps for each community plan. We have documented storm drainage issues / street flooding issues, through the surveys and workshops, and the storm water drainage plans, where they exist, for the few communities that have them.
Improve sewer facilities and treatment	US EPA Green Long-Term Control Plan EZ template 2011	Number of individual septic and estimated pollution levels	Map and calculate amount of contamination and increased centralized sewer system designs	Design and seek funding for centralized sewer facilities and remediation of existing sewer systems	Known sewer capacity and ground water quality from sewerage. Design centralized systems.	Current conditions both in qualitative, from the community perspective; and quantitative, through the 2015 Housing Element Action Program 9 and SB 244 Studies, have been documented. In addition, during the drought, the severe water shortage has further been analyzed and reported in the Community Plans.
Improve mobility and walkability	TCAG Bike and Pedestrian Plan 2011 and Regional Transportation Plan (limited information in both suggested documents listed.	Study existing conditions <i>(completed)</i>	Map existing conditions and identify areas of need (completed)	Design and seek funding for multimodal transportation (actively doing this. These plans have been submitted as part of the ATP process, and in creating a County Wide Complete	Known capacity and identification of new areas needs	Complete Streets Projects have been created for some of these communities. Communities have commented on the desired improvements and through

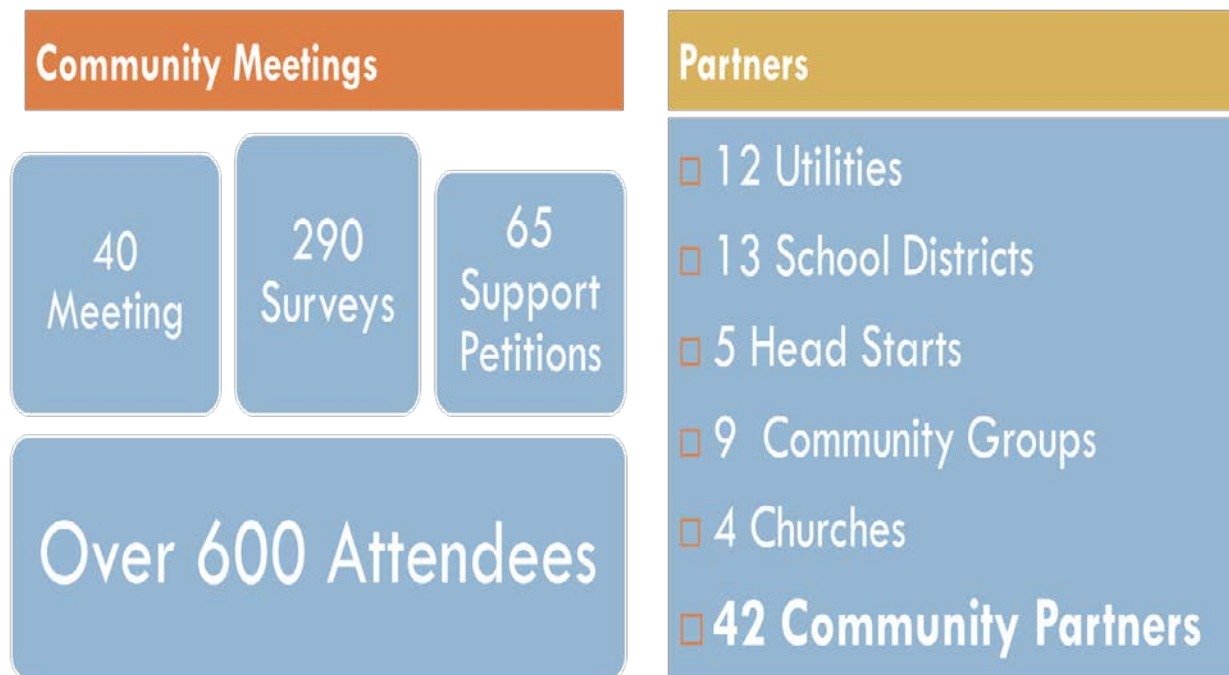
## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

	Created complete street plans for most of the communities instead).			Streets Program. TCAG has authorized the spending of \$5,000,000 for road improvements / complete streets projects in these communities.)		other TCAG funded complete streets planning studies have been able to create Programs for some of these communities. Allensworth and Earlimart just received \$1.2 million from the CTC for their complete streets programs.
Improve fire protection	County Fire Department Master Plan and OES	Number of existing fire structures <i>(completed)</i>	Map existing conditions and identify acceptable levels of fire protection. <i>(in process)</i>	Work with County Fire and seek funding for future facilities to increase level of protection	Known stations and suppression facilities identification	The location of fire facilities has been documented in each Community Plan and response times are identified.
<b>Notes:</b> This section should include limitations of data, assumptions, uncertainties, time lags in collection, method for tracking progress, and other necessary information about this indicator.						

**Table 4 - Summary of Indicator Measured**

Indicator Measured	Data Source (Include date compiled)	Indicator Baseline	grant-term milestones (1-3 years)	implementation milestones (3+ years)	Actual Outcome and/or change from the baseline	Indicator Measured
Land use and zoning language in existing plans	Tulare County Land Use Element 2010	Existing land use and zoning designations existing communities are already designated mixed use; we are going to designate the Legacy Communities mixed use as well.)	Analyze existing text and update designations to allow businesses to locate in communities	Update land use and zoning designations in community plans (currently	New allowed uses and by right, mixed use, and infill designations (the official plan designation allows infill policies to be applied to existing communities.)	Land use and zoning language in existing plans
Issuance of special or conditional use permits	Tulare County Permits Tracking Software 2012 – utilization of PALMS	Existing special or conditional use permits	Track economic interests that invest in communities	Track investment in communities based on planning text changes	Increased business interest and location in the unincorporated County reduce use permit requirement so tracking software not as vital for entitlements.	The County reduced the need for Use Permits in each one of these Communities through the Zoning Code.
<b>Notes:</b> This section should include limitations of data, assumptions, uncertainties, time lags in collection, method for tracking progress, and other necessary information about this indicator.						

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY



## OUTREACH

The grant's requirement for outreach were that it provide an update on how the Project has collaborated with and included stakeholders in its planning efforts. The grant requires that the report should include discussion of the following, as applicable:

- Specific actions taken to engage with members of the community in the development and the execution of the Project.
- How the Project has determined the needs of its most vulnerable residents, describing groups and organizations that assisted with and were included in the outreach.
- Identify the number and types of events held, the number of participants at each event, and the outcomes from these interactions.

To complete outreach, we partnered up with a consultant, Self Help Enterprises (SHE) and Leadership Counsel (LC). The outreach process was completed in three components: 1. Pre-meeting, 2. Facilitating of meeting / conducting the survey, and 3. Generating a report of the community outreach. SHE and LC invited residents, school districts, and schools, churches, local organization, and other local stakeholders to participate in public meetings, with the goal of receiving direct input from them to create a disadvantage communities' infrastructure and planning policy.

A quantitative (based on available physical data) and qualitative assessment (based on survey data) has been performed for each community within the Planning Area. We identified 25 communities to receive outreach within the area and have assessed surveys. The intention of the analysis survey was to integrate infrastructure analysis with the needs of our individual rural disadvantaged communities. (See Appendix A – Outreach Reports).

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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In our outreach efforts we have conducted 22 original community meetings, with one community requiring administering the surveys door to door, in the second round of feedback meetings there were 24 scheduled meetings. Additionally, in January of 2017, the County met with Tulare County LAFCO. Overall there have been over 313 attendees with 14 doors to door participants. We collected 290 overall surveys which is significant given in any community there are populations of less than 100 residents.

**Figure 3 - Community Survey**

<b>Tulare County SGC Sustainable Communities Strategy</b>	
Community Name: _____	
Date _____	
I am a: Community Resident Board Member, What Board? _____ School Administrator Stakeholder (Circle One)	
<b>Schools</b>	
Do you have a school(s) in your community? Yes/No	How many? _____
Name of school(s)? _____ _____	
If not, where is the nearest school? _____	
How many miles away? _____	
<b>Libraries</b>	
Is there a public Library in your community? Yes/No _____	
Name of library _____ _____	
If not, where is the nearest public library? _____	
How many miles away? _____	
Is your community library open enough hours? Yes/No	
<b>Housing</b>	
Do you rent or own your home? Rent/ Own	
What is the condition of the home you live in? Good/ Fair/ Poor	
Do you plan to buy a home in your community? Yes / No	

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### Parks

Is there a park(s) in your community? Yes/No      How many parks \_\_\_\_\_

If not, where is the nearest park? \_\_\_\_\_ How many miles away? \_\_\_\_\_

### Shopping Opportunities

Is there a grocery store(s) in your community? Yes/No      How many grocery stores? \_\_\_\_\_

If not, where is the nearest grocery store? \_\_\_\_\_

How many miles away? \_\_\_\_\_

Is there a supermarket in town? Yes/No      How many supermarkets? \_\_\_\_\_

If not, where is the nearest supermarket? \_\_\_\_\_

How many miles away? \_\_\_\_\_

Is there a corner store in town? Yes/No      How Many corner stores? \_\_\_\_\_

If not, where is the nearest corner store? \_\_\_\_\_

How many miles away? \_\_\_\_\_

### Gas Station

Is there a gas station in your community? Yes/No      How many gas stations? \_\_\_\_\_

If not, where is the nearest gas station? \_\_\_\_\_ How many miles away? \_\_\_\_\_

### Access to Medical Facilities

Is there a medical clinic in town? Yes/No      How many clinics? \_\_\_\_\_

What is the name of the clinic(S)? \_\_\_\_\_

Where is the nearest medical clinic? \_\_\_\_\_

How many miles away? \_\_\_\_\_

Is there a hospital in your community? Yes/No      How many hospitals? \_\_\_\_\_

If not, where is the nearest hospital? \_\_\_\_\_

How many miles away? \_\_\_\_\_

Is there a pharmacy in the area? If not, would you like to see one? \_\_\_\_\_

### Natural Gas

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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Does your home have natural gas? Yes/No

If not, what kind of fuel does your home have? \_\_\_\_\_

### **Internet Access**

Does your home have Internet access? Yes/No What company do you use? \_\_\_\_\_

How would you rate your internet connection? Good / Fair / Poor

### **Transportation**

Does your community have a bus stop(s)? Yes/ No If so, what is the name of the bus?  
\_\_\_\_\_

How many bus stop locations in town? \_\_\_\_\_ How often does it stop in your community? \_\_\_\_\_

Does the bus stop in your community frequently enough? Yes/ No

### **Access to Transportation**

Does your community have access to dial-a- ride? Yes/No Have you used this service? Yes/ No

Does your community have access to a taxi service? Yes/No

Have you used this service? Yes? No

What kind of transportation do you use to travel to work?

Walk /Bike / Drive / Bus/ Ride Share / Taxi

What kind of transportation do you use to take your kids to school?

Walk/ Bike / Drive / Bus/ Ride Share / Taxi

What kind of transportation do you use to shop?

Walk/ Bike / Drive / Bus/ Ride Share / Taxi

### **Walkability**

Are there sidewalks to walk on in your neighborhood? Yes / No

Are there sidewalks to walk on as your walk your kids to school? Yes / No

Are there sidewalks to walk on as you walk to your corner store? Yes / No

### **Fire & Safety**

Is there a fire station in your community? Yes/No

How many fire hydrants are in the community?



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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How is the pressure in the water in these hydrants if a fire was to occur?

Is there a police station in your community? Yes/No

Is there a community based officer in your community? \_\_\_\_\_

### **Water Quality & Quantity**

Does your water come from a water system or a private well? \_\_\_\_\_

Do you pay a water bill? \_\_\_\_\_ Who do you pay your water bill to? \_\_\_\_\_

### **Water system**

Do you have safe drinking water? Yes / No

Does your water have contaminants? Yes/No      If so, what contaminant(s) does it have?  
\_\_\_\_\_

Do you drink your tap water? Yes/No      Do you buy bottled water? Yes/No

How would you describe your water pressure? Strong/ Fair/ Poor

### **Private Well**

Do you have safe drinking water? Yes / No

Have you tested your well water for contamination? Yes/ No if so, when? \_\_\_\_\_

What contaminants did you test for? \_\_\_\_\_

What were the test results? \_\_\_\_\_

How often do you test your well water for contaminants? \_\_\_\_\_

Do you drink your tap water? Yes/No      Do you buy bottled water? Yes/No

How would you describe your water pressure? Strong/ Fair/ Poor

### **Waste Water**

Are you connected to a community sewer system? Yes/ No

Do you have your own septic tank? Yes/No. When is the last time it was pumped? \_\_\_\_\_

Have you had any problems with your septic tank? Yes/No, if so, please describe? \_\_\_\_\_

### **Storm Water Drainage**

Does your community have problems with storm water drainage? Yes/ No

If so, please describe? \_\_\_\_\_

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### Multimodal Opportunities

What language do you speak best? \_\_\_\_\_

What language do you read best? \_\_\_\_\_

Does your community have access to a Newspaper(s)? Yes/No if so, which newspaper(s)?  
\_\_\_\_\_ Is the newspaper available in your language? Yes/ No

How do you get your news? TV / Radio / Newspaper

Figure 4 - Community Meeting Flyer

Self Help  
County RMA invites you  
**Strategic**





Enterprises and Tulare  
to the  
**Growth Council**

**Community Meeting**  
**Thursday August 20, 2015 at 6:00p.m. at the**  
**Gomez Residence**  
**38660 Monson Drive**  
**Monson, CA 93618**

What improvements are needed in the community of Monson?  
The Strategic Growth Council (SGC) team invites you to give us input to create a plan for disadvantage communities infrastructure and planning.  
We want your thoughts on Sewer, Water, Water Quality & Quantity, Storm Water Drainage, Fire, Access to Transportation, Housing, Schools, Libraries, Parks, Access to Healthy Foods and Shopping Opportunities, Access to Medical Facilities, Internet Access and more!  
Please come join us  
For more information call: Abigail at 559-802-1659  
Or Kyria 559-624-7154



Self Help Enterprises y El condado de Tulare le invita a la  
Junta Comunitaria del Consejo de Crecimiento Estratégico  
**Jueves, 20 de agosto 2015 a las**  
**6:00 p.m. en**  
**La casa de la Familia Gomez**  
**38660 Monson Drive**  
**Monson, CA 93618**

¿Qué puede mejora la comunidad de Monson?  
El equipo del Consejo de Crecimiento Estratégico (SGC) le invita a darnos informacion para crear un plan para la infraestructura y planificación de las comunidades en desventaja de Monson.  
Queremos sus pensamientos sobre Alcantarillado, Agua, Calidad y Cantidad del Agua, Drenaje Pluvial Agua, Fuego, el acceso al transporte, vivienda, escuelas, bibliotecas, parques, acceso a alimentos y tiendas, acceso a servicios médicos, acceso a Internet y mucho más!

Si quiere mas informacion llame a: Abigail 559-802-1659 o Kyria 559-624-7154

## **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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The consultant was in charge of obtaining a facility, creating translated flyers in English and Spanish, and presenting a PowerPoint about the project and purpose of the study. The presentation was presented in English and Spanish to ensure all parties understood in their respective language. They also had to ensure the public was notified of the meeting. To achieve maximum attendance, the consultant sent these flyers (see adjacent figure) to the school in the community, did door to door outreach, and placed phone calls to the residents the night before the meeting as a courtesy reminder. The consultant could obtain residents phone numbers from local community advocates who have created small outreach groups within each community. The local community advocates were very helpful in notifying the residents. The consultants then administered the surveys to every member who attended the meetings. In all, over 42 official meetings, and 6 informal meetings and/or door to door surveys were conducted (50 in all), 35 were held in the evening to allow residents, who worked, the opportunity to attend.

We valued the stakeholder input, allowing the community to voice concerns, and address their concerns to area experts. A County representative or staff member attended the community meeting. The official data gathering meetings were concluded in February 2016, with a follow up of the meetings with our consultants to review the outreach summary. The 42 formal meetings were as follows.

1. Alpaugh September 3, 2015 (19 residents attended) – Returned for 2<sup>nd</sup> Meeting on January 11, 2017
2. Allensworth January 12, 2016 (12 residents attended) – Returned for 2<sup>nd</sup> Meeting on January 11, 2017
3. Delft Colony February 4, 2016 (33 residents attended) – Returned for 2<sup>nd</sup> Meeting on November 29, 2016.
4. Earlimart November 4, 2015 (28 residents attended)
5. East Orosi, September 8<sup>th</sup>, 2016 (10 residents attended) – Returned for 2<sup>nd</sup> Meeting on December 15, 2016
6. East Tulare Villa February 2, 2016 (5 residents attended)
7. Hypericum February 21, 2016 (8 residents attended)
8. Lindcove April 7, 2016 (door to door surveys)
9. London February 17, 2016 (22 residents attended) – Returned for 2<sup>nd</sup> Meeting on November 14, 2016
10. Matheny Tract January 9, 2016 (13 residents attended) – Returned for 2<sup>nd</sup> Meeting on January 9, 2017
11. Monson August 20, 2015 (11 residents attended) – Returned for 2<sup>nd</sup> Meeting on December 7, 2016
12. Richgrove January 14, 2016 (35 residents attended) – Returned for 2<sup>nd</sup> Meeting on November 21, 2016
13. Seville December 15, 2016 (4 attended) – Only meeting planned for Seville
14. Strathmore October 29, 2015 (7 residents attended) - Returned for 2<sup>nd</sup> Meeting on January 26, 2016

## **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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15. Sultana November 19, 2015 (20 residents attended) – Returned for 2nd Meeting on December 7, 2016. Community Plan Completed in 2015 – 6 meetings with School District
16. Teviston December 10, 2015 (30+ residents attended) – Returned for 2nd Meeting on December 9, 2016
17. Tonyville February 11, 2016 (5 residents attended)
18. Tooleville October 20, 2016 (10 residents attended) – Returned for 2<sup>nd</sup> Meeting on January 17, 2017
19. Waukena, February 20, 2016 (10 residents attended)
20. West Goshen, September 17, 2015 (21 residents attended) – Returned for 2<sup>nd</sup> Meeting on November 21, 2016
21. Woodville, October 8, 2015 (16 residents) – Returned for 2<sup>nd</sup> Meeting on January 18, 2017
22. Yettem, December 9, 2015 (door to door surveys) - Returned for 2<sup>nd</sup> Meeting on December 15, 2016

Other Communities within the Cal EnviroScreen Area were not specifically included in outreach, for the following reasons.

23. El Monte Mobile Home – Mobile home park owner would not let us on site
24. Jovista – No land owners would answer door to door surveys
25. Lort - No land owner would answer door to door surveys
26. Goshen (2 years of Outreach Meetings – Project Continuing but already completed EIR and Community Plan Admin Drafts)
27. Pixley (2 years of Outreach Meetings – Community Plan Approved in 2015)
28. Ducor (1 years of Outreach Meetings – Community Plan Approval in 2015)
29. Traver (1 years of Outreach Meetings – Community Plan Approval in 2015)

A typical report produced by Self Help Enterprises included the following components, as exemplified by the Earlimart Outreach Report below:

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## **Sustainable Communities Strategy Strategic Growth Council Outreach Report Earlimart**

Earlimart is a small, rural community located on the State Route 99, 7 miles north of Delano. According to the United States Census Bureau, the CDP has a total area of 2.1 square miles (5.4 km<sup>2</sup>), with a population of approximately 10,194. This community is considered disadvantaged. The most recent US Census Bureau American Community Survey (2010-2014) Five Year Estimate of the Median Household Income for the Earlimart CDP is \$23,458. Earlimart is listed in the top 10 percentile of Cal Enviroscreen 1.1 and top 20 percentile under Cal Enviroscreen 2.0.

### Community Outreach and Invitation Process

Self-Help Enterprises (SHE) staff conducted the following outreach efforts to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer.
- 2.) SHE staff distributed and posted flyers around town at local businesses, clinics and the post office. SHE talked with business owners about the project and invited them to attend the meeting.
- 3.) SHE staff distributed flyers at the local food pantry where over 250 residents receive food. At this event, SHE explained the project and the importance in participating.
- 4.) SHE partnered with the Earlimart Unified School District to advertise the meeting; flyers were sent home with each student. Meeting flyers were sent home from school with 2,600 students of the ESD.
- 5.) Meeting invitations were sent to the Earlimart Public Utility District and Earlimart School District Board members.
- 6.) During the door to door community outreach process, SHE staff explained the goals of the project and talked with residents about the community benefits of participating in the SGC community meeting.

### Community Meeting

Self-Help Enterprises held a SGC community meeting in Earlimart on Wednesday, November 4, 2015 at 6:00 pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Veterans Memorial Building, 712 E. Washington Ave in Earlimart. This is an ideal meeting location because it is centrally located, and most residents know where the Memorial Building is. The purpose of the meeting was to discuss Earlimart improvement needs, gather community data, and report the findings of the meeting in a final report. Over 30 people attended the meeting and 28 community surveys were collected. Representatives from Proteus, United Health

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Center, Earlimart Town Council and a local group, Rural Committee for Community Advancement were present at this community meeting.



### Community Survey

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asked about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Earlimart SGC community input meeting, residents concluded that the following is a list of Earlimart priority improvement needs:

### Priority Improvements

1. Community Safety/Police Patrol
2. Sidewalks
3. Drainage
4. Street Lights
5. Affordable Housing
6. Supermarket
7. Speed Bumps
8. Road Conditions/Street Repairs
9. Sewage Capacity

### Sheriff Presence

The Tulare County Sheriff's Department (TCSD) is responsible for patrolling the community of Earlimart. Residents report that the Sheriff's Department response time is unacceptable and that there is little Sheriff patrol within the community. Residents are worried about the rate of crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the



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local crime rate and makes criminals feel unstoppable in Earlimart. Over the last few years, Earlimart has seen a rise in gang violence, and increased police presence would help with that problem.

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### Safety

Residents are worried for their safety and feel that the Sheriff's department should be doing more to alleviate the recent rise in gang related crimes. The community is very concerned with the rise in shootings and drug related violence over the last couple of years. The recent crime activity is driving some residents away and has caused some people to move to neighboring communities such as Delano and Tulare.

### Storm Water Drainage

Earlimart residents report that the community does not have adequate storm water drainage. Large puddles form and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season making it difficult to drive and walk on certain roads. After it rains the stagnant water attracts insects and mosquitos causing additional problems. The community needs curbs and gutters to help with this problem.

### Road Improvements

Road conditions are a big concern to Earlimart residents. It is reported that road conditions are poor, and some roads need work, streets have potholes, cracks and bumps. Drivers feel unsafe and are worried about damage to their vehicles while driving on some Earlimart roads. According to residents, Earlimart roads are not regularly maintained. The following roads are a priority to the residents:

- Washington Ave.
- State Street
- Front Road
- Ave 148

### Sidewalks

Many streets in Earlimart need sidewalks, some residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Sometimes the traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

### Street Lights

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's department seldom patrols the streets of Earlimart. Residents are especially concerned about how this affects the safety of local children. Residents state that some streets do not have light poles. Other streets have light poles but the light bulb is out, this issue should be easy to solve by simply replacing the bulb. Residents have

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asked their County Supervisor to address this issue and have been told that Tulare County does not know which poles belong to them and which belong to Southern California Edison. Residents report being told that there is a cost to them to install a light pole on their street. Residents have been asking the County of Tulare for help with solving this problem for many years and that this issue has not been treated as a priority.

### **Neighborhood Park**

In the community of Earlimart, there is only one small park space for the community to use. Survey results show that most residents use the local schools green space or the street to be physically active. Residents are concerned about the health and safety of their children, they worry that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. The County of Tulare received a grant to build a community park in Earlimart, the park project is a partnership with the Earlimart School District. The completion of the Earlimart Park is a priority to the community and they want to see this project completed in a timely manner as scheduled and as promised. Currently the nearest community park is located in the neighboring community of Pixley, 6 miles away. It is not practical for residents to travel that far to use the park.

### **Local Grocery Store**

Residents expressed the need for an affordable grocery store in town. Currently, Earlimart has two markets that sell some produce, meat and dairy, however residents are not satisfied with the quality and freshness of the food and food prices are much higher in town than in the neighboring community of Delano. Over the last ten years Earlimart has grown to a population of over 10,000, with a population this size and because Earlimart is located directly off of SR 99, residents feel a large grocery store will be sustained. Earlimart residents regularly travel to Delano (7 miles) or Tulare (25 miles) to buy their groceries. This travel would be eliminated if there was a large grocery store in town.

### **Affordable Housing**

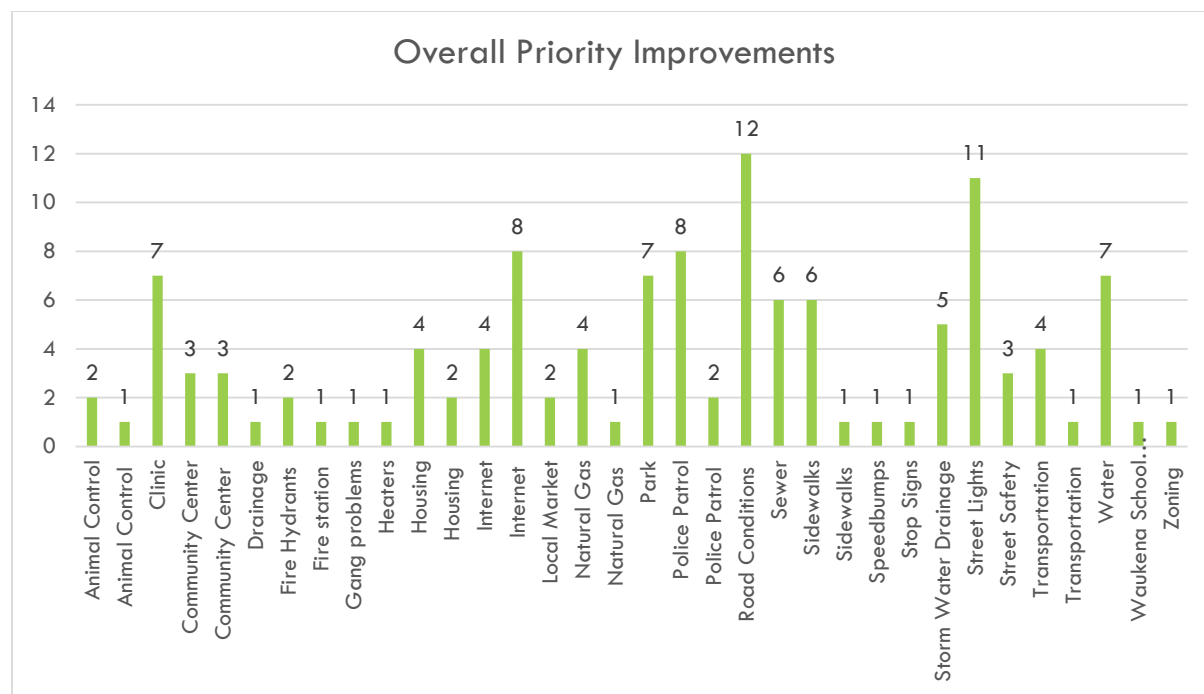
Earlimart residents want new affordable housing to be built in town. Many people expressed that they would buy a new home in Earlimart if it were available to them. For several years, the Earlimart Public Utility District has not allowed new sewer connections to the sewer system due to capacity issues. This has put limitations on new home development in Earlimart. Residents would like to see growth in town and want to work with the Earlimart PUD to allow for new sewer connections for new homes. Residents feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Delano and Tulare.

### **Public Services**

Sheriff –Earlimart is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Earlimart and the nearest Sheriff substation is located in Pixley, 6 miles away. Fire – There is a Tulare County Fire station located in Earlimart. Schools –Earlimart School District (K-8), has 4 schools in Earlimart. Students attend high school in Delano CA. Libraries

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– Earlimart Public Library located in Earlimart. Parks – Earlimart Rotary Park, small pocket park located in town.



The above example of priorities improvements information was gathered from the different community meetings in Earlimart (with larger attendance), the results indicate the highest priorities are as follows:

- 1) The highest priority was the maintenance of roads, especially after the flooding in 2017; and after the three years of dry weather, it caused road failures namely potholes throughout the County.
- 2) Street lighting was the second highest priority. As was discussed at the meetings, street lighting in the County are located for traffic safety reasons and locating lighting because the County pays for, it in most instances, requires a warrant study. However, land owners have the option of paying for it themselves. In some cases, the bulbs had also burned out or people had broken the bulbs (so it was a matter of maintenance).
- 3) Thirdly, people wanted better internet service and had concerns about the levels of police patrols and response times. The issues of having internet service in the schools because of the lack of WiFi has caused students to stay late at school, or to do their homework outside the school even at night, or the schools place their school busses in the community because

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they can create “hot spots” around the busses. The response times for the sheriff’s department is of concern to the citizens at every one of the meetings.

- 4) Local parks are the next highest priority. There are local parks in some of these communities, but citizens do not feel safe in those locations, and the school also double up for park recreational purposes, but they are not always open. Therefore, residents end up having to drive to other community parks.
- 5) Clinics and water were the next priority, especially through the drought years, but water quality is an issue that is made worse by limited access to water, and because of the distances people have to commute for medical attention, localized clinics were in the forefront of the discussions depending on the community. Sewer, sidewalks and housing were the next highest priorities.

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## Community Priorities by community are as follows:

### Alpaugh

1. Road Conditions & Street Safety
2. Community Safety
3. Medical Care Access
4. Community Sewer System
5. Local Market
6. Internet Access
7. Community Resource Center
8. Loose Dogs
9. Natural Gas
10. County Park Management

### Allensworth

1. Water Issues
2. Road Conditions
3. Natural Gas
4. Police Patrol
5. Home Repair Assistance
6. Heaters
7. Sewer
8. Internet Access

### Delft Colony

1. Community Center
2. Side Walks
3. Water Quality
4. Street Lights
5. Road Conditions
6. Animal Control
7. Internet Access

### Earlimart

10. Community Safety/Police Patrol
11. Sidewalks
12. Drainage
13. Street Lights
14. Affordable Housing
15. Supermarket
16. Speed Bumps
17. Road Conditions & Street Repairs
18. Sewage Capacity

### East Orosi

1. Road Conditions
2. Sidewalks
3. Curbs & Gutters
4. Water Issues
5. Street Lighting
6. Gang Problems

### Hypericum

1. Water Quantity
2. Water Quality
3. Sewer
4. Natural Gas
5. Storm Water Drainage
6. Road Conditions
7. Street Lights
8. Internet
9. Community Park
10. Transportation

### London

1. Road Conditions & Street Safety
2. Community Park
3. Internet Access
4. Sidewalks

5. Storm Water Drainage
6. Affordable Housing

### Monson

1. Water Issues
2. Storm Water Drainage & Street Flooding
3. Street Lights
4. Bus Transportation
5. Community Park
6. Clinic
7. Road Improvements
8. Zoning Changes
9. Community Based Officer/Sheriff patrol
10. Fire Hydrant

### Richgrove

1. Housing
2. Clinic
3. Road Conditions
4. Internet
5. Police Patrol
6. Water Issues
7. Storm Water Drainage
8. Street Lights
9. Community Center

### Strathmore

1. Street Safety
2. Clinic
3. Community Park
4. Internet Access
5. Police Patrol



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### Teviston

1. Housing Rehab
2. Sewer
3. Road Safety
4. Transportation
5. Renovate the Community Center
6. New Housing

### Seville

1. Natural Gas
2. Road Conditions
3. Internet
4. Street Lights
5. Sidewalks
6. Park
7. Fire Hydrants

### Waukena

1. Waukena School Improvements
2. School Traffic Safety
3. Road Conditions

### Tooleville

1. Stop Signs
2. Street lights
3. Clinic
4. Internet
5. Police Patrol

### West Goshen

1. Natural Gas
2. Sewer
3. Transportation/Bus Stops
4. Street Conditions
5. Street Lights
6. Internet
7. Community Center

### Woodville

1. Street Lighting
2. Internet Access
3. Side Walks
4. Public Transportation
5. Police Station
6. Fire Station
7. Clinic
8. Police Patrol
9. Community Resource Center
10. Animal Control

## **ASSESSMENT REPORT OF INFRASTRUCTURE CAPACITY AND AVAILABILITY (NEEDS AND CONSTRAINTS REPORT)**

The County through Housing Element Action Program 9 and the SB 244 Report studied infrastructure within each Community. This grant allowed the County to incorporate those studies into these plans and further analyze the infrastructure deficiencies within each plan area. The individual infrastructure studies have been incorporated into each plan. (See Appendix B-D for all the Community Plans).

By way of background, the SGC funded Highway 99 Corridor Plan (2015) showed the high need and low capacity for any future development within the Counties' largest unincorporated communities along the State Route 99 Corridor. (See Highway 99 Corridor Plan Page 6-20). This study further expanded this uncertainty about the ability of the Community Service Districts (CSD's), private purveyors, and Tulare County, in a few instances, to provide long term delivery of both water quality and quantity without continued expansion in the number of wells and well depth or consolidation with other CSD's.

Without conservation measures, the demand along the Highway 99 Corridor alone could blossom in to a need for another 1,115 additional water connections within the General Plan horizon of 2030, which includes some of the plans within this Planning Area, including Teviston and Earlimart. Concurrently and cumulatively, there is a need for 340 connections now along the 99 Corridor, with between 20 to 314 connections available, depending on location. Therefore, the connections will be sucked up quickly, if development proposals move forward.

In additions, new water regulations (State Groundwater Management Act) and sewer regulations (Local Area Management Program), as discussed below, when implemented in Tulare County may cause further constraints on development to make new projects more infeasible.

But there are no immediate plans for any of these water purveyors to accommodate future growth, unless the developers "pay as they go" for their own improvements. The cost is the full price for the improvements necessary, minimally in the hundreds of thousands of dollars (i.e. Earlimart's Public Utility District cost to connect to sewer). And if there is theoretically adequate water for now, in some of these communities (i.e. Earlimart 202 available water connections); the districts will only sparingly issue will serve letters. Plus, there is also constraints on sewage and drainage that is also considered in this study.

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## 3.1 OVERALL EXISTING INFRASTRUCTURE CONDITIONS

The Infrastructure Development Priorities Matrix (IDPM – See Table 6) indicates that the infrastructure within these communities is severely deficient based on Housing Element Action Program 9 and the Community Surveys (See Table 5). There is no community that is not deficient in at least one of the utilities. Of the major utilities (water, sewer, and drainage) only the Earlimart Urban Development Boundary has all these systems. It also has the largest population (+/- 10,000 people) of all the communities in the planning area, but is the 3rd largest community by square mile (2.1 sq. miles) in the Study Area, the Hamlets of Teviston and Allensworth are larger. Earlimart has the most sidewalks and street lights, but still has drainage problems (storm drainage running into homes) even with a drainage system. Moreover, the Earlimart Public Utility District (EPUD) states that they have multiple sewer connections (theoretically 600) in the Action Program 9 Report, but as of the writing of the Earlimart Community Plan (2017) the EPUD engineer states they have no capacity and are requesting large scale projects pay hundreds of thousands of dollars for hook up for new sewer lines, as they lack overall sewer pipe / facility capacity. And last year they would not provide a “will serve” letters to the newly designed Earlimart Park initially for fears of not having overall adequate water supply due to drought, while showing a modest amount of water hook ups. So, these conditions can change over the course of weeks.

Another issue is citizens may or may not have a willingness to pay for additional infrastructure (even with the drought), but we don’t know. The citizens at the meetings had concerns over capacity, but some of their concerns were also that they were paying too much for water. So, it’s hard for the utility providers to increase provisions for new water or waste water without knowing for certain if there is a willingness to pay for improved services. This leaves the purveyors to have to go forward looking for grants from the State and Federal Agencies that are not tied to the rate payers. Moreover, the County and Department of Water Resources had a strong bottled water program and water tank program from 2013-2016 that made the drought conditions more tolerable for the individual home owners, and with some exceptions for tenants to allow for utilization of these programs.

Throughout the outreach program for this study, it became apparent that the citizens’ concerns regarding infrastructure in relationship to the realities of the actual status of infrastructure were complicated by the drought, and flooding, that occurred in 2013-2016 and 2017 respectively. The perceived lack of infrastructure, and the actual status of infrastructure, are in most instances inconsistent, but the outreach program did shed light on the sentiments, and more so the expectations by the citizens of Tulare County, about the lack of services they are experiencing.

## 3.2 Water

As part of the historical Tulare Lake Basin, there is intense interest in the status of available water in the southern San Joaquin Valley by the State Water Resources Control Board.

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As an example, in August 2010, the Board of Supervisors approved an agreement with the California Department of Water Resources to accept \$2 million in funding for the Tulare Lake Basin Disadvantaged Community Water Study Project. The Project was completed in 2014, and the project web site can be found at: <http://www.tularecounty.ca.gov/cao/index.cfm/tulare-lake-basindisadvantaged-community-water-study/>.

The Study took over three years working with the Department of Water Resources and can be found at: [https://d3n8a8pro7vhmx.cloudfront.net/communitywatercenter/pages/43/attachments/original/1409014249/TLB\\_Final\\_Report\\_2014-Aug.pdf?1409014249](https://d3n8a8pro7vhmx.cloudfront.net/communitywatercenter/pages/43/attachments/original/1409014249/TLB_Final_Report_2014-Aug.pdf?1409014249). The Plan itself: “developed an integrated water quality and wastewater treatment program... as appropriated by Senate Bill SBX2 1 (California Water Code §83002(b)(3)(D)).

The objectives of the TLB Study are defined within the grant agreement as follows: “Develop a plan that provides rural, disadvantaged communities with a safe, clean and affordable potable water supply and effective and affordable wastewater treatment and disposal. The plan will include recommendations for planning, infrastructure, and other water management actions, as well as specific recommendations for regional drinking water facilities, regional wastewater treatment facilities, conjunctive use sites and groundwater recharge, groundwater for surface water exchanges, related infrastructure, project sustainability, and cost sharing mechanisms. Identify projects and programs that will create long-term reliability, while optimizing the ongoing operation.” (See the Tulare Lake Basin Disadvantaged Community Water Study - 2015 Housing Element, page 6-25).

### 3.2.1 The Status of Water in Tulare County

The US Clean Water Act has been delegated to California, and it is implemented through the Porter-Cologne Water Quality Control Act. The Planning Area lies in the three water sheds of the Kings River, Kaweah River, and Tule River that are regulated under this Act.

- The Kaweah and Tule River head water dam/flows are regulated by the Army Corp of Engineers within Tulare County.
- Water quality is under the Central Valley Regional Water Quality Control Board (anti-degradation policy).
- Delegation has been given to Tulare County for onsite storm water control (Small Community MS-4 Permit).
- Flood control is mostly by the County, with the major waterways under the jurisdiction of the Department of Water Resources (DWR), and Central Valley Flood Control District.
- Community Wells are overseen by the Department of Water Resources.
- Community Sewer systems are overseen by the Regional Water Quality Control Board.

The SGC Highway 99 Corridor Study (roughly covering one third of the communities in the Planning Area) showed there is an inability to meet current water quality and quantity demands for the projected 2030 General Plan Annual Growth Rate (1.3%). This would require an additional 1,115,000 gallons of water a year or 294 connections within the Highway 99 Corridor study area alone.

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Outside of the limited water infrastructure within the Communities, the State Groundwater Management Act (SGMA) may create further limitations, as new sources of water compete with other interests, and conservation measures on the water providers disallow serving new development. The County is implementing SGMA at many levels and is thus far in compliance with SGMA's requirements. SGMA will ultimately require the following,

SGMA (SB1168/1319, AB 3030/1938):

“The Department of Water Resources (DWR) has developed a Strategic Plan for its Sustainable Groundwater Management (SGM) Program. DWR's SGM Program will implement the new and expanded responsibilities identified in the 2014 Sustainable Groundwater Management Act (SGMA). Some of these expanded responsibilities include: (1) developing regulations to revise groundwater basin boundaries; (2) adopting regulations for evaluating and implementing Groundwater Sustainability Plans (GSPs) and coordination agreements; (3) identifying basins subject to critical conditions of overdraft; (4) identifying water available for groundwater replenishment; and (5) publishing best management practices for the sustainable management of groundwater” (Department of Water Resources Web Page (2016)).

Under SGMA, Groundwater Sustainability Agencies (GSA) have formed sub-basins throughout the Kings River and Tulare Basins with Groundwater Sustainability Plans (GSP). (See Figure 5 – Proposed Kings GSP). The northern area of Tulare County within the Study Area is being included in the Kings River Groundwater Sub-basin. Most of the communities are located in the Kaweah Sub-basin, but also includes areas of the Tule, Alpaugh, and Tulare Sub-basins. Tulare County also formed a GSP to cover any areas not included in the other GSP's.



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Moreover, irrigators and water districts continue to overdraft the Basin's Aquifers static condition (equalizing inflow and outflow) at higher rates, averaging 3.12 Acre Feet/ Acre through 2014, (a 44% overdraft usage above the 2010 Tulare Basin Study "balanced" yield of 1.77-acre foot of water / acre foot). The drought condition had worsened to the point that no surface water has been allotted from 2013, until 2015 (surface water was provided in 2016 and has been heavily allocated in 2017). The Tule GSP has indicated "sustainable yields" will be around 1-acre foot of water per year per acre. The service districts and County fulfilled the first requirement of SMGA in creating the GSP's boundaries, and the next steps are to create the water budgets in order to determine what a "sustainable water yield" will be under the new legislation. The more localized solution to reduction in overcharging the aquifers is, besides water conservation, for expanded or new recharge basins and new surface water sources. Without surface flows, immediate dam projects, or recharging / recycling water, creating new water bodies in a drought situation is a progressive solution, but yields no immediate result within these disadvantaged communities.

### 3.3 Waste Water

The largest problem with waste water in Tulare County is that historically the septic tanks were built too close to the domestic wells, and the wells were built shallow (because ground water used to be found at near the surface). These old wells are also aged and lack the requisite seals or depths of those required today. Since the septic systems are aged and failing, the domestic wells are then being compromised and waste water is flowing into domestic water. The citizens within these communities do not have sufficient funds to fix these problems.

In the communities that have waste water infrastructure, there are limitations on available capacity, as East Orosi, Richgrove, and Seville, have no capacity. Earlimart has deficiencies in their system (stating they have no capacity) while the County's Action Program 9 shows a potential 600 additional connections reported. London, Tonyville, Delft Colony and Yettem have sewage capacity, but only Yettem has sufficient water capacity for its growth projection. The cities of Tulare and Exeter have thus far found waste water connection to the Matheny Tract and Tonyville to be infeasible, but will and do supply water to them.

#### New Septic Regulations:

On June 19, 2012, the SWRCB adopted Resolution No. 2012-0032, adopting the Water Quality Control Policy for Siting, Design, Operation and Maintenance of Onsite Wastewater Treatment Systems (OWTS Policy – See: [https://www.waterboards.ca.gov/board\\_decisions/adopted\\_orders/resolutions/2012/rs2012\\_0032.pdf](https://www.waterboards.ca.gov/board_decisions/adopted_orders/resolutions/2012/rs2012_0032.pdf)).

The new regulations will require that "in areas with no approved Local Agency Management Plan, new and replacement OWTS that conform to the requirements of Tier 1 will remain in Tier 1 as long as they continue to meet those requirements. A new or replacement OWTS will temporarily move from Tier 1 to Tier 4 if it is determined that corrective action is needed. The new or replacement

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OWTS will return to Tier 1 once the corrective action is completed. A new or replacement OWTS will move from Tier 1 to Tier 3 if it is adjacent to an impaired water body, or is covered by a TMDL implementation plan.

Section 7.8. The average density for any subdivision of property made by Tentative Approval pursuant to the Subdivision Map Act occurring after the effective date of this Policy and implemented under Tier 1 shall not exceed the allowable density values in Table 1 for a single-family dwelling unit, or its equivalent, for those units that rely on OWTS.

Tier 1 – Low Risk New or Replacement OWTS New or replacement OWTS meet low risk siting and design requirements as specified in Tier 1, where there is not an approved Local Agency Management Program per Tier 2...” The following is a list of allowed densities per acre based on rainfall under Table 1: Allowable Average Densities per Subdivision under Tier 1 in the County OWTS report.

Average Annual Rainfall (in/yr.) / Allowable Density (acres/single family dwelling unit)

0 - 15	2.5
>15 - 20	2 – Tulare County Valley (where these study area communities are located).
>20 - 25	1.5 – Tulare County Foothills
>25 – 35	1 – Tulare County Mountains
>35 – 40	0.75
>40	0.5

Section 8.0 – 8.2.5., of the LAMP Regulation contains *Minimum OWTS Design and Construction Standards* including percolation rate requirements. The replacement of older septic tanks will not be able to rationally meet the 2.0 acre / per OWTS standards; nor will the affordability of the replacement systems that most residents will be utilizing meet these standards. For new projects, this may be one of the requirements that makes development infeasible.

Therefore, the County submitted its Draft Local Agency Management Program (LAMP) to the RWQCB on November 8, 2016. This report is still in Draft version. The number of developed parcels with septic systems are conservatively estimated around 4,400 known OWTS. The requirement for future development will be based on a sliding scale from .5 to 2.5 acres related to rainfall and warranted on a case by case basis, with hopefully the typical minimum acreage being 1 acre in Tulare County, overall. For the most part, it will require 2-acre minimums for non-engineered septic systems in the Planning Areas in the report that are on the west side of Tulare County. (See Draft Tulare County LAMP Section 5 Appendix A Supporting Rational for Tulare County OWTS Siting and Design Criteria).

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### **2.4 Storm Drainage / Flood Control**

Every Community in the Planning Area is subject to roadway flooding and only Earlimart and Matheny Tract have regular maintained and drainage systems that attach drainage inlets to detention facilities. Therefore, there is a need in 20 of the 22 communities for drainage systems and limited capacity in even the two communities that have drainage systems. The existing drainage systems were not sufficient in 2010 to keep the waters from flooding into individuals' houses, despite the system being in place. And the surveys indicate that flooding in Earlimart is still present through the rains in 2017. Flooding was a part of the discussion in every community, and there were stories of flooding throughout last year. It was listed as the second largest concern in several communities, but was the chief concern in only two communities. The solution for groundwater recharge is to collect flood water in detention basins around these communities and several irrigation districts are moving forward with plans to detain water around these communities.

### **2.5 Electricity/ Gas**

Electrical services are provided in most of the communities, but in communities such as West Goshen and Allensworth, power was raised as a concern for many of the citizens. In most of the Communities Natural Gas was not a problem, but in the unique situation of West Goshen and in some places in Tooleville, it was too expensive for people to run the gas line from the street to their house. Other communities such as Lindcove, Seville, and Hypericum use propane because no gas service exists. There is one proposal occurring in Allensworth, which does not have the ability to connect to gas, to bring the gas line along the southern border into the Community and PG&E and SCE are both proposing options to connect gas and power to the community.

### **2.7 Communications / Internet**

Limited internet availability is common place in the communities, and the school districts have the largest issues in providing wifi service to students who are required to use the internet to do their homework. Students must stay late or access the school even at night to use the wifi located at the schools, or some districts allow the bus to locate within the community after school in order for students to access wifi. The County has streamlined the review process and is approving use permits (28 use permits) for AT&T in order for them to receive a federal grant to build increased internet capacity in remote disadvantaged community areas including many of the communities within the Planning Area.

## **DIAGRAM OF EXISTING INFRASTRUCTURE ANALYSIS**

It states in the Tulare County Housing Element (2015), Action Program 9 that a “major constraint to development of affordable housing throughout the County is the lack of sufficient infrastructure and basic municipal services. The County continues to identify housing related infrastructure needs, such as; water, sewer, natural gas or streetlights, using community needs assessments, housing condition surveys, public comments at community meetings, redevelopment implementation plans and amendments [when redevelopment existed], community plans and other relevant information from HHSA Environmental Health Services, Regional Water Quality Control Board, public utility districts, community services districts and other agencies” (Action Program 9, page 154 of the Housing Element).

Therefore, these Tulare County Communities’ infrastructure deficiencies are well documented in the Housing Element and the SB 244 Study. (See figure 5). Hence, the needs are far greater than the actual capacity to serve infrastructure today, but substantially more so over the long term.

The solutions are not inexpensive. Currently, development projects are having to find their own solutions to water, sewer and drainage, which include working around the standard infrastructure requirements, variances or scalping plants metering sewer flow, or installing additional wells or septic within the boundaries of a special district, which can still cost greater than half a million dollars (i.e. the proposed Earlimart High School’s well and septic facilities cost \$500,000 versus the EPUD hook up costs to sewer and water services being \$750,000). Because these communities do not have their own drainage facilities, except in a few circumstances like Earlimart, the County requires all drainage to be provided on site for large projects. Drainage in other communities generally collects in the streets along the shoulders.

The Housing Element Action Program 9 calls for the creation of an Infrastructure Development Priorities Matrix (IDPM). (See Figure 7). Preliminarily this was done in Figure 6 to show the deficiencies. The IDPM matrix details the aggregation of information and data in two phases (1<sup>st</sup> Phase within the next 5 years, and Phase 2 at some later point). This Section of Chapter One, aggregates the infrastructure information from various sources and has resulted in a matrix, which is a draft IDPM in fulfillment of this Housing Element requirement. The difference between the phases is whether infrastructure improvements are required for the general health of the citizens, versus Phase 2 improvements are what is required for additional future development. The criteria to establish the Phasing are as stated in the below:

Criteria 1: Public Health:

- C1.1 Failing water or well systems or sewer or septic systems
- C1.2 Known Flooding hazards, dangerous (condition or criminal activity) streets
- C1.3 Public purveyors vs. Private purveyor

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## Criteria 2: Economic Development:

- C2.1 Increased construction cost reducing development opportunities
- C2.2 Have most infrastructure available but lack a major infrastructure component
- C2.3 Request for assistance made to County

## Criteria 3: Environmental Concerns:

- C3.1 Known Pollution or Subsidence of a water body or aquifer
- C3.2 Known Impacts to other environmental resources
- C3.3 Known Impact to larger commuting times / reduced air quality

**Figure 6 - Housing Element and SB 244**

								Community Infrastructure Needs (Draft)								
Disadvantaged Unincorporated Community (DUC)	Public Sewer Available	Public Water Available	Public Stormwater Available	Existing Streetlights	Existing Sidewalks	Existing ADA Curb Ramps	Existing Fire Hydrants	Community	Wastewater	Water	Stormwater	Streetlights	Sidewalks	ADA Curb Ramps	Fire Infrastructure	
								Alpaugh	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Badger	No	No	No	No	No	No	No	Cutler	Deficiencies	Deficiencies	Potential deficiencies	No deficiencies	Deficiencies	Deficiencies	No deficiencies	
Balance Rock	No	No	No	No	No	No	No	Ducor	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Calgro	No	No	No	Yes	No	No	No	Earlmarl	Deficiencies	Deficiencies	Potential deficiencies	No deficiencies	Deficiencies	Deficiencies	No deficiencies	
California Hot Springs, CDP	Yes	Yes	No	Yes	Yes	No	No	East Orsi	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Camp Nelson, CDP	No	Yes	No	No	No	No	No	Lemon Cove	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Cedar Slope, CDP	No	Yes	No	No	No	No	No	London	Deficiencies	Deficiencies	Deficiencies	No deficiencies	Deficiencies	Deficiencies	Deficiencies	
Citro	Yes	Yes	No	No	No	No	No	Orsi	Deficiencies	Deficiencies	Potential deficiencies	No deficiencies	Deficiencies	Deficiencies	No deficiencies	
Deer Creek Colony	Yes	Yes	No	No	No	No	Yes	Pixley	Deficiencies	Deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
East Porterville, CDP	No	No	Yes	Yes	Yes	Yes	Yes	Plainview	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
El Monte Mobile Home	No	Yes	No	Yes	No	No	No	Poplar-Cotton Center	Potential deficiencies	Potential deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Elderwood	No	No	No	No	No	No	No	Richgrove	Deficiencies	Deficiencies	Deficiencies	No deficiencies	No deficiencies	No deficiencies	No deficiencies	
Fairview	No	No	No	No	No	No	No	Springville	Deficiencies	Deficiencies	No deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Globe	No	Yes	No	No	No	No	No	Strathmore	Deficiencies	Deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Hartland, CDP	No	No	No	No	No	No	No	Sultana	Deficiencies	Deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Hawkins	No	Yes	No	No	No	No	No	Terra Bella	Deficiencies	Deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Higby	Yes	Yes	No	No	No	No	No	Three Rivers	Potential deficiencies	Deficiencies	No deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Hypericum	No	No	No	No	No	No	No	Tipton	Deficiencies	Deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Idlewild, CDP	No	No	No	No	No	No	No	Traver	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Johnsontale	No	Yes	No	Yes	No	No	No	Woodville	Deficiencies	Deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Jones Corner	No	No	No	Yes	Yes	No	Yes	Hamlet	Wastewater	Water	Stormwater	Streetlights	Sidewalks	ADA Curb Ramps	Fire Infrastructure	
Jovista	No	No	No	No	No	No	No	Allensworth	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Kennedy Meadows, CDP	No	No	No	No	No	No	No	Delft Colony	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	No deficiencies	
Lort	No	No	No	No	No	No	No	Lindrove	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Naranjo	No	Yes	Yes	Yes	No	No	No	Monson	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Paige	No	No	No	Yes	No	No	No	Seville	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Panorama Heights, CDP	No	No	No	No	No	No	No	Teviston	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Peral	No	No	No	No	No	No	No	Tonyville	Potential deficiencies	Potential deficiencies	Deficiencies	No deficiencies	Deficiencies	Deficiencies	Deficiencies	
Pine Flat, CDP	Yes	Yes	No	No	No	No	No	Waukena	Potential deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Ponca	No	No	No	Yes	Yes	Yes	Yes	West Soshen	Potential deficiencies	Potential deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	Deficiencies	
Ponderosa, CDP	No	Yes	No	No	No	No	No	53 Yettem	Potential deficiencies	Deficiencies	Deficiencies	No deficiencies	Deficiencies	Deficiencies	No deficiencies	
Posey, CDP	No	No	No	No	No	No	No									
Sandspur	Yes	Yes	Yes	No	No	No	No									
Silver City	No	No	No	No	No	No	No									
Sugarloaf Mountain Park, CDP	No	No	No	No	No	No	No									
Sugarloaf Park/Guernsey Mill, CDP	No	No	No	No	No	No	No									
Sugarloaf Village, CDP	No	No	No	No	No	No	No									
Taurus	No	No	No	No	No	No	No									
Tooleville, CDP	Yes	Yes	Yes	Yes	No	No	No									
Vance	Yes	Yes	No	Yes	Yes	Yes	No									
Venida	No	No	No	No	No	No	No									
West Venida	No	No	No	No	No	No	No									
Wilsonia	No	No	No	No	No	No	No									
Worth	No	No	No	No	No	No	No									
Yokohl	No	No	No	No	No	No	No									
Zante	No	No	No	No	No	No	No									

# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

Figure 7 - Infrastructure Development Priorities Matrix

Needs and Capacity of Planning Area Communities																												
				water				sewer				drainage			electricity			light		gas				roads				
	Sq. Miles	Phase 1	Phase 2	WS	WCA	FWD 1.3%/4	water improvement needed	SS	SCA	FSD1	sewer improvement needed	DCA Sumps /inlets	FDC	Drainage improvement needed	ES	ECA	EE Imp needs	#	LIN	GS	GCA	FGD	Gas Imp needed (assumed maintenance required)	# to repair	# of Side walk			
Community Plans																												
Alpaugh (APUD)	1		X	377	0	123	Well/Connect	septic only (+/- 377 tanks)				no drainage system			PGE	Yes	Yes		12	Yes	Yes	Yes	Yes	Yes	maintenance	14	9	
Earlimart (EPUD)	2.1	X		1485	203	483	New Well	1485	600	483	conveyance	108	0	35	capacity	no drainage system			PGE	Yes	Yes	10	Yes	Yes	Yes	maintenance	75	110
East Orosi (C/OPUD)	0.2	X		108	0	35	Consolidation	108 0 35 capacity				no drainage system			PGE	Yes	Yes		10	Yes	Yes	Yes	Yes	Yes	maintenance	5	0	
London (LCSD)	6	X		430	0	140	Well/Connect	430	645	140	none	no drainage system			PGE	Yes	Yes		25	Yes	Yes	Yes	Yes	Yes	maintenance	7	1	
Richgrove (RCSD)	0.5	X		523	0	170	Nit Treatment	523	0	170	capacity	no drainage system			SCE	Yes	Yes		27	No	Yes	Yes	Yes	Yes	maintenance	10	34	
Sultana (CSD/CPUD)	0.4	X		150	0	49	Maintenance	unavailable				no drainage system			PGE	Yes	Yes		13	Yes	Yes	Yes	Yes	Yes	maintenance	4	5	
HAMLET PLAN																												
Allensworth (CSD)	3.1	X		116	0	38	Ars Treatment	ind. or community septic				no drainage system			PGE	Yes	Yes		11	Yes	Yes	Yes	Yes	Yes	maintenance	2	0	
Delft Colony (TC)	0.07		X	112	0	36	Maintenance	112	60	36	none	no drainage system			Both	Yes	Yes		4	Yes	Yes	Yes	Yes	Yes	maintenance	4	1	
E Tulare Villa (TC)	0.5	X		Cal Water				unavailable - septic?				no drainage system			SCE	Yes	Yes		5	Yes	Yes	Yes	Yes	Yes	maintenance	9	1	
Lindcove	0.7		X	wells (+/- 140 wells)				ind. or community septic				no drainage system			PGE	Yes	Yes		0	Yes	None	None	Yes	Service		2	0	
Monson	0.5	X		37 wells (1g. community well installed by County management by Sultana CSD) - nitrate treatment				ind. or community septic				no drainage system			PGE	Yes	Yes		2	Yes	Yes	Yes	Yes	maintenance	5	0		
Seville (TC)	0.6	X		114	negative capacity (plans for lg. well) Yettem connect FWD 37 connects			99	4	32	capacity	no drainage system			PGE	Yes	Yes		3	Yes	None	None	Yes	Service		9	0	
Teviston (CSD)	2.2	X		125	20	41	Consolidation	septic only (+/- 125 tanks)				no drainage system			SCE	Yes	Yes		13	Yes	Yes	Yes	Yes	Yes	maintenance	23	0	
Tonyville (LSID)/TC	0.5	X		LSID (+/- 79 connections FWD 9)				79	91	26	none	no drainage system			SCE	Yes	Yes		7	Yes	Yes	Yes	Yes	Yes	maintenance	2	0	
West Goshen (Cal)	1.2	X		80	70	26	long term pipe	ind. or community septic				no drainage system			PGE	Yes	Yes		0	Yes	Some	Yes	Yes	Yes	Can't afford	3	0	
Yettem	0.2	X		69	527	Seville Connect-FWD?	69	124	22	none	no drainage system			PGE	Yes	Yes		3	Yes	Yes	Yes	Yes	Yes	Yes	maintenance	2	0	
Waukena TC	0.9		X	TC (+/- 45 connections)				ind. or community septic				no drainage system			PGE	Yes	Yes		1	Yes	Yes	Yes	Yes	Yes	maintenance	3	3	
LEGACY PLAN																												
El Monte Mobile Home (MHP)	0.008		X	unavailable				Private water purveyor	community septic only				private drainage system			PGE				2	Yes	Unavailable			private		0	
Hypericum	0.004	X		Wells				septic	no drainage system				SCE	Yes	Yes		0	Yes	None		Service			2		0		
Jovista	0.01	X		Wells				ind. or community septic	no drainage system				SCE				0	Yes	Unavailable - Service			all			0			
Lort	0.25	X		Wells				ind. or community septic	no drainage system				SCE				0	Yes	Unavailable - Service			all			0			
Matheny Tract (PMWC)	0.04	X		Has City of Tulare Service				septic only (+/- 322 tanks) City Pipe adjacent to community				10	0	limited System	SCE	Yes	Yes		5	Yes	Yes	Yes	Yes	maintenance	6		0	
Tooleville (Water Co.)/ TC	0.06	X		TWC (+/- 76 connections) - nit treatment - (+/- 6 ind. Wells)				TC (+/- 76 connections)				deficient public system			SCE	Yes	Yes		1	Yes	Yes	Yes	Yes	maintenance	3		0	
Legend																												
W - Water, S - SEWER, D- Storm Drainage, E- Electricity, G-Gas, CSD-Community Services District, PUD-Public Utility District, C/OPUDCutler-Orosi Public Utility District, FWD - Future Water Demand, LSID-Lindsay-Cal-Calwater, PMWC-Pratt Mutual Water System, MHP-El Monte Mobile Home Park, TC - Tulare County, TWC - Tooleville Water Co.)																												
SS	Sewer Supply							SS	SCA	FSD1	SIN																	
SCA	Sewer Capacity Available																											
FSD1	Future (Sewer) Demand = 1.3% growth rate/ 4 persons per/unit																											
SIN	Sewer Improvements Needed																											
TC Tulare County water systems have been taken over by the Dept. of Water Resources. TC Sewer Systems are still maintained by Tulare County																												



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### 3.1 TULARE COUNTY STRATEGY FOR IMPROVING INFRASTRUCTURE

In rural areas, elimination of all barriers to economic development is the foundation for growth. Reducing these barriers includes implementing the strategy of pursuing, approving and implementing the following opportunities and planning measures.

- **Grants:** Tulare County has submitted for several grants in the hamlets for Active Transportation (ATP) Grant (Tonyville, Seville, and other hamlets). Grants have been also submitted for water projects throughout the County (i.e. Seville and Yettem).
- **Complete Streets:** Other improvements could be realized through implementation of the Complete Streets, as they are added to the ATP grants program.
- **Infrastructure:** In order for more development to occur, service levels for water are to be expanded. Grant funding is needed to increase service levels.
- **Zoning District Changes:** As part of this Implementation Program for the Disadvantaged Communities, there are a variety of changes to existing zoning districts including the “mixed use” overlay zone.
- **Use Permits:** There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In correlation with grants and in order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.
- **Education:** Tulare County has five satellite campuses for four-year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.
- **Health Care:** Health care is important for economic development as businesses need healthy employees.

#### 4.2.2 Description of Grant Funding Sources

- **Caltrans Active Transportation Program (ATP):** On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation. The projects associated with the Completes Streets Program for some of the Hamlets will be suggested at the next available round of ATP funding.
- **Tulare County Measure R:** On November 7, 2006, the voters of Tulare County approved Measure R, imposing a 1/2 cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years (i.e., Year 2036). The



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transportation measure will generate slightly more than \$652 million over 30 years to assist Tulare County in meeting its transportation needs.

- Local Projects (35% of Measure R Funding: Measure R Expenditure Plan allocated 35% of revenues to local programs. Each city and the county will receive funding based on a formula using population, maintained miles, and vehicles miles traveled. The funding will help cities and the county to meet scheduled maintenance needs and to rehabilitate their aging transportation systems.
- Regional Projects (50% of Measure R Funding: The Regional Projects Program comprises 50% of Measure R and includes specific funding for: interchange improvements, regional bridges, regional railroad crossings, regional signals, regional widening projects, and signal synchronization projects. These projects provide for the movement of goods, services, and people throughout Tulare County. Major highlights of this program include the funding of regional projects throughout the county.
- Bike /Transit /Environmental Projects (14% of Measure R Funding: The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.
- San Joaquin Valley Air Pollution Control District (SJVAPCD or District) Bike Path Grants: The District has a grants program for the construction of bicycle infrastructure projects, including Class I (Bicycle Path Construction) or Class II (Bicycle Lane Striping) projects. These grants provide funding to assist with the development or expansion of a comprehensive bicycle-transportation network.
- Strategic Growth Council Grants (SGC) - Affordable Housing - Sustainable Communities: The SGC will allocate 50% of its Cap and Trade funding toward disadvantaged communities and 50% for affordable housing. Projects will include: affordable housing that supports infill and compact development, transit capital and programs that support transit ridership, active transportation projects (infrastructure, and non-infrastructure), TOD projects, capital projects that implement complete streets, projects that reduce CHG emissions by reducing auto trips and VMT, acquisition of easements or other approaches to protect agricultural lands under threat of development, planning to support SCS (sustainable communities scope) implementation, including local plans, must be in draft or adopted SCS, subject to SGC guidelines.
- Congestion Mitigation Air Quality (CMAQ) (TCAG Funds): Congestion Mitigation Air Quality (CMAQ) funds are allocated through the Tulare County Association of Governments (TCAG). The CMAQ program funds transportation projects or programs that will contribute to improved air quality standards. Projects include: transportation activities, transportation control measures, public-private partnerships, alternative fuel programs, traffic flow

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improvements, transit, bicycle/pedestrian projects, rideshare activities, telecommuting, planning, experimental pilot projects, intermodal freight, and public outreach.

- Department of Transportation (DOT) - Transportation Investment Generating Economic Recovery (TIGER) Grant: TIGER is a multimodal, merit-based discretionary grant program that funds surface transportation capital projects, including transit and rail. Open to state, tribal, local agencies, and subdivisions.
- Community Development Block Grant (CDBG) - Business Assistance: The CDBG Economic Development grant provides assistance to local businesses and low-income microenterprise owners to create or preserve jobs for low-income workers in rural communities. Funding includes planning and evaluation studies related to any activity eligible for these allocations, business lending, and public infrastructure.
- Choice Neighborhoods Planning Grants: support the development of comprehensive neighborhood revitalization plans which focused on directing resources to address three core goals: Housing, People and Neighborhoods. To achieve these core goals, communities must develop and implement a comprehensive neighborhood revitalization strategy, or Transformation Plan. The Transformation Plan will become the guiding document for the revitalization of the public and/or assisted housing units while simultaneously directing the transformation of the surrounding neighborhood and positive outcomes for families.
- Neighborhoods Implementation Grants: support those communities that have undergone a comprehensive local planning process and are ready to implement their “Transformation Plan” to redevelop the neighborhood.
- California Department of Water Resources (DWR) Prop 50 (Contaminant Removal): Funds are available to disadvantage communities for developing UV or Ozone systems to disinfect drinking water or to set up pilot/demonstration sites.
- Drought Response Funding (State Water Regional Control Board (SWCB)): The Governor and Legislature have directed DWR to expedite the solicitation and award of \$200 million (of the \$472.5 million) in IRWM funding to support projects and programs that provide immediate regional drought preparedness, increase local water supply reliability and the delivery of safe drinking water, assist water suppliers and regions to implement conservation programs and measures that are not locally cost-effective, and/or reduce water quality conflicts or ecosystem conflicts created by the drought.
- DWR: Water-Energy Grant Program: The 2014 Water-Energy grant supports the implementation of residential, commercial, and institutional water efficiency programs or projects that reduce Green House Gas Emissions and also reduce water and energy use. Funding will go toward urban water management, groundwater management, and surface water diversion.
- California Department of Public Health (CDPH) Clean Water State Revolving Fund (SRF): The Safe Drinking Water State Revolving Fund (SDWSRF) provides funding to correct public water system deficiencies based upon a prioritized funding approach that addresses the systems' problems that pose public health risks, systems with needs for funding to comply

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with requirements of the Safe Drinking Water Act, and systems most in need on a per household affordability basis.

- iBank (Infrastructure State Revolving Fund Program and Economic Development Bank): iBank provides low cost, long term financing for local governments to fund a variety of public infrastructure projects. (Although this is not a grant, loan rates are largely determined by level of distress within a disadvantaged community).
- Actual Grant Funds Being Pursued or Awarded: The County and CSD's are looking for funding and planning strategies to try to close the gap on some of the existing deficiencies. The following is a list of grant funds that have been procured to assist in helping the larger of the more severely disadvantaged communities, with the most severe infrastructure issues. These funds will help in these specific communities with the greatest need, but will not provide adequate infrastructure for any additional development.
  - Proposition 1B funds for water and waste water. Both the Earlimart PUD and Tulare County are seeking funds for planning and construction projects.
  - Matheny Tract Waste Water Feasibility Study funded through the Strategic Growth Council (SGC) and funds from California State Water Revolving Fund (CWSRF).
  - Rural Community Assistance Corporation (RCAC) to work within the disadvantaged communities of Alpaugh, Allensworth, and Angiola, to develop a program for regional water and waste water.
  - \$2.0 million in Community Development Block Grant (CDBG) funds for housing related infrastructure projects in London and Traver.
  - Additional water / waste water projects in Tulare County are being or have been studied and grant funds sought or procured for:
    - Tipton (by the Tipton CSD)
    - Plainview (by Tulare County)
    - Yettem-Seville (by Tulare County)
    - East Porterville (by the Department of Water Resources)
    - Monson and Sultana (By Tulare County)
    - Pixley and Earlimart (by Pixley Utility District and Earlimart Public Utility District)
- Complete Streets Program: The County is in the process of updating its Community Plans for some of the communities in the Planning Area (as underlined below), and doing related Complete Streets Programs throughout the County through Measure R funding. Traver, Pixley, Tipton, Strathmore and the Porterville Area Plans have been completed, and Goshen, Earlimart (complete in 2018), Terra Bella/Ducor, Cutler/Orosi, and Three Rivers will be completed by the end of 2019. These plans also include working with local special districts, schools, and town councils for planning future complete streets (transit, bicyclist, pedestrians, roads, sidewalks, and bus), water, sewer, storm drainage and dry utilities.

## **ENVIRONMENTAL ANALYSIS**

### **4.1 Summary of the Findings of Consistency / Addendum to 2012 General Plan EIR**

The purpose of the Addendum and Consistency Checklist (“Finding of Consistency”) is to analyze whether the General Plan and Zoning Code Updates for up to 23 unincorporated areas and the addition of adjacent areas to these Unincorporated Development Boundaries, is within the scope of the Program described by the Tulare County General Plan (TCGP) EIR, and whether it could result in any new or substantially more severe significant environmental impacts that would be greater than those identified in the TCGP EIR, or require new mitigation measures. This Consistency Checklist will serve as the Addendum to the EIR, as the CEQA document for this General Plan Amendment 17-033 and 17-035.

As determined in the analysis provided herein, the Plan will not involve “new significant environmental effects or a substantial increase in the severity of previously identified significant effects,” which was not previously identified in the TCGP EIR.

Additionally, no new mitigation measures would be required; mitigation measures that were adopted for the TCGP EIR continue to remain applicable. The environmental impacts associated with the GPA would be within the envelope of impacts analyzed in the TCGP EIR, and/or do not constitute a new or greater significant impact. Based on substantial evidence and in the light of the whole record, the County has determined that no further CEQA documentation is required for adoption of the Plan.

It should be noted that upon adoption of GPA 17-33 and GPA 17-035, future projects within the Planned Areas may be subject to their own environmental review on a case-by-case basis in accordance with CEQA. If necessary, these future projects will be required to have their own CEQA documentation prepared to analyze project-specific environmental effects that have not been sufficiently analyzed in previous EIRs.

**Land Use:** The land use changes were considered but determined to not change the overall methodology or analysis under CEQA. For this Project, the County looked at the General Plan’s anticipated changes for the underlying land uses that were already in place in the 2012 TCGP Policy Framework. Hamlets and Urban Development Boundaries without plans were designated “Mixed-Use” Land Uses, which would allow for residential and commercial development, up to and through 30 units per acre and 10,000 min. square foot lot commercial developments. However, no zone changes were made to implement the 2012 General Plan, even though the TCGP and EIR anticipated residual “actual uses” consistent with the Mixed-Use Land Use Designation and ultimately consistent Zoning District patterns consistent with the community plan. Therefore, some of the lands remained in A-1 (Agricultural 1-Acre Minimum) zoning post-2012; and in agricultural zones generally, even though they contained existing residential development or Mixed-Use Land Use Designations. The growth proposed by this Project does not change the analysis for either the Land Uses / Aesthetic Impacts, as they are less than what the TCGP EIR fully anticipated through 2030.

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**Traffic:** The Project itself does not change any of the circulation patterns not already anticipated in the TCGP EIR. The urban development boundaries and hamlet development land use designations were Mixed Use in the 2012 General Plan with potential for up to 30 units per acre, but generally consistent with the General Plan's 1.3% average annual growth rate. The Project slightly changes the overall analysis for the proposed land uses that were consistent with the TAZ's at the time of the 2012 General Plan Update EIR. However, trip generation numbers are being calculated to a higher degree by actual zone changes to quantify Vehicle Miles Traveled versus merely a Level of Service Analysis under SB 743 and SB 375. This method has shown a better link between job-to-housing balances that are fostered in the Community Plan Re-zonings.

**Water:** In 2016, the Board adopted new changes to the Mapping in the General Plan Health and Safety Element for Groundwater Recharge Areas for groundwater recharge, which are now more explicitly defined. These are represented as New Figure 10-7 of the General Plan: Groundwater Recharge Areas. The Figure identifies major recharge areas, rivers, creeks, streams, flood corridors, riparian habitats, and land that may accommodate floodwater for purposes of groundwater recharge; and stormwater management and retention feasibility areas. The map identifies existing and proposed stormwater retention and detention basins. The significant rivers and streams located in Tulare County are identified and discussed in the Tulare County 2016 Health and Safety Element Update. Since 2012 new groundwater legislation has been passed. SGMA has not been fully implemented, so the regulations impact on the general plan and environment would still be speculative.

Evaluation of the County's water resources involved understanding of existing and anticipated water supplies from local watersheds, imported surface water and groundwater. In developing the environmental setting for this section, information from the 2005 Water Plan (which contains 2003 data) was used in this EIR because data for the 2010 Water Plan (for the Tulare Lake Basin) was not yet available. The 2005 Water Plan's existing and anticipated demand for known groundwater supplies were identified. By comparing existing and future anticipated supply and demand, potential impacts related to water quality and groundwater supplies were identified. Flooding and drainage impacts were considered in the context of existing floodplain protection and the changing regulatory context of flooding and drainage issues. These potential impacts were then assessed in the context of the proposed project policies to determine impact levels before and after mitigation. Assessment of Countywide water supply plus water and wastewater service-related issues are discussed in TCGP EIR Section 3.9 "Public Services, Recreation, and Utilities" under Impact 3.9-1. Lastly, it would be speculative to use any other information outside of the 2005 Water Plan as ensuing years resulted in severe drought (2011 - 2015) likely impacted both existing and forecasted water supply. The most recent Water Plan (2013) contains data through 2010; the year before the drought began. As such, water data during drought years 2011-2015 will not be available until the 2018 Water Plan update is available. Therefore, using the General Plan EIR's background studies in 2012 remain applicable to this Project.

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## 1.1 Environmental Analysis for Land Use / Air Quality / GHG Analysis

### Purpose of Analysis

- The Project planning areas are proposed for rezoning to bring existing, non-conforming land uses into conformity with the General Plan and to provide for future growth in a manner that reduces the overall vehicle miles traveled (VMT) in the Project planning areas, thereby reducing greenhouse gas emissions.
- Rezoning of the existing planning areas will accommodate higher density, mixed-use developments while maintaining the County's agricultural base and avoiding "leap-frog" developments.
- SGC Grant includes 21 planning areas:
  - Legacy Plans: El Monte, Hypericum, Jovista, Matheny Tract, Tooleville
  - Hamlet Plans: Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem
  - Community Plans: Alpaugh, East Oroshi, London, Richgrove, Sultana
- Three components of analysis:
  - Evaluation of anticipated growth and development within the Project area using U.S. Census data and an annual growth rate of 1.3% consistent with the General Plan.
  - Evaluation of land uses within the Project area based on existing zoning as established in the General Plan.
  - Evaluation of land uses within the Project area based on proposed zoning, including a small increase (less than 1%) in the total planning area.

### Methodology

- *Existing Zoning:* The potential buildout of the Project planning areas was determined by identifying all parcels within the planning area and applying the allowable uses to each parcel.
  - For low-density residential uses, it was assumed that one (1) residential unit is located on each parcel zoned A-1, AE (all variations), R-1 (all variations), and R-A (all variations).
  - For medium-density residential uses, it was assumed that two (2) residential units are located on each parcel zoned R-2. For high-density residential uses, it was assumed that four (4) residential units are located on each parcel zoned R-3.
  - For non-residential uses, it was assumed that each parcel is developed at a 0.25 floor-to-area ratio; that is, 25% of the acreage is developed with buildings and/or structures, on each parcel zoned C-1 (all variations), C-2 (all variations), C-3 (all variations), C-O, O, P-O, M-1, and M-2.
    - Non-residential development was divided into three (3) land use types for use in the emissions model: commercial, general light industrial, and general heavy industrial.
    - Although the Project planning areas include a wider variety of commercial and retail uses, because of the representative trip rates for the types of uses anticipated



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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for future development, commercial and retail uses were assessed as Strip Mall (60% of total commercial/retail square footage) and Office Park (40% of total commercial/retail square footage) to represent a conservative emissions analysis.

- *Proposed Zoning:* The potential buildout of the Project planning areas based on proposed zoning was determined by identifying all parcels within the planning area plus the expansion areas (less than 1% of the total area) and applying the allowable uses of the proposed zones to each parcel.
  - Assumptions for development are the same as for the existing zoning, except for uses with mixed-use (MU) overlay zoning. For MU zones it was assumed that two (2) dwelling units are located on parcels zoned C-2-MU and M-1-MU, and that four (4) dwelling units are located on parcels zoned C-3-MU.
- *Projected Growth:* The 1.3% annual growth rate was applied to the potential growth based on the population and dwelling units in 2015 as identified in the 2011-2015 American Community Survey 5-Year Estimates and the non-residential development under the existing zoning to determine whether the Project planning area could accommodate future growth under the proposed zoning.

### Emissions Analysis

- *Zoning Comparison:* One CalEEMod modeling run was prepared for each of the zoning scenarios based on the information provided in Table 1.
  - These uses were further split into sub-types (single family residential, low-rise apartments, strip mall, office park, light industrial, and heavy industrial) for modeling.
  - The zoning summary provided in Table 5 does not accurately represent what is currently built out or what could be built out. For example, there are legal, non-conforming, non-residential uses in agricultural zones that would be more appropriate to classify as commercial or industrial. Also, the zoning ordinance does not limit residential and commercial developments to what was used in the analysis. For example, R-3 zones could have more than 4 dwelling units per parcel and non-residential zones can vary greatly in the site coverage based on use, from none to minimal building coverage to up to 50% developed.
  - R-A zone has the largest area increase under proposed zoning, followed by R-3 zoning, and zone A has the largest area decrease. As such, the proposed zoning could result in residential areas, specifically the R-2 with 5.15 units/acre and the R-3 with 9.94 units/acre, that could meet or exceed the Tulare County Bluebook target of 5.3 units per acre.



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Table 5 - Zoning Summary			
	Existing Zoning	Proposed Zoning	Change
<b>Residential &amp; Agricultural</b>			
Parcels	3,876	3,318	-558
Acres	5,290	3,967	-1,323
Dwelling Units	4,184	5,241	+1,057
Population	17,740	22,222	+4,482
<b>Commercial / Office / Retail</b>			
Parcels	153	673	+520
Acres	65	1,253	+1,246
Square Footage	710,355	13,646,586	+12,936,231
<b>Industrial</b>			
Parcels	21	72	+51
Acres	65	291	+226
Square Footage	709,919	3,170,950	+2,461,031
<b>Other Non-Specific</b>			
Parcels	8	0	
Acres	634	595.54	-38.77
Total			
Parcels	4,058	4,063	+5
Acres	6,055	6,107	+52

- The emissions analysis presented in Table 6 represents potential emission at buildout of the Project planning areas, based on the assumptions provided in Table 5.

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Table 6 - Emissions Comparison			
	Existing Zoning	Proposed Zoning	Increase
Criteria Pollutant Emissions			
ROG	316.27	563.05	246.78
NO <sub>x</sub>	261.10	1,500.59	1,239.49
CO	707.49	2,727.02	2,019.53
SO <sub>2</sub>	1.40	5.40	4.00
PM <sub>10</sub>	91.55	328.23	318.68
PM <sub>2.5</sub>	53.82	121.75	67.93
Greenhouse Gas Emissions			
MTCO <sub>2</sub> e	129,453	569,837	440,384
MTCO <sub>2</sub> e per capita	7.3	25.65	18.35
MTCO <sub>2</sub> e per planning area	5,628	24,776	19,147

- The emissions analysis does not include the following reductions:
  - Compliance with the Air District's Rule 9510 (Indirect Source Review);
  - Increased Renewable Portfolio Standards requirements;
  - Pavley emissions standards;
  - Shorter commutes times and VMT reduction as a result of new employment opportunities in close proximity; and
  - Other design features that cannot be identified until project specific details are known.
- *Projected Growth:* One CalEEMod modeling run was conducted for the 1.3% annual projected growth of the total Project planning area through Year 2030.
  - Analysis included residential, commercial/retail, and industrial uses (see Table 7).
  - These uses were further split into different land use sub-types (see Table 8).
  - The 2011-2015 American Community Survey 5-Year Estimates were used for baseline (2015) population and dwelling units.
    - The average household size for these planning areas is 4.24, which is greater than the 3.37 average for the County used in the General Plan Housing Element.

Table 7 - Projected Growth through Year 2030							
Year	Residential			Commercial / Retail		Industrial	
	Population	Dwelling Units	Acres	Square Feet	Acres	Square Feet (Sq. ft.)	Acres
2015	18,595	4,679	323	710,355	65	709,919	65
2030	22,570	5,679	392	862,218	79	861,689	79
15-Year Growth	3,975	938	69	151,863	14	151,770	14

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- Total population growth: 3,975 persons
  - Annual (15 years): 265 persons
  - Per planning area (23): 173-person total, or 12 persons annually
- Total new dwelling units: 938 units
  - Annual (15 years): 63 units per year
  - Per planning area (23): 40 total units, or 3 units annually
- Total new commercial: 151,863 s.f.
  - Annual (15 years): 10,124 s.f. per year
  - Per planning area (23): 6,603 s.f. total, or 440 s.f. annually
- Total new industrial: 151,770 s.f.
  - Annual (15 years): 10,118 s.f. per year
  - Per planning area (23): 6,599 s.f. total, or 440 s.f. annually

Table 8 - Growth by Use Type			
Residential	Population	Units	Acres
2015-2030 Increase	3,975	938	69
Low-Density	2,226	525	39
Med - High Density	1,749	413	30
Commercial	Square Feet	Acres	
2015-2030 Increase	151,863	14	
Strip Mall	91,118	8	
Office Park	60,745	6	
Industrial	Square Feet	Acres	
2015-2030 Increase	151,770	14	
Light Industrial	132,040	12	
Heavy Industrial	19,730	2	

- Based on model defaults, construction of total growth would not take the full 15 years. As such, construction was totaled and then averaged to get the incremental annual construction emissions.
- Criteria pollutant emissions from the projected 1.3% annual growth rate are provided in Table 9.
- Greenhouse gas emissions from projected growth are provided in Table 10.

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**Table 9 - Criteria Pollutant Emissions from Projected Growth (tons)**

	ROG	NO <sub>x</sub>	CO	SO <sub>x</sub>	PM <sub>10</sub>	PM <sub>2.5</sub>
Total Construction	20.43	41.92	38.73	0.10	7.99	3.54
Average Annual Construction	1.36	2.79	2.58	0.01	0.53	0.24
Total Operations at Buildout	14.56	28.16	73.90	0.16	11.21	3.26
Average Annual Operations	1.12	2.17	5.68	0.01	0.86	0.25
Annual Significance Threshold	10	10	100	27	15	15
Threshold Exceeded	No	No	No	No	No	No

**Table 10 - Greenhouse Gas Emissions from Projected Growth**

Construction	Operations	Total
9,101	18,760	27,861

- 9,101 MTCO<sub>2</sub>e amortized over the 50-year life span of buildings equals 182 MTCO<sub>2</sub>e and is less than the Air District's 320 MTCO<sub>2</sub>e Zero Equivalency threshold.
- Per capita GHG emissions using 3,975 persons:
  - 4.72 MTCO<sub>2</sub>e, using the 18,760 MTCO<sub>2</sub>e operational emissions; and
  - 7.04 MTCO<sub>2</sub>e, using the 27,861 MTCO<sub>2</sub>e total emissions.

### Conclusion

#### Criteria Pollutants:

- Projected growth, consistent with the General Plan, would not obstruct or conflict with any applicable air quality plans. *Less Than Significant Impact, consistent with the General Plan EIR.*
- The incremental annual increases of criteria pollutant emission from projected future growth of the 21 communities, collectively, at 1.3% per year, would not exceed the Air District's annual thresholds of significance. As such, projected growth will not substantially contribute to existing air quality violations. *Less Than Significant Impact, Consistent with the General Plan EIR on air quality.*
- The projected incremental annual increase of criteria pollutants do not exceed any significance threshold. However, because there are 21 communities in which development could occur simultaneously, there is a possibility that development could occur at an unexpected rate (for example, rather than taking 15 years to build out, it could build out in three). As the sum of the incremental annual increase in operational emissions would exceed the Air District's threshold of significance for ROG and NO<sub>x</sub>, the project would have a Potentially Significant Cumulative Impact. Compliance with Air District Rule 9510 (Indirect Source Review) could be sufficient to reduce ROG emissions to a less-than-significant level; however, additional reductions would be needed to reduce NO<sub>x</sub> emissions to that level. As there are no developments associated with this project, use of

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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- a Voluntary Emissions Reduction Agreement (VERA) is not feasible. As such, NOx emissions would be considered to have a *Cumulatively Significant and Unavoidable Impact, Consistent with the General Plan EIR*.
- There is potential for future development to expose receptors to substantial pollutant concentrations. However, future development will be evaluated on a project-by-project basis and in coordination with the Air District to determine, and potentially mitigate, potential impacts. As such, exposure to substantial pollutant concentrations will have a *Less Than Significant Impact, Consistent with the General Plan EIR*.
  - There is potential for future development to create objectionable odors. Implementation of the applicable General Plan, Legacy, Hamlet and Community Plan policies and compliance with applicable District rules and regulations specifically designed to address air quality and odor impacts, would reduce potential odor impacts. Future development projects would be evaluated on a project-by-project basis. If a future development project may be a source of odors it will, if technically possible, mitigate any potential nuisance impacts. As such, potential odors will have a *Less Than Significant Impact, Consistent with the General Plan EIR*.

### Greenhouse Gases:

- The GHG emissions from the projected future growth of the 21 communities, collectively, at 1.3% per year, would be consistent with the Tulare County Climate Action Plan (CAP). This is demonstrated by project-related per capita GHG emissions that are lower than the CAP 2020 target of 8.8 MTCO<sub>2</sub>e per person and the 2015 CAP Update estimate of 8.1 MTCO<sub>2</sub>e per person. Development projects requiring discretionary approval would be required to comply with the CAP, which would further reduce GHG emissions. As such, GHG emissions from projected future growth, consistent with the General Plan, would have a *Less Than Significant Impact, Consistent with the General Plan EIR*.
- There are many variables left to be answered to enable a more accurate assessment of potential GHG emissions resulting from the change in zoning. Although the preliminary analysis indicates that per capita emissions are quite high, inclusion of data regarding commute lengths and existing development, and evaluation at higher density levels, could significantly reduce the preliminary assessment.
- Furthermore, based on the existing annual growth rate of 1.3%, it would take nearly 200 years to achieve the amount of commercial and industrial development that would be allowed under the proposed zoning.
- Even if GHG emissions could be demonstrated to be lower than this preliminary assessment, GHG emissions are cumulative in nature. Projects allowed by-right would not be required to reduce GHG emissions by 6% at the project level as required for projects obtaining discretionary approvals. As this analysis indicates a substantial increase in GHG emissions *could* result of the proposed rezoning, it would be speculative to make any conclusion otherwise. Although it is likely that the emissions presented in the analysis overstate potential emission, more research and evaluation is needed to assess county-wide

density-related impacts. As such, the GHG emissions resulting from implementation of the rezoning of the 23 legacy, hamlet, and community planning areas are considered to have a *Cumulatively Significant and Unavoidable Impact, Consistent with the General Plan EIR*.

## **GENERAL PLAN AMENDMENTS AND ZONE CHANGES**

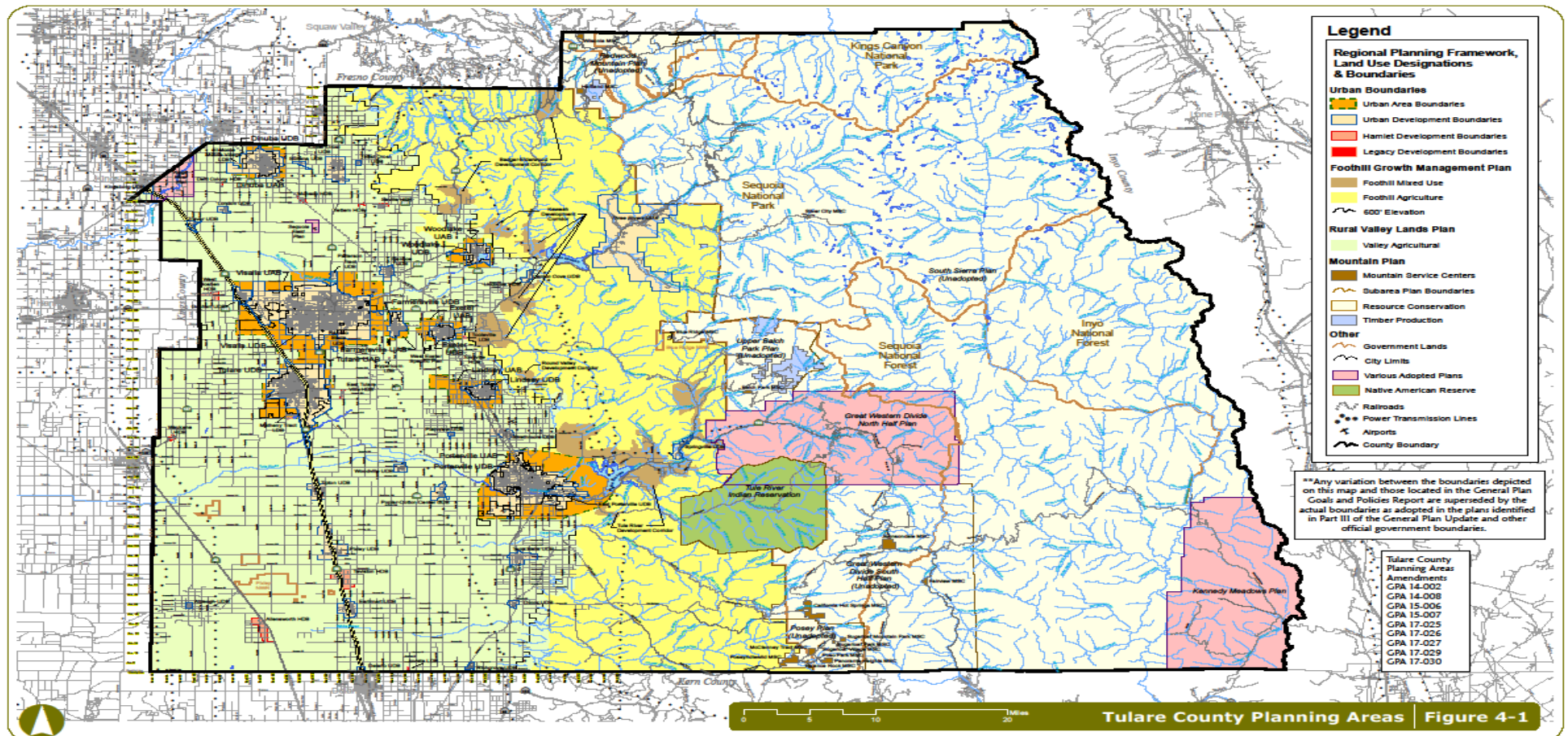
The Tulare County Board of Supervisors, on April 4, 2017, approved the General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Legacy Plans and Hamlet Plans. The purpose of the Unincorporated Community Plans, Legacy Plans and Hamlet Plans project is to perform an Unincorporated Communities Infrastructure and Planning Policy Analysis consistent with the Work Plan and Schedule of Deliverables identified in the County of Tulare Strategic Growth Council Grant No. 3014-631. These are communities with nonexistent, aged or failing infrastructure that face serious challenges and a lack of resources.

On September 27 and October 17, 2017, the Planning Commission and the Board of Supervisors approved the five (5) Legacy Boundary Plans (GPA 17-033). The 11 Hamlet Plans (GPA 17-035) and 5 Community Plans (GPA 17-035) were approved on November 8, 2017, by the Planning Commission and on December 5, 2017, by the Board of Supervisors. The following is an opportunities and constraints analysis of the impacts from these plans to infrastructure, housing, and economic development that are being addressed by the approval of the above 21 Plans.



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Figure 8 - Tulare County Regional Planning





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### Opportunities

**Complete Streets:** The Complete Streets Act of 2007 (Assembly Bill 1358) requires counties when updating General Plans, to identify how the jurisdiction will provide for the routine accommodation of all users of the roadway including motorists, pedestrians, bicyclists, individuals with disabilities, seniors, and users of public transportation.

**Affordable Housing:** Affordable housing will require more land in these communities than would typically be required in an area where public transit is available. In terms of siting, medium to high density housing should be located along collector streets and/or arterials. As noted earlier, the Community Plan includes Goals, Objectives, and Policies that will encourage new housing construction within the community to meet the needs of low and moderate-income residents.

**Commercial Potential:** There is no anticipated need to expand commercial zoning. With the addition of the mixed-use overlay zone, additional commercial potential is allowed. As there is limited discretionary income available from the community (based on the median incomes and proportions going towards housing), new commercial uses will mostly rely on the regional and highway market for revenue potential. As such, there is limited potential for large local community serving commercial uses. New commercial uses will likely be highway oriented, and fit under the new zoning district boundaries or under the new mixed-use overlay boundary.

### Constraints

There are several constraints or restrictions (obstacles) which will impact the nature and location of future development within the community. These constraints pertain to existing problems of public health and safety; acceptable noise levels impacts of deteriorating housing, and the lack of a full range or capacity of community services. Following are constraints that were recognized in the preparation of this plan and suggested approaches to resolve, minimize, or remove obstacles to future development.

**Infrastructure Needs:** Additional sources of water, sewer, storm drainage, and roadway maintenance will be an important part of future growth. The Special Districts provides limited water service / sewer, if at all, for these hamlets. The solution is that areas planned for future urban development outside of the Special District's boundary will require annexation to the district boundaries and connection to their facilities. Eventually, regional plants and more localized community facilities will need to be constructed to correct the shallow polluted wells and failing septic systems.

**Agricultural Lands:** Although a constraint to development, Agricultural Preserves prevent premature urban development of agricultural lands and encourage in-filling of existing vacant parcels within the immediate core of the Plan Area. The solution is after being included in development boundaries, as the need arises for developable land within development boundaries, Agricultural Preserves can be canceled more readily by a landowner with the approval of the Tulare County Board of Supervisors. Similarly, the other option available to landowners is nonrenewal of their ten-year contracts. This option allows their land to revert to "regular" agricultural lands over a ten-year period and,

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subsequently allowing the landowner an opportunity to develop his land through the regular permitting process.

Noise: As described in the background analysis of each Development Plan, noise exposure policies will restrict the type of land uses which can be developed within identified noise-impacted areas. The solutions are the mitigation measures identified in the Tulare County 2030 General Plan's Noise Element and Environmental Impact Report, as Added for these plans will allow the development of some land uses provided certain standards are met, which reduce the impact of noise within the noise-impacted areas.

### Compatibility with Existing Plans

These Hamlet Plans will encourage economic development by identifying opportunities for development. These Hamlet Plans is also a part of the implementation of the San Joaquin Valley Regional Blueprint, Tulare County Regional Blueprint, and the Tulare County 2030 General Plan.

San Joaquin Valley Regional Blueprint: "The San Joaquin Valley Blueprint [Valley Blueprint] is the result of an unprecedented effort of the eight Valley Regional Planning Agencies (RPA), that include the Fresno Council of Governments, the Kern Council of Governments, the Kings County Association of Governments, the Madera County Transportation Commission, the Merced County Association of Governments, the San Joaquin Council of Governments, the Stanislaus Council of Governments, and the Tulare County Association of Governments, to develop a long-term regional growth strategy for the future of the San Joaquin Valley. Following three years of visioning and outreach by the eight Valley RPAs, the Regional Policy Council (RPC), the decision-making body for the Valley wide process, adopted the Valley Blueprint in April 2009.

The [Valley Blueprint] is a long-range vision for a more efficient, sustainable, and livable future for the Valley. The Valley Blueprint is made up three elements: a 2050 growth scenario diagram that identifies areas of existing development, new development, and future regional transit and highway improvements; a Valley wide average target density of 6.8 units per acre for new residential growth to the year 2050; and a set of 12 Smart Growth Principles. Importantly, the [Valley Blueprint] recognizes and incorporates by reference the visioning and outreach efforts undertaken by the eight Valley Regional Planning Agencies.

Tulare County Regional Blueprint: TCAG and its member agencies felt that it was important to prepare a Tulare County Regional Blueprint that clarified Tulare County's role in the [Valley Blueprint] process. The Tulare County Regional Blueprint is a stand-alone policy document that is consistent with the San Joaquin Valley Regional Blueprint. This document represents Tulare County's local vision and goals as a participant in the San Joaquin Valley Regional Blueprint process. Key elements of the preferred growth scenario outlined in the Tulare County Regional Blueprint include a 25% increase in overall density and focused growth in urban areas.

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## 6.1 COMMUNITY (URBAN DEVELOPMENT BOUNDARIES)

**COMMUNITY PLAN 2017 UPDATE (GPA 17-035)**  
**General Plan Amendment GPA's 17-004, GPA 17-034, GPA 17-008, GPA 17-011, GPA 17-012**  
**Mixed Use Overlay PZC 17-041**  
**Rezoning of Properties PZC's 17-004, PZC 17-005, PZC 17-008, PZC 17-011, PZC 17-012,**  
**Use Permit/By Right Modifications PZC 17-042**  
**Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report**

### Requested Actions

The Recommended Actions by the Planning Commission (Recommendation on November 8<sup>th</sup>) and approval by the Board of Supervisors on December 5<sup>th</sup> for the approval of the Community Plans 2017 Update include:

1. Certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Community Plans 2017 Update;
2. Adopt the Community Plan 2017 Update, including amendments to the Tulare County General Plan as follows: Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation, and Community Plans (Part III);
3. Adopt an amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed-Use Combining Zone in Alpaugh, East Oroshi, London, Richgrove, and Sultana. (See Attachment 3);
4. Adopt an amendment to Section 16 of Ordinance No. 352, to allow additional "by-right" uses within the UDB's of Alpaugh, East Oroshi, London, Richgrove, and Sultana. (See Attachment 4);
5. Amend the Community re-zoning plan to apply said zone to reclassify properties, within the Alpaugh, East Oroshi, London, Richgrove, and Sultana Urban Development Boundary. (See Attachment 5);
6. \*\*\* Approval of Strategic Growth Council Final Report. \*\*\*
7. Adopt GPA 17-035, for the Community Plan 2017 Updates as one (1) General Plan Amendment, the third of the four allowed in 2017. (See Attachment 2).

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### Location

The proposed Project sites, or proposed general plan amendment areas, encompasses the existing communities of Alpaugh, East Orosi, London, Richgrove, and Sultana, see Figure 2.

### Community Information (See Appendix B for Community Plans)

#### **Alpaugh**

Alpaugh is a Census-Designated Place located in the southwest portion of Tulare County. It is bounded by Avenue 50 in the south, Avenue 58 in the north, Road 34 in the west, and Road 42 in the east and encompasses one (1) square mile of land. It is not directly served by any State Route. The Tulare County/Kings County Line is located approximately two miles west of Alpaugh, and the Tulare County/Kern County Line is located approximately seven miles south of Alpaugh. Communities located near Alpaugh include Allensworth and Earlimart to the east, Pixley to the northeast, Delano to the southeast, and Corcoran to the northwest. Alpaugh is an agriculturally oriented service community surrounded on all sides by lands in agricultural production, scattered rural residential uses, and vacant land. The Alpaugh Urban Development Boundary (UDB) area consists of 205.4 acres. The existing uses within the UDB are described as follows. Agricultural activities, including orchards and pasture, currently occupy 3 percent of the 205.4 acres. Urban development, including urbanized uses such as residential, commercial, public and quasi-public facilities, as well as industrial development occupy 78 percent of the 205.4 acres. The remaining 22 percent are lands dedicated for Right-of-way.

In 2015, the Population for Alpaugh was 1,103. Alpaugh's population is younger than the median age throughout all of Tulare County. Alpaugh's median age of 22.3 is lower than the median age of the State of California's median age of 35.8 years. In 2015, Alpaugh's median household income was \$27,222, whereas the State of California's median household income was \$61,818. Alpaugh's median household income was 44% of the State of California's median household income, therefore it is considered a severely disadvantaged community.

#### **East Orosi**

East Orosi is a Census-Designated Place located in the northwest portion of Tulare County. It is bounded by Avenue 416 in the south, Avenue 420 in the north, Alta East Branch Canal in the west, and Road 142 in the east and encompasses 0.2 square miles of land. It is not directly served by any State Route. The East Orosi Community Development Boundary (UDB) area consists of 179.2 acres. The Land Uses within the UDB are described as follows: Agricultural activities 130 acres, 30 acres are developed as residential or commercial development, and right-of-way is 23 acres. Urban development, including urbanized uses such as residential, occupy 16 percent of the 179.2 acres. The remaining 12 percent are lands dedicated for Right-of-way. In 2015, the population of East Orosi was 572.

East Orosi's population is younger than the median age throughout all of Tulare County. East Orosi's median age of 21.3 is lower than the median age of the State of California's 35.8 years. In 2015, East Orosi's median household income was \$34,896, whereas the State of California's median household

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income was \$61,818. East Orosi's median household income was 56% of the State of California's median household income, and therefore it is considered a disadvantaged community.

### **London**

London is a Census-Designated Place located in the northern portion of the County, approximately three miles southwest of Dinuba and ten miles northwest of Visalia. It is bounded by Avenue 376 in the south, Avenue 384 in the north, Kennedy School House Ditch in the west, and Road 60 in the east and encompasses 0.6 square miles of land. London is an agriculturally oriented service community surrounded on all sides by lands in agricultural production, vacant lands, and scattered rural residential homes. Cities and communities surrounding London include Visalia to the southeast; Dinuba to the northeast; and the community of Traver to the southwest. The Tulare County/Fresno County Line is located approximately 4.8 miles west of London. The London Community Development Boundary (UDB) area consists of 384.2 acres. Under the 2012 General Plan the only land use designation in London is for "Mixed Use." The existing use is based on actual acreage use within the UDB are described as follows: Agricultural activities occur on +/- 215 acres; and urban development, including urbanized uses such as residential development and the London community services district (LCSD) sewer treatment plant occupy +/- 115 acres. The remaining 28.9 acres are dedicated for Right-of-way. In 2015, the population of London was 2,056.

London's population is younger than the median age throughout all of Tulare County. London's median age of 24.8 is lower than the median age of the State of California's median age of 35.8 years. In 2015, London's median household income was \$24,491, whereas the State of California's median household income was \$61,818. London's median household income was 39.6% (40%) of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

### **Richgrove**

The community of Richgrove is located on the eastern side of the San Joaquin Valley, approximately 50 miles north of Bakersfield and 75 miles southeast of Fresno. Richgrove is a Census-Designated Place located in the southern portion of Tulare County, just north of the Tulare County/Kern County line. It is generally bounded by County Line Road in the south, Avenue 8 in the north, Richgrove Drive in the west, and Road 210 in the east and encompasses 0.5 square miles of land. Nearby cities and communities include, Ducor approximately 8 miles to the northeast, Delano approximately 10 miles to the west, Terra Bella approximately 12 miles to the northeast, Poplar-Cotton Center approximately 18 miles to the north, and Porterville approximately 20 miles to the northeast. Richgrove is an agriculturally oriented service community surrounded on all sides by lands in agricultural production, and vacant lands. Richgrove is a community with a strong agricultural industry including many grape vineyards, citrus orchards, and row crops. State Highway 65 is located approximately 3 miles to the east of the community and State Highway 99 is located approximately 7 miles to the west. Both of these highways serve as primary regional access routes to the community from outside areas. Richgrove is a small, unincorporated community with an Urban Development Boundary (UDB) area of approximately 234.1 acres. The Richgrove Community Urban Development Boundary (UDB) area consists of 234.1 acres. Under the 2012 General Plan the existing uses are based on actual acreage use within the UDB are described as follows: Agricultural activities occur on

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+/- 60 acres; and urban development, including urbanized uses such as residential development, institutional, industrial, and commercial uses occupy +/- 174 acres. The remaining 52.1 acres are dedicated for Right-of-way. In 2014, the Population for Richgrove was 3,006.

Richgrove's population is younger than the median age throughout all of Tulare County. Richgrove's median age of 23.3 is lower than the median age of the State of California's 35.8 years. In 2014, Richgrove's median household income was \$29,792, whereas the State of California's median household income was \$61,094. Richgrove's median household income was 48.7% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

### **Sultana**

Sultana is a Census-Designated Place located in the northwest portion of Tulare County. It is bounded by Avenue 412 in the south, north of Avenue 416 in the north, Road 100 in the west, and Road 108 in the east and encompasses 0.4 square miles of land. It is not directly served by any State Route. The Sultana Community Development Boundary (UDB) area consists of 260.9 acres. The Land Uses within the UDB were designated as "Mixed Use" in the 2012 General Plan. Agricultural activities based on current zoning and actual uses, including orchards and pasture, currently occupy 70 percent of the 260.9 acres. Urban development, including urbanized uses such as rural residential, residential, high density and commercial development, occupy less than 10 percent of the 260 acres. The remaining 18 percent are lands dedicated for Right-of-way. In 2015, the population of Sultana was 916.

Sultana's population is younger than the median age throughout all of Tulare County. Sultana's median age of 20.7 is lower than the median age of the State of California's median age of 35.8 years. In 2015, Sultana's median household income was \$24,950, whereas the State of California's median household income was \$61,818. Sultana's median household income was 40.3% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.



# **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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## **6.2 HAMLETS (HAMLET DEVELOPMENT BOUNDARIES)**

**HAMLET PLAN 2017 UPDATE (GPA 17-035)**  
**General Plan Amendment GPA's 17-014, GPA 17-015, GPA 17-016, GPA 17-017, GPA 17-018,**  
**GPA 17-019, GPA 17-020, GPA 17-021, GPA 17-022,**  
**GPA 17-023, GPA 17-024**  
**Mixed Use Overlay PZC 17-038**  
**Rezoning of Properties PZC's 17-037, PZC 17-015, PZC 17-016, PZC 17-017, PZC 17-018,**  
**PZC 17-019, PZC 17-020, PZC 17-021, PZC 17-022, PZC 17-023 PZC 17-024**  
**Use Permit/By Right Modifications PZC 17-039**  
**Addendum to the (2012) Tulare County 2030 General Plan**  
**Final Environmental Impact Report**  
**(Amending Part III Chapter 2 Hamlet Plans of the 2012 County of Tulare 2030**  
**General Plan Update)**

### Requested Action

The Recommended Actions by the Planning Commission on November 8<sup>th</sup> and by the Board of Supervisors on December 5<sup>th</sup> for the approval of the Hamlet Plans 2017 Update include:

1. Certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update. (See Attachment 1);
2. Adopt the Hamlet Plan 2017 Update, including amendments to the Tulare County General Plan as follows: (Part I) Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation, and Hamlet Plans (Part III), (See Attachment 2);
3. Adopt an amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed-Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem. (See Attachment 3);
4. Adopt an amendment to Section 16 of Ordinance No. 352, to allow additional "by-right" uses within the HDB's of Allensworth, Delft Colony, East Tulare Villa, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem. (See Attachment 4);
5. Amend the Hamlet re-zoning plan to apply said zone to reclassify properties, within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary. (See Attachment 5);
7. Adopt GPA 17-035, for the Hamlet Plan 2017 Updates as one (1) General Plan Amendment, the third of the four allowed in 2017. (See Attachment 2).

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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### Location

The proposed Project sites, or proposed general plan amendment areas, encompasses the existing communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem,.

### Background

Hamlets have been in existence for over 50 years, with no comprehensive planning. This planning effort creates that key relationship, and implementation tool, for the General Plan. The 2012 Tulare County's General Plan (2030) provides a comprehensive statement of the objectives, themes and policies, which the community is seeking to achieve in the areas of land use, growth management, community design, transportation, open space, parks and public facilities, environmental conservation, health and safety, noise, and housing. The hamlet plans are essentially an extension of the General Plan, but more specific to each Hamlet.

No existing "Hamlet Plans" had been developed for or were approved in the 2012 General Plan. Additionally, the County has created Hamlet Development Boundaries (HDB's). Moreover, the textual changes in the General Plan prescribed the "mixed use" land use designation to all HDB's, through the 2012 General Plan Update.

Therefore, under this 2017 General Plan Amendment, the following General Plan chapters and sections are relevant to further implementing the Hamlet Plans: Chapter 2 Sections 2.1 through 2.11 are being updated precisely. These plans are to include specific hamlet boundaries, land uses, and zoning map updates.

These plans include changes to all the Hamlets under Chapter 2: Sections 2.1 Allensworth, 2.2. Delft Colony, 2.3 East Tulare Villa, 2.4 Lindcove, 2.5 Monson, 2.6 Seville, 2.7 Teviston, 2.8 Tonyville, 2.9 Waukena, 2.10 West Goshen, and 2.11 Yettem of the General Plan. The General Plan also provides additional elaboration on the ways in which the Hamlet Plan are responsive to this guidance. Relevant General Plan goals, policies, and programs that provide direction and input to the hamlet plans are provided in this document. In addition, this plan has specific policies for each Hamlet. The Implementation Requirements of the General Plan have been addressed and met formally the approval process of these plans.

# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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## Hamlet Related General Plan Goals and Policies

### PF-3

**To provide a realistic planning area around each unincorporated hamlet to clearly delineate the boundaries of each hamlet, provide a framework for economic development, the provision of public services, and an outstanding quality of life.**

#### PF-3.1 Hamlet Development Boundaries - Hamlets

The County shall limit urban development to the area within the designated HDB for each hamlet. The HDBs for existing hamlets are defined on Figures 2.3-2 thru 2.3-12.

#### PF-3.2 Modification of HDB - Hamlet

1. The County may consider modification of a HDB under any of the following circumstances:
  - a. All HDBs should be reviewed on a five-year cycle to reflect changes in growth and development patterns.
  - b. A request for expansion can be applied for, as part of a subdivision or specific plan proposal, or at the request of a special district or Hamlet. A request for expansion of the HDB can be applied for as part of a General Plan Amendment to the Land Use Diagram.
  - c. An HDB should be considered for expansion at such time as land for infill becomes limited. This condition is considered satisfied when 80 percent of the non-Williamson Act land within the HDB is developed.
  - d. HDBs should not be expanded onto Prime Farmland if Farmland of Statewide Importance or of lesser quality is available and suitable for expansion.
2. Prior to approval of a HDB expansion, the County shall ensure that appropriate infrastructure can be provided to serve the new areas added to the HDB and that sufficient water supplies are available. If the expansion pushes the hamlet towards a community classification, an infrastructure master plan for the hamlet should be prepared to plan and finance community water and sewer services, and representation/documentation of availability and sufficiency of long-term water supplies should be provided.
3. Preservation of productive agricultural lands shall be the highest priority when considering modifications. Expansion of a HDB to include additional agricultural land shall only be allowed when other non-agricultural lands are not available to the community for expansion.
4. Non-administrative changes to a HDB shall require a General Plan amendment.

#### PF-3.3 Hamlet Plans

The County shall ensure that Hamlet Plans are updated and maintained for each of the identified hamlets. These plans shall include the entire area within the HDB. The plans will provide a Land Use Diagram with a discussion of allowed uses and densities/intensities. A discussion of the hamlet's short

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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and long-term ability to provide necessary urban services, including the availability and sufficiency of long-term water supplies will also be provided.

The Hamlet Related General Plan Implementation Sections are as follows:

- “18. The County, led by the Planning Commission, shall conduct a citizen outreach effort in Tulare County hamlets to prepare guidelines for the preparation of hamlet plans, considering such factors as: Alternative approaches to hamlet planning including application of mixed use designations and appropriate use of form-based codes, Content of hamlet plans, Sources of funding for hamlet plans. Criteria for preparing an implementation schedule, such as prioritizing plans for hamlets where demonstrated community leadership exists, Identification of appropriate means for securing public open space, recreational areas, and other public amenities. Coordination with capital improvement plans and identification of options for addressing infrastructure deficiencies, as applicable. Defining appropriate context sensitive improvement standards, and determining feasible mechanisms to pay for new public amenities and services. These guidelines will be presented to the Board of Supervisors for their adoption [New Program].”
- “19. Upon adoption of guidelines for preparation of hamlet plans, the Planning Commission shall prepare an implementation schedule and budget that prioritizes the order in which hamlet plans and any associated environmental documents shall be programmed during the budget process. On an annual basis, or as may be needed, the Planning Commission shall review and recommend priorities for hamlet plan preparation to the Board of Supervisors [New Program].”
- “20. The County shall explore all available options to fund necessary improvements such as: establishment of redevelopment project areas, revenue sharing, formation of assessment districts, development agreements, Community Development Block Grant (CDBG) funds, grants, etc., as part of the plan development process [New Program].”
- “21. The County shall amend its Zoning Ordinance to add mixed use zones based on smart growth and neo-urban principles [New Program].”
- “22. The County shall minimize potential land use conflicts at the interface between urban development, as well as upgrades that should be planned for the hamlet, as funding allows. The County shall continue to support community and hamlet efforts to secure State and Federal funding and pursue Community Development Block Grants [New Program].”

### Land Use / Zoning Policy Changes

As with any community plan, the contents of these Hamlet Plans are not intended to be absolute. Planning is a continuous process, and to be effective, requires periodic re-evaluation and revision to reflect changing needs and priorities. These Plans, therefore, should be reviewed on a periodic basis with the assistance and participation of local citizens, groups, and agencies. By doing so, it is

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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envisioned that the hamlet plans will continue to provide meaningful and necessary guidance toward the development of the community in the foreseeable future.

Through RMA's efforts to lessen the entitlement requirements in Tulare County, the effect has been a change in land use / zoning regulations. These changes were initiated under the 2012 General Plan Update and Section 352 of the Tulare County Ordinance Code, through "mixed use" land use designations in the General Plan and "mixed use overlay" zoning districts. Moreover, there has been an overwhelming reduction in "Site Plan Review" with the removal of the "SR-Overlay" (Site Review Overlay Zones). The other arm of reduced regulations is the policy expansion of allowed uses "by right" (reduction in use permits), under Section 16 of the Zoning Code. Moreover, the County increased design standards relative to "mixed use overlays" in order to increase the performance standards for the built environment. Through these mechanisms, Tulare County RMA is attempting to increase economic development opportunities within these Hamlets; and thereby, reduce the need to commute to other areas to shop, or work, for schools or for health care.

### Infrastructure and General Services

Community wide infrastructure is non-existent in most of these Hamlets, as most homes are on either or both septic and/or individual well systems. Moreover, the franchises (telecommunications, gas, electricity) that many of the Cities and Communities in Tulare County enjoy do not exist in the Hamlets and based on some of the outreach conducted the general service response times for sheriff, fire and medical are not always consistent with the quantitative studies or agency reports.

### General Plan Implementation:

Implementation of General Plan Policy Framework PF-1.2, and PF-3-3 - PF-3.5: Under Section 2.2 Work Plan/ Implementation, Subsection 18-22 Hamlet Plans

The Guidelines for Implementation of the Hamlet Plans is outlined in the below measures stated as sub-sections 18 – 22 Implementation (see General Plan Part 1, pages 2-75 – 2-76). None of these measures were intended to hold up the update of the Hamlet Plans, and as not all the implementation measures were not addressed, there is no reason to not approve the Hamlet Plans as created, for lack of funding infrastructure or amenities within the Hamlets. The Planning Commission in the creation of Hamlet Plans and performance criteria within Hamlets has applied the zoning and zoning criteria for a "mixed use overlay" zone (Section 18.9 of the Zoning Code, Attachment A-2 and A-3). This report is made to the Planning Commission and the Board of Supervisors, clearly illustrating how the Resource Management Agency is meeting these measures with RMA's findings (*Hamlet Plan Implementation Findings 1 – 5*, below). The following criteria are used to define an unincorporated area as a "hamlet" or purposes of the General Plan:

1. Generally located in the Valley Region of the County but may be located in the Foothill region, and should be identified in the Foothill Growth Management Plan,
2. A population of over 100 persons,
3. The population resides in the area more than nine months out the year, and

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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4. A definable core that contains at least three of the following features:
  - a. Special district or town council,
  - b. Grocery store or other commercial establishment,
  - c. Wastewater system,
  - d. Community water system,
  - e. Public school,
  - f. Post office, and
  - g. Community center or other community gathering location (church, Veterans Memorial Hall, etc.)

### 5. Community Information (See Appendix B for Community Plans)

#### **Allensworth**

The community of Allensworth is located on the east side of the San Joaquin Valley. Allensworth is a Census-Designated Place located in the southwest portion of Tulare County. Allensworth encompasses 3.1 square miles of land, just east of Kings County. It is bounded by Avenue 24 in the south, Attocks Avenue in the north, and Road 76 in the west, SR 43 runs east of and parallel to the Burlington Northern-Santa Fe Railroad tracks and serves as a physical boundary in the east, however there is no station currently operating. The Allensworth Hamlet Development Boundary (HDB) area consists of 1,051.4 acres. The Land Uses within the HDB are described as follows: Agricultural activities, including orchards and pasture, currently occupy 57 percent of the 1,051.4 acres. Urban development, such as residential development occupy 4 percent of the 1,051.4 acres. The remaining 10 percent are lands dedicated for Right-of-way.

In 2015, the population in Allensworth was 565. Allensworth's population is younger than the median age throughout all of Tulare County. Allensworth's median age of 22.6 is lower than the median age of the State of California median age of 35.8 years. Allensworth is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005 (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average." In 2015, Allensworth's median household income was \$28,929, compared to \$42,031 for Tulare County and \$61,818 for the State of California (Census data: ACS 2015 5) whereas the State of California's median household income was \$61,818.

#### **Delft Colony**

The community of Delft Colony is located on the east side of the San Joaquin Valley. Delft Colony is a Census-Designated Place located in the northwest portion of Tulare County. It is bounded by Payne Avenue in the south, Avenue 400 in the north, Road 56 in the west and encompasses 0.07 square miles of land. It is directly served by State Route 201 roughly five miles southwest of the city of Dinuba in Tulare County. The Delft Colony Hamlet Development Boundary (HDB) area consists of 87.3 acres. The Land Uses within the HDB are described as follows: Agricultural activities, including orchards and pasture, currently occupy 45.8% of the 87.3 acres. Urban development, including urbanized uses such as residential and commercial development occupy 40% of the 87.3



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acres. The remaining 14 percent are lands dedicated for Right-of-way. In 2015, the population of Delft Colony was 132, Medium Household Income is \$31,156, and is comprised of approximately 43 residences.

Delft Colony's median age of 16.7 years is lower than the median age of the State of California 35.8 years. In 2015, Delft Colony's median household income was \$6,917, whereas the State of California's median household income was \$61,818. Delft Colony's median household income was 11.1% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

### **East Tulare Villa**

The community of East Tulare Villa is located on the east side of the San Joaquin Valley. East Tulare Villa is a Census-Designated Place located in the western portion of Tulare County. It is bounded by Bardsley Avenue in the south, State Route (SR) 137 in the north, Munson Road in the west, and Road 132 in the east and adjacent to the city of Tulare. The East Tulare Villa Hamlet Development Boundary (HDB) area consists of 178.5 acres. The Land Uses within the HDB are described as follows: Agricultural activities, including orchards and pasture, currently occupy 54 percent of the 178.5 acres. Urban development, including urbanized uses such as residential and commercial development occupy 18 percent of the 178.5 acres. The remaining 10 percent are lands dedicated for Right-of-way. The population in East Tulare Villa is 951 people.

East Tulare Villa's median age of 38.1 years is higher than the median age of the State of California median age of 35.8 years. In 2015, East Tulare Villa's median household income was \$50,163, whereas the State of California's median household income was \$61,818. East Tulare Villa's median household income was 80% of the State of California's median household income, and therefore it is considered a disadvantaged community.

### **Lindcove**

The community of Lindcove is located on the east side of the San Joaquin Valley. Lindcove is a Census-Designated Place located in the central east portion of Tulare County. It is bounded by Avenue 312 in the south, Boston Avenue in the north, and Road 226 in the west, and Road 228, roughly five (5) miles northeast of the city of Exeter in Tulare County. The Lindcove Hamlet Development Boundary (HDB) area consists of 73.3 acres. The Land Uses within the HDB are described as follows: Agricultural activities, including orchards and pasture, currently occupy 76 percent of the 73.3 acres. The remaining 23 percent are lands dedicated for Right-of-way. In 2015, the population for Lindcove was 494.

In 2015, Lindcove's median household income was \$43,571, whereas the State of California's median household income was \$61,818. Lindcove's median household income was 70.8% of the State of California's median household income.

### **Monson**

The community of Monson is located on the east side of the San Joaquin Valley. Monson is a Census-Designated Place located in the northwest portion of Tulare County. It is bounded by Avenue 384 in

## **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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the south, Avenue 388 in the north, Sand Creek in the west, and Road 108 roughly three (3) miles southeast of the city of Dinuba in Tulare County. The Monson Hamlet Development Boundary (HDB) area consists of 115 acres. The Land Uses within the HDB are described as follows: Agricultural activities, including orchards and pasture, currently occupy 82.7 percent of the 115 acres. Urban development, including urbanized uses such as commercial development occupies 0.8 of the 115 acres. The remaining 12.4 percent are lands dedicated for Right-of-way. In 2015, the population of Monson was 237.

Monson's population is younger than the median age throughout all of Tulare County. Monson's median age of 26.4 is lower than the median age of the State of California median age of 35.8 years. Monson is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005 (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average." Monson's median household income was unavailable according to the 2011-2015 American Community Survey Income data.

### **Seville**

Seville is a Census-Designated Place located in the northwest portion of Tulare County. It is bounded by Inyo Avenue in the south, Avenue 384 in the north, Road 152 in the west, and east of Road 156, roughly 8.5 miles southwest of Cutler in Tulare County. The Seville Hamlet Development Boundary (HDB) area consists of 178.5 acres. The Land Uses within the HDB are described as follows: Agricultural activities, including orchards and pasture, currently occupy 21.5 percent of the 178.5 acres. Urban development, including urbanized uses such as residential, commercial, public and quasi-public facilities, as well as industrial development occupy 67.7 percent of the 178.5 acres. The remaining 21.4 percent are lands dedicated for Right-of-way. In 2015, the Seville population was 545.

Seville's population is younger than the median age throughout all of Tulare County. Seville's median age of 24.3 is lower than the median age of the State of California of 35.8 years. In 2015, Seville's median household income was \$23,750, whereas the State of California's median household income was \$61,818. Seville's median household income was 38.4% of the State of California's median household income, and therefore it is considered a severely disadvantaged community. As defined by the State of California Public Resources Code 75005 (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

### **Teviston**

The community of Teviston is located in Tulare County on the eastern side of the San Joaquin Valley and along State Route 99. Teviston is a Census-Designated Place located in the southwest portion of the County, southwest of Porterville. Teviston is bounded by Avenue 72 in the south, Avenue 84 in the north, Road 126 in the west, and Road 136 in the east and encompasses 2.2 square miles of land. Teviston is an agriculturally oriented service community surrounded on the north, west and south by lands in agricultural production, and on the east by scattered rural residential, agricultural, and vacant

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land. Recently, the northern Urban Development Boundary for the Community of Earlimart was adopted along Deer Creek just south of Teviston's southern Hamlet Development Boundary. The Teviston Hamlet Development Boundary (HDB) was approved in the 2012 Tulare County General Plan with an area that consists of 1,443.2 acres. The Land Uses within the HDB are Mixed Use occupy 1,312 acres of the buildable land. The remaining 130.8 acres of lands are dedicated for Right-of-Way. In 2015, the population of Teviston was 1,623.

Teviston's median household income was \$24,181 compared to \$61,818 for the State of California and \$42,031 for Tulare County. In 2015, Teviston's median household income was \$24,181, whereas the State of California's median household income was \$61,818. Teviston's median household income was 39% of the State of California's median household income, and therefore it is considered a severely disadvantaged community. As defined by the State of California Public Resources Code 75005 (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

### **Tonyville**

The community of Tonyville is located on the east side of the San Joaquin Valley and is a Census-Designated Place located in the western portion of Tulare County just north of the City of Lindsay. It is bounded by Avenue 252 in the south, Avenue 254 in the north, and Road 216 in the west and encompasses 0.05 square miles of land. It is not directly served by any State Route. The Tonyville Hamlet Development Boundary (HDB) was approved in 2012 Tulare County General Plan with an area that consists of 34.2 acres. The Land Uses within the HDB are Mixed Use occupy 29.7 acres of the buildable land. The remaining 4.5 acres are lands dedicated for Right-of-way. In 2015, the Population for Tonyville was 572.

In 2015, Tonyville's median household income was not available. As defined by the State of California Public Resources Code 75005 (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

### **Waukena**

The community of Waukena is located on the east side of the San Joaquin Valley and is a Census-Designated Place located in the southwest portion of Tulare County on California State Route 137, approximately 4 miles northeast of Corcoran and approximately 13.5 miles southwest of Tulare. It is bounded by Road 24 in the west, Curti Road in the east, and north and south of Avenue 192 and encompasses 0.9 square miles of land. It is directly served by State Route (SR) 137. The Waukena Hamlet Development Boundary (HDB) area consists of 119.3 acres. The Land Uses within the HDB are Mixed Use activities that occupy 104.4 acres. The remaining 14.9 percent are lands dedicated for Right-of-way. In 2010, the population in Waukena was 108.

Waukena's median age of 20.4 years is younger than the median age throughout all of Tulare County (30.4 years) and the State of California (35.8 years). In 2015, Waukena's median household income was \$36,750, whereas the State of California's median household income was \$61,818. Waukena's

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median household income was 59% of the State of California's median household income, therefore it is considered a severely disadvantaged community. As defined by the State of California Public Resources Code 75005 (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

### **West Goshen**

The community of West Goshen is located on the east side of the San Joaquin Valley. West Goshen is a Census-Designated Place located approximately 1.5 miles of the west of the community of Goshen along Ave. 308, just east of Kings County. The community is situated approximately 1.5 miles west of Highway 99 and 1-mile north of Highway 198. The West Goshen Hamlet Development Boundary (HDB) area consists of 477.2 acres. The Land Uses within the HDB are described as follows: Agricultural activities, including orchards and pasture, currently occupy 92 percent of the 477.2 acres. Urban development, including urbanized uses such as residential development occupies 9.6 percent of the 477.2 acres. The remaining 5.7 percent are lands dedicated for Right-of-way. In 2015, the population of West Goshen was 675.

West Goshen's median age of 23.2 years is lower than the median age of Tulare County (30.3 years) and of the State of California (35.8 years). In 2015, West Goshen's median household income was \$20,700, whereas the State of California's median household income was \$61,818. West Goshen's median household income was 33.4% of the State of California's median household income, and therefore it is considered a severely disadvantaged community. As defined by the State of California Public Resources Code 75005 (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

### **Yetttem**

The community of Yetttem is located on the east side of the San Joaquin Valley. Yetttem is a Census-Designated Place located in the northeast portion of Tulare County. It is bounded by Road 140 in the west, Road 144 in the east, and north and south of Avenue 384 and encompasses 0.2 square miles of land. Yetttem is directly served by State Route 201. The Yetttem Hamlet Development Boundary (HDB) area consists of 59.9 acres. The Land Uses within the HDB are described as follows: Urban development, including urbanized uses such as residential, commercial, public and quasi-public facilities, as well as industrial development occupy 92 percent of the 59.9 acres. The remaining 1 percent are lands dedicated for Right-of-way. In 2015, the population of Yetttem was 300.

Yetttem's population is younger than the median age throughout all of Tulare County. Yetttem's median age of 22.0 years is lower than the median age of the State of California (35.8 years). Public Resources Code 75005 (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average. Yetttem's median household income was not available from the 2011-2015 American Community Survey Income data.

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## 6.3 Legacy Communities (Legacy Development Boundaries)

**LEGACY PLAN 2017 UPDATE**  
**General Plan Amendment GPA 17-033, GPA's 17-025, 17-026, 17-027, 17-029, 17-030**  
**Mixed Use Overlay PZC 17-031**  
**Rezoning of Properties PZC's 17-025, 17-026, 17-027, 17-029, 17-030**  
**Use Permit/By Right Modifications PZC 17-032**  
**Addendum to the (2012) Tulare County 2030 General Plan Final**  
**Environmental Impact Report**  
**(Amending the 2030 General Plan Appendix D – the “SB244 Disadvantage Unincorporated**  
**Communities Assessment)**

### Requested Action

The Recommended Actions by the Planning Commission to the Board of Supervisors for the approval of the Legacy Plans 2017 Update include:

1. Legacy Plans 2017 Update Staff Report
2. Certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update. (See Attachment 1);
3. Adopt the Legacy Plan 2017 Update, including amendments to the Tulare County General Plan as follows: (Part I) General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities and Financing Elements, (Part II) Rural Valley Lands Plan, and Legacy Plans (Part III), (See Attachment 2);
4. Adopt an amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed-Use Combining Zone in Hypericum, Jovista, Matheny Tract and Tooleville. (See Attachment 3);
5. Amend the Legacy re-zoning plan to apply said zone to reclassify properties, within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Development Boundary. (See Attachment 4);
6. Adopt an amendment to Section 16 of Ordinance No. 352, to allow additional "by-right" uses within the LDB's of Hypericum, Jovista, Matheny Tract and Tooleville. (See Attachment 5);
7. Adopt GPA 17-033, for the Legacy Plan 2017 Updates as One General Plan Amendment, the first of the four allowed in 2017.

### Location

The proposed Project sites, or proposed general plan amendment areas, cover approximately 316 acres in area and encompasses the existing communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville. On November 17, 2015, the Board of Supervisors approved General

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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Plan Amendment GPA 2015-D inclusive of GPA 15-010 (SB 244 – Disadvantaged Communities Amendment to the Land Use Element). This “SB 244 Report” was made as required by law prior to the 2014 Housing Element Update. This change added the following list of “Legacy Communities” as part of the General Plan’s “Assessment Part II:” (See Figure 2 and Appendix D).

“(Additional Legacy Communities are identified in Disadvantaged Unincorporated Communities Assessment Part II as follows: (1) Calgro, (2) Citro, (3) Deer Creek Colony, (4) East Porterville, (5) El Monte Mobile Home, (6) Hawkins, (7) Higby, (8) Hypericum, (9) Jones Corner, (10) Jovista, (11) Lort, (12) Naranjo, (13) Paige, (14) Peral, (15) Ponca, (16) Sandspur, (17) Taurusa, (18) Tooleville, (19) Vance, (20) Venida, (21) West Venida, (22) Worth, (23) Yokohl, (24) Zante, (25) Badger, (26) Elderwood, (27) Globe, (28) Balance Rock, (29) California Hot Springs, (30) Camp Nelson, (31) Cedar Slope, (32) Fairview, (33) Hartland, (34) Idlewild, (35) Johnsondale, (36) Kennedy Meadows, (37) Panorama Heights, (38) Pine Flat, (39) Ponderosa, (40) Posey, (41) Silver City, (42) Sugarloaf Mountain Park, (43) Sugarloaf Park/Guernsey Mill, (44) Sugarloaf Village, and (45) Wilsonia.”

This change to General Plan also made changes to the Land Use Element Policies:

“LU 7.20 Encourage the extension, construction or upgrading services to identified Disadvantaged Legacy Communities. LU 7.21 Promote the extension, construction or upgrade of public water, sewer, stormwater drainage and structural fire protection services to identify Disadvantaged Legacy Communities as depicted in Disadvantaged Unincorporated Communities Assessment Report where feasible, and identify funding mechanisms that could make the extension, construction or upgrade of services and facilities to these communities and hamlets financially feasible.

It was made clear by Board Resolution that the “[... analysis in Disadvantaged Unincorporated Communities Assessment Report is not a General Plan Amendment but provides data to comply with SB 244. A General Plan Amendment will not be required to update information contained in Disadvantaged Unincorporated Communities Assessment Report.]”

However, based on the SGC Disadvantaged Communities Grant, the Matheny Tract has been added to the list of “Legacy Communities” that are listed as part of the Legacy Communities found in Appendix D. For each one of these Legacy Communities outreach and studies have been completed and are being included as a separate Legacy Project Report.

Legacy plan creation document (senate bill 244 disadvantaged unincorporated communities’ assessment)

Based on the demographics completed for these Community Plans, the analysis sets forth that all of these Legacy Plan Communities are considered *severely disadvantaged* communities and in need of infrastructure improvements. Moreover, through infrastructure analysis and public outreach in these Legacy Communities, the County was able to ascertain the communities’ infrastructure needs and verify the qualitative data through the Housing Element Action Program 9 and the SB 244 Report, with qualitative analysis as provided through public outreach.



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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In Tulare County, the General Plan has historically been developed on a county-wide basis or by large geographic sub-areas (such as rural valley, foothill and mountain), with development policies emphasizing county-wide and area-wide issues and concerns. In establishing land use planning policies on an area-wide basis, it has been recognized that several unincorporated communities, have localized land use needs and issues that should be addressed in a more specific manner particular to its community, geographic features, location of major roadways (such as State Route 99 and 65), population characteristics, availability of water, and other issues unique to the community's area.

Therefore, the legacy plans have been prepared with an emphasis on land use and circulation, and infrastructure planning.

Senate Bill (SB) 244 was signed into law in October 2011 by Governor Jerry Brown and it affects Local Agency Formation Commissions (LAFCOs), cities, and counties in California. Disadvantaged unincorporated communities (DUCs) and their infrastructure needs and deficiencies (specifically water, wastewater, stormwater, and fire protection) are the focus of the legislation. As it pertains to a county's jurisdiction, a DUC is defined as a "legacy" community that meets the following criteria:

1. Inhabited areas that contains 10 or more dwelling units in close proximity to each other.
2. Geographically isolated and has existed for more than 50 years.
3. A median household income that is 80% or less than the statewide median household income.

The purpose of SB 244 is to identify the infrastructure deficits that exist within DUCs and address the barriers to meeting the infrastructure needs.

Requirements: The specific requirements vary for LAFCOs, cities, and counties. LAFCOs are now required to consider DUCs when performing Municipal Service Reviews (MSRs). When cities are updating their spheres of influence (SOI), LAFCOs must also consider the existing infrastructure and needs of DUCs within the SOI. In addition, SB 244 places restrictions on LAFCOs' ability to approve city annexations greater than 10 acres when a DUC is adjacent to the area.

For cities, SB 244 requires they identify DUCs within their SOI and address their infrastructure needs. If the city approves an annexation greater than 10 acres and the area is adjacent to a DUC, the DUC must be annexed as well. Counties are also required to identify DUCs within their jurisdiction and address infrastructure needs. Both cities and counties are also now required to review and update the land use element of their general plans before the adoption of their next Housing Element. For each DUC, the city and county must provide a description and a map of each community. An analysis of the water, wastewater, stormwater, and fire protection infrastructure and needs must also be provided. Finally, the cities and counties must identify potential funding alternatives to extend these services to those DUCs that lack infrastructure. The intent of the SGC Disadvantaged Communities Grant was to increase the applicability of the Principles of AB 32 (2004) regarding the reduction of Vehicle Miles Traveled (VMT) and Green House Gasses (GHG), and simultaneously attempt to address the issues of SB 244 (2012) *Disadvantaged Communities*.

## **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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Tulare County's Approach: After review of the legislation, Tulare County has selected to exceed the minimum requirements imposed by SB 244... [This] report identifies the presence and location of existing infrastructure for each DUC. In addition to review of the water, wastewater, stormwater, and fire protection infrastructure, Tulare County reviewed the streetlight, sidewalk, and ADA curb ramp infrastructure. The location of streetlights, sidewalks, ADA curb ramps, and fire hydrants were mapped and listed in a matrix for each DUC. The emergency response times were calculated to each DUC from the nearest Tulare County fire station. All of the existing infrastructure work was performed in GIS software to assist the County in future mapping and analysis of the DUCs. [The plans discuss] projects expected to provide new and/or enhanced infrastructure as well as the unmet infrastructure needs of each DUC. [There is also discussion] of potential funding sources that could assist in providing needed infrastructure to the DUCs." (See the Tulare County SB 244 Study – November 2015).

### **Community Information (See Appendix B for Legacy Plans)**

#### **El Monte Mobile Home Park**

The drinking water services are provided by the El Monte Village Mobile Home Park (MHP). Testing conducted between 2004 and 2009 and provided to the Environmental Working Group (EWG) by the California Department of Public Health did indicate nitrite and nitrate levels over the legal and health limits, as well as alpha particle activity, lead, 1, 2-dibromo-3-chloropropane, and arsenic over the health limit. Environmental Protection Agency (EPA) violations were noted for nitrate levels over the MCL (2007-2008), failure to report information to the public or state agency in the Consumer Confidence Report (2004), and failure to regularly monitor nitrate (2007). Wastewater treatment is provided by septic systems and information is unavailable regarding any potential leaching and potential deficiencies may exist. The area is prone to flooding, does not have any public stormwater infrastructure and a deficiency is identified. Streetlights are provided in some areas of the community but are lacking in others, therefore the community is deficient in these areas. There are no sidewalks and ADA curb ramps, therefore the community is deficient in all these areas. El Monte Mobile Home Park does not include its own fire station; however, the nearby community of Dinuba has a fire station. No fire hydrants exist in the area which is considered a deficiency.

#### **Hypericum**

The drinking water is provided by private and/or community wells. Data related to any well monitoring in this area is unavailable. Therefore, it is difficult to determine the availability of groundwater in the area as well as the water quality and potential deficiencies may exist. Wastewater treatment is provided by septic systems and information is unavailable regarding any potential leaching and potential deficiencies may exist. The area is prone to flooding, does not have any public stormwater infrastructure and a deficiency is identified. There are no streetlights, sidewalks, and ADA curb ramps, therefore the community is deficient in all these areas. Hypericum does not include its own fire station and the nearest fire station is in Visalia, approximately 6.5 miles away which is considered a deficiency. In addition, no fire hydrants exist in the area which is also considered a deficiency.

## **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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### **Jovista**

The drinking water is provided by private and/or community wells. Data related to any well monitoring in this area is unavailable. Therefore, it is difficult to determine the availability of groundwater in the area as well as the water quality and potential deficiencies may exist. Wastewater treatment is provided by septic systems and information is unavailable regarding any potential leaching and potential deficiencies may exist. The area is prone to flooding, does not have any public stormwater infrastructure and a deficiency is identified. There are no streetlights, sidewalks, and ADA curb ramps, therefore the community is deficient in all these areas. Jovista does not include its own fire station, however, the nearby community of Richgrove has a fire station. No fire hydrants exist in the area which is considered a deficiency.

### **Matheny Tract**

Matheny Tract is located one mile south of the City of Tulare generally located south of Avenue 216 (Paige Avenue), east of Road 96 (Pratt Street) and west of I Drive and State Route 99. Matheny Tract is located just west of industrial land uses and a Union Pacific Railroad line running through Tulare County. Physically, the Community of Matheny Tract is divided by agricultural fields that separate approximately 256 households in North Matheny from 80 households in South Matheny. The Matheny Tract Community is predominantly surrounded by agricultural land.

Along the eastern boundary, running parallel to South “I” Drive is an irrigation ditch, above ground power lines and the Union Pacific Railroad tracks. Also, visible to the east is the City of Tulare corporation yard. Above ground power lines run parallel to Pratt Road that acts as the western boundary. The City of Tulare’s Wastewater Treatment and Reclamation facility is located about  $\frac{3}{4}$  miles northwest of Matheny Tract. In addition, an industrial area is located immediately northeast of the Community.

There is a canal within Tulare Irrigation District (TID) service area that bisects North and South Matheny. The Oakland Colony Ditch runs in a north south direction through North Matheny along the Canal Street corridor and extends in an east-west direction between North and South Matheny. There are two east-west crossings of the Oakland Colony Ditch - one along Wade Avenue and the other along Addie Avenue - in North Matheny.

Matheny Tract is an unincorporated Community located south of the City of Tulare with a population of approximately 1,212 and 320 households. The total land area is 0.043 square miles (2,820.5 people per square mile) and the elevation is 269 feet above sea level. Matheny Tract is also designated as a disadvantaged Community, which is, “a census designated place that has household median incomes that are less than 80% of the statewide household median income.

### **Tooleville**

The community is connected to water and wastewater systems that are assumed to be provided by Tooleville Water Company. Testing conducted between 2005 and 2008 and provided to the Environmental Working Group (EWG) by the California Department of Public Health did indicate nitrate and nitrite levels over the legal and health limits, as well as radium-228, alpha particle activity, and lead over the health limit. Environmental Protection Agency (EPA) violations were noted for

## **DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY**

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coliform bacteria levels over the MCL (2004, 2007, and 2008), nitrate levels over the MCL (2005-2007), failure to report information to the public or state agency in the Consumer Confidence Report (2004-2005), and failure to monitor coliform bacteria (2004-2006). The area is prone to flooding but does have ponding basins located nearby. It is not clear whether the public stormwater infrastructure is sufficient and a potential deficiency is identified. There is one (1) streetlight which is insufficient for the area. There are no sidewalks and ADA curb ramps, therefore the community is deficient in all these areas. Tooleville does not include its own fire station, however, the nearby city of Exeter has a fire station. No fire hydrants exist in the area which is considered a deficiency.

# ASSESSMENT OF HOUSING CONDITIONS AND HOUSING CONDITIONS SURVEY

## Housing Analysis

Every five years Tulare County in California (“the County”), is required to conduct a review of a State mandated program called the Housing Element. As a part of this review the County is required to carry out housing condition surveys to determine the status of housing conditions. The primary focus of the County’s Housing Element is the provision of decent, safe, sanitary and affordable housing for current and future residents of the unincorporated areas of Tulare County. The Housing Element is a comprehensive assessment of current and future housing needs for all segments of the County’s population living in unincorporated areas, as well as a program for meeting those needs. It serves as a policy guide to address these issues, as well as the comprehensive housing needs of the unincorporated areas of Tulare County during the 2015 to 2023 planning period and beyond.

The purpose of this report is to determine if there is a statistically significant relationship between the percentage of substandard housing within unincorporated communities of Tulare County, California, with median income, poverty and unemployment rates and based on the Outreach conducted and the citizen’s reported perceptions of their own housing. The report also compares a perception survey of how persons within the County based on the Outreach conducted for this grant view the condition of their housing compared to the housing condition surveys performed for the County’s Housing Element. Furthermore, this report focuses on the consistency between the four housing condition surveys prepared for the Housing Element and determines if there is a strong correlation between the surveys.

This research data is to be used to assist in future grant applications to various funding sources, including the Community Development Block Grant (CDBG) Program, which provides housing rehabilitation, first-time homebuyer assistance, new residential construction, public works and other activities that will benefit low and moderate-income households in the unincorporated areas of Tulare County.

## **History**

The Housing Act of 1949 established a national housing goal for a “decent home and a suitable living environment for every American family.” One of the primary goals of both the County and the State of California is to maintain an adequate stock of safe and sound affordable housing. The goal of Tulare County is as follows: “*Attainment of a suitable, affordable, safe and sanitary living environment for every present and future resident in the unincorporated area of Tulare County, regardless of race, age, religion, sex, marital status, ethnic background, and source of income or personal disability.*”

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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Furthermore, this goal falls in line with the State of California Government Code Sections 65580 to address the basic tenants of Housing Element requirements, the pertinent section for this research being:

(a) The availability of housing is of vital statewide importance, and the early attainment of decent housing and a suitable living environment for every Californian.

The housing condition survey data assessed to date considers three main factors: a) the “soundness” of condition of the housing; b) the geographic regions used to differentiate between communities, and c) the level of poverty among the communities.

The “soundness” was computed using an evaluation of the exterior condition of all sampled residential structures, based on the State of California Housing and Community Development (HCD) criteria set forth in Chapter 16 of the Community Development Block Grant Management Manual as follows:

**Sound:** Housing units that appear new or well-maintained and structurally intact. The foundation should appear structurally undamaged and there should be straight roof lines. Siding, windows, and doors should be in good repair with good exterior paint condition. Minor problems such as small areas of peeling paint and/or other maintenance items are allowable under this category.

**Deteriorated:** Housing units in need of replacement of one (1) or more major components and other repairs, such as roof replacement, painting, and window repairs. The Deteriorated classification is divided into three (3) sub-categories: Minor, Moderate, and Substantial Rehabilitation.

**Minor:** Housing units that show signs of deferred maintenance, or which needs only one (1) major component such as a roof.

**Moderate:** Housing units in need of replacement of one (1) or more major components and other repairs, such as roof replacement, painting, and window repairs.

**Substantial:** Housing units that require replacement of several major systems and possibly other repairs (e.g. complete foundation work, roof structure replacement and re-roofing, as well as painting and window replacement).

**Dilapidated:** Housing units suffering from excessive neglect, where the building appears structurally unsound and maintenance is non-existent, not fit for human habitation in its current condition, and may be considered for demolition or at minimum, major rehabilitation will be required. A unit is considered dilapidated if it is deteriorated beyond the point of rehabilitation being economically feasible.

Existing data, drawn from the surveys carried out in 1992, 2003, 2009 and 2015, indicates that there is a nearly equal percentage of housing in a “Sound” condition (45%), as in a “Deteriorated” condition (45%), throughout the unincorporated areas of Tulare County. There is a much lower percentage indicated as “Dilapidated” (10%). Please refer to Table 11 that shows the breakdown of housing conditions within the geographic areas of the County.



# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

**Table 11 - Housing Conditions (including Minor, Moderate, and Substantial) by Survey Area**

Geography	Sound		Deteriorated		Dilapidated		Total
	Units		Units				
<b>Visalia/Farmersville/ Exeter/ Sequoia Park/Dinuba/ Cutler/Orosi Market Area</b>	<b>1,934</b>	<b>53%</b>	<b>1,493</b>	<b>41%</b>	<b>242</b>	<b>6%</b>	<b>3,665</b>
<b>Lindsay/Strathmore/ Porterville/ Foothills/Southeast Tulare County Market Area</b>	<b>811</b>	<b>41%</b>	<b>837</b>	<b>43%</b>	<b>315</b>	<b>16%</b>	<b>1,963</b>
<b>Tulare/Southwest Tulare County Market Area</b>	<b>540</b>	<b>33%</b>	<b>960</b>	<b>57%</b>	<b>171</b>	<b>10%</b>	<b>1,670</b>
<b>Total</b>	<b>3,285</b>	<b>45%</b>	<b>3,290</b>	<b>45%</b>	<b>728</b>	<b>10%</b>	<b>7,298</b>

Tulare County Housing Element 2015

The geographic data was collated using a survey of over 7,500 residential units, tabulated into fifty (50) discrete areas including twenty (21) census designated places and twenty-nine (29) other unincorporated communities. Additionally, three (3) island/fringe areas in and around incorporated cities were surveyed and other residential structures in the County general.

This illustrated the disparity in soundness between the housing stock adjacent to the incorporated cities and housing stock in the unincorporated communities of the County. Sound housing is predominant in the fringe areas and residential tracts, is higher in the census designated places, but is lower in the smaller unincorporated areas of the County. The rates of deterioration and dilapidation are more prevalent in the unincorporated areas and these are the units determined to be substandard as used in this report. Please refer to Table 12 on the next page.

## Hypothesis and Rationale

The hypothesis for this report is “communities with a lower median income, a higher rate of unemployment and/or higher poverty rate equate to a higher level of substandard housing”. The rationale for this is that a low median income, high poverty and high unemployment means less available income that is available for maintenance and repairs, causing the housing to deteriorate over time, and eventually become deteriorated or dilapidated. To determine significance, the recognized standard level of risk of 5% was used.

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

Three factors may have a statistically significant effect on substandard housing within the County: Median Income, Poverty Level and Unemployment.

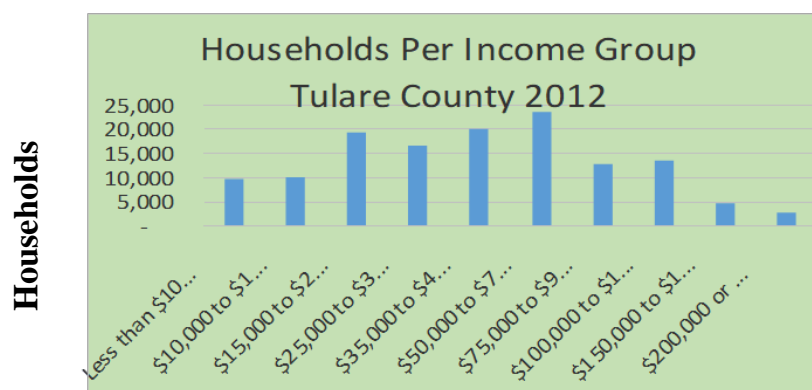
Tulare County's median income falls well below the median income of the state. Tulare County's median income, as determined by the 2010 census, is \$43,550, compared to the states median income of \$61,632. The majority of households within the unincorporated County fall below \$40,000. Please refer to Figure 9.

**Table 12 – Percentages of Substandard Housing Units – Unincorporated Communities in Tulare County (1992 -2015).**

Community Number	Unincorporated Community	1992 Survey Results	2003 Survey Results	2009 Survey Results	2015 Survey Results
1	Allensworth		63	95	38
2	Alpaugh	62	72	85	54
3	Cutler-Orosi	30	14	17	76
4	Delft Colony	67	78	83	53
5	Ducor	30	40	77	83
6	East Orosi	64	81	87	59
7	East Porterville	25	49	63	55
8	Earlimart	53	47	33	41
9	Elderwood	16	34	25	26
10	Goshen	14	24	54	36
11	Ivanhoe	28	31	37	65
12	Lemon Cove	23	48	51	75
13	Lindcove	61	56	63	63
14	London	69	62	85	55
15	Patterson Tract	37	28	56	60
16	Pixley	33	54	55	69
17	Plainview	64	80	87	58
18	Poplar/Cotton Center	72	57	57	28
19	Richgrove	51	54	24	40
20	Seville	63	67	59	55
21	Springville	10	48	60	34
22	Strathmore	27	52	81	65
23	Sultana	31	62	87	32
24	Terra Bella	71	60	65	71
25	Teviston	81	71	64	53
26	Three Rivers	1	14	10	16
27	Tipton	27	24	27	62
28	Traver	52	67	73	58
29	Woodville	51	48	79	53
30	Yettem	83	92	100	30

Source: 1992, 2003, 2009, 2015 Tulare County Housing Survey of Unincorporated Communities

Figure 9 - Households by Income Group



Source: 2010-2012 US Census, American Community Survey, 3-year Estimates

## Income in US Dollars

Poverty levels also may have a significant effect on substandard housing. Existing data derived from the 2010 Census determines that overall, approximately 25% of the County's population lives below the poverty level. Tulare County's rural communities continue to have lower incomes and a higher level of poverty overall.

In the entire County, 19.5 % of families live below the poverty level. However, in some of the rural communities, that increases to over 51 %. Figure 9, clearly shows that most of the unincorporated areas of the County exceed the countywide percentage (source: 2010 Census; 2007-2011 American Community Survey).

Unemployment is another factor that may contribute to substandard housing within the County. The County's unemployment rate is 19%, far above the state unemployment rate of 12.1% (2010 Census data). Unemployment is a contributing factor to the amount of income a household has and thus less money available to provide for sound housing conditions.

## Data and Methods

Though these factors appear interrelated when it comes to median income, poverty, unemployment and substandard housing within the County, this report provides a statistical representation of this relationship and determines if there is a statistical significance with this relationship. The analysis used to determine statistical significance is *Regression Analysis, Anova Single Factor, Means, Medians, and Person's Correlation*. Note: since the original housing condition surveys carried out through the years were not consistent, in some cases the computations were forced to use the closest comparable year, rather than an exact match.

For housing conditions, initially a histogram (Figure 10) was used to plot the percentage of substandard housing collated in the surveys from 1992 to 2015, for the unincorporated communities of the County. This was then accumulated into a scatterplot (Figure 11), to show the range of housing

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

conditions over the communities, and the cumulative % of substandard housing. For ease of visibility, this was translated into a pie chart (Figure 12). This, therefore, represents the percentage of substandard housing over the last 15 years. As shown on the chart, a substantial portion of the County's population lives in substandard conditions – with the largest groupings showing between 41-80% of substandard housing (Table 11).

Figure 10 – Histogram of Substandard Housing

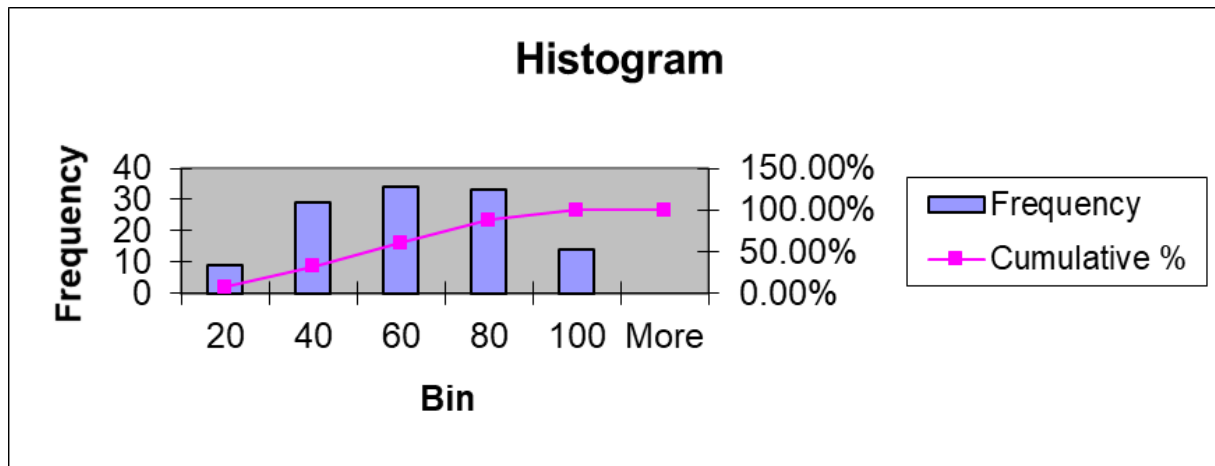
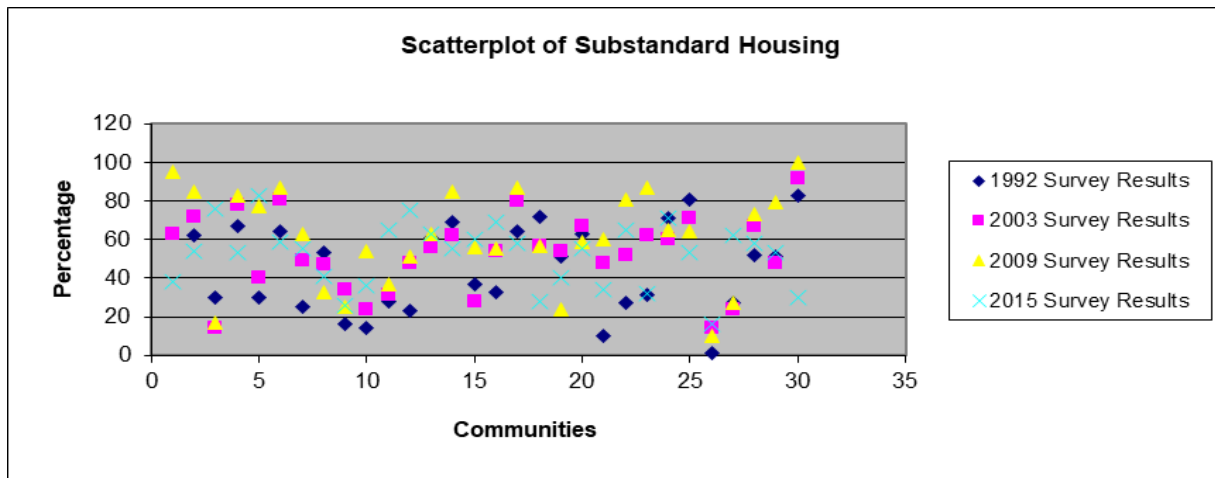
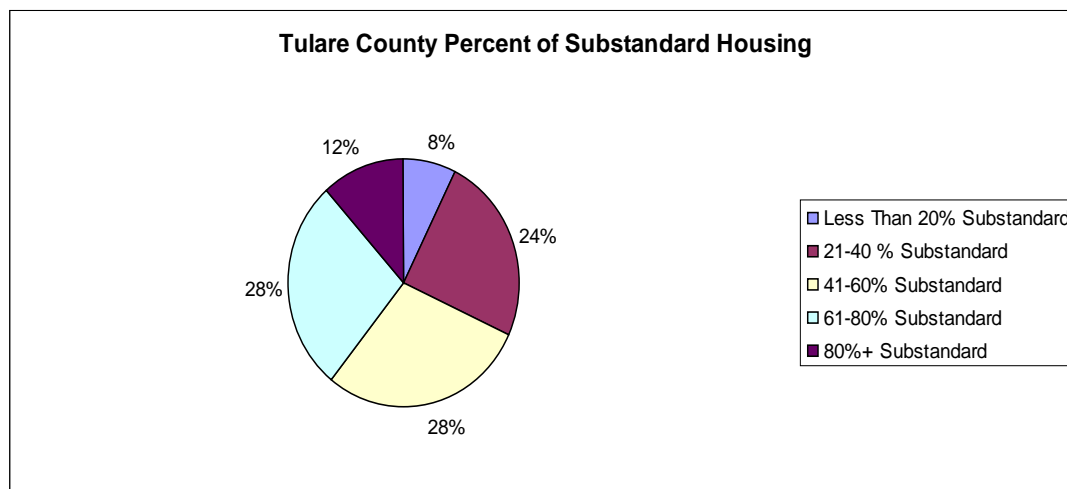


Figure 11 - Scatterplot of Substandard Housing



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Figure 12 - Tulare County Percent of Substandard Housing



*Mean* and *Median* statistics were computed for all the survey years, showing an average of 52.7% and a median of 55% substandard housing units within the unincorporated communities. The 2009 survey had the highest mean of 61.3% and median of 63% substandard housing units. The *Standard Deviation* was also calculated for each year of the survey. 2009 had the highest standard deviation (Table 13).

Table 13 - Housing Condition Surveys, Mean, Median, and Standard Deviation

	Mean	Median	Standard Deviation
1992	44.7	51.0	22.8
2003	52.6	54.0	20.0
2009	61.3	63.0	24.5
2015	52.1	55.0	16.6
Total	52.7	55.0	21.7

The relationship between survey data was computed to determine if there was a correlation between the data. A *Person's Correlation* statistic was used to determine a relationship between previous surveys and the 2015 survey. The data showed a weak direct correlation between the 1992 and 2009 survey data compared to the 2015 survey data. This weak correlation between the 1992 and 2009 survey data with the 2015 data may represent a different contractor performing the surveys. It also showed a strong direct correlation between the 2003 survey and the 2015 survey. The same contractor performed the 2003 and 2015 survey, and this proved to be a strong direct correlation (Table 14).

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

**Table 14 - Housing Condition / (2016 - 2017) Outreach Survey Correlation**

<b>Years Compared</b>	<b>1992-2015</b>	<b>2003-2015</b>	<b>2009-2015</b>
<b>Correlation</b>	<b>0.09</b>	<b>0.79</b>	<b>0.11</b>

An *Anova Single Factor* test was also computed for all four surveys to determine variance and significance between the data. This test also confirmed the similarities between the 2003 and 2015 surveys. The 1992 and 2009 surveys both had much larger variances (522+) than the 2003 and 2015 surveys. The test confirmed a significant difference in the relationship between the surveys ( $F=3.03$ ,  $P=0.032$ ). This belies that there is a statistical difference between the tests, and suggests that some other factor is affecting the survey's relationship. The difference in survey techniques between the contractors could account for this significance.

A survey was also taken within the communities asking households their perception on housing conditions of their own house during 2016 and 2017. A correlation statistic was calculated for this survey in relationship with the 2015 housing conditions survey. The calculated correlation between the two resulted in negative weak correlation (-0.17). A regression analysis was then used to determine the level of significance in rejecting the null hypothesis. This analysis discovered no significant deviations from the mean from any of the Housing Condition surveys performed by consultants. 1992 data was not used due to missing data in some communities (Table 15).

**Table 15 - Housing Condition Perception Survey Compared to Housing Condition Community Survey**

	<b>T Stat</b>	<b>P Value</b>
<b>2003</b>	<b>-0.77</b>	<b>0.45</b>
<b>2009</b>	<b>0.20</b>	<b>0.85</b>
<b>2015</b>	<b>0.49</b>	<b>0.49</b>

With the housing conditions analyzed, the data was then computed to analyze the potential impact of income, poverty level and unemployment rates. A regression analysis was performed with the 2009 survey to compare the Median Household Income, Poverty Rate and Unemployment Rate. The results of this analysis were that these factors did not have a significant effect on the neighborhoods analyzed. Median Income had the most effect on substandard housing, however, it did not fall within the probability of being significant ( $t=-1.36$ ,  $p=0.19$ ). See Table 16.



## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

Table 16 - Housing Condition Survey Regression Statistics

	T Stat	P Value
Median Household Income	-1.36	0.19
Poverty Percent	0.11	0.91
Unemployment Percent	0.74	0.47

### Results

When considering the results of the analyses carried out, it is important to note the differences in agencies performing some of the data collection, and the apparent effect this may have had on some of the statistics, and the variation of completeness of the data across the different years of data collation.

The data reaffirms that a substantial portion of the County's population lives in substandard conditions. The analysis of the Housing Condition survey, based on the most correlative data from 2003 and 2015, indicates little change in the condition of housing over that period. The *Mean, Median* and *Standard Deviation* analysis did reflect an increase in substandard housing through the first three sets of survey data. The most recent survey (2015) reflected a decrease in the overall median statistic around substandard housing, but only changed from 51 to 55.

The correlation analysis indicated that where the same consultants performed the surveys, a strong correlation existed (2003 and 2015). This was less apparent where different consultants performed the surveys (1992 and 2009), a factor to be considered when analyzing the results.

The correlation and regression analyses carried out to compare the residents' perception of housing conditions with the County-initiated surveys did not exceed the level of significance required to reject the null hypothesis.

The analyses carried out on sub-standard housing, and the potential effect of median income, poverty and unemployment rates, did not exceed the level of significance to reject the null hypothesis, with P-values of 0.19 (median income), 0.91 (poverty) and 0.47 (unemployment).

### Conclusion

In conclusion, this report documents that the relationship between median income, unemployment and poverty did not have a significant effect on substandard housing, though they did have some effect upon it. This report also concludes that the correlations between substandard housing surveys are not very consistent in their classifications, but overall were within the 95% level of significance to confirm the null hypothesis. The perception survey in 2017 further validated the results of the

## DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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previous County surveys in terms of level of substandard housing; this result did not reach the level of significance to reject the null hypothesis.

This report highlights the need for housing rehabilitation in unincorporated communities throughout the county, regardless of level of income, poverty and unemployment. The data will be used to update the County's housing data and to demonstrate the need for housing rehabilitation in the County's General Allocation CDBG applications.

This report does not describe the relationship between substandard housing and the cost of housing, or the number of rental units versus home owners, foreclosures, homelessness and farmworker migration.

## FINAL REPORT CONCLUSIONS

- (1) Outreach was successful in providing the County with the concerns of the people within the disadvantaged communities. Despite attempts to reach people electronically because of the lack of internet or phone service and internet capabilities in these communities, or available community centers, the old-fashioned form of going door to door was still required. The surveys were a greater resource in providing information to the County than merely through discussions, and the consultant shared this information with the County through Share Point. This information qualified a lot of the quantified information already garnered in the *Housing Element / SB 244 Report - Infrastructure Studies*, and has great potential to assist with grants in the future.
- (2) Infrastructure and services are deficient in all these communities. New statewide policies such as SGMA and the LAMP programs will have a negative effect up on these communities. Quantitatively the biggest issues are water / water quality and waste water. However, the citizens concerned were mostly around the road conditions safety and internet services. The water and sewage issues were high priority to them, but they saw those issues as solvable over the long term. They were all concerned about roadway flooding and other policing and fire service response times.
- (3) The proposed zone changes were considered under CEQA. The County did a Finding of Consistency and Addendum to the 2012 GP EIR for the 21 community plans. Environmentally, the GHG/Air Quality analysis indicated that these changes will be a benefit in Vehicle Miles Traveled and fall under the GHG / Air Quality thresholds of the Air District and Tulare County Climate Action Plan.
- (4) The Community Plans and Zone Changes fulfill the requirements of the 2012 General Plan and are in exceedance of the requirements for the Grant. The Zone Changes will increase commercial zoning 150%, from a less than a percent of all zoning within the development boundaries, to 1.5 million square feet of commercial zoning. The reduction of use permit requirements and the mixed-use zoning overlay have also been applied to these communities in order to foster economic development.

**Appendices:**

**Appendix A – Outreach Reports**

**Appendix B – Community Plans**

Alpaugh Community Plan (*separate document*)  
East Orosi Community Plan (*separate document*)  
London Community Plan (*separate document*)  
Richgrove Community Plan (*separate document*)  
Sultana Community Plan (*separate document*)

**Appendix C – Hamlet Plans**

Allensworth Hamlet Plan (*separate document*)  
Delft Colony Hamlet Plan (*separate document*)  
East Tulare Villa Hamlet Plan (*separate document*)  
Lindcove Hamlet Plan (*separate document*)  
Monson Hamlet Plan (*separate document*)  
Seville Hamlet Plan (*separate document*)  
Teviston Hamlet Plan (*separate document*)  
Tonyville Hamlet Plan (*separate document*)  
West Goshen Hamlet Plan (*separate document*)  
Yetter Hamlet Plan (*separate document*)  
Waukena Hamlet Plan (*separate document*)

**Appendix D – Legacy Plans**

El Monte Mobile Village Legacy Plan (*separate document*)  
Hypericum Legacy Plan (*separate document*)  
Jovista Legacy Plan (*separate document*)  
Matheny Tract Legacy Plan (*separate document*)  
Tooleville Hamlet Plan (*separate document*)

**Appendix E – Housing Condition Study**

# DISADVANTAGED COMMUNITIES INFRASTRUCTURE AND PLANNING POLICY STUDY

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## **Appendix A – Outreach Reports**

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

##### **Allensworth**

The Tulare County community of Allensworth is located in the southwestern corner of Tulare County, in the old Tulare lakebed area. Allensworth is about 8 miles west of Earlimart, along Highway 43. The community is located immediately south of the historic settlement, which is now a state historic park and therefore not occupied. Allensworth is a Census Designated Place (CDP) with the latest 2010 United States Census Bureau decennial census indicating a population of 471. This community is considered disadvantaged by at least two criteria. The most recent US Census Bureau American Community Survey (2010-2014) Five Year Estimate of the Median Household Income for the Allensworth CDP is \$25,625 with a margin of error of \$12,095. This is 41.7% of the Statewide Median Household Income (MHI) for that period, well below the threshold of less than 60 percent of MHI to qualify as a severely disadvantaged community. In addition, Allensworth is listed in the top 5 percentile in Cal EnviroScreen 1.1 and top 10 percentile in Cal EnviroScreen 2.0.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) On January 6 and January 9, 2016 SHE staff went door to door to distribute flyers to homes in Allensworth.
- 3.) SHE staff partnered with Allensworth Progressive Association, a local service organization to create community involvement, gather support and to spread the word about the importance of attending this meeting. This group worked together with SHE to distribute fliers on January 9, 2016.
- 4.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about its importance and reviewed the benefits of participating in the SGC community meeting.
- 5.) SHE partnered with the Allensworth Elementary School District to distribute flyers to every student in the school district and posted flyers at the school site.



6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted around town on street signs and fences.

### **Community Meeting**

The Strategic Growth Council held a community input meeting in Allensworth on Tuesday, January 12, 2016 at 6:00pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held at the Allensworth Community Center located at 8123 Ave 36 Allensworth CA 93219. This is an ideal location because most residents know where the community center is located and they feel welcomed and comfortable there. Over 35 Allensworth residents attended the meeting and 30 surveys were collected. Present were representatives from the Allensworth Community Services District, Allensworth Elementary School District and Tulare County Sheriff's Department.



### **Community Survey**

A detailed survey was developed in efforts to gather specific community information about different areas of improvement. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/ concerns:

#### **Priority Concerns**

- 1. Water Issues**
- 2. Road Conditions**
- 3. Natural Gas**

#### 4. Energy Sources

#### 4. Sheriff Patrol

#### 5. Home Repair Assistance

#### 6. Heat

#### 7. Sewer

#### 8. Internet Access



### Water

Water is the highest priority for the residents of Allensworth. It is a long time problem in town and residents are very aware and invested in solving water related problems. Allensworth has had arsenic contamination since the 1960's. To residents, this is a huge unresolved problem. A regional project could be a good solution for this community; the Strategic Growth Council grant awarded to Tulare County in 2012 will investigate the feasibility of a regional solution for Allensworth and Alpaugh, building on a potential partnership with Angiola Water District south of Corcoran. Working with the neighboring community of Alpaugh through some form of consolidation could be a good way to resolve Allensworth's water problems. Although Allensworth is located at a distance of several miles from Alpaugh, the two communities face similar problems with regard to economy of scale, contamination and revenue deficiencies. At our SGC meeting, people had questions about how the possible consolidation project will affect them. Residents were advised and encouraged to attend the next consolidation project meeting scheduled on Thursday January 14, 2016 at 6pm at the Allensworth School Cafeteria. The Strategic Growth Council grant is a fantastic opportunity to explore this option, and should be coupled with the Tulare Lake Basin Disadvantaged Community Pilot Study to advance some solutions for the region. The other big unresolved problem is the Community Services District's moratorium on new water connections due to concern over insufficient water supplies.

The chart below demonstrates the Arsenic Levels over twelve years.

Allensworth Community Services District Arsenic Levels in Active Wells Arsenic MCL = 10 ppb		
Date	East Well #1 (ppb)	West Well #2 (ppb)
3/6/1990	17	
11/23/1993	16	
11/4/1996	15	
9/28/1999	10	
10/24/2002	9	
10/26/2005	11	
9/21/2007	11	
12/11/2007	12	
12/13/2007	13	
3/26/2008	13	
8/7/2008	10	
12/19/2008	11	
11/30/2010	12	13
3/29/2011	14	14
6/13/2011	12	12
8/8/2011	10	6
11/16/2011	11	11
4/5/2012	12	7
9/13/2012	12	9
<b>Times Exceeding MCL</b>	<b>18</b>	<b>4</b>
Old off-line well at storage tank site		
6/6/1996	65	

## Road Conditions

Road conditions are of high concern to Allensworth residents. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on Allensworth roads. Residents report that roads are narrow and that two passing cars each other can hardly fit on one road at the same time. According to residents, Allensworth roads are not regularly maintained and the few times that roads have been serviced, they consider the repair work to be poor because the roads were patched and not actually repaired.

The following roads are a priority (and need the most work) to the residents:

- Avenue 32
- Avenue 36
- Road 24
- Road 28
- Road 84

## **Natural Gas**

Allensworth residents expressed a need for natural gas service. Residents believe this can be done by extending service from an existing natural gas transmission line located on the southern end of the community on Avenue 24. A majority of the residents reported that they use propane as a source of fuel. Families reported the high cost of propane is a burden for them, this cost results in people not heating their homes to a comfortable temperature. Parents are especially concerned about this because it can lead to increased cold and flu in small children, as well as in elderly people. Families report that they sometimes use their stove tops to warm their homes, this is safety hazard as people report sometimes falling asleep with the stove top or oven on.

## **Energy Sources**

Residents expressed an interest for alternative energy sources such as Solar Panels and other energy saving alternatives. Allensworth residents urge the State and the County to invest in solar powered project in town whenever possible. People request information about low income or government programs and funding opportunities that can help residents afford the cost of transitioning to alternative energy sources.

## **Sheriff Patrol**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Allensworth. Residents report that the TCSD response time is unacceptable and that there is little to zero police patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go unresponded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate and makes criminals feel unstoppable in that area of Allensworth. Increasing the Sheriff Patrol in Allensworth is a priority to residents.

## **Home Repairs**

Allensworth residents are concerned because many of their neighbors are living in homes that desperately need repairs. People report that some of their neighbors are living in houses and trailers that are substandard and are not up to building code. This is especially concerning because families with children and elderly people live in many of these dwellings. Some older homes have deteriorated and are facing situations such as bad flooring, no functioning heat or cooling system, no running water or sewer. Home owners and landlords are requesting resources that will help with the home repair costs.

## **Heaters**

Many Allensworth residents do not have a standard home heating system because they do not have natural gas. Residents report the need for safe, reliable heating units. Families report using their stove tops to warm their homes, creating safety hazards as people report sometimes falling asleep with the stove top or ovens left on. The San Joaquin Valley Air Pollution Control District has a program that supplies families with

safe wood stoves, this may be one way to help to safely warm the homes of Allensworth residents.

### **Sewer System**

A community sewer system is a priority concern for many residents. Currently the community is dependent on individual septic tanks for sewage disposal. Residents stated that they would prefer to be on a community wide sewer system. They report that they have been trying to make this happen for many years and although they are willing to pay a monthly fee for the service, they have not been successful at securing funding to make this a reality in Allensworth.

### **Internet Access**

Most families in Allensworth do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. This is a major problem to residents. Without access to the internet, Allensworth residents are unable to do very basic things such as online job hunting, apply for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Allensworth.

### **Public Services**

Sheriff –Allensworth is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Allensworth and the nearest Sheriff substation is located in Pixley, 15 miles away.

Fire – Tulare County Fire, nearest to the Alpaugh Fire Station, located in Alpaugh, CA – 7 miles away.

Schools – Allensworth Elementary School District (K-8), located in Allensworth, is part of Tulare County Office of Education.

Libraries – The nearest library is located in Alpaugh, CA - 7 miles away

Parks – The nearest community park is located in Alpaugh, CA - 7 miles away

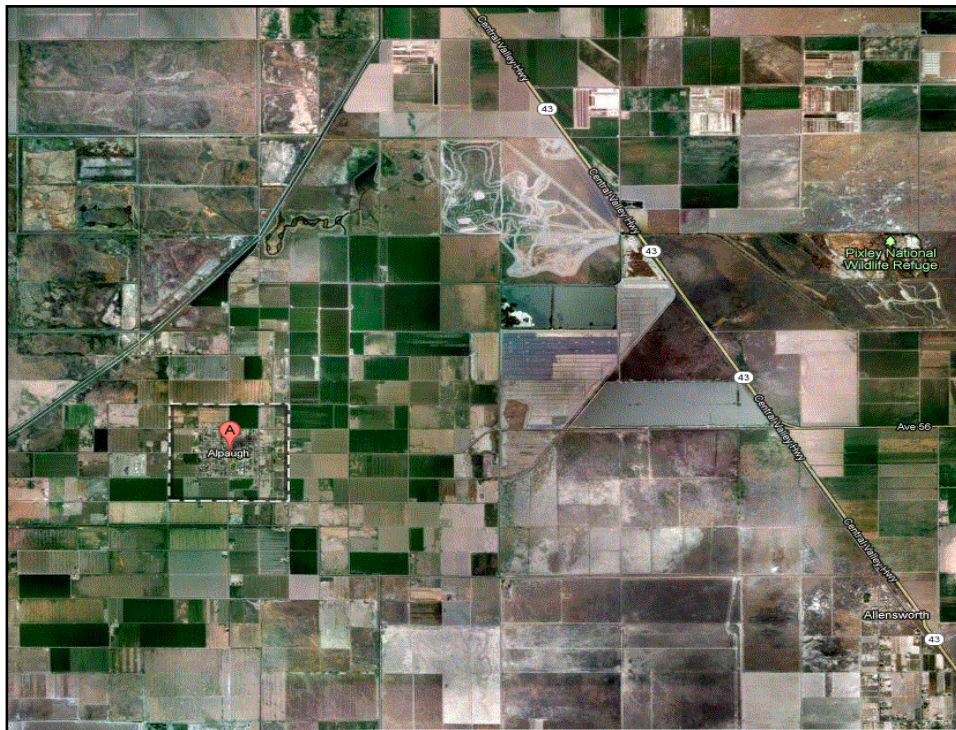


## Sustainable Communities Strategy

### Strategic Growth Council

#### Outreach Report

#### Alpaugh



Alpaugh is a rural community located about 3.5 miles west of State Highway 43 (about 13 miles west of the community of Earlimart) near the southwestern border of Tulare County with neighboring Kings and Kern counties. Alpaugh is a Census Designated Place (CDP) with a total area of 1.0 square mile with a population of approximately 1,050. This community is considered disadvantaged by at least two criteria. The most recent US Census Bureau American Community Survey (2010-2014) Five Year Estimate of the Median Household Income for the Alpaugh CDP is \$26,927 with a margin of error of \$4,678. This is 44 percent of the Statewide Median Household Income for that period, well below the threshold of less than 60 percent of MHI to qualify as a severely disadvantaged community. In addition, Alpaugh is listed in the top 10 percentile of both Cal Enviroscreen 1.1 and 2.0.



## **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff went door to door to distribute flyers to every home, business and school in the community of Alpaugh. Door to door outreach was done on August 30, September 1, and September 2, 2015.
- 3.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- 4.) SHE partnered with the Alpaugh Unified School District to spread the word about the meeting; meeting flyers were sent home from school with each student.
- 5.) Meeting invitations were sent to the Boards of Directors of the Alpaugh Unified School District, the Alpaugh Community Service District and the Alpaugh Irrigation District.
- 6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner store, local businesses and the Alpaugh post office.

## **Community Meeting**

Self-Help Enterprises conducted a community meeting (with five SHE staff members present) in Alpaugh on Thursday, September 3, 2015 at 6:00pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held in the cafeteria of the Alpaugh Elementary School located at 5313 Road 39 in Alpaugh. This is an ideal meeting location because residents know where the school is located and most feel welcomed and comfortable being there. The purpose of the meeting was to discuss Alpaugh improvement needs, gather community data, and report the findings of the meeting in a final report. Nineteen (19) Alpaugh residents attended the meeting. Sixteen (16) community surveys were collected.



## **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Alpaugh SGC community input meeting, residents concluded that the following is a list of Alpaugh priority improvement needs

### **Priority Improvements**

- 1) Road Conditions & Street Safety
- 2) Community Safety
- 3) Medical Care Access
- 4) Community Sewer System
- 5) Local Market
- 6) Internet Access
- 7) Community Resource Center
- 8) Loose Dogs
- 9) Natural Gas
- 10) County Park Management

### **Road Conditions and Street Safety**

Alpaugh residents expressed that street safety, including the need for sidewalks, street lighting, speed limit signs, stop signs, and the need for enforcement of traffic laws are

the highest priority improvement needs in Alpaugh. Survey results show that most residents are not satisfied with road conditions. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on Alpaugh roads. Residents report that roads are narrow and that two cars barely fit on one road at the same time. Alpaugh roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired. 100% of the respondents are not satisfied with the conditions of the roads. 100% of the respondents described the roads to be in “poor” conditions.

The following roads are a priority to the residents:

- Tule Road
- Avenue 54
- Ellis Road
- Avenue 50

#### **Survey comments regarding road conditions:**

**“Very bad road conditions”**

**“Roads have not been fixed in a many years”**

**“Dark roads, no street lights”**

**“Street lighting is not safe for children”**

**“All of the roads bumps and pot holes make for dangerous driving conditions”**

**“Road need sand or patching”**

**“Roads are so bad, we need a four wheel drive to drive down the road”**

**“Road conditions cause a bumpy ride and many burst tires”**

**“The roads around the school are bad”**

**“Caution signs needed around the school”**

**“Narrow roads, only room for one car”**

#### **Sidewalks**

The community of Alpaugh has too few sidewalks. There are sidewalks around the north and west side of the school, and a short stretch of sidewalk in front of the library, post office and store only. The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Alpaugh traffic travels at a high rate of speed (see next section), and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Traffic Law Enforcement**

Residents report that in Alpaugh, some vehicles travel at an unsafe rate of speed and the roads in town have few speed limit signs. This adds to the problem of speeding. Alpaugh residents believe that by installing speed limit signs to clearly display the legal speed limit, this issue could be improved. A speed limit sign is especially needed on Alpaugh's main road, Road 54. Road 54 has the most traffic, including heavy truck traffic, and the most speeders. Along with speed limit signs, residents feel a need for better enforcement of traffic laws. The California Highway Patrol and the Tulare County Sheriff's Department are rarely seen in the community of Alpaugh; the result of this is that people are rarely cited for violating traffic laws.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Alpaugh. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Community Safety**

Community safety is a major concern and ranked among the highest concerns for the residents of Alpaugh. Safety concerns stem from the Tulare County Sheriff Department's delayed response time to 911 emergency calls. Residents report that on multiple occasions they have called the Tulare County Sheriff Department at the time of an emergency and the sheriff department either arrived roughly three or four hours after the call or, at times, they did not respond at all. Families expressed that they do not feel safe living in the community of Alpaugh. Multiple residents stated that they have had home burglaries and other emergencies when they had to resort to using personal weapons as a form of protection because the Sheriff Department did not respond in a timely manner. During the SGC community meeting, the community talked about a need for more Sheriff's patrols in Alpaugh. Safety at the County's Park in Alpaugh is also a concern. Residents reported that two young girls were raped in the park restroom and that families are scared to use the park. Residents feel that if there was more of a Sheriff's presence and more police patrol this would be noticed by criminals and deter them from criminal activity. This law enforcement presence would also help families feel safer in town.

## **Sheriff Presence**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Alpaugh. Residents report that the TCSD response time is unacceptable and that there is little to zero police patrol within the community. Residents are worried

about the rate of unreported crimes and crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate and makes criminals feel unstoppable in that area of Alpaugh. Alpaugh residents are asking for increased Sheriff presence in their community.

### **Medical Clinic**

Alpaugh residents do not have access to medical care within the community. The nearest medical clinic is approximately 13 miles away in Earlimart. Most residents report traveling to Delano, 20 miles, Corcoran, 17 miles and Tulare 35 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. The Tulare County Area Transit does not make regularly scheduled stops in Alpaugh, the TCAT will pick up Alpaugh residents by appointment when calling the day before. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Delano, 20 miles away, and it takes approximately 30 minutes for the nearest ambulance to get to Alpaugh from Delano.

### **Sewer**

There is no community wide sewer system in Alpaugh. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the Alpaugh residents are in favor of creating a community wide sewer system.

### **Internet Access**

Most families in Alpaugh do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Alpaugh.

### **Community Resource Center**

Alpaugh does not have a community hub. The community lacks many resources and needs a place where residents can go to access information and county resources. People in Alpaugh expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service

Alpaugh residents. This could also be the place where residents access a reliable internet connection.

### **Local Market**

The community of Alpaugh is a food desert. This community does not have a local food market that sells fresh produce, meat and dairy. The Alpaugh Grocery Store and Express Gas Mart sell some food; however, most of it is highly processed, packaged, and not considered nutritious. In addition, the food prices are much higher there than in neighboring communities and the quality of food is not as fresh as it is in other communities. Alpaugh residents regularly travel to Delano (20 miles) or Corcoran (17 miles) to buy their groceries.

### **Natural Gas**

Some residents, particularly those on the outskirts of the community, report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that the residents that do not have natural gas would like to have it but cannot afford the cost of extending gas lines to their homes.

### **Loose Dogs**

Loose stray dogs are a problem in the community of Alpaugh. Residents feel unsafe and are worried that their children will be bitten by a stray dog. Residents report unwanted dogs in their yards and on Alpaugh roads. Residents say the Tulare County Animal Control no longer comes to Alpaugh to pick up stray dogs. Residents said that this service is needed and would help with this problem.

### **County Park Management**

Residents report that the county park in Alpaugh is not maintained properly and that it is an unsafe place for to be. The restrooms are dirty and the park grounds are not regularly maintained by the county. Residents do not use the public park because they are worried about gang and criminal activity there. The people of Alpaugh would like to use their park and feel safe while there. However, the lack of park maintenance and lack of Sheriff Patrol make it unwelcoming.

### **Storm Water Drainage**

Alpaugh residents report that the community does not have adequate storm water drainage. This is a problem because the stagnant water attracts insects. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season.



## **Water**

The chief problem facing Alpaugh's water system is its consistent violation of the arsenic Maximum Contaminant Level (MCL). A pilot study to analyze the feasibility of arsenic treatment has recently been concluded. This pilot study, funded by the State Water Resources Control Board with a planning grant under Proposition 84, concluded that arsenic removal is a feasible option using an adsorptive process. Design for the new water treatment plant was completed in December 2015 and Alpaugh CSD is applying to the State for construction funding. Alpaugh also has some hydrogen sulfide odor problems, which they address by chlorinating. During the SGC community meeting residents expressed that the water quality problem is an important issue; however, most feel that the Alpaugh Community Services District is working to address the issue and that they are satisfied with the current work being done to analyze multiple solutions. For that reason the Alpaugh water issue is not listed as a priority improvement in this report. (Note: under a separate grant from the Strategic Growth Council, regional solutions to address water quality problems in Alpaugh and Allensworth are being assessed and vetted with the communities, and it is possible that the Alpaugh water treatment plant may supply Allensworth as well.) Alpaugh also faces challenges with its extensive and aging water distribution system. The core of the system was replaced in the last ten years, but many miles of pipe to the outlying areas is disintegrating and needs replacement. Attempts at repairs result in worse breakage.

## **Public Services**

**Sheriff** – Alpaugh is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Alpaugh and the nearest Sheriff substation is located in Pixley, 19 miles away.

**Fire** –Tulare County Fire, Alpaugh Station, located in Alpaugh

**Schools** - Alpaugh Unified School District (K-12), located in Alpaugh, is part of Tulare County Office of Education.

**Libraries** – There is a small public library in Alpaugh, operated by the Tulare County Library system, located on Avenue 54 and open twice a week.

**Parks** – Alpaugh Community Park operated by the Tulare County Department of Parks and Recreation is located in Alpaugh on Tule Road.

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

##### **Delft Colony**

#### **Location and introduction:**

The Tulare County community of Delft Colony is located approximately 6 miles southwest of the city of Dinuba. Delft Colony is a Census Designated Place (CDP) with the latest 2010 United States Census Bureau decennial census indicating a population of 454. This community is considered disadvantaged by at least two criteria. The most recent US Census Bureau American Community Survey (2010-2014) Five Year Estimate of the Median Household Income (MHI) for the Delft Colony CDP is \$8,456 with a margin of error of \$3,396. This is 13.8 percent of the Statewide Median Household Income for that period, well below the threshold of less than 60 percent of MHI to qualify as a severely disadvantaged community. In addition, Delft Colony is listed in the top 10 percentile in Cal Enviroscreen 1.1 and top 20 percentile in Cal Enviroscreen 2.0.



#### **Community Outreach**

In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises went door to door throughout the community of Delft Colony to distribute flyers at every home, nearby business and local school. During this process SHE talked with residents about the importance of participating in this process. SGC partnered with Tulare County Head Start Preschool to send meeting flyers home with every student at the school. SHE also partnered with a local church to advertise the meeting.

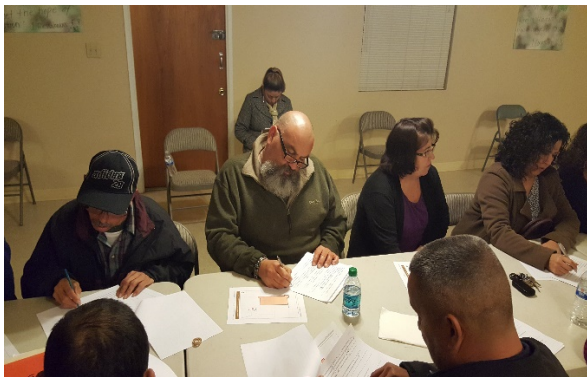
## **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) On Saturday, January 30, and Wednesday February 3, two SHE staff went door to door to distribute flyers to homes in Delft Colony.
- 3.) SHE staff partnered with members of A Better Community for Delft, a community service group advocating for local improvements. This group helped to distribute fliers and spread the word throughout the community.
- 4.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance of participating in the SGC community meeting.
- 5.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner stores and all businesses.

## **Community Meeting**

SHE (including three staff) held a Strategic Growth Council community input meeting in Delft Colony on Thursday, February 4<sup>th</sup> at 6:00pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the meeting room at the Church of God, 5636 Lawrence Avenue in Delft Colony. This is an ideal meeting place because it is centrally located and because people feel welcomed and safe there. 33 residents attended the meeting and 23 surveys were collected.



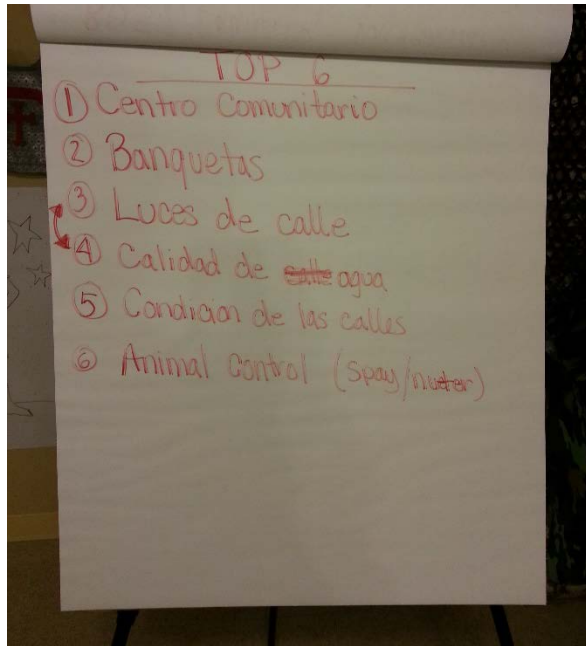
## **Community Survey**

A detailed survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools,

Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/ concerns:

### **Priority Concerns**



1. **Community Center**
2. **Side Walks**
3. **Water Quality**
4. **Street Lights**
5. **Road Conditions**
6. **Animal Control**
7. **Internet Access**

### **Community Resource Center**

Delft Colony does not have a community hub. The community lacks many resources and needs a place where residents can go to access information and county resources. People in Delft Colony expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Delft Colony residents. This could also be the place where residents access a reliable internet connection.

### **Side Walks**

The community of Delft Colony has too few sidewalks. The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk around town in the mud and through big

puddles of water. The absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

### **Water Quality**

Residents expressed that they are concerned with their water quality. Most believe that the water is not safe to drink because it is contaminated with bacteria. Resident stated that they do not drink their tap water instead they buy bottled water. Residents shared that they have received several notices advising customers not to drink tap water before boiling it. A review of the SDWIS website indicates that violations have been issued to the water system due to not properly reporting per Total Coliform Rule requirements, however, there was no actual contamination noted.

### **Road Conditions**

Roads conditions are one of the biggest concerns to the residents of Delft Colony. Residents are concerned with driving conditions and walkability. They describe road conditions as poor and stated that roads have potholes and large cracks. Roads need maintenance, most reported that they are not satisfied with street safety. Fast traffic is also a concern.

#### **Priority roads:**

- \*Ave 308
- \*Road 52
- Ave 309
- Road 50
- Road 48
- Markham

### **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since according to residents the Sheriff's Department seldom patrols Delft Colony. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

### **Internet Access**

Most families in Delft Colony do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as

online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Delft Colony.

### **Public Services**

Sheriff – Delft Colony is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Delft Colony and the nearest Sheriff substation is located in Visalia,

Fire –Tulare County Fire Station 3 located northeast on, Road 80, 5 miles away

Schools – Grand View School, 2 miles away; Kings River Elementary School, 4 miles away; Dinuba High School 3-4 miles away

Libraries – The community does not have a library

Parks – The nearest park is in Reedley, 6 miles away or Dinuba miles away



## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

##### **Earlimart**

Earlimart is a small, rural community located on the State Route 99, 7 miles north of Delano. According to the United States Census Bureau, the CDP has a total area of 2.1 square miles (5.4 km<sup>2</sup>), with a population of approximately 10,194. This community is considered disadvantaged. The most recent US Census Bureau American Community Survey (2010-2014) Five Year Estimate of the Median Household Income for the Earlimart CDP is \$23,458. Earlimart is listed in the top 10 percentile of Cal EnviroScreen 1.1 and top 20 percentile under Cal EnviroScreen 2.0.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff distributed and posted flyers around town at local business, clinics and post office. SHE talked with business owners about the project and invited them to attend the meeting.
- 3.) SHE staff distributed flyers at the local food pantry where over 250 residents receive food. At this event, SHE explained the project and the importance in participating.
- 4.) SHE partnered with the Earlimart Unified School District to distribute flyers to their students. Meeting flyers were sent home from school with 2,600 students of the ESD.
- 5.) Meeting invitations were sent to the Earlimart Public Utility District and Earlimart School District Board members.
- 6.) During the door to door community outreach process, SHE staff explained the goals of the project and talked with residents about the community benefits of participating in the SGC community meeting.

#### **Community Meeting**

Self-Help Enterprises held a SGC community meeting in Earlimart on Wednesday, November 4, 2015 at 6:00pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Veterans Memorial Building, 712 E. Washington Ave in Earlimart. This is an ideal meeting location because it is centrally located and most residents know where the Memorial Building is. The

purpose of the meeting was to discuss Earlimart improvement needs, gather community data, and report the findings of the meeting in a final report. Over thirty people attended the meeting. 28 community surveys were collected. Representatives from Proteus, United Health Center, Earlimart Town Council and a local group, Rural Committee for Community Advancmenet were present at this community meeting.



## **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Earlimart SGC community input meeting, residents concluded that the following is a list of Earlimart priority improvement needs

## **Priority Improvements**

1. Community Safety/Police Patrol
2. Sidewalks
3. Drainage
4. Street Lights
5. Affordable Housing
6. Supermarket
7. Speedbumps
8. Road Conditions/Street Repairs
9. Sewage Capacity

## **Sheriff Presence**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Earlimart. Residents report that the Sheriff's Department response time is unacceptable and that there is little Sheriff patrol within the community. Residents are worried about the rate of crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate makes criminals feel unstoppable in Earlimart. Over the last few years, Earlimart has seen a rise in gang violence, increased police presence would help with that problem.

## **Safety**

Residents are worried for their safety and feel that the Sheriff department should be doing more to alleviate the recent rise in gang related crimes. The community is very concerned with the rise in shootings and drug related violence over the last couple of years. The recent crime activity is driving some residents away and has caused some people to move to neighboring communities such as Delano and Tulare.

## **Storm Water Drainage**

Earlimart residents report that the community does not have adequate storm water drainage. Large puddles form and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season making it difficult to drive and walk on certain roads. After it rains the stagnant water attracts insects and mosquitos causing additional problems. The community needs curbs and gutters to help with this problem.

## **Road Improvements**

Road conditions are a big concern to Earlimart residents. It is reported that road conditions are poor and some roads need work, streets have potholes, cracks and bumps. Drivers feel unsafe and are worried about damage to their vehicles while driving on some Earlimart roads. According to residents, Earlimart roads are not regularly maintained. The following roads are a priority to the residents:

- Washington Ave.
- State Street
- Front Road
- Ave 148

## **Side Sidewalks**

Many streets in Earlimart need sidewalks, some residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season.

When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Sometimes the traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

### **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's department seldom patrols the streets of Earlimart. Residents are especially concerned about how this affects the safety of local children. Residents state that some streets do not have light poles. Other streets have light poles but the light bulb is out, this issue should be easy to solve by simply replacing the bulb. Residents have asked their county Supervisor to address this issue and have been told that Tulare County does not know which poles belong to them and which belong to Southern California Edison. Residents report being told that there is a cost to them to install a light pole on their street. Residents have been asking the County of Tulare for help with solving this problem for many years and that this issue has not been treated as a priority.

### **Neighborhood Park**

In the community of Earlimart there is only one small park space for the community to use. Survey results show that most residents use the local schools green space or the street to be physically active. Residents are concerned about the health and safety of their children, they worry that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. The County of Tulare received a grant to build a community park in Earlimart, the park project is a partnership with the Earlimart School District. The community is anxious to see this park happen and is also concerned that there has been little progress made, they feel that the County is not treating this project as a priority. The completion of the Earlimart Park is a priority to the community and they want to see this project completed in a timely manner as scheduled and as promised. Currently the nearest community park is located in the neighboring community of Pixley, 6 miles away. It is not practical for residents to travel that far to use the park.

### **Local Grocery Store**

Residents expressed the need for an affordable grocery store in town. Currently, Earlimart has two markets that sell some produce, meat and dairy, however residents are not satisfied with the quality and freshness of the food and food prices are much higher in town than in the neighboring community of Delano. Over the last ten years Earlimart has grown to a population of over 10,000, with a population this size and because Earlimart is located directly off of SR99, residents feel a large grocery store will be sustained. Earlimart residents regularly travel to Delano (7 miles) or Tulare (25

miles) to buy their groceries. This travel would be eliminated if there was a large grocery store in town.

### **Affordable Housing**

Earlimart residents want new affordable housing to be built in town. Many people expressed that they would buy a new home in Earlimart if it were available to them. For several years, the Earlimart Public Utility District has not allowed new sewer connections to the sewer system due to capacity issues. This has put limitations on new home development in Earlimart. Residents would like to see growth in town and want to work with the Earlimart PUD to allow for new sewer connections for new homes. Residents feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Delano and Tulare.

### **Public Services**

Sheriff –Earlimart is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Earlimart and the nearest Sheriff substation is located in Pixley, 6 miles away.

Fire – There is a Tulare County Fire station located in Earlimart.

Schools –Earlimart School District (K-8), has 4 schools in Earlimart. Students attend high school in Delano CA.

Libraries – Earlimart Public Library located in Earlimart

Parks – Earlimart Rotary Park, small pocket park located in town.

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

#### **Hypericum**

Hypericum also known as “Dog-town” is a small community comprised of two residential roads located approximately 3 miles south of the City of Farmersville. The exact population Medium Household Income is unknown. Hypericum is comprised of approximately 40 parcels of land.



#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff went door to door to distribute flyers to every home on February 13 and February 19, 2016.
- 3.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the community benefits of participating in the SGC community meeting.
- 4.) SHE partnered with the Outside Creek School District to spread the word about the meeting; meeting flyers were sent home from school with each student.
- 5.) SHE partnered with a community resident who helped to distribute flyers to his neighbors and allowed us to have the community meeting at his home.



## **Community Meeting**

Self-Help Enterprises conducted a community meeting (with two SHE staff members present) in Hypericum on Sunday, February 21, 2016 at 3:00pm. The meeting was scheduled on a weekend to make it easier for working residents to attend. The meeting was held at 25649 Hypericum Road, the home of a local resident. This is an ideal meeting location because the community of Hypericum does not have a community meeting space and because most residents feel welcome and comfortable with their neighbors. The purpose of the meeting was to discuss Hypericum improvement needs, gather community data, and report the findings of the meeting in a final report. 9 Alpaugh residents attended the meeting. 8 community surveys were collected.



## **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Hypericum SGC community input meeting, residents concluded that the following is a list of Hypericum priority improvement needs

### **Priority Improvements**

- Water quantity
- Water quality
- Sewer
- Natural Gas

- Storm water drainage
- Road Conditions
- Street lights
- Internet
- Community Park
- Transportation

## **Water**

Hypericum is a private well community, residents own and maintain their own wells. Over the last year many wells have gone dry in this community. Through the help of Self Help Enterprises and Tulare County, residents whose wells have gone dry have been connected to temporary water tanks as an interim solution. Residents are receiving an allocated amount of water per month. Resident expressed that they are interested in exploring their options for connecting to a neighboring community water system, they understand that this may include an initial cost and would result in paying a monthly water bill.

Residents are concerned with their water quality and perceive their water to be unsafe to drink. Most families do not drink the water from their tap. In 2014, SHE tested three wells in Hypericum, test results show that two of the three wells tested positive for total coliform and the Nitrate levels are as follows:

- Well 1 - 47ppm
- Well 2 - 65ppm
- Well 3 - 47ppm

## **Sewer System**

The community is dependent on individual septic tank systems for sewage disposal. Many residents would prefer to be part of a community wide sewer system, they understand that this may include an initial cost and would result in paying a monthly sewer bill.

## **Natural Gas**

Residents report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that Hypericum residents would like to have natural gas but cannot afford the cost of extending gas lines to their homes. The community is open to any option of converting to natural gas.

## **Road Improvements**

The community of Hypericum is comprised of two main roads, Hypericum Road and Road 159. Road conditions are a major concern to Hypericum residents. It is reported

that road conditions are poor and the roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving on that road. Residents report that roads are narrow and that two cars barely fit on one road at the same time. According to residents, Hypericum roads are not regularly maintained.

### **Storm Water Drainage**

Residents report that the community does not have adequate storm water drainage. This is a problem because large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season. Storm water drainage – residents report that huge flooding problems when it rains in this area, the stagnant water attracts insects and mosquitos.

### **Internet Access**

Most families in Hypericum do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Hypericum.

### **Street Lights**

Residents report dark streets at night because the streets do not have lighting. People stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols this area. Residents are especially concerned about how this affects the safety of local children. Residents said that their streets do not have light poles.

### **Neighborhood Park**

In the community of Hypericum there is no safe open green space for children to play and adults to be physically active. The nearest park is located 4 miles away in the city of Farmersville, survey results show that in Hypericum, children play in the street or do not play outside. Hypericum residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

### **Public Transportation**

Hypericum lacks access to public transportation. Hypericum residents that do not drive find it hard to travel outside of town. Residents often need to travel to Visalia or

Farmersville for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes out to Hypericum. Bus transportation to and from Hypericum would help many residents with their daily travel needs. It is reported that the nearest bus stop is 4 miles away in Farmersville.

Outside Creek School -1.5 miles away

Farmersville city limits - 3 miles away

Nearest gas station - Farmersville 3.5 miles away

Nearest park - Farmersville 4 miles

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

##### **Lindcove**

The community of Lindcove is located in the northeastern portion of the County of Tulare. Approximately 15 miles North East of Visalia. Lindcove is a Census Designated Place (CDP) with a population of approximately 406 in 2010.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff made phone calls to local residents to determine who was interested in allowing us to have a community meeting at their home
- 2.) SHE staff visited the community and did outreach in efforts to identify a meeting location.
- 3.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC project.
- 4.) SHE staff went door to door to complete resident surveys. SHE collected seven surveys in the community of Lindcove.

#### **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

On Thursday April 7, 2016, SHE staff went door to door in Lindcove to ask residents to participate in a community survey for SGC. Based on the information collected in the surveys, the following is a list of communities' top improvement needs:

## **Priority Improvements**

- 1) Natural Gas
- 2) Sidewalks
- 3) Street lighting
- 4) Community Center
- 5) Community Park
- 6) Community Water System
- 7) Public Transportation

### **Natural Gas**

Lindcove residents report that they do not have natural gas lines and that propane is their source of fuel. Families report a high cost of propane, this cost results in people not heating their homes to a comfortable temperature, and this can lead to increased cold and flu in small children and elderly people.

### **Sidewalks**

The community of Lindcove has too few sidewalks. The residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking around town and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Lindcove traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

### **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Lindcove. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

### **Neighborhood Park**

In the community of Lindcove there is no safe green space for children to play and adults to be physically active. The nearest park is located over 3.5 miles away in the city of Exeter. Lindcove residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.



## **Water System**

Lindcove is a private well community, residents their own and maintain their own well. Resident expressed that they are interested in exploring their options for connecting to a neighboring community water system, they understand that this may include an initial cost and would result in paying a monthly water bill.

Some residents are concerned with their water quality and perceive their water to be unsafe to drink. Most families do not drink the water from their tap, they either buy bottled water or have a water filtration system. In 2014, SHE tested nine water wells in Lindcove. In summary, four of the nine wells had Total Coliform present, all 9 wells tested over the MCL for Nitrates and four wells exceeded the MCL for 123 TCP.

## **Public Transportation**

Residents expressed their frustration for the lack of public transportation access. The Lindcove residents that do not drive or own a vehicle, find it hard to travel outside of town. Often residents need transportation to go into Visalia or Exeter for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes out through their community. Bus transportation to and from Lindcove would help many residents to travel on a daily basis.

Sheriff – Lindcove is in the jurisdiction of Tulare County Sheriff's Department (TCSD).

Fire –Tulare County Fire, nearest fire station is unknown.

Schools – Sequoia Union School District.

Libraries – The nearest library is located 4.5 miles away in Exeter

Parks – The nearest community park is located 4.5 miles away in Exeter

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

### **Outreach Report**

#### **London**



The community of London is located in the northwestern portion of the County of Tulare between the cities of Visalia and Dinuba. London is a Census Designated Place (CDP) with a total area of 1.0 square mile with a population of approximately 2,085.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff went door to door to distribute flyers to every home, business and school in the community of London. Door to door outreach was done on Wednesday, February 10 and Saturday February 13, 2016.
- 3.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.

4.) SHE partnered with the London Head Start Pre School, meeting flyers were sent home from school with each student. SHE partnered with the Hodges London Community Center to post fliers and spread the word about the meeting.

5.) Meeting flyer were distributed to residents at the local London food pantry on Thursday February 4, 2016.

6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at local corner stores and churches.

### **Community Meeting**

Self-Help Enterprises held a SGC community meeting (with two SHE staff members present) in London on Wednesday, February 17, 2016 at 5:30pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Hodges Community Center 5750 Ave 378, Dinuba CA. This is an ideal meeting location because it is centrally located and residents feel welcomed and comfortable being there. The purpose of the meeting was to discuss London improvement needs, gather community data, and report the findings of the meeting in a final report. Twenty two residents attended the meeting, twenty one community surveys were collected.



### **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the London SGC community input meeting, residents concluded that the following is a list of their priority improvement needs

### **Priority Improvements**

- 1) Road Conditions & Street Safety including:
  - Flashing warning lights
  - Speed limit signs

- Speed bumps
  - four way stop
  - Street lights
- 2) Community Park
  - 3) Internet Access
  - 4) Sidewalks
  - 5) Storm water drainage
  - 6) Affordable Housing

## **Road Conditions and Street Safety**

London residents expressed that street safety, including the need for sidewalks, street lighting, speed limit signs, stop signs, and the need for enforcement of traffic laws are the highest priority improvement needs in London. Survey results show that most residents are not satisfied with road conditions. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on London roads. Residents report that some roads are narrow and that two cars barely fit on one road at the same time. It was said that roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired. 100% of the respondents are not satisfied with the conditions of the roads. 100% of the respondents described the roads to be in “poor” conditions.

The following roads are a priority to the residents:

- Rd 58
- Ave 377
- Pound Rd
- All major roads

## **Survey comments regarding road conditions:**

“Fix the roads”

“Some roads are all dirt”

“Lots of potholes”

“We pay Measure R, but they don’t fix our roads”

“Nobody maintains our roads”

## **Sidewalks**

The community of London has too few sidewalks. Some of the residents that were surveyed reported that the street they live on does not have sidewalks. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. London traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols London. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Internet Access**

Most families in London do not have internet access at their homes. Any available internet service is unreliable and too expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Some residents use the computers and internet at the Hodges Community Center, however usage is limited to hours our operation. Internet access ranked as a high priority improvement need in London.

## **Storm Water Drainage**



(Pictured above is storm water in London, CA)

London residents report that the community does not have adequate storm water drainage. This is a problem because the stagnant water attracts insects. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season.

## **Neighborhood Park**

In the community of London there is no safe green space for children to play and adults to be physically active. The nearest park is located 8 miles away in the city of Dinuba, survey results show that in London, children play in the street or do not play outside at all. London residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.



## **Affordable Housing**

London residents want new affordable housing to be built in town. Many people expressed no new homes have been built in recent years and that they would buy a new home in town if it were available to them. Residents would like to see growth in town and feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Dinuba or Visalia.

## **Water**

The London Community Services District water system does not have sufficient source water capacity to meet the California Waterworks Standards contained in Title 22. Based on the Title 22 standards and as identified in the report by the Department of Health Services, the minimum water source capacity for London Community Services District should be 1,000 gpm. Currently, the District is using two (2) wells (2A and 4) with a total probable combined capacity of 740 gpm. There are many periods of time when the water system has low pressure caused by inadequate supplies. The existing water system has no water storage. Well No. 1 is dry and failure of the other shallow Well No. 3 could occur at any time. This would cause the District's lack of adequate water supply to be even more severe. Well #2A is an open: 1 bottom cable tool well which is not gravel packed and develops water through mining of a cavity in the aquifer at the bottom of the well casing. The well is subject to failure in the well casing because of age or a collapse of the mined opening at the bottom of the casing. The collapse of the cavity on the well has occurred in the past. London CSD has recently completed a State Revolving Fund planning application to prepare for a new well, storage tank, and distribution system and has also applied for emergency funds to drill the new well due to drought impacts.

## **Public Services**

Sheriff –London is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of London and the nearest Sheriff substation is located in Visalia, 12 miles away.

Fire –Tulare County Fire, Dinuba 8 miles

Schools – Kings River School 6 miles, Dinuba High School 8 miles

Libraries – Dinuba, 8 miles away

Parks – Dinuba, 8 miles away



**Sustainable Communities Strategy**  
**Strategic Growth Council**  
**Outreach Report**  
**Monson**



Monson is a small low-income severely disadvantaged community, comprised mainly of farm-workers and their families. It is an approximately 250-acre, unincorporated area of Tulare County that is located approximately 14 miles north of the City of Visalia within the vicinity of the City of Dinuba and the community of Sultana. Monson is comprised of approximately 37 parcels of property containing approximately 40 residential units.

### **Community Income Survey**

In December 2010, SHE conducted door to door surveys in the rural community of Monson. The purpose of the Monson survey was to determine the median household income (and gather demographic information that would be useful in applying for future funding for water and/or wastewater system improvements) Surveys were conducted during weekday mornings, afternoons, evenings, and weekends, during the month of December 2010. Parcel maps were utilized to document which households have been visited and successfully surveyed. In summary, a total of forty households (n=40) exist in the community of Monson. The total number of housing units surveyed equaled thirty seven (n=37, or 92.5%). Once surveying was completed, the survey data was entered into the SHE database for compilation and analysis. Results indicate that the Median household income for the community of Monson is \$15,000;

## **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) Two SHE staff went door to door to distribute flyers to every home in Monson during the day and in the evening on Monday August 17 and Wednesday August 19, 2015.
- 3.) SHE staff partnered with La Voz de Monson (a local community group of residents that was formed to advocate for Monson improvement, especially around water issues) to spread the word about the importance of attending this meeting.
- 4.) SHE made phone calls to local residents.
- 5.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- 6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner store.

## **Community Meeting**

Self-Help Enterprises conducted a community meeting (with two SHE staff members and one Tulare County RMA employee present) in Monson on Thursday, August 20, 2015 at 6:30pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at 38660 Monson Drive, the home of a local resident. The home of a community resident is an ideal meeting location because the community of Monson does not have a meeting space and because most residents feel comfortable with their neighbors. The purpose of the meeting was to discuss Monson improvement needs, gather community data, and report the findings of the meeting in a final report. Twenty Monson residents attended the meeting. Eleven (11) community surveys were collected.

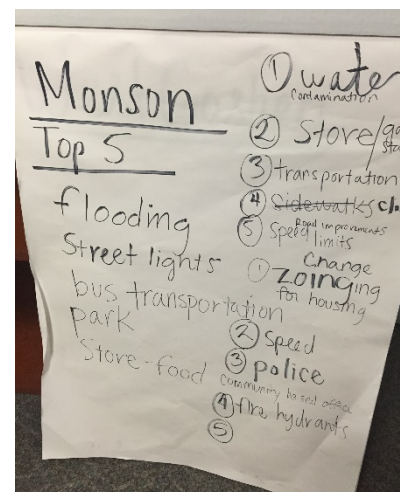


**Community Survey** A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Monson SGC community input meeting, residents concluded that the following is a list of Monson priority improvement needs

### Priority Improvements

1. Water Issues
2. Storm Water Drainage/ Street Flooding
3. Street lights
4. Public transportation
5. Community Park
6. Clinic
7. Road improvements
8. Zoning changes
9. Community based officer/Sherriff patrol
10. Fire hydrant



### Water Quality

Monson is currently not served by a public water system. Approximately 40 homes in the community of Monson obtain their drinking water from private domestic water wells, many of which are shared between multiple households. Nitrate levels in the area's groundwater

have long been known to be high, as Self-Help Enterprises (SHE) tested these same wells twenty years ago. In 2008, testing of a dozen domestic wells was conducted in the community of Monson. Testing results revealed nitrate levels between 46 and 130 parts per million (ppm), which are all over the EPA and California Maximum Contaminant level (MCL) of 45 ppm. Furthermore, the average level of nitrate contamination among the wells tested was 105 ppm which is over twice the MCL.

The community is concerned about the quality and safety of their water, it is hard to know how to solve the community's water problems. Consequently, most families end up spending scarce resources on buying bottled water as an interim measure until a solution is reached. SGC survey results show that 82% of people (that attended the SGC meeting) would like to be connected to a community water system, 18% did not respond to the question.

### **Water Quantity**

Monson is comprised of approximately 37 lots with approximately 40 residential units and a local store. Currently all residential lots are served by private water wells and septic tanks. The local store is also served by an individual private water well but is regulated as Transient Non-Community Water System (TNCWS).

The majority of the lots have their own individual water supply wells. Nearly all of the community's wells were drilled several decades ago. Almost all of the wells are shallow. Typical wells are 80 feet deep. **Figure 3** indicates the depth to water in 3 wells monitored by the Alta Irrigation District, which are located within a one mile radius of the community. Data from this Figure indicate that between Fall of 2011 and Spring of 2014 water levels in well O127A have declined from 19 feet below ground surface to 49 feet - a drop of 30 feet and water levels in well O134A have declined from 27 feet below ground surface to 63 feet a drop of 36 feet and this drop in the water table is accelerating as the drought continues. (See attached)

In May 2014 and July 2014, the depth to water in a single domestic well in Monson was measured. Results of the measurements show the water level dropped from 54 feet to 63 feet (9 feet) between May and July. Due to the current drought conditions, the measured well and several others have gone dry and many more wells remain at risk of going dry.

### **Storm Water Drainage/Flooding**

Monson residents report that the community does not have adequate storm water drainage. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. This is a problem because the stagnant water attracts insects and mosquitos. Residents report that some streets flood during the rainy season, most of the roads in town are dirt roads this makes mud and pot holes. Not having curb and gutters or sidewalks also adds to the problem.

## **Fire Hydrants**

Residents are worried because their community does not have fire hydrants. Residents feel vulnerable to fire emergencies. The nearest fire water supply is in Sultana. In case of a fire, Dinuba Fire Department would service the Monson community.

## **Road Improvements**

Road conditions are a big concern to Monson residents. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on Monson roads. Residents report that roads are narrow and that two cars barely fit on one road at the same time. According to residents, Monson roads are not regularly maintained.

## **Speed Limit Signs**

During the SGC community meeting residents expressed concern about the speed of traffic in town. Many vehicles travel at a high rate of speed on Monson Drive as well as on the other main roads in town. Residents believe that in order to avoid more car crashes, the County should install a speed limit sign on Monson Drive and speed bumps on residential streets to slow traffic. Residents shared that there is little to zero patrol done by the Sheriff and California Highway Patrol, this adds to the speeding problem.

## **Public Transportation**

Residents expressed their frustration from the lack of public transportation access. The Monson residents that do not drive or own a vehicle, find it hard to travel outside of Monson. Often residents need transportation to go into Visalia or Dinuba for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes out through Monson. Bus transportation to and from Monson would help many residents to travel on a daily basis.

## **Neighborhood Park**

In the community of Monson there is no safe open green space for children to play and adults to be physically active. The nearest park is located 8 miles away in the city of Dinuba, survey results show that in Monson, children play in the street or do not play outside at all. Monson residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

## **Local Market**

The community of Monson is a food desert. This community does not have a local food market that sells fresh produce, meat and dairy. The Monson Market sells some food however, most of it is highly processed, packaged, and not considered nutritious. In addition, the food prices are much higher there than in neighboring communities and the quality of food is not as fresh as it is in other communities. Monson residents regularly travel to Dinuba (8 miles) or Visalia (15 miles) to buy their groceries.

## **Medical Clinic**

Monson residents do not have access to medical care within the community. The nearest medical clinic is approximately 8 miles away in Dinuba. Most residents report traveling to Visalia, 14 miles, Dinuba, 8 miles or Orosi, 8 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. The Tulare County Area Transit does not make regularly scheduled stops in Monson, the TCAT will pick up Monson residents by appointment when calling the day before. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Visalia, 15 miles away, and it takes approximately 20 minutes for the nearest ambulance to get to Monson from Visalia.

## **Zoning**

During the community meeting residents stated that they are unable to build new homes or start businesses due to the way land is currently zoned in Monson. Residents expressed that zoning changes are needed to allow residents to build new housing in the community. During this meeting, it appeared that multiple residents would be interested in building a home in Monson if zoning laws permitted it.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Monson. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Community Based Officer**

The Tulare County Sheriff Department is responsible for patrolling the community of Monson. Residents report that the TCSD response time is unacceptable and that there is little to zero Sheriff Patrol within the community. Residents are worried about the rate



of unreported crimes and crimes that go un-responded to. Monson residents are asking for increased Sheriff Presence in their community.

### **Sewer System**

There is no community wide sewer system in Monson. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent.

### **Public Services**

Sheriff –Monson is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Monson and the nearest Sheriff substation is located in Visalia, 15 miles away.

Fire –Tulare County Fire, Monson does not have a fire station in town.

Schools – Sultana School District (K-8), located in Sultana, is part of Tulare County Office of Education - 4 miles away

Libraries – The nearest public library is in Dinuba, operated by the Tulare County Library system – 8 miles away

Parks – Dinuba Community Park, Dinuba CA – 8 miles away

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

##### **Richgrove**

Richgrove, an unincorporated community in Tulare County is located in the southern portion of Tulare County, just north of the Tulare County/Kern County line. Nearby cities and communities include Ducor, approximately 8 miles to the northeast and Delano approximately 10 miles to the west; Terra Bella, approximately 12 miles to the northeast; and Porterville approximately 20 miles to the northeast. In 2010, the population was 2,882 and the MHI is \$29,792.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff went door to door to distribute flyers to every home in Richgrove on Thursday January 7 and Wednesday January 13, 2016
- 3.) SHE staff partnered the Richgrove Community Food Pantry to distribute flyers to residents at the food distribution event on Wednesday January 13, 2016
- 4.) SHE staff partnered with Richgrove School District and the Richgrove Head Start Preschool to send home flyers with every student.
- 5.) SHE partnered with the Richgrove Veterans Memorial Building, they helped us by posting meeting flyers and the community meeting was held there.
- 6.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- 7.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at local businesses and post office.

#### **Community Meeting**

Self-Help Enterprises conducted a community meeting (with one SHE staff member and one Tulare County RMA employee present) in Richgrove on Thursday, January 14, 2016 at 5:30pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Richgrove Community Memorial Building. This is an ideal meeting location because it is centrally located and

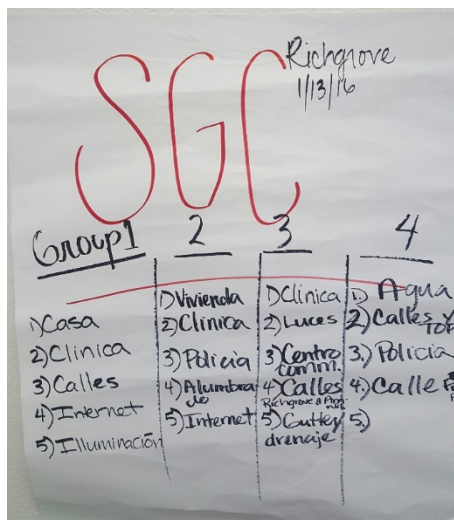
most residents feel comfortable there. The purpose of the meeting was to discuss Richgrove improvement needs, gather community data, and report the findings of the meeting in a final report. Thirty five Richgrove residents attended the meeting and twenty six community surveys were collected.



## Community Survey

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Richgrove SGC community input meeting, residents concluded that the following is a list of priority improvement needs



## Priority Improvements

1. Affordable Housing
2. Clinic
3. Street lights
4. Road Conditions
5. Internet
6. Police Patrol
7. Water Issues
8. Storm Water Drainage
9. Community Center

## **Affordable Housing**

Richgrove residents want new and affordable housing built in their community. Many people said that they would buy a new home in town if it were available to them. Residents want to see their community grow. Residents feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Delano.

## **Medical Clinic**

Richgrove residents do not have access to medical care within the community. The nearest medical clinic is approximately 10 miles away in Delano. Most residents report traveling to Delano, 10 miles or Porterville 21 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Delano, 10 miles away, and it takes approximately 15 minutes for the nearest ambulance to get to Richgrove from Delano.

## **Road Conditions**

Road conditions are a big concern to Richgrove residents. It is reported that road conditions are poor and most roads need work due to potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving in town. Residents are especially concerned with the condition of Richgrove Drive. This road is not regularly or properly maintained. It is reported that the few times that roads have been serviced, the repair work was poor. Many heavy trucks and lots of traffic travel on this road, this results in the road needing repairs on a regular basis.

The following roads are a priority to the residents: Richgrove Drive and Ames Drive.

## **Storm Water Drainage**

Richgrove residents report that the community does not have adequate storm water drainage. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season. This becomes a bigger problem when the stagnant water attracts insects and mosquitos.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's Department seldom patrols Richgrove. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets

have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

### **Internet Access**

Most families in Richgrove do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Richgrove.

### **Community Resource Center**

Richgrove does not have a community hub. The community lacks many resources and needs a place where residents can access information and county resources. People in Richgrove expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Richgrove residents. This could also be the place where residents access a reliable internet connection. Currently residents must travel 10 miles to Delano to the nearest community center.

### **Sheriff Presence**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Richgrove. Residents report that the TCSD response time is long and that there is too little Sheriff patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate and makes criminals feel unstoppable in that area of Richgrove. Richgrove residents are asking for increased Sheriff Presence in their community.

### **Water**

Residents are concerned with the current water situation and want to see solution happen soon. In Richgrove, the town is provided water by Richgrove Community Services District. The District has two operable wells that supply Richgrove residents and businesses with potable water. One of these wells, Well No. 4, has experienced problems with two water quality contaminants, arsenic and DBCP, which have at times exceeded the MCLs. The other well, Well No. 5, has experienced problems with nitrates and arsenic. The failure of either of these wells would jeopardize the District's ability to meet demand. California (Rodriguez) Camp is a privately owned labor camp located two miles to the west of Richgrove. The California Camp water system has one water well that provides water to its residents. The well produces water that exceeds

the Maximum Contaminant Level (MCL) for nitrate with levels in the range of 130 ppm. The system's owner has received Notices of Violation and Compliance Citations as a result of this high nitrate level, almost three times the MCL. The Camp's system has no supply redundancy and residents would be completely without water in the event of the well's failure. No emergency connection exists.

A Preliminary Engineering Report, completed in January 2008, was prepared with the understanding that consolidation between California Camp and the RCSD was a priority. The preferred project alternative is to drill a new community water well (including a test well) and construct transmission lines that will connect the new well to the District and the Labor Camp. The planning study will include the evaluation of the feasibility of blending to achieve water quality improvement and reduce the concentration of contaminants to within the MCL required for drinking water systems. A blending project would require the construction of new storage facilities.

### **Public Services**

Sheriff – Richgrove is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling Richgrove and the nearest Sheriff substation is located in Porterville, 21 miles away.

Fire – Tulare County Fire Department

Schools – Richgrove School District (K-8), located in Richgrove, is part of Tulare County Office of Education

Libraries – The nearest public library is in Delano, 10 miles away

Parks – There is currently a park project in construction in Richgrove



## Strategic Growth Council

### Outreach Report

#### Seville

Seville is a small, rural community in the eastern part of Tulare County, located about 14 miles east of Visalia, CA. It is a census designated place with a total area of 1.0 square mile of land. Seville is considered a low income, disadvantaged community. Based on 2010 Census information, the population was 480.

#### Community Outreach and Invitation Process

In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises went door to door throughout the community of Seville to distribute flyers at every home, business and school. During this process SHE talked with residents about the importance of participating in this process. SGC partnered with the Stone Corral School and sent meeting flyers home with every student in the school district. Meeting invitation were sent to the members of the Stone Corral School Board Members. Fliers were posted at the local store.

#### Community Meeting

The Strategic Growth Council held a community input meeting in Seville on Wednesday, December 9, 2015 at 5:30pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Stone Corral School located at 15590 Ave 383 in Seville. This is an ideal location because most residents know where the school is located. A total of 8 surveys and 9 Community Support Petitions were collected in Seville.





## **Community Survey**

A survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

### **Priority Concerns**

1. Reliable Internet Service
2. Road Conditions
3. Natural Gas Lines
4. Side Walks
5. Community Park
6. Street Lights
7. Grocery Store

### **Reliable Internet Access**

Most families in Seville do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Seville. Some said this issue was more important than any other.

### **Road Improvements**

Road conditions are a big concern to Seville residents. It is reported that road conditions are poor and some roads need work, streets have potholes, cracks and bumps. Drivers feel unsafe and are worried about damage to their vehicles while driving on most roads. According to residents, Seville roads are not regularly maintained. The following roads are a priority to the Seville residents:

- Inyo Ave
- Road 154
- Ave 383
- Ave 381



## **Natural Gas**

Residents report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that residents rather have natural gas but cannot afford the cost of extending gas lines to their homes. Seville residents are requesting assistance from the County to help with the cost of extending natural gas lines to Seville. This is the most important issue to all residents that were surveyed.

## **Local Market**

The community of Seville is a food desert. This community does not have a local food market that sells fresh produce, meat and dairy. There is one corner gas station that sells mostly highly processed, packaged food that is not considered nutritious. Seville residents regularly travel to Orosi (8 miles) or Visalia (14 miles) to buy their groceries.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Seville. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Side Walks**

The community of Seville has too few sidewalks. The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk around town in the mud and through big puddles of water. The absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Community Park**

In the community of Seville there is no safe public green space for children to play and adults to be physically active. The nearest park is located 8 miles away in the city of Orosi. Residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and concerned for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. Residents express that having a community park is a major priority for them.



## **Public Services**

Sheriff –Seville is in the jurisdiction of Tulare County Sheriff's Department (TCSD).

Fire –Tulare County Fire, nearest fire station is in Oroshi, 8 miles

Schools – Stone Corral School in Seville

Libraries – The nearest public library is in Visalia

Parks – The nearest park is in Cutler-Orosi, 8 miles away

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

##### **Strathmore**

Strathmore is a small community located off of Highway 65, 6 miles north of Porterville and 4.5 miles south of Lindsay. According to the United States Census Bureau the population was 2,819 at the 2010 census. This community of Strathmore is considered disadvantaged based on the Median Household Income of \$26,250.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff distributed and posted flyers around town at local business, clinics and post office and schools.
- 3.) SHE partnered with the local High Schools to distribute flyers to their students. Meeting flyers were sent home from school with all students at Strathmore High School and Harmony Magnet Academy.
- 4.) Meeting invitations were sent to the Strathmore Public Utility District and local school administrators.
- 5.) SHE staff went door to door to distribute fliers in the community.
- 6.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- 7.) SHE partnered with local residents to canvass their streets to have neighbors sign support forms for the needs identified at the community meeting.

#### **Community Meeting**

Self-Help Enterprises held a community meeting on Thursday, October 29, 2015 at 6:00pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Harmony Magnet Academy 19429 Road 225 in Strathmore. This is an ideal meeting location because it is centrally located and most residents know where the school is located. The purpose of the meeting was to discuss Strathmore improvement needs, gather community data, and report the findings of the meeting in a final report. 7 residents attended the meeting and 7

community surveys were collected. In addition to the surveys, 31 Community Improvement Request Forms were collected.

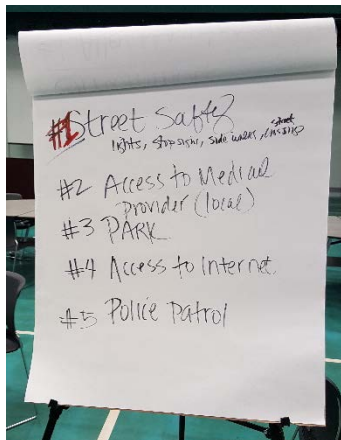


## Community Survey

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Strathmore SGC community input meeting, residents concluded that the following is a list of Strathmore priority improvement needs

## Priority Improvements



1. Road Improvements/ Street Safety
2. Medical Provider
3. Community Park
4. Internet Access
5. Sheriff Patrol



## **Road Improvements**

Road conditions are a big concern to Strathmore residents. It is reported that road conditions are poor and some roads need work, streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving on some Strathmore roads. According to residents, Strathmore roads are not regularly maintained. Residents expressed that many alley roads are in bad conditions and hard to drive on, this is concerning because some homes are only accessible from alleys.

## **Street Safety**

Residents reported that street safety is a high priority in Strathmore. Stop signs and speed bumps are needed to slow traffic in high traffic area. Residents would like to see cross walks for the elderly and children to use when crossing major roads.

## **Sidewalks**

The community of Strathmore has too few sidewalks. The residents that were surveyed reported that many streets in town do have a sidewalk. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's department seldom patrols the streets of Strathmore. Residents are especially concerned about how this affects the safety of their children. Residents state that some streets do not have poles and other streets have light poles but the light bulb is out, this issue should be easy to solve by simply replacing the bulb. Residents have asked their county Supervisor to address this issue and have been told that Tulare County does not know which poles belong to them and which belong to Southern California Edison.

## **Sheriff Presence**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Strathmore. Residents report that the Sheriff's Department response time is unacceptable and that there is little patrol within the community. Residents are worried about the rate of crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate makes

criminals feel unstoppable in Strathmore. Residents feel unsafe and they are asking for increased Sheriff Presence in their community.

### **Neighborhood Park**

In the community of Strathmore there is no safe public green space for children to play and adults to be physically active. The nearest park is located 6 miles away in the city of Porterville. Residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and concerned for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. Residents expresses that having a community park is a major priority for them.

### **Medical Clinic**

Strathmore residents do not have access to medical care within the community. The nearest medical clinic is approximately 6 miles away in Porterville. Residents report traveling 6 miles to Porterville or 5 miles to Lindsay for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in another community by paying for a ride or using public transportation to travel there. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Tulare 19 miles away, and it takes approximately minutes 25 minutes for the nearest ambulance to get to Strathmore from Tulare.

### **Internet**

Most families in Strathmore do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents. Without access to the internet, Strathmore residents are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Strathmore.

### **Animal Control**

Strathmore residents report that they have a problem with stray dogs and cats. Some dogs are dangerous and scare children, families feel that their kids are at risk of getting bit. This has become a big problem over the last few years. Residents are requesting that the Tulare County Animal Control Agency visit Strathmore to pick up stray animals on a regular basis.

## **Public Services**

Sheriff –Strathmore is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Strathmore and the nearest Sheriff substation is located in Porterville, 6 miles away.

Fire – There is a Tulare County Fire station located in Earlimart.

Schools – Strathmore Union Elementary School District

Libraries –Strathmore Public Library

Parks – The nearest park is located in Porterville CA, 6 miles

## **Sustainable Communities Strategy**

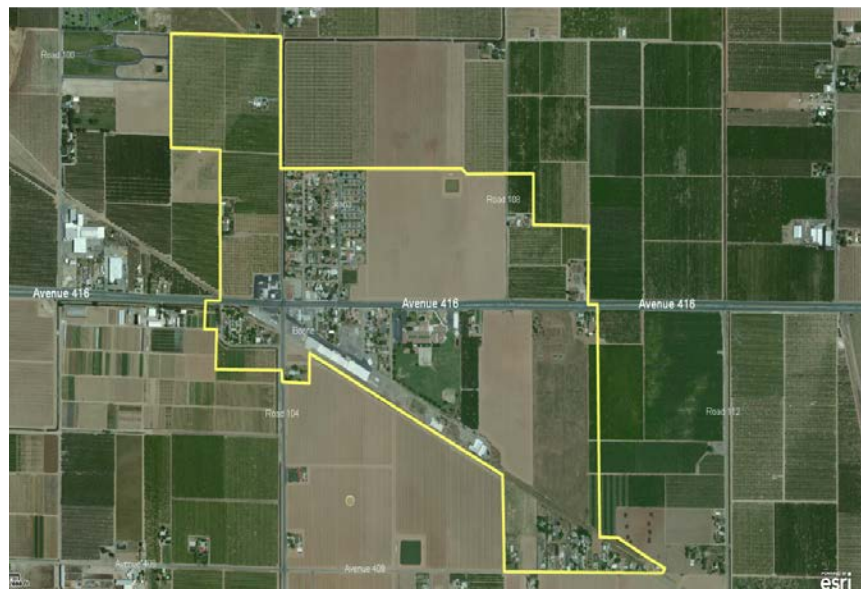
### **Strategic Growth Council**

#### **Outreach Report**

##### **Sultana**

#### **Location and introduction:**

The Tulare County community of Sultana is located approximately 3 miles east of the city of Dinuba. The 2010 United States census reported Sultanas population was 775 people.



#### **Community Outreach**

In efforts to invite all local residents and make them aware of the SGC project, Self Help Enterprises went door to door throughout the community of Sultana to distribute flyers at every home, business and school. During this process SHE talked with residents about the importance of participating in this process. Self Help Enterprises partnered with the Monson – Sultana School District and the Sultana Community Services District to spread the word about the project.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) On Thursday, November 12 and Wednesday November 18, 2015 two SHE staff went door to door to distribute flyers to homes in Sultana.
- 3.) SHE staff partnered with a member of the Sultana CSD to distribute fliers and spread the word throughout the community.
- 4.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance of participating in the SGC community meeting.
- 5.) SHE staff partnered with the Monson – Sultana School District to send meeting flyers to all 440 students at their school.
- 6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner stores, gas stations and businesses.

### **Community Meeting**

Self Help Enterprises (including two SHE staff) held a Strategic Growth Council community input meeting in Sultana on Thursday, November 19, 2015. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Monson – Sultana School. This is an ideal meeting place because it is centrally located and because people feel welcomed and safe there. 20 residents attended the meeting and 20 surveys were collected.



### **Community Survey**

A detailed survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodel Opportunities, and Priority Improvements.

During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/ concerns:

## **Priority Concerns**

1. Street Lights
2. Side walks
3. Community Park
4. Clinic
5. Internet
6. Police Patrol

## **Side Walks**

The community of Sultana has too few sidewalks. Some of residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk around town in the mud and through big puddles of water. The absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Sultana. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Internet Access**

Most families in Sultana do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Sultana.

## **Neighborhood Park**

In the community of Sultana there is no safe green space for children to play and adults to be physically active. The nearest park is located 4 miles away in the city of Dinuba, survey results show that in Sultana, children play in the street or do not play outside at all. London residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for



the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

### **Sheriff Presence**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Sultana. Residents report that the Sheriff's Department response time is unacceptable and that there is little police patrol within the community. Residents are worried about the rate of crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate makes criminals feel unstoppable in town.

### **Medical Clinic**

Sultana residents do not have access to medical care within the community. The nearest medical clinic is approximately 4 miles away in Dinuba. Most residents report traveling to Dinuba, 4 miles or Culter-Orosi, 5 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Visalia 17 miles away, and it takes approximately 20-25 minutes for the nearest ambulance to get to Sultana from Visalia.

### **Public Services**

Sheriff –Sultana is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Sultana and the nearest Sheriff substation is located in Visalia.

Fire –Tulare County Fire

Schools – Monson - Sultana School District in Sultana

Libraries – The nearest library is 4 miles away in Dinuba

Parks – The nearest park is located in Dinuba 4 miles away

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

##### **Teviston**



The Tulare County community of Teviston is located between the communities of Pixley and Earlimart along US Highway 99. According to the United States Census Bureau the population was 2,819 at the 2010 census. This community is considered disadvantaged based on the Median Household Income of \$26,250.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff went door to door to distribute flyers to every home and business. Door to door outreach was done on Wednesday December 2 and Tuesday December 8, 2015
- 3.) SHE staff distributed flyers at local churches
- 4.) SHE partnered with Teviston Betterment Association to do outreach for our community meeting.
- 5.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.

## **Community Meeting**

Self-Help Enterprises held a community meeting with two staff in Teviston on Thursday December 10, 2015 at 5:30pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Teviston Community Center 12934 Ave 80 in Teviston. This is an ideal meeting location because it is centrally located and most residents feel welcomed there. The purpose of the meeting was to discuss Teviston improvement needs, gather community data, and report the findings of the meeting in a final report. Over thirty people attended the meeting. 22 community surveys were collected. Representatives from the Teviston Betterment Association were present.



## **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Teviston SGC community input meeting, residents concluded that the following is a list of Teviston priority improvement needs

### **Priority Improvements**

- 1. Housing Rehab**
- 2. Sewer**
- 3. Road Safety**
- 4. Transportation**
- 5. Renovate the Community Center**

## **6. New Housing**

### **Road Conditions and Street Safety**

Teviston residents expressed that street safety, including the need for sidewalks, street lighting, speed limit signs, stop signs, and the need for enforcement of traffic laws are the highest priority improvement needs. Survey results show that most residents are not satisfied with road conditions

Road conditions are a big concern to Teviston residents. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on Teviston roads. Residents report that roads are narrow and that two cars barely fit on one road at the same time. Teviston roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired.

### **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Teviston. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

### **Affordable Housing**

Teviston residents want new affordable housing to be built in town. Many people expressed that they would buy a new home in town if it were available to them. Residents would like to see the town grow. Residents feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Tulare and Delano.

### **Home Repairs**

Teviston residents are concerned because many of their neighbors are living in homes that need repairs. People report that some of their neighbors are living in houses and trailers that are below the standard of living and are not up to building code. This is especially concerning because families with children and elderly people live in these spaces. Old homes are falling apart and facing situations such as bad flooring, no functioning heat or cooling system, no running water or sewer. Many homes need new energy saving windows and doors. Home owners and landlords are requesting housing rehabilitation.

## **Sewer**

There is no community wide sewer system in Teviston. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the Teviston residents are in favor of creating a community wide sewer system.

## **Renovate Community Center**

In Teviston, there is a building that is used as the local community center. The building is old and in need of repairs. If repaired, this community center is an ideal location for a multi-purpose center that could be used by kids for homework help, afterschool programs and as a computer lab, it also be available for use by outside organizations that need office space to service residents. This could also be the place where residents access a reliable internet connection. Teviston residents see this as a priority need and are asking for the County's assistance in identifying resources to improve and repair their local community center.

## **Public Transportation**

Teviston lacks access to public transportation. Residents that do not drive find it hard to travel outside of town. Often Teviston residents need to travel to Delano or Tulare for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes through Teviston, this would help many residents with their daily travel needs. The nearest bus stops are three miles away in Earlimart and Pixley.

## **Public Services**

Sheriff –Teviston is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Earlimart and the nearest Sheriff substation is located in Pixley, 3 miles away.

Fire –The nearest Tulare County Fire station located in Earlimart, 3 miles away

Schools –Pixley School District, 3 miles away

Libraries – Earlimart Public Library, 2 miles away

Parks – Pixley Park, 3 miles away

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

#### **Tooleville**

##### **Location and introduction:**

Tooleville is a small rural community located on the east side of Spruce Road roughly a mile and a half east of the city of Exeter in Tulare County. Homes in Tooleville are located along Alfred Avenue on the north and Morgan Avenue on the south, with a few homes fronting Spruce Road. In the 2010 census, the population was 339.



##### **Median household income**

Per the last decennial census to calculate median household income, the 2000 Census indicated the median annual income for households in Tulare County Census Tract 14 Block Group 4 that incorporates the community of Tooleville, was \$29,330 or 61.8% of the statewide median household income at that time. A 2005 community survey conducted by Tulare County and Self-Help Enterprises indicated the median household income at that time was \$15,500 which was roughly 26% of the statewide income at that time. It is recommended that the 2005 survey data be adjusted to the year 2010 for comparison purposes. With a CPI increase of 10.4% from 2005 to 2010, this would equate to \$17,118 in 2010 or 30% of the \$57,287 statewide MHI at that time. By comparison, the 2007-11 ACS figure is 42% of the statewide median household income during that period.



As such, Tooleville's median household income is well below the 60 percent of the statewide median household income threshold, justifying a determination that Tooleville is a severely disadvantaged community.

### **Community Outreach and Invitation Process**

In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises went door to door throughout the community of Tooleville to distribute flyers at every home and local business. During this process SHE talked with residents about the importance of participating in this process. Meeting invitations were sent to the members of the Tooleville Mutual Water Company. Fliers were posted at the local taco stand.

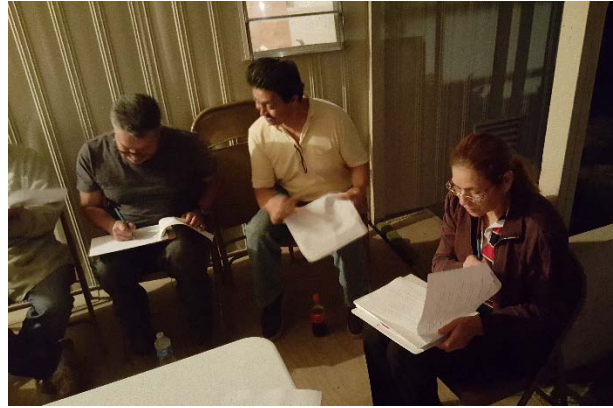
### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff partnered with Tooleville Mutual Water Company to post flyers and spread the word about the meeting. The Tooleville Water Mutual Company Board Members were invited to attend the meeting.
- 3.) SHE staff did door to door outreach on Tuesday October 20 and Thursday October 22, 2015.
- 4.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.

### **Community Meeting**

The Strategic Growth Council held a community input meeting in Tooleville on Tuesday October 27, 2015 at 6:00pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held at the office of the Tooleville Water Mutual Company. This is an ideal location because most residents know where the office is located.



## **Community Survey**

A detailed survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/ concerns:

### **Priority Concerns**

1. Stop Signs
2. Street lights
3. Clinic
4. Internet
5. Police Patrol
6. Storm Water Drainage
7. Water

### **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's Department seldom patrols Tooleville. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Stop Signs on Alfred and Morgan**

Tooleville is a small community comprised of two residential roads. Residents are concerned with road safety on the corners of Alfred and Spruce as well as Morgan and Spruce. Drivers state they cannot see oncoming traffic and that this has led to car crashes at both corners. Residents are asking for stop signs or flashing lights to slow traffic on Spruce Road.

## **Storm Water Drainage**

Tooleville residents report that the community does not have adequate storm water drainage. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season. This becomes a bigger problem when the stagnant water attracts insects and mosquitos.

## **Clinic**

Tooleville residents do not have access to medical care within the community. The nearest medical clinic is approximately 2.5 miles away in Exeter. Most residents report traveling to Visalia, 13 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. The nearest hospital is in Visalia, 13 miles away, and it takes approximately 20 minutes for the nearest ambulance to get to Tooleville from Visalia.

## **Internet**

Most families in Tooleville do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Tooleville.

## **Police Patrol**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Tooleville. Residents report that the TCSD response time is long and that there is too little Sheriff patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate and makes criminals feel unstoppable in that area of Tooleville. Tooleville residents are asking for increased Sheriff Presence in their community.

**Water** – Tooleville Mutual Non-Profit Water Association is a small mutual water company ran by a five-member board. Tooleville has two undependable water wells and is planning to drill a new well once the location has been determined. They are activity searching for potential well sites in Tooleville and neighboring Exeter. Tooleville is exploring the different ways that could potentially partner with Exeter. TMNPWA is currently reviewing three options: Water wheeling, Master meter or full consolidation with the City of Exeter.

### **Public Services**

Sheriff – Tulare County Sherriff Department

Fire –Tulare County Fire Department

Schools – Exeter School District, 2 miles away

Libraries – The nearest public library is in Exeter

Parks – Exeter Library, 2 miles away

## **Sustainable Communities Strategy**

### **Strategic Growth Council**

#### **Outreach Report**

#### **Waukena**

Waukena is a small community located on California State Route 137, 4 miles northeast of Corcoran and 13.5 southwest of Tulare. Waukena covers an area of 0.9 square miles, the population was 108 at the 2010 census. The Median Household Income is



#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff went door to door to distribute flyers to every home and business in Waukena during the day and in the evening on Saturday February 13 and Thursday February 18, 2016
- 3.) SHE partnered with the Waukena School District to distribute flyers to their students. Meeting flyers were sent home from school with each of the 250 students of the WSD.
- 5.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- 6.) Flyers were posted at the local corner store and the local post office.

## **Community Meeting**

Self-Help Enterprises conducted a community meeting in Waukena on Saturday February 20, 2016 at 3:00pm. The meeting was scheduled during the weekend to make it easier for working residents to attend. The meeting was held at Waukena School. The purpose of the meeting was to discuss Waukena improvement needs, gather community data, and report the findings of the meeting in a final report. Ten residents attended the meeting. 9 community surveys were collected.



## **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Waukena SGC community input meeting, residents concluded that the following is a list of Waukena priority improvement needs

### **Priority Improvements**

1. Waukena School Improvements
2. School Traffic Safety
3. Road Conditions

### **Waukena School Improvements**

The residents expressed concern for the condition of the Waukena School and said that repairing the school is the priority improvement need in town. Waukena School is a small k-8 school district with approximately 260 students, residents said that the schools buildings desperately needs repairs and modernization. Additionally, the building has an



asbestos problem. The school district lacks the resources and funding opportunities to cover the cost of repairs, the community is looking for funds to help. The residents in attendance agreed that the safety and condition of the school for their kids is their highest priority.

### **School Traffic Safety**

Parents expressed concern for the speed of traffic that travels in front of the school house. Residents are requesting assistance to slow down the traffic, this can be done with a speed bump, flashing lights or caution signs warning drivers of the school zone and children crossing, parents are concerned for the safety of their students.

### **Road Conditions**

Residents expressed a need for improvement to Waukena residential roads, the main road gets repaired but the other roads are over looked. The residents are requesting repairs to the following streets:

- Rd 28
- Rd 36
- Rd 42
- Rd 32
- Ave 176
- Ave 192

### **Waukena Schools**

The Waukena School District water well has continuously exceeded the MCL for Nitrates, the district has had to provide bottled water to students for many years, currently they are receiving grant funds to cover the costs of the bottle water through a 36 months grant from the County.

### **Public Services**

Sheriff – Waukena is in the jurisdiction of Tulare County Sheriff's Department (TCSD).

Fire –Tulare County Fire. Nearest fire station is unknown

Schools –Waukena School District (K-8)

Libraries – The nearest Library is Corcoran

Parks – The nearest Park is in Corcoran

# Sustainable Communities Strategy

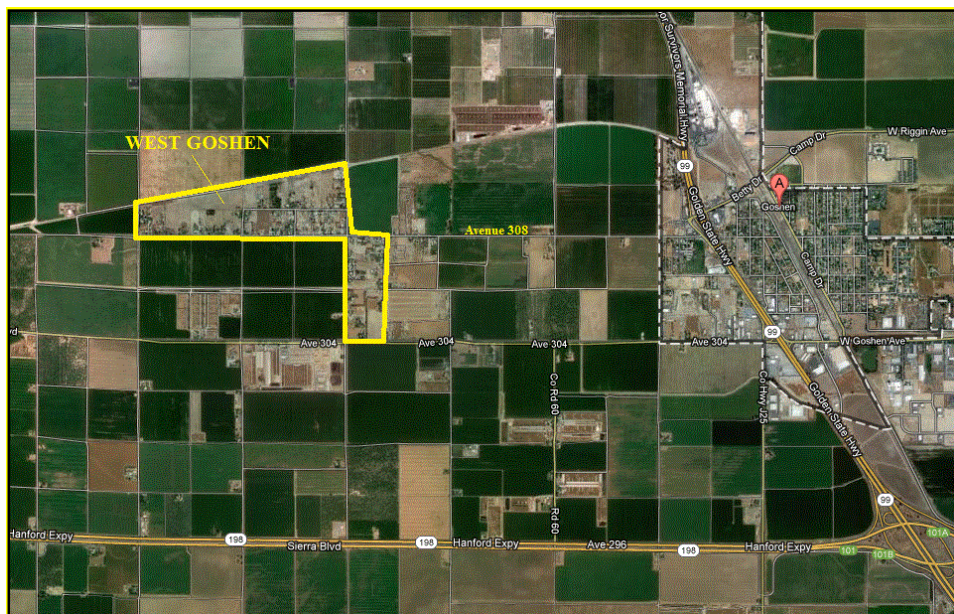
## Strategic Growth Council

### Outreach Report

#### West Goshen

#### Location and introduction:

The Tulare County community of West Goshen is located approximately one mile and a half west of the town of Goshen along Avenue 308, just east of the border with Kings County.



#### Median household income

Per the last decennial census to calculate median household income, the 2000 Census indicated the median annual income for households in Tulare County Census Tract 9 Block Group 3 that incorporates the community of West Goshen, was \$36,528 or 76.9% of the statewide median household income at that time. This data is expressed as a 5-year adjusted average. In 2010 West Goshen became a Census Designated Place (CDP). The median annual household income for the Year 2000 Census of the larger Tulare County Census Tract 9 Block Group 3, the past four rounds (one as a Census Tract Block Group and 3 as a CDP) of the ACS, and the community survey conducted in 2013 is expressed as:

Period	Area	MHI	Margin of Error	% of State MHI
2000	CT9BG3	\$36,528		76.9%
2005-09	CT9BG3	\$52,500	+/- \$13,971	86.9%
2006-10	CDP	\$41,250	+/- \$8,558	72.0%
2007-11	CDP	\$24,083	+/- \$35,214	39.1%
2013 Survey		\$21,000		34.2%
2008-12	CDP	\$14,208	+/- \$27,289	23.1%
2009-13	CDP	\$14,208	+/- \$27,289	23.1%
2010-14	CDP	\$20,350	+/- \$15,351	23.1%

Earlier Census Tract Block Group data and even CDP data from the ACS with high margins of error were not very representative of the community's true annual median household income. For this reason, a community survey was conducted in May 2013 by Rural Community Assistance Corporation assisted by Self-Help Enterprises and funded by CDPH. The annual Median Household Income was found to be \$21,000. As such, West Goshen's median household income is well below the 60 percent of the statewide median household income threshold, justifying a determination that West Goshen is a severely disadvantaged community.

### **Community Outreach and Invitation Process**

In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises went door to door throughout the community of West Goshen to distribute flyers at every home, business and school. During this process SHE talked with residents about the importance of participating in this process. SGC partnered with the Goshen School to send flyers home with every student. Meeting invitations were sent to the members of the West Goshen Mutual Water Company, county Supervisor Steve Worthley was invited. Fliers were posted at local stores and gas stations.

### **Community Outreach Steps**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) On September 13 and 14, 2015 SHE staff went door to door to distribute flyers to homes in West Goshen.
- 3.) SHE staff partnered with members of the West Goshen Water Mutual Company to spread the word about the importance of attending this meeting. This group distributed fliers on September 15 and 16, 2015
- 4.) SHE made phone calls to local residents.

5.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.

6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner store and all businesses.

### **Community Meeting**

The Strategic Growth Council held a community input meeting in W. Goshen on Thursday, September 17, 2015 at 6:00pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Goshen school located at 6505 Ave 308 Visalia CA. This is an ideal location because most residents know where the school is located and they feel welcomed and comfortable there. 21 West Goshen residents attended the meeting. 20 surveys were collected.

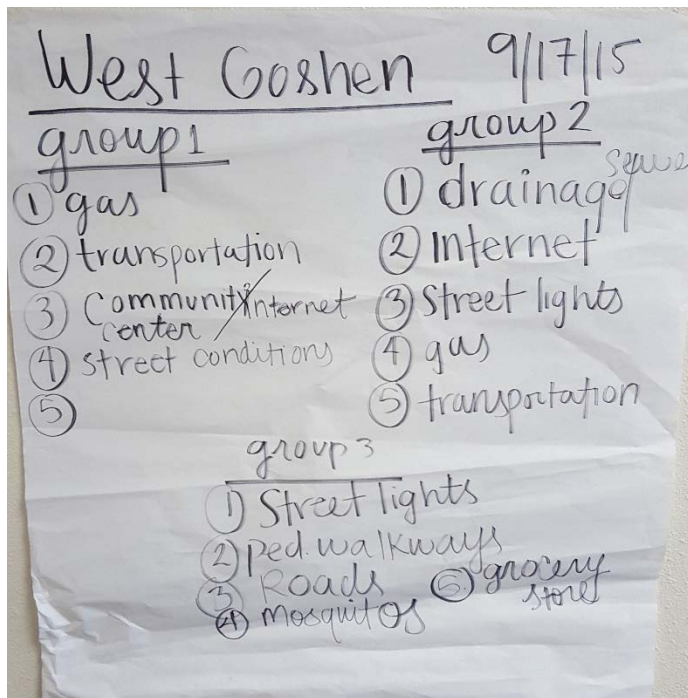


### **Community Survey**

A detailed survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/ concerns:

## Priority Concerns



1. Natural Gas
2. Sewer
3. Transportation/Bus Stops
4. Street Conditions
5. Street lights
6. Internet
7. Community Center

## Road Conditions

Roads conditions are a huge concern to the residents of W. Goshen. It is reported that road conditions are poor and most roads need work. The roads have potholes, cracks and bumps. This makes residents concerned about both driving conditions and walkability. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving. West Goshen roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not repaired.

The following roads are a priority to the residents:

- \*Ave 308
- Ave 309
- \*Road 52
- Road 50

- Road 48
- Markham

## **Sidewalks**

The community of West Goshen has too few sidewalks. Residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids and parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk in the mud and through big puddles of water. West Goshen traffic travels at a high rate of speed (see next section), and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Traffic Rules Enforcement to Slow Traffic**

The area of West Goshen has a problem with speeding traffic. Residents expressed that many vehicles travel at an unsafe rate of speed and there are no speed limit signs to enforce the speed limit. Residents believe that by installing speed limit signs to clearly display the legal speed limit, this issue could be improved. A speed limit sign is especially needed on West Goshen's main road, Road 48. Along with speed limit signs, residents feel traffic law enforcement is needed. The California Highway Patrol and the Tulare County Sheriff's Department are rarely seen in West Goshen, the result of this is that people are rarely cited for violating traffic laws.

## **Natural Gas**

Some residents report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that the residents that do not have natural gas would like to have it but cannot afford the cost of extending gas lines to their homes. Residents are requesting assistance from Tulare County to access resources to help them cover the cost of extending gas lines to their homes.

## **Sewer**

There is no community wide sewer system in West Goshen. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the West Goshen residents are in favor of creating a community wide sewer system.

## **Storm Water Drainage**

Residents report that the community does not have adequate storm water drainage. Large puddles form when it rains and the water does not drain because there is



nowhere for the water to go. Residents report that some streets flood during the rainy season especially Road 308 and Road 309. The stagnant water attracts insects and mosquitos which adds another problem to the issue.

### **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols West Goshen. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

### **Internet Access**

Most families in West Goshen do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in West Goshen.

### **Community Resource Center**

West Goshen does not have a community hub. The community lacks many resources and needs a place where residents can go to access information and county wide resources. People in West Goshen expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service West Goshen residents. This could also be the place where residents access a reliable internet connection.

### **Water**

West Goshen Mutual Water Company, a small mutual made up of approximately 90 water customers is in the process of dissolving as they are now fully consolidated with Calwater. A few private wells in the area were also able to join in on the consolidation.

### **Community Resources**

Sheriff –West Goshen is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of West Goshen.

Fire –Tulare County Fire

Schools – Goshen School is part of the Visalia Unified School District.

Libraries – The nearest library is located 8 miles away in Visalia.

Parks – The nearest community park is located 8 miles away in Visalia.

## **Strategic Growth Council**

### **Outreach Report**

#### **Yettem**

Yettem is a small, rural community located in the eastern part of Tulare County, located about 12 miles east of Visalia CA. Yettem is a census designated place, the 2010 United States census reported Yettem's population was 211.

#### **Community Outreach and Invitation Process**

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff developed an informational flyer. (See attached.)
- 2.) SHE staff went door to door to distribute flyers to every home, business and school in the community of Woodville. Door to door outreach was done on October 1, October 3 and October 5, 2015.
- 3.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- 4.) SHE partnered with the Stone Corral, located in neighboring Seville to spread the word about the meeting; meeting flyers were sent home from school with each student including those students that live in Yettem.
- 5.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local stores and local businesses.

#### **Community Meeting**

The Strategic Growth Council held a community for input meeting for Yettem and Seville residents in Seville on Wednesday, December 9, 2015 at 5:30pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Stone Corral School located at 15590 Ave 383 in Seville. This is an ideal location because most residents know where the school is located. One Yettem resident attended this meeting.

#### **Door to door Surveys**

In efforts to reach Yettem residents and gather the information needed for this study, SHE conducted door to door surveys throughout the community of Yettem. 9 surveys were collected.

## **Community Survey**

A survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

### **Priority Concerns**

1. Street lighting
2. Internet Access
3. Road Conditions
4. Speed Bumps
5. Side Walks
6. Public Transportation
7. Community resource center
8. Community Park

### **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's Department seldom patrols Yettem. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

### **Internet Access**

Most families in Yettem do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Yettem.

### **Road Conditions**

Yettem survey results show that most residents are not satisfied with road conditions. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving on their roads. Yettem roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired.

The following roads are a priority for residents:

- Road 383
- Road 153
- Road 142

### **Side Walks**

The community of Yettem has too few sidewalks. The residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Yettem traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

### **Speed Bumps**

Residents reported that they are concerned for the safety of their children while they are outside because traffic travels at a high speed in remote areas of the County such as Yettem. Parents are requesting that the County install speed bumps to help slow traffic.

### **Additional Bus Stop**

Residents report that the TCAT has one bus stop in Yettem. This location is too far for some people to walk to. Residents are requesting an additional bus stop location in town.

### **Community Resource Center**

Yettem does not have a community hub. The community lacks many resources and needs a place where residents can access information and county resources. People surveyed in Yettem, expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Yettem residents. This could also be the place where residents access a reliable internet connection.

### **Community Park**

In the community of Yettem there is no safe public green space for children to play and adults to be physically active. The nearest park is located 6 miles away in the city of Orosi. Residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and concerned for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. Residents express that having a community park is a major priority for them.

**Public Services**

Sheriff –Yettem is in the jurisdiction of Tulare County Sheriff's Department (TCSD).

Fire –Tulare County Fire, nearest fire station is in Oroshi, 6 miles

Schools – Stone Corral School in Seville, 1.5 miles away

Libraries – The nearest public library is in Visalia

Parks – Yettem, the nearest park is in Cutler-Orosi, 6 miles away



## **Appendix B: Community Plans**

**Alpaugh  
East Orosi  
London  
Richgrove  
Sultana**

## **Alpaugh Community Plan**

# ALPAUGH COMMUNITY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

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## Alpaugh Community Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9371, 9372, 9373, 9334, 9375, and 9376

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-042  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-041

Alpaugh Community Plan: GPA 17-004  
Zoning District Map: PZC 17-004



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000



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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

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Alexandra J. Vander Poel, Grant Specialist I

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## EXECUTIVE SUMMARY

### INTRODUCTION

The primary objective in preparing a Community Plan for Alpaugh is to develop a plan which can accurately reflect the needs and priorities of this community. Alpaugh is currently designated as a Community in the 2030 Tulare County General Plan (2012). A more precise plan is needed to allow for the potential to increase the availability of infrastructure funding (for things such as drinking water system improvements [for example wells, water distribution piping, and storage tanks], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

Alpaugh is a Census-Designated Place located in the southwest portion of Tulare County. It is bounded by Avenue 50 in the south, Avenue 58 in the north, Road 34 in the west, and Road 42 in the east and encompasses one (1) square mile of land. It is not directly served by any State Route. The Tulare County/Kings County Line is located approximately two miles west of Alpaugh, and the Tulare County/Kern County Line is located approximately seven miles south of Alpaugh. Communities located near Alpaugh include Allensworth and Earlimart to the east, Pixley to the northeast, Delano to the southeast, and Corcoran to the northwest. Alpaugh is an agriculturally oriented service community surrounded on all sides by lands in agricultural production, scattered rural residential uses, and vacant land.

Alpaugh's location (once also called Hog Island, Root Island, and Atwell's Island) was once either on an island or a narrow peninsula near the south end of the Tulare Lake. A. J. Atwell was a Visalia attorney (and newspaper owner) who raised hogs on the island. The lake at different times supported a very large Indian population, a commercial fishery, herds of tule elk and game birds. The island was a regular port of call for the lake's commercial ferry service. 1878 was the last time Tulare Lake overflowed into the San Joaquin River to the San Francisco Bay.

### Location

Alpaugh is an agriculturally oriented service community surrounded by agricultural production, scattered rural residential uses, and vacant land. The Community of Alpaugh (**see Figure 1**) is located on the east side of the San Joaquin Valley. Alpaugh is a census-designated place located in the southwest portion of Tulare County. It is generally bounded by Avenue 50 in the south, Avenue 58 in the north, Road 34 in the west, and Road 42 in the east and encompasses one (1) square mile of land. It is not directly served by any State Route.

The Tulare County/Kings County Line is located approximately two miles west of Alpaugh, and the Tulare County/Kern County Line is located approximately seven miles south of Alpaugh. Communities located near Alpaugh include Allensworth and Earlimart to the east, Pixley to the northeast, Delano to the southeast, and Corcoran to the northwest.

Alpaugh is in Sections 27, 28, 33, & 34, Township 23 South, Range 23 East MDB&M, and can be found within the Alpaugh, United States Geological Survey 7.5-minute topographic quadrangle. Alpaugh is located at an elevation of 213 feet above sea level. The coordinates of Alpaugh are: Latitude: 35.8876 and Longitude: -119.4873 (**See Figure 2**).

# ALPAUGH COMMUNITY PLAN

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## Planning Area

The Alpaugh Urban Development Boundary (UDB) area consists of 205.4 acres (**see Figure 3**). The existing uses within the UDB are described as follows. Agricultural activities, including orchards and pasture, currently occupy 3 percent of the 205.4 acres. Urban development, including urbanized uses such as residential, commercial, public and quasi-public facilities, as well as industrial development occupy 78 percent of the 205.4 acres. The remaining 22 percent are lands dedicated for Right-of-way (**see Table 1**).

**Table 1 - Alpaugh Land Use**

Land Use	Acres
Residential	146.4
Commercial	7.6
Agricultural	6.6
Rights-of-way	44.7
<b>Total</b>	<b>205.4</b>

Source: Tulare County GIS

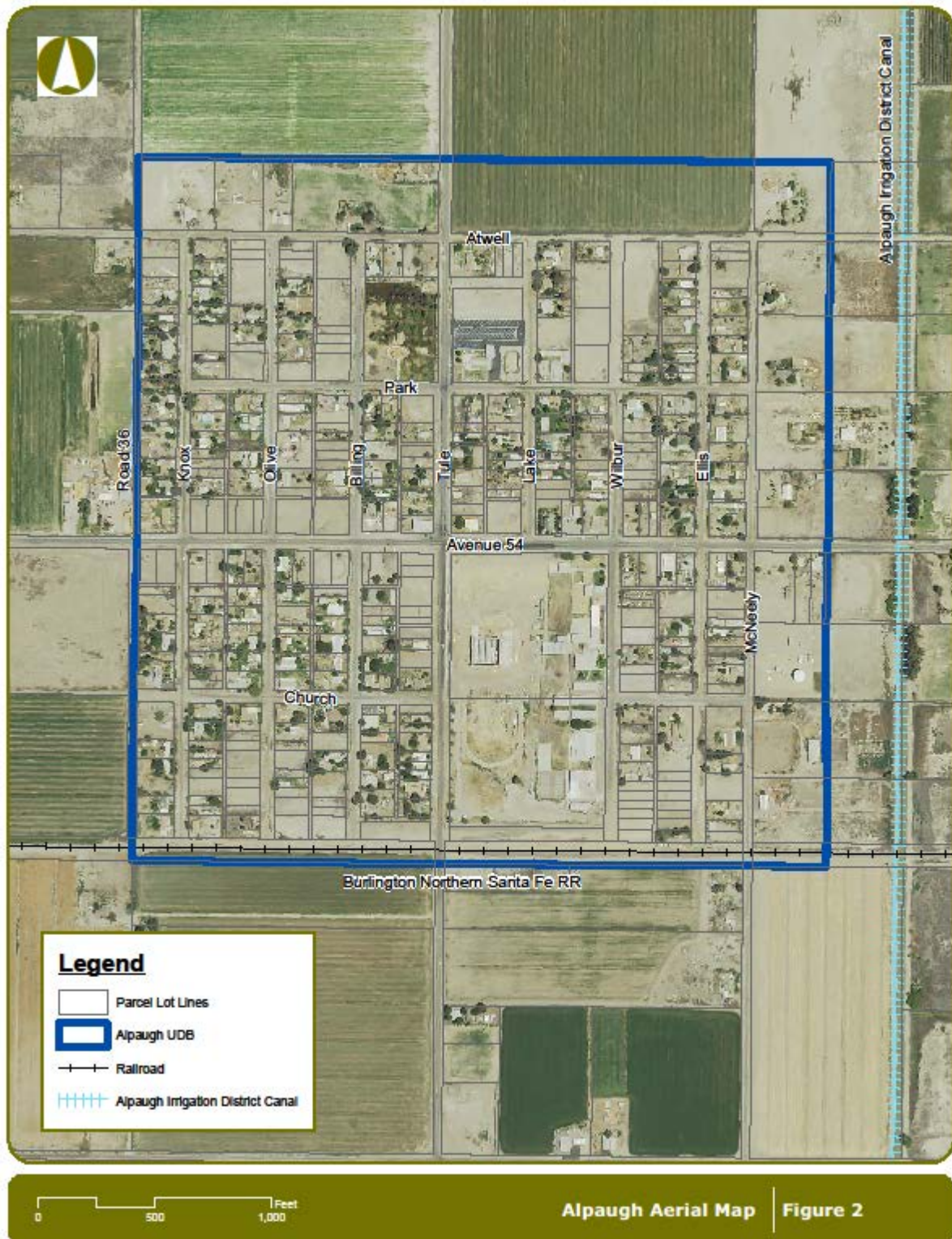
No change is proposed to the Urban Development Boundary.





# ALPAUGH COMMUNITY PLAN

Figure 2 – Aerial Map - Alpaugh



# ALPAUGH COMMUNITY PLAN

Figure 3 - Alpaugh UDB



# ALPAUGH COMMUNITY PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Outreach and Invitation Process

The County of Tulare was successful in obtaining a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Community Plan. Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

### Community Outreach Steps

Self-Help Enterprises (SHE) staff conducted the following outreach efforts to promote community participation and make local residents aware of the SGC project:

- 1.) An informational flyer was developed.
- 2.) SHE staff distributed flyers to homes, businesses, and schools in the community of Alpaugh. Door to door outreach was done on August 30, September 1, and September 2, 2015.
- 3.) During the community outreach process, SHE staff described the goals of the project and emphasized the importance and community benefits of participating in the SGC community meeting.
- 4.) SHE partnered with the Alpaugh Unified School District to advertise meetings. Flyers were sent home with each student.
- 5.) Meeting invitations were sent to the Boards of Directors of the Alpaugh Unified School District, the Alpaugh Community Service District and the Alpaugh Irrigation District.
- 6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner store, local businesses and the Alpaugh post office.

### Community Meetings

Self-Help Enterprises held a community meeting (with five SHE staff members present) on September 3, 2015 at 6:00 pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held in the cafeteria of the Alpaugh Elementary School located at 5313 Road 39 in Alpaugh. A second meeting was held with the County and SHE on January 11, 2017 to review the findings of the survey. The purpose of the meeting was to discuss Alpaugh improvement needs, gather community data, and report the findings of the meeting in a final report. Nineteen Alpaugh residents attended the meetings and 16 community surveys were collected. At the follow up meeting over 5 members attended the meeting.



# ALPAUGH COMMUNITY PLAN

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## Community Survey

A survey was developed to gather a variety of community information about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After discussion at the Alpaugh SGC community input meeting, residents concluded that the following is a list of Alpaugh priority improvement needs

## Priority Improvements

- 1) Road Conditions & Street Safety
- 2) Community Safety
- 3) Medical Care Access
- 4) Community Sewer System
- 5) Local Market
- 6) Internet Access
- 7) Community Resource Center
- 8) Loose Dogs
- 9) Natural Gas
- 10) County Park Management

## **Road Conditions and Street Safety**

Alpaugh residents expressed that street safety, including the need for sidewalks, street lighting, speed limit signs, stop signs, and the need for enforcement of traffic laws are the highest priority.

## **Road Conditions and Street Safety**

Alpaugh residents expressed that street safety, including the need for sidewalks, street lighting, speed limit signs, stop signs, and the need for enforcement of traffic laws are the highest priority improvement needs in Alpaugh. Survey results show that most residents are not satisfied with road

# ALPAUGH COMMUNITY PLAN

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conditions. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on Alpaugh roads. Residents report that roads are narrow and that two cars barely fit on one road at the same time. Alpaugh roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired. 100% of the respondents are not satisfied with the conditions of the roads. 100% of the respondents described the roads to be in “poor” conditions.

The following roads are a priority to the residents:

- Tule Road
- Avenue 54
- Ellis Road
- Avenue 50

## **Survey comments regarding road conditions:**

- “Very bad road conditions”
- “Roads have not been fixed in many years”
- “Dark roads, no street lights”
- “Street lighting is not safe for children”
- “All of the roads bumps and pot holes make for dangerous driving conditions”
- “Road need sand or patching”
- “Roads are so bad, we need a four-wheel drive to drive down the road”
- “Road conditions cause a bumpy ride and many burst tires”
- “The roads around the school are bad”
- “Caution signs needed around the school”
- “Narrow roads, only room for one car”

## **Sidewalks**

The community of Alpaugh has too few sidewalks. There are sidewalks around the north and west side of the school, and a short stretch of sidewalk in front of the library, post office and store only. The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Alpaugh traffic travels at a high rate of speed (see next section), and the absence of sidewalks means there is a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Traffic Law Enforcement**

Residents report that some vehicles travel at an unsafe rate of speed and the roads in town have few speed limit signs. Alpaugh residents believe that by installing speed limit signs to clearly display the legal speed limit, this issue could be improved. A speed limit sign is especially needed on Alpaugh’s main road, Road 54. Road 54 has the most traffic, including heavy truck traffic, and the most speeders. Along with speed limit signs, residents feel a need for better enforcement of traffic laws. The California Highway Patrol and the Tulare County Sheriff’s Department are rarely seen in the community of Alpaugh; the result of this is that people are rarely cited for violating traffic laws.

# ALPAUGH COMMUNITY PLAN

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## **Street Lights**

Residents report dark streets at night due to little or no street lighting, and that they stay indoors because they do not feel safe walking on a dark street. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Community Safety**

Community safety is ranked among the highest concerns for the residents of Alpaugh. Safety concerns stem from the Tulare County Sheriff Department's delayed response time to 911 emergency calls. Residents report that on multiple occasions they have called the Tulare County Sheriff Department at the time of an emergency and the sheriff department either arrived roughly three or four hours after the call or, at times, they did not respond at all. Families expressed that they do not feel safe living in the community of Alpaugh. Multiple residents stated that they have had home burglaries and other emergencies when they had to resort to using personal weapons as a form of protection because the Sheriff Department did not respond in a timely manner. During the SGC community meeting, the community talked about a need for more Sheriff's patrols in Alpaugh. Safety at the County's Park in Alpaugh is also a concern. Residents reported that two young girls were raped in the park restroom and that families are scared to use the park. Residents feel that if there was more of a Sheriff's presence and more police patrol this would be noticed by criminals and deter them from criminal activity. This law enforcement presence would also help families feel safer in town.

## **Sheriff Presence**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Alpaugh. Residents report that the TCSD response time is long and that there is inconsistent police patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate. Alpaugh residents are asking for increased Sheriff presence in their community.

## **Medical Clinic**

Alpaugh residents do not have access to medical care within the community. The nearest medical clinic is approximately 13 miles away in Earlimart. Residents report traveling to Delano, 20 miles, Corcoran, 17 miles and Tulare 35 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. The Tulare County Area Transit does not make regularly scheduled stops in Alpaugh, the TCAT will pick up Alpaugh residents by appointment when calling the day before. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Delano, 20 miles away, and it takes approximately 30 minutes for the nearest ambulance to get to Alpaugh from Delano.

## **Sewer**

There is no community wide sewer system in Alpaugh. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the Alpaugh residents are in favor of creating a community wide sewer system.



# ALPAUGH COMMUNITY PLAN

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## **Internet Access**

Most families in Alpaugh do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Alpaugh.

## **Community Resource Center**

Alpaugh does not have a community hub. The community lacks many resources and needs a place where residents can go to access information and county resources. People in Alpaugh expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Alpaugh residents. This could also be the place where residents access a reliable internet connection.

## **Local Market**

The community of Alpaugh is a food desert. This community does not have a local food market that sells fresh produce, meat and dairy. The Alpaugh Grocery Store and Express Gas Mart sell mainly highly processed, packaged, and un-nutritious food. In addition, the food prices are much higher compared to neighboring communities and the quality of food is not as fresh as it is in other communities. Alpaugh residents regularly travel to Delano (20 miles) or Corcoran (17 miles) to buy their groceries.

## **Natural Gas**

Some residents, particularly those on the outskirts of the community, report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that the residents that do not have natural gas would like to have it but cannot afford the cost of extending gas lines to their homes.

## **Loose Dogs**

Stray dogs are a problem in the community of Alpaugh. Residents feel unsafe and are worried that their children will be bitten by a stray dog. Residents report unwanted dogs in their yards and on Alpaugh roads. Residents say the Tulare County Animal Control no longer comes to Alpaugh to pick up stray dogs. Residents said that this service is needed and would help with this problem.

## **County Park Management**

Residents report that the county park in Alpaugh is not maintained properly and that it is an unsafe place to be. The restrooms are reported unclean and the park grounds are not regularly maintained. Residents do not use the public park because they are worried about gang and criminal activity there. Alpaugh residents would like to use their park and feel safe while there. However, the lack of park maintenance and lack of Sheriff Patrol make it unwelcome.

## **Storm Water Drainage**

Alpaugh residents report that the community does not have adequate storm water drainage. This is a problem because the stagnant water attracts insects. Large puddles form when it rains and the water does not drain. Residents report that some streets flood during the rainy season.

# ALPAUGH COMMUNITY PLAN

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## **Water**

The chief problem facing Alpaugh's water system is its consistent violation of the arsenic Maximum Contaminant Level (MCL). A pilot study to analyze the feasibility of arsenic treatment has recently been concluded. This pilot study, funded by the State Water Resources Control Board with a planning grant under Proposition 84, concluded that arsenic removal is a feasible option using an adsorptive process. Design for the new water treatment plant was completed in December 2015 and Alpaugh CSD is applying to the State for construction funding. Alpaugh also has some hydrogen sulfide odor problems, which they address by chlorinating. During the SGC community meeting residents expressed that the water quality problem is an important issue; however, most feel that the Alpaugh Community Services District is working to address the issue and that they are satisfied with the current work being done to analyze multiple solutions. For that reason water is not listed as a priority improvement in this report. (Note: under a separate grant from the Strategic Growth Council, regional solutions to address water quality problems in Alpaugh and Allensworth are being assessed and vetted with the communities, and it is possible that the Alpaugh water treatment plant may supply Allensworth as well.) Alpaugh also faces challenges with its extensive and aging water distribution system. The core of the system was replaced in the last ten years, but many miles of pipe to the outlying areas is disintegrating and needs replacement. Attempts at repairs result in worse breakage.

## **Public Services**

- Sheriff – Alpaugh is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Alpaugh and the nearest Sheriff substation is located in Pixley, 19 miles away.
- Fire – Tulare County Fire, Alpaugh Station, located in Alpaugh
- Schools - Alpaugh Unified School District (K-12), located in Alpaugh, is part of Tulare County Office of Education.
- Libraries – There is a small public library in Alpaugh, operated by the Tulare County Library system, located on Avenue 54 and open twice a week.
- Parks – Alpaugh Community Park operated by the Tulare County Department of Parks and Recreation is in Alpaugh on Tule Road.

## Survey Results Meeting

A follow up meeting was held on January 11<sup>th</sup> at 4:00 in at the Alpaugh Community Service District Building, which included County Staff, a SHE Representative and 5 residents (including the CSD Executive Officer) attended. After the results of the survey were reported to the residents the remaining issues were discussed:

- Street lights – the residents reported that they know street lights exist but that they are not functioning
- Safety – the residents reported that increased presence by the Sherriff would assist in deterring crime in the area, and they suggested police cameras be used.
- Roads – generally the state of roads was of concern, but the striping on the roads was fading and would assist in driving in the fog.
- Speed and “blinkly lights” (single aspect yellow LED lights) – that posting speeds, with the warning “blinkly lights” on Ave 56 and Ave 62/ Tule would be helpful in reducing speed and make the community feel safer, if they could slow down the truck driving through.
- It was stated that nearly 17 % of Alpaugh residents are still using propane, but of those in attendance, all were on natural gas.

# ALPAUGH COMMUNITY PLAN

## ALPAUGH TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Community's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Community plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Community allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Community. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of a smaller community such as Alpaugh.

#### Population

In 2015, the Population for Alpaugh was 1,103 (See Table 2).

Table 2 - Population						
	California	%	Tulare County	%	Alpaugh	%
Male	19,087,135	49.7%	227,426	50.1%	558	50.6%
Female	19,334,329	50.3%	226,607	49.9%	454	49.4%
Total	38,421,464		454,033		1,103	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Alpaugh's population during the Planning Period (see Table 3).

Table 3 - Projected Annual Growth Rates		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7

# ALPAUGH COMMUNITY PLAN

## Median Age

Alpaugh's population is younger than the median age throughout all of Tulare County. Alpaugh's median age of 22.3 is lower than the median age of the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Alpaugh	23.0

2011-2015 American Community Survey  
5-year Estimates

## Ethnicity and Race

In 2015, approximately 17% of Alpaugh's population was White, 0% was African American, 4.7% was Native American, 0.4% was Asian, and 0.4% was two races or more (see Table 5). Approximately 77.4% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues, which may affect their community.

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Alpaugh	%
Total	38,421,464		454,033		1,103	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	854	77.4%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	187	17%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	52	4.7%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	4	0.4%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	6	0.5%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Alpaugh

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see Table 6).

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Alpaugh was about 7.8% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 7.8% includes only the employable labor force (that is, not every person of the population), results in about 51 unemployed persons of Alpaugh's person labor force of 353.

# ALPAUGH COMMUNITY PLAN

Table 6 - Employment Status			
Employment Status	California	Tulare County	Alpaugh
Population 16 years & over	30,312,429	325,404	653
In labor force	19,269,449	194,420	353
Civilian labor force	19,137,441	194,102	353
Employed	17,246,360	170,780	302
Unemployed	1,891,081	23,322	51
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	300

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Median income in Alpaugh was lower than the State of California and the County of Tulare. Alpaugh's median household income was \$27,222, compared to \$61,818 for the State of California and \$42,031 for Tulare County (see Table 7).

Table 7 - 2011-2015 American Community Survey Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Alpaugh CDP	\$27,222	\$27,232	\$8,935

2011-2015 American Community Survey 5-Year Estimates

## Severely Disadvantaged Community

Alpaugh is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Alpaugh's median household income was \$27,222, whereas the State of California's median household income was \$61,818. Alpaugh's median household income was 44% of the State of California's median household income, therefore it is considered a severely disadvantaged community.

# ALPAUGH COMMUNITY PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes) (see Table 8 and 9).

Table 8 - Housing Tenure			
	California	Tulare County	Alpaugh
Occupied housing units	12,717,801	133,570	262
Owner-occupied	6,909,176	75,685	136
Renter-occupied	5,808,625	57,885	126

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Alpaugh
Owner-occupied	54.3%	56.7%	51.9%
Renter-occupied	45.7%	43.3%	48.1%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in; without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Alpaugh	1	2	1	2	14	25	14	27	23	44	52

Source: Tulare County 2015 Housing Element

According to the 2015, Tulare County Housing Element, a sampling of housing conditions in Alpaugh indicated that approximately 2% of the housing units were sound. Approximately 51% of Alpaugh's housing units were deteriorated and 44% were dilapidated (see Table 10).

# ALPAUGH COMMUNITY PLAN

## Age of Structures

According to the U.S. Census, the 2011 – 2015 Community survey noted that most of the housing structures in Alpaugh were built between 1950 and 2009 (see Table 11). In 2015, the total number of housing units in Alpaugh is 289.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Alpaugh
Total housing units	13,845,790	144,792	289
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	27
Built 1990 to 1999	1,495,571	21,767	28
Built 1980 to 1989	2,117,819	22,733	31
Built 1970 to 1979	2,503,688	27,111	94
Built 1960 to 1969	1,871,029	15,500	48
Built 1950 to 1959	1,907,512	13,694	37
Built 1940 to 1949	865,607	7,494	16
Built 1939 or earlier	1,298,438	8,186	8

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015, the average renter occupied household size in Alpaugh was 4.38 persons per household (see Table 12). The average owner occupied household size is 4.05.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Alpaugh	4.05	4.38

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults. Permanent

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Alpaugh was 3.1%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Alpaugh	0.0%	3.1%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)



# ALPAUGH COMMUNITY PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

A California Natural Diversity Database (CNDDDB) search conducted on August 9, 2016 (see **Figure 4**) indicates there are special status species within the Alpaugh Quadrant Species List (which includes the Alpaugh Planning Study Area) consisting of seven animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Swainson's hawk (*Buteo swainsoni*, state endangered); vernal pool fairy shrimp (*Branchinecta lynchi*, federal threatened); Tipton kangaroo rat (*Dipodomys nitratoides*, federal endangered); Blunt-nosed leopard lizard (*Gambelia sila*, federal endangered); Western snowy plover (*Charadrius alexandrinus nivosus*, federal threatened); and San Joaquin woollythreads (*Monolopia congdonii*, federal endangered).

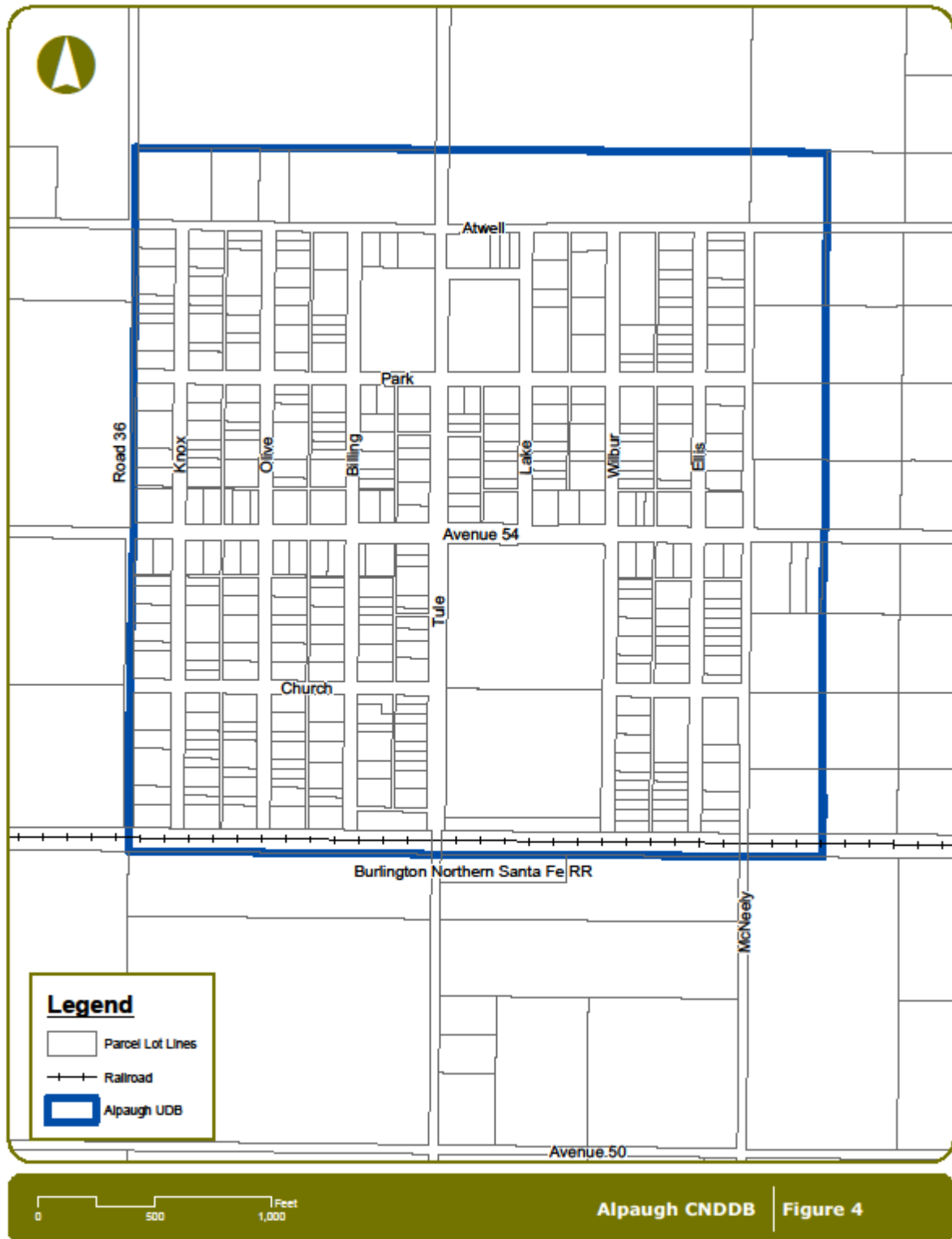
The Pixley National Wildlife Refuge was established in 1959 by executive order to provide wetland habitat for migratory waterfowl and shorebirds. Of the 6,939 acres that comprise the Refuge, approximately 5,350 is upland habitat made up of grassland, alkali playa, and vernal pool habitat, 755 acres consists of seasonal wetlands, and 15 acres consist of riparian habitat. The Pixley National Wildlife Refuge is located 5 miles to the northwest.

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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <http://www.dfg.ca.gov/biogeodata/cnddb/mapsanddata.asp>; California Department of Fish and Wildlife. California Natural Diversity Database. November 1, 2016

# ALPAUGH COMMUNITY PLAN

Figure 4 - Alpaugh CNDDDB Map



# ALPAUGH COMMUNITY PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Alpaugh are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest).

According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Alpaugh] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the Soil Survey of Tulare County, California, Western Part (2003), prepared by the United States Department of Agriculture (USDA), Soil Conservation Service, (see **Figure 5**), the following soil type is located in Alpaugh:

**Posochanet silt loam, 0 to 2 percent slopes**, is moderately deep and moderately well drained, with slow permeability. The soil is suitable for crops such as cotton, seed alfalfa, sugar beets, wheat and safflower native annual grasses, forbs, and saltbrush.

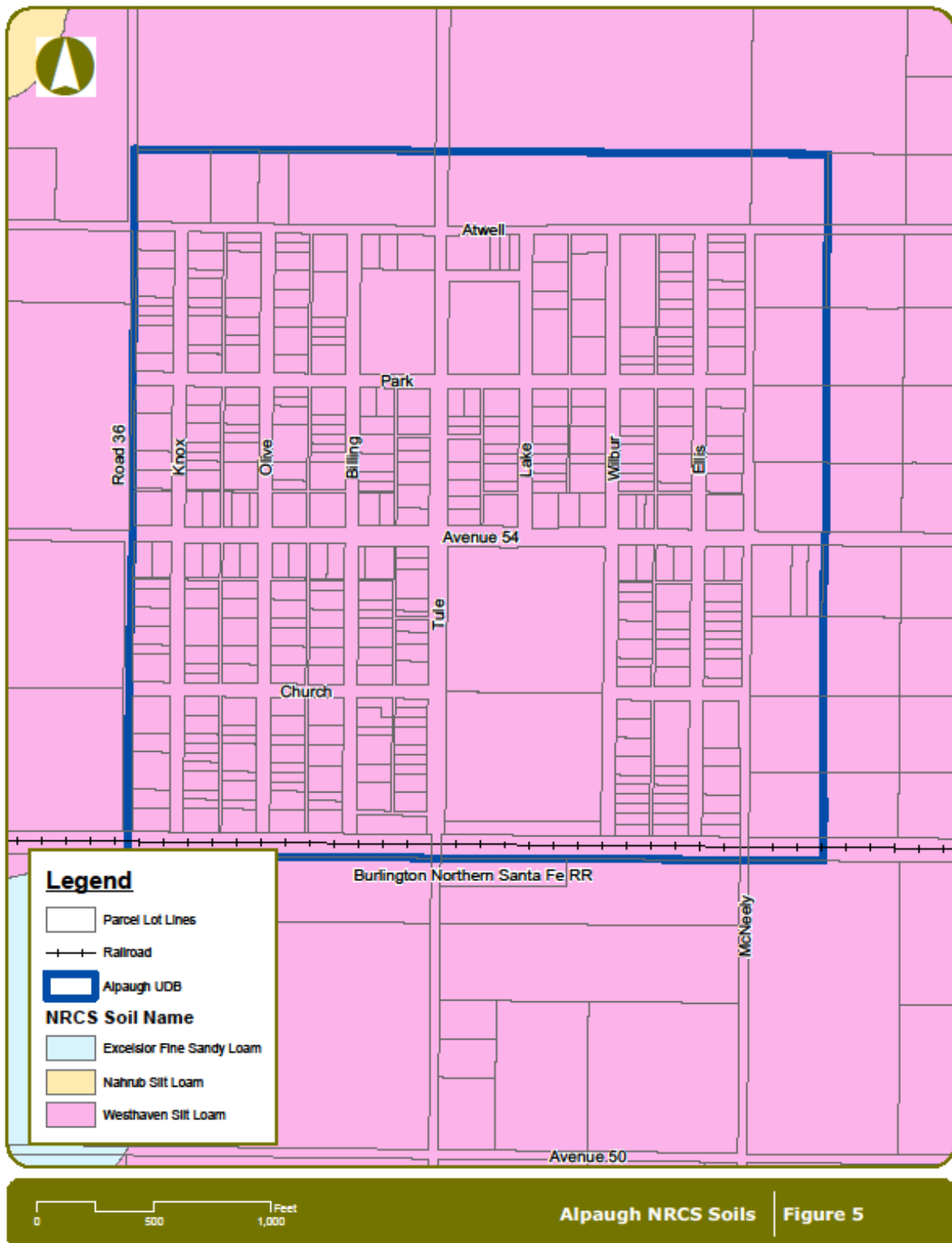
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# ALPAUGH COMMUNITY PLAN

Figure 5 - NRCS Soils Map



# ALPAUGH COMMUNITY PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Alpaugh Community Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Alpaugh is located near the southwestern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of

# ALPAUGH COMMUNITY PLAN

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adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Alpaugh include agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C1900E, shows Alpaugh within Flood Zoned X (see **Figure 6**), a portion of areas within 0.2% chance flood; areas of 1 % annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance of flood. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”<sup>7</sup> Although some areas of Tulare County have experienced major flooding along its major rivers, the Alpaugh Plan Area has not. There are portions of Alpaugh, however, that are within and adjacent to the FEMA 500 year flood zones. According to the Tulare County General Plan Update, substantial flooding could occur in Tulare County if the two (2) major dams were to experience failure. The inundation area below the Success Dam does not extend to Alpaugh

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Alpaugh there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps

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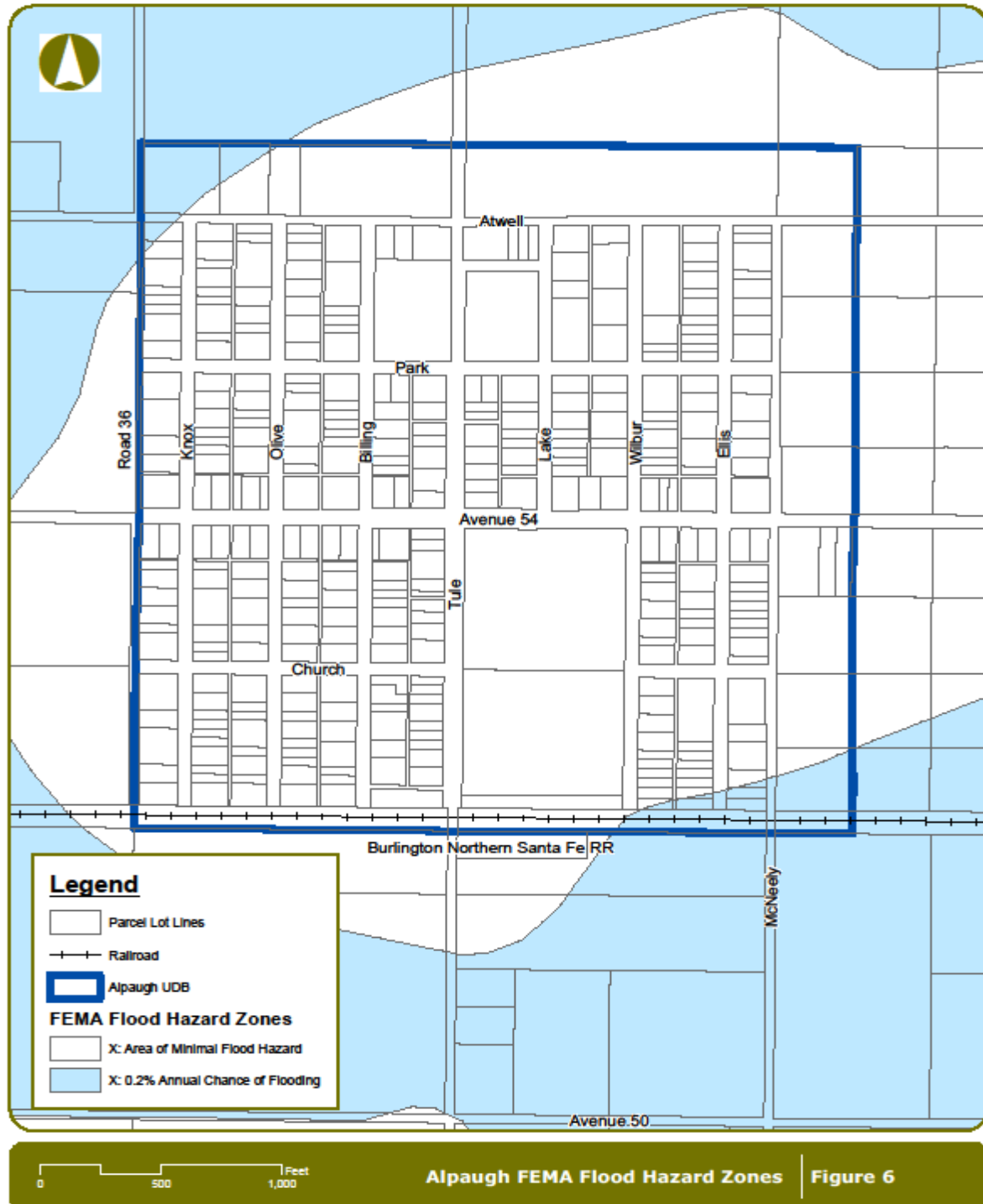
<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C1900E, Panel Number 1900 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

# ALPAUGH COMMUNITY PLAN

(DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

Figure 6 - FEMA Flood Map





# ALPAUGH COMMUNITY PLAN

## Noise

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. **Table 10** summarizes the daily traffic volumes along Avenue 54 from the Kings County Line to State Route 43. Together, these noise sources place a portion of Alpaugh's urbanized areas within the 60 dB Ldn noise contour. The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise.

Table 14 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
Avenue 54 from King County Line to State Route 43	600	6	14	29	63

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Alpaugh, this Community Plan is intended to address deficiencies and the need for improvements to the Community, for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc..

### Energy/Natural Gas/Electricity

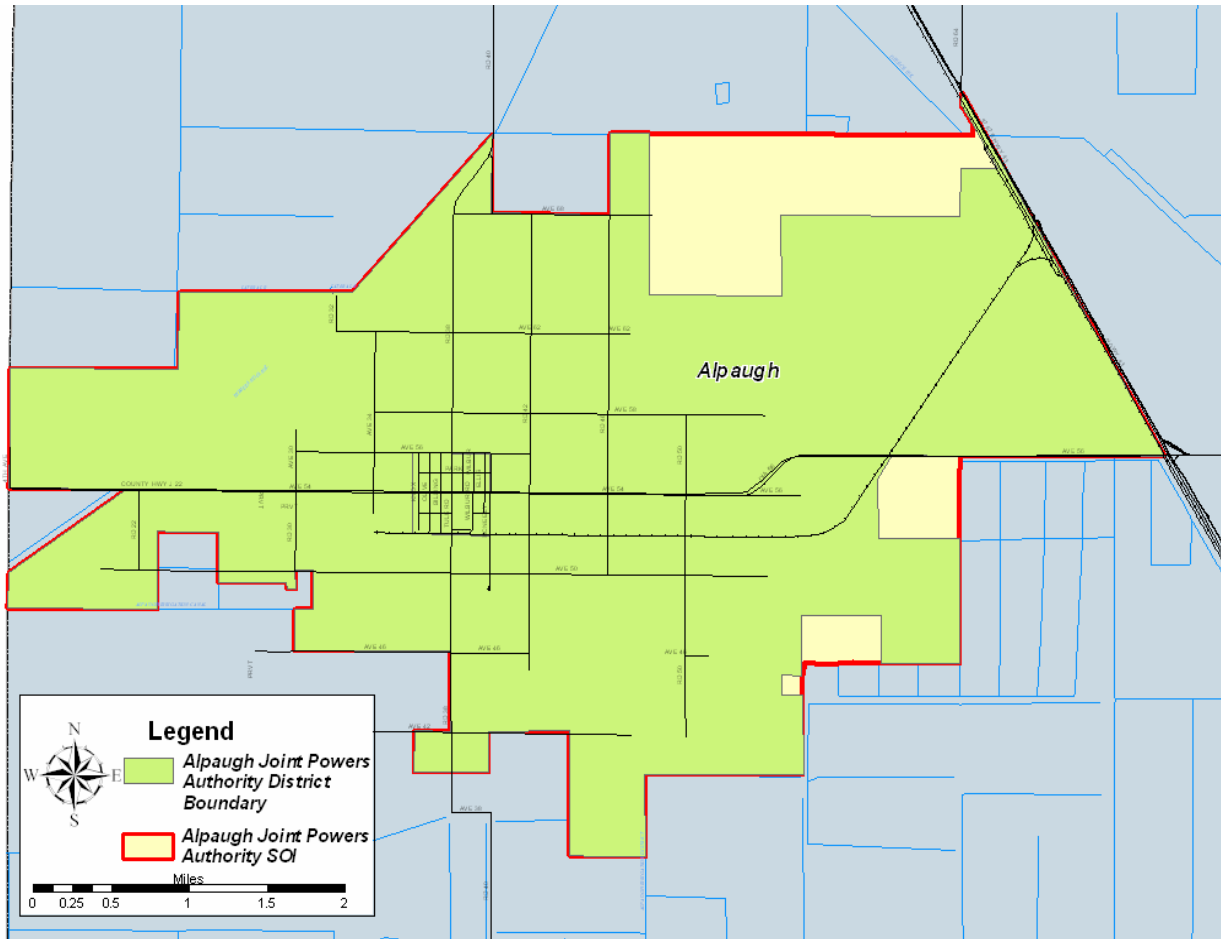
Southern California Edison provides service to Alpaugh.

### Water System

Domestic water service in Alpaugh is provided by the Alpaugh Joint Powers Authority (AJPA) which was formed in March 2003 and has expanded to its current size, the Alpaugh CSD Sphere of Influence is shown in **Figure 8**. It is a separate governing agency responsible for all operations and maintenance to the domestic water system in the rural community. **Table 11** shows the number of existing water connections, the capacity of the system, and the number of additional connections the system can accommodate for new development (AJPA, February 2014). These connections do not directly correspond to number of housing units but include the number of service connections, both residential and commercial. **Figure 7** graphically displays the approximate location of water wells and water lines. These are based on correspondence with AJPA because accurate mapping is unavailable.

# ALPAUGH COMMUNITY PLAN

Figure 7 - Alpaugh AJPA

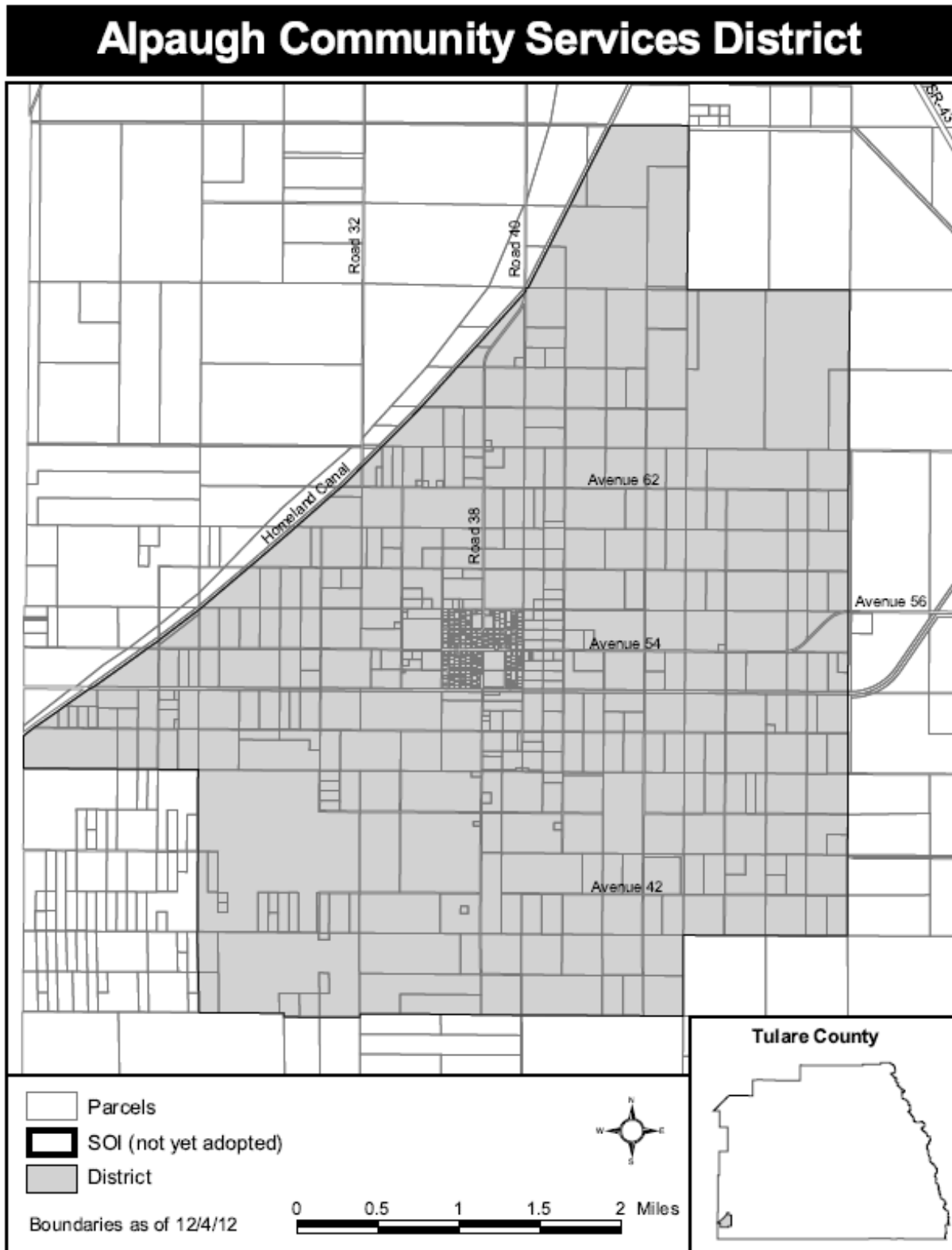


According to the Municipal Service Review 2006 (MSR), domestic water service providers for Alpaugh have been unable to support any new connections to their water system in recent years due to severe water quality problems (including arsenic contamination), inadequate system pressures, and deterioration of water pipelines resulting in breaks and leaks. Water system problems have halted any new development from occurring in the community.

Since its formation, the AJPA has received over \$4 million in grants and loans to improve the community's water supply and distribution system. The water supply is currently derived from a single well (Well #10). Well #9, owned and operated by the Alpaugh Irrigation District (AID), is used as a backup in case Well #10 fails to function. The AJPA expects to have an additional well drilled in the future, at which time Well #10 would function as the backup well.

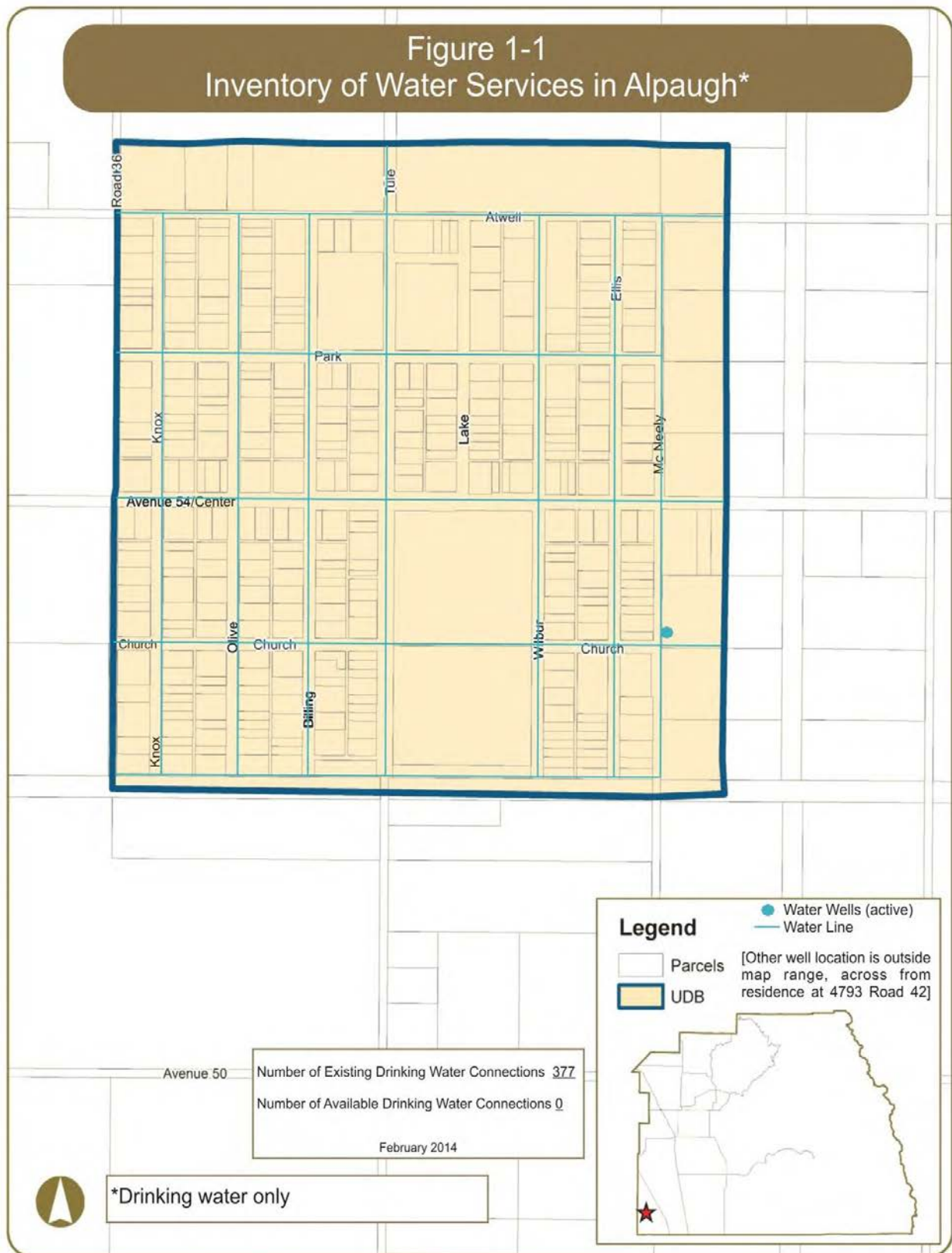
# ALPAUGH COMMUNITY PLAN

Figure 8 - Alpaugh CSD Sphere of Influence



# ALPAUGH COMMUNITY PLAN

Figure 9 - Inventory of Water



# ALPAUGH COMMUNITY PLAN

While the AJPA has struggled over recent years to supply customers with safe, affordable drinking water, they appear to be taking steps in the right direction by obtaining funding necessary for a complete overhaul of its water system. While the AJPA is unable to support additional connections at this time, ongoing system improvements will improve the system capacity and level of service and allow for additional service connections in the future. Assuming 290 equivalent dwelling units (EDUs) in order to meet Tulare County Improvement Standards, the AJPA water system would need to be capable of delivering a combined flow rate (from all source and storage facilities) of 1,030 gallons per minute (GPM) (500 GPM fire flow and 530 GPM domestic demand) for a period of two hours while maintaining a minimum pressure of 25 pounds per square inch (PSI) to each lot served. EDUs include housing units and other types of connections such as commercial uses. The current pumping efficiency of the AJPA water system is unknown, and therefore it cannot be determined if the water system meets the requirements of the Tulare County Improvement Standards.

## Sewer

There is no community wide sewer system in Alpaugh. The community depends on individual on-site septic tank systems for wastewater disposal.

**Table 15 - Existing Water & Wastewater Connections in Alpaugh**

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
377 <sup>1</sup>	377 <sup>2</sup>	0	Septic Only		

\*Data current as of February 2014 (per conversations with Alpaugh CSD)

1 Twenty (20) of these connections are not currently in use

2 Per Alpaugh CSD, the system is not technically "at capacity" since at present they are using only half the delivery capability of the two wells. The real "capacity" problem arises from sinking of the water table.

## Storm Water Drainage

A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.

# ALPAUGH COMMUNITY PLAN

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- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration

Alpaugh currently does not have a storm drainage system.”<sup>8</sup>

## Solid Waste

Solid waste disposal services for the Community of Alpaugh is provided by Tule Trash, a private company. Solid waste generated in Alpaugh can be disposed at Teapot Dome Landfill, located at 21063 Avenue 128, Porterville.

## **PUBLIC SERVICES**

### Community Resource Center

The community lacks many resources and needs a place where residents can go to access information and county wide resources. The School and Community Service District Buildings provided the base for this currently.

### Sheriff

Police protection services are provided in Alpaugh by the Tulare County Sheriff's Department sub-station, located at 161 North Pine Street, in Pixley, approximately 12 miles east of Alpaugh. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

### Fire

Fire protection and emergency medical services are provided for Alpaugh by the Tulare County Fire Department. The community is served by Tulare County Fire Department Station #9, located at 3939 Avenue 54 in Alpaugh, California. Engine 9 is assigned to this location. The fire department personnel also provide emergency medical aid.

Thirty-five fire hydrants are found within Alpaugh (**see Table 20**). These fire hydrants are located within the County right-of-way. **Figure 10** display Existing Fire Hydrants in Alpaugh.

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<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

# ALPAUGH COMMUNITY PLAN

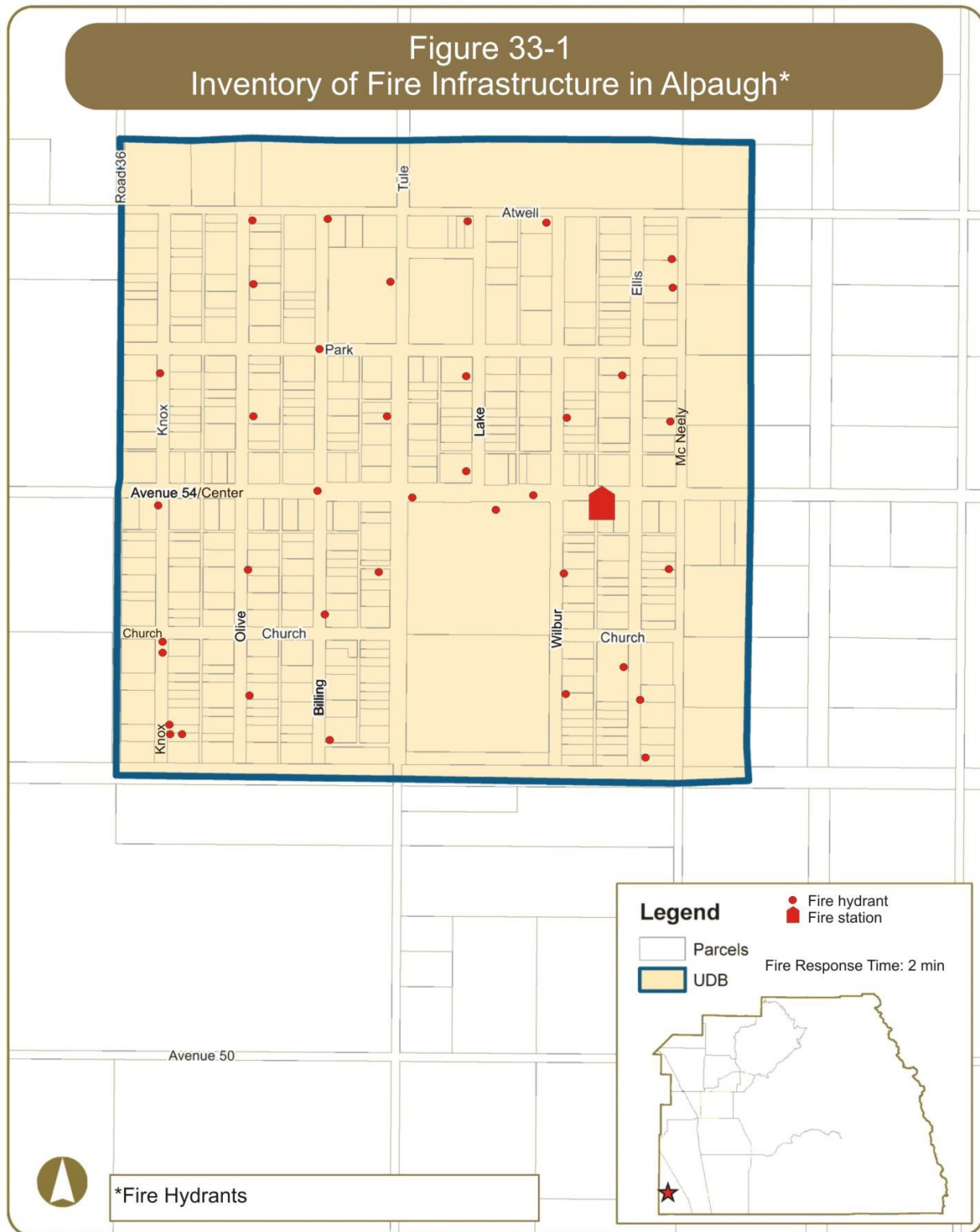
**Table 16 - Existing Fire Hydrants**

<b>Existing Fire Hydrants</b>	
<b>No.</b>	<b>Location</b>
1	Attwell Street and Olive Road
2	Attwell Street and Billing Road
3	Attwell Street and Lake Road
4	Attwell Street and Wilbur Road
5	Olive Road south of Attwell Street
6	Tule Road south of Attwell Street
7	McNeely Road south of Attwell Street
8	McNeely Road south of Attwell Street
9	Park Avenue at Knox Road
10	Park Avenue at Billing Road
11	Park Avenue at Lake Road
12	Park Avenue at Ellis Road
13	Olive Road south of Park Avenue
14	Tule Road south of Park Avenue
15	Wilbur Road south of Park Avenue
16	McNeely Road south of Park Avenue
17	Center Street at Knox Road
18	Center Street at Billing Road
19	Center Street at Tule Road
20	Center Street at Lake Road
21	Center Street at Lake Road
22	Center Street at Wilbur Road
23	Olive Road south of Center Street
24	Tule Road south of Center Street
25	Wilbur Road south of Center Street
26	McNeely Road south of Center Street
27	Church Avenue at Knox Road
28	Church Avenue at Knox Road
29	Church Avenue at Billing Road
30	Church Avenue at Ellis Road
31	Olive Road south of Church Avenue
32	Wilbur Road south of Church Avenue
33	Ellis Road south of Church Avenue
34	Boswell Avenue at Knox Road
35	Boswell Avenue at Knox Road
36	Boswell Avenue at Knox Road
37	Boswell Avenue at Billing Road
38	Boswell Avenue at Ellis Road



# ALPAUGH COMMUNITY PLAN

Figure 10 - Inventory of Fire Infrastructure in Alpaugh



# ALPAUGH COMMUNITY PLAN

## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Alpaugh Community Plan Area is within the Alpaugh Unified School District. The District provides K – 12 grade education at its Alpaugh School.

## Libraries

“Alpaugh was established as the sixth County Library Branch in 1913. In 1910, a deposit station had been set up on two shelves, hung on the wall of the “merchandise store” of W. N. Billing on North Broadway. The Alpaugh Library Association raised money for a library building. On May 1, 1913 the first Alpaugh Library was opened. In 1915, the rental money paid by the Tulare County Library was used to purchase awnings and screens. This was felt to be a much needed improvement for hot afternoons. In April of 1916, the Times Delta reported that Alpaugh had 811 books and 196 readers. The present Alpaugh Branch was built and dedicated in the fall of 1960. The old building was then removed to make room for a lawn and landscaping for the new building.”<sup>9</sup>



“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch”<sup>10</sup> (see Table 21).

Table 17 - Alpaugh Branch Library		
Branch	Address	Service Hours (2017)
Alpaugh	Alpaugh Branch 3816 Avenue 54 Alpaugh, CA 93201	Tuesday & Wednesday: 9:00 am – 1:00 pm 2:00 pm – 5:00 pm

Library hours current as of August 2017

## Parks

Alpaugh Park, has a three (3) acre community recreation park, located on Road 38, near the northwest corner of Tule Road and Park Avenue, across from the Alpaugh Memorial Building, is owned and maintained by Tulare County. There are also a number of arbors that are available for rental. No entrance fee is required.

# THE ALPAUGH COMMUNITY PLAN

## CIRCULATION/COMPLETE STREETS/TRANSPORTATION

### Existing Circulation and Transportation Conditions

Alpaugh is a small agricultural community located near State Route 43. The major rural collector roads is Avenue 54 (see Figure 11). There are no proposed major streets as part of this Community Plan.

<sup>9</sup> Tulare County Library <http://www.tularecountylibrary.org/alpaughbranch.html>

<sup>10</sup> General Plan Background Report, page 7-96

# ALPAUGH COMMUNITY PLAN

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## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.



# ALPAUGH COMMUNITY PLAN

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## Roads

There are various roadways in Alpaugh that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads”<sup>11</sup>

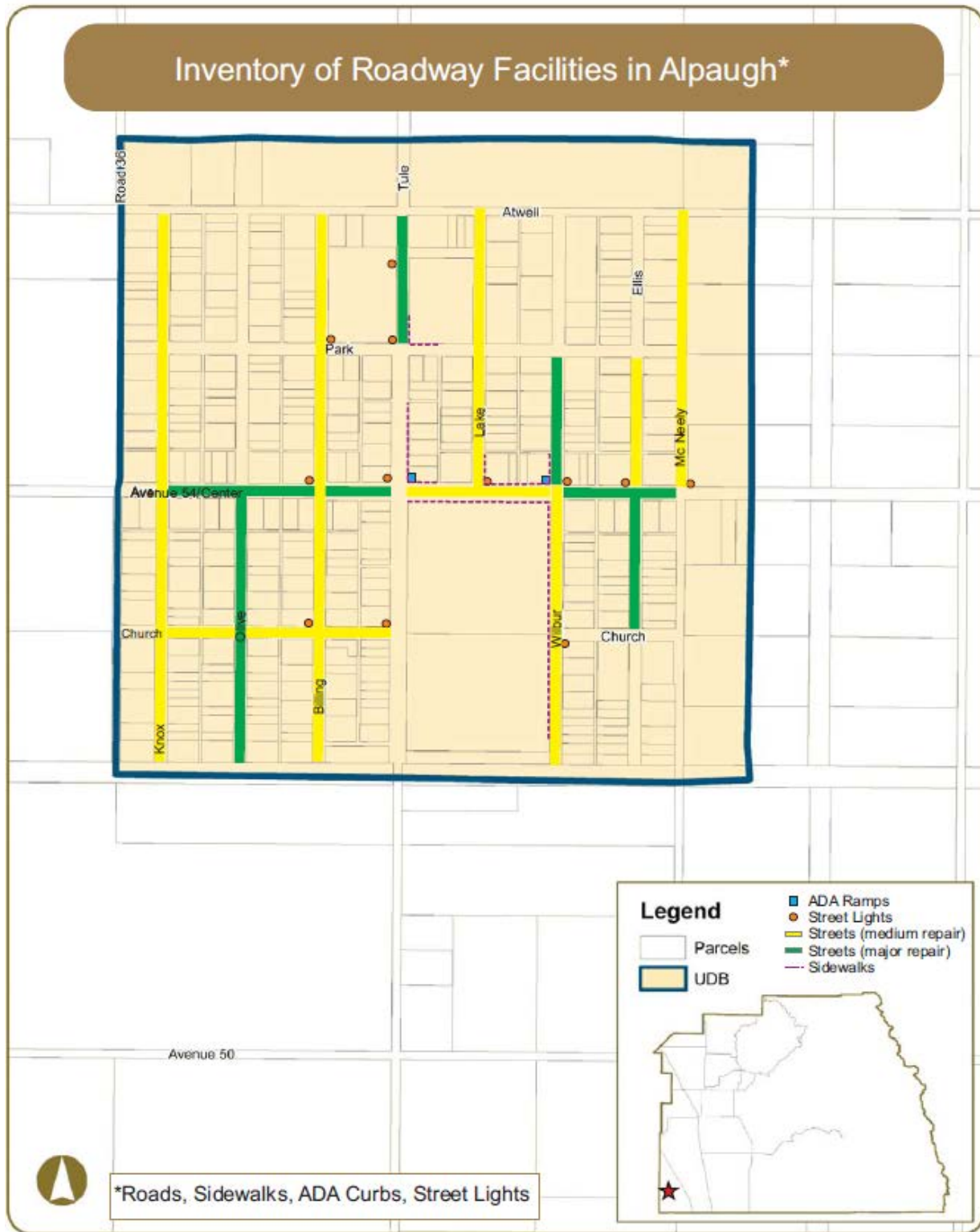
**Table 18** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 12** graphically displays this information on a map.

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<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element

# ALPAUGH COMMUNITY PLAN

Figure 12 - Inventory of Roadway Facilities in Alpaugh



# ALPAUGH COMMUNITY PLAN

**Table 18 - Road in need of Major and Medium Repair**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Billing Road	Atwell Avenue to south end	CHIP
2	Center Street	Knox Road to Rule Road	GRX
3	Center Street	Tule Road to Wilbur Road	CHIP
4	Center Street	Wilbur Road to Mc Neely Road	GRX
5	Church Avenue	Knox Road to Tule Road	CHIP
6	Ellis Road	Church Avenue to Center Street	GRX
7	Ellis Road	Center Street to Park Avenue	CHIP
8	Knox Road	Atwell Avenue to south end	CHIP
9	Lake Road	Center Street to north end	CHIP
10	Mc Neely Road	Center Street to north end	CHIP
11	Olive Road	Center Street to south end	GRX
12	Tule road	Park avenue to Atwell Avenue	GRX
13	Wilbur Road	Boswell Avenue to Center Street	CHIP
14	Wilbur Road	Center Street to Park Avenue	GRX

OLAY – overlay resurfacing operation  
 CHIP – chip seal  
 GRX – grind and remix

ACST – asphalt reconstruction  
 RCST – cold mix reconstruction

(Source: County of Tulare Public Works, 2012)

## Street Lights

**Table 19** identifies the location of existing street lights that are maintained by Tulare County, in Alpaugh, as well as their specifications. **Figure 12** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete”<sup>12</sup>

**Table 19 - Existing Street Lights in Alpaugh**

Specifications of Existing Street Lights								
No	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Atwell	Tule Road	S of Atwell	GTC1039233	5800	W	E	PG&E
2	Center	Billing Road	NW Corner	859	5800	W	S	PG&E
3	Center	Tule Road	NW Corner	857	5800	W	S	PG&E
4	Center	Lake Road	NE Corner	858	5800	W	S	PG&E
5	Center	Wilbur Road	NE Corner	861	5800	W	S	PG&E
6	Center	Ellis Road	NW Corner	866	5800	W	S	PG&E
7	Center	Mc Neely Road	NE Corner	862	5800	W	N	PG&E
8	Church	Billing Road	NW Corner	860	5800	W	E	PG&E
9	Church	Tule Road	NW Corner	855	5800	W	E	PG&E
10	Church	Wilbur Road	SE Corner	856	5800	W	N	PG&E
11	Park	Billing Road	NE Corner	865	5800	W	S	PG&E
12	Park	Tule Road	NW Corner	863	5800	W	E	PG&E

(Source: Tulare County Public Works, March 2013)

<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element



# ALPAUGH COMMUNITY PLAN

## Sidewalks

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

**Table 20 - Existing Sidewalks in Alpaugh**

<b>Location of Existing Sidewalks</b>			
<b>No.</b>	<b>Roadway</b>	<b>Limits</b>	<b>Location</b>
1	Center Street	Tule Road to 150' east	North Side
2	Center Street	Tule Road to Wilbur Road	South Side
3	Center Street	Lake Road to Wilbur Road	North Side
4	Lake Road	Center Street to 100' north	East Side
5	Park Avenue	Tule Road to 150' east	North Side
6	Tule Road	Park Avenue to 150' north	East Side
7	Tule Road	Center Street to 150' north	East Side
8	Wilbur Road	Center Street to 150' north	West Side
9	Wilbur Road	Center Street to 100' north of Boswell Avenue	West Side

(Source: County of Tulare Public Works and VRPA Technologies, February 2014)

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are several ADA compliant curb ramps located within Alpaugh (see Table 21).”<sup>13</sup>

**Table 21 - Existing ADA Curb Ramps in Alpaugh**

<b>Location of Existing ADA Ramps</b>			
<b>No.</b>	<b>East-West Roadway</b>	<b>North-South Roadway</b>	<b>Location</b>
1	Center Street	Wilbur Road	NW Corner
2.	Center Street	Tule Road	NE Corner

(Source: County of Tulare Public Works, August 2013)

<sup>13</sup> Action Program 9, Tulare County 2015 Housing Element

# ALPAUGH COMMUNITY PLAN

## Complete Streets

The Complete Streets Programs Policies, Objectives, and Standards are hereby incorporated by reference. Included in the plan are policies and implementation measures. The physical plan includes a bicycle network and connected pedestrian travel system incorporating complete safe routes to school network (see Table 22) and (Attachment A-4: Alpaugh Complete Streets Program).

The Board of Supervisors on February 28, 2017, approved inclusion of the Complete Streets Program as part of the Circulation Element of the Community Plan (see Attachment A-4). The Complete Streets Programs Policies, Objectives, and Standards are incorporated by reference.

Table 22 - Proposed Complete Streets Projects for Alpaugh					
Segment	Sidewalk	Curb & Gutter	Drainage	Lighting	Measure R Complete Streets
Church Avenue between Tule Road (Road 38) and Knox Road	X	X			X
Church Avenue between Wilbur Road and Ellis Road	X	X			X
Tule Road on west side between Avenue 54 and Park Road	X	X			X
Avenue 54 between Wilbur Road and Ellis Road	X	X			X
Source: Tulare County Complete Streets - Alpaugh					

## Complete Streets Bicycle Facilities

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III facilities are proposed along Atwell Avenue. Although not signed on many roads in Alpaugh, bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways (see Figure 13).

## Complete Streets Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

## Multituse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. There is currently no multiuse trails in the Alpaugh Community.

# ALPAUGH COMMUNITY PLAN

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## **Atwell Island-Alpaugh-Allensworth Trail (AAAT) Project February 2017 (Update)**

**Project Description:** This Project will create a multi-use trail to connect the communities of Alpaugh and Allensworth with one another as well as with two unique and historical attractions; the Bureau of Land Management Atwell Island Project and CA State Parks & Recreation Colonel Allensworth Historic State Park. The AAAT Project will offer recreational and educational opportunities as well as expand access to southwest Tulare County's outstanding birdwatching and wildlife viewing for community members, youth, and visitors alike.

**Location:** Southwest Tulare County, in the Deer Creek sub-watershed. Deer Creek is unique in that it is one of the few undammed streams remaining in California, which enables it to serve the full range of natural ecosystem functions that benefit humans and wildlife. Deer Creek is emblematic of Tulare Valley's native landscape, with riparian corridors adjacent to upland desert scrub habitat. The region has many natural treasures that already call attention to it as a tourist destination, especially among birders. It is also home to many cultural treasures, including Colonel Allensworth Historic State Park and the Native American Yokut heritage found in the vast, historic Tulare Lakebed. There is significant opportunity to invest in the region to elevate it as a tourist destination bringing more wealth to the communities and to invest in youth education and nurture future stewards of the valley.

**Route:** Under consideration

**Estimated Funding:** \$230,000 (Phase 1 April 2016 – March 2017)

**Opportunities:** The project will align with other conservation efforts to achieve multiple benefits including ecosystem restoration, land stewardship, environmental education and youth leadership development. It will celebrate and educate the public regarding the cultures of the earliest settlers in the Atwell Island, Alpaugh and Allensworth area; develop capacity for intra-community leadership and collaboration through the Alpaugh & Allensworth Restoration and Conservation Collaborative (AARCC), create tourism opportunities, reduce greenhouse gases due to vehicle travel between communities and enhance quality of life.

**Challenges:** No insurmountable challenges expected.

**Partners/participants to date:** Alpaugh and Allensworth citizen advocates and schools, BLM Atwell Island Project, Colonel Allensworth Historic State Park, Friends of Allensworth, Tachi Yokuts Tribe, Tulare Basin Wildlife Partners, Self-Help Enterprises, Deer Creek Storm Water District and local water agencies, Southern California Edison, High Speed Rail, CSET, Tulare County Resources Management Agency and Tulare County Board of Supervisors. Other potential partners include agricultural - philanthropic interests and the Central Valley Community Foundation.

**Atwell Island Project Work-based Learning (AIP-WBL), Allensworth-Alpaugh- Atwell Island Trail (AAAT), and Allensworth-Alpaugh Restoration and Conservation Collaborative (AARCC) Programs 2017-2019**

**AIP-WBL Program Description:** The Bureau of Land Management (BLM) AIP-WBL Program is envisioned as a permanent four-week summer work-based learning program for 10 - 20 11th and 12th grade high school students from the rural underserved communities of Allensworth and Alpaugh in SW Tulare County, California. The program begun by BLM in 2016, is conducted at the BLM AIP and contiguous Ton Tache wetlands with appropriate field trips. The permanent continuation of this

# ALPAUGH COMMUNITY PLAN

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program will enable Allensworth and Alpaugh high school students to develop career readiness and the technical skills needed to address regional drought, flood, and other climate change adaptation issues. The 2017-2019 program cost is estimated to be \$260,000.

As soon as funding is available in 2017, the Tulare Basin Wildlife Partners (TBWP) in its official BLM “Friends of Atwell Island” role will collaborate with American Conservation Experience (ACE) to hire an Onsite Coordinator to coordinate the program year-round. The Coordinator will work with the Allensworth and Alpaugh School principals and selected teachers as well as with regional partners from key socio-economic sectors to ensure implementation of the program. The Coordinator will be housed at the AIP and will be supported by BLM staff for the equivalent of one day per week.

Students chosen to participate in the summer 2017 program will be given a focus topic upon which to design a region-specific, natural resource-focused project. Focus topic experts will give a series of presentations in classrooms and assemblies throughout the school year, introducing the focus topic from a variety of perspectives. Possible pathways which students will choose in approaching the focus topic include but are not limited to:

- Hands-on: native plant identification, cataloguing, and seed collection for the utilization of land restoration
- Science, Technology, Engineering & Math (STEM): exploring and examining land use in the Tulare Lake bed area through data accumulation, dissemination, and GIS mapping
- Historic: examining and exploring the evolution of the Tulare Lake bed landscape throughout its settlement
- Artistic: examining and exploring existing landscapes through photography, drawing, painting, etc.
- Topics suggested by key partner participants: key partners include regional water agencies, agricultural entities, environmental justice groups and higher education institutions

AIP-WBL, AAAT and AARCC Programs: During the past two years the BLM AIP, Allensworth-Alpaugh community advocates, and TBWP have also collaborated on the Allensworth-Alpaugh-Atwell Island (AAAT) and the Allensworth-Alpaugh Restoration and Conservation Collaborative (AARCC), integrating them with the AIP-WBL program. Together, these three programs comprise a holistic local-regional demonstration project – a model for rural communities throughout the Central Valley.

Under the AAAT and AARCC, with TBWP coordination during the 2016-2019 start-up phase, AARCC community advocates are guiding development of a nature education and recreation trail to connect the communities of Alpaugh and Allensworth with one another and with the two unique local-regional historical attractions – the Atwell Island Project-Ton Tache Wetlands and Colonel Allensworth State Historic Park (CASHP).

The AAAT will provide opportunities for AIP-WBL participant hands-on project applications, as well as youth and citizen science education to advance drought or flood readiness and climate adaptation. The AAAT will nourish community environmental stewardship, youth leadership development, and youth career readiness. AAAT program coordination by the TBWP in 2017 benefits from partial support from the International Edison Foundation. The AAAT program 2017-2019 cost is approximately \$250,000 in cash and in-kind services value. Significant in-kind value services are

# ALPAUGH COMMUNITY PLAN

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provided by the TBWP, BLM, Colonel Allensworth State Historic Park, Deer Creek Storm Water District and the Angiola Water District. Under the AARCC program, community advocates are preparing to assume management and fiscal responsibility for the WBL and AAAT programs, guided by the highly experienced and trusted retired Tulare County United Way Executive Director (Ernie, Hernandez) and coordinated by the TBWP. During 2017-2019, the AARCC will develop governance capacity to undertake additional future resource-sharing community benefit projects, enhancing the ecological and economic sustainability of the Allensworth and Alpaugh communities permanently. The Central Valley Community Foundation has offered to hold an endowment for AARCC, providing an important incentive. Except for TBWP coordination funded in 2017 by Edison International, costs/expenses under this program are donated by participants. (February 6, 2017)



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## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation. Public transit is not currently available in Alpaugh.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Public transit is not currently available in Alpaugh. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Alpaugh, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

## AMTRAK

The Corcoran AMTRAK station, located 10 miles to the northwest in Kings County, is the closest station to Allensworth providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

## Aviation

The nearest operational general aviation airport is Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 30 miles south of Alpaugh and offers direct flights to several destinations. Fresno Yosemite International Airport (FAT), approximately 70 miles northwest of Alpaugh, is the principal passenger and airfreight airport in the central San Joaquin Valley. Visalia Municipal Airport, is approximately 40 miles northeast.

## State Route 99

State Route (SR) 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South. Alpaugh is located approximately seven (7) miles west of SR 99.



# ALPAUGH COMMUNITY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Alpaugh Community Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Legacy Boundaries are proposed to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Alpaugh to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan, Alpaugh Community Plan, and input received from Alpaugh citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

It is important to recognize that County policies reflect the unique attributes of the various locations and geographic areas in the County. As such, there are policies applicable to one area of the County that are not applicable to others based on natural setting, topography, habitat, existing development, or other attributes which are unique within the planning context of the County. The following planning goals, objectives, policies and standards reflect, as applicable, the rural, suburban, or urban contexts of the Community planning areas designated by the County.

### *Community Development*

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Create a mix of uses with easy access to major roads**

Objective: Promote mixed use zoning and land uses.

#### Policies:

1. Locate high density residential uses in close proximity to planned commercial areas.
2. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
3. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
4. The County shall work with the Schools to provide safe routes to school.
5. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Alpaugh Community Plan.
- 6.

# ALPAUGH COMMUNITY PLAN

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7. The Alpaugh Community Plan should be reviewed every five years to determine if amendments are appropriate.
8. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to initiate home maintenance/repair programs in Alpaugh.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate-income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Alpaugh.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

# ALPAUGH COMMUNITY PLAN

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## **Economic Base**

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new commercial development.

#### Policies:

1. Encourage the Alpaugh to give priority to community service development in the areas reserved for commercial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial areas.

Objective: Provide the necessary safe guards to attract quality commercial development to the community.

#### Policies:

1. Assure that commercial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of commercial areas.

## **Environmental Quality and Public Safety**

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Alpaugh citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Alpaugh.

#### Objective: Protect Agricultural Lands:

Land within the respective Alpaugh, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

# ALPAUGH COMMUNITY PLAN

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The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Alpaugh.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Alpaugh.

# ALPAUGH COMMUNITY PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 205.4 acre Alpaugh Urban Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 3.21% of the CDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 1,103 by 205.4 urbanized acres (area within the CDB), which equals 5.37 persons per urbanized acre (**see Tables 23 and 24** showing population projections). Projecting the population at a 1.3% growth rate adds 236 persons in year 2030. Multiplying the 236 persons by the 1/3 ratio suggests an additional 3 acres of land will be required to accommodate development by the year 2030 if projections are realized.

### Population Growth Forecast

**Table 23 - Alpaugh Population Projections**

Growth Rate	2015	2020	2030
0.013	1,103	1,177	1,339

### Demand Forecast

To determine whether there is enough land within the exiting CDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the CDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>14</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2015 the community had 289 dwelling units (including vacant dwellings) with a population of 1,103. At an annual growth rate of 1.3%, the projected housing units are 308 and 351 in years 2015 and 2020, respectively, and projected population is 1,177 and 1,339 in Years 2016 and 2030, respectively.

**Table 24 - Population Projections**

Year	Population	Growth (%)
2015	1,103	0.013
2016	1,117	0.013
2017	1,132	0.013
2018	1,147	0.013
2019	1,161	0.013
2020	1,177	0.013
2021	1,192	0.013
2022	1,207	0.013
2023	1,223	0.013
2024	1,239	0.013
2025	1,255	0.013
2026	1,271	0.013
2027	1,288	0.013
2028	1,305	0.013
2029	1,322	0.013
2030	1,339	0.013

The total land area developed in these existing residential units was provided by the Tulare County Resource Management Agency. There are currently approximately 205.4 acres within the UDB that are developed for residential uses. (**see Table 24**). Assuming the land area needed to accommodate future population, increases are consistent with annual population growth projections, the land use growth projections are also assessed at an annual growth rate of 1.3%. In 2015, there were 216 units, at that growth rate there would be the need for an additional 40 residential units.

<sup>14</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

# ALPAUGH COMMUNITY PLAN

Currently there are 146 acres of R-A (Rural Residential Zoned) land, which is developable typically at 2 units per parcel but allows 1 unit, per 6,000 square feet. However, to minimally allow for community septic or for 6,000 square foot lots would be too small, R-A-12.5 would allow for future residential zoning and allows for the raising of domestic animals and other more rural uses (see **Table 25**).

Under the proposed zoning (see **Table 26**), there is the total of approximately 129.2 acres of residential (R-A 12.5) / and industrial - commercial mixed use zoned land (C-2-MU and M-1-MU); that is a net change to the Alpaugh Zoning Plan to allow for an additional 25 acres of R-A-12.5 (up to 75 units net) land and an overall potential for up to 72 additional units per acre with the Commercial and Industrial mixed-use zoning and .3 acres of C-3. As such, there is adequate land available for development within the existing UDB to accommodate future residential growth through the year 2030 and beyond.

<b>Table 25 - Existing Zoning</b>	
AE-40	6.6
C-1	3.5
C-2	4.1
R-A	146.4
RIGHT OF WAY	44.7
<b>TOTAL</b>	<b>205.4</b>

<b>Table 26 - Proposed Zoning</b>	
C-2-MU	15.8
C-O	2.7
M-1	6.6
M-1-MU	2.4
O	3.3
P-O	18.9
R-3	0.3
R-A-12.5	110.7
Unclassified (Right-of-Way)	44.7
<b>TOTAL</b>	<b>205.4</b>

## **COMMERCIAL AND INDUSTRIAL USES**

The total land area developed in commercial and industrial units in year 2015 was provided by the Resource Management Agency. There are currently approximately 7.6 acres within the CDB that are developed for commercial uses. Assuming that the land area needed to accommodate future commercial and industrial growth is consistent with annual population growth projections, the land use growth projections are also assessed at an annual growth rate of 1.3%. At this growth rate, there will be a total of approximately 9.23 acres of commercial uses and approximately less than one acre of industrial uses in the year 2030. As shown in **Table 26**, there are approximately 16 acres of proposed commercial / mixed use lands and approximately 8.4 acres of industrial / mixed use lands within the existing CDB. As such with the proposed zoning changes, there is adequate land available for development within the existing CDB to accommodate future potential for residential, commercial and industrial growth through the year 2030 to allow for ultimate flexibility in market conditions.

# ALPAUGH COMMUNITY PLAN

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are in Corcoran. The nearest hospital is located in Corcoran, approximately 16 miles away.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix.

Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Based on the 2011-2015 American Community Survey (see Table 25), the educational barrier in Alpaugh begins in grade school. Of the adults age 25 and older, 39.5%

had an educational level of less than 9<sup>th</sup> grade. This lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment needs to begin in elementary school. As part of the Safe Routes to Schools, Tulare County is working provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

Table 27 - Educational Attainment			
Educational Attainment	California	Tulare County	Alpaugh
Population 25 years and over	25,257,858	260,964	527
Less than 9th grade	10.0%	20.8%	39.5%
9th to 12th grade, no diploma	8.2%	10.9%	17.3%
High school graduate (& equivalency)	20.7%	25.2%	20.9%
Some college, no degree	21.8%	22.3%	18.6%
Associate's degree	7.8%	7.0%	0.9%
Bachelor's degree	19.8%	9.5%	0.9%
Graduate or professional degree	11.6%	4.3%	1.9%

2011-2015 American Community Survey 5-Year Estimates



# ALPAUGH COMMUNITY PLAN

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Urban Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 28**) and Zoning Plan map (see **Table 29**) have been updated.

<b>Table 28 – Existing / Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	160.7
Right-of Ways	44.7
Alpaugh UDB	205.4

<b>Table 29 - Proposed Zoning</b>	
C-2-MU	15.8
C-O	2.7
M-1	6.6
M-1-MU	2.4
O	3.3
P-O	18.9
R-3	0.3
R-A-12.5	110.7
Unclassified (Right-of-Way)	44.7
<b>TOTAL</b>	<b>205.4</b>

### IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the Alpaugh Community Plan. The following components comprise the Alpaugh Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Alpaugh Community Plan, there are a variety of changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

# ALPAUGH COMMUNITY PLAN

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## Mixed Use Overlay District

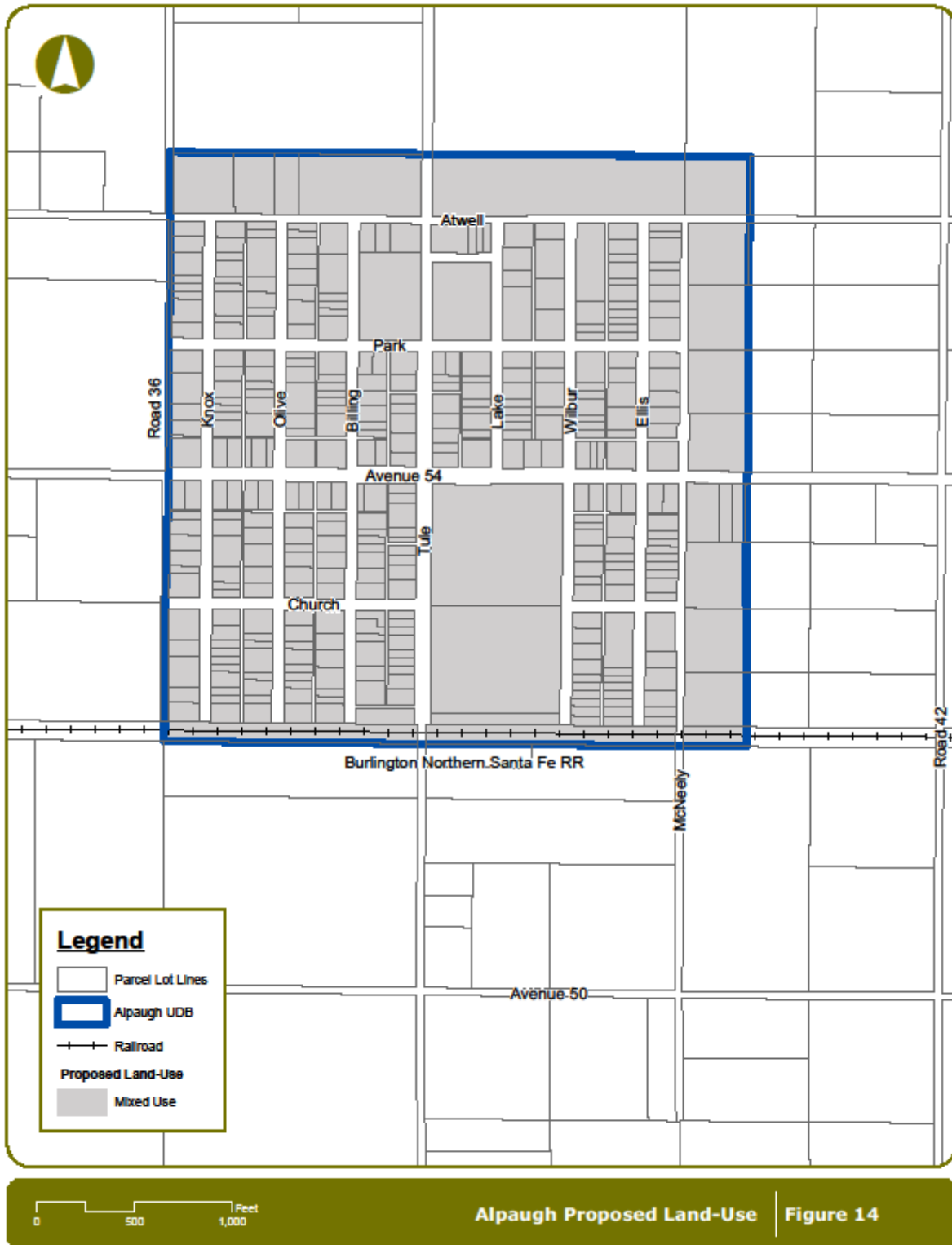
This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Alpaugh.

## Zoning Map Update

The current Zoning Map for Alpaugh (**see Figure 15**) will be amended to be compatible with the Land Use Map (**see Figure 14**) outlined in the General Plan. There are a couple of zoning district changes (**see Figure 16**) that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 17**) Alpaugh proposed Zoning Districts).

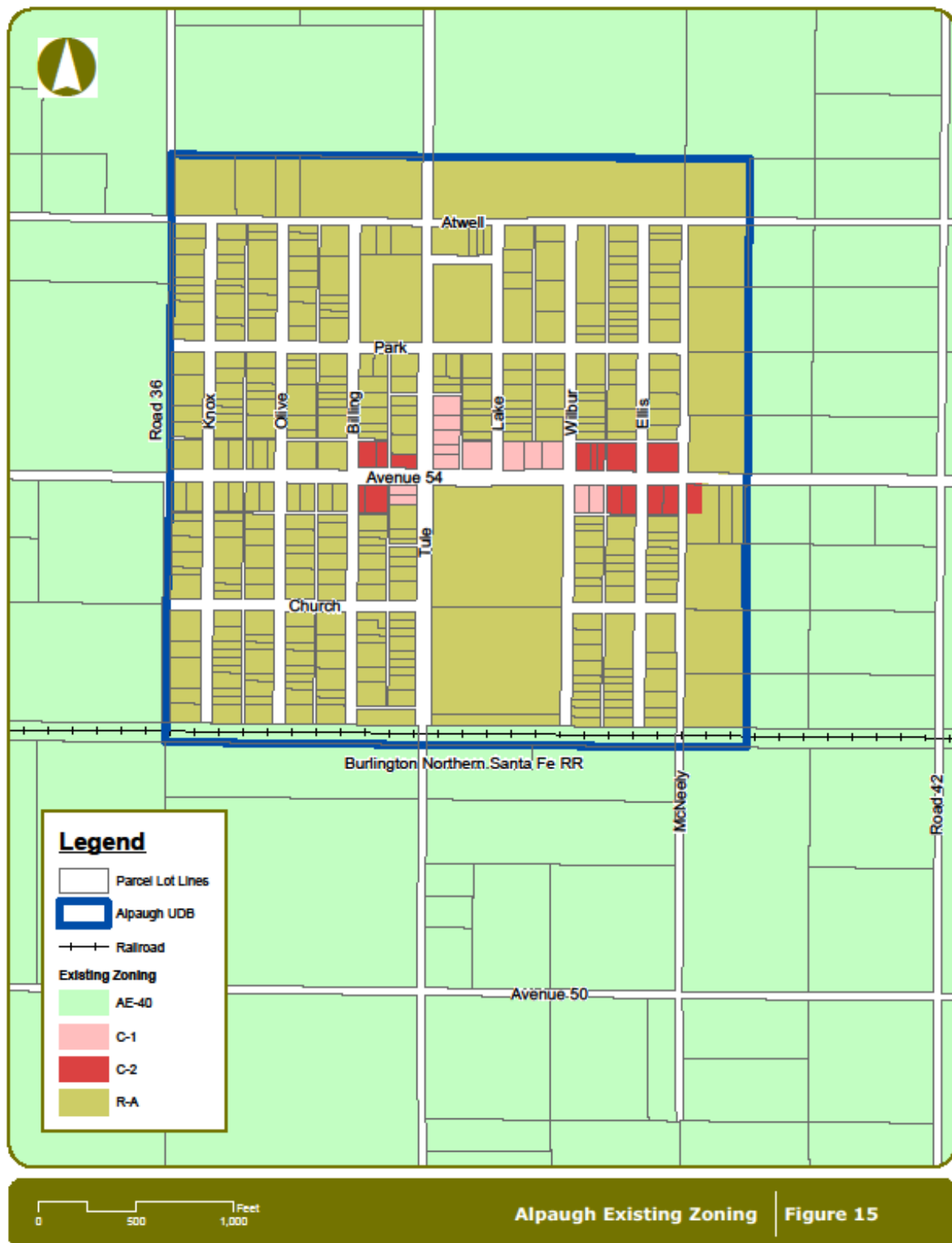
# ALPAUGH COMMUNITY PLAN

Figure 14 - Proposed Land Use Plan



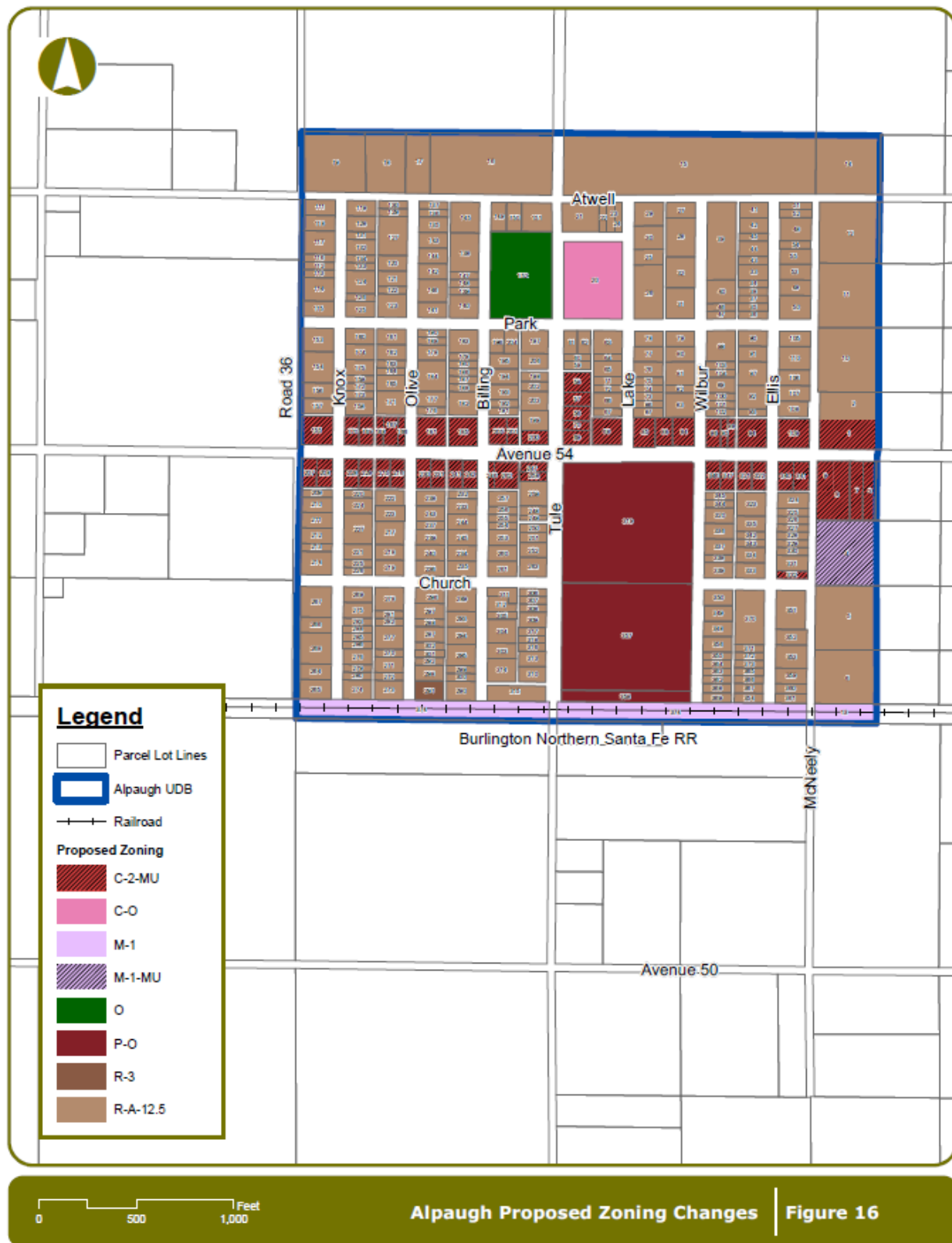
# ALPAUGH COMMUNITY PLAN

Figure 15 - Existing Zone Districts



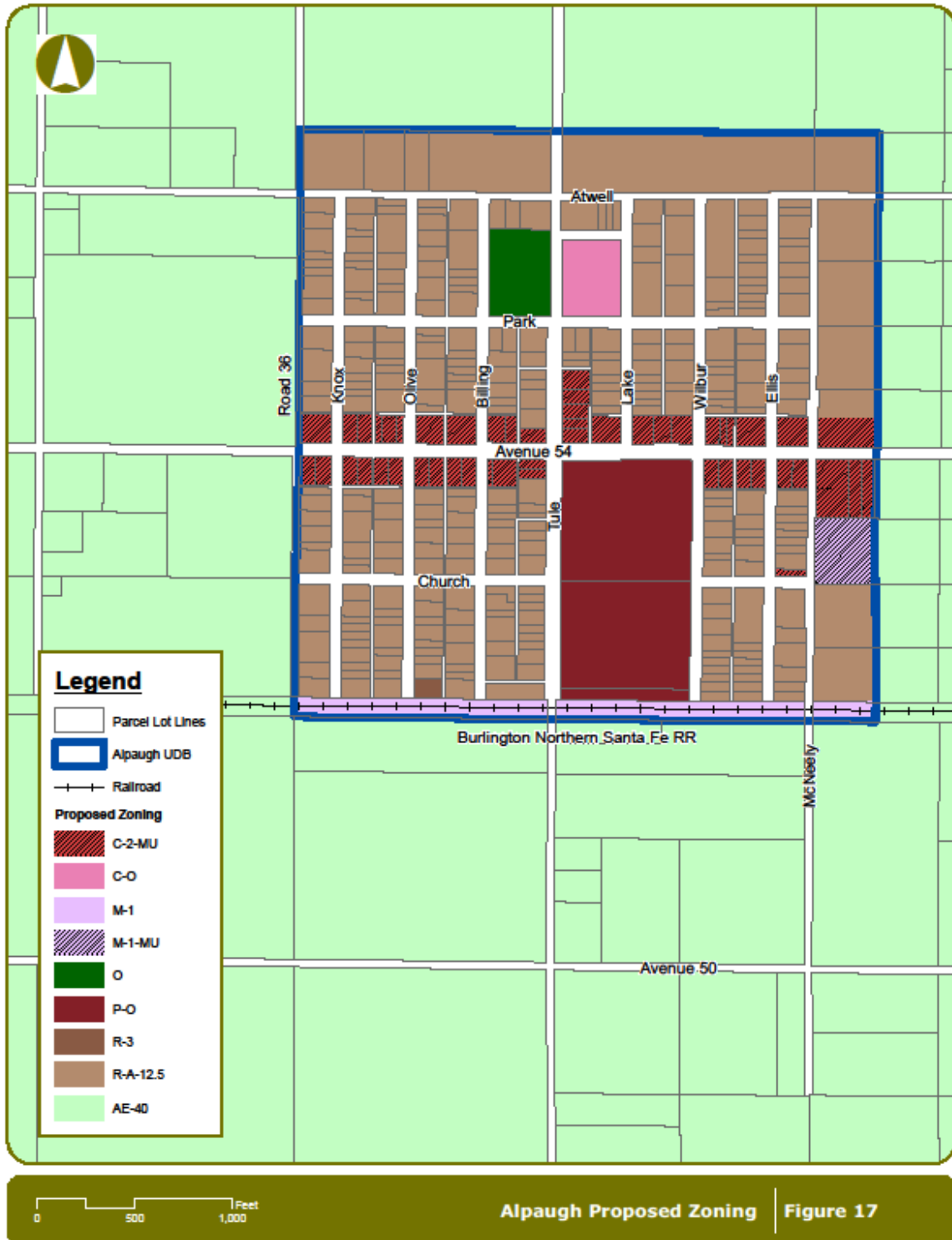
# ALPAUGH COMMUNITY PLAN

Figure 16 - Proposed Zoning Changes



# ALPAUGH COMMUNITY PLAN

Figure 17 - Proposed Zoning Districts



# ALPAUGH COMMUNITY PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

A-4 – Complete Streets



# ALPAUGH COMMUNITY PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1,C-2, C-3, M-1, R-2, R-3	C-1,C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Christmas tree sales lots as a temporary use.	CO, C-1,C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1,C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1,C-2,M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1,C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1,C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1,C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1,C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1,C-2,M-1, R-1, R-2, R-3, RA	C-1,C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1,C-2,M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1

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Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1,C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1,C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1,C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-3	R-3, C-1,C-2, C-3, M-1

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Nursery school. Structure < 10,000 sq. ft.	CO, C-1,C-2, M-1, R-1, R-2, RA, R3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1,C-2, M-1, R-1, R-2, R-3,PO	C-1,C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O,CO, C-1,C-2, C-3, M-1, M-2	
Post Office.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1,C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1,C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1,C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1,C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	

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Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1,C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1,C-2, C-3, M-1	
School, private.	CO, C-1,C-2, C-3, M-1, PO	
School, public.	CO, C-1,C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1,C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1

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Tourist Court. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1



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## A-2 *Mixed Use Overlay District (Zone Change Text)*

The following regulations shall apply in the community of Alpaugh, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Alpaugh.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Alpaugh.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

# ALPAUGH COMMUNITY PLAN

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Alpaugh. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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## A-3 *Development Standards (Mixed Use Zoning District)*

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To promote Economic Development within the Alpaugh Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Alpaugh. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Alpaugh Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Alpaugh. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios,

and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

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A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side

and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

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RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## SITE PLANNING

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

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SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.



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LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

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WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

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SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# ALPAUGH COMMUNITY PLAN

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## **A-4      Alpaugh Complete Streets Program**



# Tulare County Complete Streets Alpaugh

## Final



Prepared by:

**Tulare County Resource  
Management Agency**

Paid for by a grant from:





## **Tulare County Complete Streets – Alpaugh**

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## APPENDIX

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# Introduction

## Complete Streets Vision

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.”

The Tulare County Resource Management Agency (RMA) is committed to fully integrating modal options in its General Plan and various Community Plans within Tulare County. This includes supporting projects that enhance walking and bicycling infrastructure. Additionally, RMA will improve access to public transportation facilities and services. This includes supporting urban development patterns and Americans with Disabilities Act (ADA) infrastructure that allow for greater accessibility to transit stops and stations. Finally, RMA continues to improve safety for all users and encourages street connectivity to create a comprehensive, integrated and connected circulation network. This is particularly important for those who rely on transportation infrastructure to be physically active and for students who walk or bike to school.


Steady population growth in Tulare County has directly impacted transportation needs. In the past, many of Tulare County’s federal, state, and local funding sources were used to develop new or improved traffic signals, interchanges, provide more travel lanes and to maintain existing roadway facilities. Historically, these funding sources have run well short of what is needed. The typical roadway transportation project that adds capacity and infrastructure is insufficient given these conditions. The RMA must adhere to its vision, which is to “provide a safe, convenient and effective County transportation system that enhances mobility and air quality for residents and visitors.”

Recent RMA and RMA-supported projects have already fulfilled some of these desires. There are already expanded bus transit routes in the County and more are being constructed for implementation in the near future. New transit centers are being placed throughout the County and efforts are underway to add more bicycle lanes and routes. Recent planning studies are looking to improve roadway safety, pedestrian safety, and access management between roadways and building developments. These efforts are consistent with greenhouse gas (GHG) emissions reductions efforts to reduce vehicle miles travelled (VMT) set forth under SB 375.

Promoting Complete Streets projects can offer Tulare County the ability to reduce traffic congestion, improve air quality, and increase the quality of life of residents by providing safe, convenient, and comfortable routes for walking, bicycling, and public transportation. Integration of Complete Streets into Tulare County’s existing policies allows the potential to prevent chronic diseases, reduce motor vehicle related injury and deaths, improve environmental health, stimulate economic development, and ensure access of transportation options for all people in Tulare County.

## Complete Streets Definition

Complete Streets are roadways designed to safely and comfortably accommodate all users, regardless of age, ability or mode of transportation. Users include motorists, cyclists, pedestrians and all vehicle types, including public transportation, emergency responders, and



freight and delivery trucks among others. In addition to providing safety and access for all users, Complete Street design treatments take into account accommodations for disabled persons as required by the ADA. Design considerations for connectivity and access management are also taken into account for non-motorized users of the facility.

Implementation of Complete Street design treatments will be based on whether it connects the networks for all modes, whether it improves the functionality for all users, and whether it is appropriate given the surrounding context of the community. The final elements of a Complete Street roadway will be largely based on these factors. At a minimum, a Complete Street roadway includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

## Complete Streets Attributes

While every street cannot be designed to serve all users equally, there are opportunities to enhance service for all users while maintaining its principal transportation function. Complete Streets incorporate community values and support adjacent land uses while ensuring safety and mobility. Proper applications of Complete Streets concepts support sustainable growth and preservation of scenic, aesthetic and historic resources.


## Report Outcomes

As a part of the Circulation Element for the Community Plan Update, this Complete Streets Report (Implementation Work Plan) and the following Implementation and Policies Section achieved the following outcomes:

- (i) *Addressed* congestion, climate change and oil dependence by shifting to lower-carbon modes;
- (ii) *Improved* safety by addressing shoulders, sidewalks, better bus placement, traffic speed reduction, treatments for travelers with disabilities;
- (iii) *Created* “livable communities” by encouraging walking and bicycling for health, and by providing a safe walking and bicycling environment as an essential part of improving transportation movement and safety within the roadways studied.

These outcomes were achieved by the following:

- (a) *Included all users* namely, pedestrians, bicyclists, transit vehicles and users, and motorists. In drafting this report, all users were invited to comment on how the County could better serve the community. The implementation of complete streets directly shifts the emphasis to lower-carbon (using) modes of transportation. The shift from the gas using automobile to pedestrian and bicycle transport is achieved through the creation of sidewalks, improving sidewalks, and including bike lanes and/or bike routes for a wider range of people to use. The shift to transit is included in improving policies, programs and facilities in the operations of the County’s transit systems.
- (b) *Created a comprehensive integrated and connected network* that supports “livable communities” that promote a safe interwoven fabric are provide for by the Policies



Section using the transportation goals in the 2030 General Plan Circulation Element and by further defining complete streets network (see Appendix C).


- (c) *Emphasized flexibility* recognizing that all streets with these communities are different, and thus, balancing user needs. No one standard was applied to all streets and the street designs were adjusted to existing conditions, differing jurisdictions and the desires of the community.
- (d) *Considered both new and retrofit projects*, including design, planning, maintenance, and operation, for the entire right-of-way within these communities. In addition to the various sections discussed below Appendix A – D include plans that show the plans, designs, and existing and proposed maintenance plans and operations of the Complete Streets Plan.
- (e) *Used the latest and best design standards*. By using newer design standards as represented in the preliminary design plans verses the County's Roadway Standards the County is able to provide wider sidewalks and include such amenities as traffic calming measures (bulbouts).
- (f) *Conducted extensive public outreach* to ascertain the solutions that best fit within the context of these communities. This culminated in a community meeting on September 3, 2015, wherein the Community provided final feedback on the preliminary designs.

## Conclusions and Future Funding Opportunities

The intended effect of identifying the outcomes and reaching the conclusions in this report is that future funding opportunities will be enhanced because the Community will be supported by fully updated Community Plans. The conclusion to the report includes the Circulation Element of the Community Plan including the policies, and plans. The other conclusion to the report includes preliminary design drawings.

Specifically, the funding sources that are found in the Funding Section will be pursued actively by Tulare County to complete the work identified in the studies include, but are not limited to, the following:

- **Active Transportation Program Funding**
- **Highway Safety Improvement Funds**
- **Federal Transportation Activity Program (TAP) Funds**
- **Federal Transit Funds**
- **Federal Communities Putting Prevention to Work Grant**
- **Federal Highway Administration Pedestrian Safety and Design**
- **Strategic Growth Council**

- 
- **Walkable and Livable Communities Institute**
  - **California's Local Public Health and Built Environment Program**
  - **State Cap and Trade Funding**

## **Complete Street Policies**

### **Complete Street Goals**

The purpose of the RMA Complete Streets Policy is to create a comprehensive and uniform Complete Streets vision and policy for Tulare County. This will allow the implementing entities to incorporate Complete Streets guidelines and standards into both development and redevelopment actions. The County's goals are:

- Tulare County's transportation network will be supported through a variety of feasible transportation choices, which allows for sustainable growth.
- The livability of neighborhoods and commercial centers located along the County's transportation corridors will be enhanced by a safe and inviting pedestrian environment.
- The design of multimodal roadway facilities will not compromise the needs of larger vehicles such as transit vehicles, fire trucks and freight delivery trucks.
- Inclusion of Complete Streets design elements will allow for design flexibility on different street functions and neighborhood contexts.
- Inclusion of Complete Streets design elements will improve the integration of land use and transportation, while encouraging economic revitalization through infrastructure improvements.

### **Complete Streets Objectives**

- To create an integrated and connected transportation network that supports transportation choices and sustainable growth.
- To ensure that all transportation modes are accommodated to the extent possible in all public roadway facilities in the County.
- To develop and use the latest design standards and guidelines in the design of Complete Streets.
- To provide flexibility in the implementation of this policy so that streets chosen for implementation of Complete Streets elements can be developed to fit within the context of their principal purpose and surroundings without compromising the safety of users and needs of larger vehicles.





## Complete Streets Policies

### Tulare County General Plan Policies

The Tulare County General Plan Update (2030) in complying with AB 1358 calls for 4 Complete Streets related principles including:

*Principle 1: County-wide Collaboration*

Support countywide transportation plans that provide choices in travel modes.

*Principle 2: Connectivity*

Emphasize connectivity among cities, communities, and hamlets to ensure County residents have access to jobs and services.

*Principle 3: Community Circulation*

Anticipate and provide transit, traffic, and roadway connections that support the interconnectivity of all communities.

*Principle 4: Pedestrian and Bicycle Facilities*

Plan for the development and expansion of pedestrian paths and bicycle facilities that provide residents with alternative modes of travel.

These principles are expressed mainly in following policies including:

- TC-1.6 Intermodal Connectivity
- TC-1.7 Intermodal Freight Villages
- TC-5.1 Bicycle/Pedestrian Trail System
- TC-5.2 Non-motorized Modes in Planning and Development

### Complete Street Policy Design Criteria

1. Tulare County promotes the incorporation of Complete Streets concepts and design standards in all appropriate new and retrofit County public streets (except State highways and freeways).
2. Tulare County will seek every opportunity to provide funding for the planning, design, and implementation of Complete Streets.
3. New Class I Multi-Use Paths should be a minimum of eight (8) feet wide.
4. New Class II Bike Lanes should be a minimum of five (5) feet wide.
5. New sidewalks should be a minimum of five (5) feet wide.
6. Bulb-outs should be considered in areas of higher speed (35 mph or greater) where sufficient turning radii for trucks is available or as determined by the County Engineer.
7. As determined by the County Engineer, installation of posted speed limit vehicle activated traffic calming signs (VATCS) are encouraged in instances of high



speed to promote safety.


8. Transit shelters and benches are encouraged at all County transit stops if FTA grants are available.
9. Street lighting and cross walk are encouraged to promote safety if considered feasible by the County Engineer.
10. Design policies should be consistent with the Tulare County Improvement Standards; other references include existing design guides, such as those issued by Caltrans, AASHTO and the ADA Accessibility Guidelines.
11. Public streets excluded from this policy include those where:
  - Complete streets concepts are in conflict with existing laws, codes, or ordinances.
  - Compliance with this policy would conflict with goals or physical conditions related to the unique aspects of the location.
12. Exceptions from Complete Street Policies:
  - Accommodation is not necessary where non-motorized use is prohibited, such as freeways.
  - Cost of accommodation is excessively disproportionate to the need or probable use as determined by the County Engineer.
  - A documented absence of current or future need.

## Complete Street Mobility Plan

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” Essentially, this bill requires a circulation element to plan for multimodal transportation accommodating all modes of transportation where appropriate, including walking, biking, car travel, and transit. The current functional classification system plan is shown in Appendix B (Circulation Plan).

The Complete Streets Act also requires circulation elements to consider the multiple users of the transportation system, including children, adults, seniors, and the disabled. For further clarity, AB 1358 tasks the Governor’s Office of Planning and Research to release guidelines for compliance with this legislation by January 1, 2014. Implementation of complete streets principles should be tailored to the individual jurisdiction and the individual roadway. The Complete Streets Program for Tulare County focuses on a network-based approach that has been tailored to the needs of the Community of Alpaugh. Another principle that is being applied is under SB 743, requiring a change to evaluating traffic using Vehicle Miles Traveled (VMT) versus Level of Service under CEQA analysis, and under AB 32 in reducing Green House Gasses.

*Complete Streets:* According to the National Complete Streets Coalition, complete streets are a means by which, “... planners and engineers (can) build road networks that are safer, more livable, and welcoming to everyone.... Instituting a complete streets policy ensures that transportation planners and engineers consistently design and operate the entire roadway with all users in mind – including bicyclists, public transportation vehicles and riders, and pedestrians of all ages and abilities.”



*Network-Based Complete Streets:* Combines individual travel mode networks into one multimodal transportation system, integrating infrastructure where appropriate, ultimately ensuring that all users can safely and efficiently access their destination.

*Vehicle Miles Traveled (VMT):* Vehicle miles traveled is the metric that identifies the total distance traveled in a car per driver. VMT drives roadway needs (the more people who drive, the more capacity and maintenance are needed on the roadway system). Under the Tulare County Climate Action Plan, in reducing VMT greenhouse gas emissions are reduced, and the County has an overall target of reducing 6% of its greenhouse gas emissions through a reduction in VMT.

Community Plans adopt these principles, which are combined into the following mission statement:

*The Community Complete Streets Network comprises four types of facilities—vehicular, pedestrian, bicycle, and public transit. This complete streets approach will enable residents to choose which travel mode best suits them. It also will ensure that streets are designed with the users in mind—accommodating for businesses, children, the elderly, bicyclists, and transit users.*

## **Caltrans and Complete Streets**

Under Caltrans District Order 64-R1, Caltrans requires that a Complete Streets Implementation Action Plan be developed and implemented for Caltrans owned and maintained Streets. Their Implementation Action plan provides a background by which the Tulare County Completes Street Plan will be implemented.

### *TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS)*

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Alpaugh. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describe bicycle circulation patterns and Pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

### *Tulare County Climate Action Plan (CAP)*

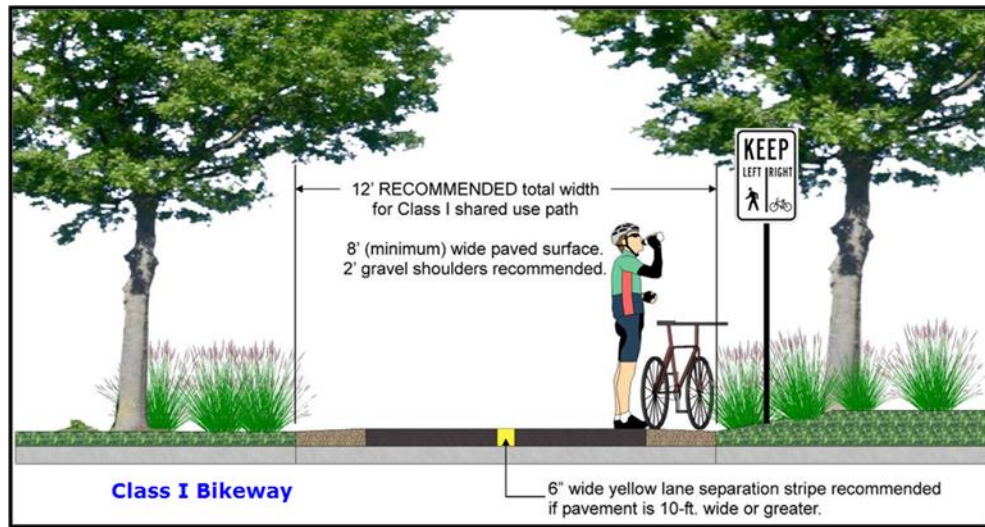
The Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% through a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets.

## **Bicycle Facilities**

Bicycle facilities consist of Class I, Class II, and Class III facilities as defined below. In Tulare County, this General Plan and the Bicycle Transportation Plan envision a system of bicycle lanes on roadways that will connect the activity centers of the communities to the residents. County has identified pedestrian corridors on the Community of Alpaugh Bicycle, Bus and Pedestrian Plan (see Appendix C).

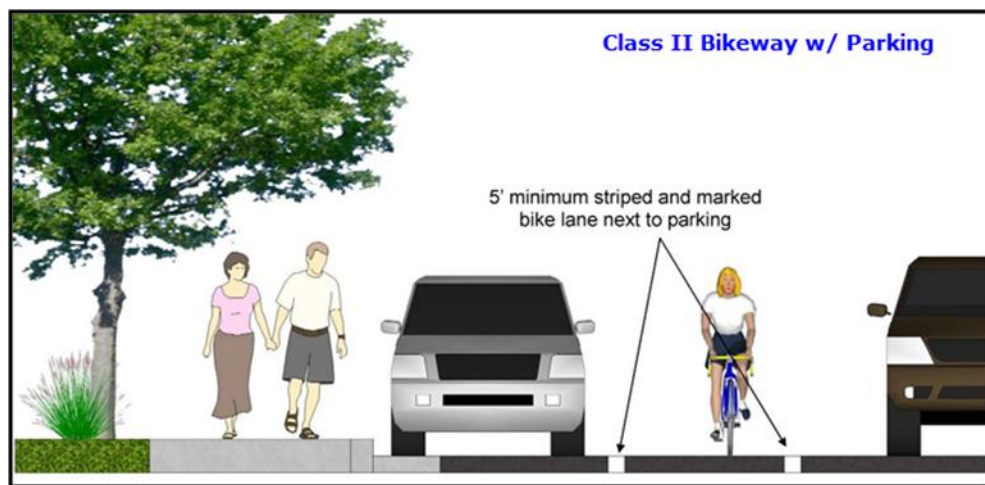
### Class I

Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County, Class I facilities will primarily be implemented through TCAG. Future bicycle facilities have also been identified through the *Bicycle Transportation Plan* (TCAG - 2010). The Communities of Alpaugh and Allensworth are proposing a Class 1 / pedestrian path from one community to the other.



### Class II

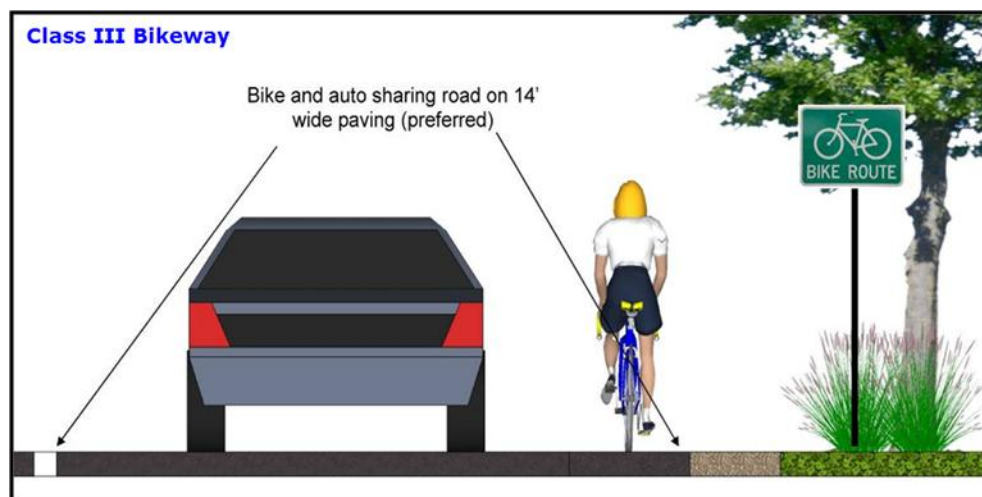
Bikeway that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. For the purposes of Complete Streets, the County is proposing no Class II bicycle facilities in Alpaugh.



### Class III

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III facilities are proposed along Atwell Avenue. Although not signed on many roads in Alpaugh,

bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways.



## Pedestrian Facilities

### *Pedestrian Paths and Sidewalks*

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

### *Multiuse Trails*

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. There is currently no multiuse trails in the Alpaugh Community.

## Transit Facilities

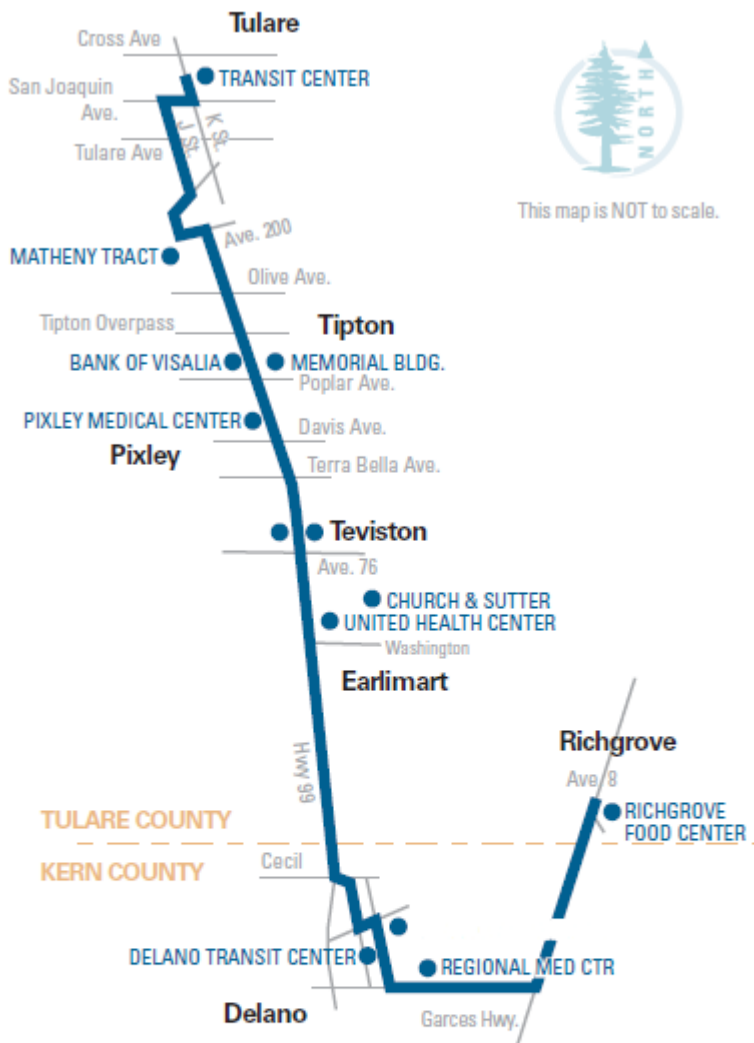
Transit options give users the ability to get to a destination without relying on the automobile. This also provides other community benefits, including reduced vehicle miles traveled (VMT). Reducing VMT will help the County achieve their greenhouse gas reduction target,

Public transportation services and facilities in Tulare County consist of public bus service, paratransit service, and could also consider park-and-ride locations.

### *Public Bus Service*

Public bus service is provided by Tulare County Area Transit (TCaT) in rural areas. Existing transit routes and designated bus stops are shown in the following figure.





## Pass Sales Outlets

Dinuba Transit Center	189 Merced St., Dinuba
Porterville Transit Center	35 W. Oak St., Porterville
Tulare County Government Plaza	5961 S. Mooney Blvd., Visalia
Tulare Transit Center	360 N. 'K' St., Tulare
Visalia Transit Center	Oak & Santa Fe, Visalia

Additionally, Tulare County has provided guidance for including transit within facilities. These guidelines should be applied when considering new development to ensure appropriate connectivity and design features to support bus service.

## Paratransit Service

Paratransit is an alternative mode of passenger transportation that does not follow fixed routes or schedules. Typically, vans or minibuses are used to provide paratransit service. Paratransit services vary considerably on the degree of flexibility they provide their customers. The most flexible systems offer on-demand, call-up, door to door service from any origin to any destination in a service area.

## Park-and-Ride Lots

Park-and-ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the community's population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community along Highway 99.

## Cost Benefits Analysis, Implementation, and Funding Mechanisms

Caltrans lists the following benefits of Complete Streets in their implementation plan. They include:

- Increased Transportation Choices: Streets that provide travel choices can give people the option to avoid traffic congestion, and increase the overall capacity of the transportation network.
- Economic Revitalization: Complete streets can reduce transportation costs and travel time while increasing property values and job growth in communities.
- Improved Return on Infrastructure Investments: Integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project spares the expense of retrofits later.
- Quality of Place: Increased bicycling and walking are indicative of vibrant and livable communities.
- Improved Safety: Design and accommodation for bicyclists and pedestrians reduces the incidence of crashes.
- More Walking and Bicycling: Public health experts are encouraging walking and bicycling as a response to the obesity epidemic. Streets that provide room for bicycling and walking help children get physical activity and gain independence.

## Benefits of Complete Streets

The health benefits from walking and bicycle riding include increased overall health, and a reduction in air quality and greenhouse gas emissions. According to the Caltrans accepted, Victoria Transport Policy Institute, walking has a \$.25 per mile health benefit, while the cost of Greenhouse Gas (GHG) reductions is \$23 per ton. According to the Federal Highway Administration, sidewalks reduce incidences to pedestrians over 80%.<sup>1</sup> According to Caltrans, the average costs of highway incidents are stated below.

Cost of Highway Accident	Dollars Per Accident
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Fatal Accident	\$4,800,000
Injury Accident	\$67,400
Property Damage Only (PDO) Accident	\$10,200
Average Cost per Accident	\$52,500

Cost of an Event	Dollars Per Event
------------------	-------------------

Cost of a Fatality	\$4,400,000
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### Cost of an Injury

<sup>1</sup> [http://www.dot.ca.gov/hq/tpp/offices/eab/benefit\\_cost/LCBCA-economic\\_parameters.html](http://www.dot.ca.gov/hq/tpp/offices/eab/benefit_cost/LCBCA-economic_parameters.html)





Level A (Severe)	\$221,400
Level B (Moderate)	\$56,500
Level C (Minor)	\$26,900
Cost of Property Damage	\$2,500

Source: California Department of Transportation

## Community Specific Complete Street Implementation Measures

As part of a network-based approach, the County has identified (and will implement through pursuing further roadway studies and infrastructure design updates) a complete network for pedestrians. The County will also work to deliver infrastructure to support all modes of transportation. In addition to the General Plan Circulation Element Implementation Section, the key implementation measures include:

1. Evaluating Roadways as potential Bike/Pedestrian travel routes,
2. Completing pedestrian infrastructure, as appropriate,
3. Providing safe and accessible pedestrian facilities in high use areas,
4. Designing and building sidewalks for safer routes to school,
5. Designating roadways for bicycle routes that are aligned with the Tulare County comprehensive bicycle network,
6. Coordination with County Transit.
7. Submitting the following list of project and cost to TCAG and Caltrans for consideration under further grant funding opportunities.

## Measure R


Bike/Transit/Environmental Projects (14% of Measure R Funding)

On November 7, 2006, the voters of Tulare County Approved Measure R, imposing a ½ cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

## Active Transportation Program (ATP)

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation ([Senate Bill 99, Chapter 359](#) and [Assembly Bill 101, Chapter 354](#)). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation



Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation. The proposed projects have been included in the County's Active Transportation Plan (2015).

## **Citizen Feedback**

### **Public Outreach Efforts**

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues.

Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Complete Streets and Community Plan Outreach (2015 - 2016) – is located in the Appendix G.

### **Improvement Standards**

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Transportation related facilities for public use are built within existing right of way (R/W) owned by a public agency, e.g., county, city or state. Within this R/W is a standard cross section, which is a term that is used to define the configuration of existing or proposed roadways at right angles to the centerline (CL). Typical sections show the width, thickness and descriptions of the pavement section, as well as the geometrics of the graded roadbed, side improvements and side slopes.

In Tulare County, the two most common cross sections are shown for two or four lane roads, varying in width based upon the number of lanes, parking, sidewalks, shoulders, bike lanes, etc. Figure 1 shows the cross section for two lane roads and Figure 2 identifies a typical four lane cross section.

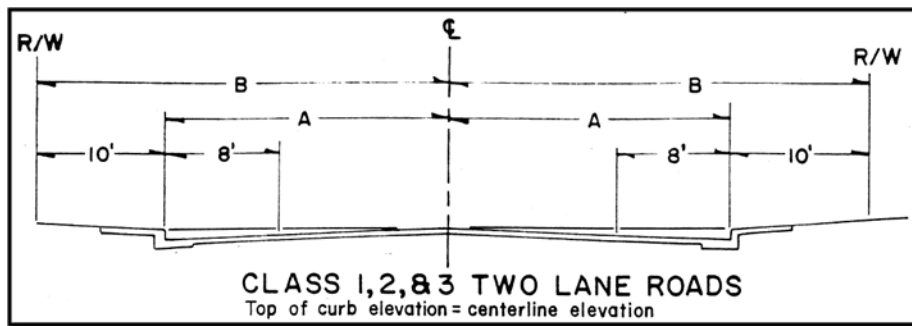


Figure 1 - Tulare County Class 1, 2 & 3 Two Lane Roads

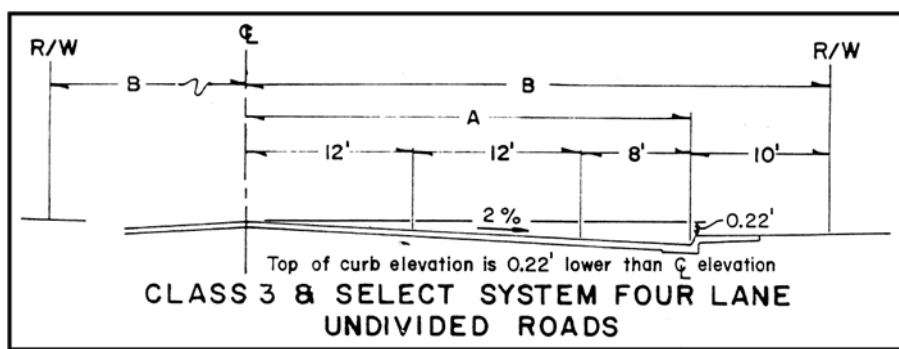


Figure 2 - Tulare County Class 3 Four Lane Road


## Tulare County Pavement Management System

### *Pavement Management*

Pavement management is the process of planning the maintenance and repair of a network of roadways or other paved facilities in order to optimize pavement conditions over the entire network. Pavement management incorporates life cycle costs into a more systematic approach to minor and major road maintenance and reconstruction projects. The needs of the entire network as well as budget projections are considered before projects are executed. Pavement management encompasses the many aspects and tasks needed to maintain a quality pavement inventory, and ensure that the overall condition of the road network can be sustained at desired levels.

### *Pavement Management System*

The Tulare County Pavement Management System (PMS) is a planning tool used to aid pavement management decisions. PMS software programs model future pavement deterioration due to traffic and weather, and recommend maintenance and repairs to the road's pavement based on the type and age of the pavement and various measures of existing pavement quality. Measurements can be made by persons on the ground, visually from a moving vehicle, or using automated sensors mounted to a vehicle. PMS software assists RMA staff to create composite pavement quality rankings based on pavement quality measures on



roads or road sections. Recommendations are usually biased towards preventive maintenance, rather than allowing a road to deteriorate until it needs more extensive reconstruction.

Typical tasks performed by Tulare County PMS include:

- Inventory pavement conditions, identifying good, fair and poor pavements;
- Assign importance ratings for road segments, based on traffic volumes, road functional class, and community demand;
- Schedule maintenance of good roads to keep them in good condition; and,
- Schedule repairs of poor and fair pavements as remaining available funding allows.

Research has shown that it is far less expensive to keep a road in good condition than it is to repair it once it has deteriorated. This is why pavement management systems place the priority on preventive maintenance of roads in good condition, rather than reconstructing roads in poor condition. In terms of lifetime cost and long term pavement conditions, this will result in better system performance.

The County is proposing a Road Maintenance Plan (see Appendix D) for the Community of Alpaugh that is a result of the PMS.

## Implementation

### Selection of Community Priorities

An effort is under way in Tulare County to implement Complete Streets Policies in the unincorporated communities within Tulare County's boundary. Just as the County updated its General Plan in 2012, many of the Community Plans are going through the update process. As a result of the Community Plan update process, several public meeting have been held in order to garner input from the local residents and business owners. Balancing the needs of what the people want while following local, state and federal policies and laws with a limited amount of available funding is the principal challenge in each community.

Transportation and related infrastructure costs tend to be exceedingly high may take years to implement. For purposes of this Study five transportation corridors were selected within the community (see Appendix A), and two roadway segments in the community were selected to be further evaluated for implementation of Complete Street standards. These roadway segments generally represent the highest volume roadways with a blend of residential and mixed land uses that also provide for regional access.

General themes that were voiced from residents in each community related to transportation included the need for:

- Sidewalks
- Better road conditions
- Safe walking and biking areas
- Street lights
- Pedestrian crossings

- Safe (lower) vehicle speeds
- Improved drainage
- Increased transit stops
- Improved connectivity (railroad crossings)

Given the information provided by the residents and business owners, conceptual layouts and designs based upon the citizens concerns were presented to collect input. Based upon the community planning process, the following sections identify proposed projects for each community.

## Project Phasing

Generally, Tulare County RMA is proposing two types of projects coming from the community based upon the complexity of the project. The first types of projects are “shovel ready” that could be built immediately. They would be considered Phase 1 Projects and would have only minor needs for storm drain facilities, fence relocations, utility conflicts, etc. Phase 2 Projects are more inclusive and would be classified as medium to long range projects. These projects would need other infrastructure improvements such as storm water basins, major storm drain improvements, utilities to be undergrounded, etc.

Phase 1 Projects	Phase 2 Projects
Curb, gutter & sidewalk (storm drain water into existing system); pedestrian ramps; bulb outs (where appropriate)	Curb, gutter & sidewalk (new drainage system)
Street lights	Major storm drain facilities (new pipelines and storm water basins)
Bus shelters, benches, trash receptacles, etc.	Utility relocations (undergrounding)
Fence relocations	Major land acquisition
Street signage and striping	Railroad crossing improvements
Minor utility conflicts	
Minor land acquisition	

# Alpaugh Community Needs

*Center (Avenue 54) & Church Street*



Figure 3: Avenue 54, between Lake Rd and Wilber. Sidewalk is non-existing to differentiate from roadway. Sidewalk, curb, gutter, asphalt paveout, and driveway are proposed.

Figure 4: Sidewalk improvements proposed at Avenue 54 at Lake Road. No sidewalk to differentiate from road, making current state a potential hazard for pedestrian and children walking to school.

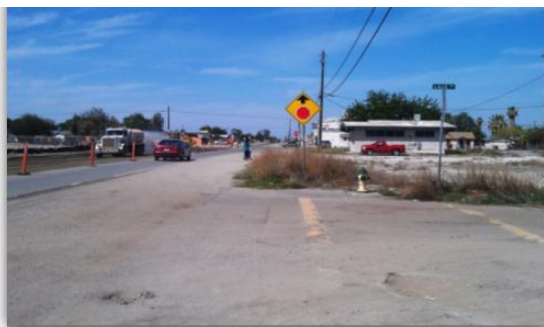


Figure 5: Sidewalk improvement proposed at Avenue 54 at Lake Road, Existing sidewalk is less than 3 feet wide and not in compliance with ADA standards.





Figure 6: Sidewalk improvement proposed at Avenue 54 at Tule Road, Existing sidewalk is less than 3 feet wide and not in compliance with ADA standards. Westbound view.



Figure 7: Avenue 54 and Lake Road, Sidewalk curb and gutter project proposed along Avenue 54. No existing sidewalk to differentiate from roadway. Westbound view.



## Outreach: September 3, 2015

1. Road 38/Tule Road (Church Ave to Park Ave)
2. Avenue 54/Center Ave (Tule Road to Wilbur Road)
3. Ellis Road (Church Ave to Center Ave)
4. Avenue 53/Church Ave (Knox Road to Ellis Road with the school in the middle)



Based on the Community's desired roadway improvements, the Resource Management Agency's Public Works Division devised plans and project descriptions for the following roadways.


## Improvement Plans

The following roadways were selected by the Community based on outreach and improvements designed by the Resource Management Agency's Public Works Division (See Appendix A).

### Church Avenue:

Approx. 870 LF of 5ft wide concrete sidewalk is proposed with curb and gutter on the north side of Church Ave between Tule Road (Road 38) and Knox Road. Existing road profile along Church Ave is very flat except high points at the crossing of Billing and Olive Road. Smooth surface can be maintained with bubble ups at those crossing mentioned above. Property fence is encroaching in to the right of way which will push the proposed sidewalk to south unless right of way is cleared. There are about 5 driveways that need to be addressed in this 870 LF stretch of sidewalk. A crosswalk is proposed across Tule Road to connect to the existing sidewalk at the school. ADA standard ramps will be placed on both ends of the crosswalk across Tule Road.

Another 310 LF of 5ft wide concrete sidewalk is proposed on the north side of Church Ave between Wilbur Road and Ellis Road. Putting the sidewalk to the north is more logical since it is adjacent to the school parking lot and aligns with the existing crosswalk. However, sidewalk on the school side across Wilbur Road does not have any ramp now and there is a water valve right in the middle of the crosswalk, which may trigger relocation of the crosswalk to further



north of Wilbur to clear the existing water connection. ADA standard ramps shall be placed on both sides of the cross walk across Wilbur Road.

Tule Road:

Approximately 595 LF of 5ft wide concrete sidewalk is proposed with curb and gutter on the west side of Tule Road between Ave 54 and Park Road. At the south end, this sidewalk will be stopped right at the existing grocery store parking to avoid conflict with the parking lot.

Avenue 54:

About 565 LF of sidewalk is proposed with curb and gutter on the north side of Ave 54 on several locations such as between the county library and Lake Road; between Lake Road and existing sidewalk to the east; and between Wilbur Road and Ellis Road. Some utilities such as (fire hydrants, power poles, signs etc.) are in the way and will need to be relocated. An average of 15ft wide asphalt section is proposed to match between the existing pavement and the proposed curb and gutter

Overall Improvements:

Included in this project is a Non-Infrastructure aspect. This will include pedestrian safety discussions with the school, and may include safety/activity seminars such as a “bike rodeo”, planning for future non-infrastructure needs, or other pedestrian/cyclist safety meetings.

## Projects

### Complete Streets Project Plans

The plans and projects in the appendices are identified as part of the complete streets policy to identify corridors for various user types and to demonstrate examples of design policies. These plans and are the result of input obtained through the community outreach process, multiple Tulare County agencies and divisions and professional engineering consultants.

The four projects identified herein represent the priority improvements to the backbone of the complete streets network within the community of Alpaugh. Two of these projects have been developed to a 30% design stage and the remaining three projects have been preliminarily scoped and budgetary estimates have been prepared. These four projects were developed to provide the County and various funding agencies with a list of projects to move toward funding, design, and ultimately construction.

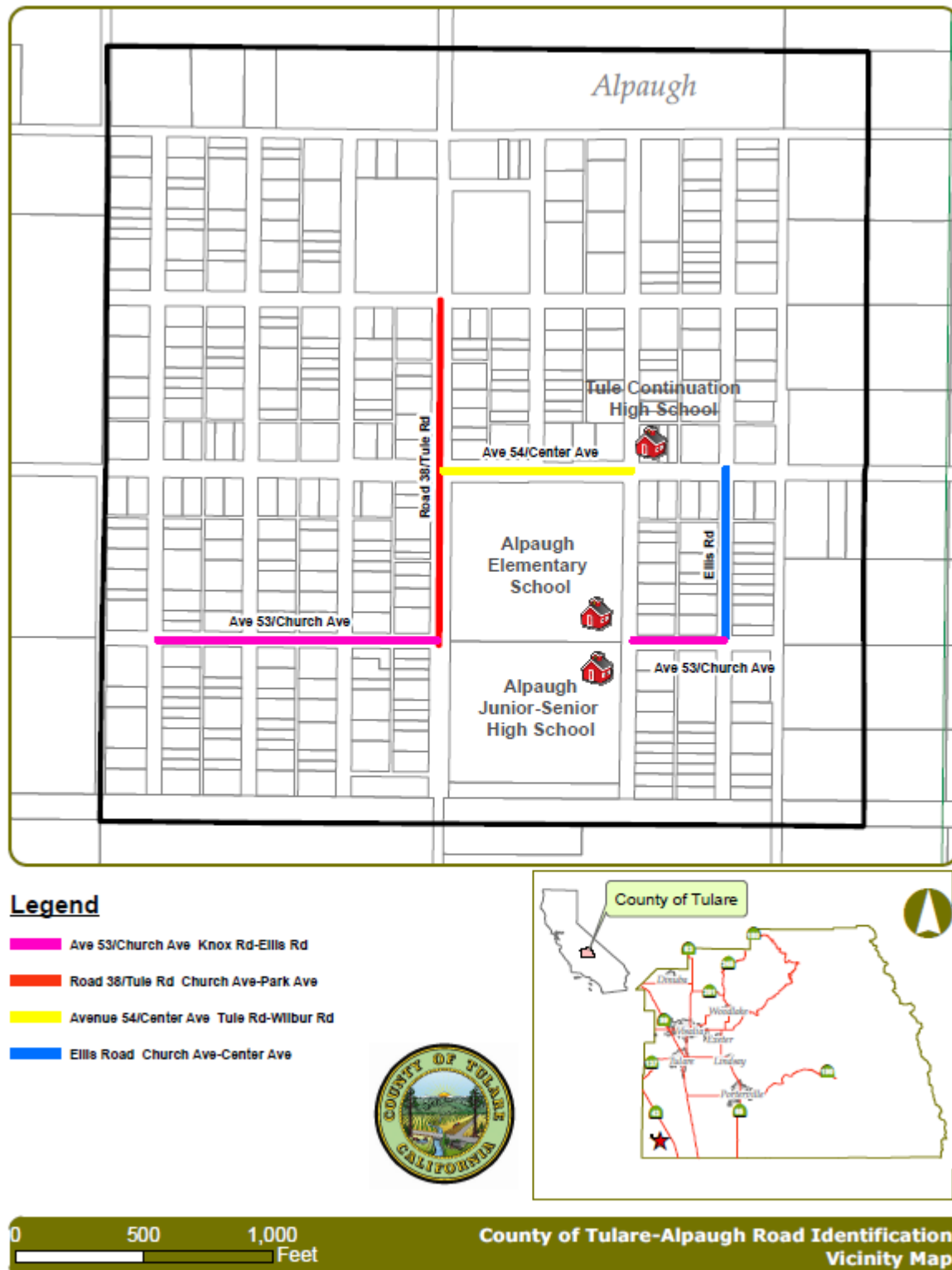
### Complete Streets Funding Opportunities

The following sections identify opinions of probable cost estimates for Complete Street transportation related improvements in Alpaugh. As shown in the tables, the funding sources include local, state and federal programs. Typically, local matches are required for acquiring state and federal funds. Measure R, a Tulare County sales tax for transportation, is available for such matches.

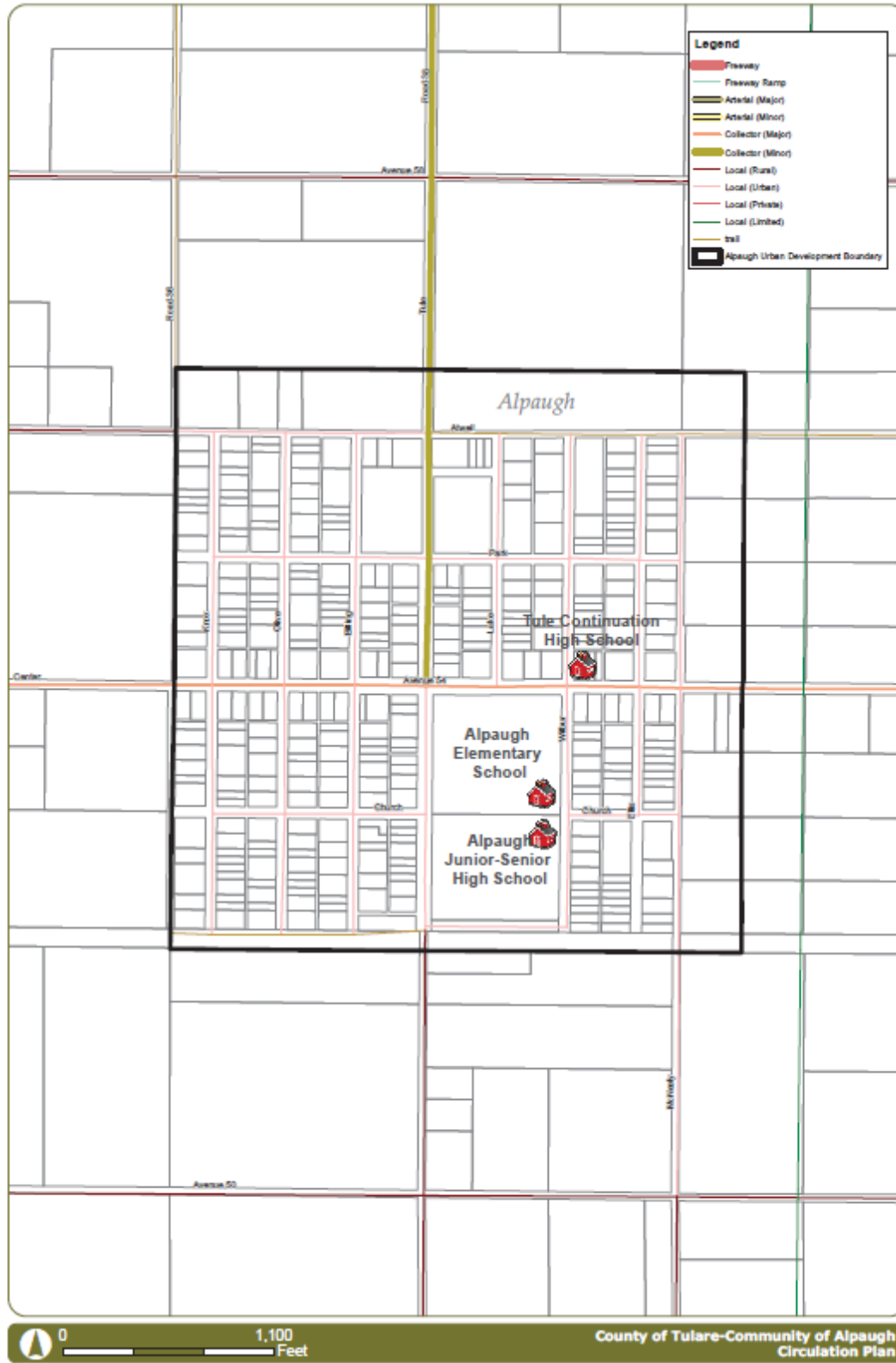
### Cost Estimates

Detailed cost estimates are included in Appendix E.

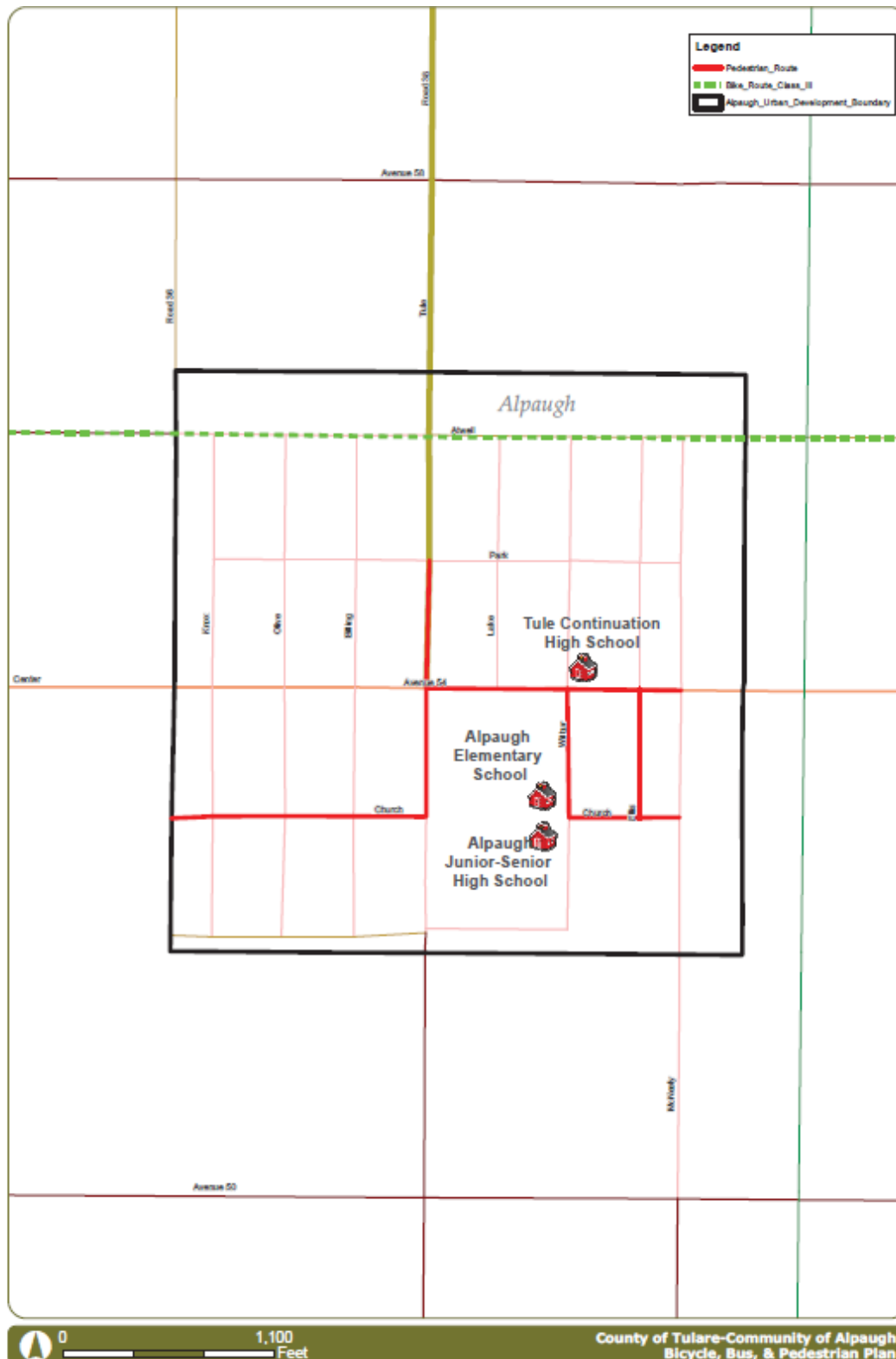
## ***Appendix A*** – Proposed Complete Streets Projects



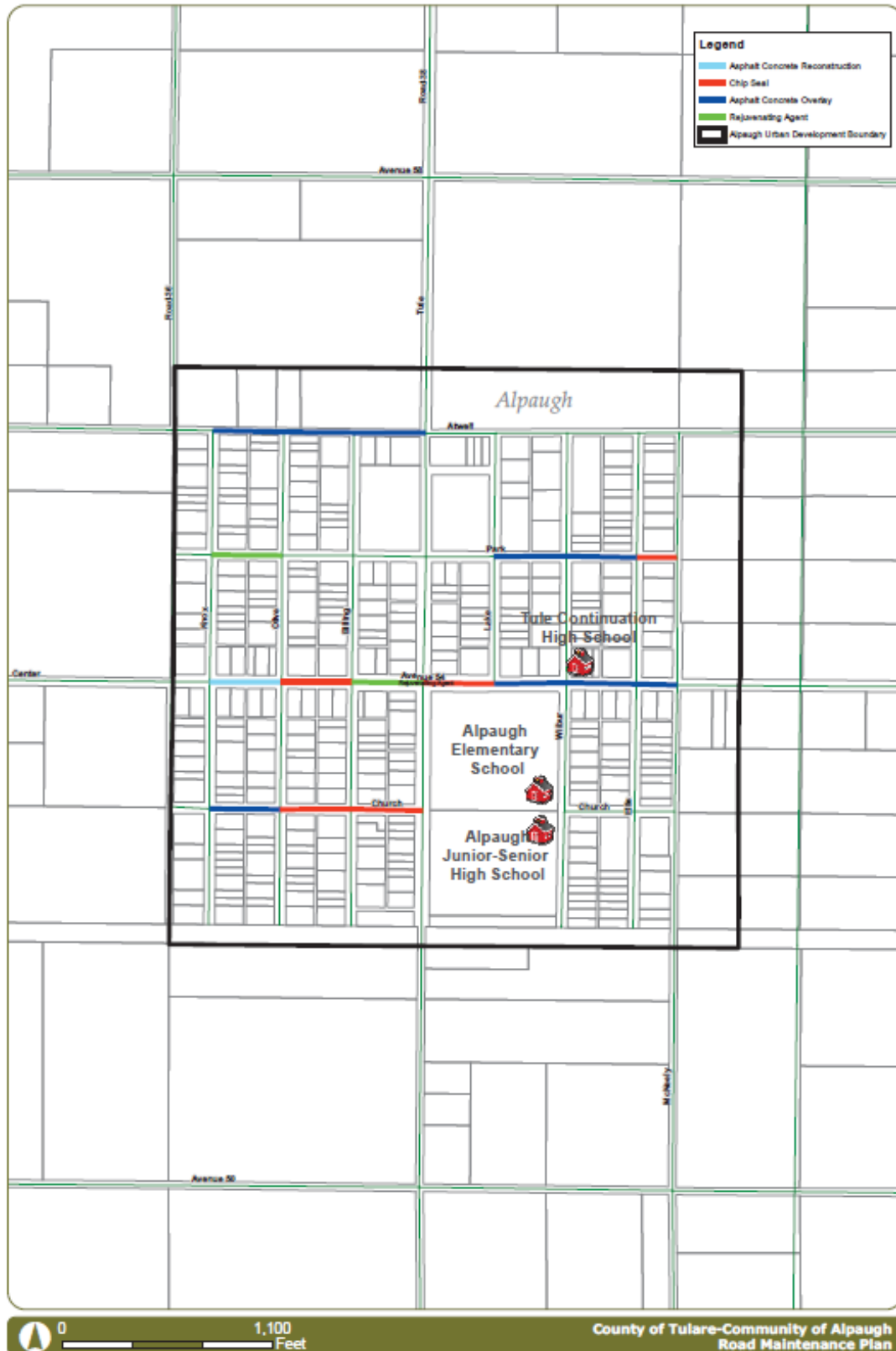
## Appendix B – Circulation Plan



## ***Appendix C*** – Bicycle, Bus, and Pedestrian Plan



## Appendix D – Road Maintenance Plan



# Appendix E – Cost Estimates for Alpaugh

Detailed Engineer's Estimate						
	<b>Agency:</b>	Tulare County Resource Management Agency				
	<b>Project Name:</b>	Alpaugh Improvements Sidewalk Project				
	<b>Project Location:</b>	Court Ave - E/O Park Dr to School St & School St - Court Ave to Terra Bella Ave				
	<b>Date of Estimate:</b>	December 9, 2016				
	<b>Prepared by:</b>	Ather Razaq/Diego Corvera				

Construction Items						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
1	999990	Mobilization	LS	1	\$100,000	\$100,000
2	050126	Construction Staking	LS	1	\$20,000	\$20,000
3	120090	Construction Area Signs	LS	1	\$25,000	\$25,000
4	120100	Traffic control system	LS	1	\$25,000	\$25,000
5	120300	Temporary Pavement Marker (Refl.)	LS	1	\$20,000	\$20,000
6	130200	Prepare Water Pollution Control Program	LS	1	\$10,000	\$10,000
7	220101	Finishing Roadway	LS	1	\$15,000	\$15,000
8	152390	Remove Roadside Sign	EA	27	\$200	\$5,400
9	152379	Relocate Fence	LF	974.98	\$20	\$19,500
10(F)	190101	Roadway excavation	CY	1,175	\$100	\$117,503
11(F)	250201	Class 2 Aggregate Base	CY	708	\$95	\$67,246
12	390133	Hot Mix Asphalt (Type B)	TON	1,019	\$120	\$122,305
13	394090	Place hot mix asphalt (miscellaneous area)	SQYD	47	\$100	\$4,700
14	731504	Minor Concrete (Curb & Gutter)	LF	3200.41	\$25	\$80,010
15	731521	Minor Concrete (Sidewalk)	SQFT	11045	\$8	\$88,357
16	731623	Minor Concrete (Ramp)	EA	13	\$4,000	\$52,000
17	731516	Minor Concrete (Driveway)	SF	3491	\$15	\$52,371
18	152434	Adjust riser	EA	6	\$800	\$4,800
19	152469	Adjust Utility Cover to Grade	EA	1	\$800	\$800
20	568023	Install Roadside Sign	EA	27	\$500	\$13,500
21		Bio Swale	SQFT	255	\$20	\$5,100

Sub-Total: **\$848,591**

\* Up to 10% Contingency may be included in Engineer's Estimate

\*Contingency: **\$84,859**

(F) = Final Pay Item

TOTAL: **\$933,451**

Non-Construction Related Cost						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
22	-	Environmental Clearance	% of CON	5%	\$848,591.49	\$42,429.57
23	-	Preliminary Engineering (PE)	% of CON	10%	\$848,591.49	\$84,859.15
24	-	Construction Engineering (CE)	% of CON	15%	\$848,591.49	\$127,288.72
Total:						<b>\$254,577.45</b>

<b>Total Construction &amp; Non-Construction Items</b>	<b>\$1,188,028.09</b>
--	-----------------------





INDEX OF SHEETS

SHEET NO.	PAGE NO.	DESCRIPTION
T1	1	TITLE SHEET
X1	2	TYPICAL CROSS SECTIONS
C1	3	CONSTRUCTION DETAILS
L1-L2	4-5	PLAN SHEETS CHURCH AVE
L3-L4	6-7	PLAN SHEETS CENTER ST / AVE 54
L5	8	PLAN SHEETS TULE RD / ROAD 38
G1-G9	9-17	GRADING PLANS
PD1-PD2	18-19	SIGNING AND STRIPING PLANS CHURCH AVE
PD3-PD4	20-21	SIGNING AND STRIPING PLANS CENTER ST / AVE 54
PD5	22	SIGNING AND STRIPING PLANS TULE RD / ROAD 38
CAS1	23	CONSTRUCTION AREA SIGN PLAN

ABBREVIATIONS

AB	AGGREGATE BASE
AC	ASPHALT CONCRETE
AP	ANGLE POINT
BC	BEGIN HORIZONTAL CURVE
CL, ☿	CENTER LINE
CONC	CONCRETE
C-C	CENTER TO CENTER
DI	DROP INLET
DIA	DIAMETER
DWY	DRIVEWAY
EC	END OF HORIZONTAL CURVE
EP	EDGE OF PAVEMENT
EXIST	EXISTING
FL	FLOW LINE
GALV	GALVANIZED
GB	GRADE BREAK
HCR	HANDICAP RAMP
INV	INVERT
LT	LEFT
MAX	MAXIMUM
MIN	MINIMUM
NO	NUMBER
OG	ORIGINAL GROUND
PROP	PROPOSED
R	RADIUS
R/W	RIGHT OF WAY
RP	RADIUS POINT
RT	RIGHT
SCH	SCHEDULE
STA	STATION
SW	SIDEWALK
TFC	TOP FACE OF CURB
TYP	TYPICAL
VAR	VARIES

BASIS OF BEARING

XXXX

BASIS OF ELEVATION

xxx



Know what's below.  
Call before you dig.

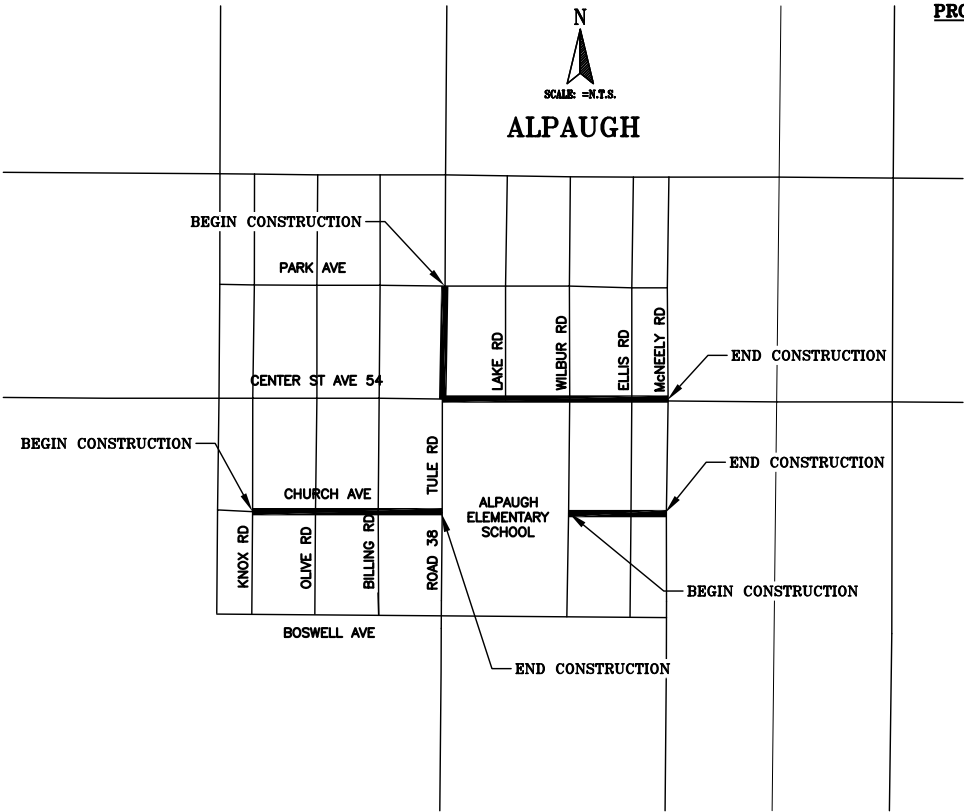
Contractor shall call  
Underground Service Alert at  
811 two working days prior  
to excavation

STATE OF CALIFORNIA  
COUNTY OF TULARE

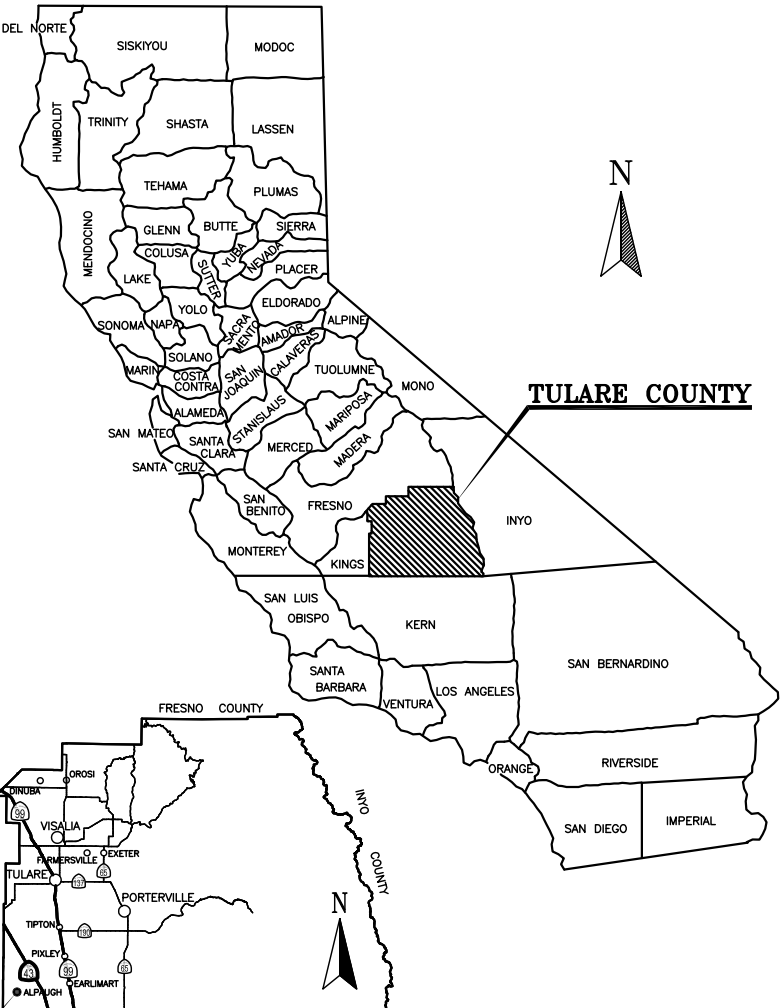
PROJECT PLANS FOR CONSTRUCTION OF  
ALPAUGH SIDEWALK  
IMPROVEMENTS PROJECT  
FUNDING  
INFO

TO BE SUPPLEMENTED BY STANDARD PLANS AND STANDARD SPECIFICATIONS OF  
THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION DATED 2010

Appendix F – Alpaugh Avenue 30%  
Submittal Plan Set



VICINITY MAP



PROJECT LOCATION

LOCATION MAP

APPROVED BY: DATE:

Peter Vander Pohl, CHAIRMAN  
COUNTY OF TULARE BOARD OF SUPERVISORS

APPROVED BY: DATE:

Reed Schenke, P.E. ASSISTANT DIRECTOR-PUBLIC WORKS  
COUNTY OF TULARE RESOURCE MANAGEMENT AGENCY

SIGNED BY: DATE:

Jabed Khan, P.E. Engineer IV  
COUNTY OF TULARE RESOURCE MANAGEMENT AGENCY



REVISIONS	DATE	BY
No.		
DESCRIPTION		

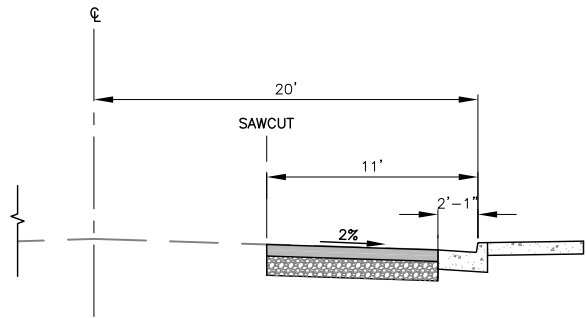
COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA



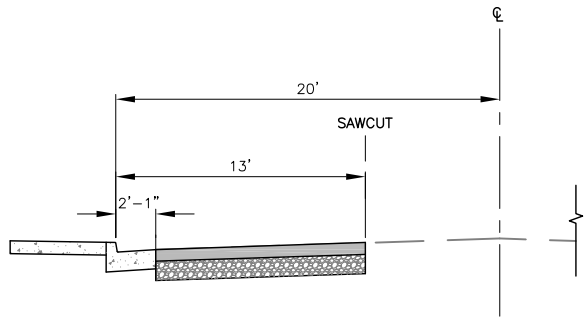
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ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT  
TULARE COUNTY

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DIVISION	DESIGN
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DESIGNED	PAO
DRAWN	PAO
CHECKED	JRK
FILE	14008-1T001.DWG
DATE	04-16-2015
SHEET No.	

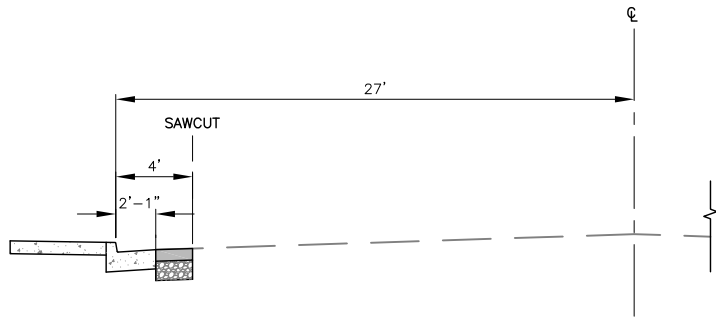
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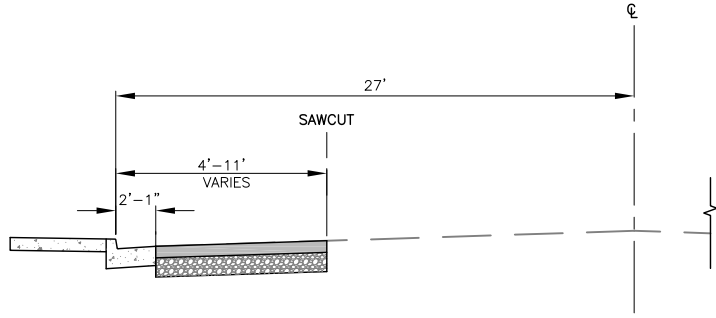
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STA 16+19.48 TO STA 19+11.76  
STA 19+89.76 TO STA 22+71.30



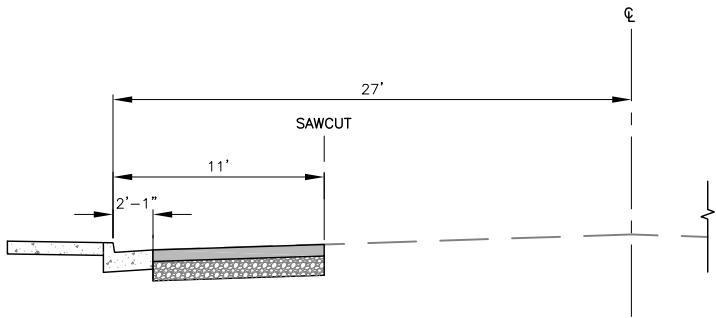
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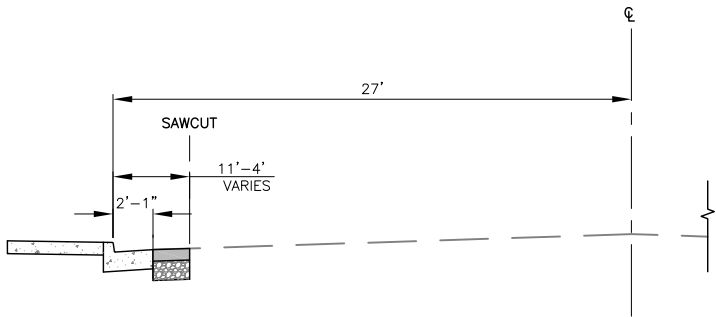
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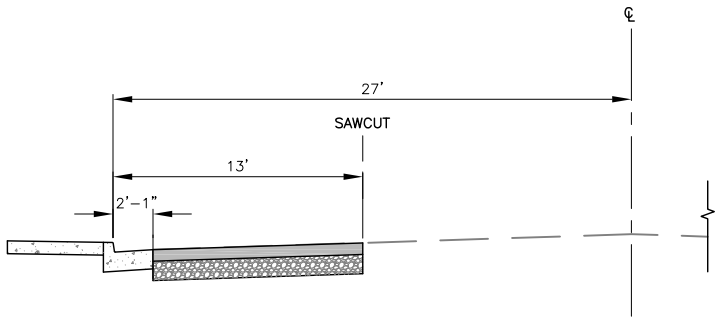
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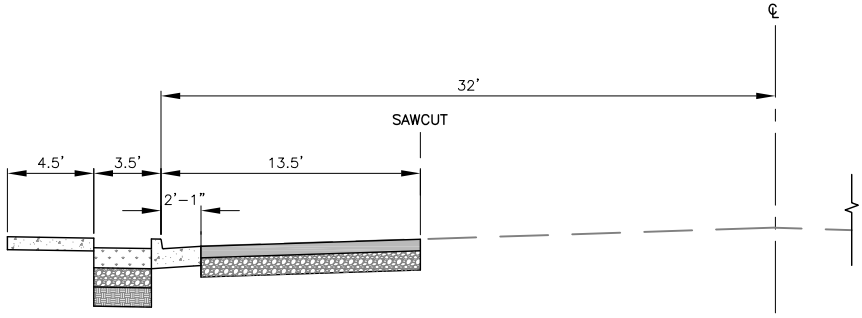
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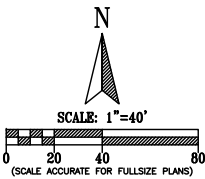
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**CENTER ST AVE 54**  
STA 40+98.13 TO STA 43+94.26



**ROAD 38 / TULE RD**  
STA 23+68.88 TO STA 29+52.11



REVISIONS

No.	DESCRIPTION	DATE	BY

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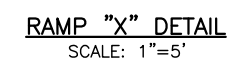
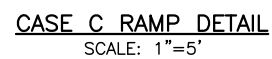
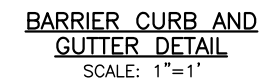
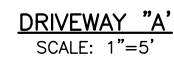
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ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT

TULARE COUNTY

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DRAWN	PAO
CHECKED	JRK
FILE	14008-1X001.DWG
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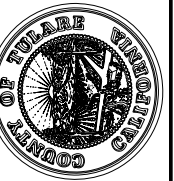
2 OF X



1. THE CURB RAMP SHALL BE OUTLINED WITH GROOVES PER GROOVING DETAIL CALTRANS STANDARD PLAN A88A.
2. COUNTER SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO AND WITHIN 24 INCHES OF THE CURB RAMP SHALL NOT BE STEEPER THAN 1:20 (5%). GUTTER PAN SLOPE SHALL NOT EXCEED 1" OF DEPTH FOR EACH 2'-0" OF WIDTH.
3. CURB RAMPS SHALL HAVE A DETECTABLE WARNING SURFACE THAT EXTENDS THE FULL WIDTH AND 3'-0" DEPTH OF THE RAMP. DETECTABLE WARNING SURFACES SHALL CONFORM TO CALTRANS STANDARD PLAN A88A.
4. THE EDGE OF THE DETECTABLE WARNING SURFACE NEAREST THE STREET SHALL BE BETWEEN 6" AND 8" FROM THE GUTTER FLOWLINE.
5. ALL DIMENSIONS AND LAYOUT OF CURB RAMPS SHALL CONFORM TO THE MIN AND MAX PER CALTRANS STD PLAN A88A.

[illegible]

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CONSTRUCTION DETAILS

**ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT**

TULARE COUNTY

SCALE	PER DETAIL
VISION	DESIGN
DB NO.	14008-1
DESIGNED	PAO
DRAWN	PAO
CHECKED	JRK
FILE	14008-1C001.DWG
DATE	04-16-2015
SHEET No.	

C1

3 OF X



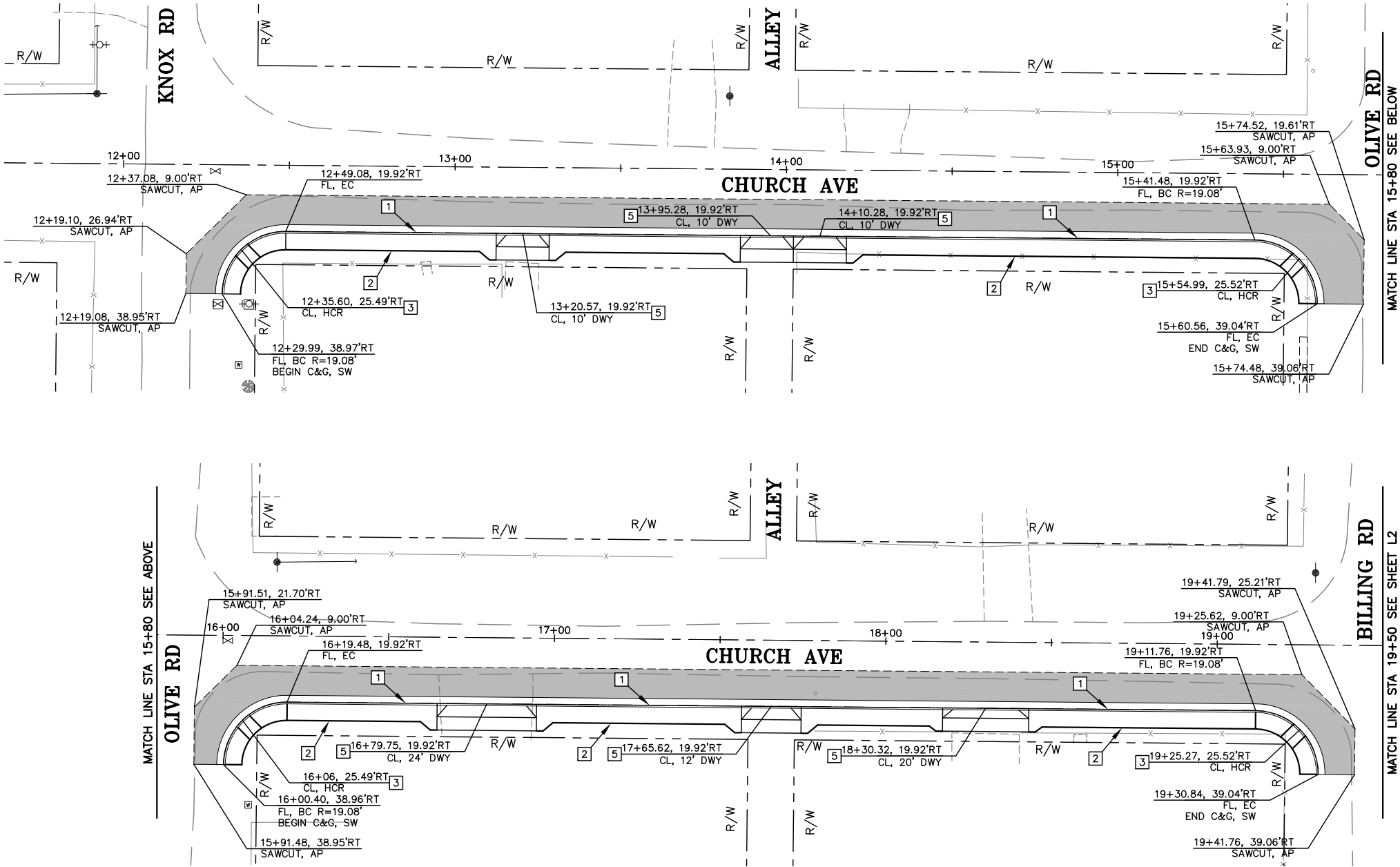
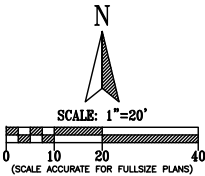
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[2] SIDEWALK, WIDTH PER PLAN.  
[3] CASE C RAMP PER DETAIL ON SHEET C1.  
[4] RAMP "X" PER DETAIL ON SHEET C1.  
[5] DRIVEWAY "A" PER DETAIL ON SHEET C1.  
[6] DRIVEWAY "B" PER DETAIL ON SHEET C1.

LEGEND

- PROPOSED STRUCTURAL SECTION  
EXIST MANHOLE  
EXIST DI  
EXIST MAILBOX  
PROP MAILBOX  
EXIST FIRE HYDRANT  
EXIST WATER METER  
EXIST WATER VALVE  
EXIST TELEPHONE POLE  
EXIST ELECTRICAL POLE

- EXIST FENCE  
EXIST GAS LINE  
EXIST SEWER LINE  
EXIST STORM DRAIN  
EXIST WATER LINE



REVISIONS

No.	DESCRIPTION	DATE	BY

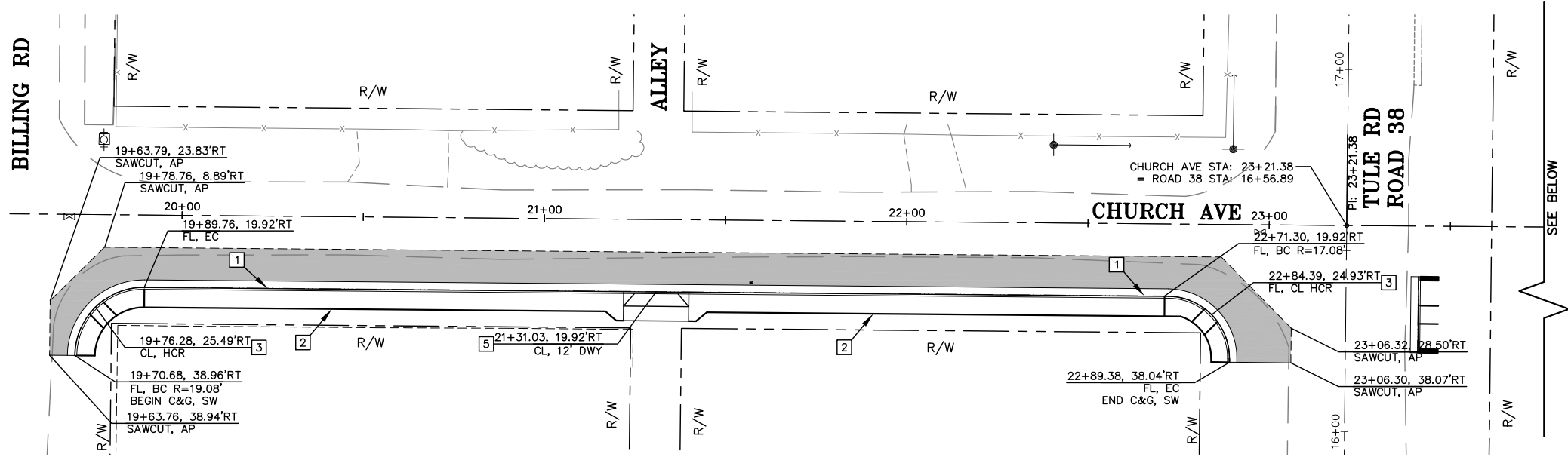
COUNTY OF TULARE  
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PLAN SHEETS  
CHURCH AVE  
ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT  
TULARE COUNTY

SCALE 1"=20'  
DIVISION DESIGN  
JOB NO. 14008-1  
DESIGNED PAO  
DRAWN PAO  
CHECKED JRK  
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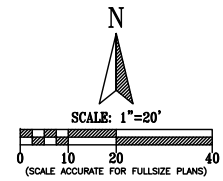
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[illegible]

- 1 BARRIER CURB AND GUTTER PER DETAIL ON SHEET C1.
- 2 SIDEWALK, WIDTH PER PLAN.
- 3 CASE A RAMP PER DETAIL ON SHEET C1.
- 4 CASE C RAMP PER DETAIL ON SHEET C1.
- 5 DRIVEWAY "A" PER DETAIL ON SHEET C1.



**COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY**

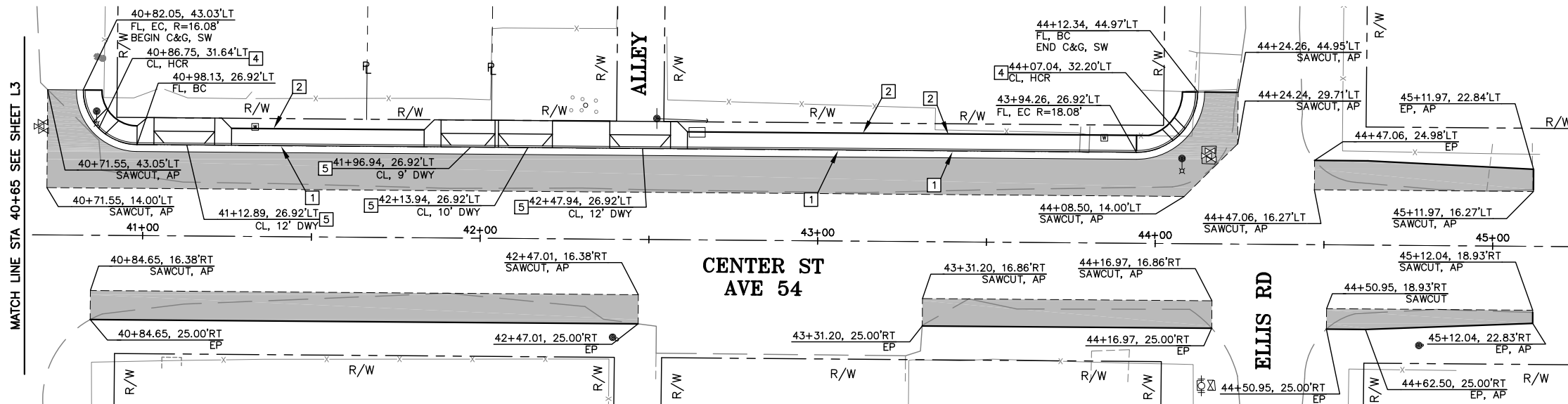
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559) 624-7000  
[WWW.TULARECOUNTY.CA.GOV/RMA](http://WWW.TULARECOUNTY.CA.GOV/RMA)



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DRAWN	PAO
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**6** *OF* **X**

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- 2 SIDEWALK, WIDTH PER PLAN.
- 3 CASE A RAMP PER DETAIL ON SHEET C1.
- 4 CASE C RAMP PER DETAIL ON SHEET C1.
- 5 DRIVEWAY "A" PER DETAIL ON SHEET C1.

[illegible]

**COUNTY OF TULARE**  
**RESOURCE MANAGEMENT AGENCY**  
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PLAN SHEETS  
CENTER ST / AVE 54  
**ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT**  
TULARE COUNTY

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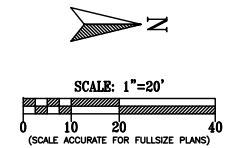
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[illegible]

**COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY**

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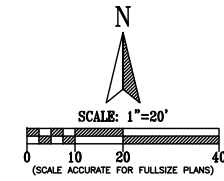


PLAN SHEETS  
TULE RD / RD 38  
**ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT**  
TULARE COUNTY

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8 OF X

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GRADING PLANS

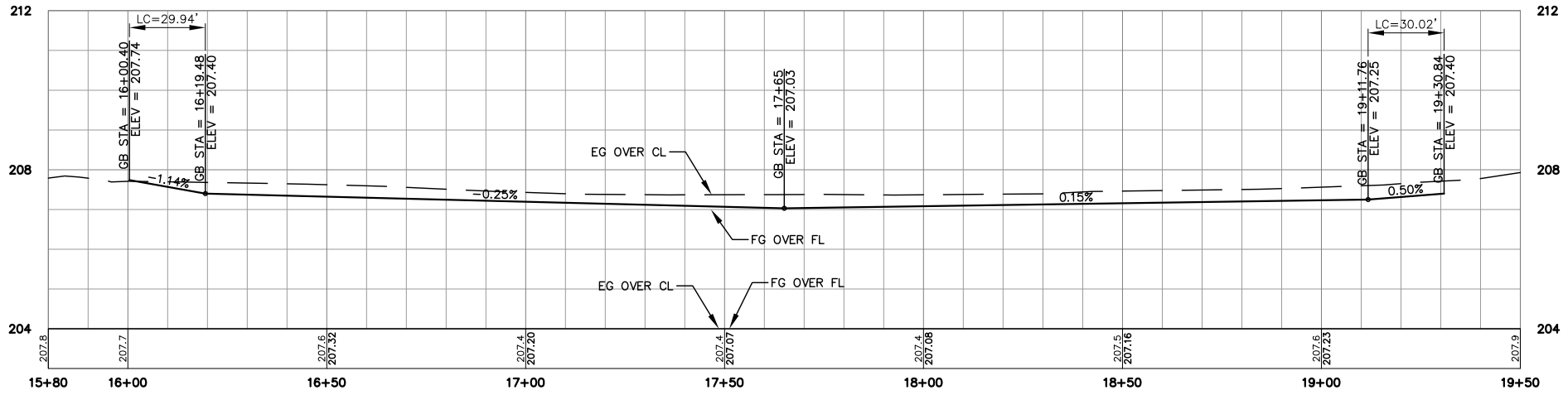
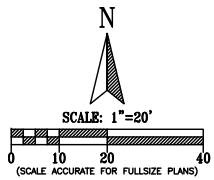
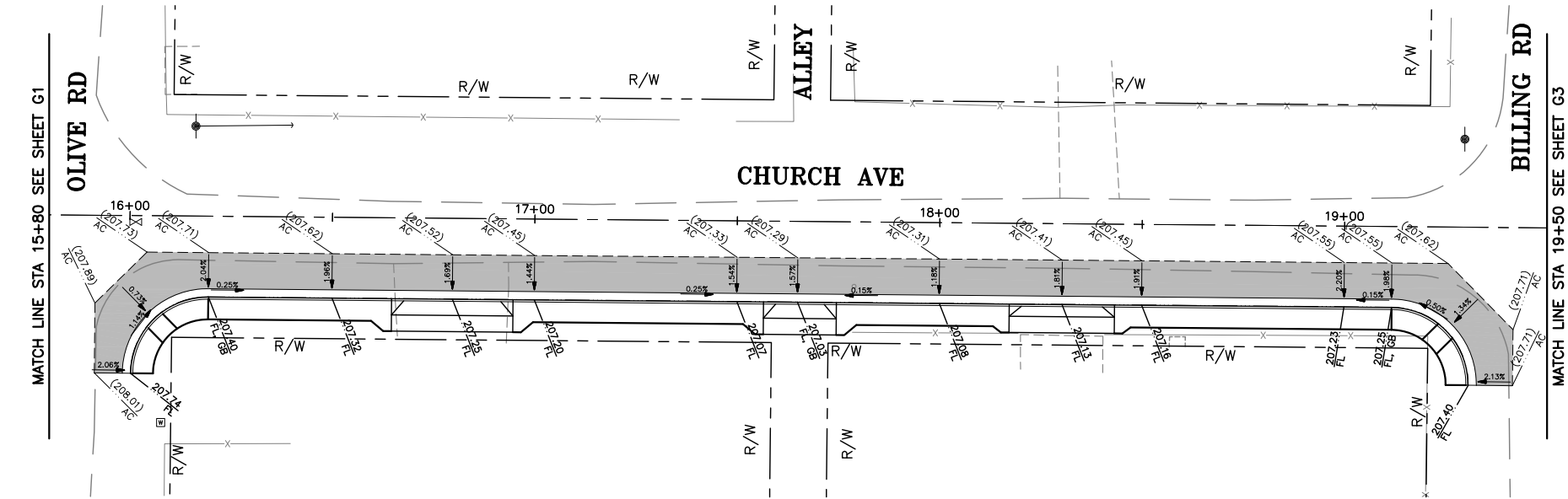
**ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT**

TULARE COUNTY

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DATE	04-16-2015

**G1**

**9 OF X**



CHURCH AVE PROFILE



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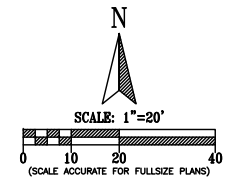
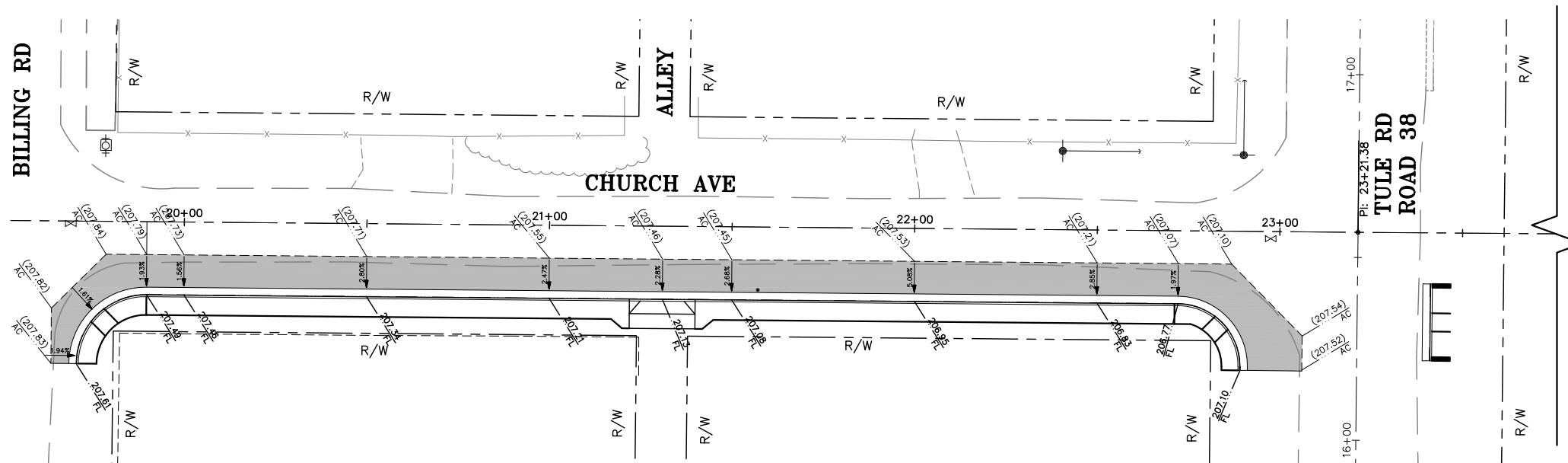
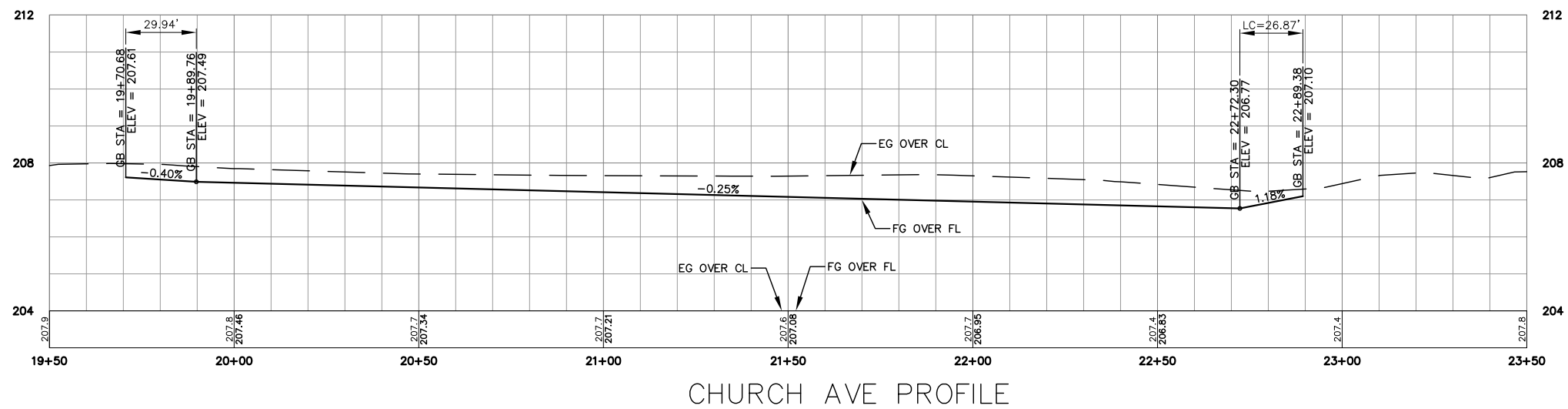
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REVISIONS	
No.	DESCRIPTION



COUNTY OF TULARE  
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GRADING PLANS  
ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT  
TULARE COUNTY



SCALE	1"=20'
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**G3**

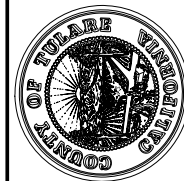
11 OF X

GRADING PLANS

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**ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT**

TULARE COUNTY



COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
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VISALIA, CA 93277  
(559) 624-7000  
[WWW.TULARECOUNTY.CA.GOV/RMA](http://WWW.TULARECOUNTY.CA.GOV/RMA)

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SIGN NOTES

- ① NEW SIGN, SIGN NUMBER
- ② SIGN TO REMAIN
- ③ SIGN TO BE REMOVED
- ④ RELOCATE EXIST SIGN, PROVIDE 12' MIN OFFSET FROM EDGE OF TRAVELED WAY

LEGEND

- ◁21▷ STRIPING DETAIL TYPE PER STANDARD PLANS
- ◁LL▷ LIMIT LINE PER STANDARD PLANS
- ◁CW▷ CROSSWALK PER STANDARD PLANS (WHITE)
- ◁CY▷ CROSSWALK PER STANDARD PLANS (YELLOW)

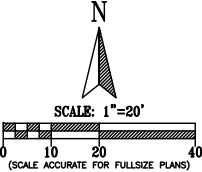
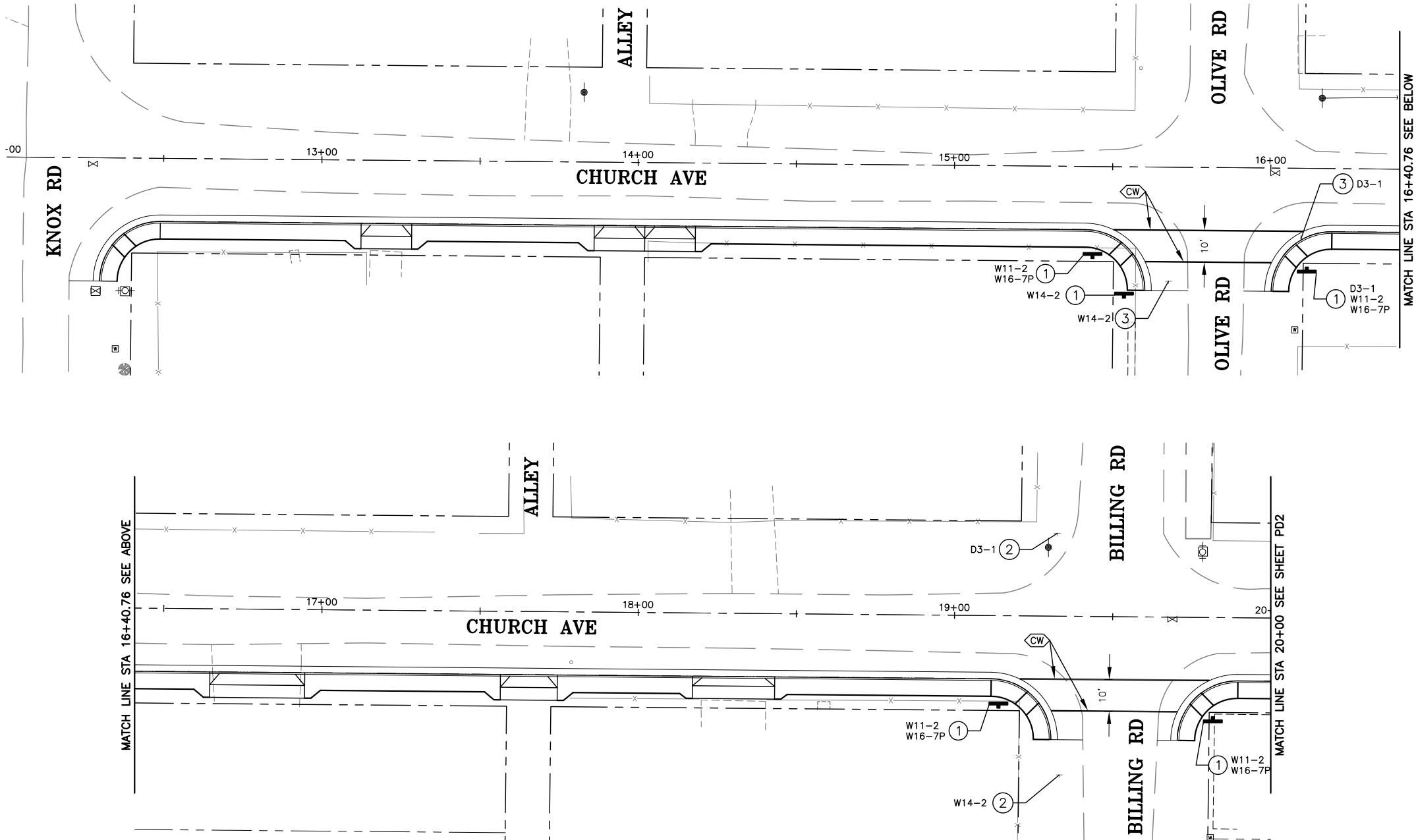
- ▬ STRIPING DETAIL LIMIT
- ▲ STRIPING DETAIL ANGLE
- ⚡ STRIPING DETAIL CHANGE
- ⊕ ONE POST ROAD SIGN

▬ PAVEMENT MARKING PER STANDARD PLANS

➡ DIRECTION OF TRAVEL

GENERAL NOTES:

- 1. ALL EXISTING STRIPING AND PAVEMENT MARKING WITHIN STRIPING LIMITS SHALL BE REMOVED BY GRINDING AS DIRECTED BY THE ENGINEER.



REVISIONS	
No.	DESCRIPTION

COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA

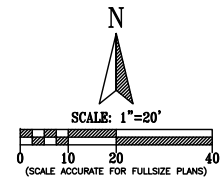
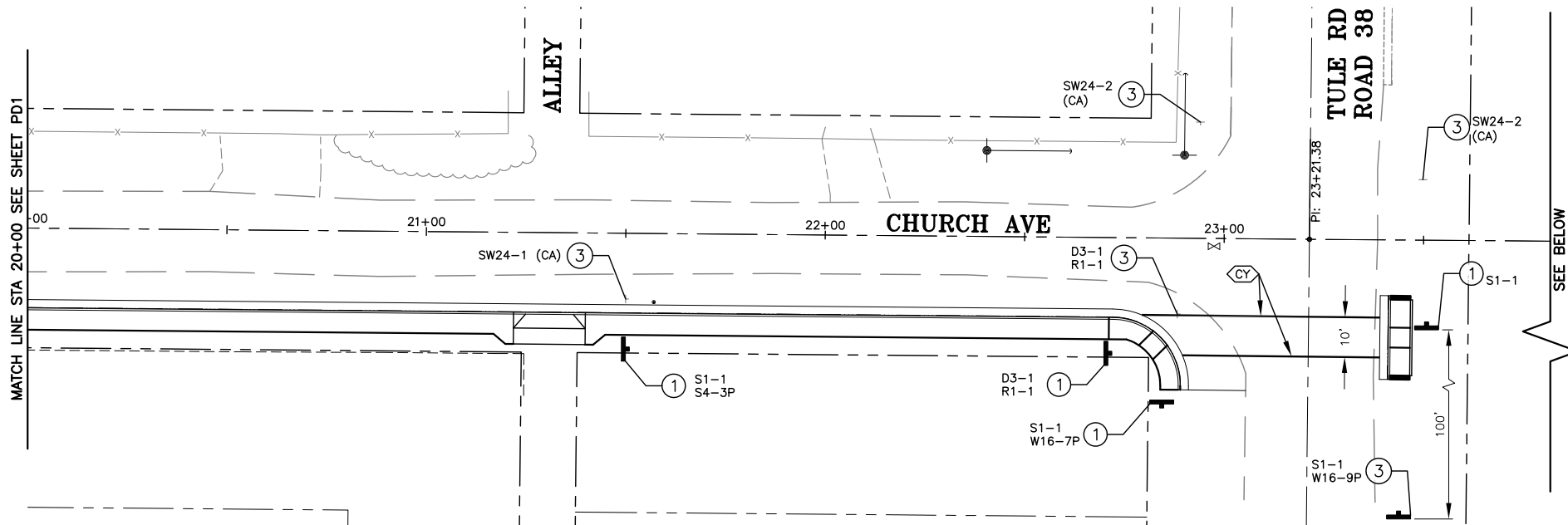


SIGNING AND STRIPING PLAN  
CHURCH AVE  
ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT  
TULARE COUNTY

SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	14008-1
DESIGNED	PAO
DRAWN	TMD
CHECKED	JRK
FILE	14008-1PD001.DWG
DATE	04-16-2015
SHEET No.	

PD1  
18 OF X

\\MP\PROJECTS\14008-1\14008-1PD001.DWG 9/20/2016 10:05 AM



SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	14008-1
DESIGNED	PAO
DRAWN	TMD
CHECKED	JRK
FILE	14008-1PD001.DWG
DATE	04-16-2015
SHEET No.	

PD2

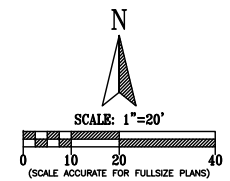
19 OF X

SIGNING AND STRIPING PLAN  
CHURCH AVE  
**ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT**  
TULARE COUNTY



COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA

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**COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY**  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
[WWW.TULARECOUNTY.CA.GOV/RMA](http://WWW.TULARECOUNTY.CA.GOV/RMA)

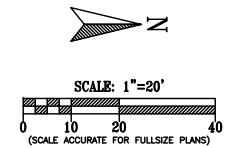
SIGNING AND STRIPING PLAN  
CENTER ST / AVE 54  
**ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT**  
TULARE COUNTY


SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	14008-1
DESIGNED	PAO
DRAWN	TMD
CHECKED	JRK
FILE	14008-1PD002.DWG
DATE	04-16-2015
SHEET No.	

PD3  
20 OF X

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<div>SIGNING AND STRIPING PLAN TULE RD / RD 38 ALPAUGH IMPROVEMENTS SIDEWALK PROJECT TULARE COUNTY</div>		<div><div>COUNTY OF TULARE RESOURCE MANAGEMENT AGENCY 5961 SOUTH MOONEY BLVD. VISALIA, CA 93277 (559)624-7000 WWW.TULARECOUNTY.CA.GOV/RMA</div></div>	REVISIONS			
			No.	DESCRIPTION	DATE	BY

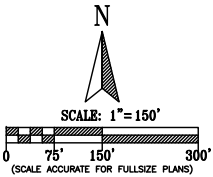
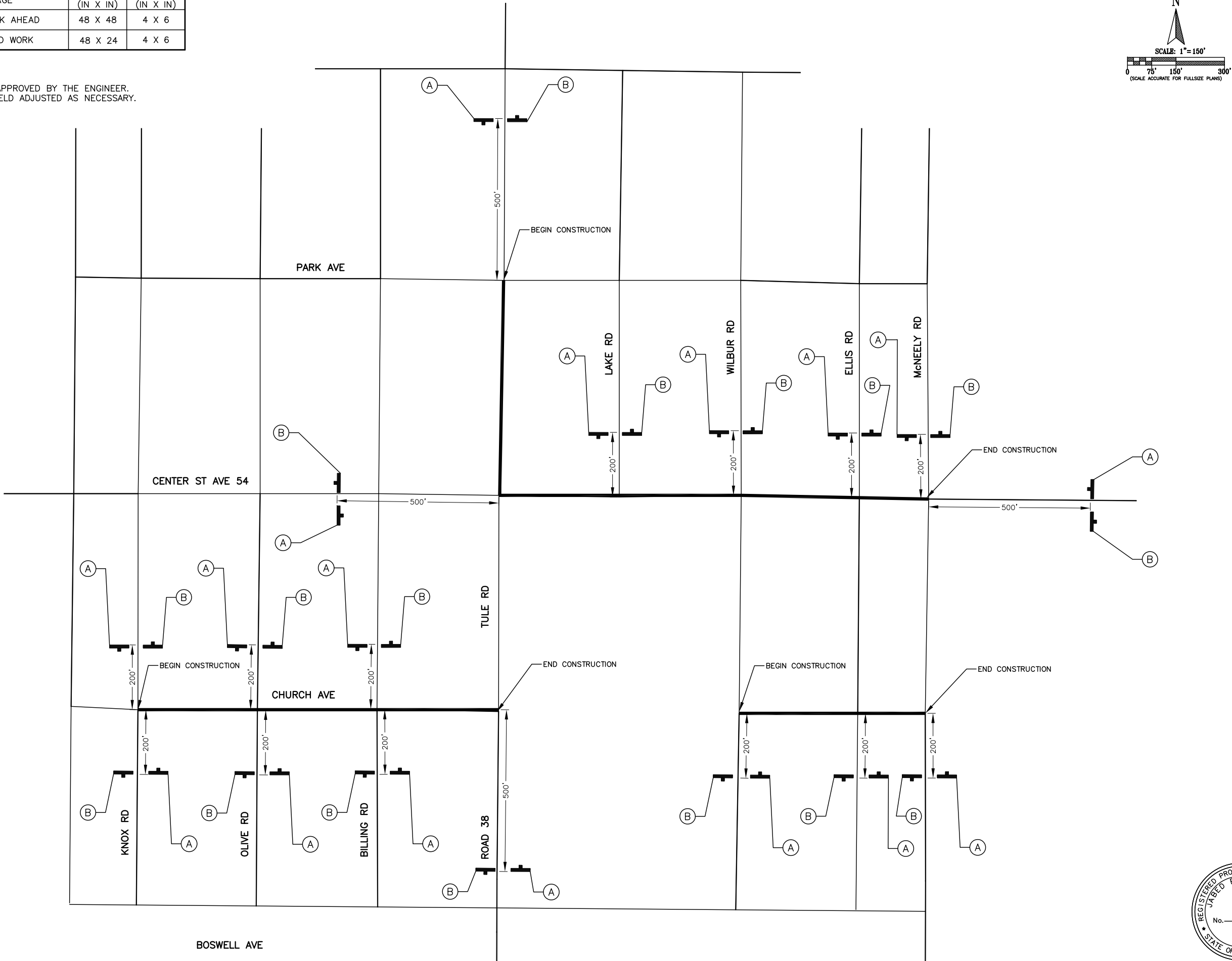
SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	14008-1
DESIGNED	PAO
DRAWN	TMD
CHECKED	JRK
FILE	14008-1PD003.DWG
DATE	04-16-2015
SHEET No.	

PD5

22 OF X

CONSTRUCTION AREA SIGNS				
TYPE	CODE	MESSAGE	PANEL SIZE (IN X IN)	POST SIZE (IN X IN)
(A)	C-23(CA)	ROAD WORK AHEAD	48 X 48	4 X 6
(B)	G20-2	END ROAD WORK	48 X 24	4 X 6

1. LOCATIONS TO BE APPROVED BY THE ENGINEER.  
2. SIGNS SHALL BE FIELD ADJUSTED AS NECESSARY.



REVISIONS

No.	DESCRIPTION	DATE	BY

COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY



5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA

CONSTRUCTION AREA SIGN PLAN

ALPAUGH IMPROVEMENTS  
SIDEWALK PROJECT

TULARE COUNTY

SCALE	1"=150'
DIVISION	DESIGN
JOB NO.	14008-1
DESIGNED	PAO
DRAWN	TMD
CHECKED	JRK
FILE	14008-1CAS001.DWG
DATE	04-16-2015
SHEET No.	CAS1

23 OF X

MAPPROJECTS\14008-1\14008-1\CAS001.DWG 9/20/2016 10:57 AM

## ***Appendix G –***

### **Complete Streets Outreach**

#### **Community Meeting**

Self-Help Enterprises conducted a community meeting (with five SHE staff members present) in Alpaugh on Thursday, September 3, 2015 at 6:00pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held in the cafeteria of the Alpaugh Elementary School located at 5313 Road 39 in Alpaugh. This is an ideal meeting location because residents know where the school is located and most feel welcomed and comfortable being there. The purpose of the meeting was to discuss Alpaugh improvement needs, gather community data, and report the findings of the meeting in a final report. Nineteen (19) Alpaugh residents attended the meeting. Sixteen (16) community surveys were collected.

#### **Community Survey**

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Alpaugh SGC community input meeting, residents concluded that the following is a list of Alpaugh priority improvement needs

#### **Priority Improvements**

- 1) Road Conditions & Street Safety
- 2) Community Safety
- 3) Medical Care Access
- 4) Community Sewer System
- 5) Local Market
- 6) Internet Access
- 7) Community Resource Center
- 8) Loose Dogs
- 9) Natural Gas
- 10) County Park Management

#### **Road Conditions and Street Safety**

Alpaugh residents expressed that street safety, including the need for sidewalks, street lighting, speed limit signs, stop signs, and the need for enforcement of traffic laws are the highest priority improvement needs in Alpaugh. Survey results show that most residents are not satisfied with road conditions. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their





vehicles while driving on Alpaugh roads. Residents report that roads are narrow and that two cars barely fit on one road at the same time. Alpaugh roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired. 100% of the respondents are not satisfied with the conditions of the roads. 100% of the respondents described the roads to be in “poor” conditions.

The following roads are a priority to the residents:

- Tule Road
- Avenue 54
- Ellis Road
- Avenue 50

**Survey comments regarding road conditions:**

**“Very bad road conditions” “Roads have not been fixed in a many years” “Dark roads, no street lights” “Street lighting is not safe for children” “All of the roads bumps and pot holes make for dangerous driving conditions” “Road need sand or patching” “Roads are so bad, we need a four wheel drive to drive down the road” “Road conditions cause a bumpy ride and many burst tires” “The roads around the school are bad” “Caution signs needed around the school” “Narrow roads, only room for one car”**

**Sidewalks**

The community of Alpaugh has too few sidewalks. There are sidewalks around the north and west side of the school, and a short stretch of sidewalk in front of the library, post office and store only. The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Alpaugh traffic travels at a high rate of speed (see next section), and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.



# ALPAUGH COMMUNITY PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions  
Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9371

General Plan Amendment GPA 17-035-Resolution No. 9372

Section 18.9 “Mixed Use” Combining Zone-Resolution No. 9373

Section 16 “By Right Uses”-Resolution No. 9374

Zoning Districts Map-Resolution No. 9375

Sustainable Community Plans-Resolution No. 9376

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE COMMUNITY PLANS 2017 ) RESOLUTION NO. 9371  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Community Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Community Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Community Plans and Urban Development Boundaries, (Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, and Sultana General Plan Amendment No. GPA 17-012, Change of Zones (PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map , for an amendment PZC 17-042 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundary, and Zone Ordinance Amendment No. PZC 17-041 establishing a Mixed Use Combining Zone in Alpaugh, East Orosi, London, Richgrove, and Sultana (incorporated by reference herein). The unincorporated Communities are located within the Mount Diablo Base & Meridian as follows: Alpaugh, Sections, 27, 28, 33 & 34, Township 23S, Range 23E, East Orosi, Sections 9, & 16, Township 16S, Range 25E, London, Sections 1, 2, 11 & 12, Township 20S, Range 25E, Richgrove, Section 36, Township 24S, Range 26E, and Section 31, Township 24S, Range 27E, and Sultana, Sections 10, 11, 14 & 15, Township 16S, Range 24E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Alpaugh, East Orosi, London, Richgrove, and Sultana to the Mixed-Use Combining Zone; (3) allow additional by-right uses

within the aforementioned Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Community Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-042 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map within the respective Urban Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-041 an amendment to Section 16 to allow additional by-right uses in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Community Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Community Plans;
  - b. Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Community Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Community planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Community Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Community Plans 2017 Update General Plan Amendment 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004; Change of Zone No. PZC 17-004 (Zoning District Map); East Orosi General Plan Amendment No. GPA 17-034; Change of Zone No. PZC 17-005 (Zoning District Map); London General Plan Amendment No. GPA 17-008; Change of Zone No. PZC 17-008 (Zoning District Map); Richgrove General Plan Amendment No. GPA 17-011; Change of Zone No. PZC 17-011 (Zoning District Map); Sultana General Plan Amendment No. GPA 17-012; Change of Zone No. PZC 17-012 (Zoning District Map; Change of Zone No. PZC 17-042 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-041 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Community Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,



4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Community Plans 2017 Update. Therefore, the public will benefit from the Community Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III COMMUNITY PLANS, OF THE ) RESOLUTION NO.9372  
TULARE COUNTY GENERAL PLAN FOR THE )  
COMMUNITY PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-004, GPA 17-034, )  
GPA 17-008, GPA 17-011, GPA 17-012, )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Community Plan of the Tulare County General Plan as set forth in Exhibits "A to E" for the Community Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, Sultana General Plan Amendment No. GPA 17-012 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the communities as part of the Community Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and



WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Community Plans of the Tulare County General Plan for the Community Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Community Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Community Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Urban Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the adoption of urban development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Community Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Community Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:


C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Community Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

- Exhibit “A” – 2017 Alpaugh Community Plan (Part III)
- Exhibit “B” – 2017 East Orosi Community Plan (Part III)
- Exhibit “C” – 2017 London Community Plan (Part III)
- Exhibit “D” – 2017 Richgrove Community Plan (Part III)
- Exhibit “E” – 2017 Sultana Community Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO            )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT        )        RESOLUTION NO. 9373  
WITH THE ADOPTED COMMUNITY PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE    )  
NO. PZC 17-041                                    )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Community Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352.

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana ;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, Earlimart, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and



Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Community Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Communities of Alpaugh, East Oroquieta, London, Richgrove, and Sultana, consistent with the adopted Community Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:       Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:       None  
ABSTAIN:   None  
ABSENT:     None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9374
CONSISTENT WITH THE ADOPTED	)	
COMMUNITY PLANS, AS PROPOSED IN	)	
CHANGE OF ZONE NO. PZC 17-042	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Earlimart, Ducor Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hypericum, Jovista, Matheny Tract and Tooleville. The Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
ORDINANCE NO. 352, ZONING ORDINANCE )  
DISTRICT MAPS TO REZONE PROPERTIES )  
IN THE URBAN DEVELOPMENT BOUNDARIES )  
OF ALPAUGH, EAST OROSI, LONDON, )  
RICHGROVE, AND SULTANA ) RESOLUTION NO. 9375  
CONSISTENT WITH THE ADOPTED )  
COMMUNITY PLANS 2017 UPDATE (GPA 17-035), )  
AS PROPOSED IN CHANGE OF ZONE NO. PZC's )  
PZC 17-004, PZC 17-005, PZC 17-008, PZC 17-011, )  
AND PZC 17-012, )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Alpaugh Rezoning Plan (PZC 17-004), to include the Community of Alpaugh, consistent with the adopted Alpaugh Community Plan 2017 Update, Exhibit "E" East Orosi Rezoning Plan (PCZ 17-004), to include the Community of East Orosi, consistent with the adopted East Orosi Community Plan 2017 Update, Exhibit "H" London Rezoning Plan (PCZ 17-016), to include the Community of London, consistent with the adopted London Community Plan 2017 Update, Exhibit "K" Richgrove Rezoning Plan (PCZ 17-017), to include the Community of Richgrove, consistent with the adopted Richgrove Community Plan 2017 Update, Exhibit "N" Sultana Rezoning Plan (PCZ 17-018), to include the Community of Sultana, consistent with the adopted Sultana Community Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities;
3. The proposed changes in zone district designations identified in the Alpaugh, East Orosi, London, Richgrove, and Sultana Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.



AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Alpaugh, East Oroshi, London, Richgrove, and Sultana Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Alpaugh Change of Zone No. PZC 17-004, East Oroshi Change of Zone No. PZC 17-005, London Change of Zone No. PZC 17-008, Richgrove Change of Zone No. PZC 17-011, Sultana Change of Zone No. PZC 17-012, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" Rezoning Plans consistent with the adopted Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Alpaugh Change of Zone No. PZC 17-004***

*Exhibit "A" – Zoning District Ordinance Map*

*Exhibit "B" – Zoning Map*

*Exhibit "C" – List of Affected Properties/Map*

***East Orosi Change of Zone No. PZC 17-005***

*Exhibit "D" – Zoning District Ordinance Map*

*Exhibit "E" – Zoning Map*

*Exhibit "F" – List of Affected Properties/Map*

***London Change of Zone No. PZC 17-008***

*Exhibit "G" – Zoning District Ordinance Map*

*Exhibit "H" – Zoning Map*

*Exhibit "I" – List of Affected Properties/Map*

***Richgrove Change of Zone No. PZC 17-011***

*Exhibit "J" – Zoning District Ordinance Map*

*Exhibit "K" – Zoning Map*

*Exhibit "L" – List of Affected Properties/Map*

***Sultana Change of Zone No. PZC 17-012***

*Exhibit "M" – Zoning District Ordinance Map*

*Exhibit "N" – Zoning Map*

*Exhibit "O" – List of Affected Properties/Map*

Attachment 2

***Community Plan Rezoning Full Ordinance***

*Exhibit A - Community Plan Rezoning Full Ordinance*

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF SUSTAINABLE  
COMMUNITY PLANS

)  
)

RESOLUTION NO. 9376

Resolution of the Planning Commission of the County of Tulare recommending the Board of Supervisors adopt and accept the Sustainable Community Plans. A major component of the Community planning process included a thorough examination and cataloguing of existing land uses which suggested a strong need for infrastructure improvements, land use changes and zoning changes, where feasible, in order to fulfill the goals of the General Plan and Senate Bill 244. The Unincorporated Community Infrastructure Maps provided the measurement tools needed to identify infrastructure solutions, appropriate improvement projects, and planning policy analysis. The resulting objective of the proposed Community Plans is aimed at reducing greenhouse gas emissions, promoting equity, providing economic stability; and thus, enhancing sustainability, as well as protecting the environment, and promoting healthy and safe communities. The Plans necessitated and ensured methods of public participation with a wide variety of stakeholders.

WHEREAS, The Budget Act of 2010/11 authorized the Department of Conservation (Department) to implement and administer \$63.15M in project funds (Proposition 84, the Safe Drinking Water, Water Quality and Supply, Flood Control, River and Coastal Protection Bond Act of 2006) to provide Planning Grants and Incentives awarded by the Strategic Growth Council (Council). The primary goal of this grant program is to develop and implement plans consistent with Chapter 13, Division 43, Public Resources Code, Sections 75127 and 75128; and

WHEREAS, The County received a grant from the Strategic Growth Council in 2014, Grant No. 2014-631, to study the feasibility of integrating infrastructure and land use, with the needs of rural unincorporated communities based on the Communities within Tulare County that received a top 10% ranking utilizing the *CalEnviroScreen* methodology and in fulfillment of the General Plan's Housing Element and SB 244 Plan (2014); and

WHEREAS, *"The work upon which this publication is based was funded in whole or in part through a grant awarded by the Council."* Preparation and processing of the Sustainable Community Plans took an estimated 36 months; and

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County

Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and

WHEREAS, the Sustainable Community Plans included all tasks and deliverables specified in the work plan including project development and meetings, data gathering, public workshops, plan development, public hearings, and grant reporting and administration; and

WHEREAS, the Sustainable Community Plans include strategies that reduce energy consumption, conserve water, improve air and water quality, and provide other community benefits; and

WHEREAS, staff has performed necessary investigations, prepared a written report, and recommended modifications and approval of these proposed Plans; and

WHEREAS, staff has produced the Final Report document attached as "Exhibit A" pending further review by the Planning Commission and the public, and will make all necessary administrative changes subject to this review and prior to the final submittal to the Board of Supervisors; and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. Recommend that the Board of Supervisors find that the said Sustainable Community Plans meet the Goals and Objectives of the Council in fulfillment of the Grant requirements as specified in the work plan.

The foregoing resolution was adopted upon motion of Commissioner Gong, seconded by Commissioner Aguilar, at a regular meeting of the Planning Commission on the 8th day of November, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A- Strategic Growth Council Final Report

Appendix B – Board of Supervisors  
Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

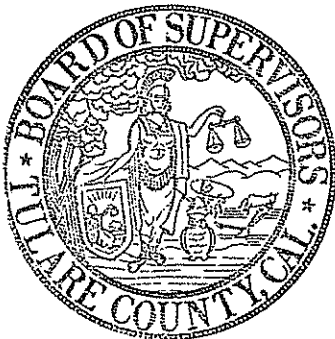
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and



3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **East Oroshi Community Plan**

# EAST OROSI COMMUNITY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# EAST OROSI COMMUNITY PLAN

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# EAST OROSI COMMUNITY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

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# EAST OROSI COMMUNITY PLAN

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## East Orosi Community Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9371, 9372, 9373, 9334, 9375, and 9376

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-042  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-041

East Orosi Community Plan: GPA 17-034  
Zoning District Map: PZC 17-005



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

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Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# EAST OROSI COMMUNITY PLAN

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# EAST OROSI COMMUNITY PLAN

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# EAST OROSI COMMUNITY PLAN

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# EAST OROSI COMMUNITY PLAN

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## EXECUTIVE SUMMARY

### INTRODUCTION

The primary objective in preparing a Community Plan for East Oroshi is to develop a plan which can accurately reflect the needs and priorities of this community. East Oroshi is currently designated as a Community in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (drinking water system improvements [such as wells, water distribution piping, and storage tanks], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

### Location

East Oroshi is a Census-Designated Place located in the northwest portion of Tulare County. It is generally bounded by Avenue 416 in the south, Avenue 420 in the north, Alta East Branch Canal in the west, and Road 142 in the east and encompasses 0.2 square miles of land. It is not directly served by any State Route. **(See Figure 1).** East Oroshi is in Section 09 & 16 Township 16 South, Range 25 East, MDB&M, and can be found within Orange Cove West United States Geological Survey 7.5 minute topographic quadrangle. East Oroshi is located at an elevation of 394 feet National Geodetic Vertical Datum. The coordinates of East Oroshi are: Latitude: 36.548005 and Longitude: -119.26067.

### Planning Area

The East Oroshi Urban Development Boundary (UDB) area consists of 179.2 acres **(See Figure 2)**. The Land Uses within the UDB are Mixed Use Activities that occupy 156.1 acres. The remaining 12% are lands dedicated for Right-of-way **(see Table 1)**.

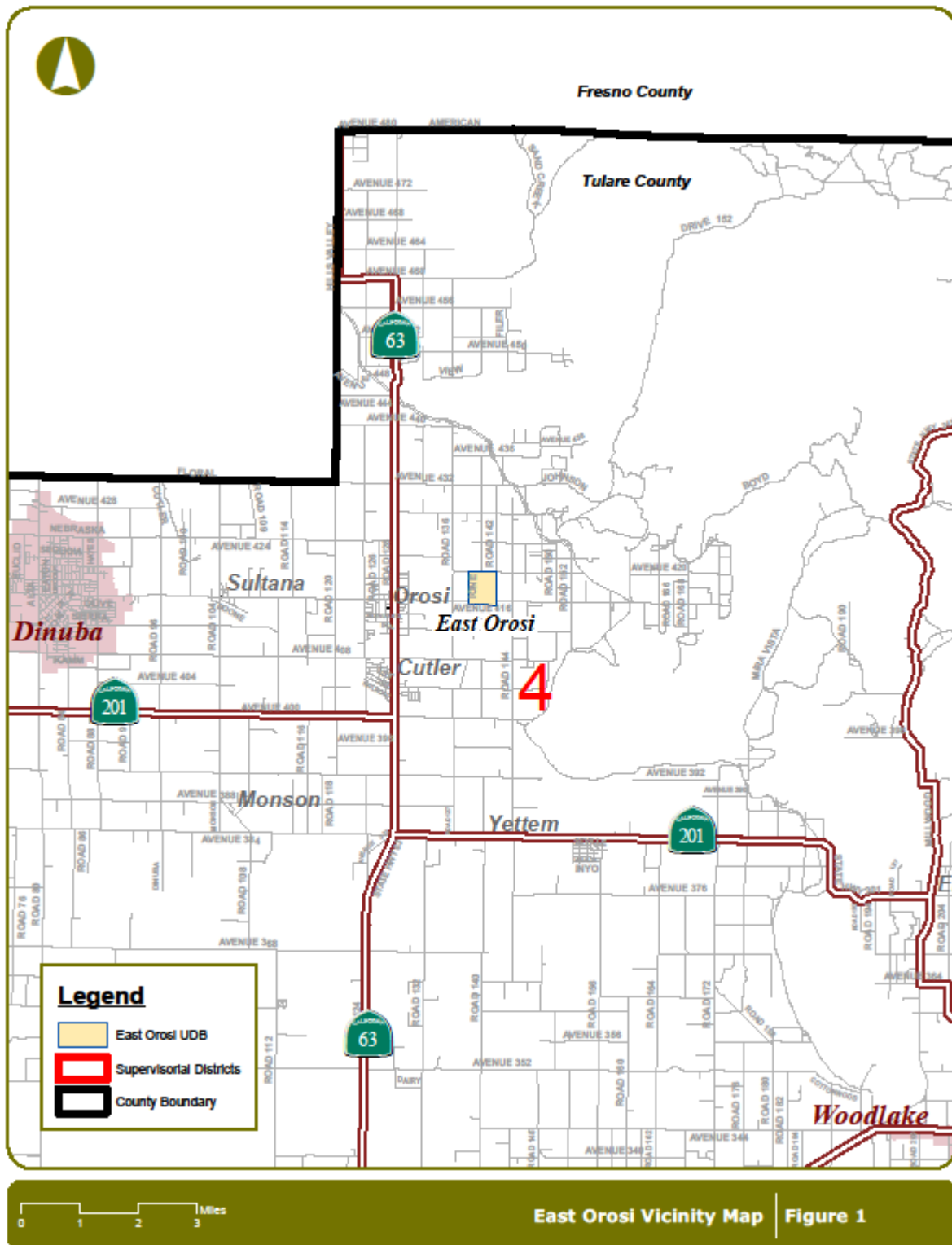
No change is proposed to the Urban Development Boundary.

Table 1 - East Oroshi Land Use	
Land Use	Acres
Mixed Use	156.1
Unclassified (Right-of-Way)	23.1
<b>TOTAL</b>	<b>179.2</b>

Source: Tulare County GIS

# EAST OROSI COMMUNITY PLAN

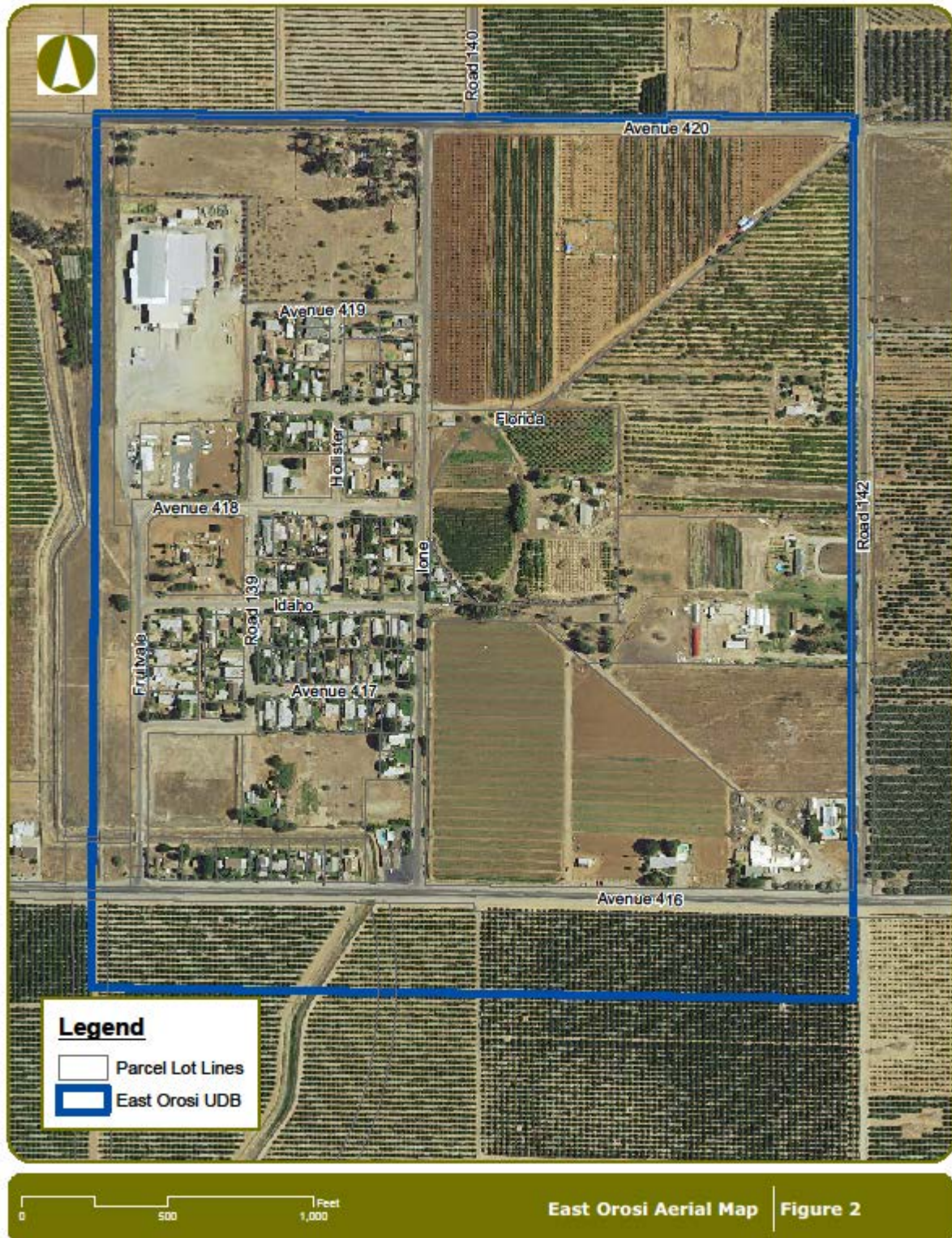
Figure 1 - Vicinity Map





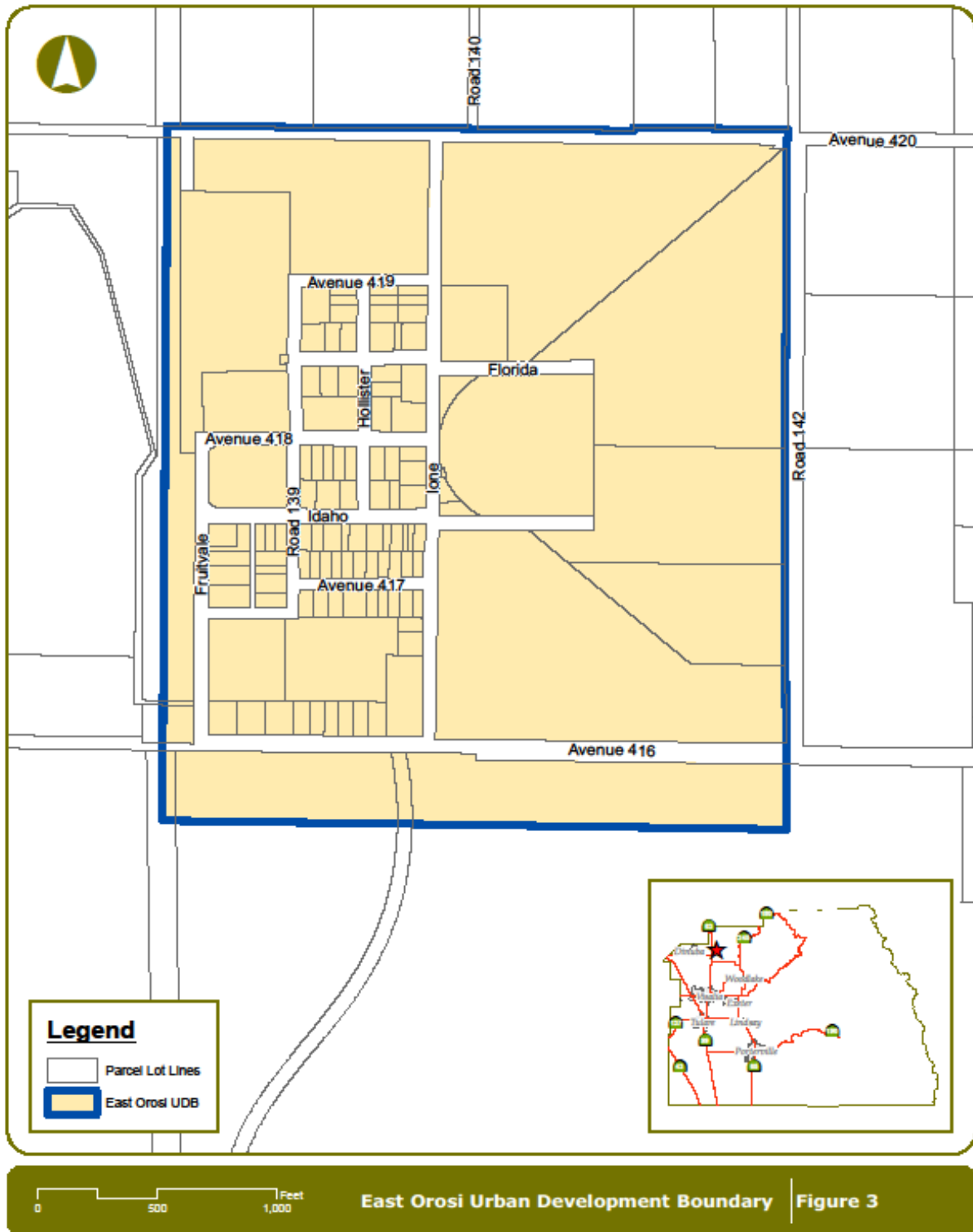
# EAST OROSI COMMUNITY PLAN

Figure 2 – Aerial East Orosi



# EAST OROSI COMMUNITY PLAN

Figure 3 - East Orosi UDB



# EAST OROSI COMMUNITY PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Outreach and Invitation Process

In efforts to increase awareness of the SGC project, Self Help Enterprises and the County went throughout the communities of Yettem, Seville, East Orosi, Cutler and Orosi to distribute flyers; to residents, businesses, and schools in Cutler Orosi. During this process SHE and the County talked with residents from all these communities about the importance of participating in this process. SGC partnered with the School and members of the Community to send flyers home with every student.

### Community Outreach Steps

Self-Help Enterprises (SHE) and Tulare County staff performed various outreach methods to promote community participation and make residents aware of the Strategic Growth Council (SGC) project.

- 1.) An informational flyer was developed.
- 2.) During 3 outreach meetings, County Staff and SHE explained the goals of the project and emphasized the importance and the community benefits of participating in the SGC community meeting.

### Community Meeting

Tulare County RMA held a SGC community meeting (with two SHE staff members present) in East Orosi on September 8 and December 15, at the Cutler Orosi Elementary 12623 Avenue 416 at 5:00 pm. This meeting location is centrally located, and residents feel welcome and comfortable there. The purpose of these meetings was to discuss East Orosi improvement needs, gather community data, and report the findings of the meeting in a final report. 10 residents attended the first meeting, three attended, the second meeting, three completed surveys were collected in total.

### Community Survey

A survey was developed to gather a variety of community information about multiple topics including Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality/Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey. After careful discussion at the East Orosi SGC community input meeting, residents concluded that the following is a list of their priority improvement needs.

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# EAST OROSI COMMUNITY PLAN

Self Help Enterprises and Tulare County  
invites you to a community meeting to review

## **E. Orosi, Seville, Yettem's Community Plan**

**Thursday, December 15, 2016 at 5:00pm at  
Cutler/Orosi School District Office  
12623 Ave 416 Orosi CA 93647**

A few months ago Tulare County and Self Help Enterprises came to your community to seek input on a planning analysis of your community. Surveys were distributed and collected. Tulare County invites you to attend a community meeting to see the findings from the study. A thorough examination and inventory of existing land uses will suggest infrastructure improvements, land use changes and possibly zoning changes, where feasible.

For more information, call: Abigail at 559-802-1659 or Kyria 559-624-7154

The work upon which this publication is based was funded in whole or in part through a grant awarded by the California Strategic Growth Council.

**Disclaimer:** The statements and conclusions of this report are those of the GRANTEE and/or subcontractor and not necessarily those of the California Strategic Growth Council of the California Department of Conservation, or its employees. The California Strategic Growth Council and the California Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text



Self Help Enterprises y El Condado de Tulare  
le invita a la Junta Comunitaria para revisar

**El Plan Comunitario de E. Orosi. Seville, Yettem  
Jueves 15 de diciembre del 2016 a las 5:00 de la tarde  
En la oficina del Distrito Escolar de Cutler/ Orosi  
12623 Ave 416 Orosi CA 93647**

Hace unos meses, el Condado de Tulare y Self Help Enterprises vino a su comunidad para buscar la opinión de la comunidad sobre un análisis de planificación de su comunidad. Encuestas fueron distribuidas y recolectadas. El Condado de Tulare le invita a asistir a una junta comunitaria para ver los resultados del estudio. Un examen completo y la organización de los usos existentes de la tierra sugerirán mejoras en la infraestructura, cambios en el uso de la tierra y posiblemente cambios de zonificación, donde sea factible. Para más información llame a: Abigail 559-802-1659 o Kyria 559-624-7154  
El trabajo sobre el que se basa esta publicación fue financiado en su totalidad o en parte, a través de una beca

# EAST OROSI COMMUNITY PLAN

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## **Survey Result:**

The results of the survey are as follows:

## **Transportation Data:**

- Community bus stops - 67% yes, 33% no
- Type of Transportation (work, home, shop) – 100% use cars
- Type of Transportation to take kids to school – 50% cars / 50% walk

Most trips in the community are by car (mostly to Dinuba for shopping, and other destinations for work). Half of all the parents drive or bus their kids to school, while the remaining portion walk. Even though a public Tulare County bus ridership is available, ridership is not part of commuting or transportation to school. Children from East Oroshi take the school bus to school and the school bus provides a large source of transportation to and from school for outlying areas.

## **Roads Data:**

- Sidewalks: 67% said yes
- Road maintenance satisfaction: 100% said no
- Conditions of roads: 67% reported poor, 33% reported fair
- Satisfaction with overall conditions: 100% reported no
- Road Flooding: 100% voted yes

From both meetings there was a sense that the road conditions were of importance to the Community. The roads around and in East Oroshi were in poor condition with some examples of where they were in fair shape. Overall, no one said they were satisfied with the road conditions in their community.

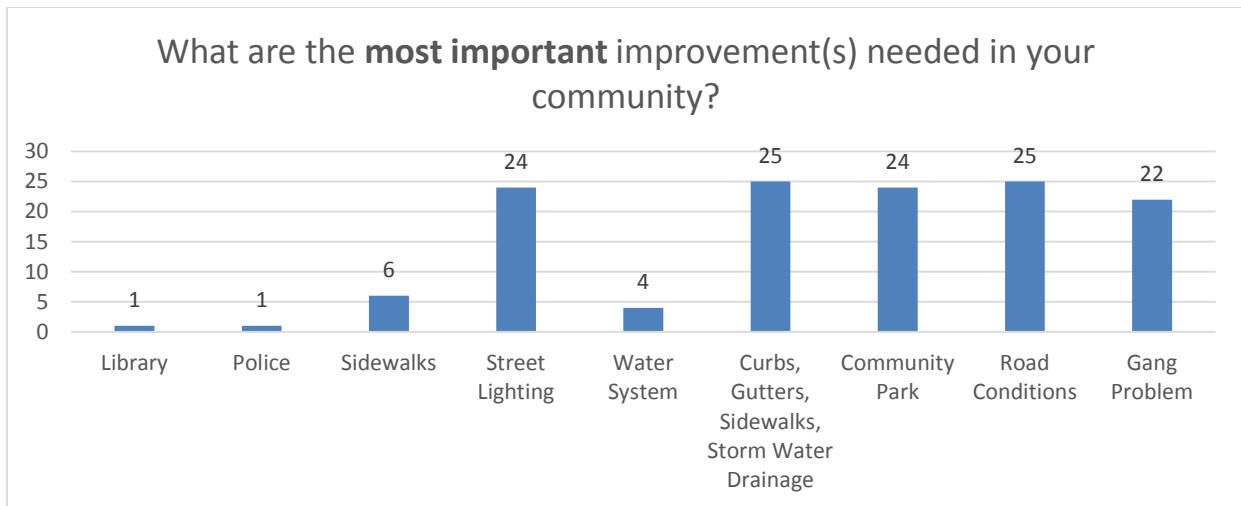
## **Water:**

- Safe Drinking Water: 33% said yes
- Bottled water: 100% said yes
- Septic Tank: 100% said no

Below is the list of what are the highest amount of concerns by issues (East Oroshi, Yettem, Seville, and Custer Oroshi) residents were in attendance:

1. Road Conditions,
2. Curbs gutters and sidewalks,
3. Street lighting,
4. Gang related problems.

# EAST OROSI COMMUNITY PLAN



Row Labels	Count of What is the MOST important improvement(s) needed?
Library	1
Police	1
Sidewalks	6
Street Lighting	24
Water System	4
Curbs, Gutters, Sidewalks, Storm Water Drainage	25
Community Park	24
Road Conditions	25
Gang Problem	22
<b>Grand Total</b>	<b>132</b>

*\*Residents submitted forms with their list of improvements after the meeting.*



County RMA Meeting in Cutler Orosi for Yettem, Seville, and East Orosi Sept. 8, 2016

# EAST OROSI COMMUNITY PLAN

## EAST OROSI TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

A significant part of planning is having information that describes the characteristics of a community's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a community plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Community allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project the amount of water and/or sewer service may be needed for a community. The following information provides a summary of the relevant demographic data needed to craft a plan that can realistically address the needs of smaller Community such as East Orosi.

#### Population

In 2015, the population of East Orosi was 572 (see Table 2).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>East Orosi</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	262	45.8%
Female	19,334,329	50.3%	226,607	49.9%	310	54.2%
Total	38,421,464		454,033		572	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project East Orosi's population during the Planning Period (see Table 3).

<b>Table 3 - Projected Annual Growth Rate</b>		
	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7



# EAST OROSI COMMUNITY PLAN

## Median Age

East Orosi's population is younger than the median age throughout all of Tulare County. East Orosi's median age of 21.3 is lower than the median age of the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
East Orosi	21.3

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 0.9% of East Orosi's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 10.5% was two races or more (see Table 4). Approximately 88.6% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community. (see Table 5)

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	East Orosi	%
Total	38,421,464		454,033		572	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	507	88.6%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	5	0.9%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	60	10.5%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in East Orosi

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

The 2010-2014 American Community Survey indicated that the unemployment rate for East Orosi was about 7.89% while the rate for Tulare County was 13.5%. The unemployment rate for the State of California was 10.1%. Keeping in mind that the 10.9% includes only the employable labor force (that is, not every person of the population) results in about 155 unemployed persons of East Orosi's person labor force of 832.

# EAST OROSI COMMUNITY PLAN

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>East Orosi</b>
Population 16 years & over	30,312,429	325,404	347
In labor force	19,269,449	194,420	251
Civilian labor force	19,137,441	194,102	251
Employed	17,246,360	170,780	224
Unemployed	1,891,081	23,322	27
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	96

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

East Orosi median household income was \$34,896, compared to \$61,818 for the State of California and \$42,031 for Tulare County. (see Table 7)

<b>Table 7 - 2011-2015 American Community Survey Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$30,318
<b>Tulare County</b>	\$42,031	\$44,814	\$17,876
<b>East Orosi CDP</b>	\$34,896	\$33,125	\$7,733

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, East Orosi's median household income was \$34,896, whereas the State of California's median household income was \$61,818. East Orosi's median household income was 56% of the State of California's median household income, and therefore it is considered a disadvantaged community.

# EAST OROSI COMMUNITY PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it is important to provide new housing opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

Table 8 - Housing Tenure			
	California	Tulare County	East Orosi
Occupied housing units	12,717,801	133,570	105
Owner-occupied	6,909,176	75,685	33
Renter-occupied	5,808,625	57,885	72

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	East Orosi
Owner-occupied	54.3%	56.7%	31.4%
Renter-occupied	45.7%	43.3%	68.6%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
East Orosi	7	14	3	6	19	37	8	16	8	16	51

(Source: Tulare County 2015 Housing Element)

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in East Orosi indicated that approximately 14% of the housing units were sound. Approximately 59% of East Orosi's housing units were deteriorated and 16 were dilapidated (see Table 10).

# EAST OROSI COMMUNITY PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	East Orosi
Total housing units	13,845,790	144,792	124
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	15
Built 1990 to 1999	1,495,571	21,767	18
Built 1980 to 1989	2,117,819	22,733	0
Built 1970 to 1979	2,503,688	27,111	19
Built 1960 to 1969	1,871,029	15,500	14
Built 1950 to 1959	1,907,512	13,694	0
Built 1940 to 1949	865,607	7,494	23
Built 1939 or earlier	1,298,438	8,186	35

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in East Orosi was 6.08 persons per household (see Table 12). The average owner occupied household size is 4.06.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
East Orosi	4.06	6.08

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in East Orosi was 17.2%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
East Orosi	0.0%	17.2%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# EAST OROSI COMMUNITY PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

A California Natural Diversity Database (CNDDDB) search conducted on November 8, 2016 indicates there are special status species within the Orange Cove West Quadrant Species List (which includes the East Orosi Planning Study Area) consisting of one animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975 (**see Figure 4**).

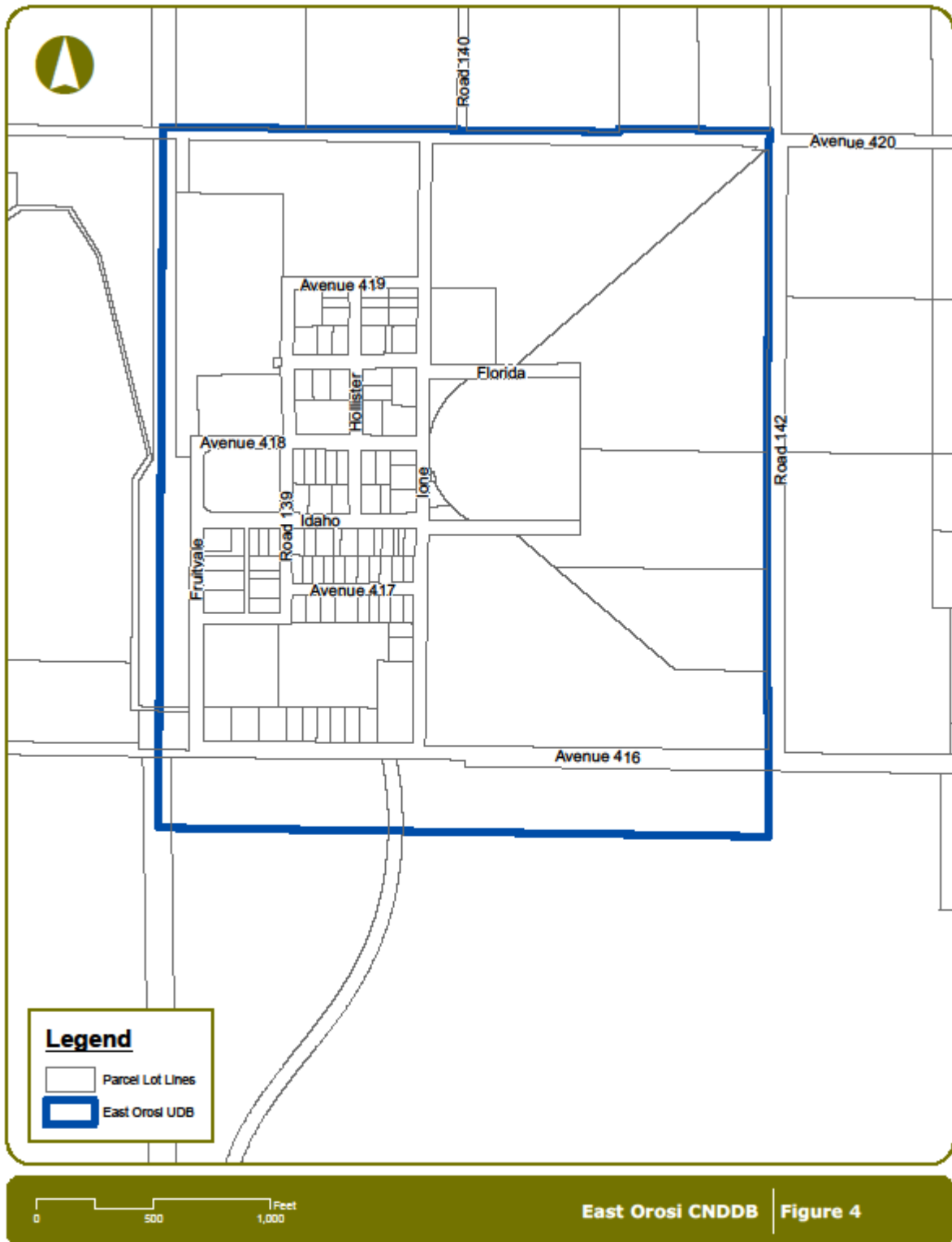
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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. November 8, 2016

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# EAST OROSI COMMUNITY PLAN

Figure 4 - CNDDb Map – Orange Cove West Quadrant



# EAST OROSI COMMUNITY PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect East Oroshi are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [East Oroshi] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal.

The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in East Oroshi (**see Figure 5**). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Urban Development Boundary (UDB). The following descriptions are provided for the above soil types:

**Calgro-Calgro, saline-Sodic, complex, 0 to 2 percent slopes,** Erosion potential is low to moderate and shrink swell potential low.

**Hanford sandy loam, 0 to 2 percent slopes,** is very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains and alluvial fans; negligible to medium runoff; moderately rapid and moderate permeability, however saline-sodic phases and soils with sandy clay loam substratums have moderately slow permeability. This soil is suitable for growing a wide range of fruits, vegetables, and general farm crops. They are also used for urban development and dairies.

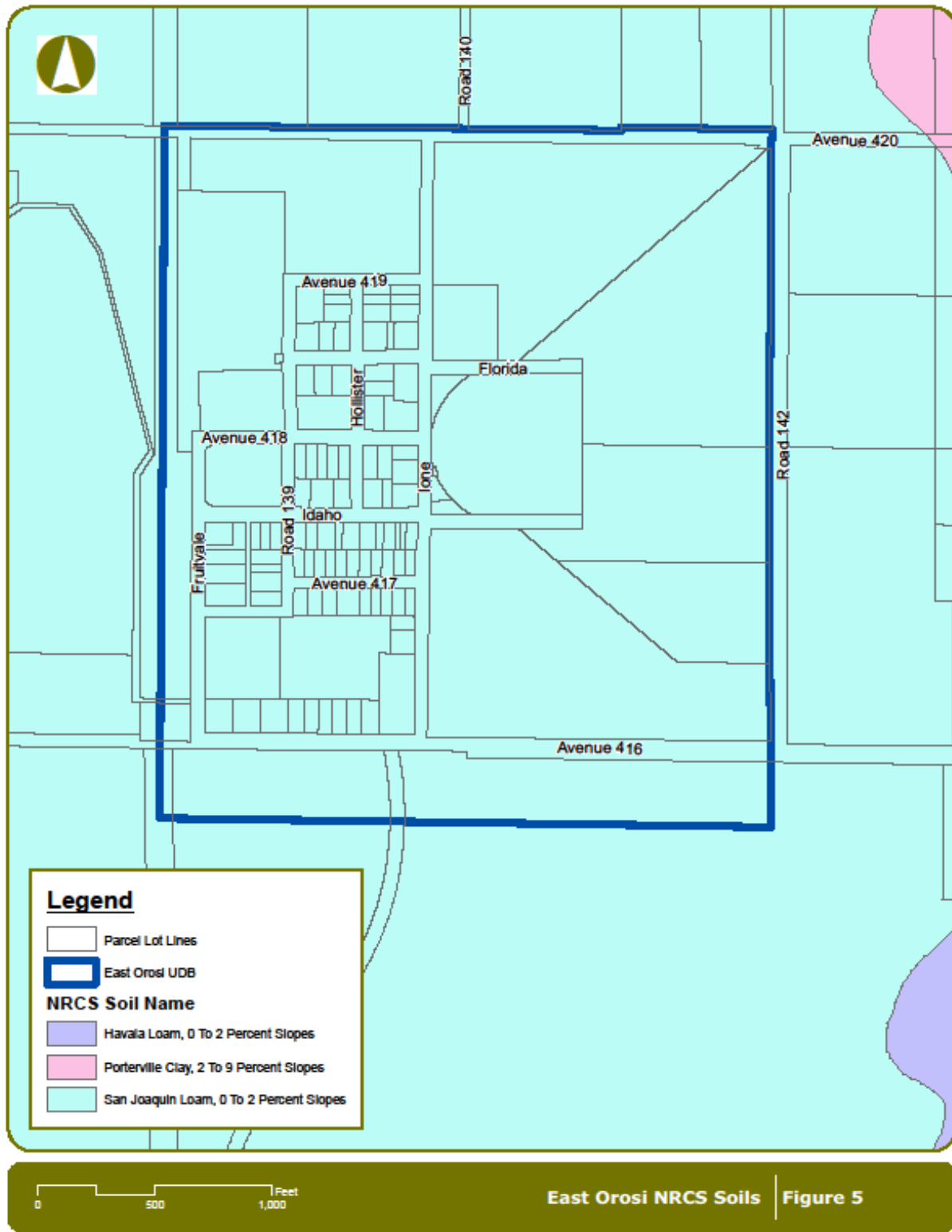
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database, accessed November 3, 2016.

# EAST OROSI COMMUNITY PLAN

Figure 5 - Soils Map - East Orosi





# EAST OROSI COMMUNITY PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The East Orosi Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies
- Adopt Local Air Quality Mitigation Fee Program
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations
- Fugitive dust regulation enforcement coordinated with SJVUAPCD
- Energy efficiency incentive programs
- Local alternative fuels programs
- Coordinate location of land uses to separate odor generators and sensitive receptors

There is a correlation between air quality and land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because East Orosi is located near the southwestern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of

# EAST OROSI COMMUNITY PLAN

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adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of East Oroshi including State Highway 201 and agricultural activities.

## Flooding<sup>6</sup>

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0345E, all portions of East Oroshi (**see Figure 6**) are located within Flood Zone X shaded and unshaded, Zones AE and AO as defined below.” Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

### **“B and X (shaded)**

Area of moderate flood hazard, usually the area between the limits of the 100-year and 500-year floods. B Zones are also used to designate base floodplains of lesser hazards, such as areas protected by levees from 100-year flood, or shallow flooding areas with average depths of less than one foot or drainage areas less than 1 square mile.

### **C and X (unshaded)**

Area of minimal flood hazard, usually depicted on FIRMs as above the 500-year flood level. Zone C may have ponding and local drainage problems that don't warrant a detailed study or designation as base floodplain. Zone X is the area determined to be outside the 500-year flood and protected by levee from 100- year flood.

### **AE**

The base floodplain where base flood elevations are provided. AE Zones are now used on new format FIRMs instead of A1-A30 Zones.

### **AO**

River or stream flood hazard areas, and areas with a 1% or greater chance of shallow flooding each year, usually in the form of sheet flow, with an average depth ranging from 1 to 3 feet. These areas have a 26% chance of flooding over the life of a 30-year mortgage. Average flood depths derived from detailed analyses are shown within these zones<sup>7</sup>.”

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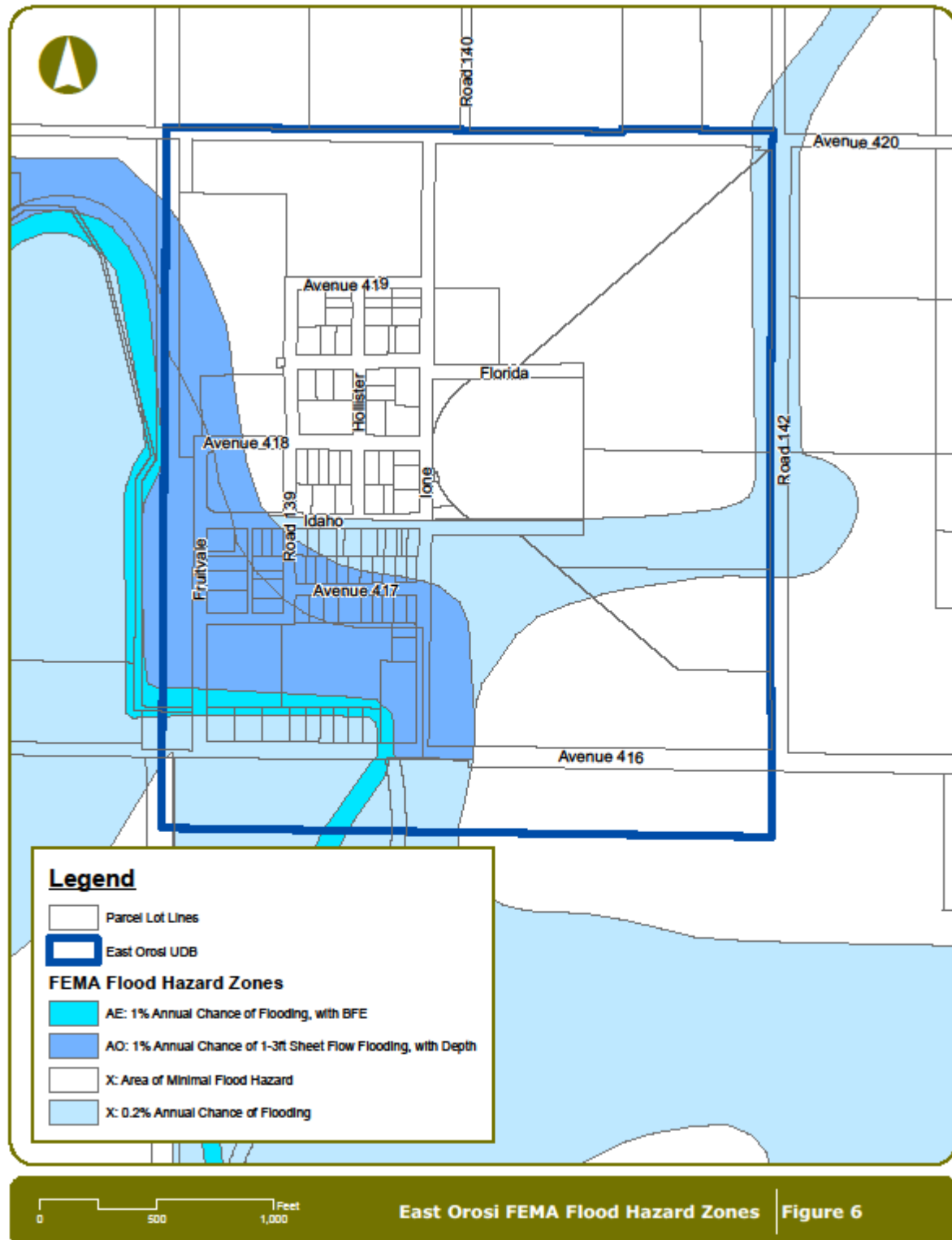
<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0605E, Panel Number 605 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>. Accessed December 13, 2016.

<sup>7</sup> FEMA Map Service Center:

<http://msc.fema.gov/webapp/wcs/stores/servlet/info?storeId=10001&catalogId=10001&langId=1&content=floodZones&title=FEMA%20Flood%20Zone%20Designations>

# EAST OROSI COMMUNITY PLAN

Figure 6 - FEMA Flood Map



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The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in Special Flood Hazard Areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

## Noise<sup>8</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan background Report East Orosi lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regard to East Orosi, this Community Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.).

## Energy: Natural Gas/Electricity

PG&E provide service to East Orosi.

## Water

The East Orosi Community Services District is located approximately .5 miles east of the community of Orosi. District boundaries are bordered by Avenue 416 to south, Ione Avenue to the east, Fruitvale Avenue to the west and the District's northern boundary is south of Avenue 420. The EOCS District boundaries encompass a 53-acre area while its LAFCO established Sphere of Influence (SOI) covers a 145-acre area. The District last amended its SOI in 1998 (LAFCO Resolution 98-016). The District's

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<sup>8</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# EAST OROSI COMMUNITY PLAN

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system is regulated by the Tulare County Environmental Health Services Division, which has been granted primacy by the California Department of Health Services. The division is responsible for the administration and enforcement of the Safe Drinking Water Act involving systems in Tulare County with less than 200 connections.

The EOCSD was formed in December of 1954. Formation of the District was not subject to LAFCO approval. According to Board of Supervisors Resolution 54-2011, which authorized formation of the District, the EOCSD was authorized to provide water for domestic use, irrigation, sanitation, industrial use, fire protection, and recreational use upon formation. The EOCSD was providing water supplies for the aforementioned uses prior to January 1, 2006; thus, the District does not possess any latent power whose activation is subject to LAFCO Commission approval or disapproval. Sewer service is provided under contract by the Cutler Public Utilities District (PUD). The Cutler PUD also provides service to Orosi PUD and Sultana CSD customers and the CSA No. 1 Yettem and Seville Zones of Benefit.

## Water System

Domestic water and sewer service in East Orosi is provided by the East Orosi Community Service District (CSD). **Table 14** shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Housing Element, May 2012). Maps of the sewer and water systems are currently unavailable.

According to the Municipal Service Review 2011 (MSR), the CSD community water system relies entirely on groundwater supplies pumped from the Kings River Sub Basin. The system consists of 2 drilled wells, which use 7.5 horse power (hp) submersible pumps to funnel water through a single check valve and into 2 corresponding pressure storage tanks. The distribution system further contains galvanized 4 inch mains and 1 inch laterals. Well 01, located at the eastern end of the District, acts as the primary source of water during the months of October through March. Well 02, located at the District's western boundary, acts as the primary source of water April through September. CSD's water system contains no method of treatment such as coagulation and flocculation, sedimentation, filtration or disinfection.

The District's Sanitary Survey conducted in January of 2011 by Tulare Environmental Health found that the CSD's source and distribution facility are capable of providing a reliable water supply and recommended that a water supply permit be issued subject to an ongoing water quality monitoring schedule, clearing of vegetation near Wells 01 and 02 and near their respective storage tanks and replacement of Well 01 vent pipe screen with fine mesh screen.

The CSD's December 2010 sample test results for bacteriological contaminates (required each month) resulted in a single positive sample for total Coliforms, one absent sample, and 3 positive repeat samples. A notice of violation was rendered to the CSD of the positive results. The notice directed the CSD management to provide the legally required notice to district customers advising them of this total Coliform violation. Proof of customer notice must be submitted to Tulare Environmental Health. The CSD's Environmental Health File does not contain proof of customers notice. Lead and copper samples (required annually) must be collected in the months of June, July, August and September only; thus, no results are available for 2011.

# EAST OROSI COMMUNITY PLAN

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Chemical sample test results, which determine Nitrate levels, are required to be submitted on an annual basis; however, once in violation, community water system operators must submit test results to Tulare County Environmental Health on a quarterly basis. In addition, water system operators must notify customers of the violation and submit proof of notice to Tulare Environmental Health. Records indicate May 2009 sample test results showed Nitrate levels exceeding the maximum level contaminants (MCL) allowed by law. A December 10, 2009 notice of violation for failure to provide quarterly sample test results was provided by Tulare Environmental Health requesting that management submit chemical Nitrate testing results for both Wells 01 and 02. A July 20, 2010 letter provided by Tulare Environmental Health indicates that the District was in violation of the aforementioned proof of notice requirement in the first 2 quarters of 2010; this notice seems to stem from the May 2009 Nitrate MCL violation.

A July 20, 2010 notice of violation once again indicates sample test results exceeded Nitrate MCL and a subsequent January 27, 2011 notice of violation for failure to provide quarterly sample test results was also provided. Proof of customer notice for this specific violation was not found.

The California Safe Drinking Water Act requires each public water system operators to prepare a Consumer Confidence Report (CCR) on an annual basis and mail/deliver a copy to each customer by July 1 of the year following the year for which the CCR is prepared. Proof of CCR distribution must be provided to Tulare Environmental Health. The CCR contains a key defining the terms used in the report, list of common contaminants found in drinking water, tables listing raw sample test results followed by a brief description of common contaminant sources. The abovementioned Nitrate violations were not identified in the 2006-2009 CCRs. A July 15, 2010 notice of violation provided by Tulare Environmental Health to the CSD indicates that the CSD failed to provide proof that a CCR was prepared and distributed for the 2009 calendar year. A similar notice was also submitted on July 15, 2009 for the 2008 CCR. Proof of 2008 and 2009 distribution was found in the CSD's file, which signifies that these CCRs were not provided to customers in a timely manner.

A Compliance Order provided by Tulare Environmental Health, dated April 15, 2010, cites the following CSD violations of law: system operating a well that produces water not in compliance with primary drinking water standards (H & S Code Section 116555 (a) (1)), failure to ensure a pure, wholesome, healthful and potable supply of water (H & S 116555 (a) (3) and Nitrate levels exceeding the MCL allowed by law (CCR 64431 (a)). The order requests the CSD provide a plan to address the violations, complete with timeline, and sets forth compliance requirements, including the aforementioned quarterly submittal of chemical sample test results and notices of violation to District customers on a quarterly basis.

A January 2008 Tulare Environmental Health notice advises the District to continue to adhere to all reporting requirements, sustain efforts to address nitrate violations, and continue to provide customer notice requirements so long as violations continue. This notice indicates that the District has been in violation of Nitrate MCLs allowed by law since at least 2008.

In accordance with the State's Safe Drinking Water Act, each water supplier must have a certified operator on staff. A Tulare Environmental Health notice of violation indicates that as of 12-9-2009, the CSD is in violation of this provision and does not have a certified operator on staff.



# EAST OROSI COMMUNITY PLAN

CCR's were prepared for the years 2006, 2007, 2008 and 2009 (no further CCRs were found in the CSD Environmental Health file). Sample data is provided, but no explanation is provided regarding what raw data means.

The CSD office consists of a mobile home that sits on land donated by a local property owner. Both day-to-day operations and district public meetings are conducted in the mobile home. It is estimated that the mobile home can only accommodate approximately 5 people at one time.

Based on the records examined, it is determined that the CSD water system is chronically in violation of maximum Nitrate levels allowed by law. It is further determined, based on the multiple notices of violation for failure to provide sample test results, CCRs, and customer notices of violation, that it is very likely system customers are not even aware of the serious contamination issues facing their water system. Without being properly informed, district customers cannot safeguard against the health hazards posed by water contamination thereby putting their health and safety at risk.

Staffs determinations are further substantiated by a series of news reports that have recently examined potable water quality in small Tulare County communities, East Orosi included. A Fresno Bee article published March 16, 2011 details a recent study conducted by the Oakland think tank, Pacific Institute. The study found that it would cost approximately \$150 million to address Valley-wide water contamination issues. The study also determined that low-income residents living within communities served by small water systems use approximately 4.6% of their income for water supplies (this includes both system user fees and bottled water); the federal standard for affordability is 1.5%. The study further found that regulatory agencies do not adequately inform customers when system contamination does occur. A news report that aired on KPMH Fox 26, a local Fox affiliate, also examined the issue of poor water quality within the Valley's small unincorporated communities. In timely fashion, the news report focused on the community of East Orosi. A CSD customer interviewed explained that she has been dealing with high Nitrate levels in her water since 2002 and must purchase bottled water for drinking and cooking, an expense that drastically drains her financial resources. Another CSD customer interviewed explained that there is no alternative for water used to shower and that system water commonly causes rashes and severe discomfort.

It is determined that a mobile home only able to accommodate 5 people at one time is an inadequate facility in which to hold public meetings, particularly for a district containing 386 customers.

It is also determined that the scenario described above, in which the District's exclusive reliance on outside funding sources creates an undue economic burden on district customers and/or exposes them to severe health risks, seems to already be taking place. State and federal grants/loans only offer short-term solutions and do not address the larger structural forces behind continual service/infrastructure needs and deficiencies. It is evident that a new approach must be pursued. Consolidation of the CSD with the various CSD's and Public Utilities Districts (PUDs) in the Cutler-Orosi region is a logical and highly feasible option.

**Table 14 -Existing Water and Wastewater Connections in East Orosi**

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
108	108	0	108	108	0

(Data current as of May 2012)



# EAST OROSI COMMUNITY PLAN

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## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration

East Orosi does not currently have a storm drainage system.”<sup>9</sup>

## Solid Waste

Solid waste disposal services for the Community of East Orosi is provided by Pena’s Disposal, a private company. Solid waste generated in East Orosi can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia.

## **PUBLIC SERVICES**

### Sheriff

Police protection services are provided in East Orosi by the Tulare County Sheriff’s Department substation, located at 40765 Road 128, in Cutler, approximately 20 miles northeast of East Orosi. Orosi Substation covering 289 square miles and serving a rural population to include the unincorporated communities of Cutler, New East Orosi, Orosi, Seville, Sultana, Traver and Yettem. The Substation runs a four shift operation which includes 23 deputies, four sergeants and one lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. In addition to that general shift staffing the communities of Cutler, Orosi and New East Orosi/Traver have assigned Community Based Officers assigned specifically to those area. The substation is open for walk-ins from 8 am to 5 pm Monday thru Friday. After hours and weekends there is a phone provided outside the substation that calls directly into the dispatch center. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff’s Office in Visalia, CA.

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<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# EAST OROSI COMMUNITY PLAN

## Fire

Fire protection and emergency medical services are provided for East Oroshi by the Tulare County Fire Department. The Tulare County Fire Station #4, is located approximately five miles west of East Oroshi at 40779 Road 128, Cutler- Oroshi Fire Station. Cutler- Oroshi Fire Station has Patrol 4, Engine, and Engine 204 are assigned to this location. Paid On-Call Fire Fighters are assigned to this station and they respond when called or tuned out to an incident. Response time is approximately five (5) minutes from this station to East Oroshi.

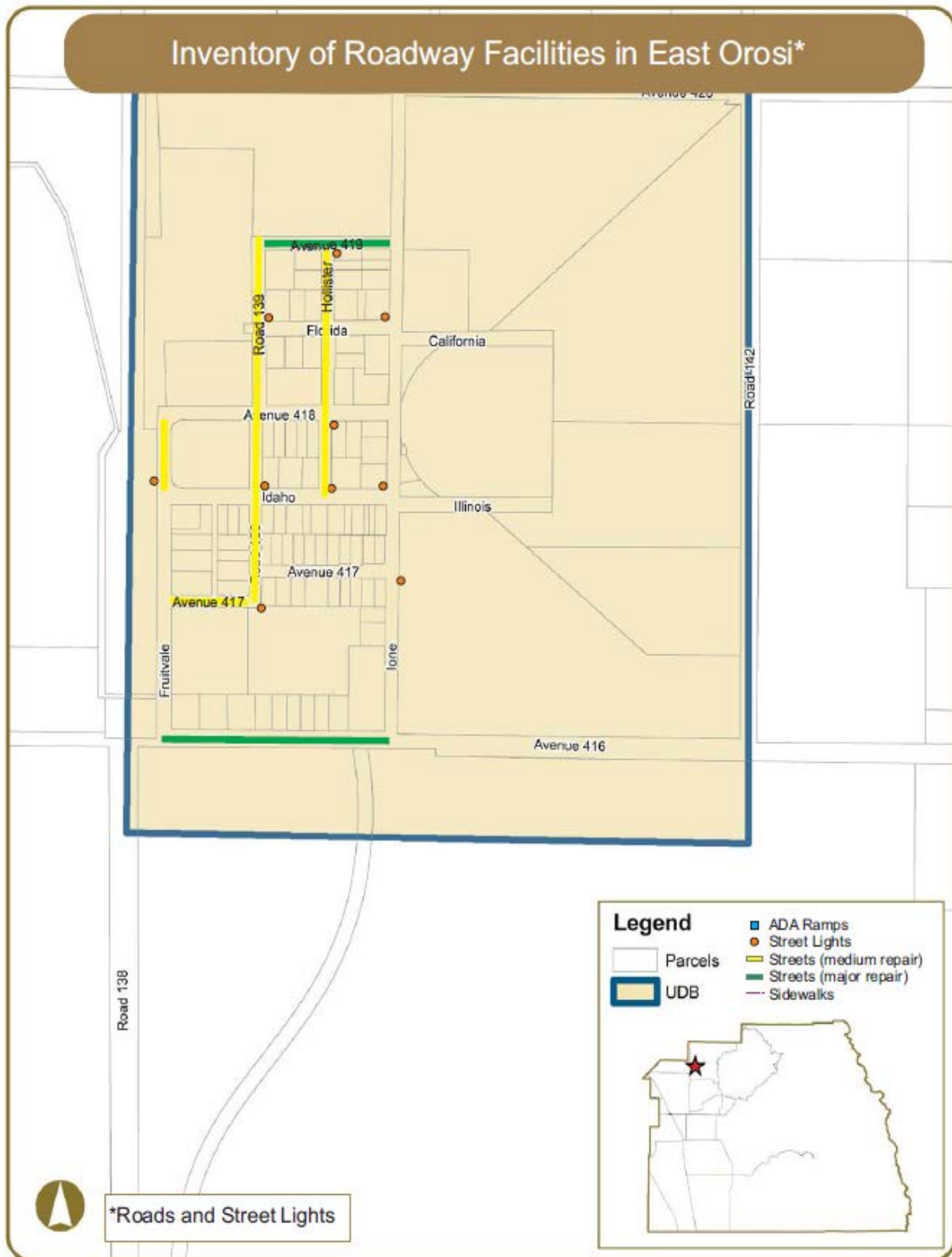
Eighteen fire hydrants are found within East Oroshi (see Table 15). These fire hydrants are located within the County right-of-way. Figure 7 displays Existing Fire Hydrants in East Oroshi.

**Table 15 - Location of Existing Fire Hydrants**

Existing Fire Hydrants	
No.	Location
1	Avenue 419 and Road 139
2	Avenue 419 east of Hollister Road
3	Ione Road north of Florida Avenue
4	Florida Avenue and Hollister Road
5	Road 139 south of Florida Avenue
6	Hollister Road south Florida Avenue
7	Ione Road south of Florida Avenue
8	Avenue 418 and Road 139
9	Road 139 south of Avenue 418
10	Avenue 418 east of Road 138
11	Idaho Avenue east of Road 138
12	Idaho Avenue east of Road 139
13	Ione Road north of Avenue 417
14	Avenue 417 and Road 139
15	Road 138 north of Avenue 417
16	Road 138 north of Avenue 416
17	Avenue 416 west of Ione Road
18	Avenue 416 west of Ione Road

# EAST OROSI COMMUNITY PLAN

Figure 7 - Inventory of Fire Infrastructure in East Orosi



# EAST OROSI COMMUNITY PLAN

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## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The East Orosi Community Plan Area is within the Cutler-Orosi Unified School District which provides K-12 grade education.

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 15 regional libraries and one main branch”<sup>10</sup> (see Table 20)

Table 16 - Library Location & Hours		
Branch	Address	Service Hours (2017)
East Orosi	Orosi Branch 12646 Avenue 416 Orosi, CA 93647	Wednesday, Thursday, and Friday 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 6:00 p.m.

Library hours current as of September 2017

## Parks

In the community of East Orosi there is no safe green space for children to play and adults to be physically active. There are no County owned/operated parks in East Orosi. The nearest County owned/operated park are the Ledbetter Park in the unincorporated community of Cutler approximately 12.5 miles west of East Orosi.

# THE EAST OROSI COMMUNITY PLAN

## CIRCULATION/COMPLETE STREETS/TRANSPORTATION

### Existing Circulation

East Orosi is a small agricultural community located near Avenue 416. The major rural collector roads is Avenue 308. There are no proposed major streets as part of this Community Plan (see Figure 8).

### Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

### Traffic

The 2014 Regional Transportation Plan and projections for Year 2040 traffic volumes do not exceed the LOS “C” threshold. Less than significant impacts will occur to Avenue 416, as a result of the Project primarily because, and the Tulare County General Plan (TCGP) policies and limited growth rates under the RTP, will maintain LOS at B or C through Year 2040 projections, while the build-out time frame of the Community Plan is Year 2030.

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<sup>10</sup> General Plan Background Report, page 7-96



# EAST OROSI COMMUNITY PLAN

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The proposed Circulation Element and Complete Street Policies of the General Plan contain policies and actions that will continue to maintain LOS C or better. The Plan's projected growth and its impact on the circulation system will continue to be less than significant as the additional goals and policies are implemented through the Community Plan. These policies and actions will be implemented by the County of Tulare over a 20-year time period.

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Roads

“There are several roadways in East Orosi that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.

# EAST OROSI COMMUNITY PLAN

- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt,
- ✓
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>11</sup>

**Table 17** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 9** graphically displays this information on a map.

**Table 17 - Road Maintenance Strategies**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 416	Fruitvale Road to Ione Road	OLAY
2	Avenue 417	Fruitvale Road to Road 139	CHIP
3	Avenue 419	Road 139 to Ione Road	GRX
4	Fruitvale Road	Idaho Avenue to Avenue 418	CHIP
5	Hollister Road	Idaho Avenue to Avenue 419	CHIP
6	Road 139	Avenue 417 west to Avenue 419	CHIP

OLAY – overlay resurfacing operation  
CHIP – chip seal  
Reconstruction  
GRX – grind and remix

ACST – asphalt reconstruction  
RCST – cold mix reconstruction

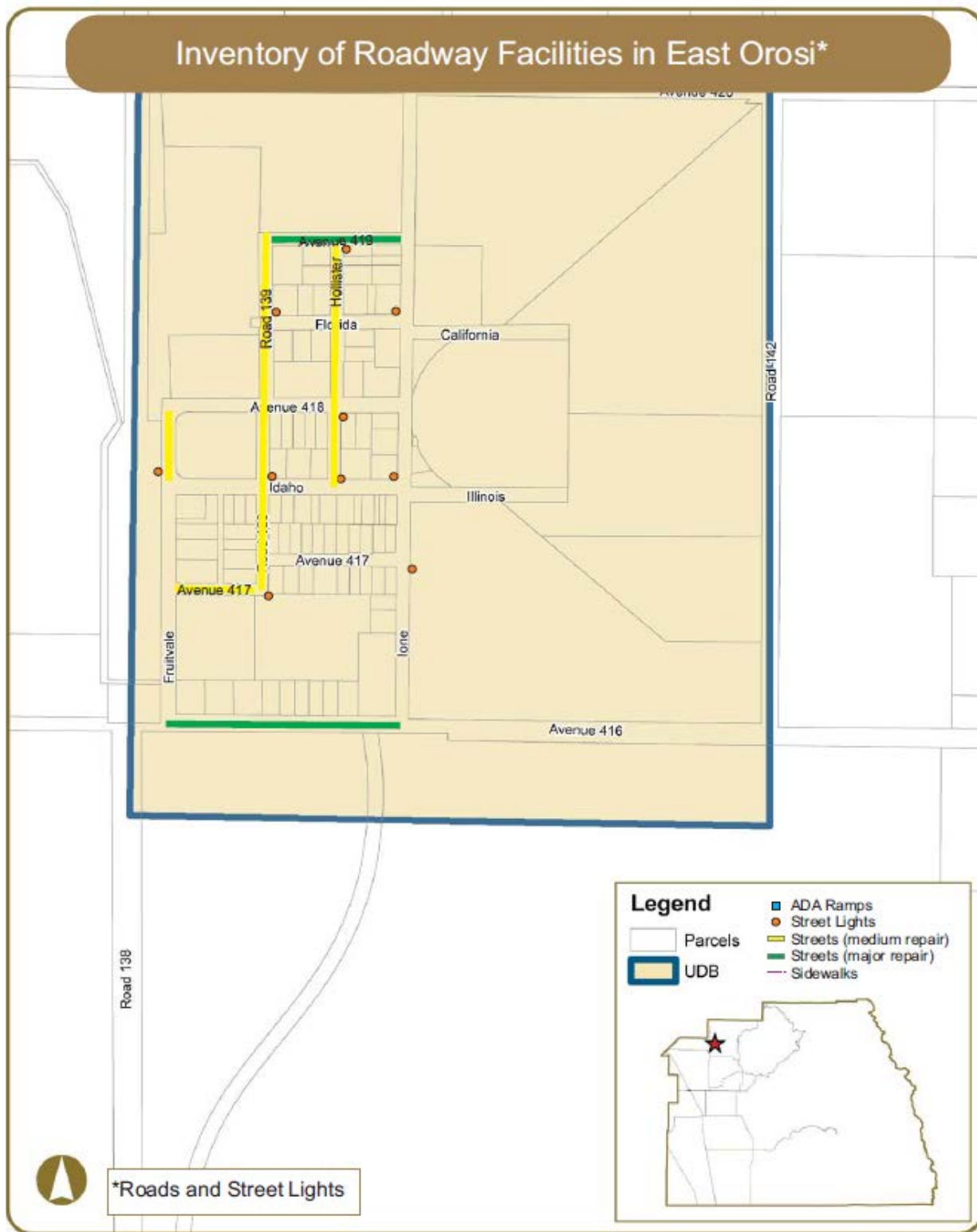
(Source: County of Tulare Public Works, 2012)

<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element



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Figure 9 - Inventory of Roadway Facilities in East Orosi



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## Street Lights

Lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

**Table 18** identifies the location of existing street lights that are maintained by Tulare County, in East Orosi, as well as their specifications. **Figure 9** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type "W" represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete.<sup>12</sup>

**Table 18 - Existing Street Lights**

Specifications of Existing Street Lights								
No	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 417	Road 139	SE Corner	1612	5800	W	W	PG&E
2	Avenue 417	Ione Road	SE Corner	1622 (3176)	5800	W	E	PG&E
3	Avenue 418	Hollister Road	SE Corner	1614	5800	W	S/W	PG&E
4	Avenue 419	Hollister Road	SE Corner	1616	5800	W	W	PG&E
5	Florida Avenue	Ione Road	NW Corner	N/A	5800	W	E	PG&E
6	Florida Avenue	Road 139	NE Corner	1626	5800	W	S	PG&E
7	Idaho Avenue	Hollister Road	NE Corner	2380	5800	W	S	PG&E
8	Idaho Avenue	Fruitvale Road	NW Corner	1619	5800	W	E	PG&E
9	Idaho Avenue	Road 139	NE Corner	1621	5800	W	S	PG&E
10	Idaho Avenue	Ione Road	NW Corner	1622	5800	W	E	PG&E

## Sidewalks

There are currently no sidewalks located within East Orosi. Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

"The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp

<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element

# EAST OROSI COMMUNITY PLAN

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itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within East Orosi.”<sup>13</sup>

## Complete Streets

The Complete Streets Programs Policies, Objectives, and Standards are hereby incorporated by reference. Included in the plan are policies and implementation measures as provided. The physical plan includes a bicycle network and connected pedestrian travel system incorporating complete safe routes to school network **(see Attachment A-4: East Orosi Complete Streets Program)**.

### **Improvement Plans:**

- 1 Avenue 416 (between SR 63 and Road 140, this part is covered by the Orosi Complete Street policy)
- 2 Avenue 418 (Road 139 to Road 140)
- 3 Ione/Road 140 (Avenue 416 to Avenue 419)
- 4 Avenue 419 (Road 139 to Road 140)

The Board of Supervisors on February 28, 2017, approved inclusion of the Complete Streets Program as part of the Circulation Element of the Community Plan **(see Attachment A-4)**. The Complete Streets Programs Policies, Objectives, and Standards are incorporated by reference **(see Attachment A-4)**

## Complete Streets Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified Road 63 as a proposed Class III Bike Project<sup>14</sup> In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is Road 63.

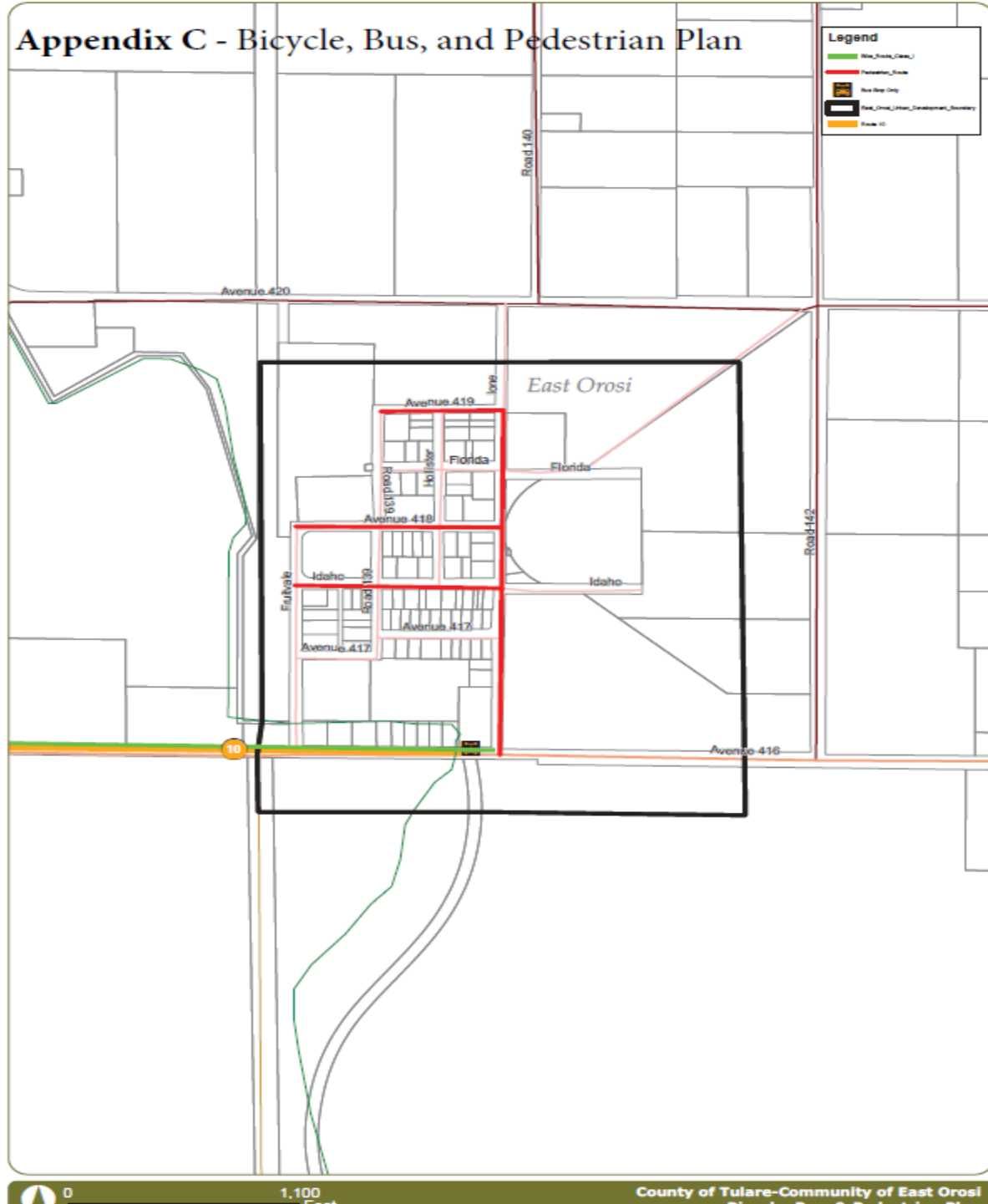
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<sup>13</sup> Action Program 9, Tulare County 2015 Housing Element

<sup>14</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# EAST OROSI COMMUNITY PLAN

Figure 10 - Bicycle, Bus, and Pedestrian Plan



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## Complete Streets Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

## Multituse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. There is currently no multiuse trails in the East Orosi Community.

## Transit and Bus Stops

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. East Orosi is currently connected via TCAT Route 10 to Dinuba and its transit center (**see Figure 10**). [www.ridetcat.org](http://www.ridetcat.org) TCAT Route 10 provides service to Dinuba, Seville, Yettem, Sultana, Orosi, East Orosi, and Visalia four times per day on weekdays and four times on Saturday. In Dinuba, transfers can be made to connect to Visalia, Tulare, and the remainder of the TCAT public transit system TCAT vehicles are wheelchair accessible and all full size buses include bike racks. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday.

Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in East Orosi, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

Figure 11 - TCAG Fixed Route Service - Route 10



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## State Route 99

State Route (SR) is the major highway between cities within the Central Valley and the primary route between the City of Fresno to the north and the City of Bakersfield to the south. East Orosi is located approximately 15 miles east of SR 99.

## AMTRAK

The Hanford AMTRAK station, located approximately 45 miles to the southwest in Kings County, is the closest station to East Orosi providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport is approximately 30 miles southwest. Fresno Yosemite International Airport (FAT), approximately 30 miles northwest of East Orosi, is a principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles southeast of East Orosi and offers direct flights to several destinations

# EAST OROSI COMMUNITY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the East Orosi Community Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Community Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for East Orosi to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

East Orosi Community Plan, and input received from East Orosi citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

##### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Create a mix of uses with easy access to major roads**

Objective: Promote mixed use zoning and land uses.

##### Policies:

1. Locate high density residential uses in close proximity to planned commercial areas.

2. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
3. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
4. The County shall work with the Schools to provide safe routes to school.
5. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the East Orosi Community Plan.
6. The East Orosi Community Plan should be reviewed every five years to determine if amendments are appropriate.
7. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.



# EAST OROSI COMMUNITY PLAN

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## Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

## Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in East Orosi.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

## Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.

2. Assure that the housing industry is made aware of residential development potentials in East Orosi.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

## Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## Economic Base

**GOAL I: Develop a strong and diversified economy.**

# EAST OROSI COMMUNITY PLAN

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Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

Policies:

1. Encourage the East Orosi to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## *Environmental Quality and Public Safety*

**GOAL I: Preserve and enhance the quality of life for present and future generation of East Orosi citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Protect Agricultural Lands:

Land within the respective East Orosi, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

# EAST OROSI COMMUNITY PLAN

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The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of East Orosi.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of East Orosi.

# EAST OROSI COMMUNITY PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 312-acre East Orosi Urban Development Boundary, agricultural activities, such as orchards and pasture, currently constitutes 70 percent (70 %) of the UDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2014 population of 572 by 30 urbanized acres (area within the UDB), which equals 20 persons per urbanized acre (**see Tables 19 and 20**) showing population projections). Projecting the population at a 1.3% growth rate adds 114 persons in year 2030. Multiplying the 114 persons by the 1/20 ratio (.05%) suggests an additional 6 acres of land will be required to accommodate development by the year 2030, if projections are realized.

### Population Growth Forecast

Table 19 - East Orosi Population Projections			
Growth Rate	2015	2020	2030
0.013	572	602	685

Table 20 - Population Projects		
Year	Population	Growth (%)
2016	572	0.013
2017	579	0.013
2018	587	0.013
2019	595	0.013
2020	602	0.013
2021	610	0.013
2022	618	0.013
2023	626	0.013
2024	634	0.013
2025	643	0.013
2026	651	0.013
2027	659	0.013
2028	668	0.013
2029	677	0.013
2030	685	0.013

### Demand Forecast

To determine whether there is enough land within the exiting UDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the UDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>15</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2013 the community had 105 dwelling units (including vacant dwellings) with a population of 572. At an annual growth rate of 1.3%, the projected housing units are 20 in year 2030. Given one existing commercial use and ag related industrial uses, the zoning plan is consistent with or greater than the community needs. As such, there is adequate land available for development within the existing UDB to accommodate future residential growth through the year 2030.

<sup>15</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

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## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, Cal State Bakersfield, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET. Based on the 2011-2015 American Community Survey (see Table

21), the educational barrier in East Orosi begins in grade school. Of the adults age 25 and older, 7.3% had an educational level of less than 9<sup>th</sup> grade. This lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment needs to begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

Table 21 - Education Attainment			
Educational Attainment	California	Tulare County	East Orosi
Population 25 years and over	25,257,858	260,964	247
Less than 9th grade	10.0%	20.8%	55.5%
9th to 12th grade, no diploma	8.2%	10.9%	7.3%
High school graduate (& equivalency)	20.7%	25.2%	28.3%
Some college, no degree	21.8%	22.3%	8.9%
Associate's degree	7.8%	7.0%	0.0%
Bachelor's degree	19.8%	9.5%	0.0%
Graduate or professional degree	11.6%	4.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates

### Health Care

Health care is important for economic development as businesses need healthy employees. There are no medical offices in East Orosi. The nearest medical offices are located to the west in the community of Orosi.

# EAST OROSI COMMUNITY PLAN

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Urban Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 22**) and Zoning Districts (**see Table 238**) have been updated.

<b>Table 22 - Proposed Land Use</b>	
Proposed Land Use	Acres
General Commercial	77.57
Heavy Industrial	206.80
High Density Residential	18.27
Light Industrial	90.02
Low Medium Density Residential	480.63
Medium Density Residential	102.04
Public/Quasi Public	98.69
Service Commercial	7.65
Urban Reserve-Residential	170.54
Right-of Ways	140.80
East Orosi UDB	1393.00

<b>Table 23 - Proposed Zoning</b>	
Proposed Zones	Acres
A-1	239.30
AE-10	48.91
C-2-MU	77.57
C-3	7.65
M-1	90.02
M-2	243.18
P-O	0.50
R-1	106.44
R-2	36.87
R-3	25.97
R-A	168.44
R-A-100	177.30
R-A-43	20.82
R-A-87	9.25
Right-of Ways	140.80
East Orosi UDB	1393.00

## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the East Orosi Community Plan. The following components comprise the East Orosi Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the East Orosi Community Plan, there are a variety of changes to existing zoning districts. These changes are described below.

Revise Chapter 16 of the Zoning Code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval

# EAST OROSI COMMUNITY PLAN

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process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

## Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of East Oroshi.

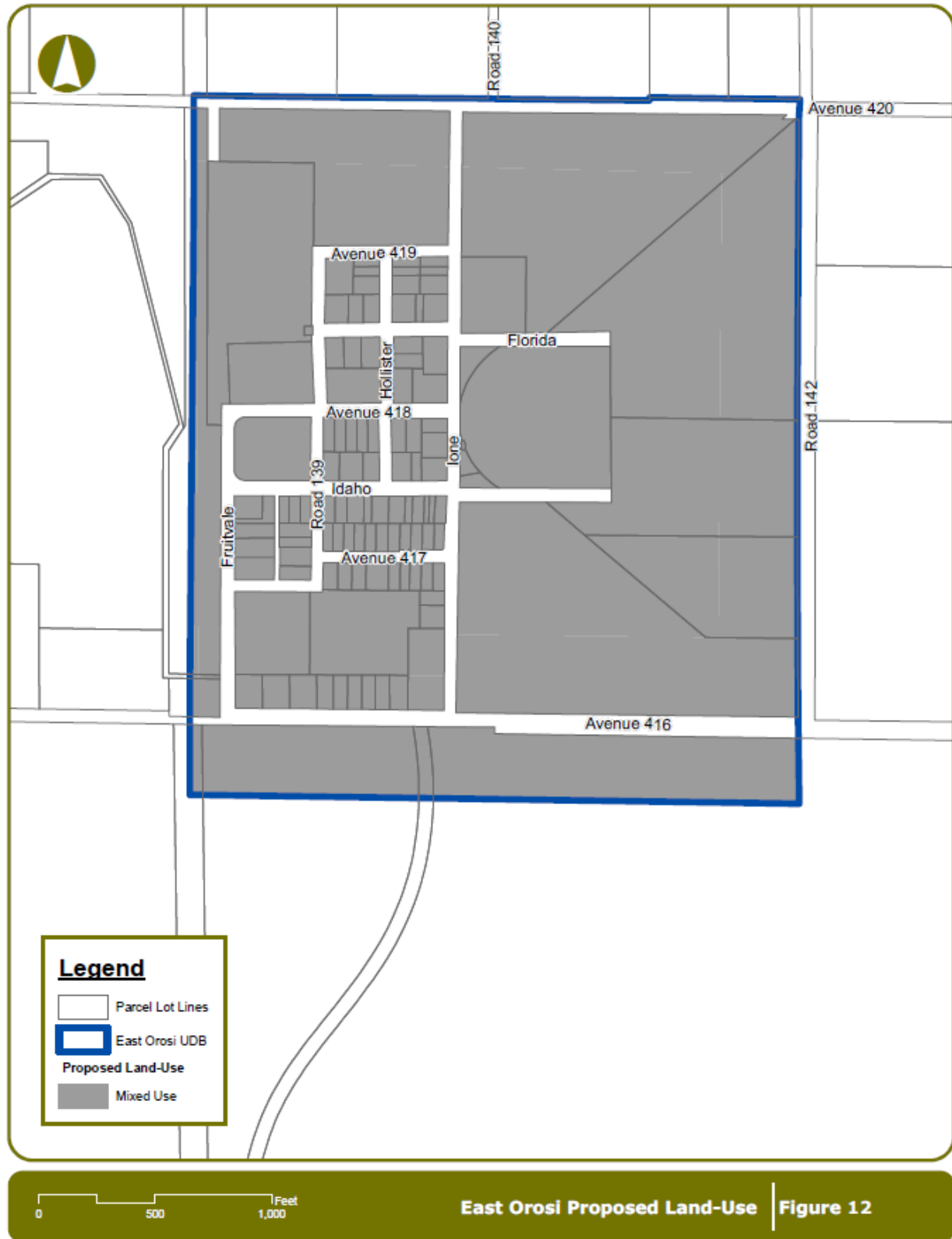
## Zoning Map Update

The current Zoning Map for East Oroshi (**see Figure 13**) will be amended to be compatible with the Land Use Map (**see Figure 12**) outlined in the General Plan. There are a couple of zoning district changes (**see Figure 14**) that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other East Oroshi Proposed Zoning (**see Figure 15**).



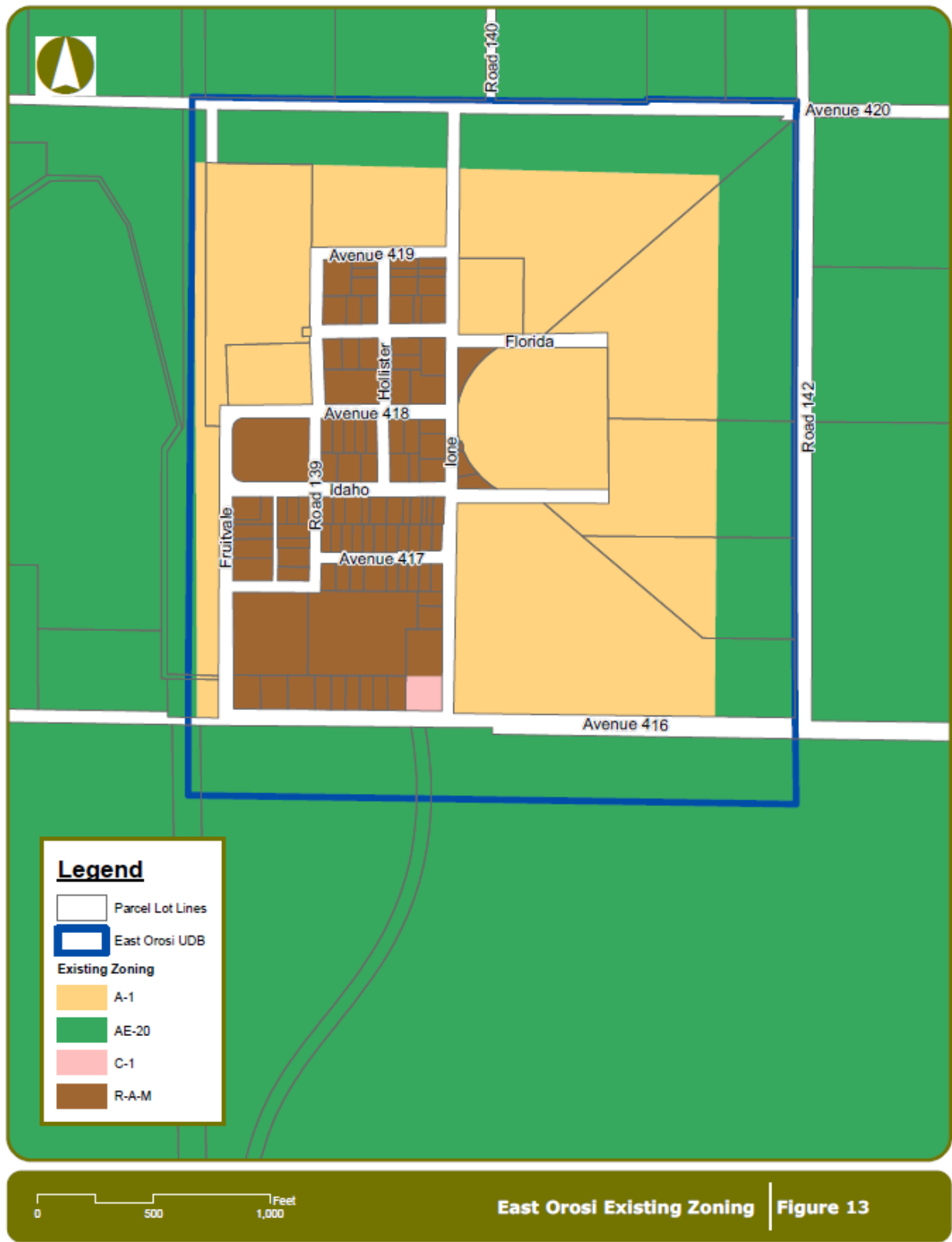
# EAST OROSI COMMUNITY PLAN

Figure 12 - Proposed Land Use Plan



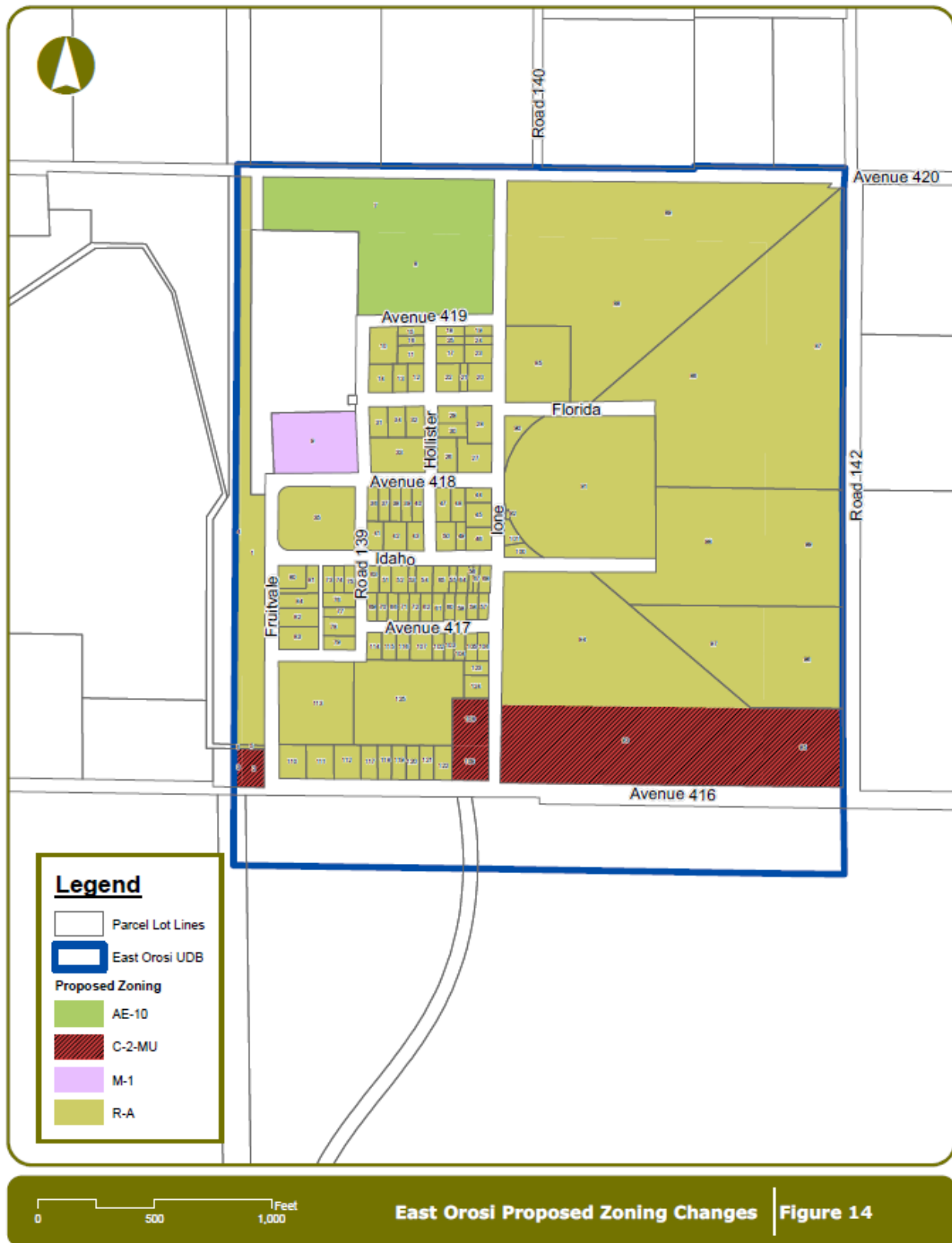
# EAST OROSI COMMUNITY PLAN

Figure 13 - Existing Zoning Plan



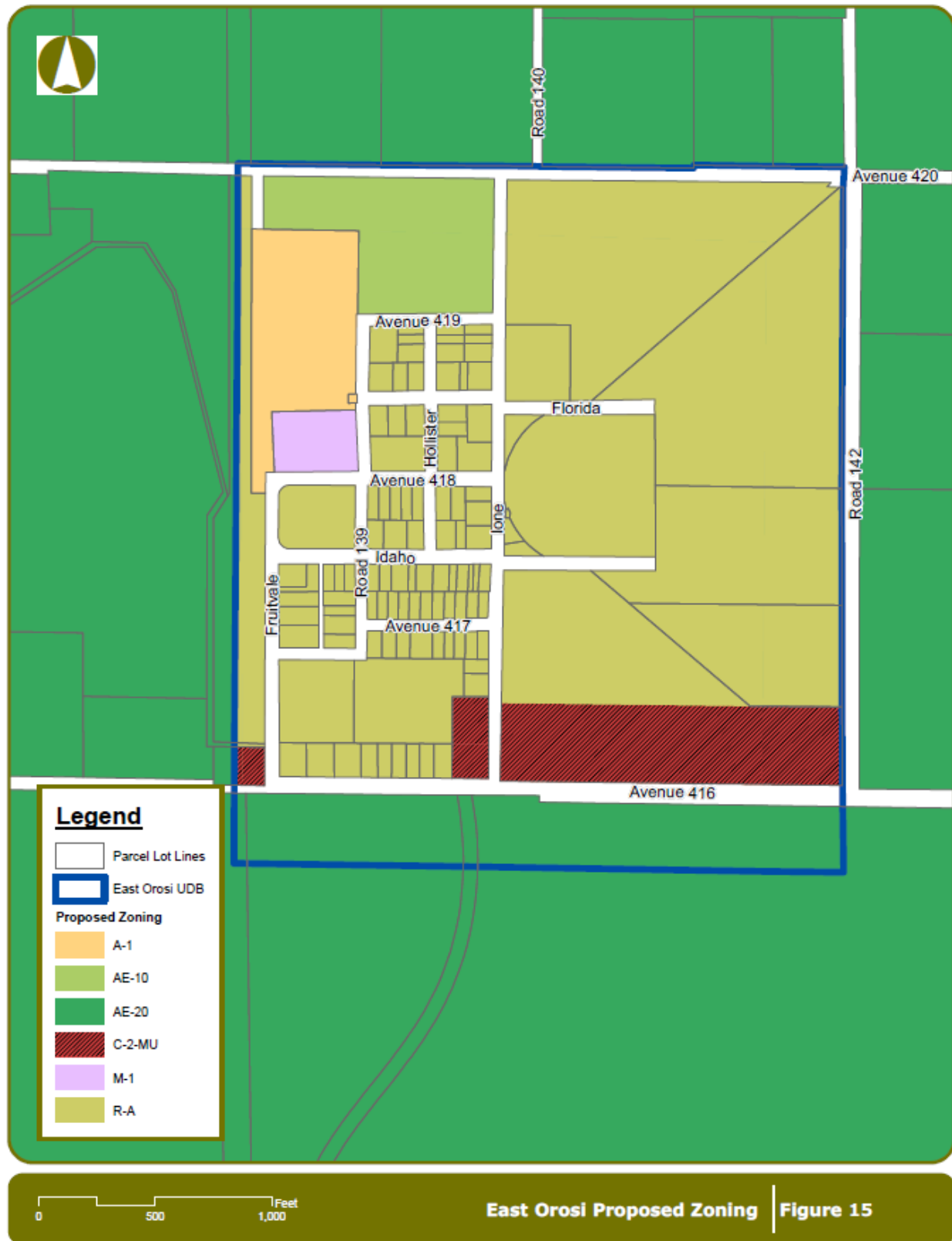
# EAST OROSI COMMUNITY PLAN

Figure 14 - Proposed Zoning Changes



# EAST OROSI COMMUNITY PLAN

Figure 15 - Proposed Zoning



# EAST OROSI COMMUNITY PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

A-4 – Complete Streets

# EAST OROSI COMMUNITY PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

# EAST OROSI COMMUNITY PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1



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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-	R-3, C-1, C-2, C-3,

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	1, R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles	C-2, C-3, M-1, M-2	

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of an airport).		
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1

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Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# EAST OROSI COMMUNITY PLAN

## A-2 *Mixed Use Overlay District (Zone Change Text)*

The following regulations shall apply in the community of East Oroshi, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of East Oroshi.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for East Oroshi.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# EAST OROSI COMMUNITY PLAN

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet



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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for East Oroshi. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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## A-3 *Development Standards (Mixed Use Zoning District)*

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To promote Economic Development within the East Orosi Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within East Orosi. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the East Orosi Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within East Orosi. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios,

and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

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A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public

areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

# EAST OROSI COMMUNITY PLAN

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RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## SITE PLANNING

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

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SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

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LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

# EAST OROSI COMMUNITY PLAN

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WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.



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SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

# EAST OROSI COMMUNITY PLAN

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

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## A-4 East Orosi Complete Streets Program



# Tulare County Complete Streets East Orosi

## Final



Prepared by:

**Tulare County Resource  
Management Agency**

Paid for by a grant from:



## **Tulare County Complete Streets – East Orosi**

**Prepared by:**

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**December, 2016**



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## Complete Streets Vision

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.”

The Tulare County Resource Management Agency (RMA) is committed to fully integrating modal options in its General Plan and various Community Plans within Tulare County. This includes supporting projects that enhance walking and bicycling infrastructure. Additionally, RMA will improve access to public transportation facilities and services. This includes supporting urban development patterns and Americans with Disabilities Act (ADA) infrastructure that allow for greater accessibility to transit stops and stations. Finally, RMA continues to improve safety for all users and encourages street connectivity to create a comprehensive, integrated and connected circulation network. This is particularly important for those who rely on transportation infrastructure to be physically active and for students who walk or bike to school.


Steady population growth in Tulare County has directly impacted transportation needs. In the past, many of Tulare County’s federal, state, and local funding sources were used to develop new or improved traffic signals, interchanges, provide more travel lanes and to maintain existing roadway facilities. Historically, these funding sources have run well short of what is needed. The typical roadway transportation project that adds capacity and infrastructure is insufficient given these conditions. The RMA must adhere to its vision, which is to “provide a safe, convenient and effective County transportation system that enhances mobility and air quality for residents and visitors.”

Recent RMA and RMA-supported projects have already fulfilled some of these desires. There are already expanded bus transit routes in the County and more are being constructed for implementation in the near future. New transit centers are being placed throughout the County and efforts are underway to add more bicycle lanes and routes. Recent planning studies are looking to improve roadway safety, pedestrian safety, and access management between roadways and building developments. These efforts are consistent with greenhouse gas (GHG) emissions reductions efforts to reduce vehicle miles travelled (VMT) set forth under SB 375.

Promoting Complete Streets projects can offer Tulare County the ability to reduce traffic congestion, improve air quality, and increase the quality of life of residents by providing safe, convenient, and comfortable routes for walking, bicycling, and public transportation. Integration of Complete Streets into Tulare County’s existing policies allows the potential to prevent chronic diseases, reduce motor vehicle related injury and deaths, improve environmental health, stimulate economic development, and ensure access of transportation options for all people in Tulare County.

## Complete Streets Definition

Complete Streets are roadways designed to safely and comfortably accommodate all users, regardless of age, ability or mode of transportation. Users include motorists, cyclists, pedestrians



and all vehicle types, including public transportation, emergency responders, and freight and delivery trucks among others. In addition to providing safety and access for all users, Complete Street design treatments take into account accommodations for disabled persons as required by the ADA. Design considerations for connectivity and access management are also taken into account for non-motorized users of the facility.

Implementation of Complete Street design treatments will be based on whether it connects the networks for all modes, whether it improves the functionality for all users, and whether it is appropriate given the surrounding context of the community. The final elements of a Complete Street roadway will be largely based on these factors. At a minimum, a Complete Street roadway includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

## Complete Streets Attributes

While every street cannot be designed to serve all users equally, there are opportunities to enhance service for all users while maintaining its principal transportation function. Complete Streets incorporate community values and support adjacent land uses while ensuring safety and mobility. Proper applications of Complete Streets concepts support sustainable growth and preservation of scenic, aesthetic and historic resources.


## Report Outcomes

As a part of the Circulation Element for the Community Plan Update, this Complete Streets Report (Implementation Work Plan) and the following Implementation and Policies Section achieved the following outcomes:

- (i) *Addressed* congestion, climate change and oil dependence by shifting to lower-carbon modes;
- (ii) *Improved* safety by addressing shoulders, sidewalks, better bus placement, traffic speed reduction, treatments for travelers with disabilities;
- (iii) *Created* “livable communities” by encouraging walking and bicycling for health, and by providing a safe walking and bicycling environment as an essential part of improving transportation movement and safety within the roadways studied.

These outcomes were achieved by the following:

- (a) *Included all users* namely, pedestrians, bicyclists, transit vehicles and users, and motorists. In drafting this report, all users were invited to comment on how the County could better serve the community. The implementation of complete streets directly shifts the emphasis to lower-carbon (using) modes of transportation. The shift from the gas using automobile to pedestrian and bicycle transport is achieved through the creation of sidewalks, improving sidewalks, and including bike lanes and/or bike routes for a wider range of people to use. The shift to transit is included in improving policies, programs and facilities in the operations of the County’s transit systems.
- (b) *Created a comprehensive integrated and connected network* that supports “livable communities” that promote a safe interwoven fabric are provide for by the Policies



Section using the transportation goals in the 2030 General Plan Circulation Element and by further defining complete streets network (see Appendix C).


- (c) *Emphasized flexibility* recognizing that all streets with these communities are different, and thus, balancing user needs. No one standard was applied to all streets and the street designs were adjusted to existing conditions, differing jurisdictions and the desires of the community.
- (d) *Considered both new and retrofit projects*, including design, planning, maintenance, and operation, for the entire right-of-way within these communities. In addition to the various sections discussed below Appendix A – D include plans that show the plans, designs, and existing and proposed maintenance plans and operations of the Complete Streets Plan.
- (e) *Used the latest and best design standards*. By using newer design standards as represented in the preliminary design plans versus the County's Roadway Standards the County is able to provide wider sidewalks and include such amenities as traffic calming measures (bulbouts).
- (f) *Conducted extensive public outreach* to ascertain the solutions that best fit within the context of these communities. The Community was consulted and provided final feedback on the preliminary designs.

## Conclusions and Future Funding Opportunities

The intended effect of identifying the outcomes and reaching the conclusions in this report is that future funding opportunities will be enhanced because the Community will be supported by fully updated Community Plans. The conclusion to the report includes the Circulation Element of the Community Plan including the policies, and plans. The other conclusion to the report includes preliminary design drawings.

Specifically, the funding sources that are found in the Funding Section will be pursued actively by Tulare County to complete the work identified in the studies include, but are not limited to, the following:

- **Active Transportation Program Funding**
- **Highway Safety Improvement Funds**
- **Federal Transportation Activity Program (TAP) Funds**
- **Federal Transit Funds**
- **Federal Communities Putting Prevention to Work Grant**
- **Federal Highway Administration Pedestrian Safety and Design**
- **Strategic Growth Council**

- 
- **Walkable and Livable Communities Institute**
  - **California's Local Public Health and Built Environment Program**
  - **State Cap and Trade Funding**

## **Complete Street Policies**

### **Complete Street Goals**

The purpose of the RMA Complete Streets Policy is to create a comprehensive and uniform Complete Streets vision and policy for Tulare County. This will allow the implementing entities to incorporate Complete Streets guidelines and standards into both development and redevelopment actions. The County's goals are:

- Tulare County's transportation network will be supported through a variety of feasible transportation choices, which allows for sustainable growth.
- The livability of neighborhoods and commercial centers located along the County's transportation corridors will be enhanced by a safe and inviting pedestrian environment.
- The design of multimodal roadway facilities will not compromise the needs of larger vehicles such as transit vehicles, fire trucks and freight delivery trucks.
- Inclusion of Complete Streets design elements will allow for design flexibility on different street functions and neighborhood contexts.
- Inclusion of Complete Streets design elements will improve the integration of land use and transportation, while encouraging economic revitalization through infrastructure improvements.

### **Complete Streets Objectives**

- To create an integrated and connected transportation network that supports transportation choices and sustainable growth.
- To ensure that all transportation modes are accommodated to the extent possible in all public roadway facilities in the County.
- To develop and use the latest design standards and guidelines in the design of Complete Streets.
- To provide flexibility in the implementation of this policy so that streets chosen for implementation of Complete Streets elements can be developed to fit within the context of their principal purpose and surroundings without compromising the safety of users and needs of larger vehicles.



## Complete Streets Policies

### Tulare County General Plan Policies

The Tulare County General Plan Update (2030) in complying with AB 1358 calls for 4 Complete Streets related principles including:

*Principle 1: County-wide Collaboration*

Support countywide transportation plans that provide choices in travel modes.

*Principle 2: Connectivity*

Emphasize connectivity among cities, communities, and hamlets to ensure County residents have access to jobs and services.

*Principle 3: Community Circulation*

Anticipate and provide transit, traffic, and roadway connections that support the interconnectivity of all communities.

*Principle 4: Pedestrian and Bicycle Facilities*


Plan for the development and expansion of pedestrian paths and bicycle facilities that provide residents with alternative modes of travel.

These principles are expressed mainly in following policies including:

- TC-1.6 Intermodal Connectivity
- TC-1.7 Intermodal Freight Villages
- TC-5.1 Bicycle/Pedestrian Trail System
- TC-5.2 Non-motorized Modes in Planning and Development

### Complete Street Policy Design Criteria

1. Tulare County promotes the incorporation of Complete Streets concepts and design standards in all appropriate new and retrofit County public streets (except State highways and freeways).
2. Tulare County will seek every opportunity to provide funding for the planning, design, and implementation of Complete Streets.
3. New Class I Multi-Use Paths should be a minimum of eight (8) feet wide.
4. New Class II Bike Lanes should be a minimum of five (5) feet wide.
5. New sidewalks should be a minimum of five (5) feet wide.
6. Bulb-outs should be considered in areas of higher speed (35 mph or greater) where sufficient turning radii for trucks is available or as determined by the County Engineer.

- 
7. As determined by the County Engineer, installation of posted speed limit vehicle activated traffic calming signs (VATCS) are encouraged in instances of high speed to promote safety.
  8. Transit shelters and benches are encouraged at all County transit stops if FTA grants are available.
  9. Street lighting and cross walk are encouraged to promote safety if considered feasible by the County Engineer.
  10. Design policies should be consistent with the Tulare County Improvement Standards; other references include existing design guides, such as those issued by Caltrans, AASHTO and the ADA Accessibility Guidelines.
  11. Public streets excluded from this policy include those where:
    - Complete streets concepts are in conflict with existing laws, codes, or ordinances.
    - Compliance with this policy would conflict with goals or physical conditions related to the unique aspects of the location.
  12. Exceptions from Complete Street Policies:
    - Accommodation is not necessary where non-motorized use is prohibited, such as freeways.
    - Cost of accommodation is excessively disproportionate to the need or probable use as determined by the County Engineer.
    - A documented absence of current or future need.


## **Complete Street Mobility Plan**

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” Essentially, this bill requires a circulation element to plan for multimodal transportation accommodating all modes of transportation where appropriate, including walking, biking, car travel, and transit. The current functional classification system plan is shown in Appendix B (Circulation Plan).

The Complete Streets Act also requires circulation elements to consider the multiple users of the transportation system, including children, adults, seniors, and the disabled. For further clarity, AB 1358 tasks the Governor’s Office of Planning and Research to release guidelines for compliance with this legislation by January 1, 2014. Implementation of complete streets principles should be tailored to the individual jurisdiction and the individual roadway. The Complete Streets Program for Tulare County focuses on a network-based approach that has been tailored to the needs of the Community of East Orosi. Another principle that is being applied is under SB 743, requiring a change to evaluating traffic using Vehicle Miles Traveled (VMT) versus Level of Service under CEQA analysis, and under AB 32 in reducing Green House Gasses.

*Complete Streets:* According to the National Complete Streets Coalition, complete streets are a means by which, “... planners and engineers (can) build road networks that are safer, more livable, and welcoming to everyone.... Instituting a complete streets policy ensures that transportation planners and engineers consistently design and operate the entire roadway with





all users in mind – including bicyclists, public transportation vehicles and riders, and pedestrians of all ages and abilities.”

*Network-Based Complete Streets:* Combines individual travel mode networks into one multimodal transportation system, integrating infrastructure where appropriate, ultimately ensuring that all users can safely and efficiently access their destination.

*Vehicle Miles Traveled (VMT):* Vehicle miles traveled is the metric that identifies the total distance traveled in a car per driver. VMT drives roadway needs (the more people who drive, the more capacity and maintenance are needed on the roadway system). Under the Tulare County Climate Action Plan, in reducing VMT greenhouse gas emissions are reduced, and the County has an overall target of reducing 6% of its greenhouse gas emissions through a reduction in VMT.

Community Plans adopt these principles, which are combined into the following mission statement:

*The Community Complete Streets Network comprises four types of facilities—vehicular, pedestrian, bicycle, and public transit. This complete streets approach will enable residents to choose which travel mode best suits them. It also will ensure that streets are designed with the users in mind—accommodating for businesses, children, the elderly, bicyclists, and transit users.*

## **Caltrans and Complete Streets**

Under Caltrans District Order 64-R1, Caltrans requires that a Complete Streets Implementation Action Plan be developed and implemented for Caltrans owned and maintained Streets. Their Implementation Action plan provides a background by which the Tulare County Completes Street Plan will be implemented.

### *TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS)*

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of East Orosi. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describe bicycle circulation patterns and Pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. The Cutler-Orosi Complete Streets Plan was prepared in 2014 and includes a Class 1 Facility along Avenue 416 through East Orosi. In addition, rail and goods movement is part of the Sustainable Communities Strategy, Regional Transportation Plan (RTP), in lieu of utilizing diesel powered freight trucks.

### *Tulare County Climate Action Plan (CAP)*

The Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% through a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets.

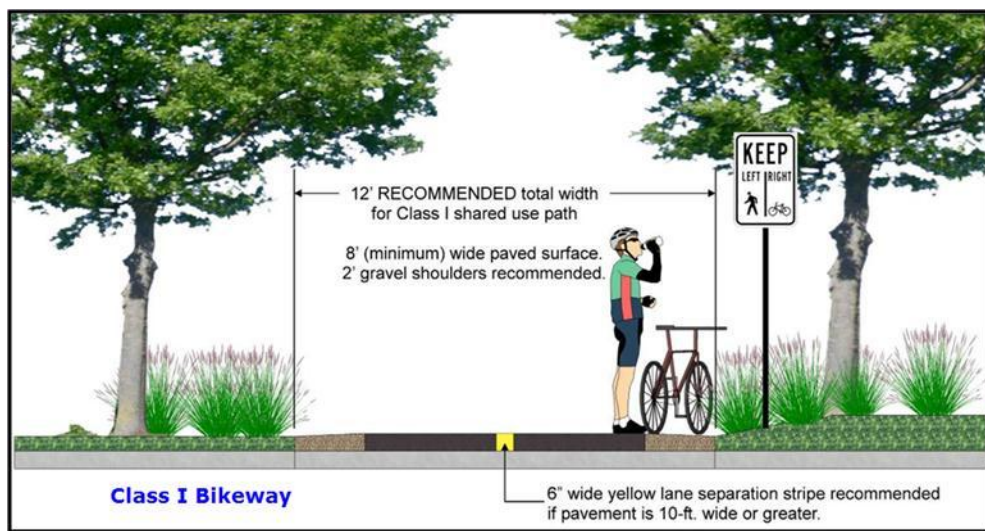


## Bicycle Facilities

Bicycle facilities consist of Class I, Class II, and Class III facilities as defined below. In Tulare County, this General Plan and the Bicycle Transportation Plan envision a system of bicycle lanes on roadways that will connect the activity centers of the communities to the residents. County has identified pedestrian corridors on the Community of East Orosi Bicycle, Bus and Pedestrian Plan (see Appendix C).

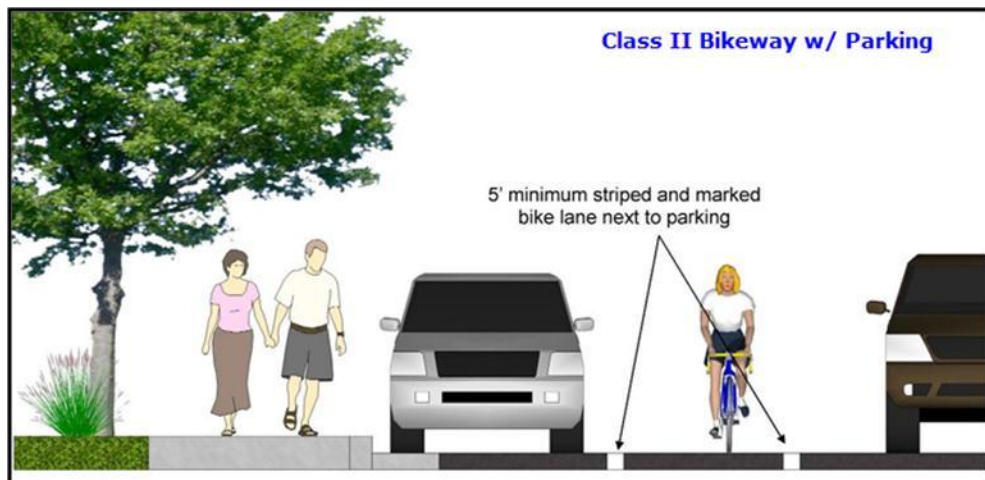
### *Class I*

Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County, Class I facilities will primarily be implemented through TCAG. Future bicycle facilities have also been identified through the *Bicycle Transportation Plan* (TCAG - 2010). The existing Cutler-Orosi Complete Streets Program calls for a Class I bicycle facilities along Avenue 416 in East Orosi.



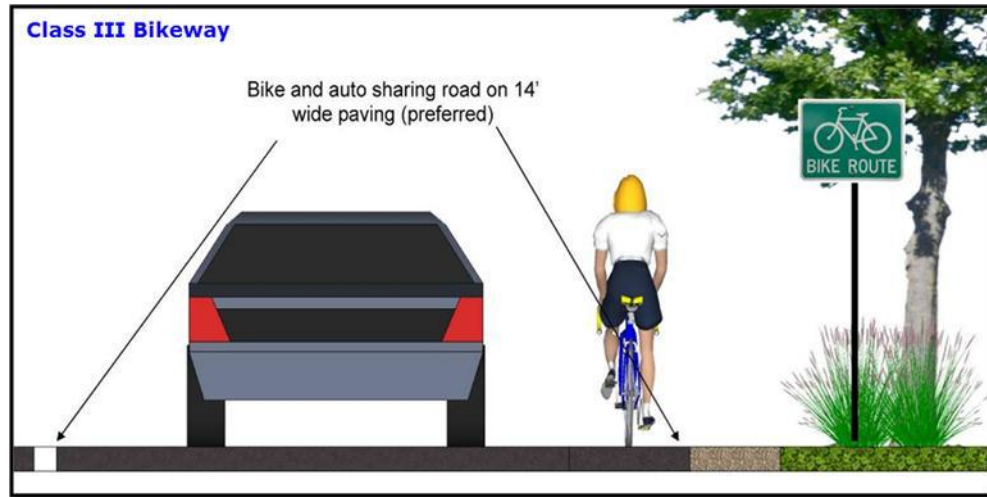
### *Class II*

Bikeway that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. For the purposes of Complete Streets, the County is proposing Class II bicycle facilities in East Orosi.



### *Class III*

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III facilities are proposed along Atwell Avenue. Although not signed on many roads in East Orosi, bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways.



## **Pedestrian Facilities**

### *Pedestrian Paths and Sidewalks*

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

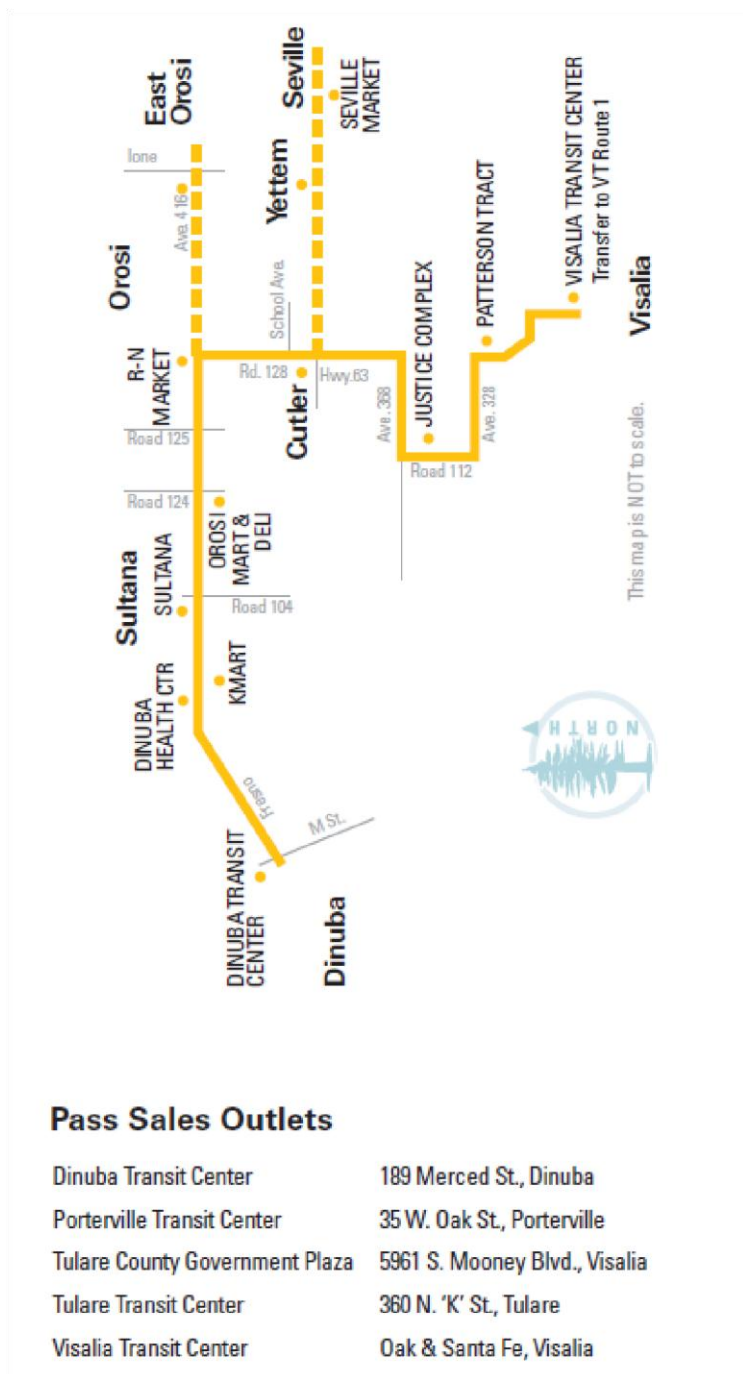
### *Multiuse Trails*

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. The Class 1 Facility along Avenue 416 could be considered a multiuse trail from Orosi to the East Orosi Community.

## **Transit Facilities**

Transit options give users the ability to get to a destination without relying on the automobile. This also provides other community benefits, including reduced vehicle miles traveled (VMT). Reducing VMT will help the County achieve their greenhouse gas reduction target,

Public transportation services and facilities in Tulare County consist of public bus service, paratransit service, and could also consider park-and-ride locations.



### Public Bus Service

Public bus service is provided by Tulare County Area Transit (TCaT) in rural areas. Existing transit routes and designated bus stops are shown in the following figures.

Additionally, Tulare County has provided guidance for including transit within facilities. These guidelines should be applied when considering new development to ensure appropriate connectivity and design features to support bus service.

### Paratransit Service

Paratransit is an alternative mode of passenger transportation that does not follow fixed routes or schedules. Typically, vans or minibuses are used to provide paratransit service. Paratransit services vary considerably on the degree of flexibility they provide their customers. The most flexible systems offer on-demand, call-up, door to door service from any origin to any destination in a service area.

### Park-and-Ride Lots

Park-and-ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the community's population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community along Highway 99.

## Cost Benefits Analysis, Implementation, and Funding Mechanisms

Caltrans lists the following benefits of Complete Streets in their implementation plan. They include:

- Increased Transportation Choices: Streets that provide travel choices can give people the option to avoid traffic congestion, and increase the overall capacity of the transportation

network.

- Economic Revitalization: Complete streets can reduce transportation costs and travel time while increasing property values and job growth in communities.
- Improved Return on Infrastructure Investments: Integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project spares the expense of retrofits later.
- Quality of Place: Increased bicycling and walking are indicative of vibrant and livable communities.
- Improved Safety: Design and accommodation for bicyclists and pedestrians reduces the incidence of crashes.
- More Walking and Bicycling: Public health experts are encouraging walking and bicycling as a response to the obesity epidemic. Streets that provide room for bicycling and walking help children get physical activity and gain independence.

## Benefits of Complete Streets

The health benefits from walking and bicycle riding include increased overall health, and a reduction in air quality and greenhouse gas emissions. According to the Caltrans accepted, Victoria Transport Policy Institute, walking has a \$.25 per mile health benefit, while the cost of Greenhouse Gas (GHG) reductions is \$23 per ton. According to the Federal Highway Administration, sidewalks reduce incidences to pedestrians over 80%.<sup>1</sup> According to Caltrans, the average costs of highway incidents are stated below.

Cost of Highway Accident	Dollars Per Accident
--------------------------	----------------------

Fatal Accident	\$4,800,000
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Injury Accident	\$67,400
-----------------	----------

Property Damage Only (PDO) Accident	\$10,200
-------------------------------------	----------

Average Cost per Accident	\$52,500
---------------------------	----------

Cost of an Event	Dollars Per Event
------------------	-------------------

Cost of a Fatality	\$4,400,000
--------------------	-------------

Cost of an Injury
-------------------

Level A (Severe)	\$221,400
------------------	-----------

Level B (Moderate)	\$56,500
--------------------	----------

Level C (Minor)	\$26,900
-----------------	----------

<sup>1</sup> [http://www.dot.ca.gov/hq/tpp/offices/eab/benefit\\_cost/LCBCA-economic\\_parameters.html](http://www.dot.ca.gov/hq/tpp/offices/eab/benefit_cost/LCBCA-economic_parameters.html)

Cost of Property Damage \$2,500  
Source: California Department of Transportation

## Community Specific Complete Street Implementation Measures

As part of a network-based approach, the County has identified (and will implement through pursuing further roadway studies and infrastructure design updates) a complete network for pedestrians. The County will also work to deliver infrastructure to support all modes of transportation. In addition to the General Plan Circulation Element Implementation Section, the key implementation measures include:

1. Evaluating Roadways as potential Bike/Pedestrian travel routes,
2. Completing pedestrian infrastructure, as appropriate,
3. Providing safe and accessible pedestrian facilities in high use areas,
4. Designing and building sidewalks for safer routes to school,
5. Designating roadways for bicycle routes that are aligned with the Tulare County comprehensive bicycle network,
6. Coordination with County Transit.
7. Submitting the following list of project and cost to TCAG and Caltrans for consideration under further grant funding opportunities.

## Measure R

Bike/Transit/Environmental Projects (14% of Measure R Funding)

On November 7, 2006, the voters of Tulare County Approved Measure R, imposing a ½ cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

## Active Transportation Program (ATP)

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation. The proposed projects have been included in the County's Active Transportation Plan (2015).



# Citizen Feedback

## Public Outreach Efforts

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues.

Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Complete Streets and Community Plan Outreach (2015 - 206) – is located in the Appendix G.

## Improvement Standards

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Transportation related facilities for public use are built within existing right of way (R/W) owned by a public agency, e.g., county, city or state. Within this R/W is a standard cross section, which is a term that is used to define the configuration of existing or proposed roadways at right angles to the centerline (CL). Typical sections show the width, thickness and descriptions of the pavement section, as well as the geometrics of the graded roadbed, side improvements and side slopes.

In Tulare County, the two most common cross sections are shown for two or four lane roads, varying in width based upon the number of lanes, parking, sidewalks, shoulders, bike lanes, etc. Figure 1 shows the cross section for two lane roads and Figure 2 identifies a typical four lane cross section.

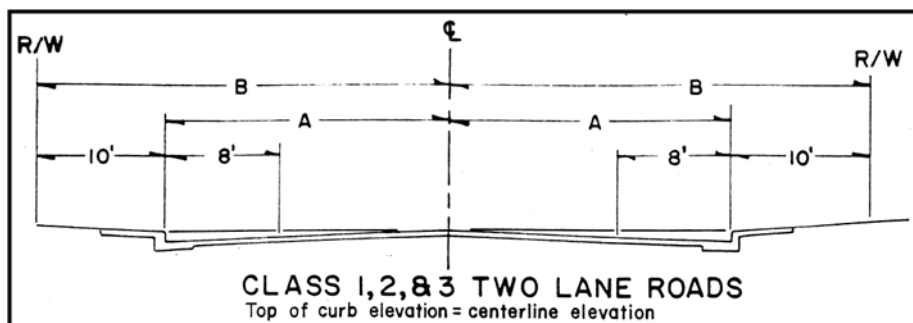


Figure 1 - Tulare County Class 1, 2 & 3 Two Lane Roads

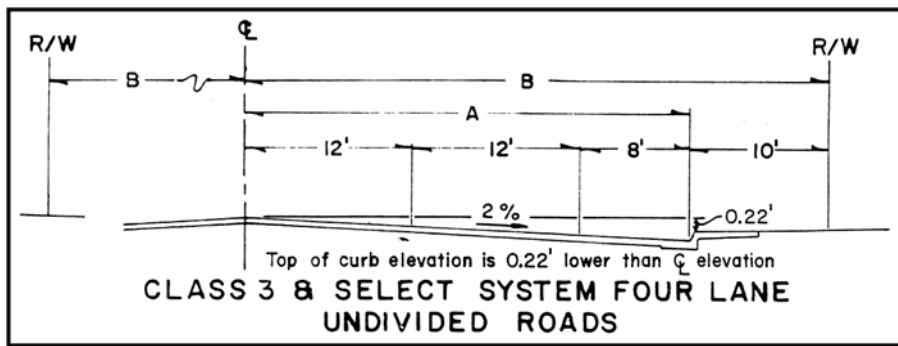


Figure 2 - Tulare County Class 3 Four Lane Road

## Tulare County Pavement Management System

### *Pavement Management*

Pavement management is the process of planning the maintenance and repair of a network of roadways or other paved facilities in order to optimize pavement conditions over the entire network. Pavement management incorporates life cycle costs into a more systematic approach to minor and major road maintenance and reconstruction projects. The needs of the entire network as well as budget projections are considered before projects are executed. Pavement management encompasses the many aspects and tasks needed to maintain a quality pavement inventory, and ensure that the overall condition of the road network can be sustained at desired levels.

### *Pavement Management System*


The Tulare County Pavement Management System (PMS) is a planning tool used to aid pavement management decisions. PMS software programs model future pavement deterioration due to traffic and weather, and recommend maintenance and repairs to the road's pavement based on the type and age of the pavement and various measures of existing pavement quality. Measurements can be made by persons on the ground, visually from a moving vehicle, or using automated sensors mounted to a vehicle. PMS software assists RMA staff to create composite pavement quality rankings based on pavement quality measures on roads or road sections. Recommendations are usually biased towards preventive maintenance, rather than allowing a road to deteriorate until it needs more extensive reconstruction.

Typical tasks performed by Tulare County PMS include:

- Inventory pavement conditions, identifying good, fair and poor pavements;
- Assign importance ratings for road segments, based on traffic volumes, road functional class, and community demand;
- Schedule maintenance of good roads to keep them in good condition; and,
- Schedule repairs of poor and fair pavements as remaining available funding allows.

Research has shown that it is far less expensive to keep a road in good condition than it is to repair it once it has deteriorated. This is why pavement management systems place the priority





on preventive maintenance of roads in good condition, rather than reconstructing roads in poor condition. In terms of lifetime cost and long term pavement conditions, this will result in better system performance.

The County is proposing a Road Maintenance Plan (see Appendix D) for the community of East Orosi that is a result of the PMS.

## Implementation

### Selection of Community Priorities

An effort is under way in Tulare County to implement Complete Streets Policies in the unincorporated communities within Tulare County's boundary. Just as the County updated its General Plan in 2012, many of the Community Plans are going through the update process. As a result of the Community Plan update process, several public meeting have been held in order to garner input from the local residents and business owners. Balancing the needs of what the people want while following local, state and federal policies and laws with a limited amount of available funding is the principal challenge in each community.

Transportation and related infrastructure costs tend to be exceedingly high may take years to implement. For purposes of this Study five transportation corridors were selected within the community (see Appendix A), and two roadway segments in the community were selected to be further evaluated for implementation of Complete Street standards. These roadway segments generally represent the highest volume roadways with a blend of residential and mixed land uses that also provide for regional access.

General themes that were voiced from residents in each community related to transportation included the need for:

- Sidewalks
- Better road conditions
- Safe walking and biking areas
- Street lights
- Pedestrian crossings
- Safe (lower) vehicle speeds
- Improved drainage
- Increased transit stops
- Improved connectivity (railroad crossings)

Given the information provided by the residents and business owners, conceptual layouts and designs based upon the citizens' concerns were presented to collect input. Based upon the community planning process, the following sections identify proposed projects for each community.

### Project Phasing

Generally, Tulare County RMA is proposing two types of projects coming from the community based upon the complexity of the project. The first types of projects are "shovel ready" that could be built immediately. They would be considered Phase 1 Projects and would have only minor needs for storm drain facilities, fence relocations, utility conflicts, etc. Phase 2 Projects are more inclusive and would be classified as medium to long range projects. These projects would need

other infrastructure improvements such as storm water basins, major storm drain improvements, utilities to be undergrounded, etc.

Phase 1 Projects	Phase 2 Projects
Curb, gutter & sidewalk (storm drain water into existing system); pedestrian ramps; bulb outs (where appropriate)	Curb, gutter & sidewalk (new drainage system)
Street lights	Major storm drain facilities (new pipelines and storm water basins)
Bus shelters, benches, trash receptacles, etc.	Utility relocations (undergrounding)
Fence relocations	Major land acquisition
Street signage and striping	Railroad crossing improvements
Minor utility conflicts	
Minor land acquisition	

**Outreach:** March 3, 2015, September 8, 2016 & December 15, 2016

The designs for roadway, bicycle and roadway facilities was based on feedback from members of the Custer-Orosi School District on March 3, 2015, and through two meetings held with members from the Community in September 2016, and December 2016. The approach RMA took was to plan for a connected network of Streets that brought travelers from Ave. 418 to the North through the Community on Lone / Road 140 down to connecting to Avenue 416.

## Improvement Plans

The following roadways were selected by the Community based on outreach and improvements designed by the Resource Management Agency's Public Works Division (See Appendix A).

Outreach: Thursday, September 8, 2016 6:00 p.m.

1. Ave 416 (between SR 63 and Road 140; this part is covered by the Orosi Complete street policy. We will include it in the East Orosi plan anyway)
2. Ave 418 (Road 139 to Road 140)
3. Lone/Road 140 (Ave 416 to Ave 419)
4. Ave 419 (Road 139 to Road 140)



# Projects

## Complete Streets Project Plans

The plans and projects in the appendices are identified as part of the complete streets policy to identify corridors for various user types and to demonstrate examples of design policies. These plans and are the result of input obtained through the community outreach process, multiple Tulare County agencies and divisions and professional engineering consultants.

The four projects identified herein represent the priority improvements to the backbone of the complete streets network within the community of East Oroshi. Two of these projects have been developed to a 30% design stage and the remaining two projects have been preliminarily scoped and budgetary estimates have been prepared. These four projects were developed to provide the County and various funding agencies with a list of projects to move toward funding, design, and ultimately construction.

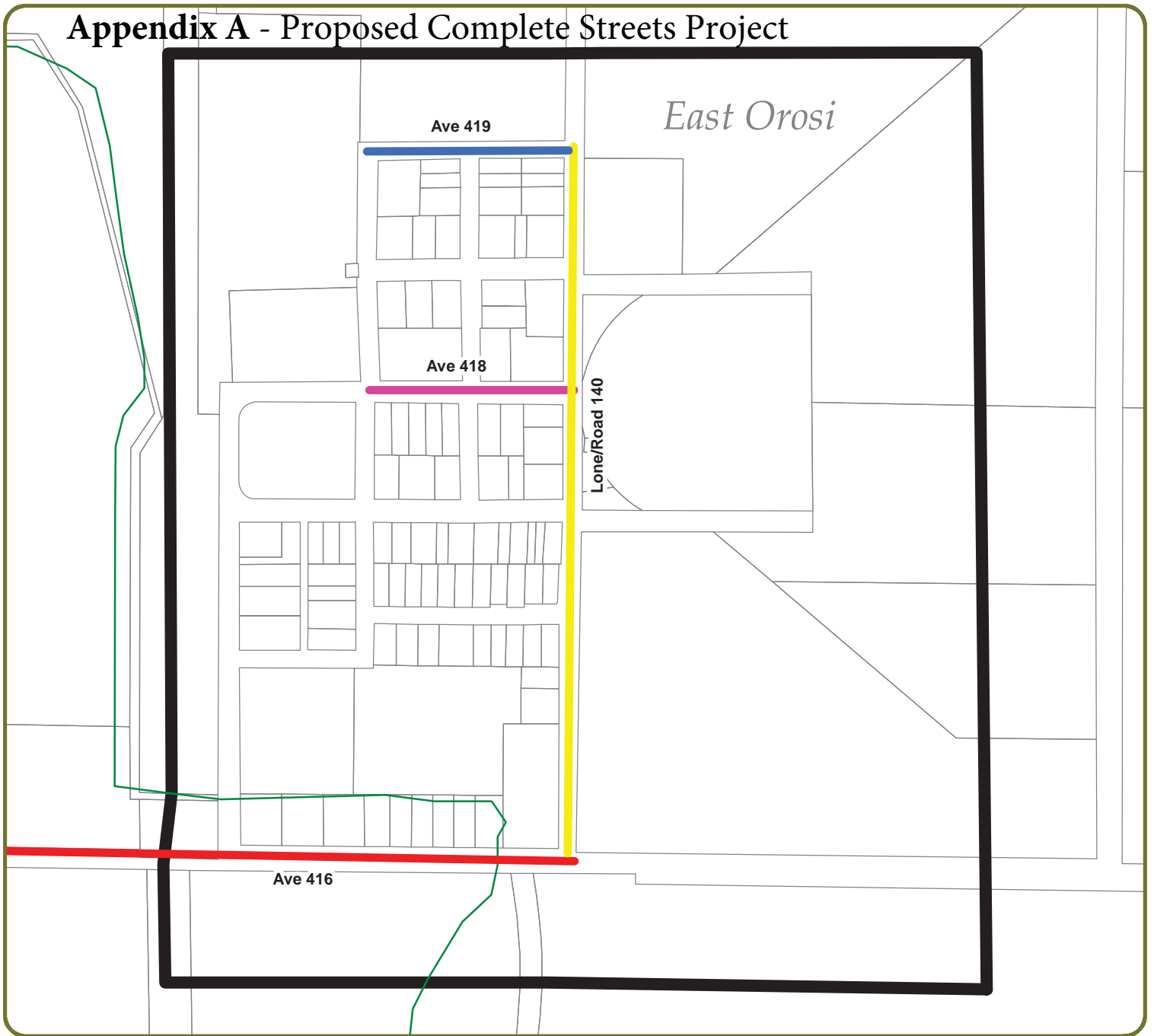
## Complete Streets Funding Opportunities

The following sections identify opinions of probable cost estimates for Complete Street transportation related improvements in East Oroshi. As shown in the tables, the funding sources include local, state and federal programs. Typically, local matches are required for acquiring state and federal funds. Measure R, a Tulare County sales tax for transportation, is available for such matches.

## Cost Estimates

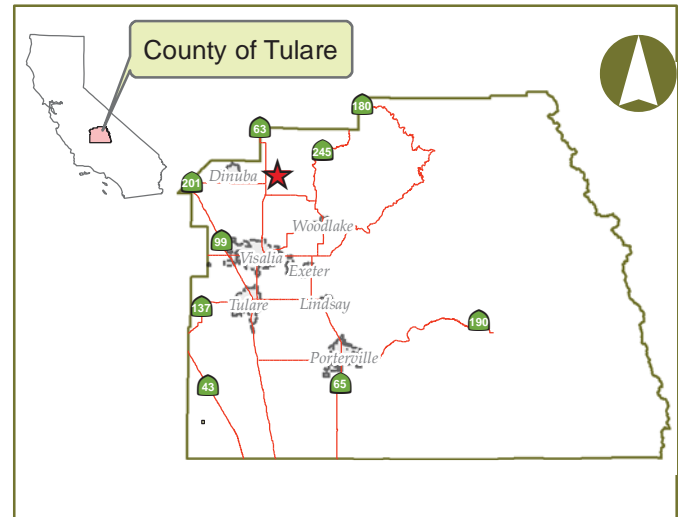
Detailed cost estimates are included in Appendix E.

# Appendix A - Proposed Complete Streets Project



## Legend

- Ave 418 Road 139-Road 140
- Ave 416 SR 63-Road140
- Lone/Road 140 Ave 416-Ave 419
- Ave 419 Road 139-Road 140

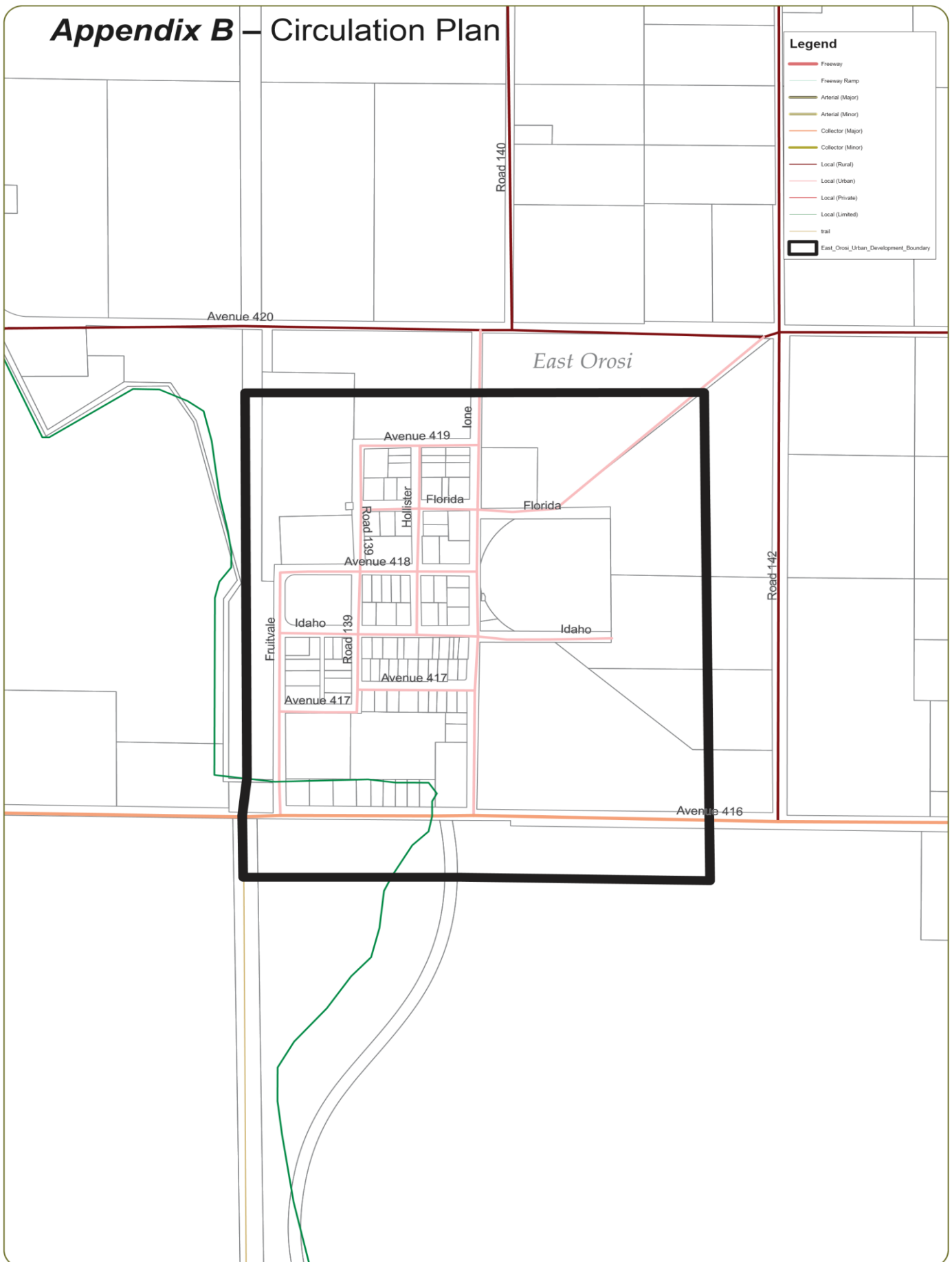


County of Tulare-East Orosi Road Identification  
Vicinity Map

# Appendix B – Circulation Plan

## Legend

- Freeway
- Freeway Ramp
- Arterial (Major)
- Arterial (Minor)
- Collector (Major)
- Collector (Minor)
- Local (Rural)
- Local (Urban)
- Local (Private)
- Local (Limited)
- trail
- East\_Orosi\_Urban\_Development\_Boundary



0

1,100  
Feet

County of Tulare-Community of East Orosi  
Circulation Plan

# Appendix C - Bicycle, Bus, and Pedestrian Plan

**Legend**

- Bike\_Route\_Class\_1
- Pedestrian\_Route
- Bus Stop Only
- East\_Orosi\_Urban\_Development\_Boundary
- Route 10

**Map Labels:**

- Avenue 420
- Road 140
- Road 142
- Avenue 419
- Avenue 418
- Avenue 417
- Avenue 416
- Road 139
- Road 138
- Road 137
- Road 136
- Road 135
- Road 134
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- Road 6
- Road 5
- Road 4
- Road 3
- Road 2
- Road 1

**Map Features:**

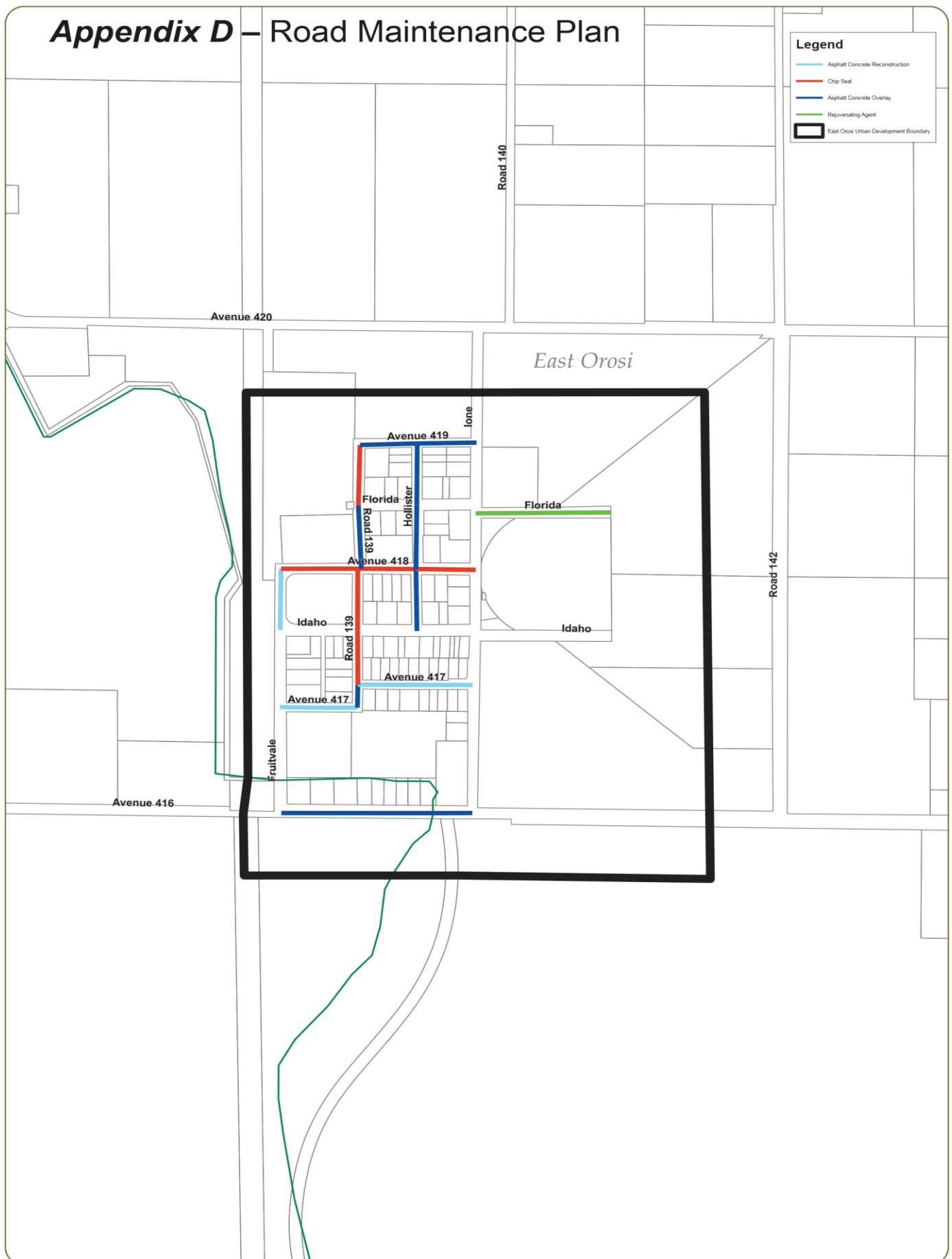
- East Orosi
- Florida
- Idaho
- Fruitvale
- Bus Stop Only
- Route 10



# Appendix D – Road Maintenance Plan

## Legend

- Asphalt Concrete Reconstruction
- Chip Seal
- Asphalt Concrete Overlay
- Rejuvenating Agent
- East Orosi Urban Development Boundary



0

1,100  
Feet

County of Tulare-Community of East Orosi  
Road Maintenance Plan



# Appendix E – Cost Estimates for East Orosi

Detailed Engineer's Estimate						
Agency:		Tulare County Resource Management Agency				
Project Name:		Complete Streets Phase 3 East Orosi				
Project Location:		Road 140 - Ave 416 to Ave 419				
Date of Estimate:		December 8, 2016				
Prepared by:		Pedro Ornelas/Diego Corvera				
Contruction Related Cost						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
1	999990	Mobilization	LS	1	\$100,000	\$100,000
2	050126	Construction Staking	LS	1	\$20,000	\$20,000
3	120090	Construction Area Signs	LS	1	\$25,000	\$25,000
4	120100	Traffic control system	LS	1	\$25,000	\$25,000
5	120300	Temporary Pavement Marker (Refl.)	LS	1	\$20,000	\$20,000
6	130200	Prepare Water Pollution Control Program	LS	1	\$10,000	\$10,000
7	220101	Finishing Roadway	LS	1	\$15,000	\$15,000
8	150712	Remove Painted Pavement Markings	SQFT	72	\$5	\$360
9	152390	Remove Roadside Sign	EA	6	\$200	\$1,200
10	152379	Relocate Fence	LF	1400	\$30	\$42,000
11(F)	190101	Roadway excavation	CY	1220	\$100	\$121,991
12(F)	250201	Class 2 Aggregate Base	CY	589	\$95	\$55,944
13	390133	Hot Mix Asphalt (Type B)	TON	636	\$120	\$76,320
14	731504	Minor Concrete (Curb & Gutter)	LF	2150	\$25	\$53,750
15	731521	Minor Concrete (Sidewalk)	SQFT	8300	\$8	\$66,400
16	731623	Minor Concrete (Ramp)	EA	12	\$4,000	\$48,000
17	731516	Minor Concrete (Driveway)	SF	2100	\$15	\$31,500
18	510502	Minor Concrete (Minor Structures)	EA	3	\$3,000	\$9,000
19	840515	Thermoplastic Pavement Marking (White)	SF	72	\$10	\$720
20	152434	Adjust riser	EA	2	\$1,000	\$2,000
21	152469	Adjust Utility Cover to Grade	EA	7	\$800	\$5,600
22	568023	Install Roadside Sign	EA	6	\$500	\$3,000
23		Remove Tree	EA	2	\$1,000	\$2,000
24		Minor Concrete (Continous Gutter)	LF	100	\$50	\$5,000
					Sub-Total:	\$739,785
					*Contingency:	\$73,979
					TOTAL:	\$813,764
* Up to 10% Contingency may be included in Engineer's Estimate						
(F) = Final Pay Item						

Non-Construction Related Cost						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
25	-	Environmental Clearance	% of CON	5%	\$739,785.19	\$36,989.26
26	-	Preliminary Engineering (PE)	% of CON	10%	\$739,785.19	\$73,978.52
27	-	Construction Engineering (CE)	% of CON	15%	\$739,785.19	\$110,967.78
28	-	R/W Acquisition	LS	1	\$15,000.00	\$15,000.00
29	-	Utility Pole Relocation	EA	10	\$25,000.00	\$250,000.00
Total:						\$486,935.56

<b>Total Construction &amp; Non-Construction Items</b>					<b>\$1,300,699.26</b>	
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INDEX OF SHEETS

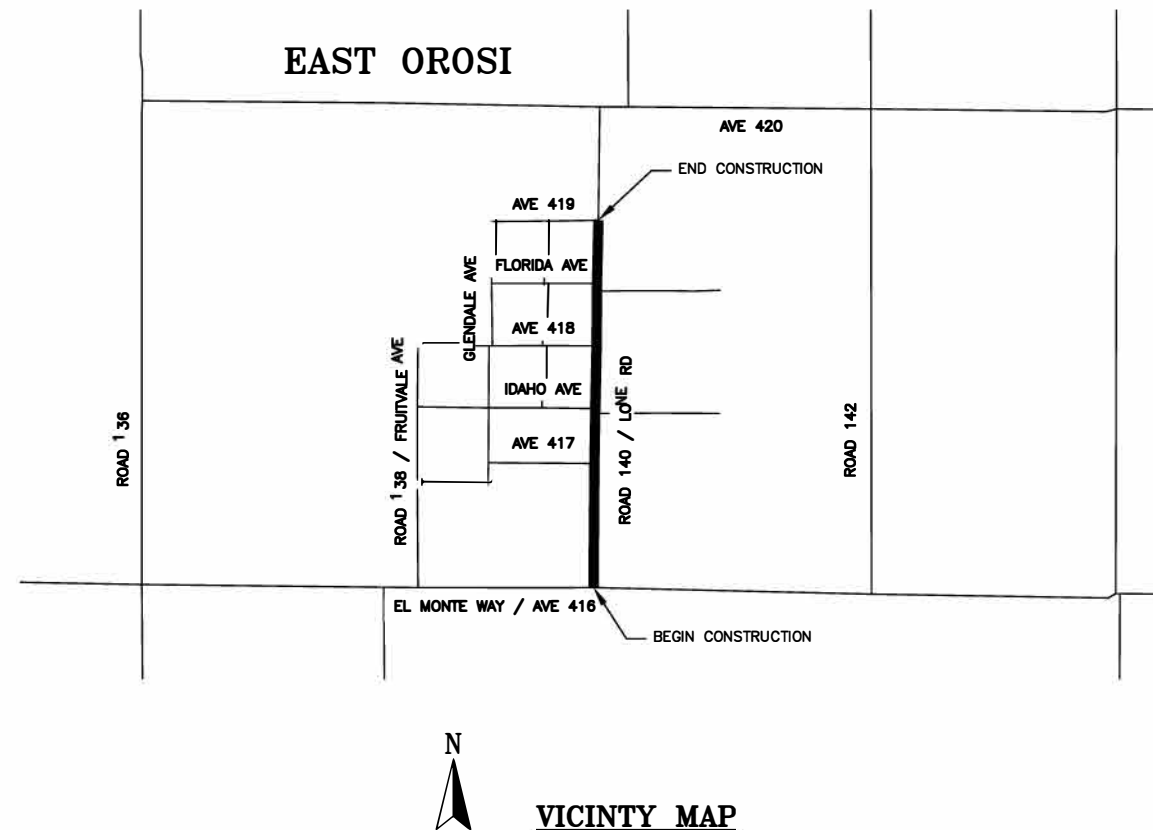
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X1	2	TYPICAL CROSS SECTIONS
L1-L2	3-4	LAYOUT SHEETS

STATE OF CALIFORNIA  
COUNTY OF TULARE

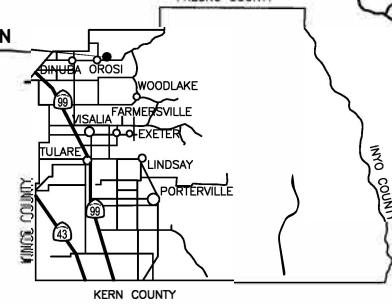
PROJECT PLANS FOR CONSTRUCTION OF  
COMPLETE STREETS PHASE III  
EAST OROSI  
IN COUNTY OF TULARE

ROAD 140 – AVENUE 416 TO AVENUE 419

Appendix F - East Orosi 30% Submittal Plan



PROJECT LOCATION



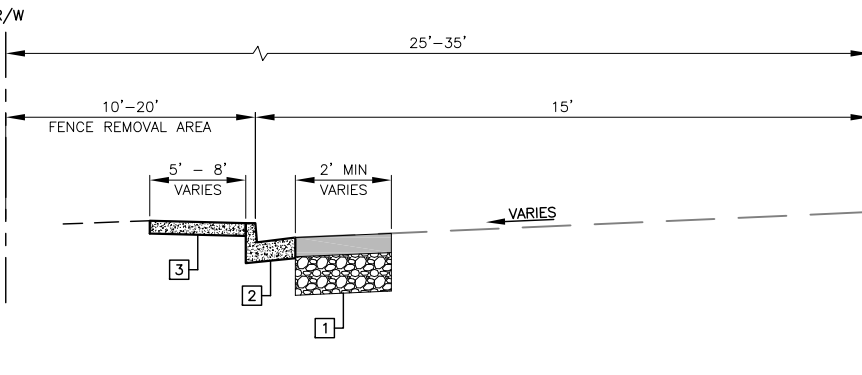
REVISIONS	DATE	BY

COUNTY OF TULARE  
RESOURCES MANAGEMENT AGENCY  
961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559) 624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA



TITLE SHEET  
COMPLETE STREETS PHASE III  
EAST OROSI  
TULARE COUNTY

SCALE	N.T.S.
DIVISION	DESIGN
JOB NO.	18018-1
DESIGNED	PAO
DRAWN	JDF
CHECKED	PAO
FILE	18018-1T001.DWG
DATE	11-15-2018
SHEET No.	T1



The diagram illustrates the proposed fence layout with the following dimensions and components:

- Overall Length:** 30' (indicated by a dimension line at the top).
- Segment 1:** 20' long, starting from the left boundary.
- Segment 2:** 10' long, ending at the right boundary.
- Barrier Components:**
  - Component 1:** A base layer of gravel, labeled with a box containing the number 1.
  - Component 2:** A middle layer of gravel, labeled with a box containing the number 2.
  - Component 3:** A top layer of gravel, labeled with a box containing the number 3.
- Dimensions and Notes:**
  - The width of the gravel layers is labeled as "2' MIN VARIES".
  - The width of the top gravel layer is labeled as "5' - 8' VARIES".
  - The area between the 20' and 10' segments is labeled "FENCE REMOVAL AREA".
  - A dashed line labeled "VARIES" indicates the boundary between the 20' and 10' segments.

2 OF 4

**30% SUBMITTAL  
PRELIMINARY, NOT  
FOR CONSTRUCTION**

CONSTRUCTION NOTES (SHEETS L1 & L2):

- 1 MINOR CONCRETE (CURB & GUTTER)
- 2 MINOR CONCRETE (SIDEWALK) WIDTH PER PLAN
- 3 MINOR CONCRETE (RAMP) CASE C
- 4 MINOR CONCRETE (DRIVEWAY)
- 5 MINOR CONCRETE STRUCTURES (DROP INLET)
- 6 STORM DRAIN PIPE
- 7 MISCELLANEOUS AC RAMP
- 8 ADJUST UTILITY TO GRADE
- 9 RELOCATE CHAIN LINK FENCE
- 10 RELOCATE WOOD FENCE
- 11 RELOCATE WROUGHT IRON FENCE

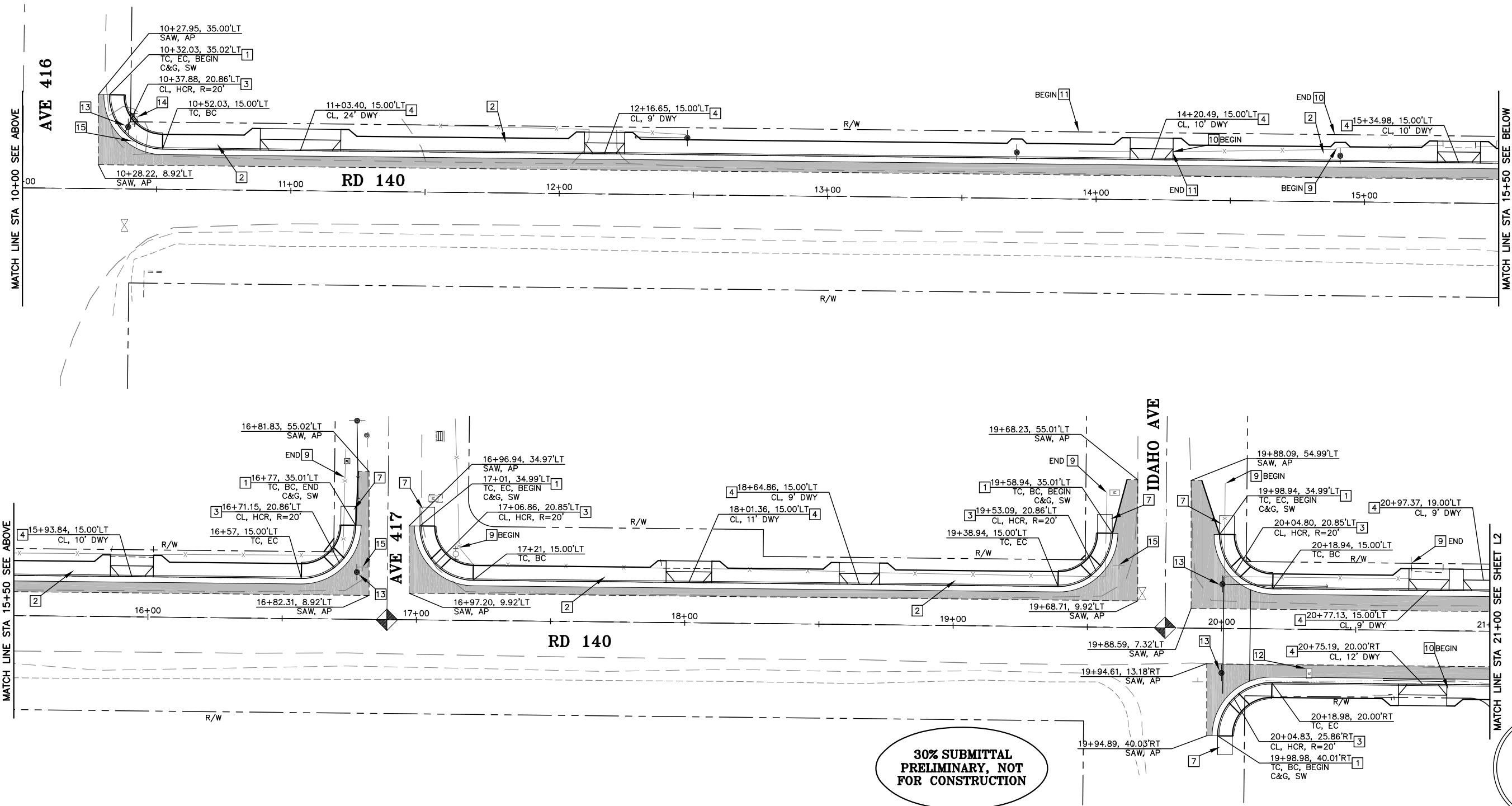
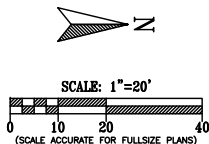
- 12 RELOCATE MAILBOX
- 13 RELOCATE UTILITY POLES (BY OTHERS)
- 14 RELOCATE FIRE HYDRANT
- 15 RELOCATE STREET SIGN
- 16 RELOCATE STONE WALL
- 17 REMOVE TREE

LEGEND

- PROPOSED STRUCTURAL SECTION
- EXIST FENCE
- EXIST MANHOLE
- EXIST ELECTRICAL POLE
- EXIST TREE
- EXIST FIRE HYDRANT
- EXIST WATER METER
- EXIST WATER VALVE
- EXIST SIGN
- EXIST MAILBOX
- MONUMENT

GENERAL NOTES:

- GRIND AREAS SHOWN ARE APPROXIMATE, EXACT GRIND & REPLACE AREAS SHALL BE MARKED BY ENGINEER.
- SAWCUT BOUNDARIES ARE APPROXIMATED FOR THE CONSTRUCTION LIMITS AND SHALL BE DEFINED IN THE FIELD BY THE ENGINEER.



30% SUBMITTAL  
PRELIMINARY, NOT  
FOR CONSTRUCTION

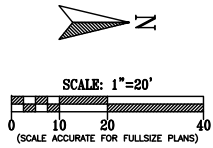
REVISIONS	
No.	DESCRIPTION

COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA



LAYOUT SHEETS  
COMPLETE STREETS PHASE III  
EAST OROSI  
TULARE COUNTY

SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	16018-1
DESIGNED	JDF
DRAWN	JDF
CHECKED	PAO
FILE	16018-1L001.DWG
DATE	11-15-2016
SHEET No.	



**30% SUBMITTAL  
PRELIMINARY, NOT  
FOR CONSTRUCTION**

[illegible]

**COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY**

5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
[WWW.TULARECOUNTY.CA.GOV/RWA](http://WWW.TULARECOUNTY.CA.GOV/RWA)



LAYOUT SHEETS

**COMPLETE STREETS PHASE III**

**EAST OROSI**

TULARE COUNTY

SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	16018-1
DESIGNED	JDF
DRAWN	JDF
CHECKED	PAO
FILE	16018-1L001.DWG
DATE	11-15-2016
SHEET No.	

L2

4 of 4

## Appendix G – Outreach

Please join us for the Community Safe Routes to School and Complete Streets meeting. The meeting will dive deeper into the goals of the Safe Routes to School plan and the Complete Streets Policy. We encourage all the members of the community to attend. We seek and welcome your input!

**Highlights:** What is Safe Routes to School, the need for the safe routes to school plan, identification of project areas, discussion of future funding and more!

**Date: Thursday, September 8th, 2016**

**Time: 6:00 p.m.**

**Location: Palm Elementary School- Cafeteria  
12915 Ave 419 Orosi, CA**

For more information call: 559-624-7000



Por favor, acompáñenos a la junta de su Comunidad para informarse acerca del programa Rutas Seguras a la Escuela y policía de calles completas. La reunión se enfocará en los objetivos y metas que tiene el plan de la comunidad y las necesidades de las carreteras. Todos los miembros de la comunidad están invitados a asistir. ¡Agradecemos su ayuda y opinión!

**Aspectos destacados:** ¿Que es "Rutas Seguras a la Escuela"? la necesidad de rutas seguras a la escuela, la identificación de áreas de proyectos, un análisis de finanzas futuras y mucho más!

**Fecha: 8 de Septiembre del 2016**

**Hora: 6:00 p.m.**

**Localidad: Palm Escuela- Cafeteria  
12915 Ave 419 Orosi, CA**

Para más información llame a: 559-624-7000

**Safe Routes to School**   



**East Orosi**

**Safe Routes to School Planning, Complete Streets Meeting,  
Community Planning  
Tulare County**

**Thursday, September 8, 2016**

**Time: 6:00 p.m.**

**Location: Palm School  
12915 Ave 419 Orosi, CA**

1. Introduction
2. Purpose of the Meeting
3. Land Use & Zoning
4. Complete Streets
5. Safe Routes to School
6. Preferred Routes
7. Public Input

Thank you for attending!  
559-624-7000



Cutler-Orosi Community Meeting hosted by the Tulare County Resource Management Agency  
Complete Streets Meeting/ Community Plan Kick off Meeting  
Tuesday, March 3, 2015  
Golden Valley Elementary School- Cafeteria  
41456 Rd. 127  
Orosi, CA 93647  
5:30p.m.

County staff Kyria Fierros, Dave Bryant and Aaron Bock from County Economic Development and Planning Branch from the Tulare County Resource Management Agency (RMA) attended the community meeting in Orosi, CA on March 3, 2015. This was the second meeting RMA held in Orosi.

On February 12, 2015, RMA gave an informational update about the Community Plan and Complete Streets program at the Cutler-Orosi Joint Unified School Board meeting.

At the meeting the Board suggested RMA hold a separate meeting with the community allowing all local residents the opportunity to come to a meeting. The Board also requested information on the past improvements completed from the previous Community Plan of 1988 and the amount of projects implemented since the passing of Measure R in 2006.

On March 3, 2015 RMA with the help from Roel Alvarado, Principal at Cutler Elementary, translated the meeting in English to Spanish.

RMA staff gave a PowerPoint explaining the Community Plan process and the Complete Streets program letting the public know we were seeking input on priority streets. Dave Bryant updated the community on infrastructure improvements from the previous community plan of 1988 and Kyria Fierros presented on the Measure R funds.

According to our Road Department there the total amount of funds used from Measure R funds since 2006:

Cutler \$242K

Orosi \$900K

Total: \$1.1 million

The five Streets County identified for the complete streets by the community were:

Orosi

1. Avenue 416 – SR 63 to Rd 140 (East Orosi) 43 Votes
2. Avenue 413 – Rd 124 to SR 63 11 Votes
3. Avenue 419 3 Votes
4. Avenue 416 – SR-63 to Dinuba 0 Votes





# EAST OROSI COMMUNITY PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9371

General Plan Amendment GPA 17-035-Resolution No. 9372

Section 18.9 “Mixed Use” Combining Zone-Resolution No. 9373

Section 16 “By Right Uses”-Resolution No. 9374

Zoning Districts Map-Resolution No. 9375

Sustainable Community Plans-Resolution No. 9376

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE COMMUNITY PLANS 2017 ) RESOLUTION NO. 9371  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Community Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Community Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Community Plans and Urban Development Boundaries, (Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, and Sultana General Plan Amendment No. GPA 17-012, Change of Zones (PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map , for an amendment PZC 17-042 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundary, and Zone Ordinance Amendment No. PZC 17-041 establishing a Mixed Use Combining Zone in Alpaugh, East Orosi, London, Richgrove, and Sultana (incorporated by reference herein). The unincorporated Communities are located within the Mount Diablo Base & Meridian as follows: Alpaugh, Sections, 27, 28, 33 & 34, Township 23S, Range 23E, East Orosi, Sections 9, & 16, Township 16S, Range 25E, London, Sections 1, 2, 11 & 12, Township 20S, Range 25E, Richgrove, Section 36, Township 24S, Range 26E, and Section 31, Township 24S, Range 27E, and Sultana, Sections 10, 11, 14 & 15, Township 16S, Range 24E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Alpaugh, East Orosi, London, Richgrove, and Sultana to the Mixed-Use Combining Zone; (3) allow additional by-right uses

within the aforementioned Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Community Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-042 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map within the respective Urban Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-041 an amendment to Section 16 to allow additional by-right uses in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Community Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Community Plans;
  - b. Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Community Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Community planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Community Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Community Plans 2017 Update General Plan Amendment 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004; Change of Zone No. PZC 17-004 (Zoning District Map); East Orosi General Plan Amendment No. GPA 17-034; Change of Zone No. PZC 17-005 (Zoning District Map); London General Plan Amendment No. GPA 17-008; Change of Zone No. PZC 17-008 (Zoning District Map); Richgrove General Plan Amendment No. GPA 17-011; Change of Zone No. PZC 17-011 (Zoning District Map); Sultana General Plan Amendment No. GPA 17-012; Change of Zone No. PZC 17-012 (Zoning District Map; Change of Zone No. PZC 17-042 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-041 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Community Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,

4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Community Plans 2017 Update. Therefore, the public will benefit from the Community Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III COMMUNITY PLANS, OF THE ) RESOLUTION NO.9372  
TULARE COUNTY GENERAL PLAN FOR THE )  
COMMUNITY PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-004, GPA 17-034, )  
GPA 17-008, GPA 17-011, GPA 17-012, )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Community Plan of the Tulare County General Plan as set forth in Exhibits "A to E" for the Community Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, Sultana General Plan Amendment No. GPA 17-012 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the communities as part of the Community Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Community Plans of the Tulare County General Plan for the Community Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Community Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Community Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Urban Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the adoption of urban development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;



5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Community Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Community Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:


C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Community Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

- Exhibit “A” – 2017 Alpaugh Community Plan (Part III)
- Exhibit “B” – 2017 East Orosi Community Plan (Part III)
- Exhibit “C” – 2017 London Community Plan (Part III)
- Exhibit “D” – 2017 Richgrove Community Plan (Part III)
- Exhibit “E” – 2017 Sultana Community Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO            )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT        )        RESOLUTION NO. 9373  
WITH THE ADOPTED COMMUNITY PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE    )  
NO. PZC 17-041                                    )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Community Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352.

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana ;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, Earlimart, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and

Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Community Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Communities of Alpaugh, East Oroquieta, London, Richgrove, and Sultana, consistent with the adopted Community Plans 2017 Update.



The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:       Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:       None  
ABSTAIN:   None  
ABSENT:     None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9374
CONSISTENT WITH THE ADOPTED	)	
COMMUNITY PLANS, AS PROPOSED IN	)	
CHANGE OF ZONE NO. PZC 17-042	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Earlimart, Ducor Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hypericum, Jovista, Matheny Tract and Tooleville. The Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
ORDINANCE NO. 352, ZONING ORDINANCE )  
DISTRICT MAPS TO REZONE PROPERTIES )  
IN THE URBAN DEVELOPMENT BOUNDARIES )  
OF ALPAUGH, EAST OROSI, LONDON, )  
RICHGROVE, AND SULTANA ) RESOLUTION NO. 9375  
CONSISTENT WITH THE ADOPTED )  
COMMUNITY PLANS 2017 UPDATE (GPA 17-035), )  
AS PROPOSED IN CHANGE OF ZONE NO. PZC's )  
PZC 17-004, PZC 17-005, PZC 17-008, PZC 17-011, )  
AND PZC 17-012, )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Alpaugh Rezoning Plan (PZC 17-004), to include the Community of Alpaugh, consistent with the adopted Alpaugh Community Plan 2017 Update, Exhibit "E" East Orosi Rezoning Plan (PCZ 17-004), to include the Community of East Orosi, consistent with the adopted East Orosi Community Plan 2017 Update, Exhibit "H" London Rezoning Plan (PCZ 17-016), to include the Community of London, consistent with the adopted London Community Plan 2017 Update, Exhibit "K" Richgrove Rezoning Plan (PCZ 17-017), to include the Community of Richgrove, consistent with the adopted Richgrove Community Plan 2017 Update, Exhibit "N" Sultana Rezoning Plan (PCZ 17-018), to include the Community of Sultana, consistent with the adopted Sultana Community Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities;
3. The proposed changes in zone district designations identified in the Alpaugh, East Orosi, London, Richgrove, and Sultana Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Alpaugh, East Oroshi, London, Richgrove, and Sultana Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Alpaugh Change of Zone No. PZC 17-004, East Oroshi Change of Zone No. PZC 17-005, London Change of Zone No. PZC 17-008, Richgrove Change of Zone No. PZC 17-011, Sultana Change of Zone No. PZC 17-012, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" Rezoning Plans consistent with the adopted Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary



Exhibits:

Attachment 1

***Alpaugh Change of Zone No. PZC 17-004***

*Exhibit "A" – Zoning District Ordinance Map*

*Exhibit "B" – Zoning Map*

*Exhibit "C" – List of Affected Properties/Map*

***East Orosi Change of Zone No. PZC 17-005***

*Exhibit "D" – Zoning District Ordinance Map*

*Exhibit "E" – Zoning Map*

*Exhibit "F" – List of Affected Properties/Map*

***London Change of Zone No. PZC 17-008***

*Exhibit "G" – Zoning District Ordinance Map*

*Exhibit "H" – Zoning Map*

*Exhibit "I" – List of Affected Properties/Map*

***Richgrove Change of Zone No. PZC 17-011***

*Exhibit "J" – Zoning District Ordinance Map*

*Exhibit "K" – Zoning Map*

*Exhibit "L" – List of Affected Properties/Map*

***Sultana Change of Zone No. PZC 17-012***

*Exhibit "M" – Zoning District Ordinance Map*

*Exhibit "N" – Zoning Map*

*Exhibit "O" – List of Affected Properties/Map*

Attachment 2

***Community Plan Rezoning Full Ordinance***

*Exhibit A - Community Plan Rezoning Full Ordinance*

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF SUSTAINABLE  
COMMUNITY PLANS

)  
)

RESOLUTION NO. 9376

Resolution of the Planning Commission of the County of Tulare recommending the Board of Supervisors adopt and accept the Sustainable Community Plans. A major component of the Community planning process included a thorough examination and cataloguing of existing land uses which suggested a strong need for infrastructure improvements, land use changes and zoning changes, where feasible, in order to fulfill the goals of the General Plan and Senate Bill 244. The Unincorporated Community Infrastructure Maps provided the measurement tools needed to identify infrastructure solutions, appropriate improvement projects, and planning policy analysis. The resulting objective of the proposed Community Plans is aimed at reducing greenhouse gas emissions, promoting equity, providing economic stability; and thus, enhancing sustainability, as well as protecting the environment, and promoting healthy and safe communities. The Plans necessitated and ensured methods of public participation with a wide variety of stakeholders.

WHEREAS, The Budget Act of 2010/11 authorized the Department of Conservation (Department) to implement and administer \$63.15M in project funds (Proposition 84, the Safe Drinking Water, Water Quality and Supply, Flood Control, River and Coastal Protection Bond Act of 2006) to provide Planning Grants and Incentives awarded by the Strategic Growth Council (Council). The primary goal of this grant program is to develop and implement plans consistent with Chapter 13, Division 43, Public Resources Code, Sections 75127 and 75128; and

WHEREAS, The County received a grant from the Strategic Growth Council in 2014, Grant No. 2014-631, to study the feasibility of integrating infrastructure and land use, with the needs of rural unincorporated communities based on the Communities within Tulare County that received a top 10% ranking utilizing the *CalEnviroScreen* methodology and in fulfillment of the General Plan's Housing Element and SB 244 Plan (2014); and

WHEREAS, *"The work upon which this publication is based was funded in whole or in part through a grant awarded by the Council."* Preparation and processing of the Sustainable Community Plans took an estimated 36 months; and

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County

Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and

WHEREAS, the Sustainable Community Plans included all tasks and deliverables specified in the work plan including project development and meetings, data gathering, public workshops, plan development, public hearings, and grant reporting and administration; and

WHEREAS, the Sustainable Community Plans include strategies that reduce energy consumption, conserve water, improve air and water quality, and provide other community benefits; and

WHEREAS, staff has performed necessary investigations, prepared a written report, and recommended modifications and approval of these proposed Plans; and

WHEREAS, staff has produced the Final Report document attached as "Exhibit A" pending further review by the Planning Commission and the public, and will make all necessary administrative changes subject to this review and prior to the final submittal to the Board of Supervisors; and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. Recommend that the Board of Supervisors find that the said Sustainable Community Plans meet the Goals and Objectives of the Council in fulfillment of the Grant requirements as specified in the work plan.

The foregoing resolution was adopted upon motion of Commissioner Gong, seconded by Commissioner Aguilar, at a regular meeting of the Planning Commission on the 8th day of November, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A- Strategic Growth Council Final Report

Appendix B – Board of Supervisors  
Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

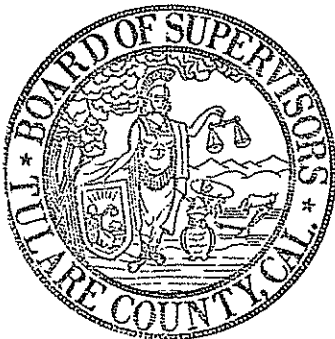
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.



9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## London Community Plan

# LONDON COMMUNITY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# LONDON COMMUNITY PLAN

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# LONDON COMMUNITY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# LONDON COMMUNITY PLAN

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# LONDON COMMUNITY PLAN

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## London Community Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9371, 9372, 9373, 9334, 9375, and 9376

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-042  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-041

London Community Plan: GPA 17-008  
Zoning District Map: PZC 17-008



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000



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# LONDON COMMUNITY PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Mike Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

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# LONDON COMMUNITY PLAN

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## EXECUTIVE SUMMARY

### INTRODUCTION

The primary objective in preparing a Community Plan for London is to develop a plan which can accurately reflect the needs and priorities of this community. London is currently designated as a Community in the 2030 Tulare County General Plan (2012). However, despite this fact, this will be the first Community Plan for London; and the plan will be to create the opportunity to increase the availability of infrastructure funding (for things such as drinking water system improvements [for example wells, water distribution piping, and storage tanks], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

### Location

London (**See Figure 1**) is a Census-Designated Place located in the northern portion of the County, approximately three miles southwest of Dinuba and ten miles northwest of Visalia. It is bounded by Avenue 376 in the south, Avenue 384 in the north, Kennedy School House Ditch in the west, and Road 60 in the east and encompasses 0.6 square miles of land. London is an agriculturally oriented service community surrounded on all sides by lands in agricultural production, vacant lands, and scattered rural residential homes. Cities and communities surrounding London include Visalia to the southeast; Dinuba to the northeast; and the community of Traver to the southwest. The Tulare County/Fresno County Line is located approximately 4.8 miles west of London (**See Figure 2**). London is in Section 01, 02, 11, & 12 Township 17 South, Range 23 East, MDB&M, and can be found within Reedley United States Geological Survey 7.5 minute topographic quadrangle. London is located at an elevation of 384 feet National Geodetic Vertical Datum. The coordinates of London are: Latitude: 36.474680 and Longitude: -119.443178.

### Planning Area

The London Urban Development Boundary (UDB) area consists of 384.2 acres (**see Figure 3**). The Land Uses within the UDB are Mixed Use activities that occupy 355.3 acres. The remaining 28.9 acres are dedicated for Right-of-way (**see Table 1**).

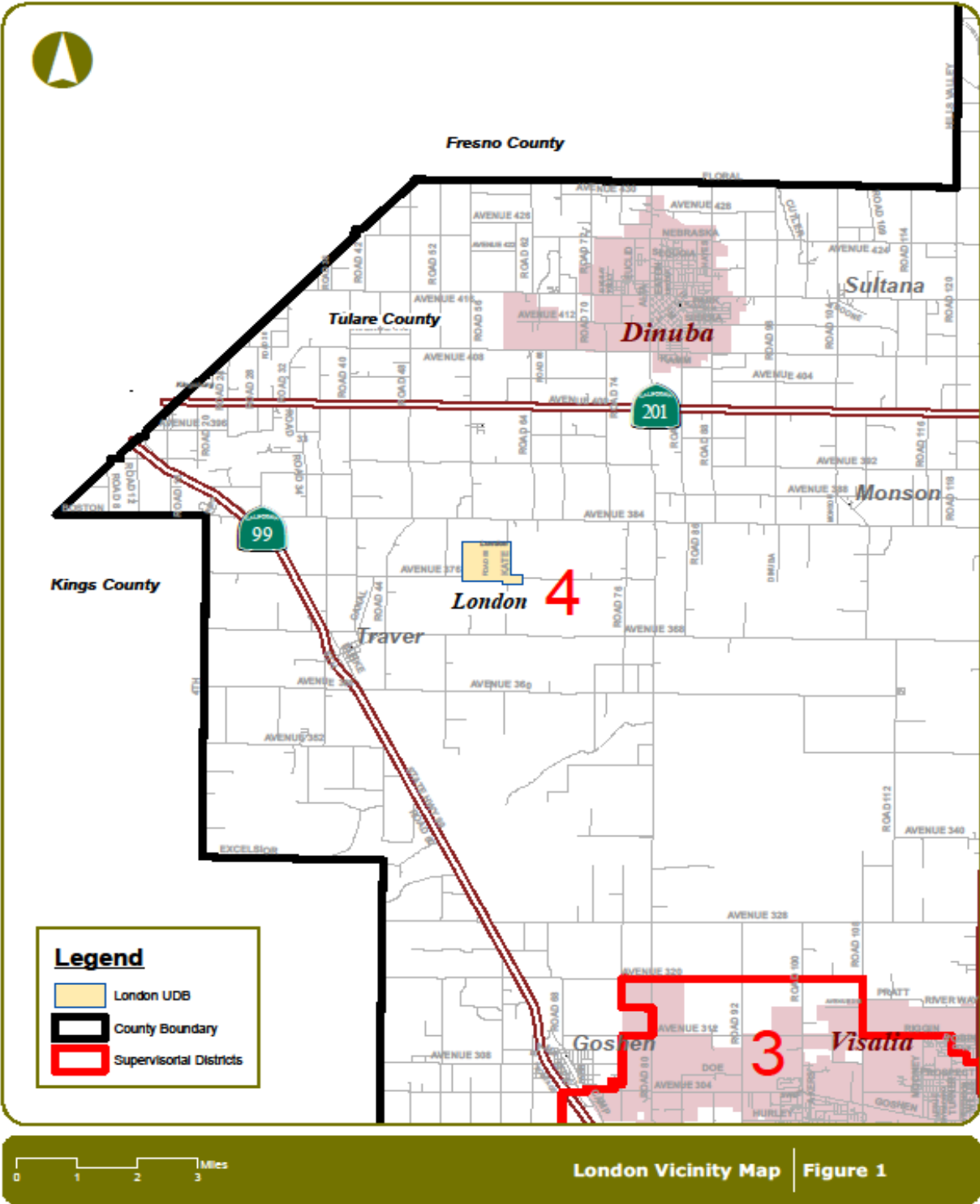
No change is proposed to the Urban Development Boundary.

Table 1 - London Land Use	
Adopted Land-Use	Acres
Mixed Use	355.3
Unclassified (Right-of-Way)	28.9
<b>TOTAL</b>	<b>384.2</b>

Source: Tulare County GIS

# LONDON COMMUNITY PLAN

Figure 1 - Vicinity Map





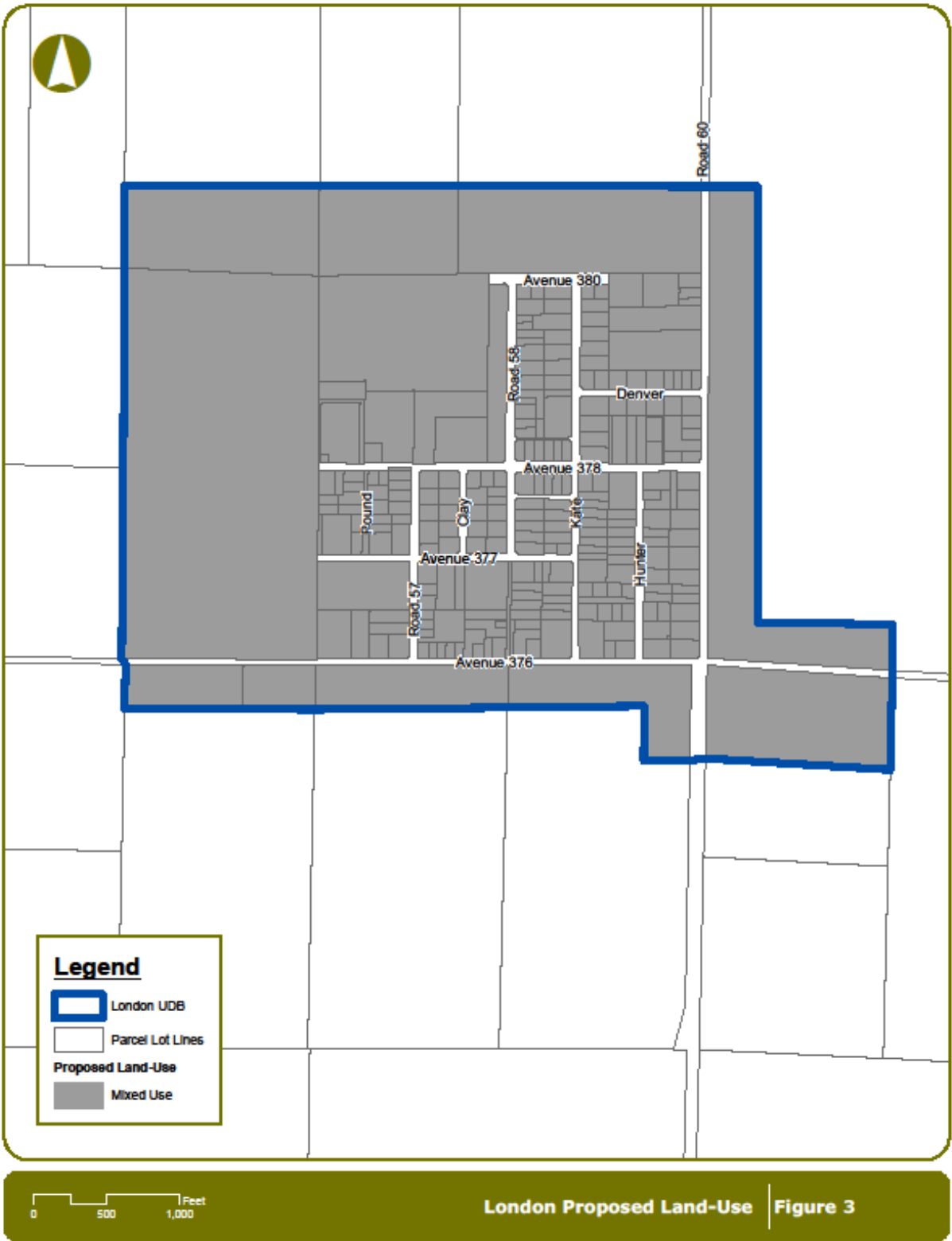
# LONDON COMMUNITY PLAN

Figure 2 - Aerial Map



# LONDON COMMUNITY PLAN

Figure 3 - London UDB



# LONDON COMMUNITY PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY

### OUTREACH REPORT

#### Community Outreach and Invitation Process

The County of Tulare was successful in obtaining a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Community Plan. In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises went door to door throughout the community of London to distribute flyers at homes, businesses, and schools. During this process SHE talked with residents about the importance of participating in this process. SGC partnered with London School to send flyers home with students. Meeting invitations were sent to the members of the London Community Service District as well as the District's County Supervisor. Fliers were posted at local stores and gas stations.

#### Community Outreach Steps

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- (1) SHE staff developed an informational flyer. (See attached.)
- (2) SHE staff went door to door to distribute flyers Door to door outreach was done on Wednesday, February 10 and Saturday February 13, 2016.
- (3) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- (4) SHE partnered with the London Head Start Pre- School, meeting flyers were sent home from school with each student. SHE partnered with the Hodges London Community Center to post fliers and spread the word about the meeting.
- (5) Meeting flyer were distributed to residents at the local London food pantry on Thursday February 4, 2016.
- (6) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at local corner stores and churches.

#### Community Meetings

Self-Help Enterprises held a SGC community meeting (with two SHE staff members present) in London on February 17, 2016 at 5:30pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Hodges Community Center 5750 Ave 378, Dinuba CA. This is an ideal meeting location because it is centrally located and residents feel welcomed and comfortable being there. The purpose of the meeting was to discuss London improvement needs, gather community data, and report the findings of the meeting in a final report. Twenty-two residents attended the meeting, twenty-one community surveys were collected.

A follow up meeting was held on November 14, 2016 (See Community Flyer; Figure 3), with over 20 people present. In attendance was also SHE and the County staff. Results of the survey were discussed at the meeting, and attendees generally agreed with the results. Long term water availability, and conservation measures were of great concern at the meeting with other issues such as the drought, flooding in the streets being discussed as well.



# LONDON COMMUNITY PLAN

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Self Help Enterprises and Tulare County invites you  
to a community meeting to review

## London's Community Plan

**Monday, November 14, 2016 at 6:00pm at  
Hodges Community Center 5750 Ave 378**

A few months ago, Tulare County came to your community to seek input on a planning analysis of your community. Surveys were distributed and collected. Tulare County invites you to attend a community meeting to see the findings from the study. A thorough examination and inventory of existing land uses will suggest infrastructure improvements, land use changes and possibly zoning changes, where feasible.

For more information, call: Abigail at 559-802-1659 or Kyria 559-624-7154

The work upon which this publication is based was funded in whole or in part through a grant awarded by the California Strategic Growth Council.

**Disclaimer:** The statements and conclusions of this report are those of the GRANTEE and/or subcontractor and not necessarily those of the California Strategic Growth Council of the California Department of Conservation, or its employees. The California Strategic Growth Council and the California Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text



Self Help Enterprises y El Condado de Tulare  
le invita a la Junta Comunitaria para revisar

## El Plan Comunitario de London

**Lunes 14 de noviembre de 2016 a las 6:00 de la tarde  
En el Centro Comunitario Hodges 5750 Ave 378**

Hace unos meses, el Condado de Tulare vino a su comunidad para buscar la opinión de la comunidad sobre un análisis de planificación de su comunidad. Encuestas fueron distribuidas y recolectadas. El Condado de Tulare le invita a asistir a una junta comunitaria para ver los resultados del estudio. Un examen completo y la organización de los usos existentes de la tierra sugerirán mejoras en la infraestructura, cambios en el uso de la tierra y posiblemente cambios de zonificación, donde sea factible.

Para más información llame a: Abigail 559-802-1659 o Kyria 559-624-7154

El trabajo sobre el que se basa esta publicación fue financiado en su totalidad o en parte, a través de una beca

# LONDON COMMUNITY PLAN

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## Community Survey

A survey was developed to gather a variety of community information about multiple topics. The survey covered the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey. After discussion at the London SGC community input meeting, residents concluded that the following is a list of their priority improvement needs.

After careful discussion at the London SGC community input meeting, residents concluded that the following is a list of their priority improvement needs:

## Priority Improvements

- 1) Road Conditions & Street Safety including:
  - Flashing warning lights
  - Speed limit signs
  - Speed bumps
  - four way stop
  - Street lights
- 2) Community Park
- 3) Internet Access
- 4) Sidewalks
- 5) Storm water drainage
- 6) Affordable Housing

## **Road Conditions and Street Safety**

London residents expressed that street safety, including the need for sidewalks, street lighting, speed limit signs, stop signs, and the need for enforcement of traffic laws are the highest priority improvement needs in London. Survey results show that most residents are not satisfied with road conditions. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage

# LONDON COMMUNITY PLAN

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caused to their vehicles while driving on London roads. Residents report that some roads are narrow and that two cars barely fit on one road at the same time. It was said that roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired. 100% of the respondents are not satisfied with the conditions of the roads. 100% of the respondents described the roads to be in “poor” conditions. The following roads are a priority to the residents:

- Rd 58
- Ave 377
- Pound Rd
- All major roads

## **Survey comments regarding road conditions:**

“Fix the roads”

“Some roads are all dirt” “Lots of potholes”

“We pay Measure R, but they don’t fix our roads” “Nobody maintains our roads”

## **Sidewalks**

The community of London has too few sidewalks. Some of the residents that were surveyed reported that the street they live on does not have sidewalks. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. When it rains, residents and kids must walk to school in the mud and through puddles of water. London traffic travels at a high rate of speed and the absence of sidewalks means there is no clear separation between walking paths and the road, which creates a safety hazard for pedestrians.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting, and that they stay indoors because they do not feel safe walking on a dark street. At night, one cannot see down the street; which is a major safety concern. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Internet Access**

Most families in London do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. This is considered to be a problem for residents, as without access to the internet, they are unable to do things such as online job hunting, applying for resources, or provide homework help to their kids.

Some residents use the computers and internet at the Hodges Community Center, however usage is limited to hours of operation. Internet access ranked as a high priority improvement need in London.

# LONDON COMMUNITY PLAN

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## **Storm Water Drainage**

London residents report that the community does not have adequate storm water drainage. This is a problem because the stagnant water attracts insects. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season.

## **Neighborhood Park**

In the community of London there is no safe green space for children to play and adults to be physically active. The nearest park is located 8 miles away in the city of Dinuba, survey results show that in London, children play in the street or do not play outside at all. London residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.



(Pictured above is storm water collecting in London, CA)

## **Affordable Housing**

London residents want new affordable housing to be built in town. Many people expressed no new homes have been built in recent years and that they would buy a new home in town if it were available to them. Residents would like to see growth in town and feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Dinuba or Visalia.

## **Water**

The London Community Services District water system does not have sufficient source water capacity to meet the California Waterworks Standards contained in Title 22. Based on the Title 22 standards and as identified in the report by the Department of Health Services, the minimum water source capacity for London Community Services District should be 1,000 gpm. Currently, the District is using two (2) wells (2A and 4) with a total probable combined capacity of 740 gpm. There are several instances when the water system has low pressure caused by inadequate supplies. The existing water system has no water storage. Well No. 1 is dry and failure of the other shallow Well No. 3 could occur at any time. If this occurs, this would cause the District's lack of adequate water supply to be even more severe. Well #2A is an open: 1 bottom cable tool well which is not gravel packed and develops

# LONDON COMMUNITY PLAN

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water through mining of a cavity in the aquifer at the bottom of the well casing. The well is subject to failure inside the well casing due to age or a collapse of the mined opening at the bottom of the casing. The collapse of the cavity on the well has occurred in the past. London CSD has recently completed a State Revolving Fund planning application for a new well, storage tank, and distribution system and has also applied for emergency funds to drill the new well due to drought impacts.

## **Public Services**

Sheriff –London is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of London and the nearest Sheriff substation is located in Visalia, 12 miles away.

Fire –Tulare County Fire, Dinuba 8 miles

Schools – Kings River School 6 miles, Dinuba High School 8 miles Libraries – Dinuba, 8 miles away

Parks – Dinuba, 8 miles away



# LONDON COMMUNITY PLAN

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## LONDON TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Community's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Community plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Community allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Community. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller Community such as London.

#### Population

In 2015, the population of London was 2,056 (see **Table2**).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>London</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	1,122	54.6%
Female	19,334,329	50.3%	226,607	49.9%	934	45.4%
Total	38,421,464		454,033		2,056	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

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<sup>1</sup> Tulare County Regional Blueprint, page 7

# LONDON COMMUNITY PLAN

## Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project London's population during the Planning Period (see Table 3). At this rate, the population is projected to grow by 407 persons by 2030.

<b>Table 3 - Projected Annual Growth Rate</b>		
	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

## Median Age

London's population is younger than the median age throughout all of Tulare County. London's median age of 24.8 is lower than the median age of the State of California (see Table 4).

<b>Table 4 - Median Age</b>	
<b>Geography</b>	<b>Median Age (years)</b>
California	35.6
Tulare County	30.1
London	24.8

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 7.1% of London's population was White, 0.3% was African American, 0.1% was Native American, 0.1% was Asian, and 0% was two races or more (see Table 4). Approximately 92.4% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community (see Table 5).

<b>Table 5 - Race &amp; Ethnicity</b>						
	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>London</b>	<b>%</b>
Total	38,421,464		454,033		2,056	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	1,900	92.4%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	145	7.1%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	6	0.3%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	2	0.1%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	3	0.1%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates

# LONDON COMMUNITY PLAN

## ECONOMIC CONDITIONS

### Employment in London

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see Table 6).

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for London was about 10.4% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 10.4% includes only the employable labor force (that is, not every person of the population) results in about 146 unemployed persons of London's person labor force of 786.

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>London</b>
Population 16 years & over	30,312,429	325,404	1,403
In labor force	19,269,449	194,420	786
Civilian labor force	19,137,441	194,102	786
Employed	17,246,360	170,780	640
Unemployed	1,891,081	23,322	146
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	617

2011-2015 American Community Survey 5-Year Estimates

### Median Household Income

London median household income was \$24,491, compared to \$61,632 for the State of California and \$42,031 for Tulare County (see Table 7).

<b>Table 7 - 2011-2015 American Community Survey Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$30,318
<b>Tulare County</b>	\$42,031	\$44,814	\$17,876
<b>London CDP</b>	\$24,491	\$24,286	\$7,973

### Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, London's median household income was \$24,491, whereas the State of California's median household income was \$61,818. London's median household income was 39.6% (40%) of the State of California's median household income, and therefore it is considered a severely disadvantaged community.



# LONDON COMMUNITY PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

Table 8 - Housing Tenure			
	California	Tulare County	London
Occupied housing units	12,717,801	133,570	475
Owner-occupied	6,909,176	75,685	190
Renter-occupied	5,808,625	57,885	285

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	London
Owner-occupied	54.3%	56.7%	40%
Renter-occupied	45.7%	43.3%	60%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
London	8	9	10	11	21	24	17	20	31	36	87

(Source: Tulare County 2015 Housing Element)

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in London indicated that approximately 9% of the housing units were sound. Approximately 36% of London's housing units were deteriorated and 20 were dilapidated (see Table 10).

# LONDON COMMUNITY PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009 (see Table 11). The total number of housing units in London is 481.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	London
Total housing units	13,845,790	144,792	481
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	46
Built 1990 to 1999	1,495,571	21,767	73
Built 1980 to 1989	2,117,819	22,733	41
Built 1970 to 1979	2,503,688	27,111	126
Built 1960 to 1969	1,871,029	15,500	24
Built 1950 to 1959	1,907,512	13,694	86
Built 1940 to 1949	865,607	7,494	50
Built 1939 or earlier	1,298,438	8,186	35

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in London was 4.27 persons per household (see Table 12). The average owner occupied household size is 4.41

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
London	4.41	4.27

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical;

however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in London was 2.1%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
London	0.0%	2.1%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See:

[http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# LONDON COMMUNITY PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

A California Natural Diversity Database (CNDDDB) search conducted on November 8, 2016 indicates there are special status species within the Reedley Quadrant Species List (which includes the London Planning Study Area) consisting of one animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

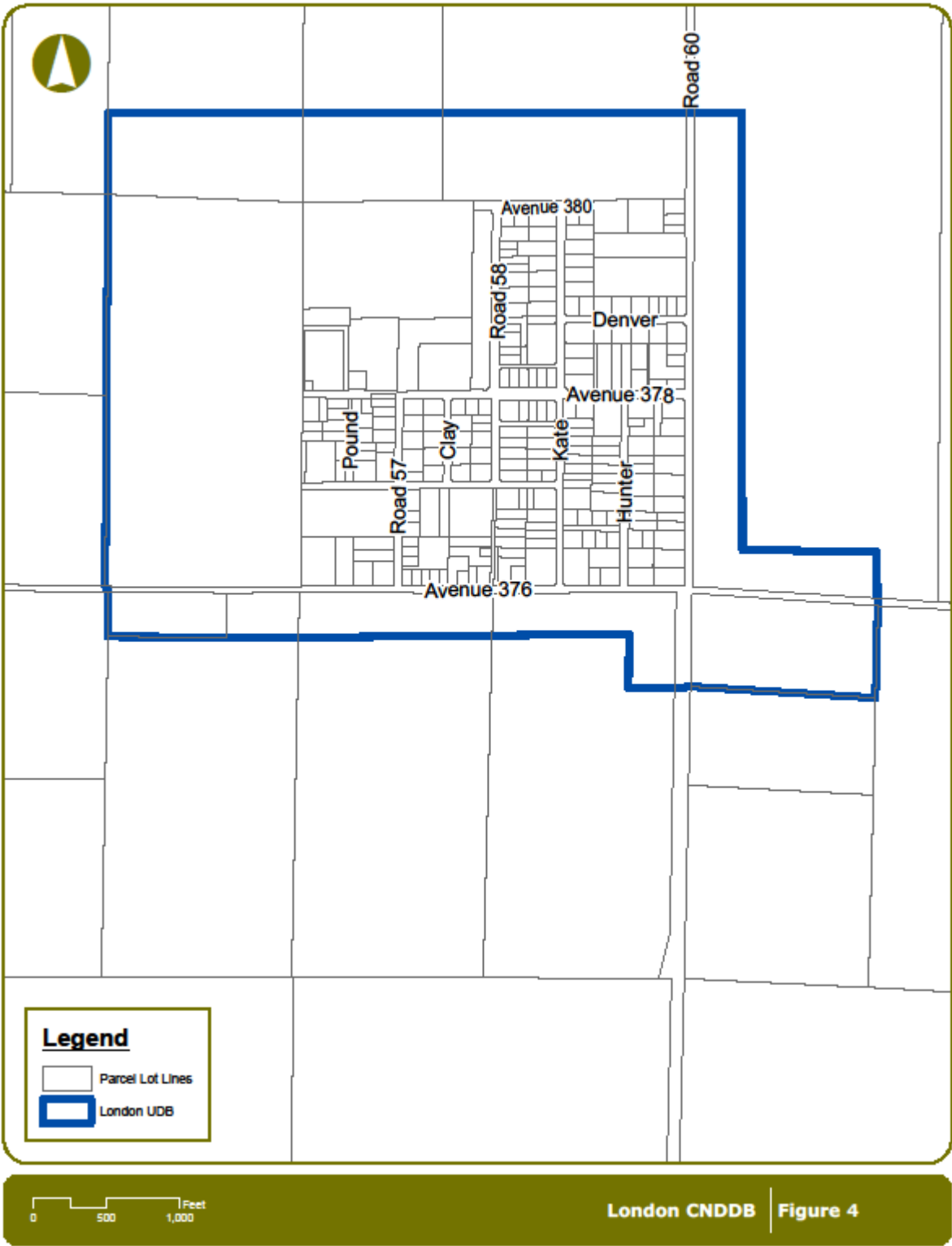
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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. November 8, 2016

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# LONDON COMMUNITY PLAN

Figure 4 - CNDDDB Map



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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect London are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [London] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal.

The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in London (see Figure 3). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Urban Development Boundary (UDB). The following descriptions are provided for the above soil types:

**Calgro-Calgro, saline-Sodic, complex, 0 to 2 percent slopes,** Erosion potential is low to moderate and shrink swell potential low.

**Hanford sandy loam, 0 to 2 percent slopes,** is very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains and alluvial fans; negligible to medium runoff; moderately rapid and moderate permeability, however saline-sodic phases and soils with sandy clay loam substratums have moderately slow permeability. This soil is suitable for growing a wide range of fruits, vegetables, and general farm crops. They are also used for urban development and dairies.

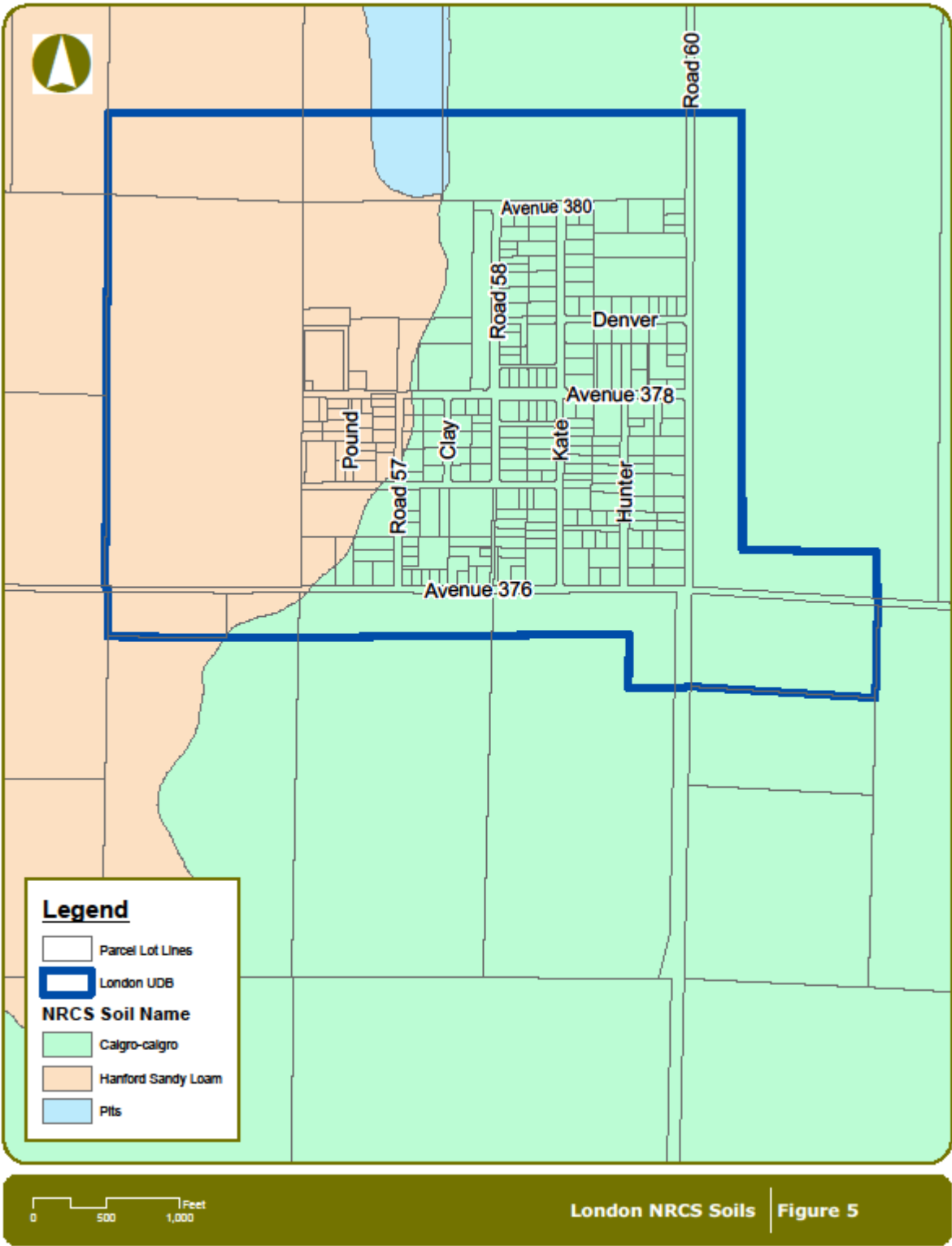
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database, accessed November 3, 2016.

# LONDON COMMUNITY PLAN

Figure 5 - NRCS Soil Map



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## PHYSICAL CONDITIONS

### Air Quality

The London Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies
- Adopt Local Air Quality Mitigation Fee Program
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations
- Fugitive dust regulation enforcement coordinated with SJVUAPCD
- Energy efficiency incentive programs
- Local alternative fuels programs
- Coordinate location of land uses to separate odor generators and sensitive receptors

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because London is located near the southwestern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of

# LONDON COMMUNITY PLAN

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adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of London including State Highway 201 and agricultural activities.

## **Flooding<sup>6</sup>**

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0605E, all portions of London are located within Flood Zone X, which is defined by FEMA as “Areas determined to be outside the 0.2% annual chance floodplain.” (See Figure 6). Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Teviston there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

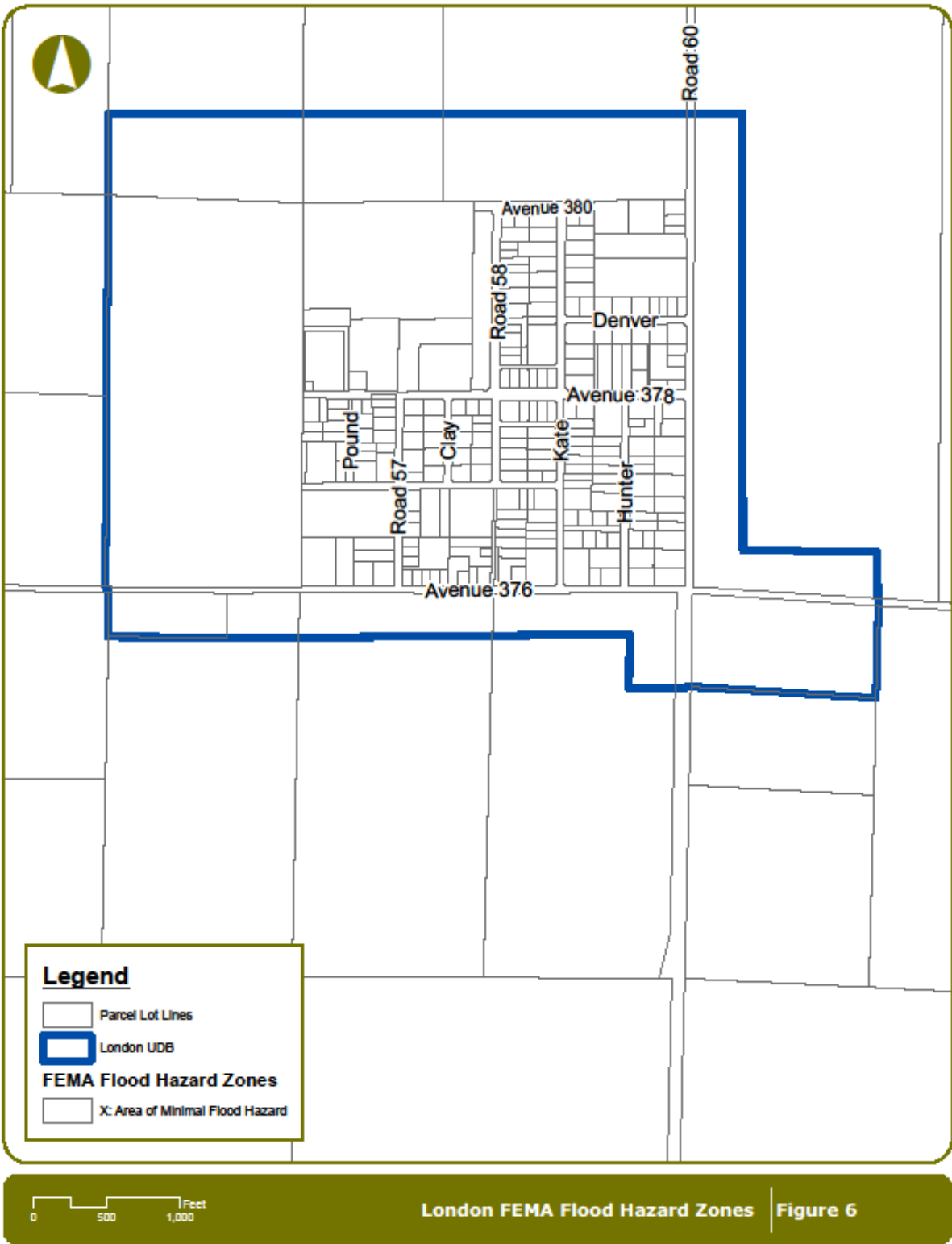
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<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0605E, Panel Number 605 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>. Accessed August 17, 2016.



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Figure 6 - FEMA Flood



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## Noise<sup>7</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan background Report London lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regard to London, this Community Plan is intended to address deficiencies, and the need for improvements to the drinking water system improvements; (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks, etc.

## Energy: Natural Gas/Electricity

PG&E provides service to London.

## Water

The London Community Services District water system does not have sufficient source water capacity to meet the California Waterworks Standards contained in Title 22. Based on the Title 22 standards and as identified in the report by the Department of Health Services, the minimum water source capacity for London Community Services District should be 1,000 gpm. Currently, the District is using two (2) wells (2A and 4) with a total probable combined capacity of 740 gpm. There are many periods of time when the water system has low pressure caused by inadequate supplies. The existing water system has no water storage. Well No. 1 is dry and failure of the other shallow Well No. 3 could occur at any time. This would cause the District's lack of adequate water supply to be even more severe. Well #2A is an open: 1 bottom cable tool well which is not gravel packed and develops water through mining of a cavity in the aquifer at the bottom of the well casing. The well is subject to failure in the well casing because of age or a collapse of the mined opening at the bottom of the casing. The collapse of the cavity on the well has occurred in the past. London CSD has recently completed a State Revolving Fund planning application to prepare for a new well, storage tank, and distribution system and has also applied for emergency funds to drill the new well due to drought impacts.

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<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# LONDON COMMUNITY PLAN

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## Domestic Water & Wastewater

Domestic water and sewer service in London is provided by the London Community Service District (CSD), formed in March 1952. **Table 14** shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Housing Element, May 2012). **Figure 7** graphically displays the approximate location of water wells and water lines. **Figure 8** graphically displays the approximate location of the sewer system and wastewater treatment plant.

According to the Municipal Service Review 2006 (MSR), the London CSD operates a water supply and distribution system under the jurisdiction of the California Department of Health Services Division of Drinking Water and Environmental Management, which is responsible for the administration and enforcement of the Safe Drinking Water Act involving those systems in California with more than 200 connections. London CSD staff has indicated that there are approximately 430 connections to the District's water system, which consists of three active wells and one hydro-pneumatic pressure tank. The water system has no permanently installed treatment at this time.

CSD staff has indicated that the water system was constructed in 1952 and experiences minor leaks. Water system leaks have the potential for causing cross contamination problems. The London CSD received Proposition 13 funding in the amount of \$98,156 to prepare an infrastructure rehabilitation feasibility study to detect and evaluate leaks and determine the feasibility of replacing the distribution system. The CSD is currently pursuing funding through the State Revolving Fund Program for construction of a new domestic water well and hydro-pneumatic tank, along with distribution system improvements.

The London CSD water system is currently un-metered, which does not promote water conservation. The CSD should consider evaluating the potential water savings and the projected total cost to water users in the community resulting from the installation of water meters. The CSD would likely need funding assistance through state and/or federal grant/loan programs to install water meters. User fees would also likely need to be increased. A fully metered water system could serve as a water conservation measure by minimizing over usage and/or wasting of water.

Assuming 430 equivalent dwelling units (EDUs), in order to meet Tulare County Improvement Standards the London CSD water system would need to be capable of delivering a combined flow rate (from all source and storage facilities) of 1,120 gallons per minute (GPM) (500 GPM fire flow, and 620 GPM domestic demand) for a period of two hours while maintaining a minimum pressure of 25 pounds per square inch (PSI) to each lot served; The total pumping efficiency of the CSD's water supply sources is unknown. Prior to granting any sphere of influence (SOI) expansions, it is recommended that Local Agency Formation Commission (LAFCO) verify that there is adequate water system capacity to meet any anticipated increased demands. It is likely that the CSD would need to expand its water supply and improve the distribution system to support any significant development projects proposed within its SOI.

The London CSD is also responsible for providing sanitary sewer service to residents within its Boundary. London CSD staff has indicated that there are approximately 430 connections to their sewer system. The District owns and operates a Wastewater Treatment Facility (WWTF) southeast of the community, which is operated under the provisions of Waste Discharge Requirements Order No. 96-172, issued by the Regional Water Quality Control Board (RWQCB).

# LONDON COMMUNITY PLAN

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Order No. 96-172 prescribes that the monthly average discharge flow shall not exceed 0.3 million gallons per day (MGD). Available data indicates that the current flow at the WWTF is 0.20 MGD. The CSD's Engineer noted that improvements completed in 2000 with US Department of Agriculture (USDA) Rural Development funding increased the plant's capacity to 0.50 MGD. The London CSD should work with the RWQCB to get the CSD's Waste Discharge Requirements (WDR) Order updated. According to WDR Order No. 96-172, the London CSD has not assessed growth in the community and has not predicted future flows. As such, the London CSD has not made any plans on increasing the capacity of the WWTF for future flows.

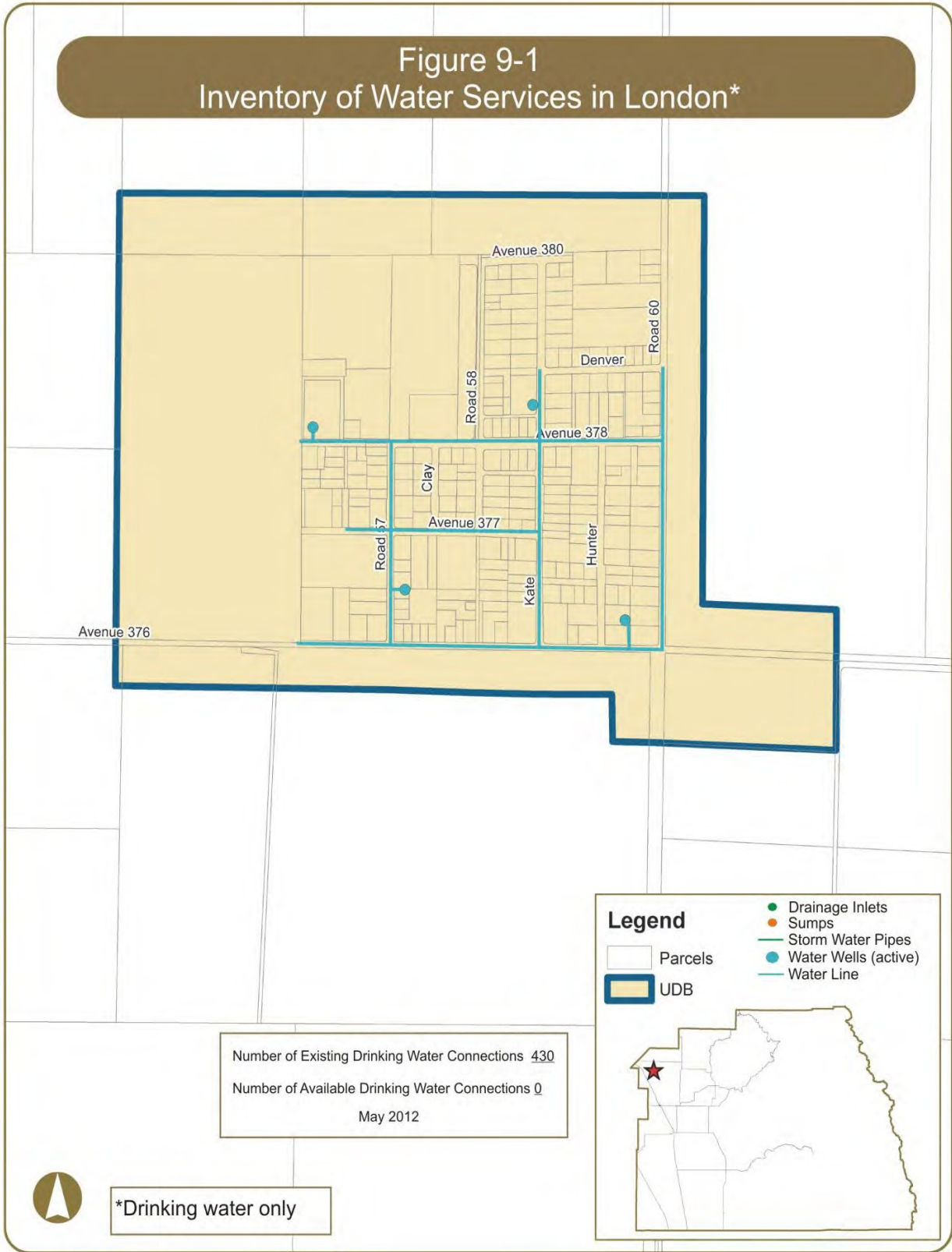
**Table 14 - Existing Water and Wastewater Connections**

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
430	430	0	430	1,075	645

(Data current as of May 2012)

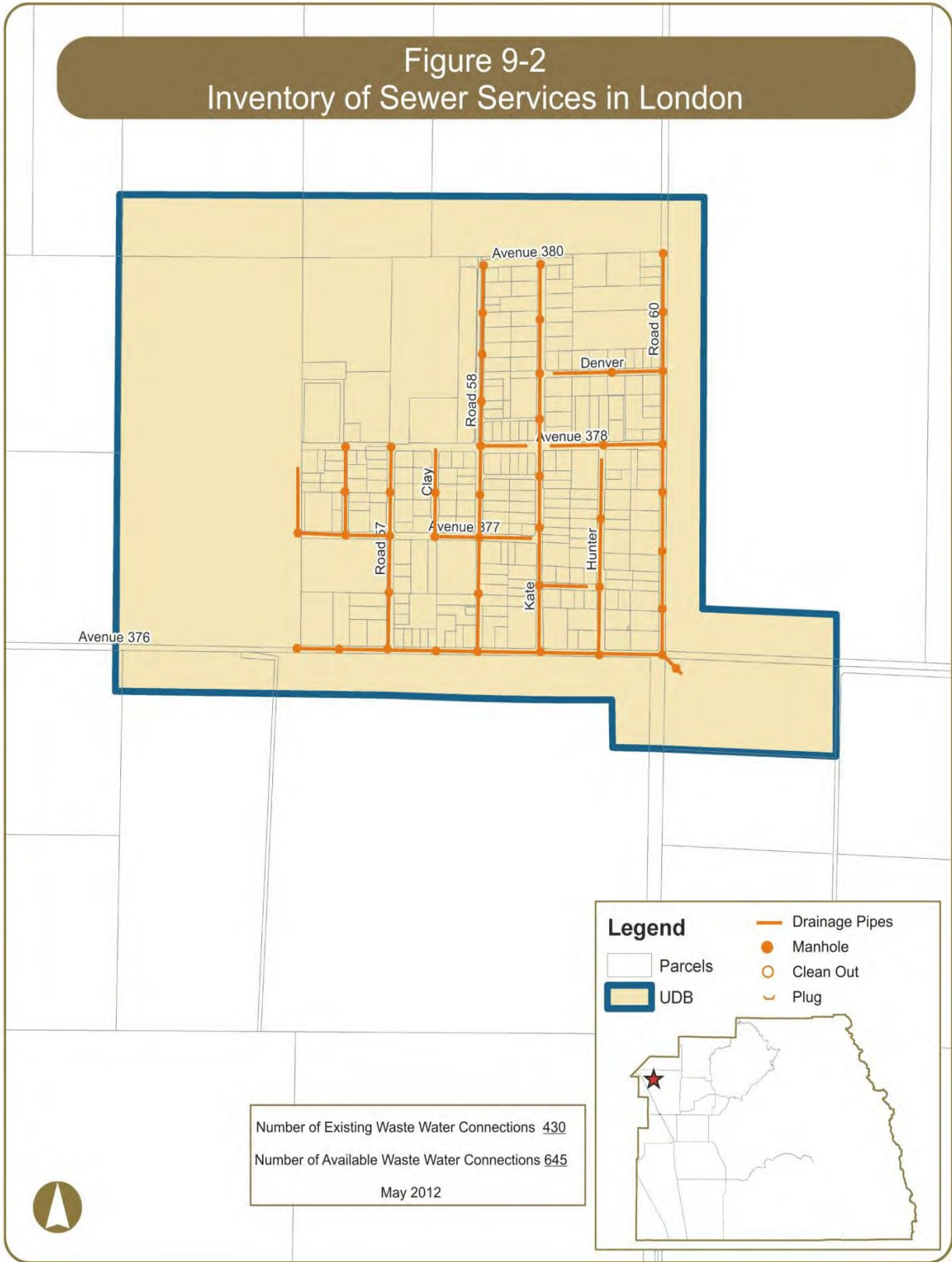
# LONDON COMMUNITY PLAN

Figure 7 - Inventory of Water Services in London



# LONDON COMMUNITY PLAN

Figure 8 - Inventory of Sewer Services in London





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## Solid Waste

Solid waste disposal services for the Community of London is provided by Pena's Disposal, a private company. Solid waste generated in London can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia.

## **PUBLIC SERVICES**

### Sheriff

Police protection services are provided in London by the Tulare County Sheriff's Department substation, located at 40765 Road 128, in Cutler, approximately 20 miles northeast of London. Oroshi Substation covering 289 square miles and serving a rural population to include the unincorporated communities of Cutler, New London, Oroshi, Seville, Sultana, Traver and Yettem. The Substation runs a four shift operation which includes 23 deputies, four sergeants and one lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. In addition to that general shift staffing the communities of Cutler, Oroshi and New London/Traver have assigned Community Based Officers assigned specifically to those area. The substation is open for walk-ins from 8 am to 5 pm Monday thru Friday. After hours and weekends there is a phone provided outside the substation that calls directly into the dispatch center. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

### Fire

Fire protection and emergency medical services are provided for London by the Tulare County Fire Department. The Tulare County Fire Station #2, Kings River Station, is located approximately five miles west of London in Kingsburg, CA. The Kings River Station has one (1) Fire Engine and it is staffed with one (1) Company officer on a forty eight (48) hour shift. This station has three (3) Company officers assigned to it, two (2) Fire Lieutenants and one (1) Fire Captain. Eight (8) Paid On-Call Fire Fighters are assigned to this station and they respond when called or toned out to an incident. Response time is approximately seven (7) minutes from this station to London.

Twenty-nine fire hydrants are found within London (**see Table 15**). These fire hydrants are located within the County right-of-way. **Figure 9** display Existing Fire Hydrants in London.

# LONDON COMMUNITY PLAN

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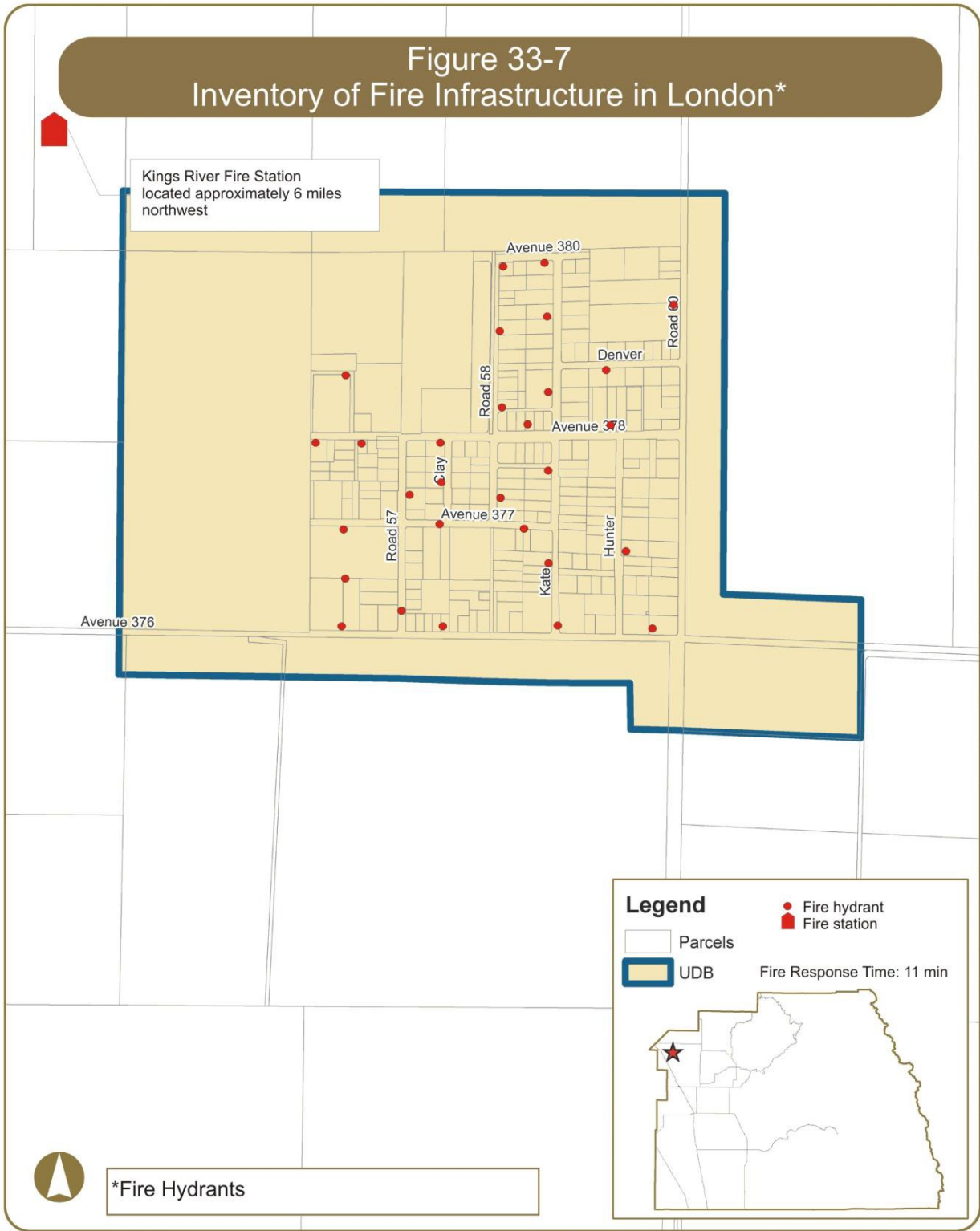
**Table 15 - Location of Existing Fire Hydrants**

Existing Fire Hydrants	
No.	Location
1	Avenue 380 and Road 58
2	Avenue 380 and Kate Road
3	Road 60 north of Denver Avenue
4	Kate Road north of Denver Road
5	Road 58 south of Avenue 380
6	Denver Avenue east of Kate Road
7	Kate Road north of Avenue 378
8	Road 58 north of Avenue 378
9	Avenue 378 east of Kate Road
10	Avenue 378 east of Road 58
11	Kate Road south of Avenue 378
12	Road 58 north of Avenue 377
13	Avenue 377 east of Road 58
14	Kate Road south of Avenue 377
15	Hunter Road south of Avenue 378
16	Avenue 376 east of Hunter Road
17	Avenue 376 and Kate Road
18	Pound Road north of Avenue 378
19	Avenue 378 west of Pound Road
20	Avenue 378 and Pound Road
21	Avenue 378 and Clay Road
22	Clay Road south of Avenue 378
23	Road 57 north of Avenue 377
24	Avenue 377 and Clay Road
25	Avenue 377 west of Pound Road
26	Pound Road south of Avenue 377
27	Avenue 376 west of Road 57
28	Road 57 north of Avenue 376



# LONDON COMMUNITY PLAN

Figure 9 - Inventory of Fire Infrastructure in London



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## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The London Community Plan Area is within the Kings River Union Elementary School District in Kingsburg, which provides K-8 grade education. High School students attend Kingsburg High School (Kingsburg Joint Union High School).

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 15 regional libraries and one main branch”<sup>8</sup> (see Table 16).

Table 16 - Library Location & Hours		
Branch	Address	Service Hours (2017)
London	London Branch 5711 Avenue 378 London, CA 93618	Wednesday & Friday 9:00 a.m. – 12:00 p.m. 1:00 p.m. – 6:00 p.m.

Library hours current as of September 2017

## Parks

In the community of London there is no safe green space for children to play and adults to be physically active. There are no County owned/operated parks in Traver. The nearest County owned/operated parks are the Ledbetter Park in the unincorporated community of Cutler approximately 12.5 miles northeast of Traver and the Kings River Nature Preserve located 2 miles East of Hwy 99 on Road 28.

# THE LONDON COMMUNITY PLAN

## CIRCULATION/COMPLETE STREETS/TRANSPORTATION

### Existing Circulation

London is a small agricultural community located near State Route 201. The major rural collector roads is Avenue 308. There are no proposed major streets as part of this Community Plan.

### Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

### Traffic

The 2014 Regional Transportation Plan and projections for Year 2040 traffic volumes do not exceed the LOS “C” threshold. Less than significant impacts will occur to SR 65 as a result of the Project primarily because increased traffic loads on SR 65, and its various entry and exit point, is more associated with growth in the Central Valley and the increased need for Californians to be able to

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<sup>8</sup> General Plan Background Report, page 7-96

# LONDON COMMUNITY PLAN

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travel north and south. As shown in Table 22, both the Highway Capacity Manual (HCM) and the Tulare County General Plan (TCGP) LOS remain at B or C through Year 2040 projections while the build-out time frame of the Community Plan is Year 2030.

The proposed Circulation Element and Complete Street Policies of the General Plan contain policies and actions that will continue to maintain LOS C or better. The Plan's projected growth and its impact on the circulation system will continue to be less than significant as the additional goals and policies are implemented through the Community Plan. These policies and actions will be implemented by the County of Tulare over a 20 year time period. Mitigating the traffic congestion associated with State Route 65 will be the responsibility of Caltrans given that traffic on SR 65 that travels along this route originates outside of London as pass-by traffic. By implementing the policies and actions contained in the Community Plan, the traffic and circulation impacts directly associated with growth in London will be accommodated.

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Roads

“There are several roadways in London that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

# LONDON COMMUNITY PLAN

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads”<sup>9</sup>

**Table 17** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 10** graphically displays this information on a map.

**Table 17 - Road Maintenance Strategies**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 376	Road 57 to Hunter Road	CHIP
2	Avenue 378	Kate Road to west end	GRX
3	Avenue 378	Kate Road to Road 60	CHIP
4	Avenue 380	Road 58 to Kate Road	GRX
5	Kate Road	Avenue 376 to Avenue 378	GRX
6	Kate Road	Avenue 378 to Avenue 380	CHIP
7	Road 60	Avenue 376 to Avenue 378	CHIP

OLAY – overlay resurfacing operation  
 CHIP – chip seal  
 Reconstruction  
 GRX – grind and remix

ACST – asphalt reconstruction  
 RCST – cold mix reconstruction

(Source: County of Tulare Public Works, 2012)

## Street Lights

Lights are typically located at the edge of roadways on the top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

**Table 18** identifies the location of existing street lights that are maintained by Tulare County, in London, as well as their specifications. **Figure 10** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.”<sup>10</sup>

<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

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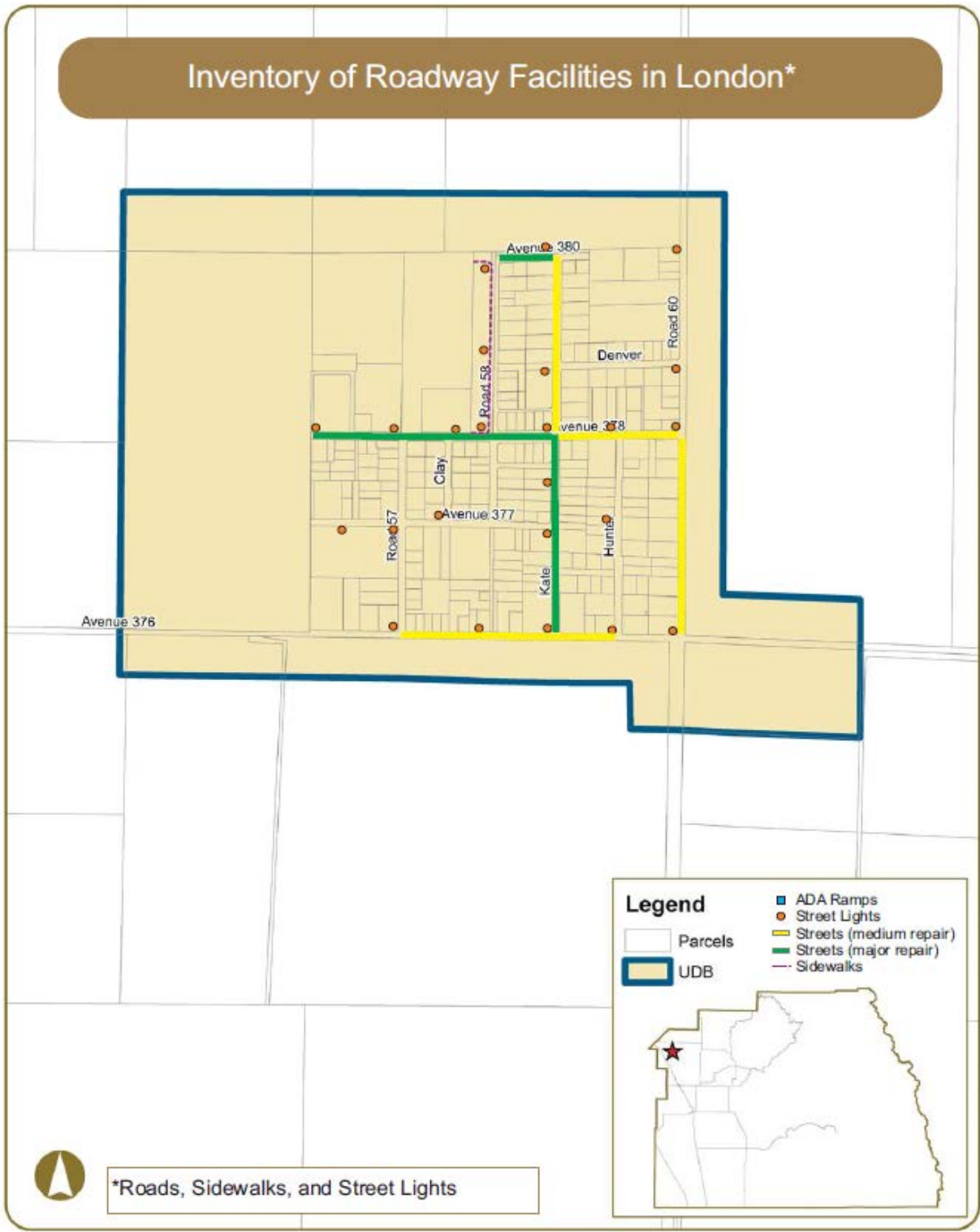
**Table 18 - Existing Street Lights**

<b>Specifications of Existing Street Lights</b>								
<b>No</b>	<b>East-West Roadway</b>	<b>North-South Roadway</b>	<b>Location</b>	<b>Pole</b>	<b>Lumens</b>	<b>Pole Type</b>	<b>Arm Direction</b>	<b>Utility</b>
1	Avenue 376	Road 57	NW Corner	1474	5800	W	S	PG&E
2	Avenue 376	Between Road 57 and	North Side	1475	5800	W	S	PG&E
3	Avenue 376	Kate Road	NW Corner	1464	5800	W	S	PG&E
4	Avenue 376	Road 60	NW Corner	1465	5800	W	S	PG&E
5	Avenue 376	Hunter Road	NW Corner	3635	5800	W	S	PG&E
6	Avenue 377	Hunter Road	West Side	1455	5800	W	E	PG&E
7	Avenue 377	Road 57	SW Corner	1472	5800	W	E	PG&E
8	Avenue 377	Between Road 57 and	South Side	54	5800	W	N	PG&E
9	Avenue 377	Road 58	SW Corner	1469	5800	W	N	PG&E
10	Avenue 377	Kate Road	SW Corner	1463	5800	W	E	PG&E
11	Avenue 377	Clay Road	West Side	3325	5800	W	N	PG&E
12	Avenue 378	West end	North Side	1473	5800	W	S	PG&E
13	Avenue 378	Road 57	NW Corner	1471	5800	W	S	PG&E
14	Avenue 378	Road 58	NW Corner	1468	5800	W	S	PG&E
15	Avenue 378	Kate Road	NW Corner	1461	5800	W	S	PG&E
16	Avenue 378	Hunter Road	NW Corner	1478	5800	W	S	PG&E
17	Avenue 378	Road 60	NW Corner	1466	5800	W	E	PG&E
18	Avenue 378	Clay Road	NE Corner	3324	5800	W	S	PG&E
19	Avenue 380	Road 58	SW Corner	1477	5800	W	E	PG&E
20	Avenue 380	Kate Road	NW Corner	1476	5800	W	S	PG&E
21	Avenue 380	Road 60	West Side	1479	5800	W	E	PG&E
22	Between Avenue 377 and Avenue 378	Kate Road	West Side	1480	5800	W	E	PG&E
23	Between Avenue 378 and Avenue 380	Road 58	West Side	1470	5800	W	E	PG&E
24	Denver Avenue	Kate Road	SW Corner	1462	5800	W	E	PG&E
25	Denver Avenue	Road 60	SW Corner	1467	5800	W	E	PG&E

(Source: Tulare County Public Works, March 2013)

# LONDON COMMUNITY PLAN

Figure 10 - Inventory of Roadway Facilities



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## Sidewalks

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles (see Table 19).

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

Table 19 - Location of Existing Sidewalk			
No.	Roadway	Limits	Location
1	Road 58	Avenue 378 to Avenue 380	West Side

(Source: County of Tulare Public Works and VRPA Technologies, February 2014)

## ADA CURB RAMPS

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within London.”<sup>11</sup>

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facility.

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<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element



# LONDON COMMUNITY PLAN

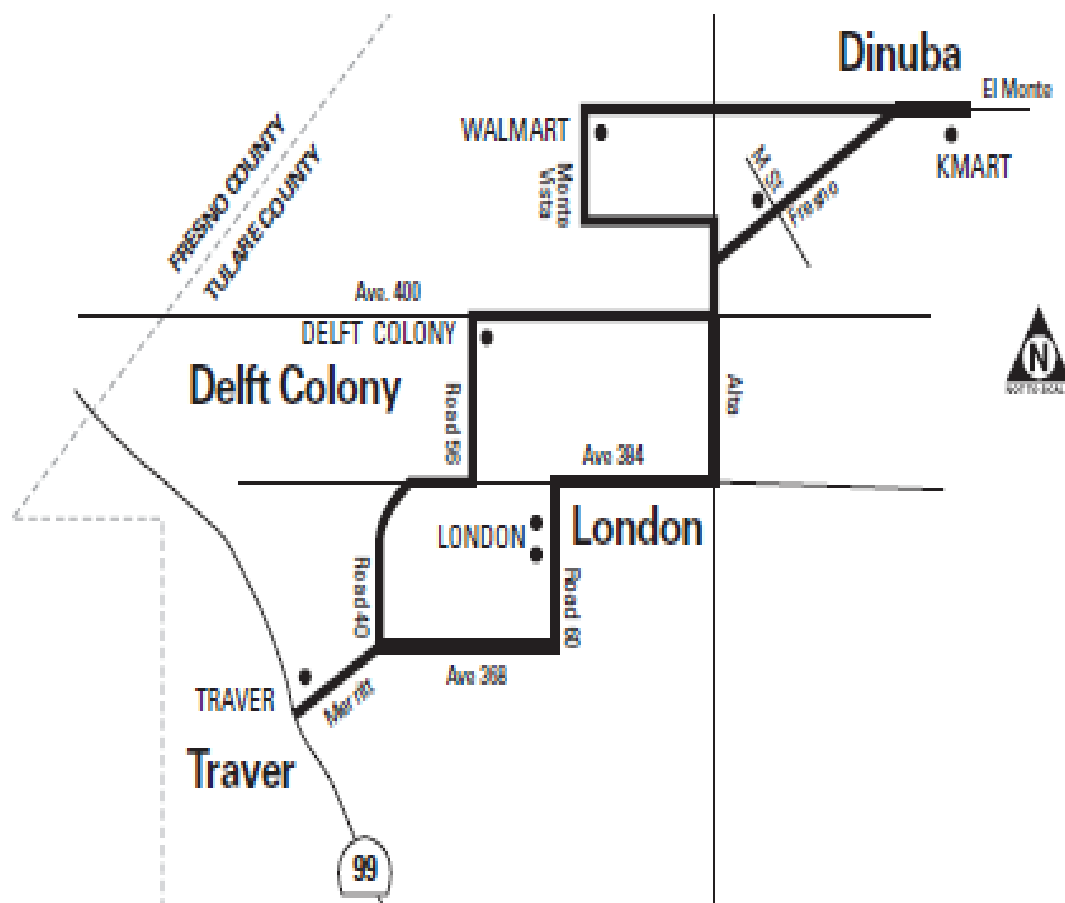
## Transit and Bus Stops

County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. London is currently connected via TCAT Route 50 to Dinuba and its transit center (see Figure 11). [www.ridetcat.org](http://www.ridetcat.org) TCAT Route 50 provides service to Dinuba four times per day on weekdays and four times on Saturday. In Dinuba, transfers can be made to connect to Visalia, Tulare, and the remainder of the TCAT public transit system. TCAT vehicles are wheelchair accessible and all full size buses include bike racks. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday.

Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in London, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

Figure 11 – TCAT – Route 50 Transit





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## Bicycle Facilities:

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified Road 80 as a proposed Class III Bike Project<sup>12</sup> In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is Road 80.

## State Route 99

State Route (SR) is the major highway between cities within the Central Valley and the primary route between the City of Fresno to the north and the City of Bakersfield to the south. London is located approximately five (5) miles east of SR 99.

## AMTRAK

The Hanford AMTRAK station, located approximately 30 miles to the southwest in Kings County, is the closest station to London providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport is approximately 30 miles south. Fresno Yosemite International Airport (FAT), approximately 30 miles northwest of London, is a principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles southeast of London and offers direct flights to several destinations

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<sup>12</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# LONDON COMMUNITY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the London Community Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Community Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for London to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

London Community Plan, and input received from London citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.

2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
4. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
5. The County shall work with the Schools to provide safe routes to school.
6. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the London Community Plan.
7. The London Urban Development Boundary should be reviewed every five years to determine if amendments are appropriate.
8. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

# LONDON COMMUNITY PLAN

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## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in London.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in London.
3. Coordinate residential zoning with availability of utilities and community services.

4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## Economic Base

**GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

# LONDON COMMUNITY PLAN

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## Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

## Policies:

1. Encourage the London to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

## Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## **Environmental Quality and Public Safety**

**GOAL I: Preserve and enhance the quality of life for present and future generation of London citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

## Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

## Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Protect Agricultural Lands: Land within the respective London, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;

# LONDON COMMUNITY PLAN

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- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of London.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of London.

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## ASSESSMENT OF LAND NEEDS

Within the existing 384.2-acre London Urban Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 215 acres or 55 percent (55%) of the UDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2014 population of 2056 by 169.2 urbanized acres (area within the UDB), which equals 12 persons per urbanized acre (**see Tables 21 and 22**) showing population projections). Projecting the population at a 1.3% growth rate adds 407 persons in year 2030. Multiplying the 145 persons by the 1/12 ratio (8.2%) suggests an additional 12 acres of land will be required to accommodate development by the year 2030 if projections are realized.

### Population Growth Forecast

Table 21 - London Population Projections			
Growth Rate	2014	2020	2030
0.013	2056	2165	2464

### Demand Forecast

To determine whether there is enough land within the exiting UDB to accommodate anticipated growth within the community, the population growth and land use projections in Year 2030 were compared to the vacant land available within the UDB.

### Population and Housing Units

The year 2015 baseline population was determined by projecting the 2014 American Community Survey (Survey)<sup>13</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in Year 2013 the community had 418 dwelling units (including vacant dwellings) with a population of 2,056. At an annual growth rate of 1.3%, the projected housing units are 440 and 501 in years 2015 and 2030, respectively, and projected population is 2,156 and 2,464 in years 2015 and 2030, respectively.

There are approximately 215 acres of undeveloped lands within the existing UDB. As such, there is adequate land available for development within the existing UDB to accommodate future residential growth through the Year 2030.

Table 20 - Population Projects		
Year	Population	Growth (%)
2016	2,056	0.013
2017	2,083	0.013
2018	2,110	0.013
2019	2,137	0.013
2020	2,165	0.013
2021	2,193	0.013
2022	2,222	0.013
2023	2,251	0.013
2024	2,280	0.013
2025	2,309	0.013
2026	2,339	0.013
2027	2,370	0.013
2028	2,401	0.013
2029	2,432	0.013
2030	2,464	0.013

<sup>13</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

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## Commercial and Industrial Use

There are currently 0 acres within the UDB that are developed for commercial uses and 0 acres developed for industrial uses. However, we are aware of non-conforming commercial, industrial and public / quasi-public uses within the Community of London. Assuming that the land area needed to accommodate future commercial and industrial growth is consistent with annual population growth projections, the land use growth projections are also assessed at an annual growth rate of 1.3%. At this growth rate, there will be a total of approximately 1 acre of commercial uses and approximately 1 acre of industrial uses in the year 2030. As such, there is adequate land available for development within the existing UDB to accommodate future commercial and industrial growth through the Year 2030.

## **ECONOMIC DEVELOPMENT**

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community

Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET. Based on the 2011-2015 American Community Survey (see Table 22), the educational barrier in London begins in grade school. Of the adults age 25 and older, 37.5% had an educational level of less than 9<sup>th</sup> grade.

Table 22 - Education Attainment			
Educational Attainment	California	Tulare County	London
Population 25 years and over	25,257,858	260,964	32
Less than 9th grade	10.0%	20.8%	37.5%
9th to 12th grade, no diploma	8.2%	10.9%	0.0%
High school graduate (& equivalency)	20.7%	25.2%	0.0%
Some college, no degree	21.8%	22.3%	62.5%
Associate's degree	7.8%	7.0%	0.0%
Bachelor's degree	19.8%	9.5%	0.0%
Graduate or professional degree	11.6%	4.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates



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This lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment needs to begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

## Health Care

Health care is important for economic development as businesses need healthy employees. There are no medical offices in London. The nearest medical offices are in Kingsburg, Visalia, and Dinuba.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Urban Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 23** and **Figure 11**) and Zoning Plan map (see **Table 24** and **Figure 12 & 13**) have been updated.

<b>Table 24 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	355.3
Unclassified (Right-of-Way)	28.9
<b>TOTAL</b>	<b>384.2</b>

<b>Table 23 - Proposed Zoning</b>	
Proposed Zones	Acres
A-1	31.8
AE-40	83.1
C-2-MU	17.4
P-O	0.4
R-1	4.3
R-2	1.4
R-2-MU	6.2
R-3	4.8
R-A	206.0
Unclassified (Right-of-Way)	28.9
<b>TOTAL</b>	<b>384.2</b>

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations contained in the London Community Plan. The following components comprise the London Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the London Community Plan, there are a variety of changes to existing zoning districts. These changes are described below.



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## Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision-making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

## Mixed Use Overlay District

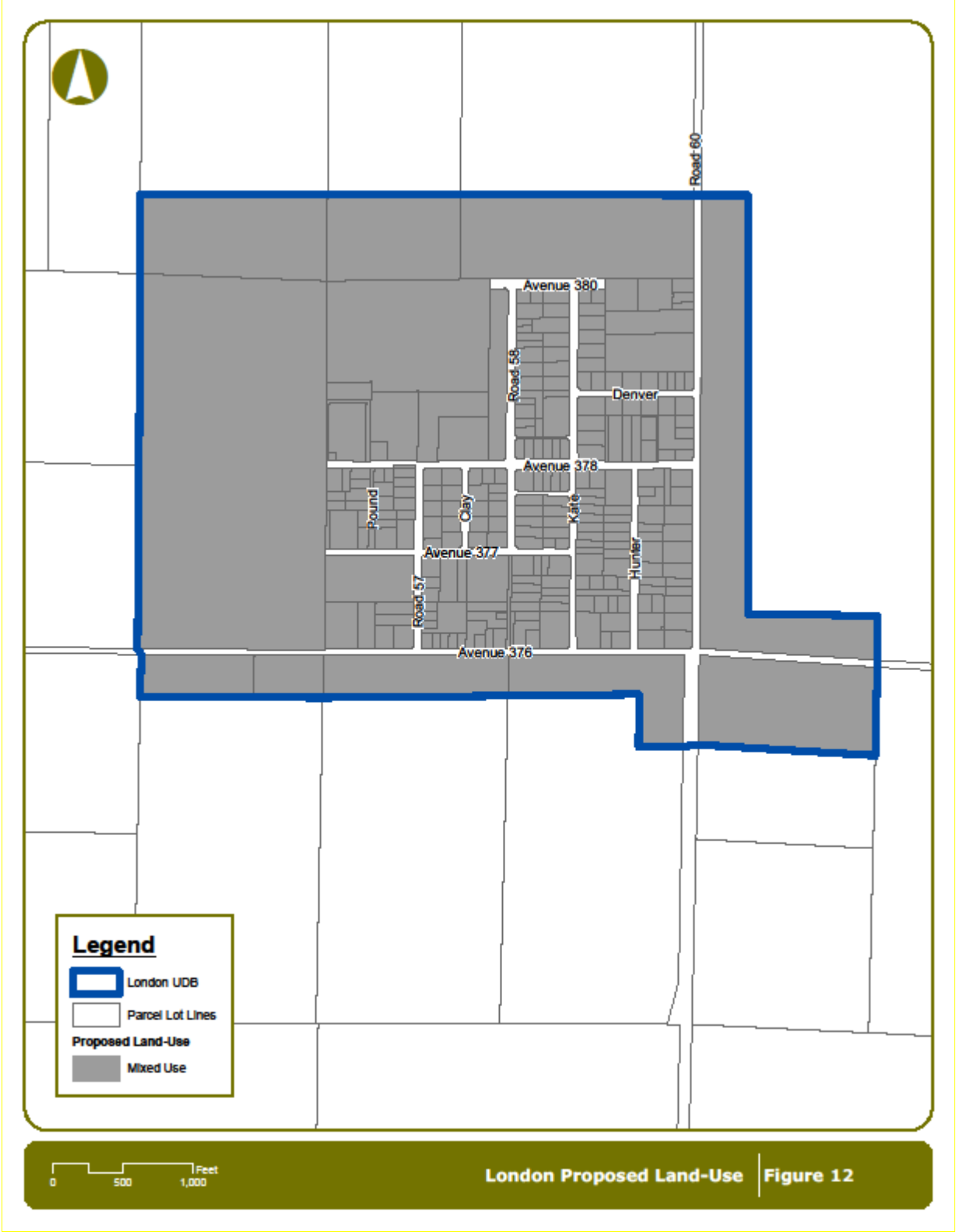
This alternative involves the creation of a Mixed-Use Zoning Designation for the Community of London.

## Zoning Map Update

The current Zoning Map for London will be amended to be compatible with the Land Use Map outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other **(See Figure 1)** London Proposed Zoning District.

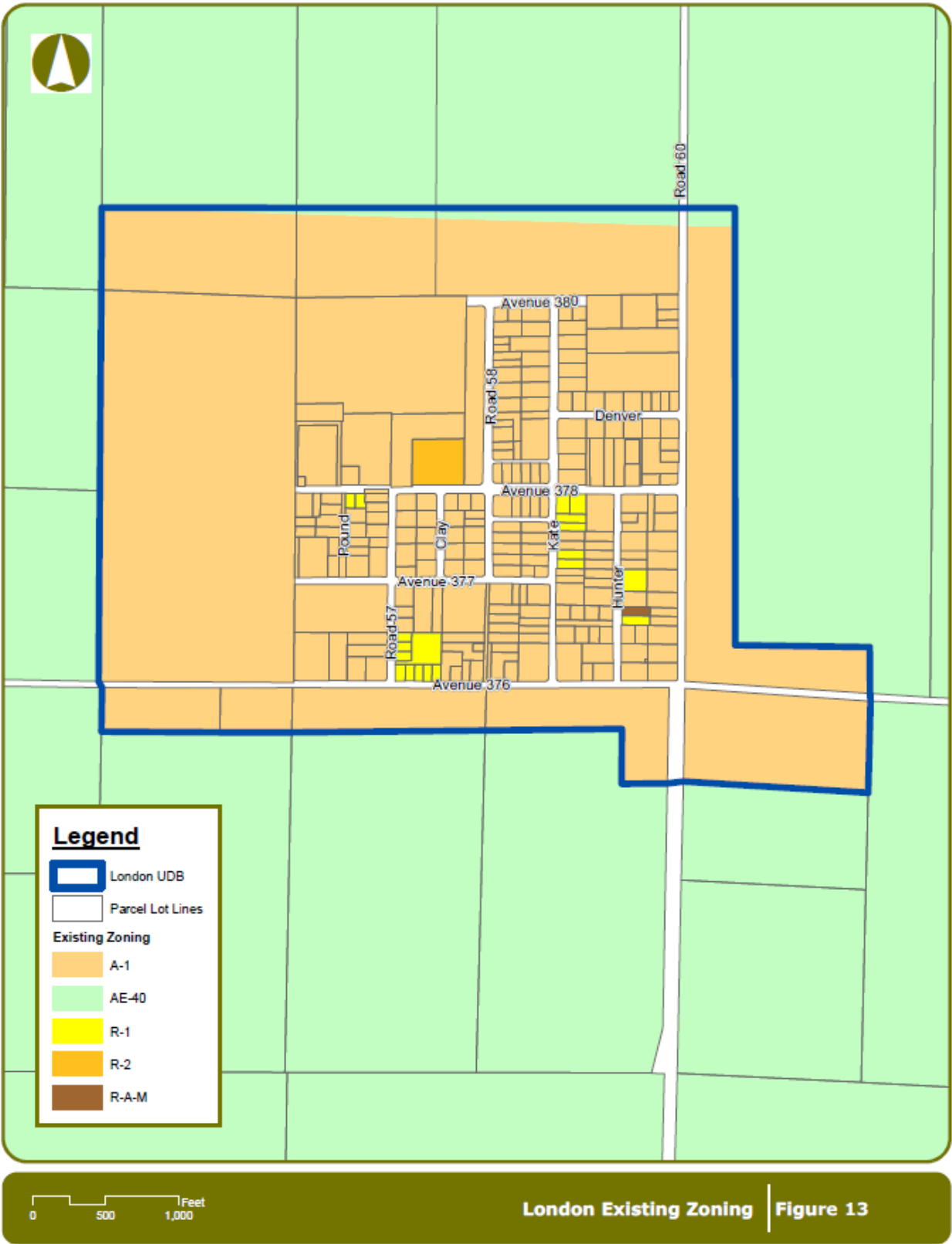
# LONDON COMMUNITY PLAN

Figure 12 - Proposed Land Use Plan



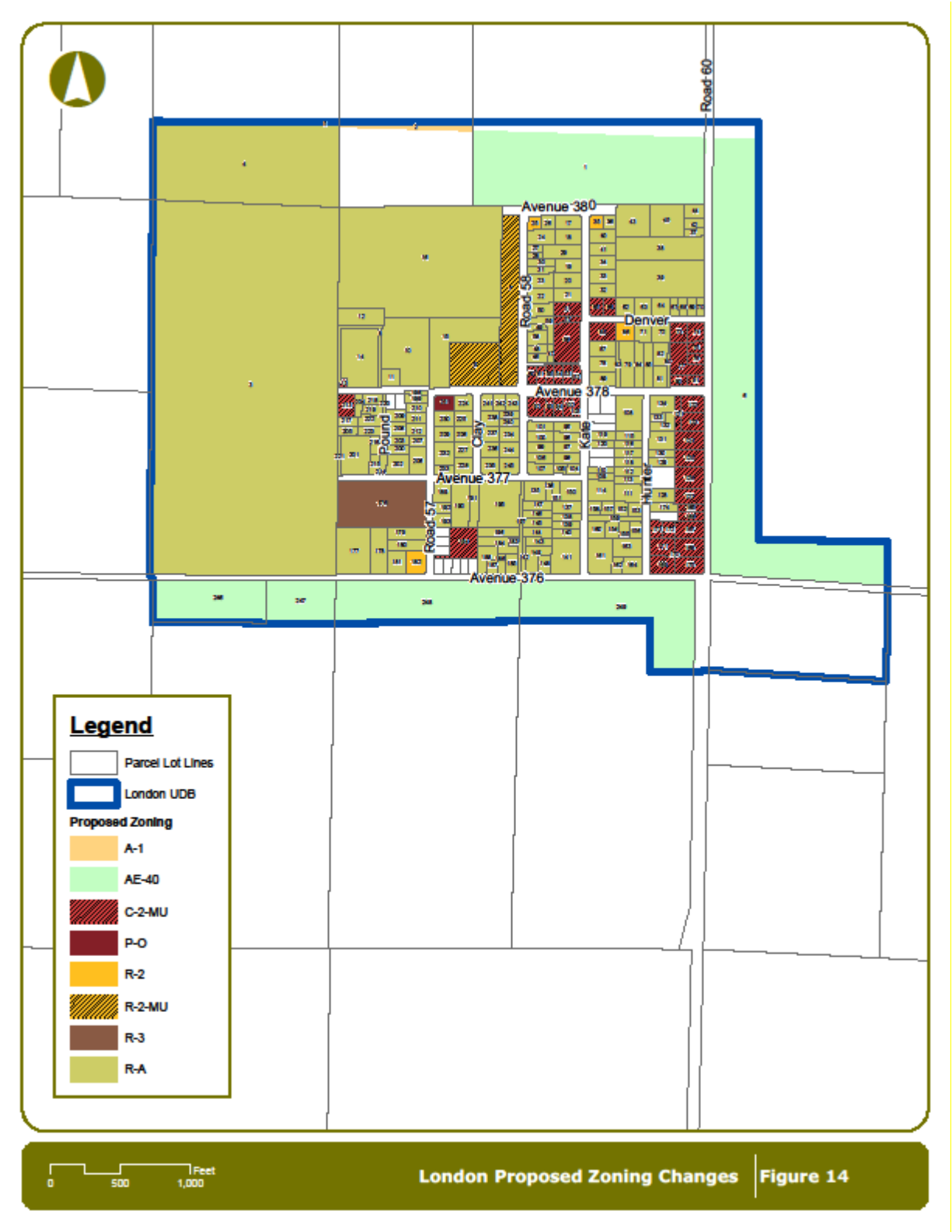
# LONDON COMMUNITY PLAN

Figure 13 – Existing Zoning Plan



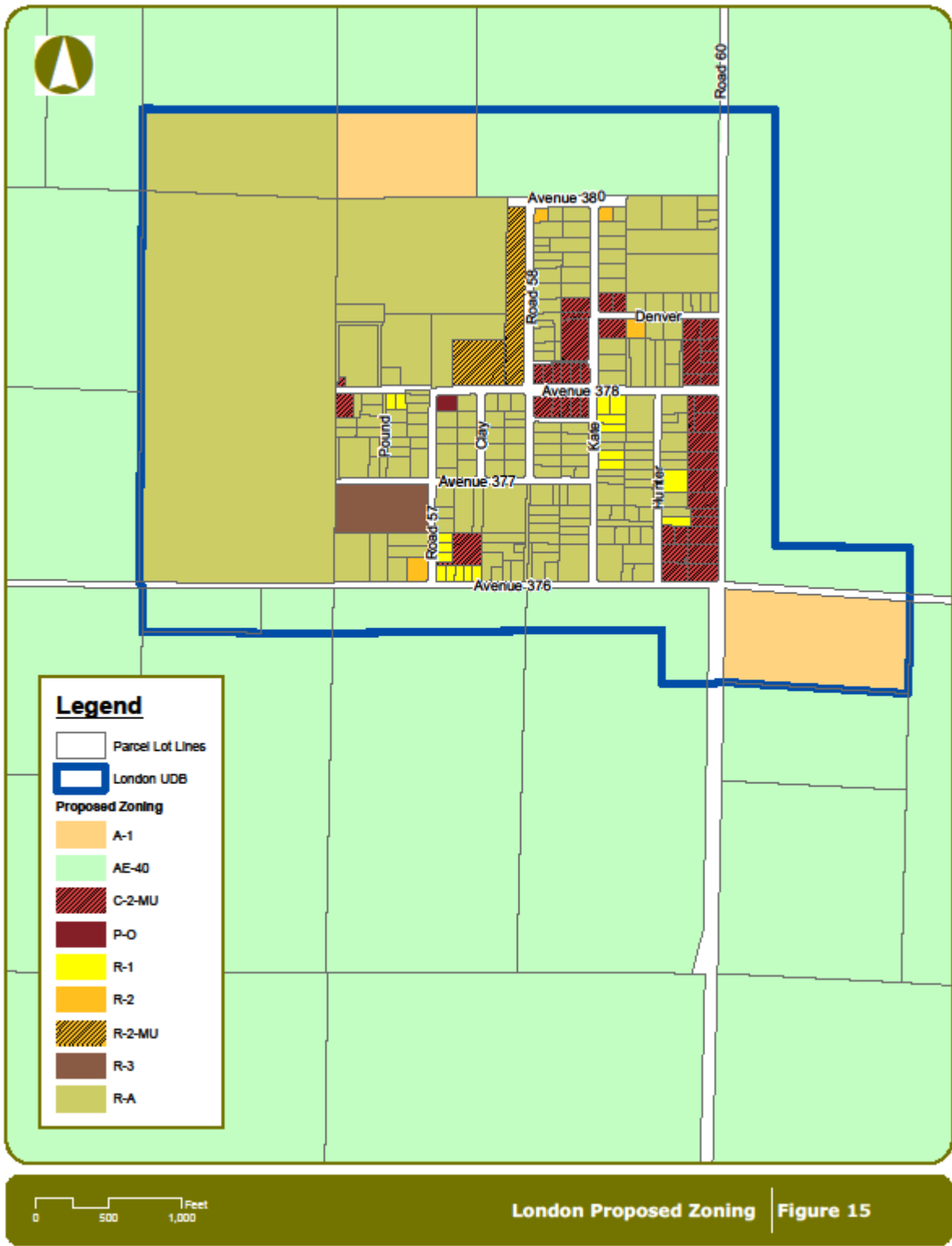
# LONDON COMMUNITY PLAN

Figure 14 - Proposed Zoning Changes



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Figure 15 - Proposed Zoning Districts



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## ATTACHMENTS

- A-1 – Use Permit Requirement Changes (Zone Change Text)
- A-2 – Mixed Use Overlay District (Zone Change Text)
- A-3 – Development Standards (Mixed Use Zoning Districts)
- A-4-Complete Streets (Complete Streets Program)

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## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1



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Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1

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Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3,

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		M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3, *C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3,

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		M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	

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School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# LONDON COMMUNITY PLAN

## A-2 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the Community of London, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of London.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for London.

Within the Mixed-Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above-mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses:

All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

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gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy-five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and

# LONDON COMMUNITY PLAN

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zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for London. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.



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## A-3 Development Standards (Mixed Use Zoning District)

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To promote Economic Development within the London Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within London. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the London Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within London. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery

(Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

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LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

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WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.



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SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.



# LONDON COMMUNITY PLAN

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# LONDON COMMUNITY PLAN

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## APPENDICES

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9371

General Plan Amendment GPA 17-035-Resolution No. 9372

Section 18.9 “Mixed Use” Combining Zone-Resolution No. 9373

Section 16 “By Right Uses”-Resolution No. 9374

Zoning Districts Map-Resolution No. 9375

Sustainable Community Plans-Resolution No. 9376

## COUNTY OF TULARE, STATE OF CALIFORNIA

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Community Plans 2017 Update set forth in attached Exhibit “A” and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit “B” for the entire Community Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Community Plans and Urban Development Boundaries, (Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, and Sultana General Plan Amendment No. GPA 17-012, Change of Zones (PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map , for an amendment PZC 17-042 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundary, and Zone Ordinance Amendment No. PZC 17-041 establishing a Mixed Use Combining Zone in Alpaugh, East Orosi, London, Richgrove, and Sultana (incorporated by reference herein). The unincorporated Communities are located within the Mount Diablo Base & Meridian as follows: Alpaugh, Sections, 27, 28, 33 & 34, Township 23S, Range 23E, East Orosi, Sections 9, & 16, Township 16S, Range 25E, London, Sections 1, 2, 11 & 12, Township 20S, Range 25E, Richgrove, Section 36, Township 24S, Range 26E, and Section 31, Township 24S, Range 27E, and Sultana, Sections 10, 11, 14 & 15, Township 16S, Range 24E; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Alpaugh, East Orosi, London, Richgrove, and Sultana to the Mixed-Use Combining Zone; (3) allow additional by-right uses

within the aforementioned Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Community Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-042 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map within the respective Urban Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-041 an amendment to Section 16 to allow additional by-right uses in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Community Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Community Plans;
  - b. Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Community Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Community planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Community Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Community Plans 2017 Update General Plan Amendment 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004; Change of Zone No. PZC 17-004 (Zoning District Map); East Orosi General Plan Amendment No. GPA 17-034; Change of Zone No. PZC 17-005 (Zoning District Map); London General Plan Amendment No. GPA 17-008; Change of Zone No. PZC 17-008 (Zoning District Map); Richgrove General Plan Amendment No. GPA 17-011; Change of Zone No. PZC 17-011 (Zoning District Map); Sultana General Plan Amendment No. GPA 17-012; Change of Zone No. PZC 17-012 (Zoning District Map; Change of Zone No. PZC 17-042 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-041 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Community Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,

4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Community Plans 2017 Update. Therefore, the public will benefit from the Community Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III COMMUNITY PLANS, OF THE ) RESOLUTION NO.9372  
TULARE COUNTY GENERAL PLAN FOR THE )  
COMMUNITY PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-004, GPA 17-034, )  
GPA 17-008, GPA 17-011, GPA 17-012, )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Community Plan of the Tulare County General Plan as set forth in Exhibits "A to E" for the Community Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, Sultana General Plan Amendment No. GPA 17-012 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the communities as part of the Community Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Community Plans of the Tulare County General Plan for the Community Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Community Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Community Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Urban Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the adoption of urban development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;



5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Community Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Community Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:


C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Community Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

- Exhibit “A” – 2017 Alpaugh Community Plan (Part III)
- Exhibit “B” – 2017 East Orosi Community Plan (Part III)
- Exhibit “C” – 2017 London Community Plan (Part III)
- Exhibit “D” – 2017 Richgrove Community Plan (Part III)
- Exhibit “E” – 2017 Sultana Community Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO            )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT    )       RESOLUTION NO. 9373  
WITH THE ADOPTED COMMUNITY PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE   )  
NO. PZC 17-041                                    )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Community Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352.

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana ;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, Earlimart, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and

Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Community Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Communities of Alpaugh, East Oroquieta, London, Richgrove, and Sultana, consistent with the adopted Community Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:       Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:       None  
ABSTAIN:   None  
ABSENT:     None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9374
CONSISTENT WITH THE ADOPTED	)	
COMMUNITY PLANS, AS PROPOSED IN	)	
CHANGE OF ZONE NO. PZC 17-042	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Earlimart, Ducor Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hypericum, Jovista, Matheny Tract and Tooleville. The Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
ORDINANCE NO. 352, ZONING ORDINANCE )  
DISTRICT MAPS TO REZONE PROPERTIES )  
IN THE URBAN DEVELOPMENT BOUNDARIES )  
OF ALPAUGH, EAST OROSI, LONDON, )  
RICHGROVE, AND SULTANA ) RESOLUTION NO. 9375  
CONSISTENT WITH THE ADOPTED )  
COMMUNITY PLANS 2017 UPDATE (GPA 17-035), )  
AS PROPOSED IN CHANGE OF ZONE NO. PZC's )  
PZC 17-004, PZC 17-005, PZC 17-008, PZC 17-011, )  
AND PZC 17-012, )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Alpaugh Rezoning Plan (PZC 17-004), to include the Community of Alpaugh, consistent with the adopted Alpaugh Community Plan 2017 Update, Exhibit "E" East Orosi Rezoning Plan (PCZ 17-004), to include the Community of East Orosi, consistent with the adopted East Orosi Community Plan 2017 Update, Exhibit "H" London Rezoning Plan (PCZ 17-016), to include the Community of London, consistent with the adopted London Community Plan 2017 Update, Exhibit "K" Richgrove Rezoning Plan (PCZ 17-017), to include the Community of Richgrove, consistent with the adopted Richgrove Community Plan 2017 Update, Exhibit "N" Sultana Rezoning Plan (PCZ 17-018), to include the Community of Sultana, consistent with the adopted Sultana Community Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities;
3. The proposed changes in zone district designations identified in the Alpaugh, East Orosi, London, Richgrove, and Sultana Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Alpaugh, East Oroshi, London, Richgrove, and Sultana Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Alpaugh Change of Zone No. PZC 17-004, East Oroshi Change of Zone No. PZC 17-005, London Change of Zone No. PZC 17-008, Richgrove Change of Zone No. PZC 17-011, Sultana Change of Zone No. PZC 17-012, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" Rezoning Plans consistent with the adopted Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Alpaugh Change of Zone No. PZC 17-004***

*Exhibit "A" – Zoning District Ordinance Map*

*Exhibit "B" – Zoning Map*

*Exhibit "C" – List of Affected Properties/Map*

***East Orosi Change of Zone No. PZC 17-005***

*Exhibit "D" – Zoning District Ordinance Map*

*Exhibit "E" – Zoning Map*

*Exhibit "F" – List of Affected Properties/Map*

***London Change of Zone No. PZC 17-008***

*Exhibit "G" – Zoning District Ordinance Map*

*Exhibit "H" – Zoning Map*

*Exhibit "I" – List of Affected Properties/Map*

***Richgrove Change of Zone No. PZC 17-011***

*Exhibit "J" – Zoning District Ordinance Map*

*Exhibit "K" – Zoning Map*

*Exhibit "L" – List of Affected Properties/Map*

***Sultana Change of Zone No. PZC 17-012***

*Exhibit "M" – Zoning District Ordinance Map*

*Exhibit "N" – Zoning Map*

*Exhibit "O" – List of Affected Properties/Map*

Attachment 2

***Community Plan Rezoning Full Ordinance***

*Exhibit A - Community Plan Rezoning Full Ordinance*

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF SUSTAINABLE  
COMMUNITY PLANS

)  
)

RESOLUTION NO. 9376

Resolution of the Planning Commission of the County of Tulare recommending the Board of Supervisors adopt and accept the Sustainable Community Plans. A major component of the Community planning process included a thorough examination and cataloguing of existing land uses which suggested a strong need for infrastructure improvements, land use changes and zoning changes, where feasible, in order to fulfill the goals of the General Plan and Senate Bill 244. The Unincorporated Community Infrastructure Maps provided the measurement tools needed to identify infrastructure solutions, appropriate improvement projects, and planning policy analysis. The resulting objective of the proposed Community Plans is aimed at reducing greenhouse gas emissions, promoting equity, providing economic stability; and thus, enhancing sustainability, as well as protecting the environment, and promoting healthy and safe communities. The Plans necessitated and ensured methods of public participation with a wide variety of stakeholders.

WHEREAS, The Budget Act of 2010/11 authorized the Department of Conservation (Department) to implement and administer \$63.15M in project funds (Proposition 84, the Safe Drinking Water, Water Quality and Supply, Flood Control, River and Coastal Protection Bond Act of 2006) to provide Planning Grants and Incentives awarded by the Strategic Growth Council (Council). The primary goal of this grant program is to develop and implement plans consistent with Chapter 13, Division 43, Public Resources Code, Sections 75127 and 75128; and

WHEREAS, The County received a grant from the Strategic Growth Council in 2014, Grant No. 2014-631, to study the feasibility of integrating infrastructure and land use, with the needs of rural unincorporated communities based on the Communities within Tulare County that received a top 10% ranking utilizing the *CalEnviroScreen* methodology and in fulfillment of the General Plan's Housing Element and SB 244 Plan (2014); and

WHEREAS, *"The work upon which this publication is based was funded in whole or in part through a grant awarded by the Council."* Preparation and processing of the Sustainable Community Plans took an estimated 36 months; and

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County



Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and

WHEREAS, the Sustainable Community Plans included all tasks and deliverables specified in the work plan including project development and meetings, data gathering, public workshops, plan development, public hearings, and grant reporting and administration; and

WHEREAS, the Sustainable Community Plans include strategies that reduce energy consumption, conserve water, improve air and water quality, and provide other community benefits; and

WHEREAS, staff has performed necessary investigations, prepared a written report, and recommended modifications and approval of these proposed Plans; and

WHEREAS, staff has produced the Final Report document attached as "Exhibit A" pending further review by the Planning Commission and the public, and will make all necessary administrative changes subject to this review and prior to the final submittal to the Board of Supervisors; and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. Recommend that the Board of Supervisors find that the said Sustainable Community Plans meet the Goals and Objectives of the Council in fulfillment of the Grant requirements as specified in the work plan.

The foregoing resolution was adopted upon motion of Commissioner Gong, seconded by Commissioner Aguilar, at a regular meeting of the Planning Commission on the 8th day of November, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A- Strategic Growth Council Final Report

Appendix B – Board of Supervisors  
Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

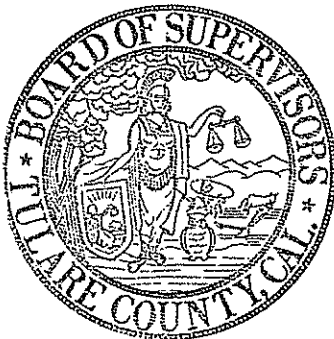
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **Richgrove Community Plan**

# RICHGROVE COMMUNITY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division





# RICHGROVE COMMUNITY PLAN UPDATE

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# RICHGROVE COMMUNITY PLAN UPDATE

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# RICHGROVE COMMUNITY PLAN UPDATE

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# Richgrove Community Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors

Resolution No.

GPA 17-035

PZC 17-041

PZC 17-042

GPA-17-011

PZC-17-011



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# RICHGROVE COMMUNITY PLAN UPDATE

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# RICHGROVE COMMUNITY PLAN UPDATE

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# RICHGROVE COMMUNITY PLAN UPDATE

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# RICHGROVE COMMUNITY PLAN UPDATE

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Mike Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I



# RICHGROVE COMMUNITY PLAN UPDATE

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# RICHGROVE COMMUNITY PLAN UPDATE

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# RICHGROVE COMMUNITY PLAN UPDATE

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# RICHGROVE COMMUNITY PLAN UPDATE

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## EXECUTIVE SUMMARY

### Introduction

The primary objective in preparing the Richgrove Community Plan Update is to accurately reflect the current needs and priorities of the unincorporated community of Richgrove. The update is also required in order to make the Community's land use and zoning consistent with the Tulare County 2030 General Plan Update (2012). Richgrove is one of the most developed unincorporated communities in Tulare County having more sidewalks, street lights, a school, water / waste water, and drainage facilities. Through the grant funds provided by the Sustainable Growth Council (SGC) of California, the County was able to do outreach with the Community, and to receive input on the Community's concerns in order to address and update the needs of the Community from the original 1987 Richgrove Community Plan (GPA 89-05).

In Tulare County, the General Plan has historically been developed on a countywide basis or by large geographic sub-areas (rural valley, foothill, and mountain) with development policies emphasizing countywide and area wide issues and concerns. Several unincorporated communities, including Richgrove, have localized land use needs and issues that need to be addressed in a specific manner. Therefore, through this latest round of community plans have been prepared for individual communities with primary emphasis being placed on land use and circulation planning.

The entire community of Richgrove is currently designated as a "Rural Service Center" in the Tulare County General Plan. It has become apparent that a more specific plan is needed to ensure orderly development of the community. As with any community plan, the contents of this document are not intended to be absolute. Planning is a continuous process and, to be effective, requires periodic re-evaluation and revision to reflect changing needs and priorities. This plan should, therefore, be reviewed on a periodic basis with the assistance and participation of local citizens, groups, and agencies. By doing so, it is anticipated that the Richgrove Community Plan will continue to provide meaningful and necessary guidance for the development of the community in the foreseeable future.

The primary objective in preparing a Community Plan for Richgrove is to develop a plan, which accurately reflect the needs and priorities of the Community of Richgrove. Richgrove is currently designated as a Community in the 2030 Tulare County General Plan (2012). It has become apparent that an updated and more relevant plan is necessary to increase the availability of infrastructure funding and to stimulate economic development within the community.

California Government Code Section 65300 et seq requires that each local agency, city or county, prepare and adopt comprehensive long-term general plans for the physical development of lands within its jurisdiction. A general plan must function as "a statement of development policies" and must include a diagram (s) and text setting forth goals, policies, standards, and plan proposals. The plan must include the following elements: land use, circulation, housing, conservation, noise, safety, and open space. State law also provides that a local agency may include one or more of several optional elements depending on the needs and characteristics of the jurisdiction. This plan includes updates to the Land Use and Circulation Elements to include a rezoning plan and complete streets policies.



# RICHGROVE COMMUNITY PLAN UPDATE

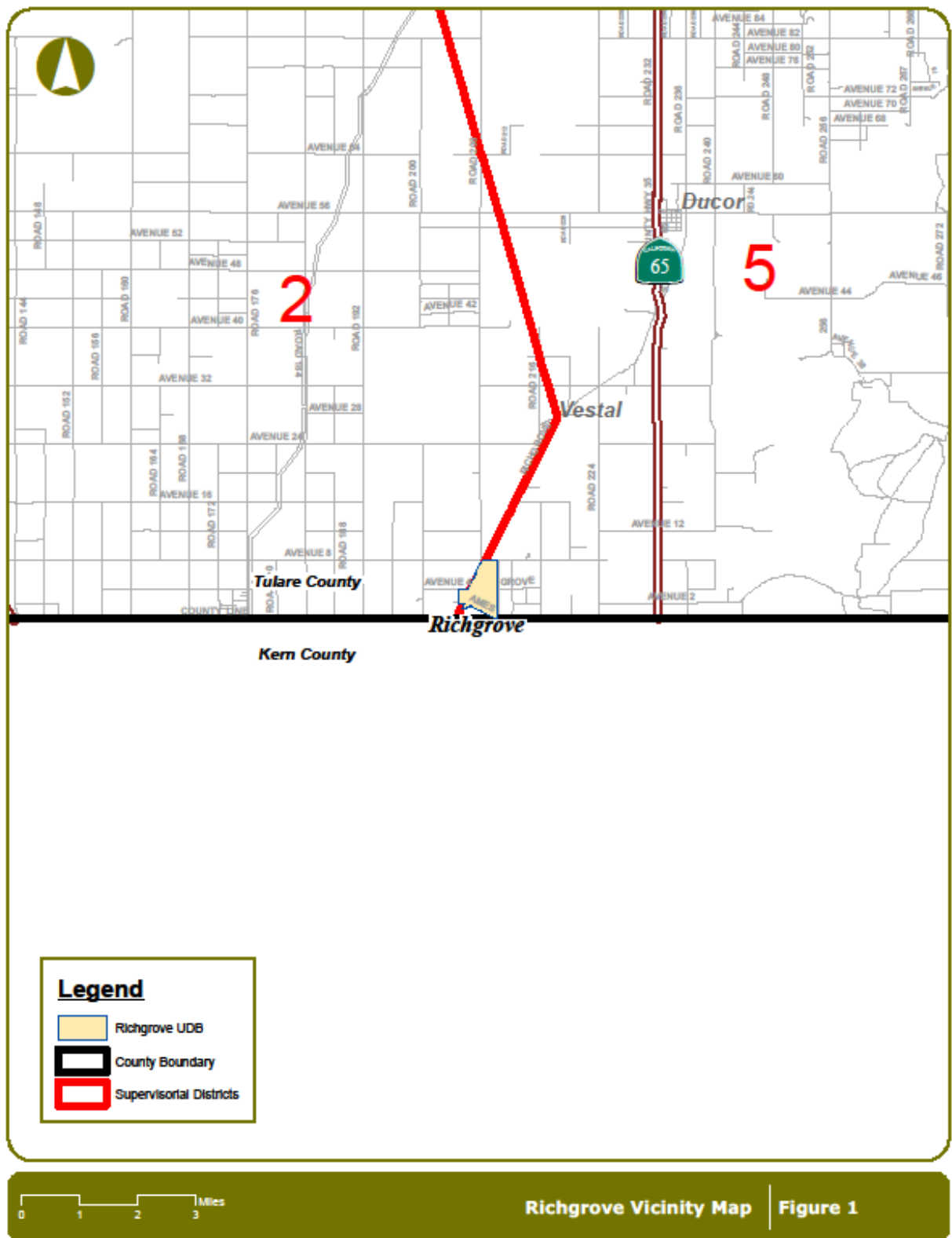
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## Location

The community of Richgrove is located on the eastern side of the San Joaquin Valley, approximately 50 miles north of Bakersfield and 75 miles southeast of Fresno. **(see Figure 1)** Richgrove is a Census-Designated Place located in the southern portion of Tulare County, just north of the Tulare County/Kern County line. It is generally bounded by County Line Road in the south, Avenue 8 in the north, Richgrove Drive in the west, and Road 210 in the east and encompasses 0.5 square miles of land. Nearby cities and communities include Ducor approximately 8 miles to the northeast, Delano approximately 10 miles to the west, Terra Bella approximately 12 miles to the northeast, Poplar-Cotton Center approximately 18 miles to the north, and Porterville approximately 20 miles to the northeast. Richgrove is an agriculturally oriented service community surrounded on all sides by lands in agricultural production, and vacant lands. Richgrove is a vibrant Hispanic community with a strong agricultural industry including many grape vineyards, citrus orchards, and row crops. State Highway 65 is located approximately 3 miles to the east of the community and State Highway 99 is located approximately 7 miles to the west. Both of these highways serve as primary regional access routes to the community from outside areas Richgrove is in Section 31, Township 24 South, Range 27 East and Section 36, Township 24 South, Range 26 MDB&M, and can be found within Richgrove United States Geological Survey 7.5 minute topographic quadrangle. Richgrove is located at an elevation of 510 feet National Geodetic Vertical Datum. The coordinates of Richgrove are: Latitude: 35.796620 and Longitude: -119.107881.

# RICHGROVE COMMUNITY PLAN UPDATE

Figure 1 - Vicinity Map



# RICHGROVE COMMUNITY PLAN UPDATE

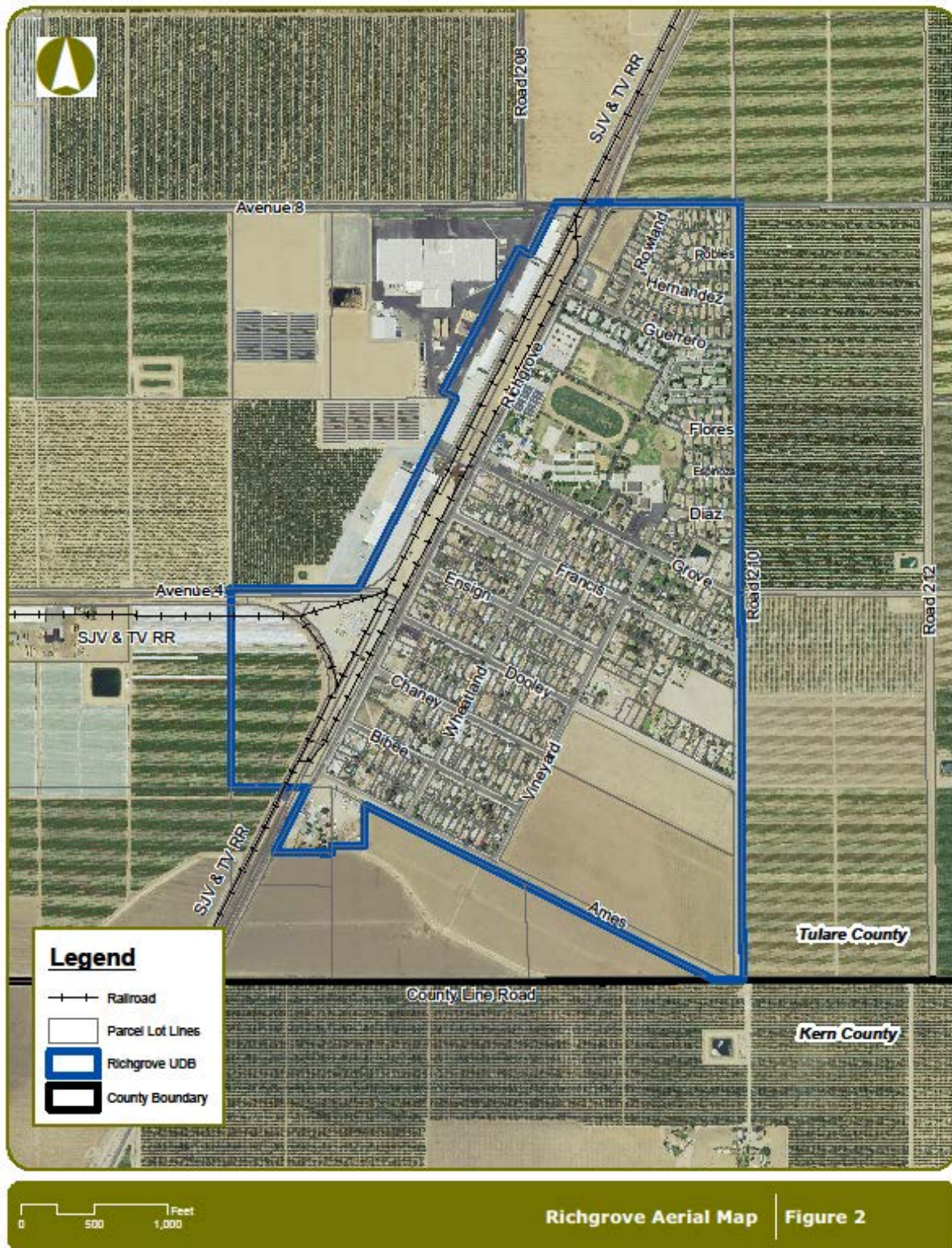
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## HISTORICAL PERSPECTIVE

The history of Richgrove dates back more than 200 years when Spanish Missionaries and explorers were looking for areas to settle. The Yokuts Indians had established trails in the area that were utilized by the scout, Kit Carson and the explorer-soldier, John C. Fremont. These Indian trails were transformed into roads as populations in the valley grew and, in 1858, the Butterfield Stage Line was established using these routes. During the 1870's cattle ranchers and wheat farmers moved into the area with the Southern Pacific Railroad following in 1890. In 1909, Richgrove became a town with a hotel to serve stage coach and train passengers. The town continued to grow, and due to the increasing availability of irrigation water after World War II, the wheat crops were replaced by vineyards and orange groves. **(see Figure 2)**. Today, Richgrove is located along Famoso Porterville Highway/Richgrove Dr.), which has created the market for its commercial and industrial development. The Community is still surround by some of the highest quality table grape vines in California.

# RICHGROVE COMMUNITY PLAN UPDATE

Figure 2 - Aerial Map





# RICHGROVE COMMUNITY PLAN UPDATE

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## COMMUNITY PROFILE

### Planning Area

Richgrove is a small, unincorporated community with an Urban Development Boundary (UDB) area of approximately 234.1 acres (see Table 1).

The Richgrove Community Urban Development Boundary (UDB) area consists of 234.1 acres (See Table 1). Under the 2012 General Plan the existing uses are based on actual acreage use within the UDB are described as follows: Agricultural activities occur on +/- +/- 60 acres; and urban development, including urbanized uses such as residential development, institutional, industrial, and commercial uses occupy +/- 174 acres. The remaining 52.1 acres are dedicated for Right-of-way.

Table 1 - Richgrove Land Use	
Adopted Land-Use	Acres
Commercial	12.0
Industrial	46.7
Public/Quasi Public	27.2
Residential	96.1
Unclassified (Right-of-Way)	52.1
TOTAL	234.1

Source: Tulare County GIS

The change being proposed is to add 43.9 acres to the Urban Development Boundary of Richgrove to include and “square off” the areas to the Southwest along Ames and Road 210 to be consistent with the Richgrove CSD Boundary.

### Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a “[d]isadvantaged community” means a community with a median household income less than 80% of the statewide average. “Severely disadvantaged community” means a community with a median household income less than 60% of the statewide average.”

In 2015, Richgrove median household income was \$28,333, whereas the State of California’s median household income was \$61,818. Richgrove’s median household income was 45.8% of the State of California’s median household income, and therefore it is considered a severely disadvantaged community.

### Climate

Hot, dry summers and mild winters characterize the San Joaquin Valley. Rainfall averages approximately 10 inches annually. Average monthly air temperatures range from 46° to 85° degrees F. The valley area is characterized by a “Mediterranean” type climate with cool moist winters and hot dry summers. Approximately 85% of the precipitation occurs between November and April and the frost-free season usually exceeds nine months. A thermal belt exists in the east valley area along the base of the foothills of the Sierra Nevada, including the Richgrove area, enabling the production of frost-sensitive tropical fruits including citrus, avocados, and olives. Another major climatic feature of the valley is the radiation fogs that occur during the winter months. These fogs are more severe and persist longer at lower elevations. The Visalia-Fresno area has seasonal air flows with wind direction in the spring, summer, and fall tending from the northwest and winter winds tending from the west.

# RICHGROVE COMMUNITY PLAN UPDATE

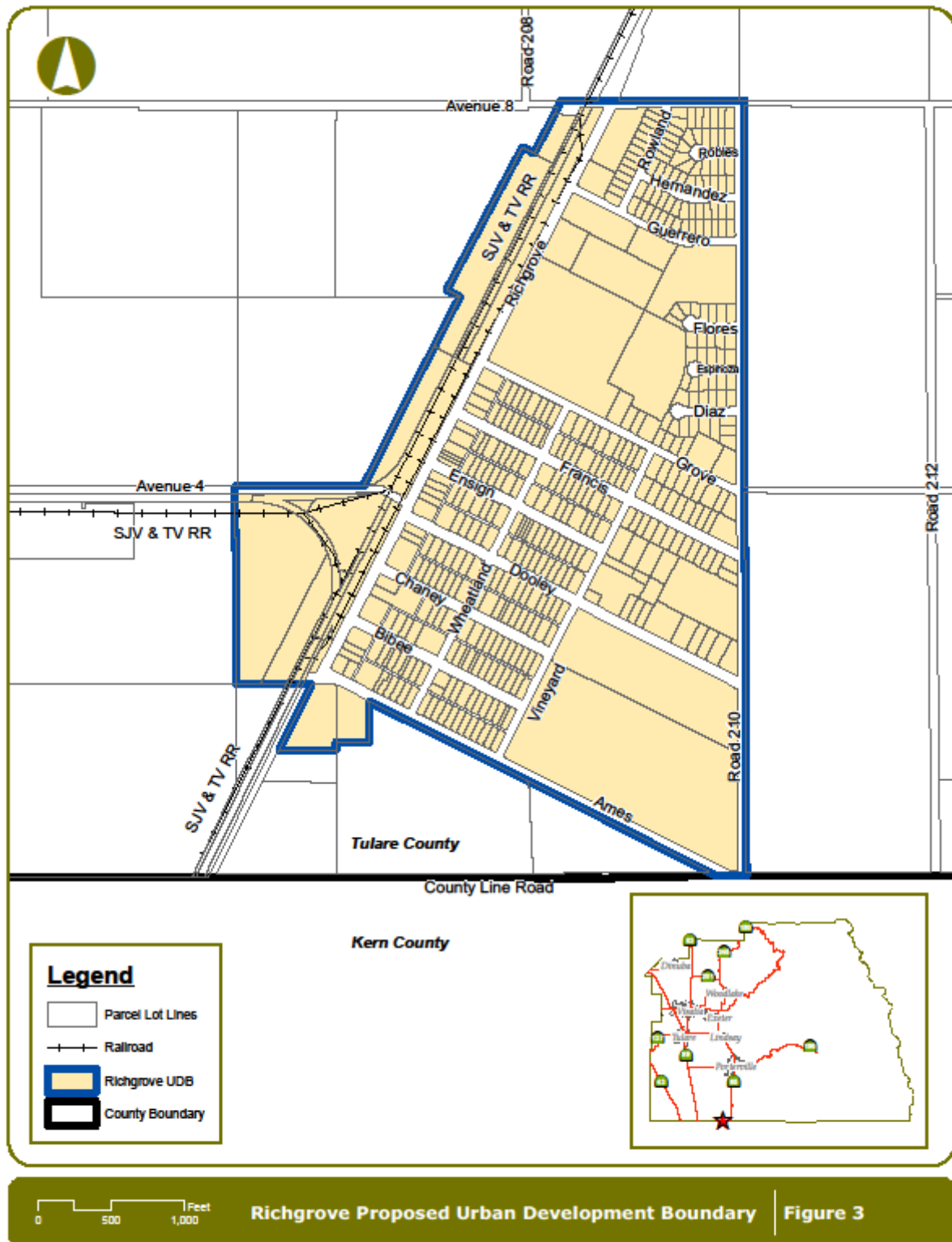
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## Topography

Richgrove is located in the southeastern San Joaquin Valley, bordered on the east by the foothills of the Sierra Nevada Mountains. The community is situated on the shallow sloping valley floor created during the uplift of the Sierra Nevada. Elevations within the community range between 500 and 530 feet above sea level. Alluvial deposits from the White River encompass most of the community forming a gently sloping plain.

# RICHGROVE COMMUNITY PLAN UPDATE

Figure 3 - Richgrove UDB



# RICHGROVE COMMUNITY PLAN UPDATE

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## Relationship to other Elements of the Tulare County General Plan

The County of Tulare has adopted all the general plan elements required by State law and has also adopted optional elements including the recent Safety Element to include added policies for fire hazard and flood mitigation under the principles of Adaptation and Resiliency. These elements are structured for application on a countywide basis; and therefore, broad in scope. In addition, there are other policy documents such as the Climate Action Plan that address Green House Gas emissions. Thus, typically these elements address the Richgrove area in a general manner only. As all general plan elements, they have equal status under the law, the policies and directives established in these adopted elements are applicable to the Richgrove area and must be complied with. The Richgrove Community Plan refines the County's general plan policies to reflect the needs, desires, and values of the community and its residents. As previously mentioned, this refinement will focus primarily on the land use and circulation issues, with secondary emphasis on the other general plan elements.

In accordance with State law, care has been taken in preparing the community plan to ensure that internal consistency with other general plan elements is maintained and that conflicts with existing General Plan policies will not occur by the adoption of the community plan. To achieve consistency, other mandatory or optional General Plan elements already adopted by the County of Tulare must necessarily be modified. The amendments being made to other existing General Plan elements are described as follows

## Urban Boundaries Element

The Urban Boundaries Element of the previous Tulare County General Plan established an Urban Area Boundary for Richgrove. The element amended the County General Plan to eliminate the Urban Area Boundary, and establish an Urban Development Boundary to be coterminous with the Richgrove study area. The Urban Development Boundary for the 2012 General Plan for Richgrove under the Policy Framework Plan, and Rural Valley Land Plan, is as shown in Figure 3.

## Land Use Element

The general designation of Richgrove as an Urban Development Boundary is supplemented with more specific land use designations for the controlled, orderly growth of the community.

## Residential

The greatest concentration of residential development in Richgrove occurs east of Richgrove Drive. This area contains a mix of older residential neighborhoods, as well as tracts of newer homes in the north and east of the Community. Both single family and multi-family residential development has occurred north of the School. The industrial areas are located west of the Richgrove Drive. Commercial uses are located immediately along Richgrove Drive, and are interspersed with residential development. Roadway noises and traffic exists generally along Richgrove Drive, and the road has recently been improved by the County to address safety concerns of the residences.

## Multi-family Residential

There are two multi-family residential complexes located north of the Richgrove Elementary School.

## Mobile Homes

Although there are no mobile home parks in Richgrove, approximately mobile homes located throughout Richgrove, but mostly as secondary units.



# RICHGROVE COMMUNITY PLAN UPDATE

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## Community Commercial

There are 9 commercial businesses along Richgrove Drive including a restaurant, Panaderia La Rosa De Saron. The other businesses include Richgrove Food Center, Family Market, Chapala's Auto Sales, and the Fast Way Market.

## Industrial

Parcels adjacent to Richgrove Drive are zoned Industrial south of Avenue 4/Annes Drive and Avenue 8. These include mostly packing and cold storage facilities. Zaninovich & Sons and Castle Rock Vineyards is located across Richgrove Drive; both are outside the UDB.

## Schools

Richgrove Elementary School District serves the Elementary School and Junior High, grades K-8. High school students attend Cesar E. Chavez high school 10 miles to the Southwest.

## Parks

There are existing plans for a Richgrove Park is approximately 4 acres in size and is located on the southeast corner of Dooley Drive.

## Use of the Richgrove Community Plan

The Richgrove Community Plan describes the manner in which the planning area will develop and grow through the planning period. Its policies will form guidelines regarding future requests for building permits, zone changes, divisions of land, and other development review processes. In addition, as the plan establishes development densities and prescribes land uses, it will undoubtedly influence private decisions pertaining to land purchases and development proposals within the community. The plan establishes a site plan review process and contains standards for the development of property, in conjunction with State law and County ordinances. It, therefore, provides the authority for requiring necessary physical improvements in conjunction with private development projects thereby enhancing the physical, social, and economic environment of the community and protecting the health, safety, and welfare of its residents.

# RICHGROVE COMMUNITY PLAN UPDATE

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

In efforts to invite and increase awareness of the Strategic Growth Council (SGC) project, Self Help Enterprises went door to door throughout the community of Richgrove to distribute flyers at homes, businesses, and schools. During this process Self-Help enterprises (SHE) emphasized the importance of participating in this process. SGC partnered with the Richgrove Community Food Pantry, to distribute flyers to residents at the food distribution event and also partnered with Richgrove School District and the Richgrove Head Start Preschool to send flyers home with every student. SHE partnered with the Richgrove Veterans Memorial Building, they helped post meeting flyers and held the community meeting.

### Community Outreach Steps

Self-Help Enterprises (SHE) staff conducted the following outreach efforts to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) An informational flyer was created.
- 2.) SHE staff distributed flyers to homes in Richgrove on January 7 and January 13, 2016
- 3.) SHE staff partnered with the Richgrove Community Food Pantry to distribute flyers to residents at the food distribution event on January 13, 2016
- 4.) SHE staff partnered with Richgrove School District and the Richgrove Head Start Preschool to advertise the meeting; flyers were sent home with every student.
- 5.) SHE partnered with the Richgrove Veterans Memorial Building, to advertise the meeting; flyers were posted at the Veterans Memorial Building. The community meeting was also held at this location.
- 6.) During the community outreach process, SHE staff described the goals of the project and emphasized the importance and community benefits of participating in the SGC community meeting.
- 7.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at local businesses and post office.

### Community Meeting

Self-Help Enterprises held a community meeting (with one SHE staff member and one Tulare County RMA employee present) in Richgrove on January 14, 2016 at 5:30 pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Richgrove Community Memorial Building. The purpose of the meeting was to discuss Richgrove improvement needs, gather community data, and report the findings of the meeting in a final report. Thirty five Richgrove residents attended the meeting and twenty six community surveys were collected.

# RICHGROVE COMMUNITY PLAN UPDATE

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## Community Survey

A survey was developed to gather information about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

A follow up meeting was held on November 22, 2017, with County Staff and Self-Help Enterprises. Over 30 residents and members of the School District attended, and were provided the results of the survey. Discussion included water, housing vacancy and absent land lords, housing street maintenance, recreational park, and a health care facility. Discussion also included the role Richgrove CSD plays in the community.



# RICHGROVE COMMUNITY PLAN UPDATE

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Self Help Enterprises and Tulare County invites you

to a community meeting to review

## **Richgrove's Community Plan**

**Tuesday, November 22, 2016 at 6:00pm at**

**Richgrove Memorial Building 607 Richgrove Drive**

A few months ago Tulare County and Self Help Enterprises came to your community to seek input on a planning analysis of your community. Surveys were distributed and collected. Tulare County invites you to attend a community meeting to see the findings from the study. A thorough examination and inventory of existing land uses will suggest infrastructure improvements, land use changes and possibly zoning changes, where feasible.

For more information, call: Abigail at 559-802-1659 or Kyria 559-624-7154

The work upon which this publication is based was funded in whole or in part through a grant awarded by the California Strategic Growth Council.

**Disclaimer:** The statements and conclusions of this report are those of the GRANTEE and/or subcontractor and not necessarily those of the California Strategic Growth Council of the California Department of Conservation, or its employees. The California Strategic Growth Council and the California Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text



Self Help Enterprises y El Condado de Tulare  
le invita a la Junta Comunitaria para revisar

## **El Plan Comunitario de Richgrove**

**Martes 22 de noviembre del 2016 a las 6:00 de la tarde**

**En el Salon de Veteranos, 607 Richgrove Drive.**

Hace unos meses, el Condado de Tulare y Self Help Enterprises vino a su comunidad para buscar la opinión de la comunidad sobre un análisis de planificación de su comunidad. Encuestas fueron distribuidas y recolectadas. El Condado de Tulare le invita a asistir a una junta comunitaria para ver los resultados del estudio. Un examen completo y la organización de los usos existentes de la tierra sugerirán mejoras en la infraestructura, cambios en el uso de la tierra y posiblemente cambios de zonificación, donde sea factible.

Para más información llame a: Abigail 559-802-1659 o Kyria 559-624-7154

El trabajo sobre el que se basa esta publicación fue financiado en su totalidad o en parte, a través de una beca

# RICHGROVE COMMUNITY PLAN UPDATE

After discussion at the Richgrove SGC community input meeting, residents concluded that the following is a list of priority improvement needs:

## Priority Improvements

### Affordable Housing

1. Clinic
2. Street lights
3. Road Conditions
4. Internet
5. Police Patrol
6. Water Issues
7. Storm Water Drainage
8. Community Center



### Affordable Housing

Richgrove residents stated that they want new and affordable housing built in their community. Many people said that they would buy a new home in town, if there was an opportunity to do so. Residents want to see their community grow. Residents feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Delano.

### Medical Clinic

Richgrove residents do not have access to medical care within the community, since the Health Clinic did not receive a will serve letter from the Richgrove Community Service District. The nearest medical clinic is approximately 10 miles away in Delano. Residents report traveling to either Delano, 10 miles or Porterville 21 miles for medical care. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Delano, 10 miles away, and it takes approximately 15 minutes for the nearest ambulance to get to Richgrove from Delano.

### Road Conditions

Road conditions are a significant concern to Richgrove residents. It is reported that road conditions are poor and most roads need work due to potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving in town. Residents are especially concerned with the condition of Richgrove Drive, as is not regularly or properly maintained. Many heavy trucks and lots of traffic travel on this road, this results in the road needing repairs on a regular basis.



# RICHGROVE COMMUNITY PLAN UPDATE

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The following roads are a priority to the residents:

- Richgrove Drive
- Ames Drive

## **Storm Water Drainage**

Richgrove residents report that the community does not have adequate storm water drainage. This is a problem because the stagnant water attracts insects. Large puddles form when it rains and the water does not drain. Residents report that some streets flood during the rainy season. This becomes a bigger problem when the stagnant water attracts insects and mosquitos.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting, and that they stay indoors because they do not feel safe walking on a dark street. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Internet Access**

Most families in Richgrove do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Richgrove.

## **Community Resource Center**

Richgrove does not have a community hub. The community lacks many resources and needs a place where residents can access information and county resources. Residents expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Richgrove residents. This could also be the place where residents access a reliable internet connection. Currently residents must travel 10 miles to get to Delano to the nearest community center.

## **Sheriff Presence**

The Tulare County Sheriff Department (TCSO) is responsible for patrolling the community of Richgrove. Residents report that the TCSO response time is long and that there is inconsistent patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate. Richgrove residents are asking for increased Sheriff presence in their community.

## **Water**

Residents are concerned with the current water situation and want to see solution happen soon. In Richgrove, the town is provided water by Richgrove Community Services District. The District has two operable wells that supply Richgrove residents and businesses with potable water. One of these wells, Well No. 4, has experienced problems with two water quality contaminants, arsenic and DBCP, which have at times exceeded the MCLs. The other well, Well No. 5, has experienced problems with

# RICHGROVE COMMUNITY PLAN UPDATE

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nitrates and arsenic. The failure of either of these wells would jeopardize the District's ability to meet demand. California (Rodriguez) Camp is a privately-owned labor camp located two miles to the west of Richgrove. The California Camp water system has one water well that provides water to its residents. The well produces water that exceeds the Maximum Contaminant Level (MCL) for nitrate with levels in the range of 130 ppm. The system's owner has received Notices of Violation and Compliance Citations as a result of this high nitrate level, almost three times the MCL. The Camp's system has no supply redundancy and residents would be completely without water in the event of the well's failure. No emergency connection exists.

A Preliminary Engineering Report, completed in January 2008, was prepared with the understanding that consolidation between California Camp and the RCSD was a priority. The preferred project alternative is to drill a new community water well (including a test well) and construct transmission lines that will connect the new well to the District and the Labor Camp. The planning study will include the evaluation of the feasibility of blending to achieve water quality improvement and reduce the concentration of contaminants to within the MCL required for drinking water systems. A blending project would require the construction of new storage facilities.

## **Public Services**

- Sheriff –Richgrove is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling Richgrove and the nearest Sheriff substation is located in Porterville, 21 miles away.
- Fire –Tulare County Fire Department
- Schools – Richgrove School District (K-8), located in Richgrove, is part of Tulare County Office of Education
- Libraries – The nearest public library is in Delano, 10 miles away
- Parks – There is currently a park with approved an approved plan and the project had broken ground for construction in Richgrove

# RICHGROVE COMMUNITY PLAN UPDATE

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## RICHGROVE TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Community's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Community plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Community allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Community. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of a smaller community such as Richgrove.

#### Population

The Historical population of Richgrove from the 1980 Census showed Richgrove's population to be 1,398 persons residing in 333 households, an average of 4.2 persons per household. Population projections in 1987 for indicate that Richgrove's population, as of 1986, was approximately 1,629 and would grow to approximately 2,653 by the year 2005. Based on the above figure of 4.2 persons per household, there are an estimated 388 households residing in Richgrove in 1986 with a projected 632 households by the end of the planning period, an increase of 244 households or 643.

The household income figures obtained from the 1980 Census showed the median household income for Richgrove to be \$12,115.

Richgrove does experiences seasonal population increases due to the influx of migrant workers and their families during the various harvest seasons. Estimates as to the number of seasonal residents range from 500 to 1,000 persons in 1980. This influx has created influxes in the availability of affordable housing for the migrant population which in 1980 had a median household income lower than the community average of \$12,115. This influx still exists and at the same time there are issues with vacancies within the community.

In 2015, the Population for Richgrove was 2,972 (see Table 2).

Table 2 - Population						
	California	%	Tulare County	%	Richgrove	%
Male	19,087,135	49.7%	227,426	50.1%	1527	51.4%
Female	19,334,329	50.3%	226,607	49.9%	1,445	48.6%
Total	38,421,464		454,033		2,972	

2011-2015 American Community Survey 5-Year Estimates



# RICHGROVE COMMUNITY PLAN UPDATE

## Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Richgrove's population during the Planning Period (see Table 3 & Table 20).

Table 3 - Projected Annual Growth Rates		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

The projected population (an estimated 19% increase over 14 years) in 2030 is 3,602, a net increase of 596 persons. The population between 1980 (1,398) and 2005 (2,653) was over 47%. Since then, the projected increase is about 11%, which is exactly consistent with the General Plan growth rate of 1.3% year of year.

## Median Age

Richgrove's population is younger than the median age throughout all of Tulare County. Richgrove's median age of 23.3 is lower than the median age of the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Richgrove	23.3

2011-2015 American Community Survey 5-Year

## Ethnicity and Race

In 2015, approximately 0.6% of Richgrove's population was White, 0% was African American, 0% was Native American, 4.8% was Asian, and 0.4% was two races or more (see Table 5). Approximately 93.7% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Richgrove	%
Total	38,421,464		454,033		2,972	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	2,785	93.7%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	19	0.6%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	143	4.8%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	25	0.8%

2011-2015 American Community Survey 5-Year Estimates

# RICHGROVE COMMUNITY PLAN UPDATE

## ECONOMIC CONDITIONS

### Employment in Richgrove

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Richgrove was about 18.9% while the rate for Tulare County was 13.5%. The unemployment rate for the State of California was 10.1%. Keeping in mind that the 5.9% includes only the employable labor force (that is, not every person of the population) results in about 368 unemployed persons of Richgrove's person labor force of 1,330.

Table 6 - Employment Status			
Employment Status	California	Tulare County	Richgrove
Population 16 years & over	30,312,429	325,404	1,949
In labor force	19,269,449	194,420	1,330
Civilian labor force	19,137,441	194,102	1,330
Employed	17,246,360	170,780	962
Unemployed	1,891,081	23,322	368
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	619

2011-2015 American Community Survey 5-Year Estimates

### Income

Richgrove's median household income was \$28,333, compared to Tulare County \$42,708 and \$61,818 for the State of California. (see Table 7)

Table 7 - 2011-2015 American Community Survey Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,8140	\$17, 876
Richgrove CDP	\$28,333	\$27,946	\$8,617

# RICHGROVE COMMUNITY PLAN UPDATE

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

Table 8 - Housing Tenure			
	California	Tulare County	Richgrove
Occupied housing units	12,717,801	133,570	628
Owner-occupied	6,909,176	75,685	230
Renter-occupied	5,808,625	57,885	398

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Richgrove
Owner-occupied	54.8%	57.2%	36.6%
Renter-occupied	45.2%	42.8%	63.4%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Richgrove	78	50	19	12	27	17	17	11	15	10	156

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Richgrove indicated that approximately 50% of the housing units were sound. Approximately 40% of Richgrove's housing units were deteriorated and 10% were dilapidated (see Table 10).

# RICHGROVE COMMUNITY PLAN UPDATE

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Richgrove is 634.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Richgrove
Total housing units	13,781,929	143,869	634
Built 2014 or later			0
Built 2010 to 2013	86,387	1,663	0
Built 2000 to 2009	1,649,540	25,647	47
Built 1990 to 1999	1,478,621	21,858	118
Built 1980 to 1989	2,109,495	23,089	167
Built 1970 to 1979	2,504,338	26,954	109
Built 1960 to 1969	1,875,711	15,021	65
Built 1950 to 1959	1,907,537	14,000	28
Built 1940 to 1949	873,498	7,403	75
Built 1939 or earlier	1,296,802	8,234	25

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Richgrove was 3.98 persons per household (see Table 12). The average owner-occupied household size is 4.83.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Richgrove	4.83	3.98

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2014, the rental vacancy rate in Richgrove was 1.5%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Richgrove	0%	1.5%

2011-2015 American Community Survey 5-Year Estimates

<sup>1</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# RICHGROVE COMMUNITY PLAN UPDATE

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>2</sup>

Vegetation native to the Richgrove - San Joaquin Valley area includes *Stipa* annual grasses, California oakwoods, tule marshes, California Steepe and *Artriplex sacrobatus* brush and shrub communities. The *Stipa* annual grasses occupy the largest portion of non-agricultural lands in the valley. Most of the original grasslands in the Richgrove area have been replaced by cultivated crops due to the increased availability of water for irrigation. Non-cultivated areas are limited primarily to urbanized areas, rough terrain, or areas without easy access to irrigation water. A list of rare and endangered plant species found in Tulare County is contained in the Tulare County Biological Resources Element, an unadopted element of the Tulare County General Plan. No rare or endangered plant species are known to occur in the project area.

Animal species in the proposed project area are typical of those found throughout Tulare County. A list of rare and endangered animal species is also found in the Biological Resources Element and shows Richgrove to be within the historic habitat of the Giant Garter snake, a rare animal and the San Joaquin Kit Fox, an endangered species.

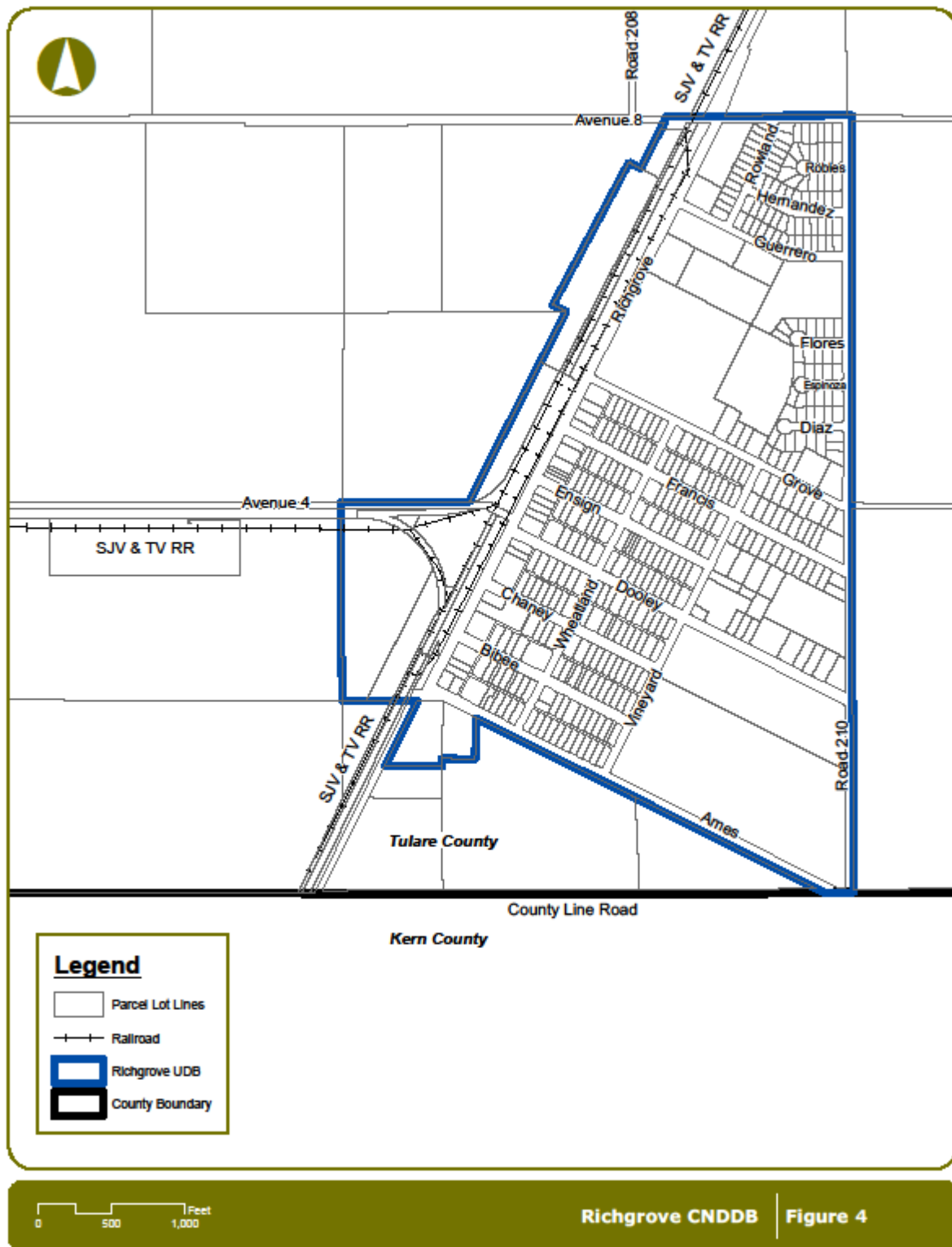
A California Natural Diversity Database (CNDDDB) search conducted on November 8, 2016, indicates there are special status species within the Richgrove Quadrant (**see Figure 4**) Species List (which includes the Richgrove Planning Study Area) consisting of one animal species and two plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); San Joaquin adobe sunburst (*Pseudobahia peirsonii*, federally threatened and state endangered), California jewelflower (*Caulanthus californicus*, federally endangered and state endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

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<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <http://www.dfg.ca.gov/biogeodata/cnddb/mapsanddata.asp>; California Department of Fish and Wildlife. California Natural Diversity Database. November 8, 2016

# RICHGROVE COMMUNITY PLAN UPDATE

Figure 4 - CNDDDB Map - Richgrove



# RICHGROVE COMMUNITY PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Richgrove are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Richgrove] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>4</sup>

The basic industry in Tulare County is agriculture due to the fact that nearly 95% of the land on the valley floor is arable. Agricultural production is enhanced by the existence of vast amounts of prime agricultural soils {Capability Class I and II as determined by the Soil Conservation Service). Prime agricultural lands are defined as those lands suitable for sustaining high yields of most climatically adapted crops with minimum costs of development and management due to the absence of major limitations in land characteristics. It should be noted that the lands best suited for cultivation are also those best suited for urban development.

According to the Soil Survey of Tulare County, California, Western Part (2003), prepared by the United States Department of Agriculture (USDA), Soil Conservation Service, (**see Figure 5** Soil Map – Richgrove) the following soil type is located in Richgrove:

**Exeter loam, 0 to 2 percent slopes**, Class III (irrigated); Class IV (non-irrigated) - moderately good cultivable land; moderately suited to urban development.

**Exeter sandy loam, 0 to 2 percent slopes**, Class III (irrigated); Class IV (non-irrigated) - moderately good cultivable land; moderately suited to urban development

Greenfield sandy loam: Class I (irrigated); Class IV (non-irrigated) - prime farmland; well suited to urban development

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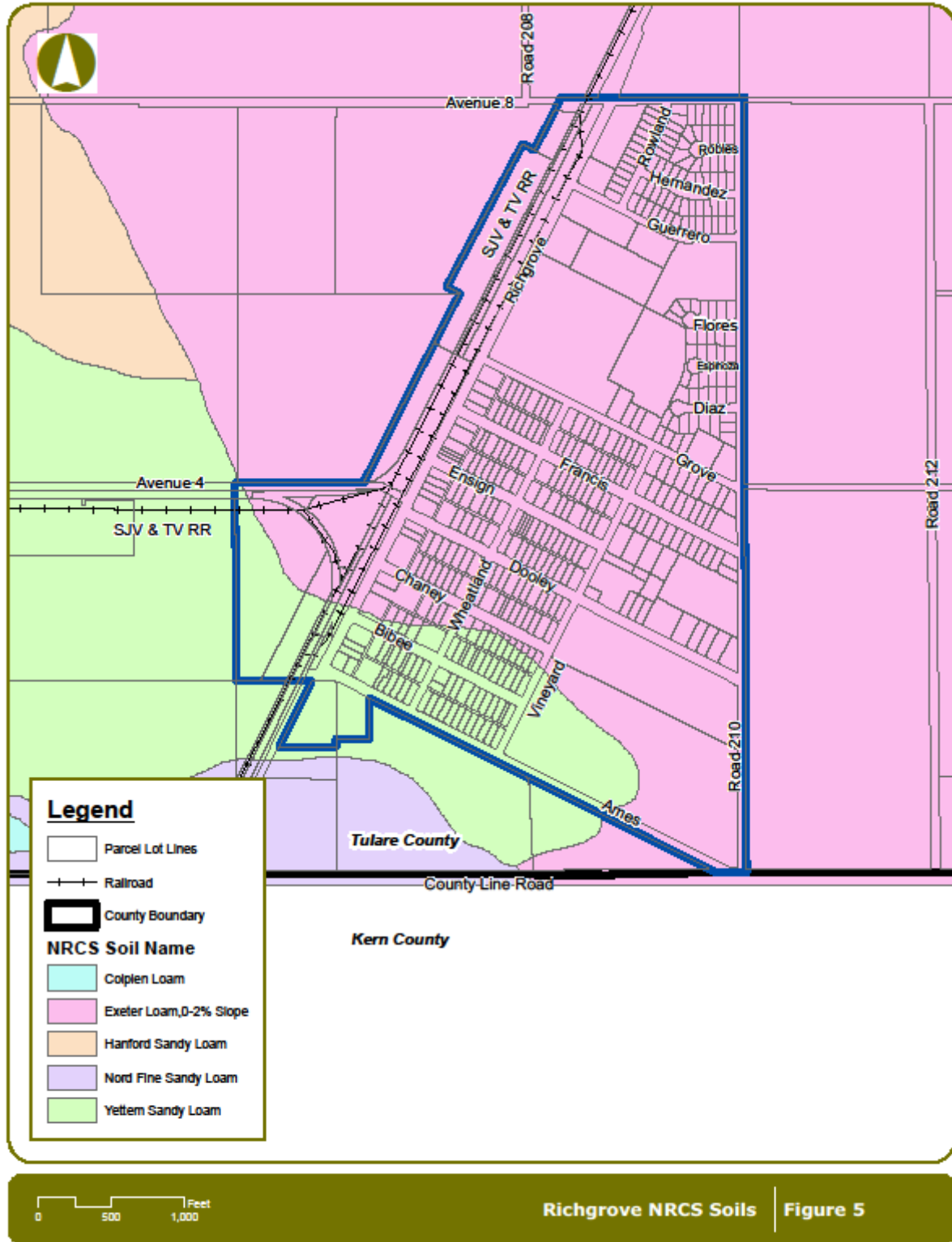
<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>4</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009



# RICHGROVE COMMUNITY PLAN

Figure 5 - NRCS Soil Types for Richgrove





# RICHGROVE COMMUNITY PLAN

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## PHYSICAL CONDITIONS

### Air Quality

Richgrove is situated within the San Joaquin Air Basin, a basin covering more than 25,000 square miles with approximately 50 square miles of inland water. The Air Basin extends from the Sacramento River Delta in the north to the southern portion of Kern County and is bounded by the Coast Range to the west, the Sierra Nevada's to the east, the Tehachapi Mountains to the south, and the Sacramento Valley and Mountain Counties Air Basin to the north.

The San Joaquin Valley experiences conditions unusually favorable to the development of air pollution. Light winds and stable atmospheric conditions provide frequent opportunities for pollutants to accumulate in the atmosphere. The general air circulation patterns permit the transportation of pollutants over long distances along the axis of the valley. Air pollution is most likely to occur when the atmosphere is stable and winds light over a long period of time. Photochemical smog in the summer is enhanced by the light winds and the almost unbroken succession of warm, sunny days. In the fall and winter the atmosphere is often stable with only light winds for long periods of time. Often, in winter, a temperature inversion occurs at or near the ground surface and tule fog conditions prevail. Under such conditions, pollutants in the atmosphere are not rapidly dispersed.

Other factors, beside area climate and meteorology, contribute to area air quality. The agricultural, industrial, and travel characteristics of persons living and working in Tulare County and the Air Basin generate materials which pollute the air. Concentrations of gaseous pollutants are largely generated by mobile and stationary sources. These pollutants include photochemical oxidants, carbon monoxide, nitrogen dioxide, sulfur dioxide, and hydrocarbons.

Agriculture and related industries (considered stationary sources) being the dominant industry within Tulare County and the Air Basin, are the main contributors of particulate matter or TSP, Total Suspended Particulates. Of all the stationary sources combined, agriculture continues to contribute the largest percentage of particulate matter in the Air Basin measured in tons per day of average daily emissions: 36% in 1970 and 86% in 1979, and 32% in 1970 and 84% in 1979 of all sources in the Air Basin. Mobile sources are the main contributors of organic gases and oxides of nitrogen which are precursor materials for the generation of photochemical smog in the atmosphere. Mobile sources were responsible for 53% and 22% respectively in 1970 and 1979 of total organic gases and 63% and 62% of oxides of nitrogen in the Air Basin. Mobile sources contributed 82% of the total carbon monoxide pollutants in 1979; motor vehicles constituted 78% of all mobile sources in the Air Basin. While the mobile source contribution of carbon monoxide in the Air Basin was similar in 1970 (83%), the motor vehicle portion of mobile source total was down from 94% in 1970. Specific air quality data for the Richgrove area is not available; the closest monitoring station is located in Visalia. Air Quality conditions in the Visalia area for 1982 are as follows:

The Richgrove Community Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although

# RICHGROVE COMMUNITY PLAN

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emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Richgrove is located near the southwestern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Richgrove include agricultural activities.

## Flooding<sup>5</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C2325E (see **Figure 6**), shows the majority of the Richgrove footprint is within Flood Zoned X, and within 0.2% chance flood; areas of 1 % annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas

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<sup>5</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C2325E, Panel Number 2325 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

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protected by levees from 1% annual chance of flood. The southern portion of the planning area in Richgrove is subject to 100-year flood hazard (Flood Zone A). The expected source of this flooding hazard is Rag Gulch / Five Dogs Creek and its distributary channels located immediately to the south and north of Richgrove. FEMA requires development in Flood Zones A to be constructed so that a building's ground floor elevation is above the flood contour line existing in the flood are.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”<sup>6</sup>

"Special flood hazard area" is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. "Floodway" means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within West Goshen there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

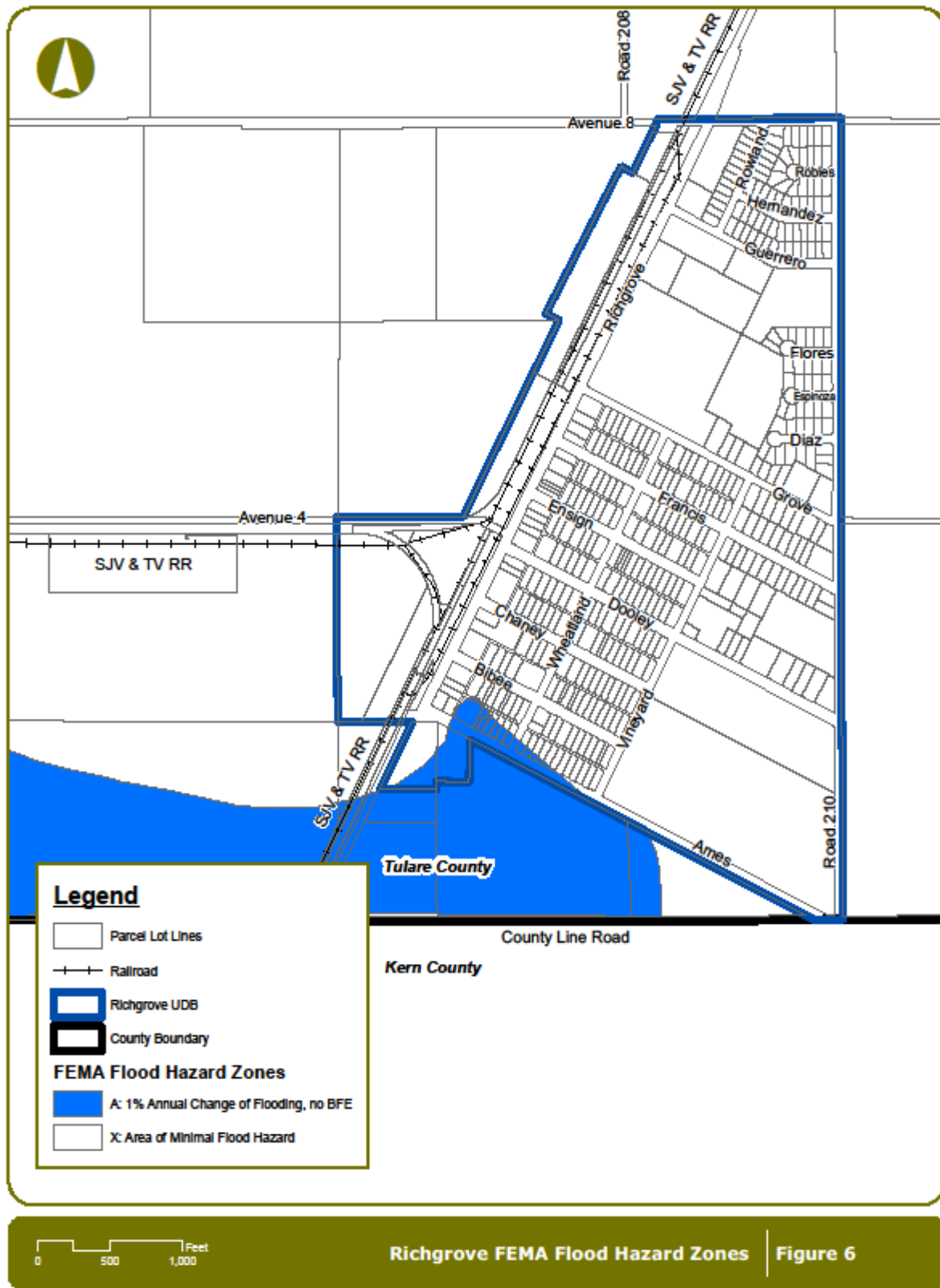
The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

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<sup>6</sup> General Plan Background Report, page 8-14

# RICHGROVE COMMUNITY PLAN

Figure 6 - FEMA Flood Map



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## Noise<sup>7</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan Background Report Richgrove lies outside any noise source.

## **INFRASTRUCTURE**

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Richgrove, this Community Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.).

### Energy: Natural Gas/Electricity

PG &E provides service to Richgrove.

### Domestic Water & Wastewater

Surface water in the Richgrove area is obtained from the Friant- Kern Canal. Groundwater is divided into three vertical zones: (1) water that is unconfined or semiconfined and has fairly free flow to the surface, (2) water that is confined below the hardpan and other confining beds, and (3) brackish and saline connate water or modified connate water that underlies most of the valley to the basement complex. The California Department of Water Resources water table map for the area shows groundwater in the principal unconfined aquifer at a depth of approximately 175 feet in 1986. A review of the water table maps for the past 20 years shows that the water has risen from a depth of 350 feet in 1970 to the current depth

Residents are concerned with the current water situation and want to it addressed soon. In Richgrove, the town is provided water by Richgrove Community Services District. The District has two operable wells that supply Richgrove residents and businesses with potable water. One of these wells, Well No. 4, has experienced problems with two water quality contaminants, arsenic and DBCP, which have at times exceeded the MCLs. The other well, Well No. 5, has experienced problems with nitrates and arsenic. The failure of either of these wells would jeopardize the District's ability to meet demand. California (Rodriguez) Camp is a privately owned labor camp located two miles to the west of Richgrove. The California Camp water system has one water well that provides water to its residents. The well produces water that exceeds the Maximum Contaminant Level (MCL) for nitrate with levels

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<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# RICHGROVE COMMUNITY PLAN

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in the range of 130 ppm. The system's owner has received Notices of Violation and Compliance Citations as a result of this high nitrate level, almost three times the MCL. The Camp's system has no supply redundancy and residents would be completely without water in the event of the well's failure. No emergency connection exists.

A Preliminary Engineering Report, completed in January 2008, was prepared with the understanding that consolidation between California Camp and the RCSD was a priority. The preferred project alternative is to drill a new community water well (including a test well) and construct transmission lines that will connect the new well to the District and the Labor Camp. The planning study will include the evaluation of the feasibility of blending to achieve water quality improvement and reduce the concentration of contaminants to within the MCL required for drinking water systems. A blending project would require the construction of new storage facilities.

Domestic water and sewer service in Richgrove is provided by the Richgrove Community Services District (CSD), which was formed in March 1977. Table 13-1 shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Housing Element, May 2012 and Municipal Service Review, March 2007). Maps of the sewer and water systems are currently unavailable.

According to the Municipal Service Review 2007 (MSR), the Richgrove CSD operates a water supply and distribution system under the jurisdiction of the California Department of Health Services Division of Drinking Water and Environmental Management, which is responsible for the administration and enforcement of the Safe Drinking Water Act involving those systems in California with more than 200 connections. Richgrove CSD staff has indicated that there are 523 connections to the District's water system, which consists of two active wells and a third well which is currently not operational. The CSD relies solely on groundwater for its water supply. The water is chlorinated at the well sites, and will likely have permanent chlorination installed in the future.

Assuming 550 equivalent dwelling units (EDUs), in order to meet Tulare County Improvement Standards the Richgrove CSD water system would need to be capable of delivering a combined flow rate (from all source and storage facilities) of 1,200 gallons per minute (GPM) (500 GPM fire flow, and 700 GPM domestic demand) for a period of two hours while maintaining a minimum pressure of 25 pounds per square inch (PSI) to each lot served. The total supply source available for the CSD's water system is unknown. Prior to granting any sphere of influence (SOI) amendments that would increase demand for water services provided the CSD, the CSD's engineer should provide evidence that the increase in demand would not result in substandard pressures, or inadequate supply capacity for the remainder of the system. There is a project planned to install treatment on the well that is currently not in operation. It potentially will add capacity to the CSD's water system, and could also serve as a backup well should one of the existing wells be out of service.

The CSD owns and operates a Wastewater Treatment Facility (WWTF) located northeast of the community, which is operated under the provisions of Waste Discharge Requirements Order No. 83-088, issued by the California Regional Water Quality Control Board, Central Valley Region. Order No. 83-088 prescribes that the monthly average discharge flow shall not exceed 0.22 million gallons per day (MGD). According to the Wastewater User Charge Survey Report FY 2005-06 (Cal EPA State Water Resources Control Board, May 2006), the average dry weather flow at the WWTF is 0.25 MGD.

# RICHGROVE COMMUNITY PLAN

Based upon this information, it is determined that the CSD's WWTF is currently operating above its permitted capacity, indicating that additional connections to the sewer system cannot be support at this time.

Treated effluent from the CSD's WWTF is recycled through irrigation of alfalfa, which is indicative of the CSD's efforts to conserve its potable water sources. The CSD recently completed a "*Wastewater Treatment Facility Performance and Capacity Study*" (Provost & Pritchard, September 2005) in order to evaluate wastewater treatment options to bring the plant into compliance regarding flow to the plant, and to address other WWTF related issues. The "*Wastewater Treatment Facility Performance and Capacity Study*" identifies potential grant sources for the implementation of the proposed improvements, planned to occur in three phases. The potential grant sources identified in the study are small community wastewater grants, community development block grants, and grant assistance provided by the economic development administration. Without increasing the capacity of its WWTF, the CSD will be unable to support any additional connections to its sewer system.

**Table 14 - Existing Water & Wastewater Connections in Richgrove**

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
533	-	(--) <sup>1</sup>	523	<523 <sup>2</sup>	0

\* Data current as of May 2012

\*\*Data current as if March 2007

1 Excess capacity likely, but further study is needed to determine available capacity  
(Source: MSR)

2 System is over capacity

## Storm Drainage

A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to a point where it is discharged into a channel, ponding basin, or piped system. The system consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Richgrove does not currently have a storm drainage system.”<sup>8</sup>

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<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

# RICHGROVE COMMUNITY PLAN

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## Solid Waste

Solid waste disposal services for the Community of Richgrove is provided by South Tulare Richgrove, a private company. Solid waste generated in Richgrove can be disposed of at Teapot Dome Landfill, located at 21063 Avenue 128, Porterville.

## **PUBLIC SERVICES**

### Community Resource Center

The community lacks many resources and needs a place where residents can go to access information and county wide resources. The School and Community Service District Buildings provided the base for this currently.

### Sheriff

Law Enforcement services are provided in Richgrove by the Tulare County Sheriff's Department sub-station, located in Porterville approximately 21 miles north of Richgrove. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

### Fire

Fire protection and emergency medical services are provided for Richgrove by the Tulare County Fire Department. The community is served by Tulare County Fire Department Station #10, located at 20892 Grove Drive in Richgrove. Engine 10 is assigned to this location.

Forty-three fire hydrants are found within Richgrove (**see Table 15**). These fire hydrants are located within the County right-of-way. **Figure 7** display Existing Fire Hydrants in Richgrove.



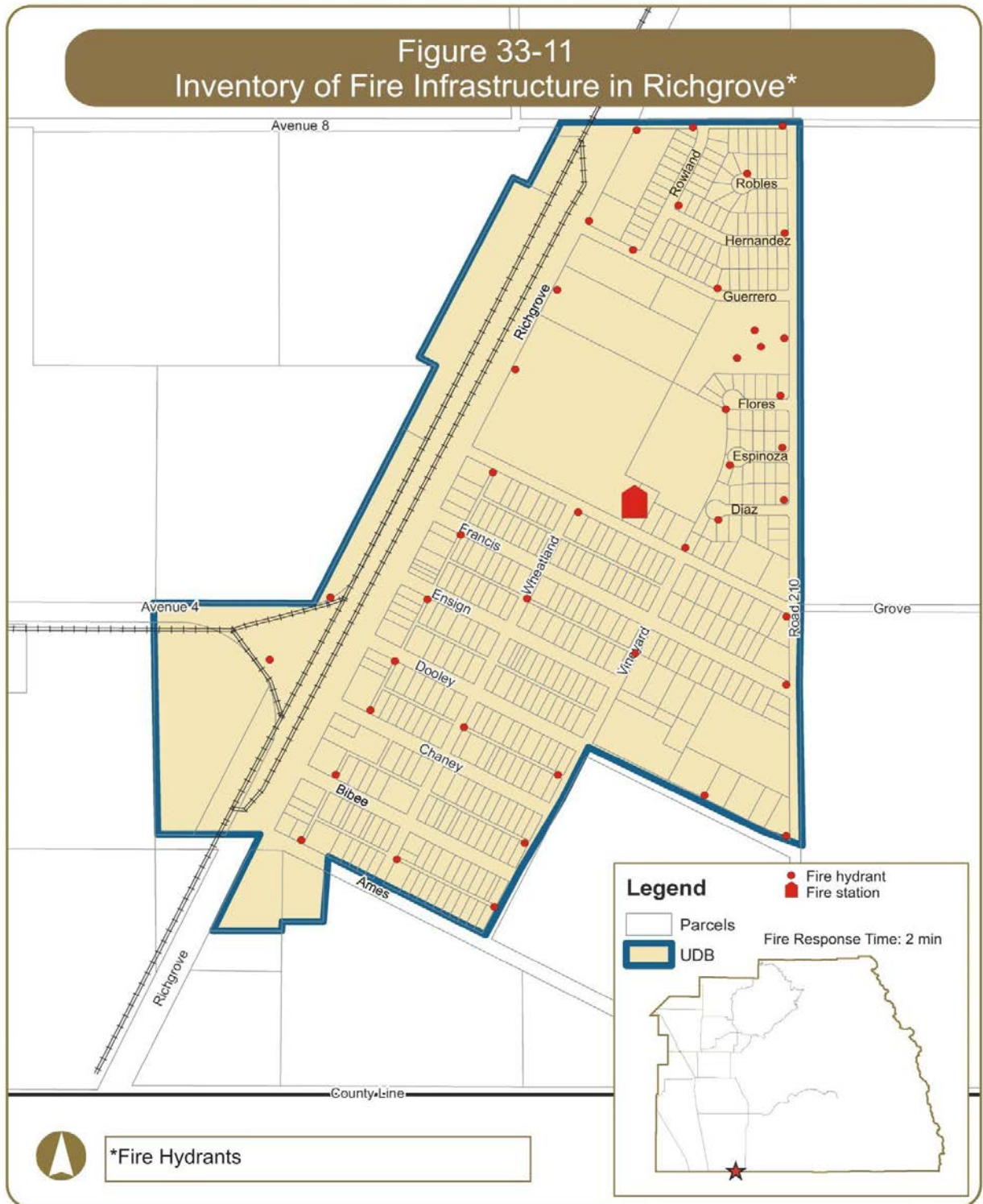
# RICHGROVE COMMUNITY PLAN

**Table 15 - Existing Fire Hydrants**

1	Avenue 8 east of Richgrove Drive	23	Grove Drive and Wheatland Drive
2	Avenue 8 and Rowland Street	24	Grove Drive and Vineyard Drive
3	Avenue 8 and Road 210	25	Grove Drive and Road 210
4	Robles Court west of Road 210	26	Francis Drive and Road 210
5	Road 210 and Hernandez Avenue	27	Francis Drive east of Richgrove Drive
6	Rowland Street and Hernandez Avenue	28	Wheatland Drive south of Francis Drive
7	Guerrero Avenue west of Road 210	29	Vineyard Drive south of Francis Drive
8	Rowland Street and Guerrero Avenue	30	Dooley Drive and Road 210
9	Richgrove Drive and Guerrero Avenue	31	Dooley Drive west of Road 210
10	Richgrove Drive south of Guerrero Avenue	32	Ensign Drive east of Richgrove Drive
11	Road 210 south of Guerrero Avenue (apartment complex)	33	Dooley Drive east of Richgrove Drive
12	Road 210 south of Guerrero Avenue (apartment complex)	34	Avenue 4 west of Richgrove Drive
13	Road 210 south of Guerrero Avenue (apartment complex)	35	Richgrove Drive south of Avenue 4
14	Road 210 south of Guerrero Avenue (apartment complex)	36	Wheatland Drive south of Dooley Drive
15	Road 210 and Flores Avenue	37	Vineyard Drive south of Dooley Drive
16	Flores Avenue west of Road 210	38	Chaney Drive east of Richgrove Drive
17	Espinoza Avenue and Road 210	39	Bibee Drive east of Richgrove Drive
18	Espinoza Avenue west of Road 210	40	Vineyard Drive south of Chaney Drive
19	Diaz Avenue and Road 210	41	Vineyard Drive south of Bibee Drive
20	Diaz Avenue west of Road 210	42	Wheatland Drive south of Bibee Drive
21	Richgrove Drive north of Grove Drive	43	Ames Drive east of Richgrove Drive
22	Grove Drive east of Richgrove Drive		

# RICHGROVE COMMUNITY PLAN

Figure 7 - Inventory of Fire Infrastructure



# RICHGROVE COMMUNITY PLAN

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## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Richgrove Community Plan Area is within the Richgrove School District (20890 Grove Drive), a one-school district. According to records from the California Department of Education, 651 students are enrollment in the District (750 students according to their website).<sup>9</sup> Of these students, approximately 97 percent are Hispanic, one percent Native American or Alaskan Native, one percent Asian, and one percent Filipino. The District provides K-8 education at its Richgrove School. High School students attend high school in the Delano Unified High School District (Cesar E. Chavez High School).

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>10</sup> The nearest public library is in Delano, 10 miles away. Terra Bella Branch Library is located 15 miles south of Richgrove (see Table 16).

Table 16 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Terra Bella	Terra Bella Branch 23825 Avenue 92 Terra Bella, CA 93201	Monday thru Thursday: 8:00 a.m. – 12:00 p.m.

Library hours current as of September 2017

## Parks

Parks and Recreation: The grounds of the Richgrove Elementary School are currently the only developed recreational facilities available to the community. While in need for improvements, it appears the community's needs are otherwise served by the school. There is currently a park project in construction in Richgrove.

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<sup>9</sup> Richgrove School District Website: <http://www.richgrove.org/school/> (November 19, 2017)

<sup>10</sup> General Plan Background Report, page 7-96

# RICHGROVE COMMUNITY PLAN

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## THE RICHGROVE COMMUNITY PLAN

### ROAD CONDITIONS/COMPLETE STREETS/TRANSPORTATION

From the previous Community Plan, traffic count data for the area obtained from the Tulare County Public Works Department show Richgrove Drive to have, by far, the highest average daily count of 2,700 trips. All other streets within the community experienced counts on the average of 100 trips with the exception of an average on Ames Drive of 250 trips.

Richgrove Drive currently acts as the main route for traffic into and out of the community, with all other streets directing local traffic to Richgrove Drive. It is assumed that Road 210 will be improved to the standards of a full county road (i.e. 60 feet width, curb and gutter, sidewalks, etc.) and extended between Dooley Drive and Avenue 8 as development takes place in the northeast portion of the community. Development of Road 210 would provide easy access to future residential development in the northern and eastern portions of the community.

#### Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was constructed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>11</sup>

#### Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”

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<sup>11</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

# RICHGROVE COMMUNITY PLAN

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For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Roads

There are several roadways in Richgrove that are in need of repair. Currently repair work is being planned and performed on Richgrove Drive. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
  - ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
  - ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- .Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>12</sup>

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<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element

# RICHGROVE COMMUNITY PLAN

**Table 17** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 8** graphically displays this information on a map.

**Table 17 - Road Maintenance Strategies**

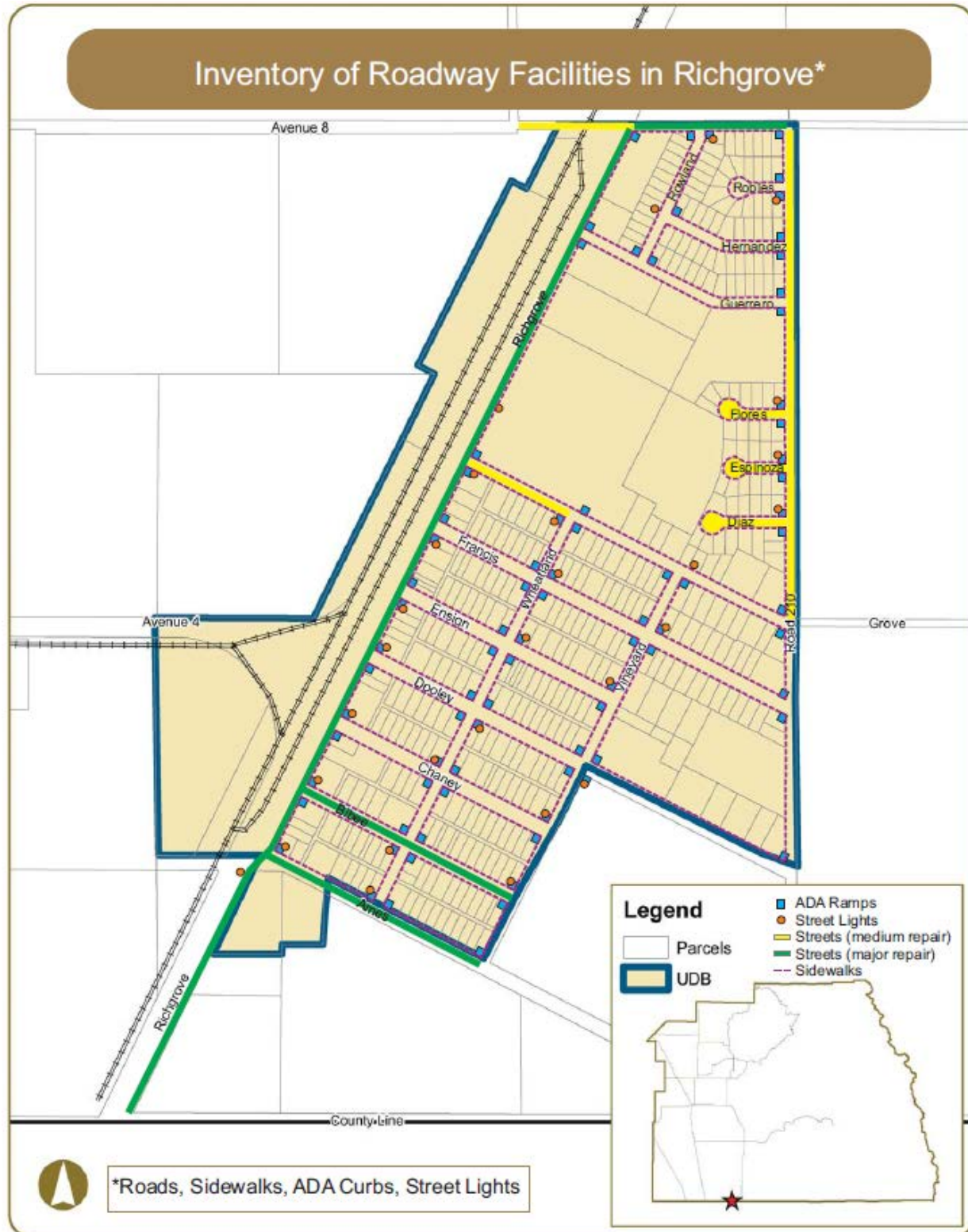
Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Ames Drive	Richgrove Drive to Vineyard Drive	GRX
2	Avenue 4	Richgrove Drive to Wheatland Drive	CHIP
3	Avenue 8	Road 208 to Richgrove Drive	CHIP
4	Avenue 8	Richgrove Drive to Road 210	GRX
5	Bibee Drive	Richgrove Drive to Vineyard Drive	RCST
6	Diaz Avenue	Road 210 to west end	CHIP
7	Espinoza Avenue	Road 210 to west end	CHIP
8	Flores Avenue	Road 210 to west end	CHIP
9	Richgrove Drive	Avenue 0 to Avenue 8	GRX
10	Road 210	Grove Drive to Avenue 8	CHIP

OLAY – overlay resurfacing operation CHIP – chip seal GRX – grind and remix	ACST – asphalt reconstruction RCST – cold mix reconstruction
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(Source: County of Tulare Public Works, 2012)

# RICHGROVE COMMUNITY PLAN

### Figure 8 - Inventory of Roadway Facilities in Richgrove





# RICHGROVE COMMUNITY PLAN

## Street Lights

**Table 18** identifies the location of existing street lights that are maintained by Tulare County, in Richgrove, as well as their specifications. **Figure 8** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.”<sup>13</sup>

**Table 18 - Existing Street Lights in Richgrove**

Specifications of Existing Street Lights								
No	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	400' south of Ames Drive	Richgrove Drive	West Side	1529884E	9500	W	S/E	SCE
2	Ames Drive	Richgrove Drive	NE Corner	859	5800	W	W	SCE
3	Ames Drive	Wheatland Drive	NW Corner	857	5800	W	D	SCE
4	Avenue 8	Rowland Street	SE Corner	858	5800	C	N	SCE
5	Bibee Drive	Vineyard Drive	NW Corner	861	5800	W	E	SCE
6	Bibee Drive	Wheatland Drive	SW Corner	866	5800	W	NE	SCE
7	Chaney Drive	Vineyard Drive	NW Corner	862	5800	W	E	SCE
8	Chaney Drive	Richgrove Drive	NE Corner	860	5800	W	W	SCE
9	Chaney Drive	Wheatland Drive	NW Corner	855	5800	W	S	SCE
10	Diaz Avenue	Road 210	NW Corner	856	5800	C	E	SCE
11	Dooley Drive	Vineyard Drive	SE Corner	865	5800	W	W	SCE
12	Dooley Drive	Richgrove Drive	NE Corner	863	5800	W	W	SCE
13	Dooley Drive	Wheatland Drive	SE Corner	866	5800	W	N	SCE
14	Ensign Drive	Richgrove Drive	SE Corner	862	5800	W	W	SCE
15	Ensign Drive	Wheatland Drive	NE Corner	860	5800	W	S	SCE
16	Ensign Drive	Vineyard Drive	NW Corner	855	5800	W	SE	SCE
17	Espinoza Ave	Road 210	NW Corner	856	5800	C	E	SCE
18	Flores Avenue	Road 210	NW Corner	865	5800	C	E	SCE
19	Francis Drive	Richgrove Drive	SE Corner	863	5800	W	W	SCE
20	Francis Drive	Wheatland Drive	NE Corner	866	5800	W	SW	SCE
21	Francis Drive	Vineyard Drive	NE Corner	862	5800	W	S	SCE
22	Grove Drive	Vineyard Drive	NE Corner	860	5800	W	S	SCE
23	Grove Drive	Richgrove Drive	SE Corner	855	5800	W	W	SCE
24	Grove Drive	Wheatland Drive	SW Corner	856	5800	W	N	SCE
25	Hernandez Ave	Rowland Street	West Side	865	5800	C	E	SCE
26	N of Grove Dr	Richgrove Drive	East Side	863	5800	W	W	SCE
27	Robles Court	Road 210	SW Corner	866	5800	C	E	SCE

(Source: Tulare County Public Works, March 2013)

## Sidewalks

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-

<sup>13</sup> Action Program 9, Tulare County 2015 Housing Element



# RICHGROVE COMMUNITY PLAN

foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

The County and VRPA Technologies surveyed existing sidewalks within the Community. **Table 19** identifies the location of existing sidewalks in Richgrove. **Figure 8** also displays this information graphically. The sidewalks represented in **Table 19** do not distinguish between ADA compliant sidewalks and noncompliant sidewalks. The majority of sidewalks represented below were constructed prior to current ADA guidelines and would be considered non-ADA compliant facilities. Such noncompliant facilities would require complete reconstruction to be considered ADA compliant

**Table 19 - Existing Sidewalks in Richgrove**

Location of Existing Sidewalks			
No.	Roadway	Limits	Location
1	Ames Drive	Richgrove Drive to Vineyard Drive	North side
2	Avenue 4	Richgrove Drive to Road 210	North side
3	Avenue 4	Richgrove Drive to Road 210	South side
4	Avenue 8	Rowland Street to Road 210	South side
5	Bibee Drive	Richgrove Drive to Vineyard Drive	North side
6	Bibee Drive	Richgrove Drive to Vineyard Drive	South side
7	Chaney Drive	Richgrove Drive to Vineyard Drive	North side
8	Chaney Drive	Richgrove Drive to Vineyard Drive	South side
9	Diaz Avenue	Road 210 to west end	North side
10	Diaz Avenue	Road 210 to west end	South side
11	Dooley Drive	Richgrove Drive to Vineyard Drive	South side
12	Dooley Drive	Richgrove Drive to Road 210	North side
13	Ensign Drive	Richgrove Drive to Vineyard Drive	North side
14	Ensign Drive	Richgrove Drive to Vineyard Drive	South side
15	Espinoza Avenue	Road 210 to west end	North side
16	Espinoza Avenue	Road 210 to west end	South side
17	Flores Avenue	Road 210 to west end	North side
18	Flores Avenue	Road 210 to west end	South side
19	Francis Drive	Richgrove Drive to Road 210	North side
20	Francis Drive	Richgrove Drive to Road 210	South side
21	Guerrero Avenue	Richgrove Drive to Road 210	North side
22	Guerrero Avenue	Richgrove Drive to Road 210	South side
23	Hernandez Avenue	Rowland Street to Road 210	North side
24	Hernandez Avenue	Rowland Street to Road 210	South side
25	Richgrove Drive	Ames Drive to Avenue 8	East side
26	Road 210	Dooley Drive to Avenue 8	West side
27	Robles Court	Road 210 to west end	North side
28	Robles Court	Road 210 to west end	South side
29	Rowland Street	Guerrero Avenue to Avenue 8	East side
30	Rowland Street	Guerrero Avenue to Avenue 8	West side
31	Vineyard Drive	Ames Drive to Avenue 4	West side
32	Vineyard Drive	Dooley Drive to Avenue 4	East side
33	Wheatland Drive	Ames Drive to Avenue 4	East side
34	Wheatland Drive	Ames Drive to Avenue 4	West side

(Source: County of Tulare Public Works and VRPA Technologies, February 2014)

# RICHGROVE COMMUNITY PLAN

## **ADA CURB RAMPS:**

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are several ADA compliant curb ramps located within Richgrove (see Table 20).”<sup>14</sup>

**Table 20- Existing ADA Curb Ramps in Richgrove**

<b>Location of Existing ADA Ramps</b>			
<b>No.</b>	<b>East-West Roadway</b>	<b>North-South Roadway</b>	<b>Location</b>
1	Ames Drive	Richgrove Drive	NE Corner
2.	Ames Drive	Wheatland Drive	NE Corner
1	Ames Drive	Wheatland Drive	NW Corner
2	Ames Drive	Vineyard Drive	NW Corner
3	Avenue 4	Richgrove Drive	NE Corner
4	Avenue 4	Richgrove Drive	SE Corner
5	Avenue 4	Wheatland Drive	SW Corner
6	Avenue 4	Wheatland Drive	NE Corner
7	Avenue 4	Wheatland Drive	SE Corner
8	Avenue 4	Vineyard Drive	SE Corner
9	Avenue 4	Vineyard Drive	SW Corner
10	Avenue 4	Road 210	NW Corner
11	Avenue 4	Road 210	SW Corner
12	Avenue 8	Richgrove Drive	SE Corner
13	Avenue 8	Rowland Street	SE Corner
14	Avenue 8	Rowland Street	SW Corner
15	Avenue 8	Road 210	SW Corner
16	Bibee Drive	Richgrove Drive	NE Corner
17	Bibee Drive	Richgrove Drive	SE Corner
18	Bibee Drive	Wheatland Drive	NE Corner
19	Bibee Drive	Wheatland Drive	NW Corner
20	Bibee Drive	Wheatland Drive	SE Corner
21	Bibee Drive	Wheatland Drive	SW Corner
22	Bibee Drive	Vineyard Drive	NW Corner
23	Bibee Drive	Vineyard Drive	SW Corner
24	Chaney Drive	Richgrove Drive	NE Corner
25	Chaney Drive	Richgrove Drive	SE Corner
26	Chaney Drive	Wheatland Drive	NE Corner

<sup>14</sup> Action Program 9, Tulare County 2015 Housing Element

# RICHGROVE COMMUNITY PLAN

Location of Existing ADA Ramps			
27	Chaney Drive	Wheatland Drive	NW Corner
28	Chaney Drive	Wheatland Drive	SE Corner
29	Chaney Drive	Wheatland Drive	SW Corner
30	Chaney Drive	Vineyard Drive	NW Corner
31	Chaney Drive	Vineyard Drive	SW Corner
32	Diaz Avenue	Road 210	NW Corner
33	Diaz Avenue	Road 210	SW Corner
34	Ames Drive	Richgrove Drive	NE Corner
35	Ames Drive	Wheatland Drive	NE Corner
36	Dooley Drive	Richgrove Drive	NE Corner
37	Dooley Drive	Richgrove Drive	SE Corner
38	Dooley Drive	Wheatland Drive	NE Corner
39	Dooley Drive	Wheatland Drive	NW Corner
40	Dooley Drive	Wheatland Drive	SE Corner
41	Dooley Drive	Wheatland Drive	SW Corner
42	Dooley Drive	Vineyard Drive	NW Corner
43	Dooley Drive	Vineyard Drive	SW Corner
44	Dooley Drive	Vineyard Drive	NE Corner
45	Dooley Drive	Vineyard Drive	SE Corner
46	Dooley Drive	Road 210	NW Corner
47	Ensign Drive	Richgrove Drive	NE Corner
48	Ensign Drive	Richgrove Drive	SE Corner
49	Ensign Drive	Wheatland Drive	NE Corner
50	Ensign Drive	Wheatland Drive	NW Corner
51	Ensign Drive	Wheatland Drive	SE Corner
52	Ensign Drive	Wheatland Drive	SW Corner
53	Ensign Drive	Vineyard Drive	NW Corner
54	Ensign Drive	Vineyard Drive	SW Corner
55	Espinoza Avenue	Road 210	NW Corner
56	Espinoza Avenue	Road 210	SW Corner
57	Flores Avenue	Road 210	NW Corner
58	Flores Avenue	Road 210	SW Corner
59	Francis Drive	Richgrove Drive	NE Corner
60	Francis Drive	Richgrove Drive	SE Corner
61	Francis Drive	Wheatland Drive	NE Corner
62	Francis Drive	Wheatland Drive	NW Corner
63	Francis Drive	Wheatland Drive	SE Corner
64	Francis Drive	Wheatland Drive	SW Corner
65	Francis Drive	Vineyard Drive	NW Corner
66	Francis Drive	Vineyard Drive	SW Corner
67	Francis Drive	Vineyard Drive	NE Corner
68	Francis Drive	Vineyard Drive	SE Corner
69	Francis Drive	Road 210	NW Corner
70	Francis Drive	Road 210	NW Corner
71	Guerrero Avenue	Richgrove Drive	NE Corner
72	Guerrero Avenue	Richgrove Drive	SE Corner
73	Guerrero Avenue	Rowland Street	NE Corner
74	Guerrero Avenue	Rowland Street	NW Corner

# RICHGROVE COMMUNITY PLAN

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Location of Existing ADA Ramps			
75	Guerrero Avenue	Road 210	NW Corner
76	Guerrero Avenue	Road 210	SW Corner
77	Hernandez Avenue	Rowland Street	NE Corner
78	Hernandez Avenue	Road 210	NW Corner
79	Hernandez Avenue	Road 210	SW Corner
80	Robles Court	Road 210	NW Corner
81	Robles Court	Road 210	SW Corner

(Source: County of Tulare Public Works, August 2013)

## **TRANSIT AND BUS STOPS:**

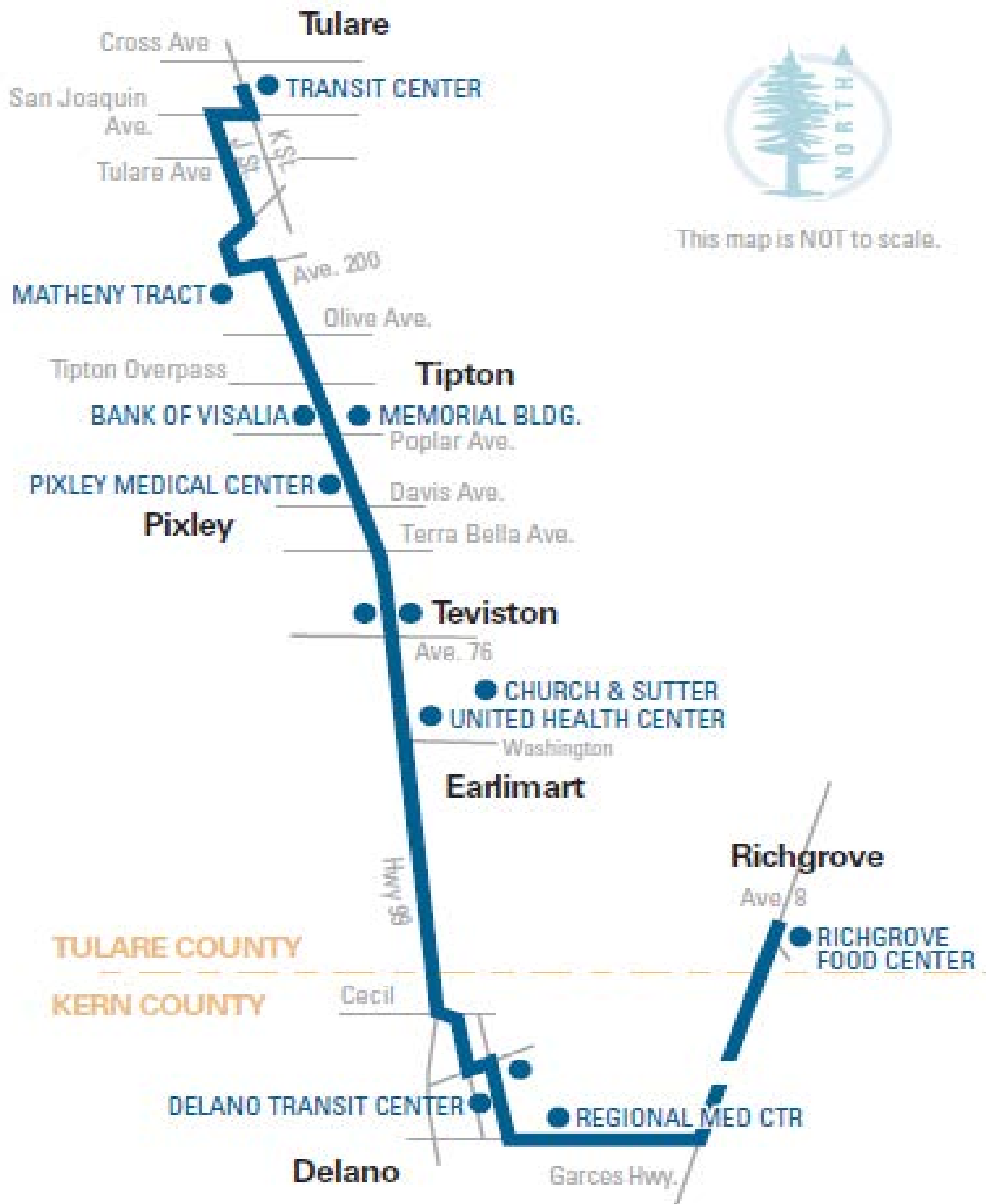
The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Transit service is provided in Richgrove through the Tulare County Area Transit (TCAT). The TCAT South County Route 20 provides a fixed-route service Monday through Sunday. Weekday service includes 10 southbound arrival/departure times, and nine northbound arrival/departure times. This route links Tipton to the northbound City of Tulare, and the cities of Tipton, Pixley, Teviston, Richgrove, and Delano to the south. All TCAT buses have wheelchair lifts and bike racks. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>).

TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Richgrove, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

# RICHGROVE COMMUNITY PLAN

Figure 9 – TCAG (TCAT) Transit



# RICHGROVE COMMUNITY PLAN

Figure 10 - Transit Southbound/Northbound

Southbound											
WEEKDAY SERVICE											
TULARE O Transit Center	MATHENY TRACT & Canal	TIPTON Bank of Visalia	TIPTON Memorial Building	PIXLEY Pixley Med Center	TEVISTON Avenue 76	EARLMART Church & Sutter	EARLMART United Health Center	DELANO Transit Center	DELANO Vallarta Mkt.	DELANO Regional Med Center	RICHGROVE Richgrove Food Center
5:45	5:56	6:09	6:11	6:21	6:26	6:32	6:35	ARR	DEP	—	—
6:45	6:56	7:09	7:11	7:21	7:26	7:32	7:35	7:48	7:53	7:56	—
7:27	7:38	7:51	7:53	8:03	8:08	8:14	8:17	8:30	8:35	8:38	—
9:04	9:15	9:28	9:30	9:40	9:45	9:51	9:54	10:07	10:12	10:15	—
10:41	10:52	11:05	11:07	11:17	11:22	11:28	11:31	11:44	11:49	11:52	11:57
12:18	12:29	12:42	12:44	12:54	12:59	1:05	1:08	1:21	1:26	1:29	12:12
1:43	1:54	2:07	2:09	2:19	2:24	2:30	2:33	2:46	2:51	2:54	2:59
2:37	2:48	3:01	3:03	3:13	3:18	3:24	3:27	3:40	3:45	3:48	3:14
4:53	5:04	5:17	5:19	5:29	5:34	5:40	5:43	5:56	6:01	6:04	—
5:51	6:02	6:15	6:17	6:27	6:32	6:38	6:41	6:54	6:59	7:02	—
WEEKEND SERVICE											
8:30	8:41	8:54	8:56	9:06	9:11	9:17	9:20	9:33	9:38	9:41	—
10:53	11:04	11:17	11:19	11:29	11:34	11:40	11:43	11:56	12:01	12:04	—
2:06	2:17	2:30	2:32	2:42	2:47	2:53	2:56	3:09	3:14	3:17	—
4:29	4:40	4:53	4:55	5:05	5:10	5:16	5:19	5:32	5:37	5:40	—

Northbound										
WEEKDAY SERVICE										
DELANO Regional Med Center	DELANO Vallarta Mkt.	DELANO Transit Center	EARLMART Church & Sutter	EARLMART United Health Center	TEVISTON Avenue 76	PIXLEY Pixley Med Center	TIPTON Bank of Visalia	TIPTON Memorial Building	MATHENY TRACT Pratt & Addie	TULARE O Transit Center
—	—	—	—	6:35	6:41	6:46	6:56	6:58	7:12	7:22
—	—	7:53	8:09	8:12	8:18	8:23	8:33	8:35	8:49	8:59
—	—	8:35	8:51	8:54	9:00	9:05	9:15	9:17	9:31	9:41
—	—	10:12	10:28	10:31	10:37	10:42	10:52	10:54	11:08	11:18
12:27	12:32	12:35	12:48	12:51	12:57	1:02	1:12	1:14	1:28	1:38
—	—	1:26	1:42	1:45	1:51	1:56	2:06	2:08	2:22	2:32
3:29	3:34	3:37	3:50	3:53	4:10	4:15	4:25	4:27	4:41	4:51
—	—	3:45	4:01	4:04	4:26	4:31	4:41	4:43	4:57	5:07
—	—	6:01	6:17	6:20	6:26	6:31	6:41	6:43	6:57	7:07
—	—	6:59	7:15	7:18	7:24	7:29	7:39	7:41	7:55	8:05
WEEKEND SERVICE										
—	—	9:38	9:53	9:56	10:02	10:07	10:17	10:19	10:33	10:43
—	—	12:01	12:16	12:19	12:25	12:30	12:40	12:42	12:56	1:06
—	—	3:14	3:29	3:32	3:38	3:43	3:53	3:55	4:09	4:19
—	—	5:37	5:52	5:55	6:01	6:06	6:16	6:18	6:32	6:42

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. Currently, none of those facilities are close to Richgrove.

## AMTRAK

The Corcoran AMTRAK station, located 25 miles to the northwest in Kings County, is the closest station to Allensworth providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

# RICHGROVE COMMUNITY PLAN

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## Aviation

The nearest operational general aviation airport is Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 30 miles south of Richgrove and offers direct flights to several destinations.

Fresno Yosemite International Airport (FAT), approximately 70 miles northwest of Richgrove, is the principal passenger and airfreight airport in the central San Joaquin Valley. Visalia Municipal Airport, is approximately 40 miles north.

## State Route 99

State Route (SR) 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South. Richgrove is located approximately seven (7) miles east of SR 99

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must "meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan." The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

# RICHGROVE COMMUNITY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Richgrove Community Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Community Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Richgrove to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan, Richgrove Community Plan, and input received from Richgrove citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### *Community Development*

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming

commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.

3. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
4. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
5. The County shall work with the Schools to provide safe routes to school.
6. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Richgrove Community Plan.
7. The Richgrove Urban Development Boundary should be reviewed every five years to determine if amendments are appropriate.
8. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.



# RICHGROVE COMMUNITY PLAN

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Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

### **GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Richgrove.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Richgrove.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

# RICHGROVE COMMUNITY PLAN

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## Economic Base

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

#### Policies:

1. Encourage the Richgrove to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

#### Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.

2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## Environmental Quality and Public Safety

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Richgrove citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

#### Objective: Protect Agricultural Lands:

Land within the respective Richgrove, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

# RICHGROVE COMMUNITY PLAN

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The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Richgrove.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Richgrove.

# RICHGROVE COMMUNITY PLAN

## ASSESSMENT OF LAND NEEDS

Currently within the Urban Development Boundary of Richgrove there is only 30 acres of agricultural use within the UDB's 234 acres, with no currently zone agriculture land within the UDB. Within the proposed Richgrove UDB of 278 acres, or an additional 44 acres, there are 76 acres still in agricultural use, such as orchards and other crops. This would constitute 28 percent (28%) of the UDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 3006 persons by 214 urbanized acres (area within the UDB), which equals 14 persons per urbanized acre (see Tables 20 and 21 showing population projections). Projecting the population at a 1.3% growth rate adds 643 persons in year 2030. Multiplying the 643 persons by the .07 (7% or a 1/14 ratio) suggests an additional 45 acres of land will be required to accommodate development by the year 2030 if projections are realized.

### Population Growth Forecast

Table 20 - Richgrove Population Projections			
Growth Rate	2015	2020	2030
0.013	3006	3165	3602

Table 21 - Population Projection		
Year	Population	Growth (%)
2016	3,045	0.013
2017	3,085	0.013
2018	3,125	0.013
2019	3,165	0.013
2020	3,207	0.013
2021	3,248	0.013
2022	3,290	0.013
2023	3,333	0.013
2024	3,377	0.013
2025	3,420	0.013
2026	3,465	0.013
2027	3,510	0.013
2028	3,556	0.013
2029	3,602	0.013
2030	3,649	0.013

### Demand Forecast

To determine whether there is enough land within the exiting UDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the UDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2013 American Community Survey (Survey)<sup>15</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2013 the community had 640 dwelling units (including vacant dwellings). At an annual growth rate of 1.3%, the projected housing units are 767 (137 more) units needed in 2030 within the UDB.

Assuming that the land area needed to accommodate future population, increases is consistent with annual population growth projections, the land use growth projections are also assessed at an annual growth rate of 1.3%. At this growth rate, there will be a total of approximately 378 acres of residential uses in year 2030; that is, an increase of approximately 68 acres.

<sup>15</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

# RICHGROVE COMMUNITY PLAN

As discussed earlier, there only 30 acres of undeveloped land within the existing UDB. As such, there is not adequate land available for development within the existing UDB to accommodate future residential growth of 137 units or 68 acres through the Year 2030. Therefore, the increase of 47 acres will greatly increase meeting this demand (137 units at 4 units per acre = 34.25 acres). Meanwhile commercial demand can be met with the mixed-use zoning overlay to increase economic development, as less than 5% of the land use is commercial. That has been adequate to date, but the Community could benefit from more commercial opportunities to increase jobs to housing balances and lower vehicle miles traveled.

**Table 22 - Existing/Proposed Land Use**

Adopted Land-Use	Acres	
Commercial	12.0	
Industrial	46.7	
Public/Quasi Public	27.2	
Residential	96.1	
Unclassified (Right-of-Way)	52.1	
<b>TOTAL</b>	234.1	

Proposed Land-Use	Acres	Percent
General Commercial	14.9	2.70%
High Density Residential	7.5	16.81%
Light Industrial	46.7	7.14%
Low-Medium Density Residential	19.8	31.47%
Medium Density Residential	87.5	0.92%
Mixed Use	2.6	0.12%
Neighborhood Commercial	0.3	0.25%
Office Commercial	0.7	14.79%
Public/Quasi-Public	41.1	20.43%
Unclassified (Right-of-Way)	56.8	5.37%
<b>TOTAL</b>	278.0	100%

## Land Use Designations

The land use designations within the Proposed Richgrove Community Plan UDB are shown in **Figure 9** and as demonstrated in **Table 22**, residential land use constitutes 50%, commercial 17%, industrial 7%, public/quasi-public 41%, and rights-of-way 5.0% of the existing UDB area.

The following land use designations along with descriptions including density and intensity are recommended for Earlimart to address land demand needs through the 2030 planning horizon year.

***Low Density Residential (LDR):*** This designation establishes areas for single-family residences with individual homes on lots generally ranging from 12,500 square feet to one acre. Uses typically allowed include: detached single-family homes; secondary dwellings; and residential support uses such as churches, schools, and other necessary public utility and safety facilities. This designation is typically found inside communities or on the outside edge of UDBs.

# RICHGROVE COMMUNITY PLAN

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Maximum Density: 1-4 Dwelling Units/Acre

- When areas in this designation are identified as primary recharge areas for a community's water system, acreage minimums should not be below 2 acres.
- Areas with 30 percent or higher average slopes should have acreage minimums in excess of 3 acres.

***High Density Residential (HDR):*** This designation established areas for multi-family dwellings in urbanized areas. Uses typically allowed include: duplexes, townhouses, and apartments located near schools, parks, and other public services. This designation is used only within UDBs. Dwelling Units are based on Gross Acreage and development shall be no less than that identified as the intensity per gross acreage High Density Residential designated lands.

Maximum Density: 14-30 Dwelling Units/Acre

***Neighborhood Commercial (NC):*** This designation establishes areas for small-scale, general retail, and service businesses that provide goods to the immediate surrounding area. Uses typically allowed include: food and beverage retail sales; limited personal, medical, professional, and repair services; and retail sales. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

***General Commercial (GC):*** This designation establishes areas for small, localized retail, recreational, and service businesses that provide goods and services to the surrounding community. Uses typically allowed include: eating and drinking establishments; food and beverage retail sales; limited personal, medical, professional services; repair services; and retail sales. Such facilities may range from a single use to a cluster of uses such as a shopping center. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

***Highway Commercial (HC):*** This designation establishes areas for retail, recreational, and service-based businesses which provide goods and services to tourists and commuters along major highways. Uses typically allowed include: big box retail; eating and drinking establishments; food and beverage retail sales; limited repair services; lodging (hotels and motels); and retail sales. Such facilities may range from a single use to a cluster of uses located at a freeway off ramp or major highway intersection. This designation is located primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

***Light Industrial (LI):*** This designation establishes areas for a range of non-intensive business park, industrial park, and storage uses that do not have detrimental noise or odor impacts on surrounding urban uses. Uses typically allowed include: warehousing, welding & fabrication shops, manufacturing & processing, and business support uses such as retail or eating establishments that serve adjacent light industrial uses and employees. This designation is found primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

# RICHGROVE COMMUNITY PLAN

**Public/Quasi-Public (P/QP):** This designation establishes areas for public and quasi-public services and facilities that are necessary to maintain the welfare of County residents and businesses. Uses typically allowed include: churches; schools; civic centers; hospitals; fire stations; sheriff stations; liquid and solid waste disposal sites; cemeteries; airports; and public utility and safety facilities. This designation is found primarily within UDBs and pursuant to regional growth corridor plans and policies.

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four-year universities: California State University-Fresno, University of California- Davis, Cal State Bakersfield, Fresno Pacific University, Brandman University, and the University of Phoenix.

Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Based on the 2009-2013 American Community Survey (see Table 23), the educational barrier in Richgrove begins in grade

Table 23 - Educational Attainment			
Educational Attainment	California	Tulare County	Richgrove
Population 25 years and over	24,865,866	258,046	1,401
Less than 9th grade	10.1%	20.8%	57.8%
9th to 12th grade, no diploma	8.4%	11.2%	10.7%
High school graduate (& equivalency)	20.7%	25.0%	15.1%
Some college, no degree	22.0%	22.3%	9.4%
Associate's degree	7.8%	7.5%	2.9%
Bachelor's degree	19.6%	8.9%	4.1%
Graduate or professional degree	11.4%	4.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates

school. Of the adults age 25 and older, 31,6.0% had an educational level of less than 9<sup>th</sup> grade. This lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment needs to begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

# RICHGROVE COMMUNITY PLAN

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## Health Care:

Richgrove has plans to place a Health Clinic on the existing school grounds, and would greatly benefit from the completion of the construction on that location.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Urban Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 24**) and Zoning Plan map have been updated. The Proposed Zoning to match the land uses is as stated in **Table 25**.

<b>Table 24 - Existing / Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	221.2
Right-of Ways	56.8
Richgrove UDB	278.0

<b>Table 25 - Proposed Zoning</b>	
Proposed Zones	Acres
C-1	0.3
C-2-MU	15.1
C-O	1.5
M-1	46.7
M-1-MU	4.2
P-O	26.1
R-1	19.8
R-1-MU	0.1
R-2	60.5
R-2-MU	39.2
R-3	7.5
Unclassified (Right-of-Way)	56.8
<b>TOTAL</b>	<b>278.0</b>

## IMPLEMENTATION STRATEGY

A community plan must identify the methods and techniques that will be utilized to implement its various goals, policies and standards. These implementation methods must provide a realistic and practical framework for the achievement of the goals established in the community plan. Through the utilization of the techniques described below, the various provisions of the Richgrove Community Plan will be gradually implemented over the planning period. However, the extent to which the Plan is eventually implemented is dependent upon certain economic and social conditions (housing market conditions, interest rates, consumer preferences, etc.) which cannot be accurately assessed at this time. For the Richgrove Community Plan, implementation will be primarily focused on the following programs:

Control of land development through the application of zoning classifications consistent with the land use designations established in the Plan. State law requires that local zoning be consistent with the adopted general plan. Thus, after the adoption of the Richgrove Community Plan, it will be necessary for the Tulare County Planning Commission and the Board of Supervisors to initiate rezoning actions to achieve zoning consistent with the Plan. The close relationship between the Plan and local zoning



# RICHGROVE COMMUNITY PLAN

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will ensure that the policies of the Plan are enforced and implemented thereby maintaining the Plan as an effective management tool. The application of appropriate zoning to implement the Land Use Plan should, to the extent possible, follow property lines, section lines, or other easily identifiable boundaries. Where zoning boundaries must divide properties, they should be situated in a manner that enables each specifically zoned area to be developed, and to function, as an individual parcel in conformance with the new zoning classification.

Control and division and infrastructural improvements through the application of the requirements of the State Subdivision Map Act, the Tulare County Subdivision Ordinance, and the Improvement Standards of Tulare County. Divisions of land are subject to the requirements of the State Subdivision Map Act and the Tulare County Subdivision Ordinance. These laws control not only the design of land division projects but also provide the basis for requiring on-site and off-site improvements (vehicular access, sewer and water, flood protection, etc) that are necessary to serve the newly created parcels, depending on their intended use. Such improvements are, for the most part, identified and categorized in the Improvement Standards of Tulare County.

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the Richgrove Community Plan. The following components comprise the Richgrove Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School.

## Zoning District Changes

As part of this Implementation Program for the Richgrove Community Plan, there are a variety of changes to existing zoning districts. These changes are described below.

## Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

## Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Richgrove.

# RICHGROVE COMMUNITY PLAN

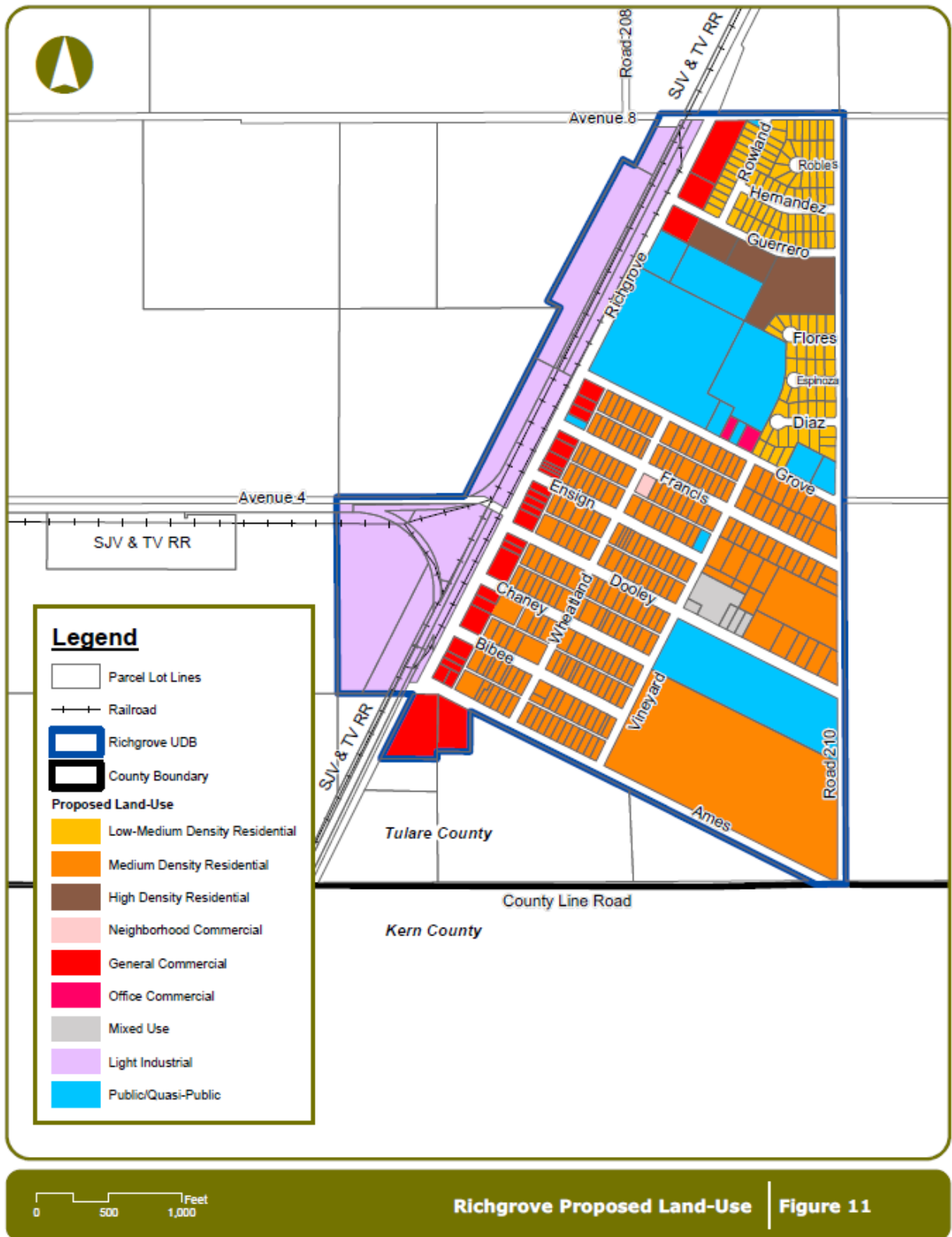
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## Zoning Map Update

The current Zoning Map for Richgrove (**see Figure 12**) will be amended to be compatible with the Land Use Map (**see Figure 11**) outlined in the General Plan. There are a couple of zoning district changes (**see Figure 13**) that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 14**) Richgrove proposed Zoning Districts. Several parcels west of SR 65 are designated as mixed use to promote highway commercial opportunities. The wastewater treatment plant is designated as heavy industrial. The M overlay is also proposed to be eliminated due to the urban nature of the community.

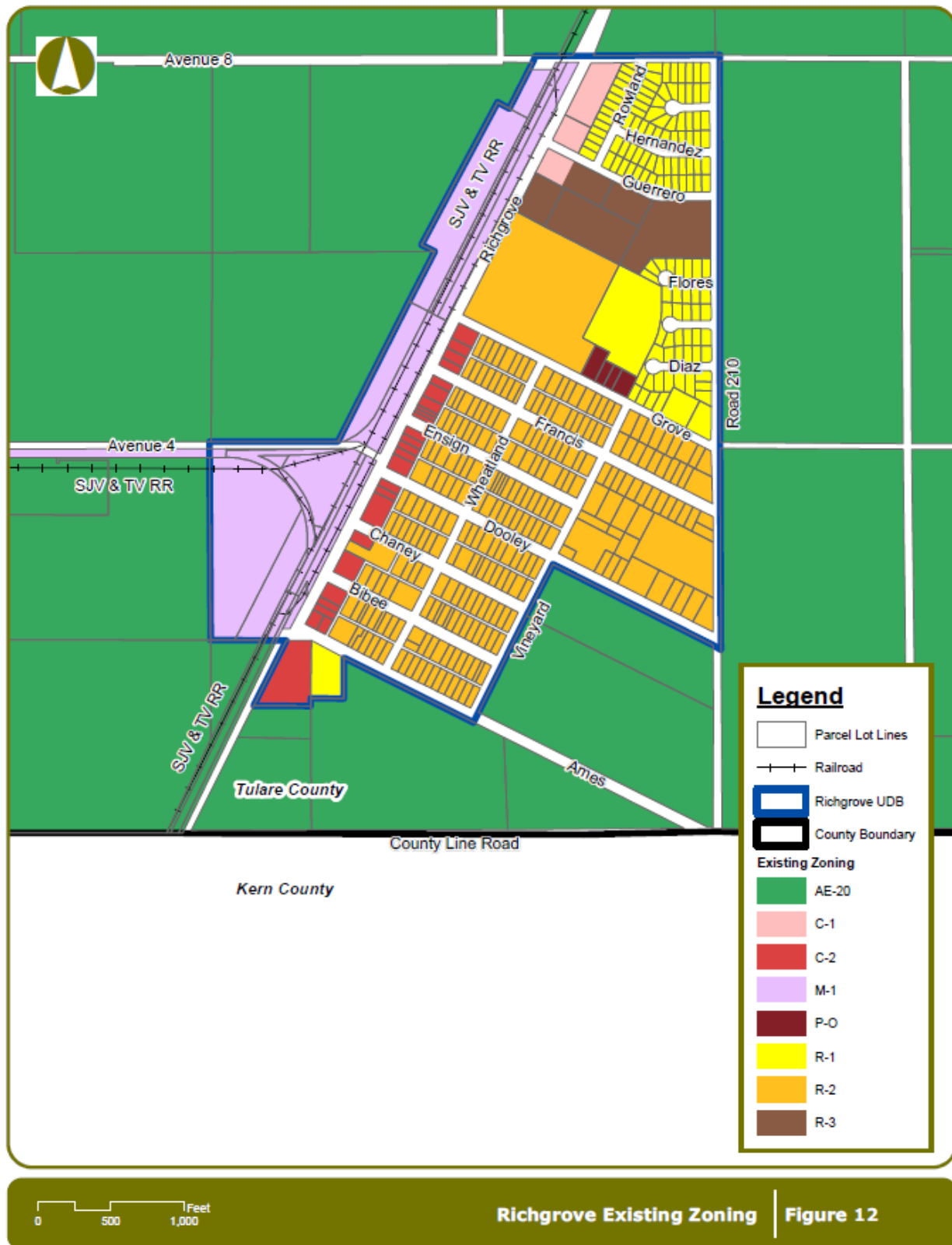
# RICHGROVE COMMUNITY PLAN

Figure 11 - Proposed Land Use Plan



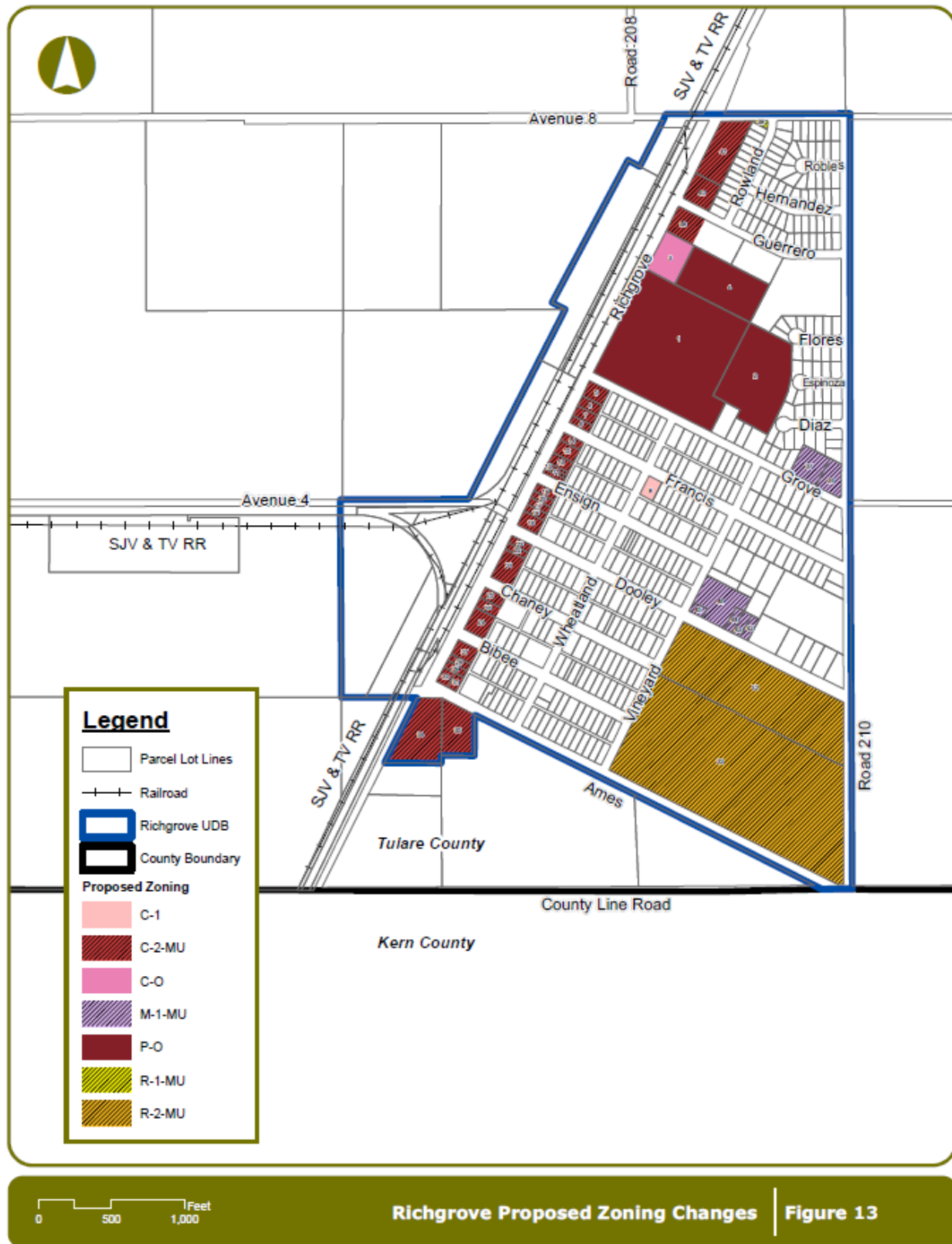
# RICHGROVE COMMUNITY PLAN

Figure 12 - Existing Zoning Districts



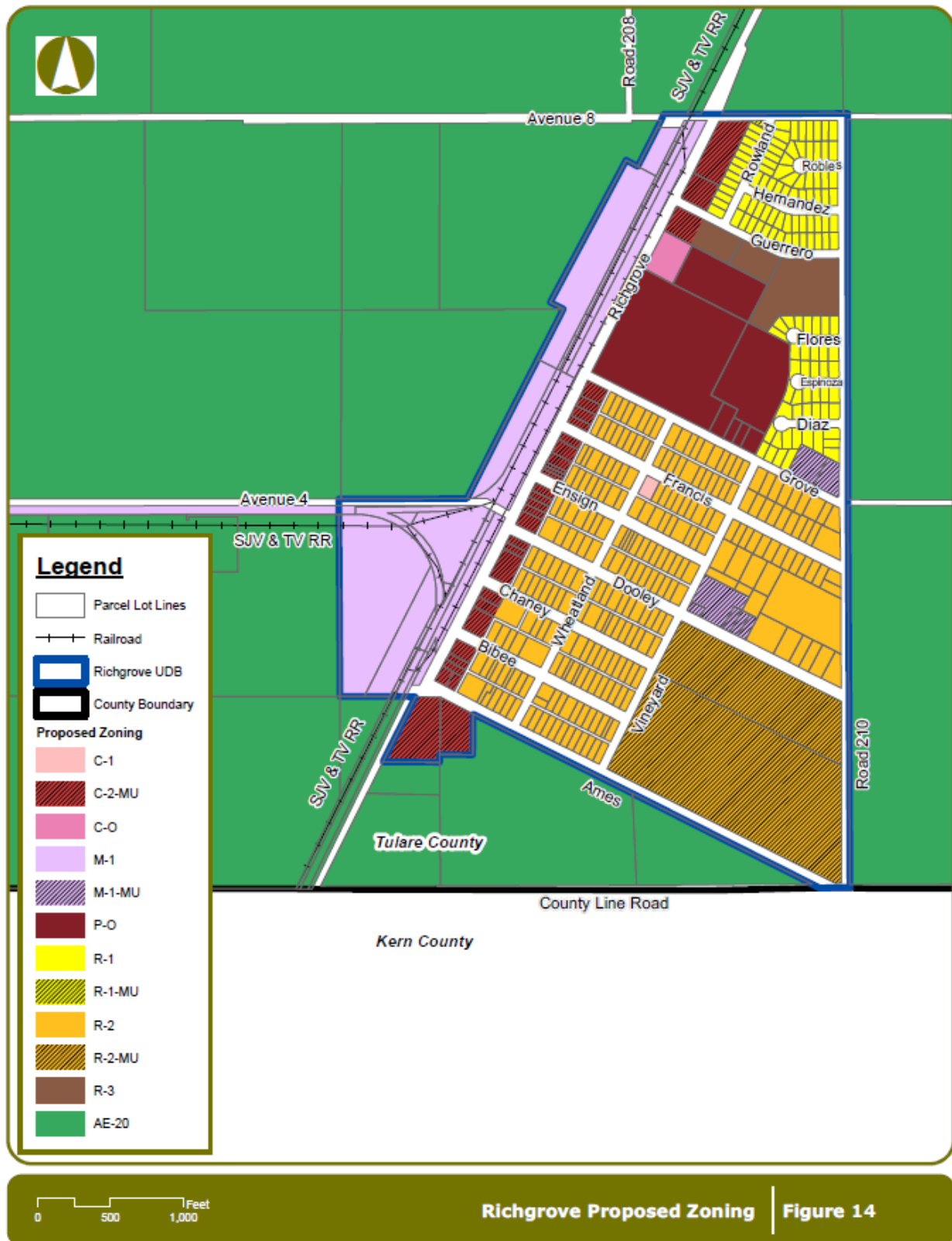
# RICHGROVE COMMUNITY PLAN

Figure 13 - Proposed Zoning Changes



# RICHGROVE COMMUNITY PLAN

Figure 14 - Proposed Zoning District



# RICHGROVE COMMUNITY PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# RICHGROVE COMMUNITY PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	



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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1

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Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1

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Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3,

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		M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3, *C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3,

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		M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	

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School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# RICHGROVE COMMUNITY PLAN

## A-2    Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the Community of Richgrove, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**    The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**    This overlay zone only applies to the community of Richgrove.

**USE**                    **C.**    No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Richgrove.

Within the Mixed-Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above-mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses:

All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# RICHGROVE COMMUNITY PLAN

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gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy-five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and



# RICHGROVE COMMUNITY PLAN

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zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Richgrove. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

# RICHGROVE COMMUNITY PLAN

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## A-3 Development Standards (Mixed Use Zoning District)

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To promote Economic Development within the Richgrove Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Richgrove. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Richgrove Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Richgrove. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery

(Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

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LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

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WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

# RICHGROVE COMMUNITY PLAN

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SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.



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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

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## APPENDICES

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9371

General Plan Amendment GPA 17-035-Resolution No. 9372

Section 18.9 “Mixed Use” Combining Zone-Resolution No. 9373

Section 16 “By Right Uses”-Resolution No. 9374

Zoning Districts Map-Resolution No. 9375

Sustainable Community Plans-Resolution No. 9376

## COUNTY OF TULARE, STATE OF CALIFORNIA

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Alpaugh, East Orosi, London, Richgrove, and Sultana to the Mixed-Use Combining Zone; (3) allow additional by-right uses

within the aforementioned Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Community Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-042 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map within the respective Urban Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-041 an amendment to Section 16 to allow additional by-right uses in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Community Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Community Plans;
  - b. Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Community Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Community planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Community Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Community Plans 2017 Update General Plan Amendment 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004; Change of Zone No. PZC 17-004 (Zoning District Map); East Orosi General Plan Amendment No. GPA 17-034; Change of Zone No. PZC 17-005 (Zoning District Map); London General Plan Amendment No. GPA 17-008; Change of Zone No. PZC 17-008 (Zoning District Map); Richgrove General Plan Amendment No. GPA 17-011; Change of Zone No. PZC 17-011 (Zoning District Map); Sultana General Plan Amendment No. GPA 17-012; Change of Zone No. PZC 17-012 (Zoning District Map; Change of Zone No. PZC 17-042 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-041 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Community Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,



4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Community Plans 2017 Update. Therefore, the public will benefit from the Community Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III COMMUNITY PLANS, OF THE ) RESOLUTION NO.9372  
TULARE COUNTY GENERAL PLAN FOR THE )  
COMMUNITY PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-004, GPA 17-034, )  
GPA 17-008, GPA 17-011, GPA 17-012, )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Community Plan of the Tulare County General Plan as set forth in Exhibits "A to E" for the Community Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, Sultana General Plan Amendment No. GPA 17-012 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the communities as part of the Community Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and



WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Community Plans of the Tulare County General Plan for the Community Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Community Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Community Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Urban Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the adoption of urban development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Community Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Community Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:


C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Community Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

- Exhibit “A” – 2017 Alpaugh Community Plan (Part III)
- Exhibit “B” – 2017 East Orosi Community Plan (Part III)
- Exhibit “C” – 2017 London Community Plan (Part III)
- Exhibit “D” – 2017 Richgrove Community Plan (Part III)
- Exhibit “E” – 2017 Sultana Community Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO            )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT    )       RESOLUTION NO. 9373  
WITH THE ADOPTED COMMUNITY PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE   )  
NO. PZC 17-041                                 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Community Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352.

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana ;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, Earlimart, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and



Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Community Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Communities of Alpaugh, East Oroquieta, London, Richgrove, and Sultana, consistent with the adopted Community Plans 2017 Update.



The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:       Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:       None  
ABSTAIN:   None  
ABSENT:    None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9374
CONSISTENT WITH THE ADOPTED	)	
COMMUNITY PLANS, AS PROPOSED IN	)	
CHANGE OF ZONE NO. PZC 17-042	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Earlimart, Ducor Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hypericum, Jovista, Matheny Tract and Tooleville. The Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
ORDINANCE NO. 352, ZONING ORDINANCE )  
DISTRICT MAPS TO REZONE PROPERTIES )  
IN THE URBAN DEVELOPMENT BOUNDARIES )  
OF ALPAUGH, EAST OROSI, LONDON, )  
RICHGROVE, AND SULTANA ) RESOLUTION NO. 9375  
CONSISTENT WITH THE ADOPTED )  
COMMUNITY PLANS 2017 UPDATE (GPA 17-035), )  
AS PROPOSED IN CHANGE OF ZONE NO. PZC's )  
PZC 17-004, PZC 17-005, PZC 17-008, PZC 17-011, )  
AND PZC 17-012, )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Alpaugh Rezoning Plan (PZC 17-004), to include the Community of Alpaugh, consistent with the adopted Alpaugh Community Plan 2017 Update, Exhibit "E" East Orosi Rezoning Plan (PCZ 17-004), to include the Community of East Orosi, consistent with the adopted East Orosi Community Plan 2017 Update, Exhibit "H" London Rezoning Plan (PCZ 17-016), to include the Community of London, consistent with the adopted London Community Plan 2017 Update, Exhibit "K" Richgrove Rezoning Plan (PCZ 17-017), to include the Community of Richgrove, consistent with the adopted Richgrove Community Plan 2017 Update, Exhibit "N" Sultana Rezoning Plan (PCZ 17-018), to include the Community of Sultana, consistent with the adopted Sultana Community Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities;
3. The proposed changes in zone district designations identified in the Alpaugh, East Orosi, London, Richgrove, and Sultana Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Alpaugh, East Oroshi, London, Richgrove, and Sultana Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Alpaugh Change of Zone No. PZC 17-004, East Oroshi Change of Zone No. PZC 17-005, London Change of Zone No. PZC 17-008, Richgrove Change of Zone No. PZC 17-011, Sultana Change of Zone No. PZC 17-012, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" Rezoning Plans consistent with the adopted Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Alpaugh Change of Zone No. PZC 17-004***

*Exhibit "A" – Zoning District Ordinance Map*

*Exhibit "B" – Zoning Map*

*Exhibit "C" – List of Affected Properties/Map*

***East Orosi Change of Zone No. PZC 17-005***

*Exhibit "D" – Zoning District Ordinance Map*

*Exhibit "E" – Zoning Map*

*Exhibit "F" – List of Affected Properties/Map*

***London Change of Zone No. PZC 17-008***

*Exhibit "G" – Zoning District Ordinance Map*

*Exhibit "H" – Zoning Map*

*Exhibit "I" – List of Affected Properties/Map*

***Richgrove Change of Zone No. PZC 17-011***

*Exhibit "J" – Zoning District Ordinance Map*

*Exhibit "K" – Zoning Map*

*Exhibit "L" – List of Affected Properties/Map*

***Sultana Change of Zone No. PZC 17-012***

*Exhibit "M" – Zoning District Ordinance Map*

*Exhibit "N" – Zoning Map*

*Exhibit "O" – List of Affected Properties/Map*

Attachment 2

***Community Plan Rezoning Full Ordinance***

*Exhibit A - Community Plan Rezoning Full Ordinance*



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF SUSTAINABLE  
COMMUNITY PLANS

)  
)

RESOLUTION NO. 9376

Resolution of the Planning Commission of the County of Tulare recommending the Board of Supervisors adopt and accept the Sustainable Community Plans. A major component of the Community planning process included a thorough examination and cataloguing of existing land uses which suggested a strong need for infrastructure improvements, land use changes and zoning changes, where feasible, in order to fulfill the goals of the General Plan and Senate Bill 244. The Unincorporated Community Infrastructure Maps provided the measurement tools needed to identify infrastructure solutions, appropriate improvement projects, and planning policy analysis. The resulting objective of the proposed Community Plans is aimed at reducing greenhouse gas emissions, promoting equity, providing economic stability; and thus, enhancing sustainability, as well as protecting the environment, and promoting healthy and safe communities. The Plans necessitated and ensured methods of public participation with a wide variety of stakeholders.

WHEREAS, The Budget Act of 2010/11 authorized the Department of Conservation (Department) to implement and administer \$63.15M in project funds (Proposition 84, the Safe Drinking Water, Water Quality and Supply, Flood Control, River and Coastal Protection Bond Act of 2006) to provide Planning Grants and Incentives awarded by the Strategic Growth Council (Council). The primary goal of this grant program is to develop and implement plans consistent with Chapter 13, Division 43, Public Resources Code, Sections 75127 and 75128; and

WHEREAS, The County received a grant from the Strategic Growth Council in 2014, Grant No. 2014-631, to study the feasibility of integrating infrastructure and land use, with the needs of rural unincorporated communities based on the Communities within Tulare County that received a top 10% ranking utilizing the *CalEnviroScreen* methodology and in fulfillment of the General Plan's Housing Element and SB 244 Plan (2014); and

WHEREAS, *"The work upon which this publication is based was funded in whole or in part through a grant awarded by the Council."* Preparation and processing of the Sustainable Community Plans took an estimated 36 months; and

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County

Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and

WHEREAS, the Sustainable Community Plans included all tasks and deliverables specified in the work plan including project development and meetings, data gathering, public workshops, plan development, public hearings, and grant reporting and administration; and

WHEREAS, the Sustainable Community Plans include strategies that reduce energy consumption, conserve water, improve air and water quality, and provide other community benefits; and

WHEREAS, staff has performed necessary investigations, prepared a written report, and recommended modifications and approval of these proposed Plans; and

WHEREAS, staff has produced the Final Report document attached as "Exhibit A" pending further review by the Planning Commission and the public, and will make all necessary administrative changes subject to this review and prior to the final submittal to the Board of Supervisors; and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. Recommend that the Board of Supervisors find that the said Sustainable Community Plans meet the Goals and Objectives of the Council in fulfillment of the Grant requirements as specified in the work plan.

The foregoing resolution was adopted upon motion of Commissioner Gong, seconded by Commissioner Aguilar, at a regular meeting of the Planning Commission on the 8th day of November, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A- Strategic Growth Council Final Report

Appendix B – Board of Supervisors  
Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

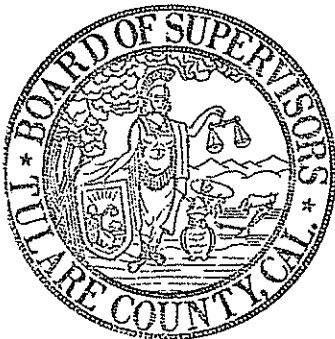
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **Sultana Community Plan**



# SULTANA COMMUNITY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# SULTANA COMMUNITY PLAN

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# SULTANA COMMUNITY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# SULTANA COMMUNITY PLAN

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## SULTANA COMMUNITY PLAN

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# Sultana Community Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors

Resolution No.

GPA 17-035

PZC 17-041

PZC 17-042

GPA-17-012

PZC-17-012



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# SULTANA COMMUNITY PLAN

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# SULTANA COMMUNITY PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# SULTANA COMMUNITY PLAN

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# SULTANA COMMUNITY PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Mike Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

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# SULTANA COMMUNITY PLAN

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# SULTANA COMMUNITY PLAN

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## EXECUTIVE SUMMARY

### INTRODUCTION

Sultana is one of the 2012 General Plan Urban Development Boundaries that does not have a Community Plan. The primary objective in preparing a Community Plan for Sultana is to develop a plan which accurately reflects the needs and priorities of this community. Sultana is currently designated as a Community in the 2030 Tulare County General Plan (2012). A precise plan is needed to increase the availability of infrastructure funding (drinking water system improvements [such as wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks, etc.) and to stimulate economic development within the community.

### Location

Sultana (**see Figure 1**) is a Census-Designated Place located in the northwest portion of Tulare County. It is bounded by Avenue 412 in the south, north of Avenue 416 in the north, Road 100 in the west, and Road 108 in the east and encompasses 0.4 square miles of land. It is not directly served by any State Route. (**See Figure 2**) Sultana is in Sections 05, 10, 11, 14, & 15 Township 16 South, Range 24 East, MDB&M, and can be found within Reedley United States Geological Survey 7.5 minute topographic quadrangle. Sultana is located at an elevation of 364 feet National Geodetic Vertical Datum. The coordinates of Sultana are: Latitude: 36.548977 Longitude: -119.328486.

### Planning Area

The Sultana Urban Development Boundary (UDB) area consists of 260.9 acres (**see Figure 3**). The Land Uses within the UDB are Mixed Use activities that occupy 242.3 acres (**see Table 1**). The remaining 18 percent are lands dedicated for Right-of-way.

No change is proposed to the Urban Development Boundary.

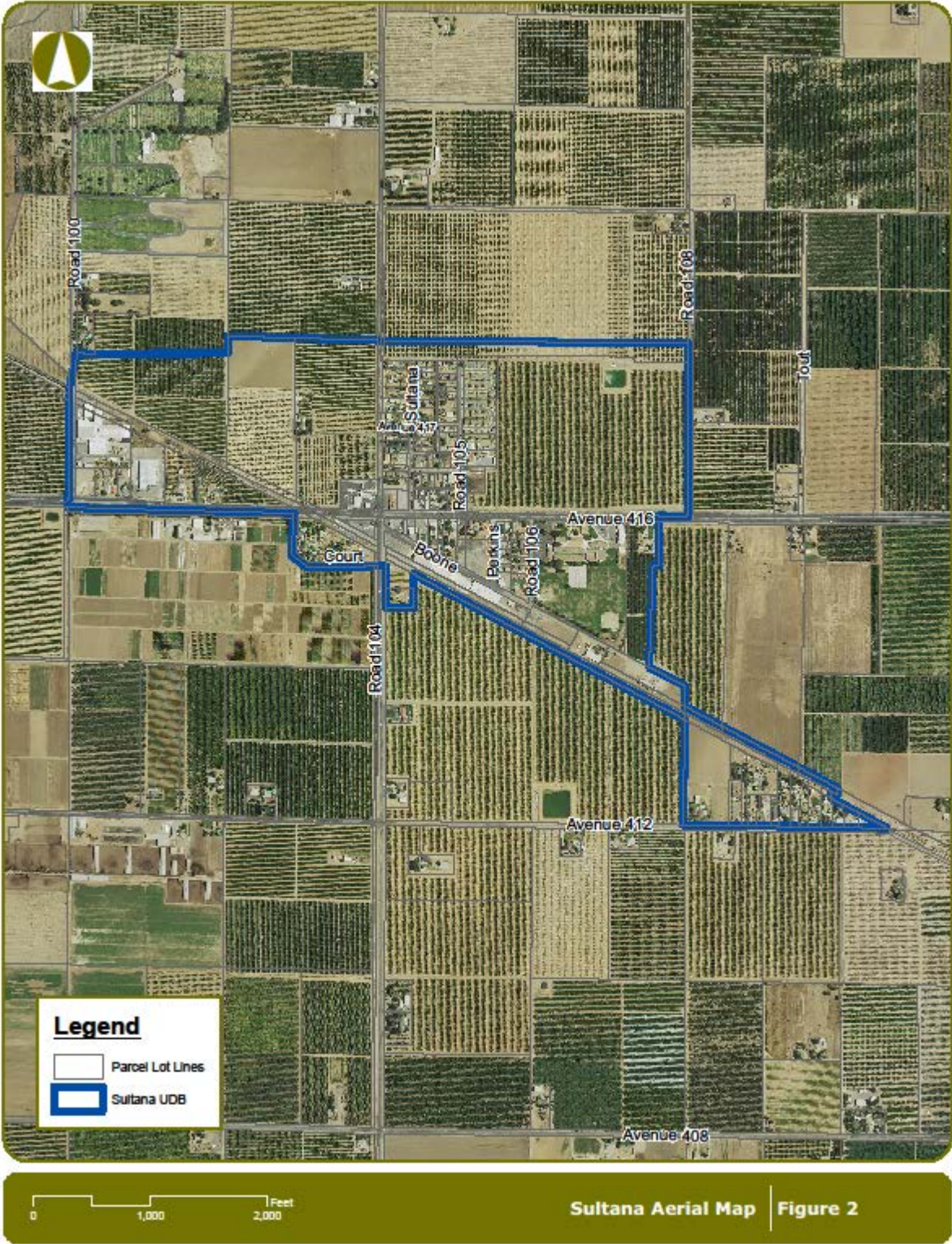
Table 1 - Sultana Land Use	
Adopted Land-Use	Acres
Mixed Use	242.3
Unclassified (Right-of-Way)	18.5
TOTAL	260.9

Source: Tulare County GIS



# SULTANA COMMUNITY PLAN

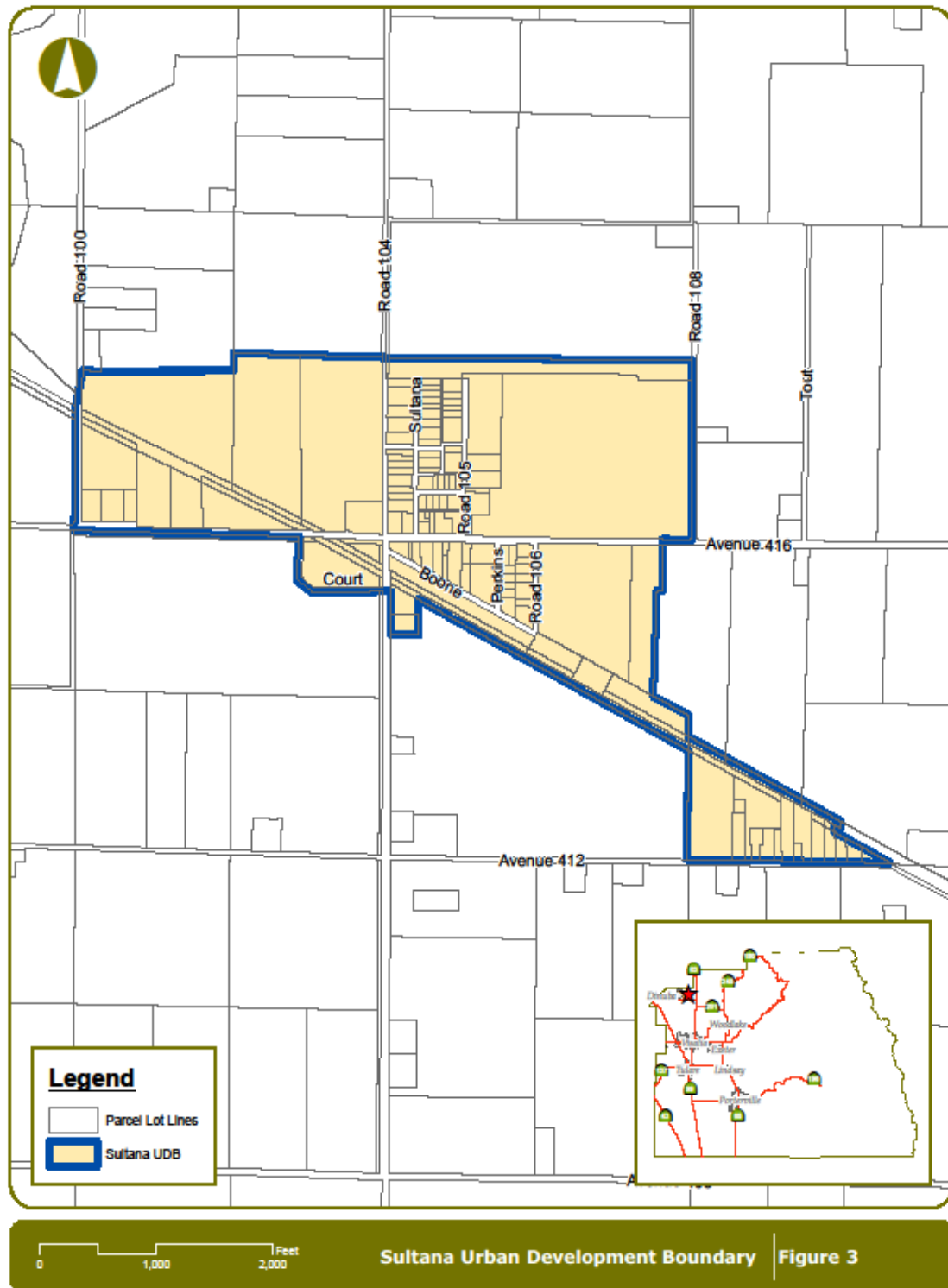
Figure 2 – Aerial Sultana





# SULTANA COMMUNITY PLAN

Figure 3 - Sultana UDB



# SULTANA COMMUNITY PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Outreach and Invitation Process

In efforts to invite all local residents and make them aware of the SGC project, Self Help Enterprises went door to door throughout the community of Sultana to distribute flyers at every home, business and school. During this process SHE talked with residents about the importance of participating in this process. Self Help Enterprises partnered with the Monson – Sultana School District and the Sultana Community Services District to spread the word about the project.

### Community Outreach Steps

Self-Help Enterprises (SHE) staff used various outreach methods to encourage community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) An informational flyer was developed.
- 2.) On November 12 and November 18, 2015 two SHE staff went door to door to distribute flyers in Sultana.
- 3.) SHE staff partnered with a member of the Sultana CSD to distribute fliers and spread the word throughout the community.
- 4.) During the community outreach process, SHE staff explained the goals of the project and emphasized the importance of participating in the SGC community meeting.
- 5.) SHE staff partnered with the Monson – Sultana School District to send meeting flyers to all 440 students.
- 6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner stores, gas stations and businesses.

### Community Meeting

Self Help Enterprises held a Strategic Growth Council community input meeting in Sultana on, November 19, 2015. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Monson – Sultana School. Over 20 residents attended the meeting and 20 surveys were collected.



A second meeting was held on December 7, 2016. In attendance, was County staff and SHE. This was a joint meeting with the communities of Monson and Sultana to discuss the new well that will provide water for Sultana and Monson, and will be operated by the Sultana CSD. The communities

# SULTANA COMMUNITY PLAN

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voiced concerns about water, lighting, road conditions, but were very receptive to land use and zoning changes to bring non-compliant uses into conformity with the General Plan and Zoning Code. Several people at the meeting discussed potential changes to their properties and for commercial opportunity areas.

## Community Survey

A survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/ concerns:

## Priority Concerns

1. Street Lights
2. Side walks
3. Community Park
4. Clinic
5. Internet
6. Police Patrol

## **Side Walks**

The community of Sultana has too few sidewalks. Some of residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk around town in the mud and through big puddles of water. The absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting, and that they stay indoors because they do not feel safe walking on a dark street. This is a major safety concern for them and residents are especially concerned about how this affects the safety of local children. Residents said that some streets have no light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Internet Access**

Most families in Sultana do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, as without access to the internet, they are unable to do things such as online job hunting, applying for resources, or provide homework help to their kids. Internet access ranked as a high priority improvement need in Sultana.

# SULTANA COMMUNITY PLAN

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## **Neighborhood Park**

In the community of Sultana there is no safe green space for children to play and adults to be physically active. The nearest park is located 4 miles away in the city of Dinuba, survey results show that in Sultana, children play in the street or do not play outside at all. Sultana residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

## **Sheriff Presence**

The Tulare County Sheriff Department (TCSD) is responsible for patrolling the community of Sultana. Most residents report that the Sheriff's Department response time is unacceptable and that there is little police patrol within the community. Residents report being concerned about the rate of crimes that go un-responded to. People feel that the unrecognized presence of law enforcement contributes to the local crime rate.

## **Medical Clinic**

Sultana residents do not have access to medical care within the community. The nearest medical clinic is approximately 4 miles away in Dinuba. Residents report traveling to Dinuba, 4 miles or Culter-Orosi, 5 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. The nearest hospital is in Visalia 17 miles away, and it takes approximately 20-25 minutes for the nearest ambulance to get to Sultana from Visalia.

## **Public Services**

Sheriff –Sultana is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of Sultana and the nearest Sheriff substation is located in Visalia.

Fire –Tulare County Fire

Schools – Monson - Sultana School District in Sultana

Libraries – The nearest library is 4 miles away in Dinuba

Parks – The nearest park is located in Dinuba 4 miles away

# SULTANA COMMUNITY PLAN

## SULTANA TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Community's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Community plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Community allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Community. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of a smaller community such as Sultana.

#### Population

In 2015, the population of Sultana was 916 (See Table 2).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Sultana</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	390	42.6%
Female	19,334,329	50.3%	226,607	49.9%	526	57.4%
Total	38,421,464		454,033		916	

2011-2015 American Community Survey 5-Year Estimates

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Sultana's population during the Planning Period (see Table 3).

**Table 3 - Projected Annual Growth Rate**

	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report



# SULTANA COMMUNITY PLAN

## Median Age

Sultana's population is younger than the median age throughout all of Tulare County. Sultana's median age of 20.7 is lower than the median age of the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Sultana	20.7

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 4.8% of Sultana's population was White, 0% was African American, 0% was Native American, 0.4% was Asian, and .0% was two races or more. Approximately 95.2% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community. (see Table 5)

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Sultana	%
Total	38,421,464		454,033		916	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	872	95.2%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	40	4.8%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	4	0.4%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Sultana

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Sultana was about 5.5% while the rate for Tulare County was 13.5%. The unemployment rate for the State of California was 10.1%. Keeping in mind that the 6.1% includes only the employable labor force (that is, not every person of the population) results in about 30 unemployed persons of Sultana's person labor force of 367.

# SULTANA COMMUNITY PLAN

Table 6 - Employment Status			
Employment Status	California	Tulare County	Sultana
Population 16 years & over	30,312,429	325,404	543
In labor force	19,269,449	194,420	367
Civilian labor force	19,137,441	194,102	367
Employed	17,246,360	170,780	337
Unemployed	1,891,081	23,322	30
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	176

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Sultana median household income was \$24,950, compared to \$61,818 for the State of California, and \$42,031 for Tulare County. (see Table 7)

Table 7 - 2011-2015 American Community Survey Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Sultana CDP	\$24,950	\$24,875	\$8,956

2011-2015 American Community Survey 5-Year Estimates

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Sultana's median household income was \$24,950, whereas the State of California's median household income was \$61,818. Sultana's median household income was 40.3% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

# SULTANA COMMUNITY PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

Table 8 - Housing Tenure			
	California	Tulare County	Sultana
Occupied housing units	12,717,801	133,570	222
Owner-occupied	6,909,176	75,685	47
Renter-occupied	5,808,625	57,885	175

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Sultana
Owner-occupied	54.3%	56.7%	21.2%
Renter-occupied	45.7%	43.3%	78.8%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Unit	%	Units	%	Units	%	Units	%	Units	%	
Sultana	41	59	2	3	31	30	9	13	6	9	69

(Source: Tulare County 2015 Housing Element)

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Sultana indicated that approximately 59% of the housing units were sound. Approximately 46% of Sultana's housing units were deteriorated and 9% were dilapidated (see Table 10).

# SULTANA COMMUNITY PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Sultana
Total housing units	13,845,790	144,792	232
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	32
Built 1990 to 1999	1,495,571	21,767	30
Built 1980 to 1989	2,117,819	22,733	14
Built 1970 to 1979	2,503,688	27,111	101
Built 1960 to 1969	1,871,029	15,500	14
Built 1950 to 1959	1,907,512	13,694	15
Built 1940 to 1949	865,607	7,494	21
Built 1939 or earlier	1,298,438	8,186	5

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Sultana was 3.92 persons per household (see Table 12).

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Sultana	4.30	4.08

2011-2015 American Community Survey 5-Year Estimates

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. Typically, a vacancy rate of about 5-6% is considered normal; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Sultana was 0%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Sultana	0.0%	0.0%

2010-2014 American Community Survey 5-Year Estimates

<sup>1</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See:

[http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# SULTANA COMMUNITY PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>2</sup>

A California Natural Diversity Database (CNDDDB) search conducted on November 8, 2016 indicates there are special status species within the Reedley Quadrant (**see Figure 4**) Species List (which includes the Sultana Planning Study Area) consisting of one animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

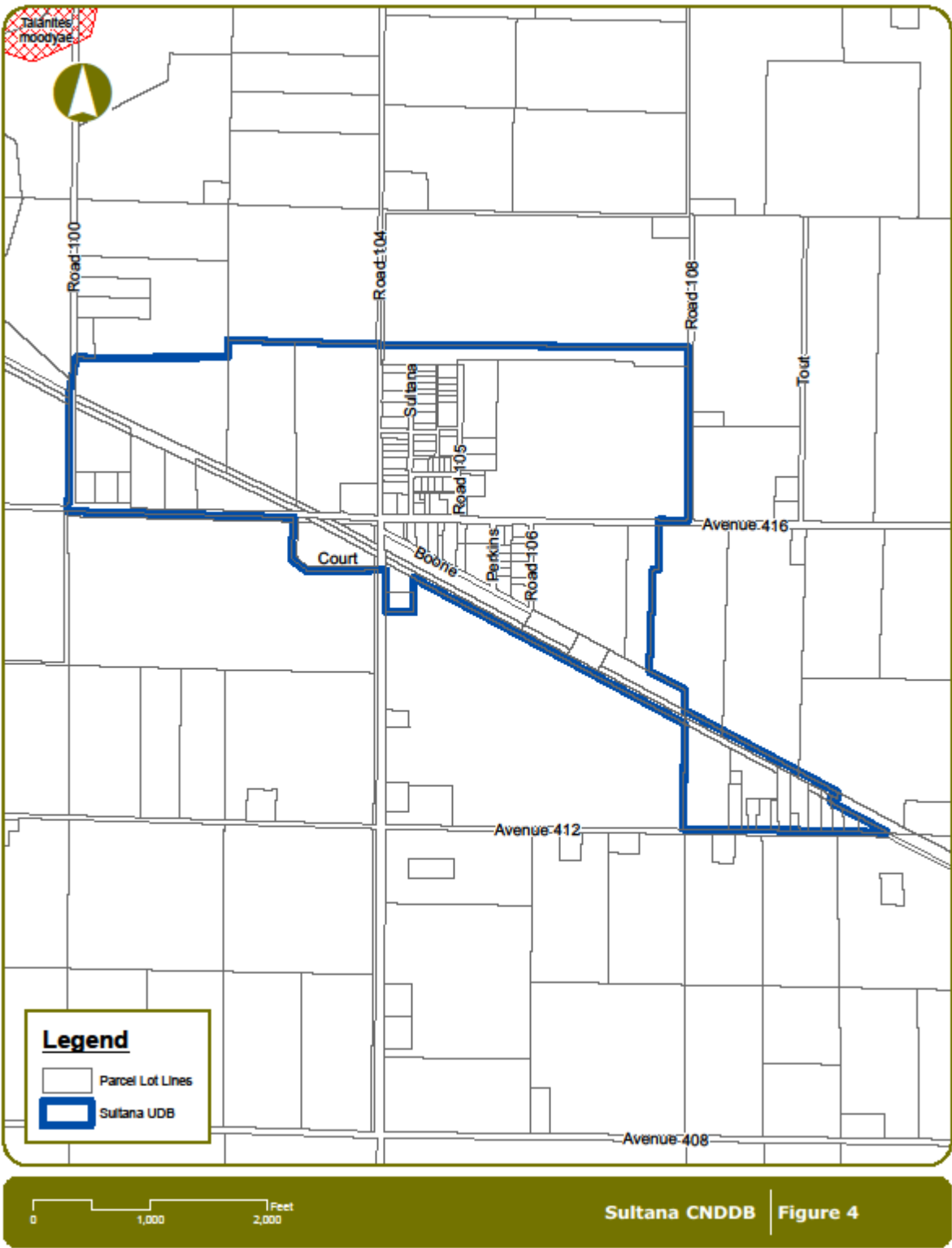
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<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. November 8, 2016

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# SULTANA COMMUNITY PLAN

Figure 4 - CNDDDB - Reedley Quadrant



# SULTANA COMMUNITY PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Sultana are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Sultana] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal.

The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>4</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Sultana (see Figure 5). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Urban Development Boundary (UDB). The following descriptions are provided for the above soil types:

**Calgro-Calgro, saline-Sodic, complex, 0 to 2 percent slopes,** Erosion potential is low to moderate and shrink swell potential low.

**Hanford sandy loam, 0 to 2 percent slopes,** is very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains and alluvial fans; negligible to medium runoff; moderately rapid and moderate permeability, however saline-sodic phases and soils with sandy clay loam substratums have moderately slow permeability. This soil is suitable for growing a wide range of fruits, vegetables, and general farm crops. They are also used for urban development and dairies.

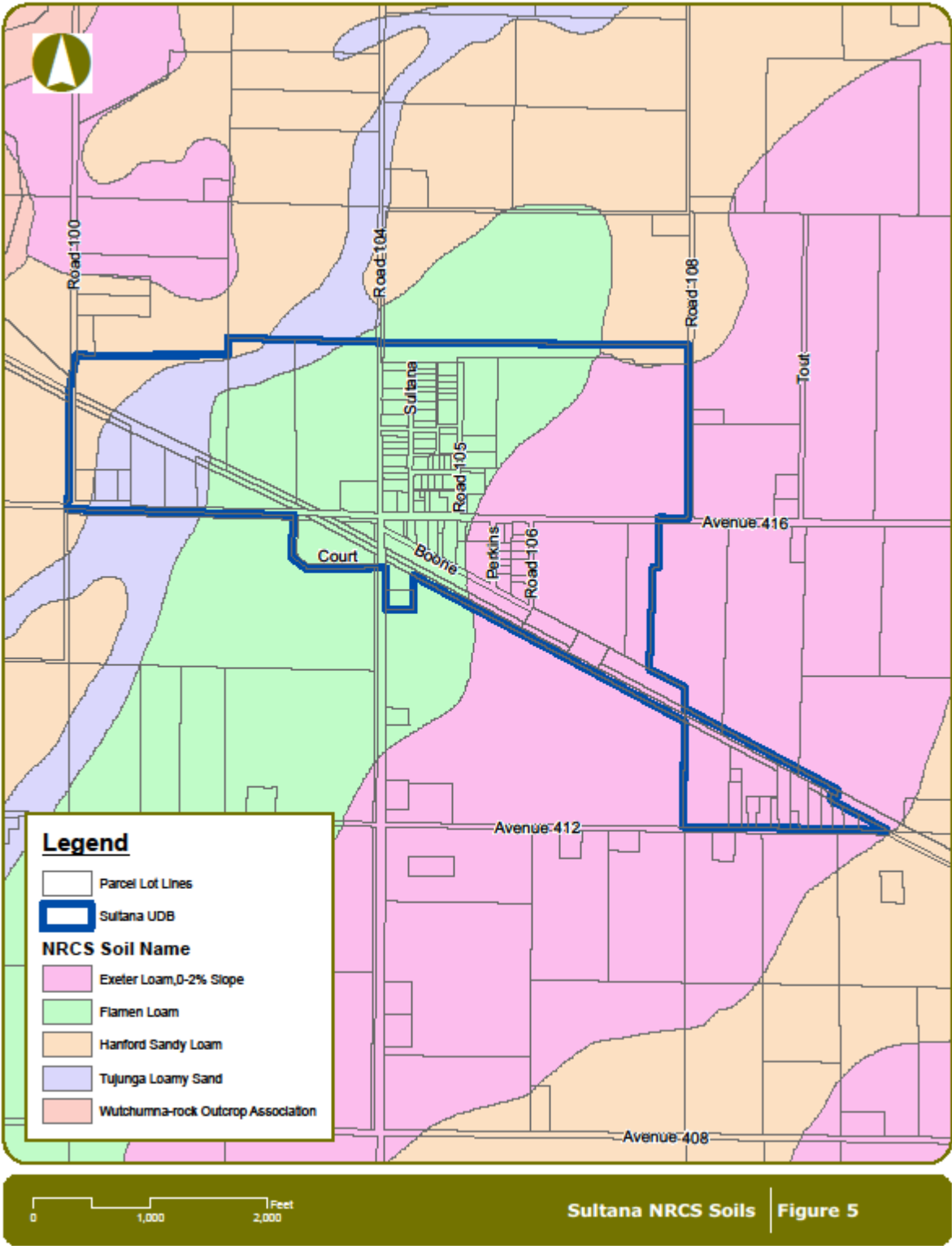
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<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>4</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database, accessed November 3, 2016.

# SULTANA COMMUNITY PLAN

Figure 5 - NRCS Soils Map





# SULTANA COMMUNITY PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Sultana Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies
- Adopt Local Air Quality Mitigation Fee Program
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations
- Fugitive dust regulation enforcement coordinated with SJVUAPCD
- Energy efficiency incentive programs
- Local alternative fuels programs
- Coordinate location of land uses to separate odor generators and sensitive receptors

There is a correlation between air quality and land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Sultana is located near the southwestern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely

# SULTANA COMMUNITY PLAN

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modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Sultana including State Highway 201 and agricultural activities.

## Flooding<sup>5</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0605E, portions of Sultana are located within the 100-year flood zone (**See Figure 6**). Flood Zone A areas are subject to inundation by the 1-percent-annual-chance flood event generally determined using approximate methodologies. Because detailed hydraulic analyses have not been performed, no Base Flood Elevations (BFEs) or flood depths are shown. Mandatory flood insurance purchase requirements and floodplain management standards apply. The remaining areas are within Flood Zone X. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). “Floodplain” or “flood-prone area” means any land area susceptible to being inundated by water from any source. “Base Flood” is the flood having a one percent chance of being equaled or exceeded in any given year. “One-hundred-year flood” or “100 year flood” has the same meaning as “base flood.” “Special flood hazard area” is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. “Floodway” means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Sultana there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain

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<sup>5</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0605E, Panel Number 605 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>. Accessed August 17, 2016.

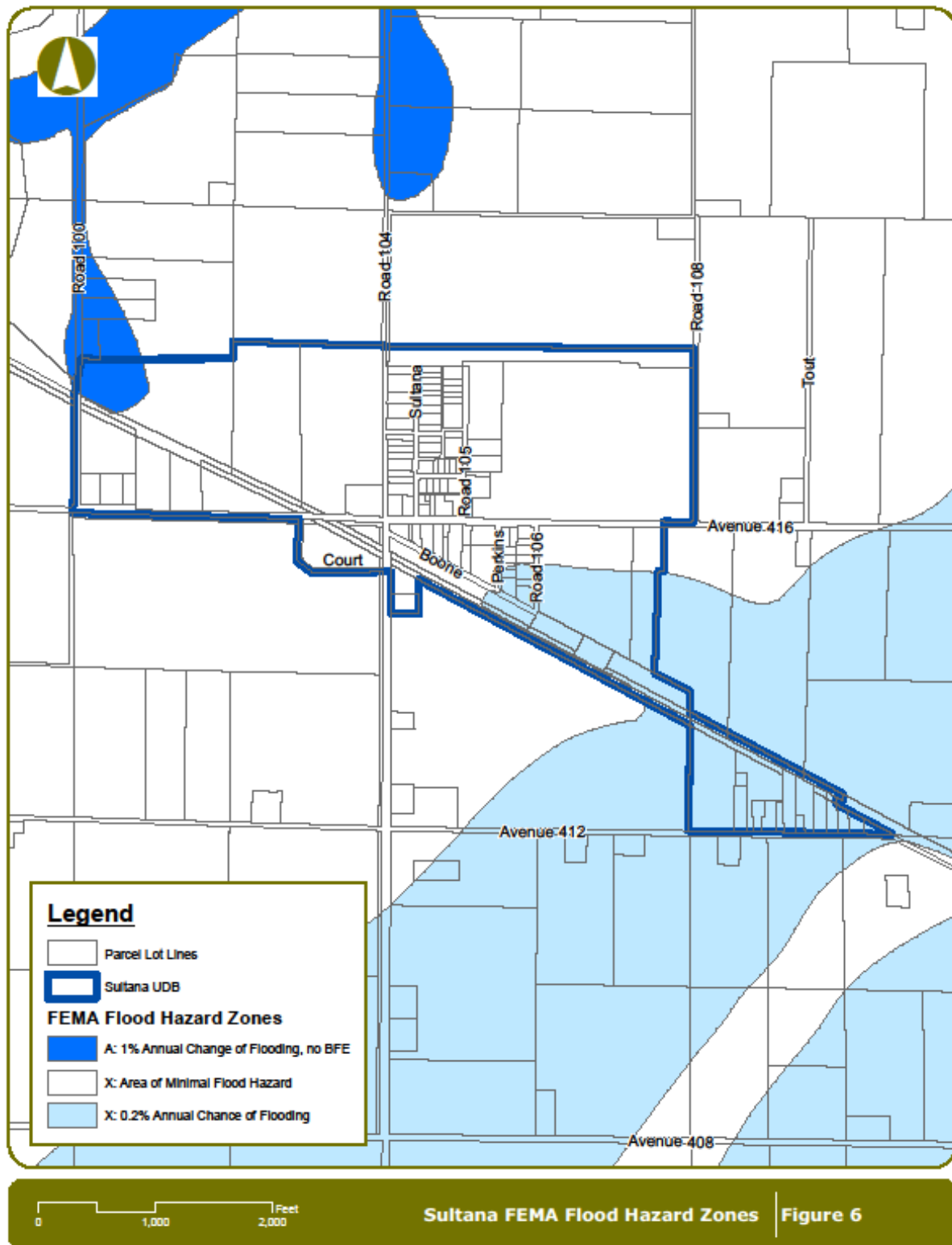
# SULTANA COMMUNITY PLAN

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flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

# SULTANA COMMUNITY PLAN

Figure 6 - FEMA Flood Map



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## Noise<sup>6</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan background Report Sultana lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Sultana, this Community Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.).

## Energy: Natural Gas/Electricity

PG&E provide service to Sultana.

## Domestic Water Service

Domestic water service in Sultana is provided by the Sultana Community Services District (CSD), which was formed in 1977. Domestic sewer service is provided by the Cutler Public Utilities District (PUD). **Table 14** shows the number of existing water connections, the capacity of the system, and the number of additional connections the system can accommodate for new development (Municipal Service Review, October 2011). Information related to domestic sewer connections, as well as sewer and water system maps, are currently unavailable.

According to the Municipal Service Review 2011 (MSR), the CSD community water system consists of 2 wells: Well No. 3 (Main Primary) and Well No. 2 (South Back-up) and Well No. 3 (North Emergency). Wells No. 3 (Main) is drilled to a depth of 430’, is equipped with a 60 horse power (hp) turbine pipe that funnels water through a single check valve and into a 5500 gallon steel pressure storage tank. Well No. 3 (Main) is also equipped with a back-up propane engine in case of power failure. Well No. 2 (South Back-up) is drilled to a depth of 358’ and contains a 75 hp turbine pump that also funnels water through a single check valve and into a 5500 gallon storage tank and then on to distribution. Well No. 3 (Main) is equipped with a Chlor- tec chlorine generator system that injects chlorine into the system as water is funneled through the check valve into the storage tank.

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<sup>6</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# SULTANA COMMUNITY PLAN

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Well No. 2 (South) has not been used for approximately 11 years, but can be put into service at any time if Well No. 3 (Main) ever becomes contaminated or compromised. Over the last 10 years, at least 2 other wells have been abandoned due to contamination (contaminants unknown).

Nitrates can be associated with septic systems, agricultural use of fertilizers and concentrated animal facilities. At least two dairies are located within the District's boundaries and the District is surrounded by agricultural uses, making the system vulnerable to high Nitrate levels. The District's 2009 Consumer Confidence Report (CCR) reiterates that leaks in the distribution plumbing, the presence of underground petroleum tanks, known contamination plumes, agricultural activity and sewer and drainage lines are the primary threats to Well No. 3 (Main) and Well No. 2. The 2009 CCR further indicates that the Sultana area has a history of DBCP contamination, a pesticide banned in the 1970s, but that the most recent sample test results for DBCP were non-detect.

In order to protect the system from vulnerabilities, the latest CCR indicates that the well system should be kept clean and free of weeds and debris to prevent contamination. The report further directs that cement surface seals need to be checked for cracks and immediately repaired or sealed if needed.

Sample test results for Nitrates are to be submitted each year. If a well sample is found to have at least 50% of the maximum contaminate level (MCL) allowed, which is 45 parts per million (ppm), the CSD must submit quarterly test results until the issue is resolved. The CSD must also provide notice of the violation to customers on a quarterly basis and proof of this notice must be submitted to Environmental Health, also on a quarterly basis. The CSD was notified that Well No. 2 samples exceeded the 50% threshold in 2006 and 2007. Proof of customer notification for these violations were not found in the District's Environmental Health file. The record shows that annual Nitrate testing results were not submitted for the year 2005. The 2009 CCR indicates that test samples showed Nitrate levels well below the 50% threshold.

The County's Environmental Health Department provided notice of violation to the CSD for exceeding total Coliform MCL on 5 separate occasions in the last 5 years (bacteriological samples are tested on a monthly basis). No evidence was found in the District's Environmental Health file indicating whether a notice of this violation was mailed to district customers as required by law.

Based on the information available, the system's well, storage, and distribution apparatus are reliable and not in need of major repair, only standard maintenance as suggested in the District's 2009 CCR. The water provided seems to be of good quality with minimal contamination, especially when compared to similar size districts surrounded by similar land uses.

It is also determined; however, that based on the dairy operations within district boundaries and the agricultural uses that surround it, the system is highly vulnerable to contamination, in particular Nitrates. This is evidenced by the number of district wells that have been abandoned over the last 7 years due to contamination. Although the District can rely on Well No. 2 if Well No. 3 (Main) is forced offline, as more wells are abandoned, the number of feasible well sites diminishes. In the future, this could put district customers in a situation where they must rely on bottled water for consumption and boiled water for all other uses while the prolonged process of securing a new well site takes place. Accordingly, the CSD must consider long-term solutions that will expand water supplies available to the District.

# SULTANA COMMUNITY PLAN

Table 14 - Description of Existing Infrastructure

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
150	150	0	N/A	N/A	N/A

\* Data current as of October 2011

## Sewer Service

There is no community wide sewer system in Sultana. The community depends on individual on-site septic tank systems for wastewater disposal.

## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration

Sultana does have a storm drainage system, however system information and mapping is currently unavailable.

## Solid Waste

Solid waste disposal services for the Community of Sultana is provided by Pena’s Disposal, a private company. Solid waste generated in Sultana can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia.

## **PUBLIC SERVICES**

### Sheriff

Police protection services are provided in Sultana by the Tulare County Sheriff’s Department sub-station, located at 40765 Road 128, in Cutler, approximately 20 miles northeast of Sultana. Orosi Substation covering 289 square miles and serving a rural population to include the unincorporated communities of Cutler, New Sultana, Sultana, Orosi, Seville, Sultana, Traver and Yettem. The



# SULTANA COMMUNITY PLAN

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Substation runs a four shift operation which includes 23 deputies, four sergeants and one lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. In addition to that general shift staffing the communities of Cutler, Oroshi and New Sultana/Traver have assigned Community Based Officers assigned specifically to those area. The substation is open for walk-ins from 8 am to 5 pm Monday thru Friday. After hours and weekends there is a phone provided outside the substation that calls directly into the dispatch center. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

## Fire

Fire protection and emergency medical services are provided for Sultana by the Tulare County Fire Department. The community is served by Dinuba Station #3, located at 40404 Road 80, in Dinuba. Parade 1, Engine 3 and Water Tender 3 is assigned to this location.

Fifteen fire hydrants are found within Sultana (see **Table 14**). These fire hydrants are located within the County right-of-way. **Figure 7** display Existing Fire Hydrants in Sultana.

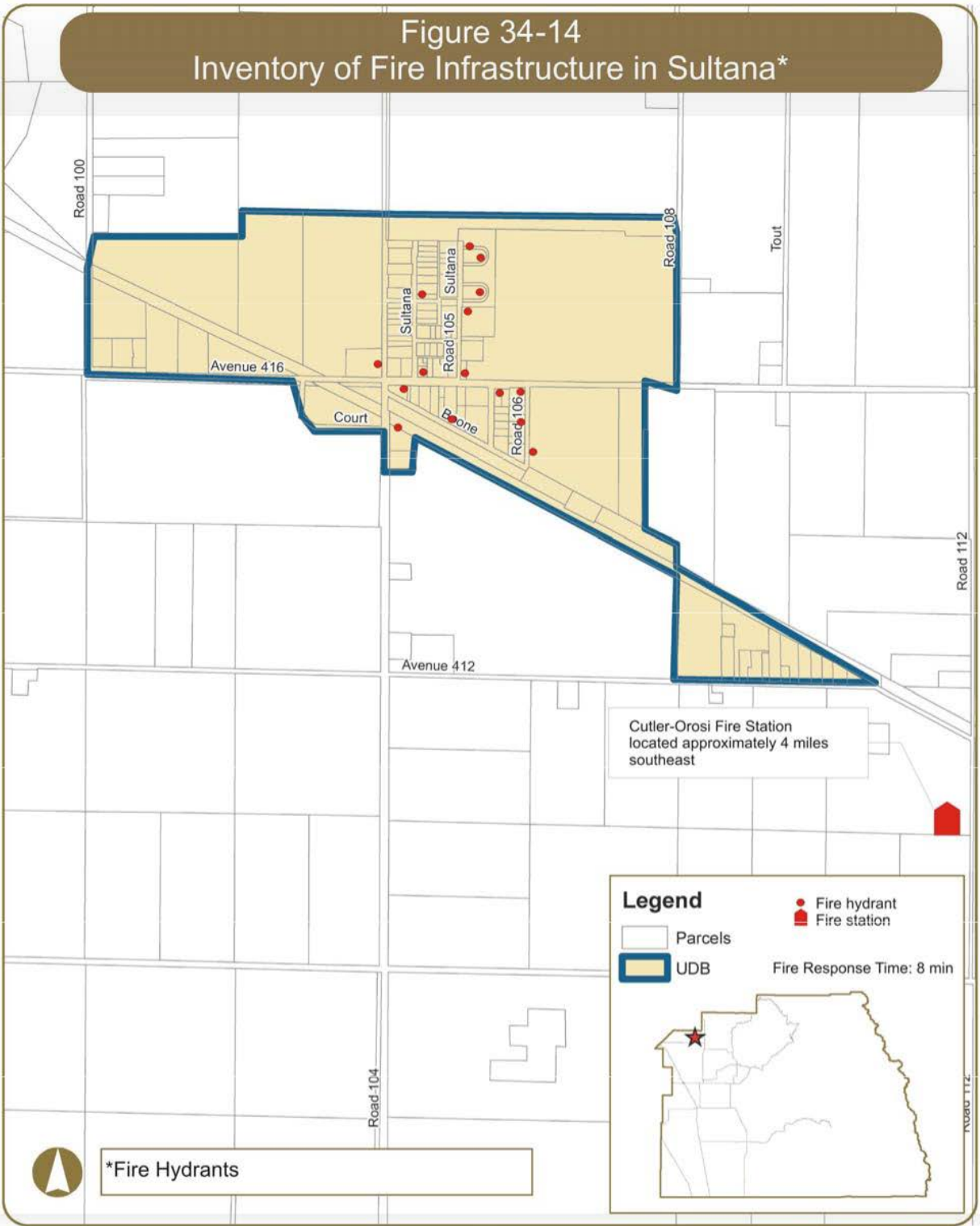
**Table 15 - Existing Fire Hydrants**

Existing Fire Hydrants	
No.	Location
1	Road 105 north of Avenue 416
2	Road 105 north of Avenue 416
3	Road 105 north of Avenue 416
4	Road 105 north of Avenue 416
5	Road 105 and Avenue 416
6	Sultana Road north of Avenue 417
7	Sultana Road and Avenue 416
8	Avenue 416 west of Sultana Road
9	Road 104 north of Avenue 416
10	Road 104 south of Avenue 416
11	Avenue 416 and Perkins Road
12	Avenue 416 and Road 106
13	Road 106 south of Avenue 416
14	Road 106 north of Boone Avenue
15	Boone Avenue south of Avenue 416



# SULTANA COMMUNITY PLAN

Figure 7 - Inventory of Fire Infrastructure



# SULTANA COMMUNITY PLAN

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## Schools

Schools are an essential component of a community. The Sultana Community Plan Area is within the Monson-Sultana Joint Union School District. The District provides K-8 education at Monson-Sultana School four (4) miles away. High School students attend high school in Dinuba. Sultana is within the Dinuba Union Joint High School District.

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 15 regional libraries and one main branch”<sup>7</sup>

Table 16 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Dinuba	Dinuba Branch Library 150 South I Street Dinuba, CA 93618	Tuesdays to Friday: 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 6:00 p.m.

Library hours current as of September 2017

(see Table 15). The closest library is located in Dinuba, two (2) miles to the west.

## Parks

The nearest park is located 4 miles away in the City of Dinuba.

# THE SULTANA COMMUNITY PLAN

## CIRCULATION/COMPLETE STREETS/TRANSPORTATION

### Existing Circulation

Sultana is a small agricultural community. The major rural collector roads is Avenue 416. There are no proposed major streets as part of this Community Plan.

### Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

### Traffic

The 2014 Regional Transportation Plan and projections for Year 2040 traffic volumes do not exceed the LOS “C” threshold. Less than significant impacts will occur to Ave. 416, as a result of the Project primarily because growth rates consistently grow at 1.3% in Tulare County.

The proposed Circulation Element and Complete Street Policies of the General Plan contain policies and actions that will continue to maintain LOS C or better. The Plan's projected growth and its impact on the circulation system will continue to be less than significant as the additional goals and policies are implemented through the Community Plan. These policies and actions will be implemented by the County of Tulare over a 20-year time period.

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<sup>7</sup> General Plan Background Report, page 7-96

# SULTANA COMMUNITY PLAN

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**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Roads

“There are several roadways in Sultana that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads”<sup>8</sup>

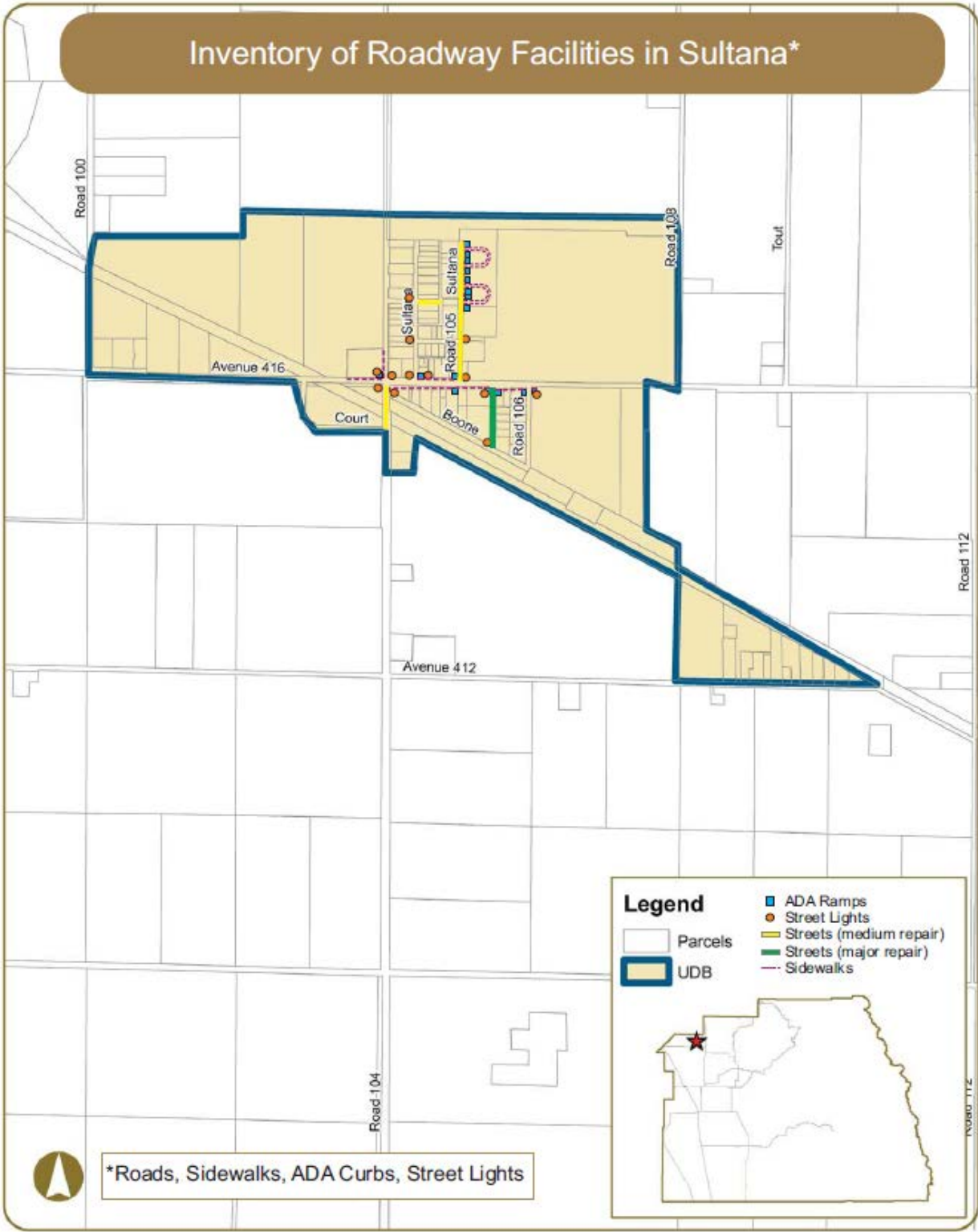
**Table 17** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 8** graphically displays this information on a map.

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<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

# SULTANA COMMUNITY PLAN

Figure 8 - Sultana Inventory of Roadway Facilities



# SULTANA COMMUNITY PLAN

**Table 17 - Road Maintenance Strategies**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 417	Sultana Road to east end	CHIP
2	Perkins Road	Boone Avenue to Avenue 416	GRX
3	Road 104	Court Avenue to Avenue 416	CHIP
4	Road 105	Avenue 416 to north end	CHIP

OLAY – overlay resurfacing operation

CHIP – chip seal Reconstruction

GRX – grind and remix

(

ACST – asphalt reconstruction

RCST – cold mix reconstruction

Source: County of Tulare Public Works, 2012)

## Street Lights

Lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

**Table 18** identifies the location of existing street lights that are maintained by Tulare County, in Sultana, as well as their specifications. **Figure 8** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.”<sup>9</sup>

<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# SULTANA COMMUNITY PLAN

**Table 18 - Existing Street Lights**

Specifications of Existing Street Lights								
No	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1		Road 57	NW Corner	1474	5800	W	S	PG&E
2	Ames Drive	Road 57	North Side	1475	5800	W	S	PG&E
3	Ames Drive	Kate Road	NW Corner	1464	5800	W	S	PG&E
4	Avenue 8	Road 60	NW Corner	1465	5800	W	S	PG&E
5	Bibee Drive	Hunter Road	NW Corner	3635	5800	W	S	PG&E
6	Avenue 376	Hunter Road	West Side	1455	5800	W	E	PG&E
7	Avenue 376	Road 57	SW Corner	1472	5800	W	E	PG&E
8	Avenue 376	Road 57	South Side	54	5800	W	N	PG&E
9	Avenue 376	Road 58	SW Corner	1469	5800	W	N	PG&E
10	Avenue 376	Kate Road	SW Corner	1463	5800	W	E	PG&E
11	Avenue 377	Clay Road	West Side	3325	5800	W	N	PG&E
12	Avenue 377	West end	North Side	1473	5800	W	S	PG&E
13	Avenue 377	Road 57	NW Corner	1471	5800	W	S	PG&E
14	Avenue 377	Road 58	NW Corner	1468	5800	W	S	PG&E
15	Avenue 377	Kate Road	NW Corner	1461	5800	W	S	PG&E
16	Avenue 377	Hunter Road	NW Corner	1478	5800	W	S	PG&E
17	Avenue 378	Road 60	NW Corner	1466	5800	W	E	PG&E
18	Avenue 378	Clay Road	NE Corner	3324	5800	W	S	PG&E
19	Avenue 378	Road 58	SW Corner	1477	5800	W	E	PG&E
20	Avenue 378	Kate Road	NW Corner	1476	5800	W	S	PG&E
21	Avenue 378	Road 60	West Side	1479	5800	W	E	PG&E
22	Avenue 378	Kate Road	West Side	1480	5800	W	E	PG&E
23	Avenue 378	Road 58	West Side	1470	5800	W	E	PG&E
24	Avenue 380	Kate Road	SW Corner	1462	5800	W	E	PG&E
25	Avenue 380	Road 60	SW Corner	1467	5800	W	E	PG&E

(Source: Tulare County Public Works, March 2013)

## Sidewalks

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

The County and VRPA Technologies surveyed existing sidewalks within the Community. **Table 19** identifies the location of existing sidewalks in Sultana. **Figure 8** also displays this information graphically. The sidewalks represented in **Table 19** and **Figure 8** do not distinguish between ADA compliant sidewalks and noncompliant sidewalks. The majority of sidewalks represented below were

# SULTANA COMMUNITY PLAN

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constructed prior to current ADA guidelines and would be considered non ADA compliant facilities. Such noncompliant facilities would require complete reconstruction to be considered ADA compliant.

**Table 19 - Existing Sidewalks in Sultana**

Location of Existing Sidewalks			
No.	Roadway	Limits	Location
1	Avenue 416	Road 104 to 250' west	North side
2	Avenue 416	Sultana Road to Road 105	North side
3	Avenue 416	Road 104 to Road 106	South side
4	Road 104	Avenue 416 to 200' north	West side
5	Road 105	Road 105 South Loop (south) to Road 105 North Loop (north)	East side

(Source: County of Tulare Public Works and VRPA Technologies, February 2014)

## **ADA CURB RAMPS:**

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are various ADA compliant curb ramps located within Sultana and are listed in **Table 20** and **Figure 8**.



# SULTANA COMMUNITY PLAN

**Table 20 - Existing ADA Ramps**

<b>Location of Existing ADA Ramps</b>			
<b>No.</b>	<b>East-West Roadway</b>	<b>North-South Roadway</b>	<b>Location</b>
1	Avenue 416	Road 104	NW Corner
2.	Avenue 416	Sultana Road	NE Corner
1	Avenue 416	Road 105	NW Corner
2	Avenue 416	Road 105	SW Corner
3	Avenue 416	Perkins Road	SE Corner
4	Avenue 416	Perkins Road	SW Corner
5	Avenue 416	Road 106	SE Corner
6	Avenue 416	Road 106	SW Corner
7	Road 105 North Loop (north)	Road 105	SE Corner
8	Road 105 North Loop (north)	Road 105	SW Corner
9	Road 105 North Loop (south)	Road 105	NE Corner
10	Road 105 North Loop (south)	Road 105	SE Corner
11	Road 105 South Loop (north)	Road 105	NE Corner
12	Road 105 South Loop (north)	Road 105	SE Corner
13	Road 105 South Loop (south)	Road 105	NE Corner
14	Road 105 South Loop (south)	Road 105	SE Corner
15	Avenue 416	Road 104	NE Corner
16	Avenue 416	Sultana Road	SE Corner

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

## Transit and Bus Stops

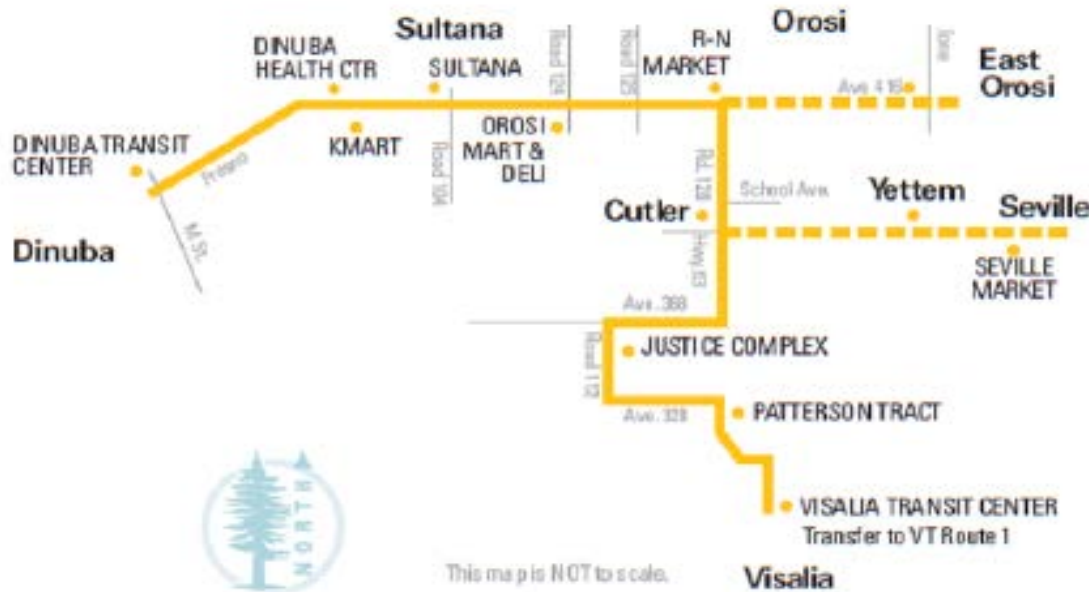
The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation. Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare’s urban transit systems. Sultana is currently connected via TCAT Route 10 to Dinuba and its transit center (**see Figure 9**). [www.ridetcat.org](http://www.ridetcat.org) TCAT Route 10 provides service to Dinuba four times per day on weekdays and four times on Saturday. In Dinuba, transfers can be made to connect to Visalia, Tulare, and the remainder of the TCAT public transit system TCAT vehicles are wheelchair accessible and all full size buses include bike racks. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full-size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents.



# SULTANA COMMUNITY PLAN

The low level of auto congestion in Sultana, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

**Figure 9 - TCAT Transit Route 10**



## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified Road 80 as a proposed Class III Bike Project<sup>10</sup>. In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is Road 80.

## State Route 99

State Route (SR) is the major highway between cities within the Central Valley and the primary route between the City of Fresno to the north and the City of Bakersfield to the south. Sultana is located approximately 10 miles east of SR 99.

<sup>10</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# SULTANA COMMUNITY PLAN

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## AMTRAK

The Hanford AMTRAK station, located approximately 45 miles to the southwest in Kings County, is the closest station to Sultana providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport is approximately 30 miles southwest. Fresno Yosemite International Airport (FAT), approximately 30 miles northwest of Sultana, is a principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles southeast of Sultana and offers direct flights to several destinations

# SULTANA COMMUNITY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Sultana Community Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Community Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Sultana to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Sultana Community Plan, and input received from Sultana citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.

2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
4. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
5. The County shall work with the Schools to provide safe routes to school.
6. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Allensworth Community Plan.
7. The Sultana Urban Development Boundary should be reviewed every five years to determine if amendments are appropriate.
8. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

# SULTANA COMMUNITY PLAN

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Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Sultana.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Sultana.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

# SULTANA COMMUNITY PLAN

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## Economic Base

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

#### Policies:

1. Encourage the Sultana to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

#### Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## Environmental Quality and Public Safety

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Sultana citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

#### Objective: Protect Agricultural Lands:

Land within the respective Sultana, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion

# SULTANA COMMUNITY PLAN

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to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Sultana.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Sultana.

# SULTANA COMMUNITY PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 260.9-acre Sultana Urban Development Boundary, agricultural activities, such as orchards and pasture and greater than five-acre rural residential properties (zoned AE-20 and A-1), currently occupy 80 percent (80%) of the UDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2014 population of 916 by 30 urbanized acres (area within the UDB), which equals 30.5 persons per urbanized acre (**see Tables 21 and 22**) showing population projections). This indicates much of the population lives in the higher density housing and the Census Designated Place is much broader. Projecting the population at a 1.3% growth rate adds 181 persons in Year 2030. Multiplying the 181 persons by the 1/4 ratio (25.30 %) suggests an additional 46 acres of land will be required to accommodate population driven development by the Year 2030, if projections are realized.

### Population Growth Forecast

**Table 21 – Sultana Population Projections**

Growth Rate	2014	2020	2030
0.013	916	965	1098

### Demand Forecast

To determine whether there is enough land within the exiting UDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the UDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2013 American Community Survey (Survey)<sup>11</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2013 the community had 232 dwelling units (including vacant dwellings) with a population of 916 persons. At an annual growth rate of 1.3%, the projected housing units are 244 and 278 in years 2020 and 2030, respectively, and projected population is 965 and 1098 in years 2020 and 2030, respectively.

**Table 22 - Population Projects**

Year	Population	Growth (%)
2016	916	0.013
2017	928	0.013
2018	940	0.013
2019	952	0.013
2020	965	0.013
2021	977	0.013
2022	990	0.013
2023	1003	0.013
2024	1016	0.013
2025	1029	0.013
2026	1042	0.013
2027	1056	0.013
2028	1070	0.013
2029	1083	0.013
2030	1098	0.013

<sup>11</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).



# SULTANA COMMUNITY PLAN

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There are currently approximately 8 acres within the UDB that are developed for residential uses. Assuming that the land area needed to accommodate future population increases is consistent with annual population growth projections, the land use growth projections are also assessed at an annual growth rate of 1.3%. At this growth rate, there will be a total of approximately 68 acres of residential uses in Year 2030; that is, an increase of approximately 10 acres. As shown in **Table 25**, there are approximately 313 acres of undeveloped residential lands within the existing UDB. As such, there is adequate land available for development within the existing UDB to accommodate future residential growth through the Year 2030.

## Commercial and Industrial Uses

There are currently approximately 2 acres within the UDB that are developed for commercial uses and approximately 2 acres developed for industrial uses. Assuming that the land area needed to accommodate future commercial and industrial growth is consistent with annual population growth projections, the land use growth projections are also assessed at an annual growth rate of 1.3%. At this growth rate, there will be a total of approximately 2.5 acres of commercial uses and approximately 2.5 acres of industrial uses in the Year 2030; that is, an increase of approximately 5 acres total. As shown in **Table 24**, there are approximately 9 acres of zoned for commercial development and approximately 26 acres of industrial lands within the existing UDB. As such, there is adequate land available for development within the existing UDB to accommodate future commercial and industrial growth through the Year 2030.

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, CSU – Bakersfield, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.



# SULTANA COMMUNITY PLAN

Based on the 2009-2013 American Community Survey (see Table 23), the educational barrier in Sultana begins in grade school. Of the adults age 25 and older, 24.0% had an educational level of less than 9<sup>th</sup> grade. This lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment needs to begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

Table 23 - Education Attainment			
Educational Attainment	California	Tulare County	Sultana
Population 25 years and over	24,865,866	258,046	417
Less than 9th grade	10.1%	20.8%	47.2%
9th to 12th grade, no diploma	8.4%	11.2%	24.0%
High school graduate (& equivalency)	20.7%	25.0%	23.3%
Some college, no degree	22.0%	22.3%	2.2%
Associate's degree	7.8%	7.5%	2.4%
Bachelor's degree	19.6%	8.9%	0.0%
Graduate or professional degree	11.4%	4.3%	1.0%

2011-2015 American Community Survey 5-Year Estimates

## Health Care

Sultana residents do not have access to medical care within the community. The nearest medical clinic is approximately 4 miles away in Dinuba. Most residents report traveling the (4) miles to Dinuba or five (5) miles to Cutler/Orosi for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Visalia 17 miles away, and it takes approximately 20-25 minutes for the nearest ambulance to get to Sultana from Visalia.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Urban Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see Table 24) and Zoning Plan map (see Table 25) have been updated.

Proposed Land Use Table 24 - Proposed Land Use	
Proposed Land Use	Acres
Mixed Use	242.3
Unclassified (Right-of-Way)	18.5
<b>TOTAL</b>	<b>260.9</b>

Table 25 - Proposed Zoning	
Proposed Zones	Acres
AE-20	74.1
C-2-MU	9.0
M-1-MU	26.1
P-O	17.2
R-2-MU	1.1
R-3	5.0
R-3-MU	5.8
R-A	104.0
Unclassified (Right-of-Way)	18.5
<b>TOTAL</b>	<b>260.9</b>

# SULTANA COMMUNITY PLAN

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## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the Sultana Community Plan. The following components comprise the Sultana Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Sultana Community Plan, there are a variety of changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

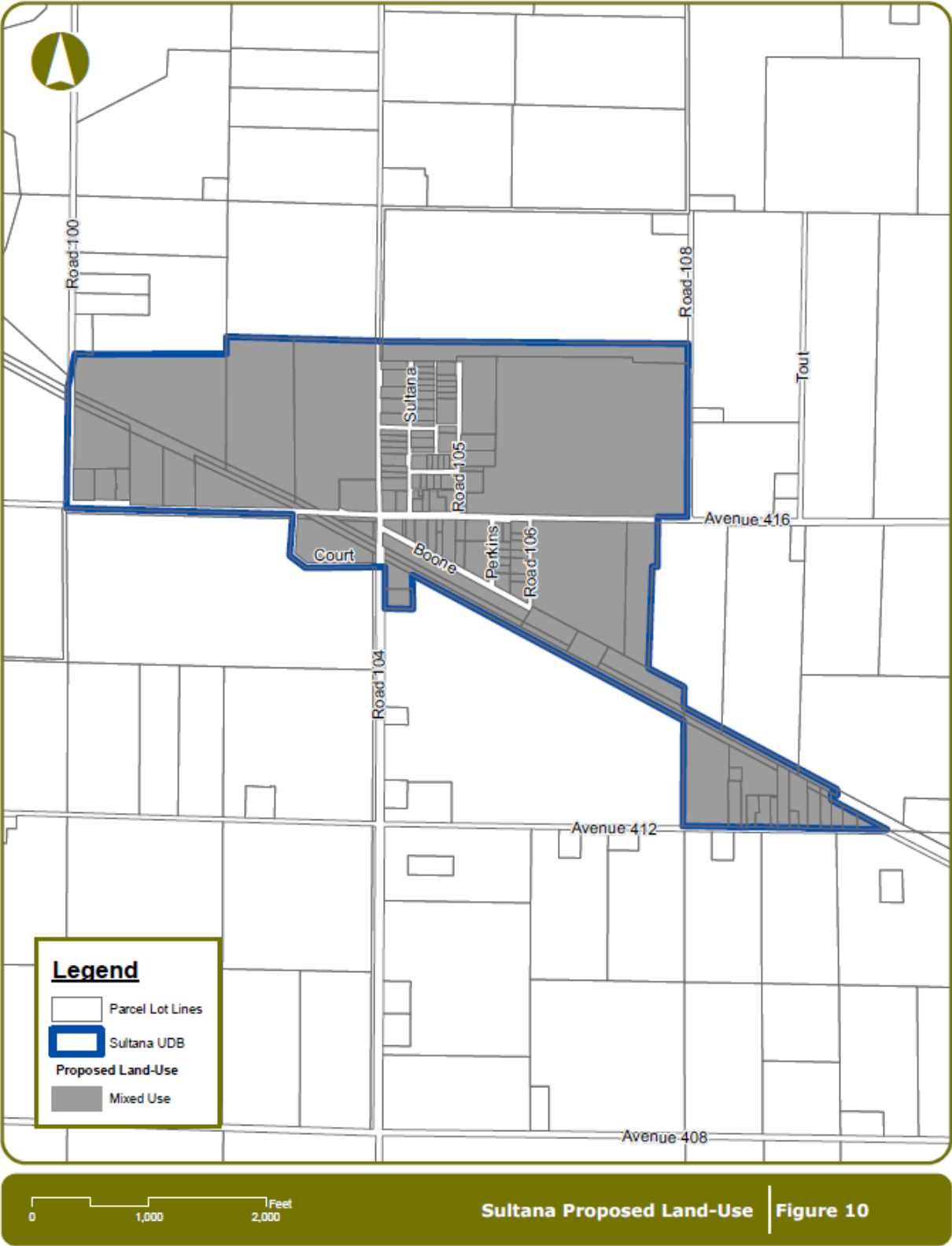
This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Sultana.

### Zoning Map Update

The current Zoning Map (see **Figure 11**) for Sultana will be amended to be compatible with the Land Use Map(see **Figure 10**) outlined in the General Plan. There are a couple (see **Figure 12**) of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (see **Figure 13**) Sultana Proposed Zoning.

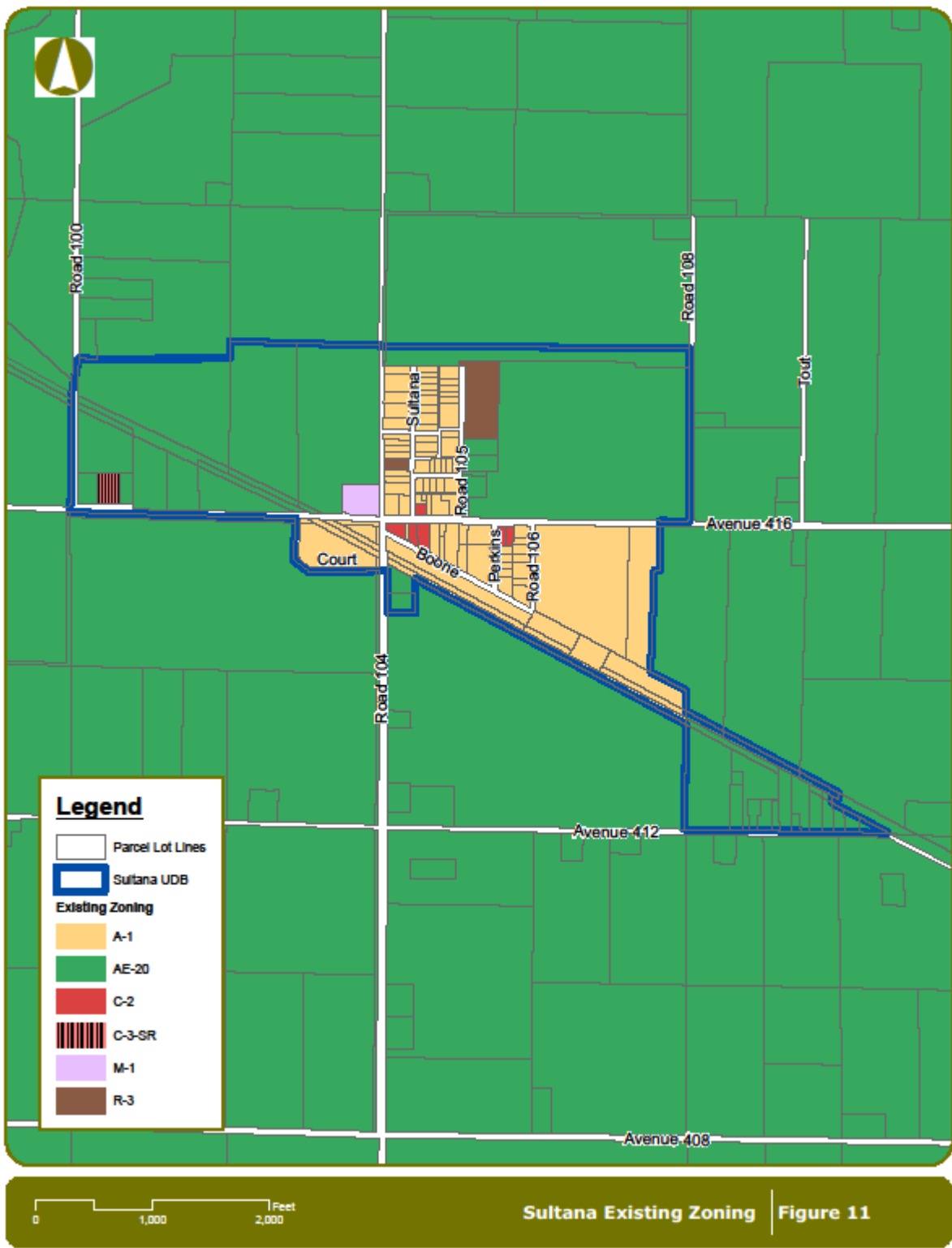
# SULTANA COMMUNITY PLAN

Figure 10 - Proposed Land Use Plan



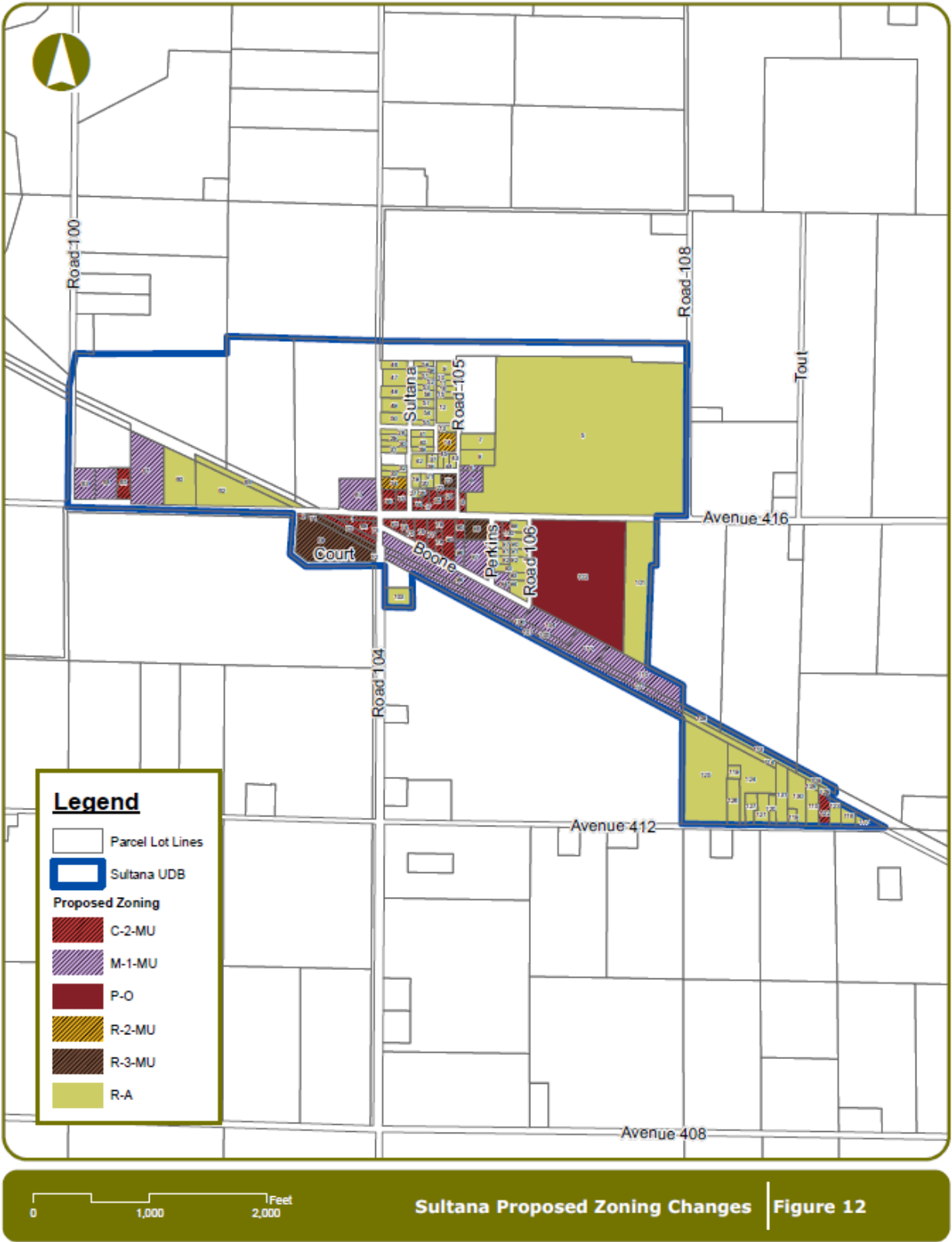
# SULTANA COMMUNITY PLAN

Figure 11 - Existing Zoning Plan



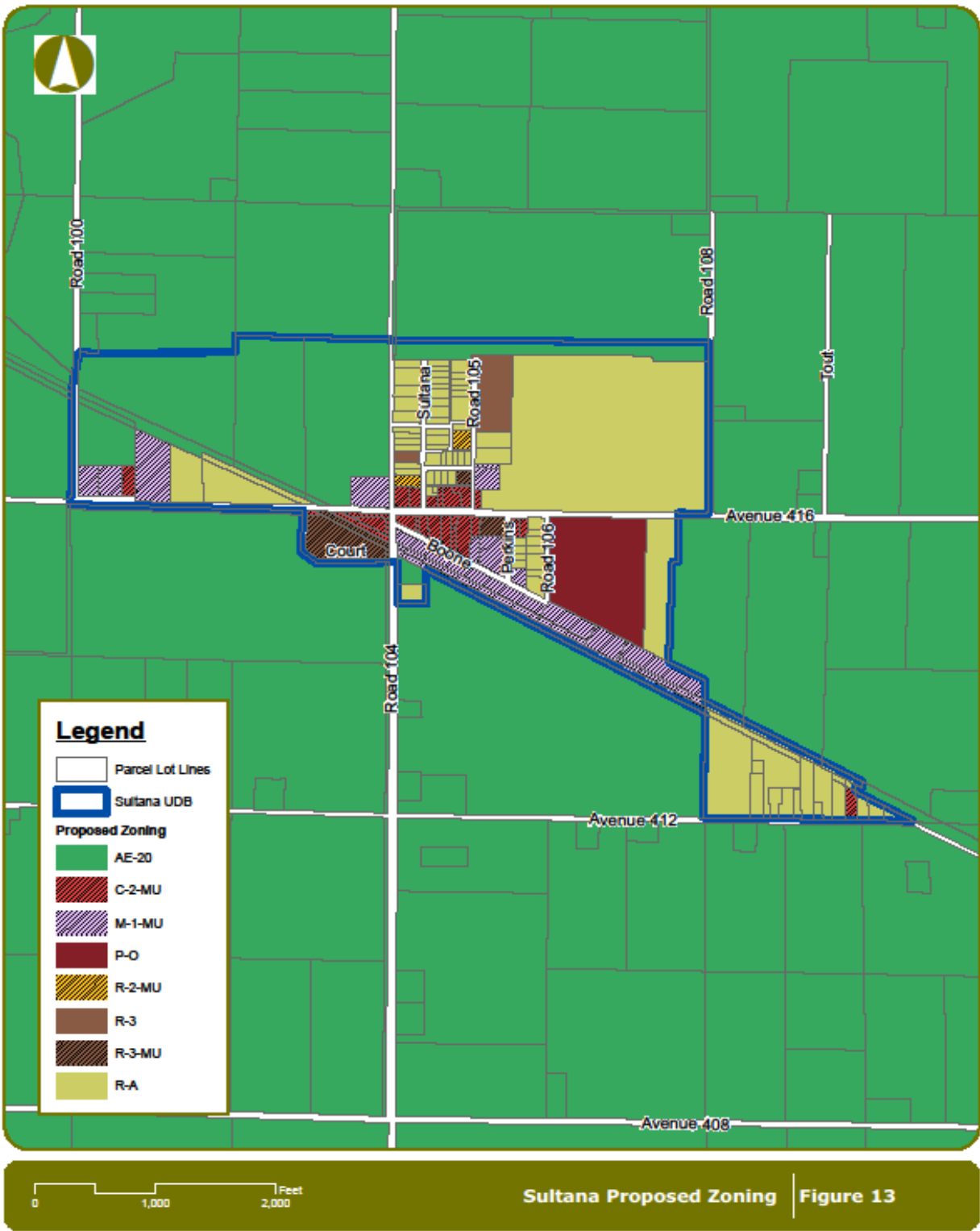
# SULTANA COMMUNITY PLAN

Figure 12 - Proposed Zoning Changes



# SULTANA COMMUNITY PLAN

Figure 13 - Proposed Zoning District



# SULTANA COMMUNITY PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# SULTANA COMMUNITY PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	



# SULTANA COMMUNITY PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure <	CO, C-1, C-2, C-3, M-1	C-3, M-1

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10,000 sq. ft.		
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1

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Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3,

# SULTANA COMMUNITY PLAN

		M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3, *C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3,

# SULTANA COMMUNITY PLAN

		M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	

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School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# SULTANA COMMUNITY PLAN

## A-2 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the Community of Sultana, unless otherwise provided in this Ordinance.

- PURPOSE

APPLICATION

USE

A.

B.

C.

The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

This overlay zone only applies to the community of Sultana.

No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Sultana.

Within the Mixed-Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above-mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses:

All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# SULTANA COMMUNITY PLAN

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gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy-five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and



# SULTANA COMMUNITY PLAN

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zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Sultana. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

# SULTANA COMMUNITY PLAN

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## A-3 Development Standards (Mixed Use Zoning District)

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To promote Economic Development within the Sultana Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Sultana. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Sultana Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Sultana. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

# SULTANA COMMUNITY PLAN

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# SULTANA COMMUNITY PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

# SULTANA COMMUNITY PLAN

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery

(Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

# SULTANA COMMUNITY PLAN

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LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

# SULTANA COMMUNITY PLAN

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WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first-floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.



# SULTANA COMMUNITY PLAN

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SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long-lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.



# SULTANA COMMUNITY PLAN

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

# SULTANA COMMUNITY PLAN

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should

be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# SULTANA COMMUNITY PLAN

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## APPENDICES

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9371

General Plan Amendment GPA 17-035-Resolution No. 9372

Section 18.9 “Mixed Use” Combining Zone-Resolution No. 9373

Section 16 “By Right Uses”-Resolution No. 9374

Zoning Districts Map-Resolution No. 9375

Sustainable Community Plans-Resolution No. 9376

## COUNTY OF TULARE, STATE OF CALIFORNIA

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Alpaugh, East Orosi, London, Richgrove, and Sultana to the Mixed-Use Combining Zone; (3) allow additional by-right uses

within the aforementioned Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Community Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-042 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for PZC 17-004 Alpaugh Zoning District Map, PZC 17-005 East Orosi Zoning District Map, PZC 17-008 London Zoning District Map, PZC 17-011 Richgrove Zoning District Map, PZC 17-012 Sultana Zoning District Map within the respective Urban Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-041 an amendment to Section 16 to allow additional by-right uses in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Community Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Community Plans;
  - b. Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Community Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Community planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Community Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Community Plans 2017 Update General Plan Amendment 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004; Change of Zone No. PZC 17-004 (Zoning District Map); East Orosi General Plan Amendment No. GPA 17-034; Change of Zone No. PZC 17-005 (Zoning District Map); London General Plan Amendment No. GPA 17-008; Change of Zone No. PZC 17-008 (Zoning District Map); Richgrove General Plan Amendment No. GPA 17-011; Change of Zone No. PZC 17-011 (Zoning District Map); Sultana General Plan Amendment No. GPA 17-012; Change of Zone No. PZC 17-012 (Zoning District Map; Change of Zone No. PZC 17-042 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-041 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Community Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,



4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Community Plans 2017 Update. Therefore, the public will benefit from the Community Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III COMMUNITY PLANS, OF THE ) RESOLUTION NO.9372  
TULARE COUNTY GENERAL PLAN FOR THE )  
COMMUNITY PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-004, GPA 17-034, )  
GPA 17-008, GPA 17-011, GPA 17-012, )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Community Plan of the Tulare County General Plan as set forth in Exhibits "A to E" for the Community Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Alpaugh General Plan Amendment No. GPA 17-004, East Orosi General Plan Amendment No. GPA 17-034, London General Plan Amendment No. GPA 17-008, Richgrove General Plan Amendment No. GPA 17-012, Sultana General Plan Amendment No. GPA 17-012 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the communities as part of the Community Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Community Plans 2017 Update, and



WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Community Plans of the Tulare County General Plan for the Community Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Community Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Community Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Urban Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the adoption of urban development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Community Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Community Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:


C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Community Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit “A” – 2017 Alpaugh Community Plan (Part III)
- Exhibit “B” – 2017 East Orosi Community Plan (Part III)
- Exhibit “C” – 2017 London Community Plan (Part III)
- Exhibit “D” – 2017 Richgrove Community Plan (Part III)
- Exhibit “E” – 2017 Sultana Community Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO            )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT    )       RESOLUTION NO. 9373  
WITH THE ADOPTED COMMUNITY PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE   )  
NO. PZC 17-041                                    )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Community Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352.

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana ;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, Earlimart, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and



Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Community Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Communities of Alpaugh, East Oroshi, London, Richgrove, and Sultana, consistent with the adopted Community Plans 2017 Update.



The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:       Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:       None  
ABSTAIN:   None  
ABSENT:    None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9374
CONSISTENT WITH THE ADOPTED	)	
COMMUNITY PLANS, AS PROPOSED IN	)	
CHANGE OF ZONE NO. PZC 17-042	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Earlimart, Ducor Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hypericum, Jovista, Matheny Tract and Tooleville. The Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Communities of Alpaugh, East Orosi, London, Richgrove, and Sultana , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
ORDINANCE NO. 352, ZONING ORDINANCE )  
DISTRICT MAPS TO REZONE PROPERTIES )  
IN THE URBAN DEVELOPMENT BOUNDARIES )  
OF ALPAUGH, EAST OROSI, LONDON, )  
RICHGROVE, AND SULTANA ) RESOLUTION NO. 9375  
CONSISTENT WITH THE ADOPTED )  
COMMUNITY PLANS 2017 UPDATE (GPA 17-035), )  
AS PROPOSED IN CHANGE OF ZONE NO. PZC's )  
PZC 17-004, PZC 17-005, PZC 17-008, PZC 17-011, )  
AND PZC 17-012, )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Alpaugh Rezoning Plan (PZC 17-004), to include the Community of Alpaugh, consistent with the adopted Alpaugh Community Plan 2017 Update, Exhibit "E" East Orosi Rezoning Plan (PCZ 17-004), to include the Community of East Orosi, consistent with the adopted East Orosi Community Plan 2017 Update, Exhibit "H" London Rezoning Plan (PCZ 17-016), to include the Community of London, consistent with the adopted London Community Plan 2017 Update, Exhibit "K" Richgrove Rezoning Plan (PCZ 17-017), to include the Community of Richgrove, consistent with the adopted Richgrove Community Plan 2017 Update, Exhibit "N" Sultana Rezoning Plan (PCZ 17-018), to include the Community of Sultana, consistent with the adopted Sultana Community Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Community Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Community Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Alpaugh, East Orosi, London, Richgrove, and Sultana Communities;
3. The proposed changes in zone district designations identified in the Alpaugh, East Orosi, London, Richgrove, and Sultana Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Alpaugh, East Oroshi, London, Richgrove, and Sultana Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Alpaugh Change of Zone No. PZC 17-004, East Oroshi Change of Zone No. PZC 17-005, London Change of Zone No. PZC 17-008, Richgrove Change of Zone No. PZC 17-011, Sultana Change of Zone No. PZC 17-012, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" Rezoning Plans consistent with the adopted Alpaugh, East Oroshi, London, Richgrove, and Sultana Community Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Alpaugh Change of Zone No. PZC 17-004***

*Exhibit "A" – Zoning District Ordinance Map*

*Exhibit "B" – Zoning Map*

*Exhibit "C" – List of Affected Properties/Map*

***East Orosi Change of Zone No. PZC 17-005***

*Exhibit "D" – Zoning District Ordinance Map*

*Exhibit "E" – Zoning Map*

*Exhibit "F" – List of Affected Properties/Map*

***London Change of Zone No. PZC 17-008***

*Exhibit "G" – Zoning District Ordinance Map*

*Exhibit "H" – Zoning Map*

*Exhibit "I" – List of Affected Properties/Map*

***Richgrove Change of Zone No. PZC 17-011***

*Exhibit "J" – Zoning District Ordinance Map*

*Exhibit "K" – Zoning Map*

*Exhibit "L" – List of Affected Properties/Map*

***Sultana Change of Zone No. PZC 17-012***

*Exhibit "M" – Zoning District Ordinance Map*

*Exhibit "N" – Zoning Map*

*Exhibit "O" – List of Affected Properties/Map*

Attachment 2

***Community Plan Rezoning Full Ordinance***

*Exhibit A - Community Plan Rezoning Full Ordinance*



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF SUSTAINABLE  
COMMUNITY PLANS

)  
)

RESOLUTION NO. 9376

Resolution of the Planning Commission of the County of Tulare recommending the Board of Supervisors adopt and accept the Sustainable Community Plans. A major component of the Community planning process included a thorough examination and cataloguing of existing land uses which suggested a strong need for infrastructure improvements, land use changes and zoning changes, where feasible, in order to fulfill the goals of the General Plan and Senate Bill 244. The Unincorporated Community Infrastructure Maps provided the measurement tools needed to identify infrastructure solutions, appropriate improvement projects, and planning policy analysis. The resulting objective of the proposed Community Plans is aimed at reducing greenhouse gas emissions, promoting equity, providing economic stability; and thus, enhancing sustainability, as well as protecting the environment, and promoting healthy and safe communities. The Plans necessitated and ensured methods of public participation with a wide variety of stakeholders.

WHEREAS, The Budget Act of 2010/11 authorized the Department of Conservation (Department) to implement and administer \$63.15M in project funds (Proposition 84, the Safe Drinking Water, Water Quality and Supply, Flood Control, River and Coastal Protection Bond Act of 2006) to provide Planning Grants and Incentives awarded by the Strategic Growth Council (Council). The primary goal of this grant program is to develop and implement plans consistent with Chapter 13, Division 43, Public Resources Code, Sections 75127 and 75128; and

WHEREAS, The County received a grant from the Strategic Growth Council in 2014, Grant No. 2014-631, to study the feasibility of integrating infrastructure and land use, with the needs of rural unincorporated communities based on the Communities within Tulare County that received a top 10% ranking utilizing the *CalEnviroScreen* methodology and in fulfillment of the General Plan's Housing Element and SB 244 Plan (2014); and

WHEREAS, *"The work upon which this publication is based was funded in whole or in part through a grant awarded by the Council."* Preparation and processing of the Sustainable Community Plans took an estimated 36 months; and

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County

Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and

WHEREAS, the Sustainable Community Plans included all tasks and deliverables specified in the work plan including project development and meetings, data gathering, public workshops, plan development, public hearings, and grant reporting and administration; and

WHEREAS, the Sustainable Community Plans include strategies that reduce energy consumption, conserve water, improve air and water quality, and provide other community benefits; and

WHEREAS, staff has performed necessary investigations, prepared a written report, and recommended modifications and approval of these proposed Plans; and

WHEREAS, staff has produced the Final Report document attached as "Exhibit A" pending further review by the Planning Commission and the public, and will make all necessary administrative changes subject to this review and prior to the final submittal to the Board of Supervisors; and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by a representative from Leadership Council, who supported the project was received and recorded at a regular meeting of the Planning Commission on November 8, 2017; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. Recommend that the Board of Supervisors find that the said Sustainable Community Plans meet the Goals and Objectives of the Council in fulfillment of the Grant requirements as specified in the work plan.

The foregoing resolution was adopted upon motion of Commissioner Gong, seconded by Commissioner Aguilar, at a regular meeting of the Planning Commission on the 8th day of November, 2017, by the following roll call vote:

AYES:	Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot
NOES:	None
ABSTAIN:	None
ABSENT:	None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A- Strategic Growth Council Final Report

Appendix B – Board of Supervisors  
Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

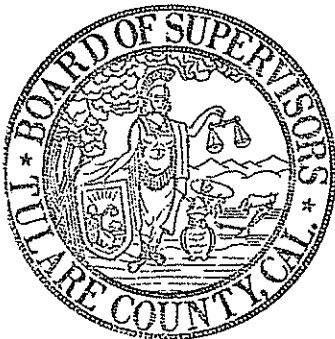
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **Appendix C: Hamlet Plans**

**Allensworth  
Delft Colony  
East Tulare Villa  
Lindcove  
Monson  
Seville  
Teviston  
Tonyville  
Waukena  
West Goshen  
Yettem**



## **Allensworth Hamlet Plan**

# ALLENSWORTH HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# ALLENSWORTH HAMLET PLAN

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# ALLENSWORTH HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# ALLENSWORTH HAMLET PLAN

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## ALLENSWORTH HAMLET PLAN

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# Allensworth Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Allensworth Hamlet Plan: GPA 17-014  
Zoning District Map: PZC 17-037



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# ALLENSWORTH HAMLET PLAN

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# ALLENSWORTH HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)



# ALLENSWORTH HAMLET PLAN

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# ALLENSWORTH HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Mike Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# ALLENSWORTH HAMLET PLAN

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# ALLENSWORTH HAMLET PLAN

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# ALLENSWORTH HAMLET PLAN

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# ALLENSWORTH HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Allensworth is to develop a plan which can accurately reflect the needs and priorities of this community. Allensworth is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks) and to stimulate economic development within the community.

Allensworth is a census-designated place located in the southwest portion of Tulare County. It is generally bounded by Avenue 24 in the south, Attocks Avenue in the north, Road 76 in the west, and State Route (SR) 43 in the east and encompasses 3.1 square miles of land.

“Allensworth is the only California town to be founded, financed and governed by African Americans. The small farming community was founded in 1908 by Colonel Allen Allensworth and a group of others dedicated to improving the economic and social status of African Americans. Uncontrollable circumstances, including a drop in the area’s water table, resulted in the town’s demise. With continuing restoration and special events, the town is coming back to life as a state historic park. The park’s visitor center features a film about the site. A yearly rededication ceremony reaffirms the vision of its pioneers.”<sup>1</sup>

“In August 1908 Colonel Allen Allensworth and four other settlers established a town founded, financed and governed by African Americans. Their dream of developing an abundant and thriving community stemmed directly from a strong belief in programs that allowed blacks to help themselves create better lives. By 1910 Allensworth’s success was the focus of many national newspaper articles praising the town and its inhabitants.

An unavoidable set of circumstances made it impossible for the residents of this tiny town located 30 miles north of Bakersfield to achieve their founders’ dreams over the long term. But the town did remain home to a handful of families and individuals throughout the 20th century, and true to the courage and resolve of its founders, the town has survived and persevered, earning the well-deserved title “The town that refused to die.”

In 1974 California State Parks purchased land within the historical townsite of Allensworth, and it became Colonel Allensworth State Historic Park. Today a collection of lovingly restored and reconstructed early 20th-century buildings—including the Colonel’s house, historic schoolhouse, Baptist church, and library—once again dots this flat farm country, giving new life to the dreams of these visionary pioneers. With continuing restoration and special events, the town is coming back to life as a state historic park.

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<sup>1</sup> Tulare County Background Report page 4-3

# ALLENSWORTH HAMLET PLAN

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## Location

The community of Allensworth (**see Figure 1**) is located on the east side of the San Joaquin Valley, and is a census-designated place located in the southwest portion of Tulare County. Allensworth encompasses 3.1 square miles of land, just east of Kings County. It is bounded by Avenue 24 in the south, Attocks Avenue in the north, and Road 76 in the west, SR 43 runs east of and parallel to the Burlington Northern-Santa Fe Railroad tracks and serves as a physical boundary in the east, however there is no station currently operating (**see Figure 2**). Allensworth is located in Section 05, 08, 09, 15, & 16, Township 24 South, Range 24 East MDB&M, and can be found within the Dinuba West, United States Geological Survey 7.5 minute topographic quadrangle. Allensworth is located at an elevation of 213 feet above sea level. The coordinates of Allensworth are: Latitude: 35.8655516 and Longitude: -119.384694.

## Planning Area

The existing Allensworth Hamlet Development Boundary (HDB) area consists of 1,051.4 acres (**see Figure 3**). The Land Uses within the HDB include agricultural activities, including agriculture (orchards and pasture), which currently occupy 57 percent of the 1,051.4 acres. Urban development, such as residential development occupy 4 percent of the 1,051.4 acres. The remaining 10 percent are lands dedicated for Right-of-way (**see Table 1**).

Table 1 - Allensworth Land Use	
Land Use	Acres
Residential	415.3
Agricultural	597.1
Rights-of-way	38.9
<b>Total</b>	<b>1,051.4</b>

Source: Tulare County GIS

There are changes being proposed to the Hamlet Development Boundary to align with the railroad tracks to the east and along Road 24 to the south and Road 80 east (**see Figure 14**).



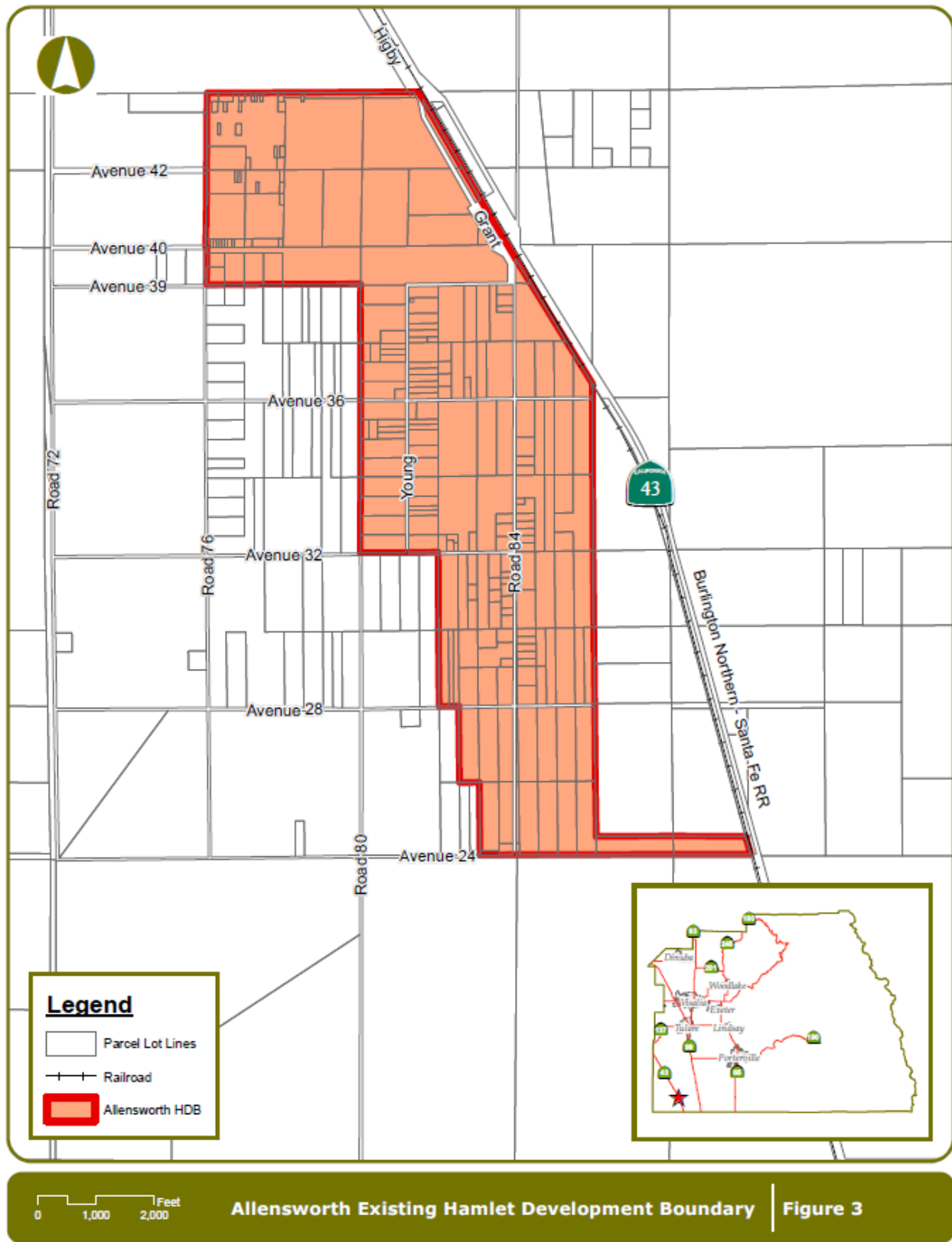
# ALLENSWORTH HAMLET PLAN

Figure 2 - Allensworth Aerial Map



# ALLENSWORTH HAMLET PLAN

Figure 3 – Existing Allensworth HDB





# ALLENSWORTH HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises (SHE) went door to door throughout the community of Allensworth to distribute flyers at homes, businesses and schools. SHE emphasized residents about the importance of participating in this process.

### Community Outreach Steps

Self-Help Enterprises (SHE) utilized various outreach methods to promote community participation and increase awareness of the SGC project.

- 1.) An informational flyer was developed.
- 2.) On January 6 and January 9, 2016 SHE staff went door to door to distribute flyers to homes.
- 3.) SHE staff partnered with Allensworth Progressive Association, a local service organization to encourage community involvement, gather support and to spread the word about the importance of attending this meeting. This group worked together with SHE to distribute fliers on January 9, 2016.
- 4.) Throughout the community outreach process, SHE staff explained the goals of the project and emphasized the importance and benefits of participating in the SGC community meeting.
- 5.) SHE partnered with the Allensworth Elementary School District (AESD) to distribute flyers to every student in the school district and posted flyers at the school site.
- 6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted around town on street signs and fences.

### Community Meeting

The Strategic Growth Council held a community input meeting in Allensworth on Tuesday, January 12, 2016 at 6:00 pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held at the Allensworth Community Center located at 8123 Ave 36 Allensworth CA 93219. More than 35 residents attended the meeting and 30 surveys were collected. Present were representatives from the Allensworth Community Services District, Allensworth Elementary School District and Tulare County Sheriff's Department.

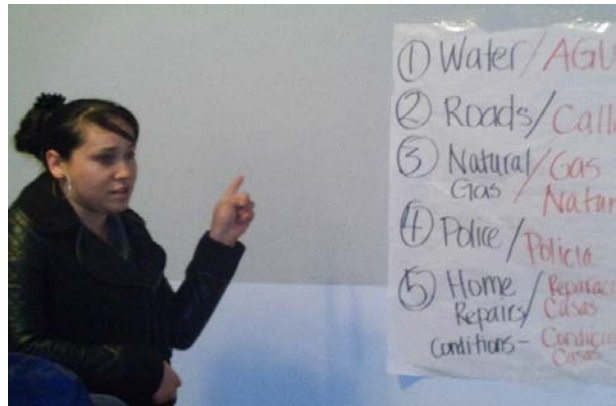


# ALLENSWORTH HAMLET PLAN

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## Community Survey

A detailed survey was developed in efforts to gather specific community information about different areas of improvement. The survey inquires about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.



During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/ concerns:

## Priority Concerns

- 1 Energy Sources
- 2 Natural Gas
- 3 Water Issues
- 4 Sewer
- 5 Home Repair Assistance
- 6 Heat
- 7 Road Conditions
- 8 Internet Access



# ALLENSWORTH HAMLET PLAN

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## **Energy Sources**

Residents expressed an interest for alternative energy sources such as Solar Panels and other energy saving alternatives. They support the State and the County investing in solar powered projects whenever possible. People request information about low income or government programs and funding opportunities that can help residents afford the cost of transitioning to alternative energy sources.

## **Natural Gas**

Allensworth residents expressed a need for natural gas service. Residents believe this can be done by extending service from an existing natural gas transmission line located on the southern end of the community on Avenue 24. A majority of the residents reported that they use propane as a source of fuel. Families reported the high cost of propane is a burden for them, this cost results in people not heating their homes to a comfortable temperature. Parents are especially concerned about this because it can lead to increased cold and flu in small children, as well as in elderly people. Families report that they sometimes use their stove tops to warm their homes, this is safety hazard as people report sometimes falling asleep with the stove top or oven on.

## **Water**

Water is the highest priority for the residents of Allensworth. as there has been a history of arsenic contamination since the 1960's. A regional project is a potential solution for this community issue. The Strategic Growth Council grant awarded to Tulare County in 2012 will investigate the feasibility of a regional solution for Allensworth and Alpaugh, by building on a potential partnership with Angiola Water District located south of Corcoran. Working with the neighboring community of Alpaugh through some form of consolidation could potentially resolve Allensworth's water problems. Although Allensworth is located several miles from Alpaugh, the two communities face similar problems with regard to economy of scale, contamination and revenue deficiencies. At our SGC meeting, people had questions about how the possible consolidation project will affect them. Residents were advised and encouraged to attend the next consolidation project meeting on Thursday January 14, 2016 at 6:00 pm at the Allensworth School Cafeteria. The Strategic Growth Council grant should be coupled with the Tulare Lake Basin Disadvantaged Community Pilot Study to advance some solutions for the region. The other big unresolved problem is the Community Services District's moratorium on new water connections due to concern over insufficient water supplies

## **Sewer System**

A community sewer system is a priority concern for many residents. The community is currently dependent on individual septic tanks for sewage disposal. Residents stated that they would prefer to be on a community wide sewer system and that they are willing to pay a monthly fee for the service.

## **Home Repairs**

Allensworth residents are concerned because many homes are desperately in need of repairs. It was reported that community members are living in houses and trailers that are substandard and are not up to building code. Some older homes have deteriorated and are facing situations such as: bad flooring, no functioning heat or cooling system, and no running water or sewer. Home owners and landlords are requesting resources that will help with the home repair costs.

# ALLENSWORTH HAMLET PLAN

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## **Heaters**

Many Allensworth residents do not have a central home heating system because they do not have natural gas. Residents report the need for safe, reliable heating units. Families report using their stove tops to warm their homes, which creates safety hazards as people report sometimes falling asleep with the stove top or ovens left on. The San Joaquin Valley Air Pollution Control District has a program that supplies families with safe wood stoves, this may be a potential solution to help to safely warm the homes of Allensworth residents.

## **Road Conditions**

Road conditions are of high concern to Allensworth residents. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on Allensworth roads. Residents report that roads are narrow and that two passing cars each other can hardly fit on one road at the same time. According to residents, Allensworth roads are not regularly maintained and the few times that roads have been serviced, they consider the repair work to be poor because the roads were patched and not actually repaired. The following roads reported by the residents to be a top priority:

- Avenue 32
- Avenue 36
- Road 24
- Road 28
- Road 84

## **Internet Access**

Most families in Allensworth do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. Without access to the internet, Allensworth residents are unable to do things such as online job hunting, apply for resources, or provide homework help to their kids.

## **Follow Up Meeting on October 11, 2017**

The County continues to work with the community and on October 11, 2017, the County met with members of the Community to go over the plans for the Community. The following issues were discussed and new information was provided as follows:

## **Allensworth Energy Alternatives**

The Public Utilities Commission of the State of California under Rulemaking 15-03-010 (Filed March 26, 2015) is instituting “Rulemaking” to identify Disadvantaged Communities in the San Joaquin Valley and analyze “Economically Feasible Options” to increase access to affordable energy in those disadvantaged communities.

Under this order, Allensworth is being considered for a Pilot Project through the above-named proceedings. If selected as a “Pilot Community,” the Allensworth Community would be the “Pilot Project Host”. Through this rulemaking proceeding, PG&E and SoCal Gas are both considering project proposals in Allensworth. The two utilities will likely compete for a project by presenting their

# ALLENSWORTH HAMLET PLAN

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respective proposals to the Community of Allensworth at a community meeting with Commission participation. After reviewing the details of both projects, the community will choose which project they prefer. PG&E is considering the proposal of an *electrification* project and SoCal Gas is considering proposing a *gas pipeline extension* project. Self Help and the Community estimate that a Commission decision on approval of pilot projects will happen in March 2018.

## **Waste water**

Self Help Enterprises applied for a planning grant in October for feasibility studies of a sewer project in Allensworth to improve the water quality.

## **Internet / Wifi Service**

It was again brought to the County's attention that there is no cell / internet service in Allensworth. It is noted to the Community that AT&T is currently pursuing a grant to secure internet service to local disadvantaged communities, and that one tower is being proposed for Allensworth.

## ALLENSWORTH TECHNICAL BACKGROUND REPORT

### **DEMOGRAPHICS**

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Allensworth.

### Population

In 2015, the Population in Allensworth was 565 (see Table 2).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Allensworth</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	281	49.7%
Female	19,334,329	50.3%	226,607	49.9%	284	50.3%
Total	38,421,464		454,033		565	

2011-2015 American Community Survey 5-Year Estimates

### Projected Population

"The San Joaquin Valley faces major challenges. One concern is how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."

# ALLENSWORTH HAMLET PLAN

## Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Allensworth's population during the Planning Period (see Table 3).

Table 3 - Projected Annual Growth Rate		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

## Median Age

Allensworth's population's median age is younger than the median age of both Tulare County, as well as the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Allensworth	22.6

2011-2015 American Community Survey 5-Year Estimates

In 2015, approximately 1.4% of Allensworth's population was White, 7.8% was African American, 0% was Native American, .04% was Asian, and 1.1% was two races or more (see Table 5). Approximately 92.3% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Allensworth	%
Total	38,421,464		454,033		565	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	521	92.3%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	7	1.4%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	44	7.8%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	2	0.4%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	6	1.1%

2011-2015 American Community Survey 5-Year Estimates

## Employment in Allensworth

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see Table 6).

# ALLENSWORTH HAMLET PLAN

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Allensworth was about 12.4% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 12.4% includes only the employable labor force (that is, not every person of the population) results in about 46 unemployed persons of Allensworth's person labor force of 220.

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>Allensworth</b>
Population 16 years & over	30,312,429	325,404	372
In labor force	19,269,449	194,420	220
Civilian labor force	19,137,441	194,102	220
Employed	17,246,360	170,780	174
Unemployed	1,891,081	23,322	46
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	152

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Allensworth's median household income was \$28,929, compared to \$42,031 for Tulare County and \$61,818 for the State of California (see Table 7).

<b>Table 7 - 2011-2015 American Community Survey: Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$30,318
<b>Tulare County</b>	\$42,031	\$44,814	\$17,876
<b>Allensworth CDP</b>	\$28,929	\$22,500	\$8,112

2011-2015 American Community Survey 5-Year Estimates

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Allensworth's median household income was \$28,929, whereas the State of California's median household income was \$61,818. Allensworth's median household income was 46.7 of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

# ALLENSWORTH HAMLET PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes). According to the California Department of Finance, the 2011-2015 American Community Survey data indicated 42.4% of the housing units in Allensworth were owner-occupied (see **Tables 8 & 9**). Similarly, 57.2% of housing units were owner occupied in Tulare County and 54.8% of the housing units were owner occupied in the State of California.

Table 8 - Housing Tenure			
	California	Tulare County	Allensworth
Occupied housing units	12,717,801	133,570	132
Owner-occupied	6,909,176	75,685	56
Renter-occupied	5,808,625	57,885	76

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Allensworth
Owner-occupied	54.8%	57.2%	42.4%
Renter-occupied	45.2%	42.8%	57.6%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing, or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Allensworth indicated that approximately 13% of the housing units were sound. Approximately 38% of Allensworth's housing units were deteriorated and 50% were dilapidated (see **Table 10**).

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Allensworth	2	13%	0	0	2	13	4	25	8	50	16

Source: Tulare County 2015 Housing Element

# ALLENSWORTH HAMLET PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009 (see Table 11). The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Allensworth
Total housing units	13,845,790	144,792	142
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	5
Built 2000 to 2009	1,646,490	25,997	17
Built 1990 to 1999	1,495,571	21,767	21
Built 1980 to 1989	2,117,819	22,733	22
Built 1970 to 1979	2,503,688	27,111	48
Built 1960 to 1969	1,871,029	15,500	17
Built 1950 to 1959	1,907,512	13,694	6
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	6

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015, the average renter occupied household size in Allensworth was 4.34 persons per household (see Table 12). The average owner occupied household size is 4.20.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Allensworth	4.20	4.34

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Allensworth was 3.7%, which was lower than both Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Allensworth	0.0%	3.7%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)



# ALLENSWORTH HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016, (see **Figure 4**) indicates there are special status species within the Allensworth Quadrant Species List (which includes the Allensworth Planning Study Area) consisting of four animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Tipton kangaroo rat (*Dipodomys nitratoide*s, federal endangered); Nelson antelope squirrel (*Ammospermophilus nelsoni*, state threatened); and Blunt-nosed leopard lizard (*Gambelia sila*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975, while the adobe starburst is has been determined to be “extirpated” (that is, “A species no longer surviving in regions that were once part of their range”) and is no longer within the Allensworth project area.

The Pixley National Wildlife Refuge was established in 1959 by executive order to provide wetland habitat for migratory waterfowl and shorebirds. Of the 6,939 acres that comprise the Refuge, approximately 5,350 is upland habitat made up of grassland, alkali playa, and vernal pool habitat, 755 acres consists of seasonal wetlands, and 15 acres consist of riparian habitat. The Pixley National Wildlife Refuge is located 5 miles to the north.

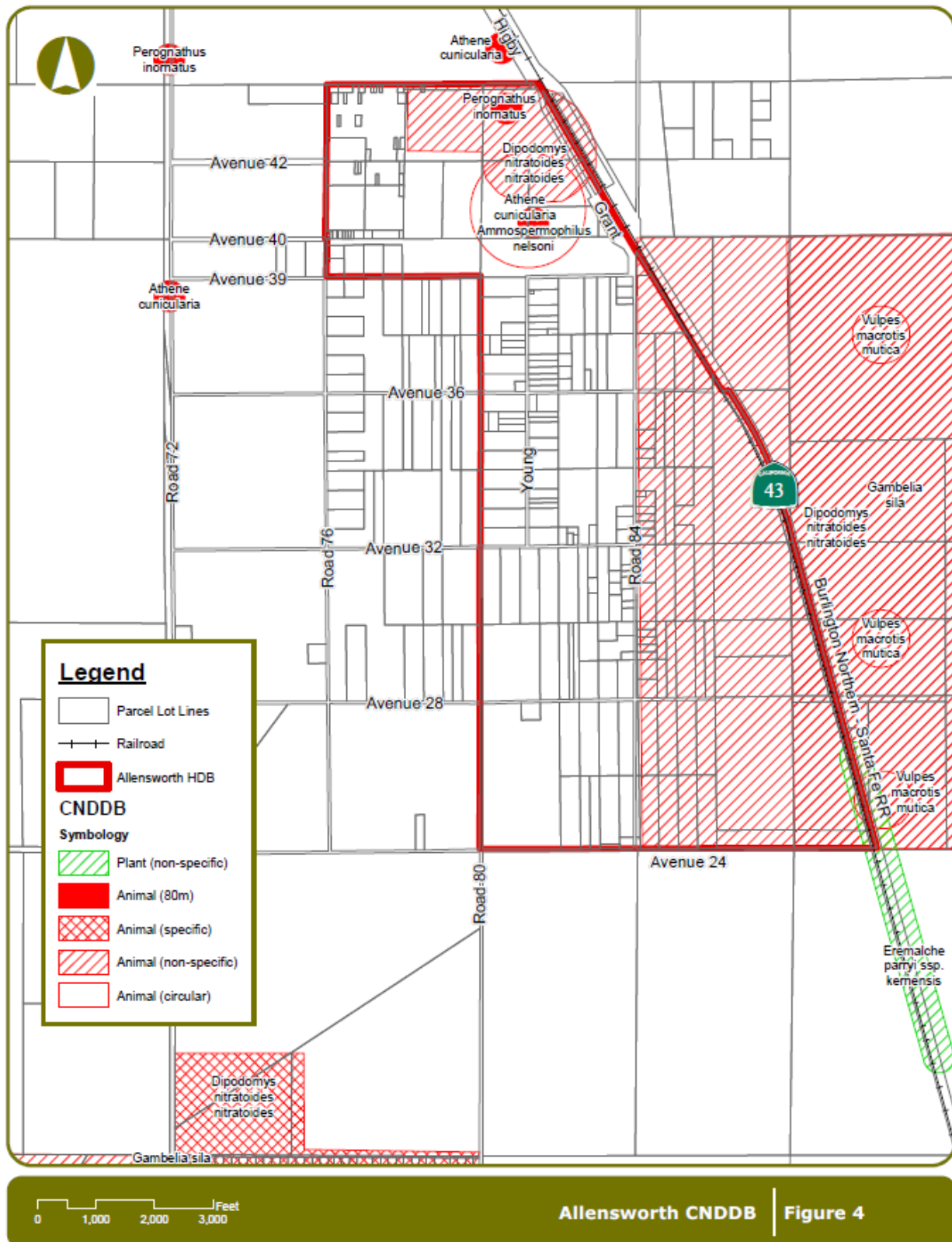
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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> California Department of Fish and Wildlife. California Natural Diversity Database. November 1, 2016



# ALLENSWORTH HAMLET PLAN

Figure 4 - CNDDDB Map



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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Allensworth are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Allensworth] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County (see **Figure 5**). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**Gareck-Garces association, 0 to 2 percent slopes**, is very deep and well drained, medium or high runoff, some areas are ponded; very slow permeability. The soil is suitable for livestock grazing. Vegetation is scattered saltbush with annual grasses and forbs.

**Kimberlina fine sandy loam, 0 to 2 percent slopes MLRA 17**, is very deep and well drained; negligible to medium runoff; moderately rapid and moderate permeability, however saline-sodic phases and soils with sandy clay loam substratums have moderately slow permeability. This soil is suitable for irrigated field, forage, and row crops. Some areas used for livestock grazing. When not irrigated, vegetation is annual grasses and forbs.

**Lethent silt loam, 0 to 1 percent slopes MLRA 17**, is moderately very deep, moderately well drained, medium or high runoff, very slow or slow permeability. Slow permeability is usually present in areas that have been highly modified by deep ripping. The soil is suited for irrigated barley, sorghum, cotton, sugar beets, safflower, and pasture. The native vegetation is sparse stands of saltgrass, saltbush, and red brome.

**Nahrub silt loam, overwashed, 0 to 1 percent slopes**, is very deep, moderately well drained, with slow permeability. The soil is suitable for livestock grazing.

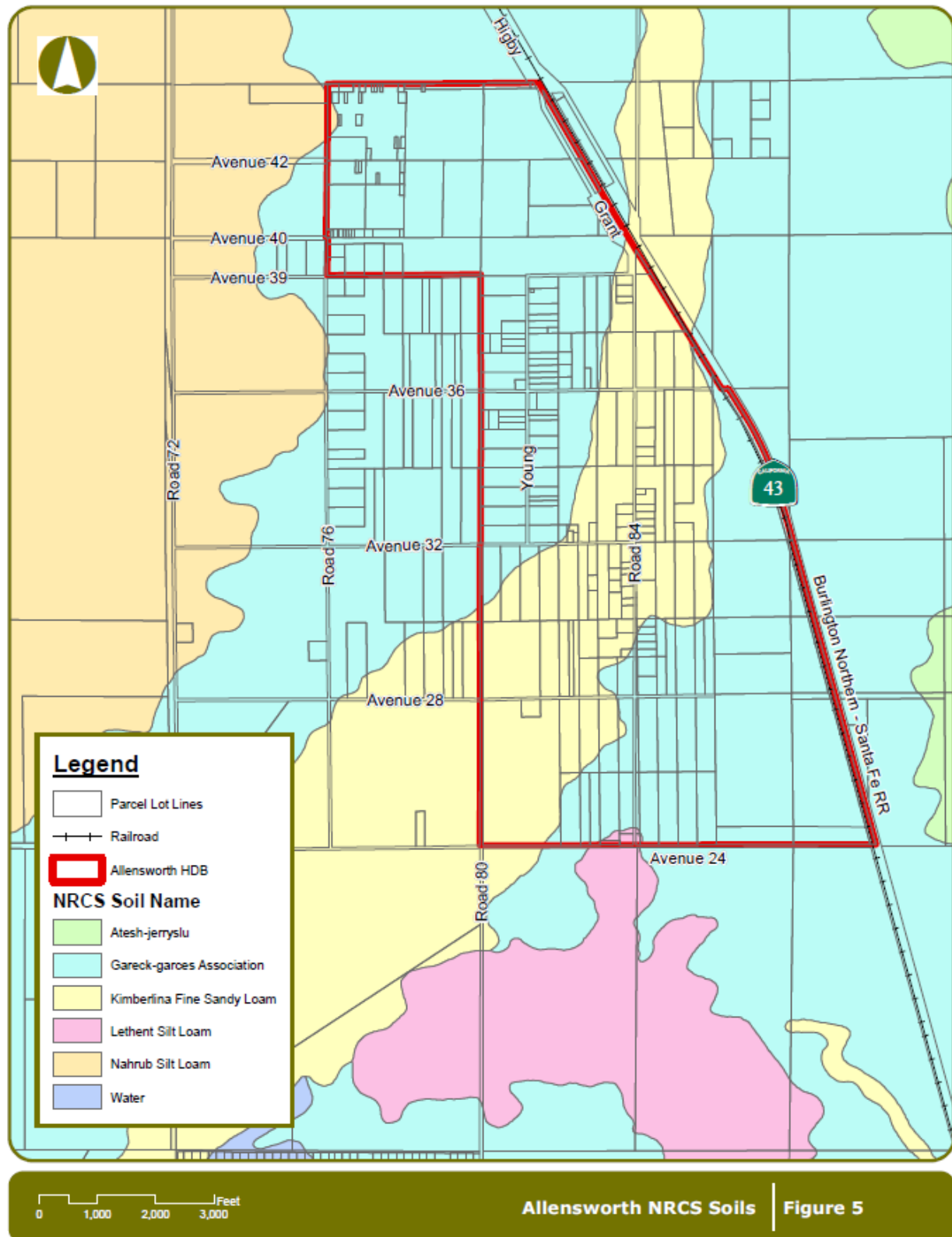
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm). Database August 17, 2016.

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

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Figure 5 - NRCS Soils Map



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## PHYSICAL CONDITIONS

### Air Quality

The Allensworth Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Allensworth is located near the southwestern end of the Valley with prevailing winds from the northwest, it is for susceptible to accumulation of adversely modified

# ALLENSWORTH HAMLET PLAN

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air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of Allensworth and within the community itself include State Highway 43 and agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C2250E, 100 year flood zone meanders along the northeast portion of Allensworth (**see Figure 6**). The expected source of this flooding hazard is White River and its distributary channels. FEMA requires development in Flood Zone AO to be constructed so that a building's ground floor elevation is above the flood contour line existing in the flood area. In addition, there are portions of Allensworth, within the FEMA 500 year flood zone. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

"Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). "Floodplain" or "flood-prone area" means any land area susceptible to being inundated by water from any source. "Base Flood" is the flood having a one percent chance of being equaled or exceeded in any given year. "One-hundred-year flood" or "100 year flood" has the same meaning as "Base Flood." "Special Flood Hazard Area" is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. "Floodway" means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Allensworth there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in Special Flood

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<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C2250E, Panel Number 2250 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

# ALLENSWORTH HAMLET PLAN

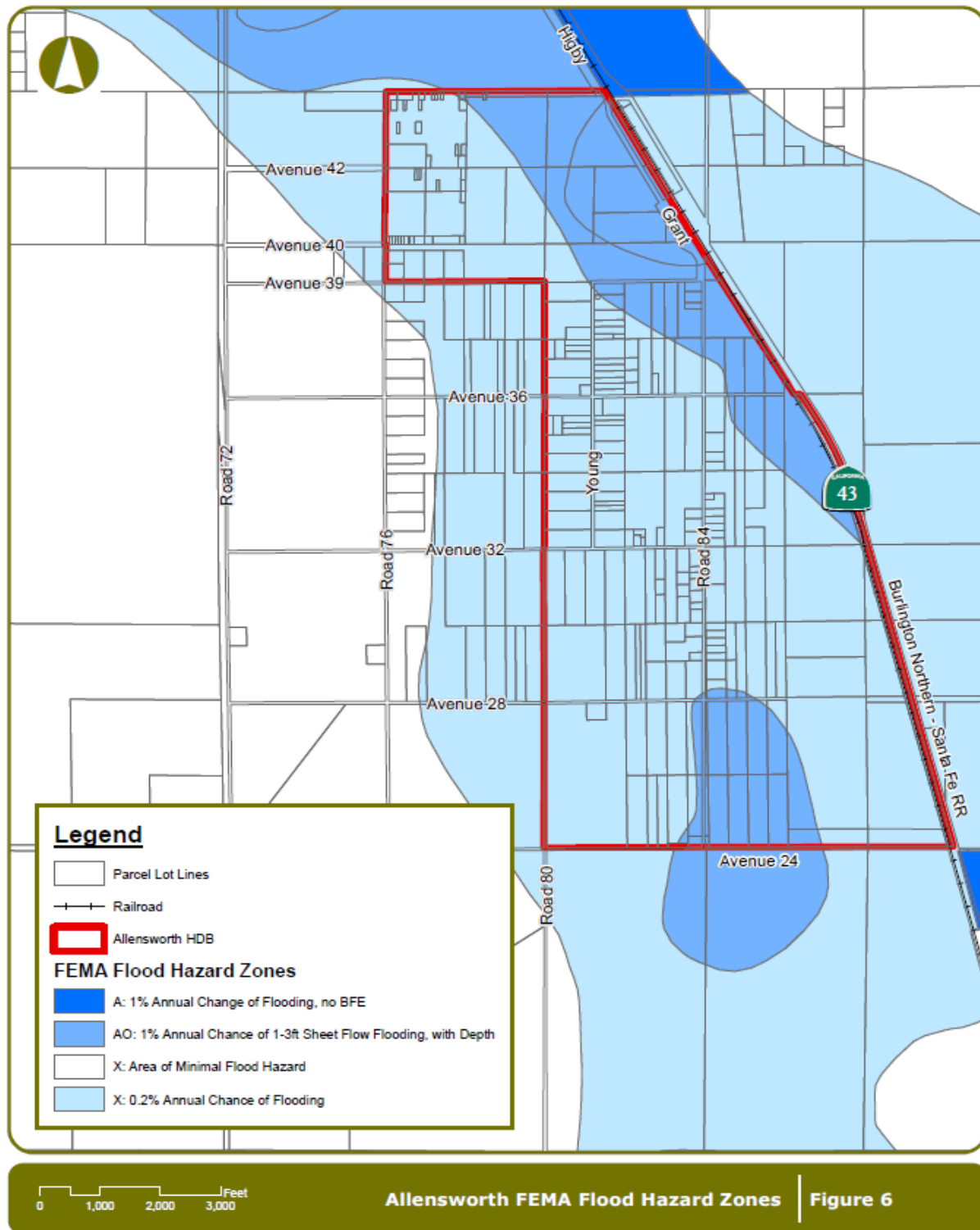
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Hazard Areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.



# ALLENSWORTH HAMLET PLAN

Figure 6 - FEMA Flood Map



# ALLENSWORTH HAMLET PLAN

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## Noise<sup>7</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. Allensworth lies outside of the 60 dB-Ldn contour line.

Operations along the Burlington Northern-Santa Fe Railroad are a primary source of noise in Allensworth. According to the Tulare County General Plan EIR, maximum speed is 70 mph for freights and 79 mph for passenger trains. Freight trains may occur at any time during the day or night and passenger trains generally operate during the daytime (7:00 a.m. – 10:00 p.m.) hours. According to the Wyle methodology, the above described type and frequency of operations will result in present noise exposures of 65 and 60 dB Ldn at approximately 345 and 650 feet, respectively, from the center of the tracks, and at approximately 420 and 820 feet, respectively, from the center of the tracks for projected future operations. There are areas in the northeast side of the Allensworth HDB that are located within the 345 feet of the Burlington Northern-Santa Fe Railroad line. This is measured over a 24 hour period but the temporary burst noises can exceed 65 dB, especially the noise levels in the vicinity of grade crossings are somewhat higher than this due to the use of the warning horn.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks.

### Energy: Natural Gas/Electricity

Pacific Gas and Electric (PG&E) is the main provider of electrical power in Allensworth, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, PG&E purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, PG&E has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom PG&E has arrangements, and rolling blackouts.

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<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)



# ALLENSWORTH HAMLET PLAN

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A gas line runs along Road 24 but is not connected into the Community. The Community currently uses Propane.

## Domestic Water & Wastewater

Domestic water service in Allensworth is provided by the Allensworth Community Service District (CSD), which was formed in 1967. Allensworth does not have sanitary sewer service and relies on individual or community septic systems. **Table 14** shows the number of existing water connections, the capacity of the system, and the number of additional connections the system can accommodate

According to the Municipal Service Review 2011 (MSR), the CSD community water system consists of 2 wells drilled 3 miles east of the community in order to avoid naturally occurring excessive levels of Arsenic in the aquifer underlying the community. The wells are a few hundred feet apart on an east-west line and alternately supply a common 6" line to a 42,000 gallon storage tank. Two centrifugal pumps draw water from the tank to a 5,000 gallon pressure tank and then on to distribution. The wells have a single check valve to prevent back flow to the well from storage. This is an automated system that is triggered by water levels in the storage tank. Wells No. 1 was drilled in 1984 to a depth of 250' and is equipped with a 10 horsepower (hp) submersible pump installed in 1995. Well No. 2 was drilled in 1999 to a depth of 320' and has a 20 hp submersible pump. According to the District's latest Sanitary Survey Report (2008), both wells are properly sealed and secured. The water system contains no treatment method."<sup>8</sup> However, there has been a chlorination system recently installed to mitigate bacteria.

There have been upgrades to the distribution system; six 6" PVC mains and six 6" laterals were installed using a 2007 Community Development Block Grant loan in the amount of \$24,000. The District is required to conduct bacteriological contaminant testing of water samples on a monthly basis. According to the District's Environmental Health file, from September 2007 to November of 2008 only a single sample tested positive for bacteriological contaminants. Additionally, a single sample returned with positive results in both 2009 and 2010. Notice of violation was submitted by Tulare Environmental Health for total Coliform on January 2011. Results of repeat samples or proof of customer's notification of the 2011 violation were not found in the District's Environmental Health file. Notice of violation was also provided in October 2008 for failing to submit Bacteriological sample test results (testing occurs each month). No violations of excessive Nitrate levels were found in the District's Environmental Health file.

Records indicate that the CSD's water system is continuously in violation of the maximum levels set for Arsenic. Most recently (December 2010), an Environmental Health compliance order was provided to the District for violation of maximum Arsenic levels. The order directs the District to notify all district customers of the violation on a quarterly basis, submit proof of customer's notification on a quarterly basis, and submit sample test results to the Tulare Environmental Health Department on a quarterly basis. This order must be followed for as long as the system remains in violation. The order further directs the District to consider various avenues to address the problem and to prepare an action plan, complete with timeline, and submit the plan to the Environmental Health.

On December 29, 2010, the CSD Board adopted Resolution 2010-1109, which imposes a moratorium on new water connections and on the drilling of new wells within district boundaries. According to the resolution, the moratorium was prompted by the high cost associated with pumping groundwater

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<sup>8</sup> <https://www.calwater.com/about/district-information/vis/>

# ALLENSWORTH HAMLET PLAN

from lower depths as a result of decreased groundwater levels coupled with the District's financial inability to drill new wells and therefore meet existing rate payer demand.

The CSD's system is highly vulnerable to Arsenic contamination, as evidenced by the fact that system wells were drilled at their current location (3 miles outside the District's bounds) specifically to avoid naturally occurring excessive levels of Arsenic as well as the numerous notices of violation for excessive Arsenic levels submitted by Tulare Environmental Health. It is further determined that the present groundwater supplies available to the District are inadequate. State or federal grants/loans will be needed to secure new well sites and ensure that the system will not experience failure in the near future. The chart below demonstrates the Arsenic Levels over twelve years.

<b>Allensworth Community Services District                      Arsenic Levels in Active Wells                      Arsenic MCL = 10 ppb</b>		
Date	East Well (ppb)	West Well (ppb)
3/6/1990	17	
11/23/1993	16	
11/4/1996	15	
9/28/1999	10	
10/24/2002	9	
10/26/2005	11	
9/21/2007	11	
12/11/2007	12	
12/13/2007	13	
3/26/2008	13	
8/7/2008	10	
12/19/2008	11	
11/30/2010	12	13
3/29/2011	14	14
6/13/2011	12	12
8/8/2011	10	6
11/16/2011	11	11
4/5/2012	12	7
9/13/2012	12	9
<b>Times Exceeding MCL</b>	<b>18</b>	<b>4</b>
Old off-line well at storage tank site		
6/6/1996	65	

As mentioned above, the December 2010 Environmental Health compliance order directs the District to prepare a plan, complete with timeline, to address the Arsenic contamination issue. It is determined that the scope of solutions contained in the action plan also encompass the groundwater level challenge facing the District and the low-income condition of its customer pool.

Table 14 - Existing Water Connections		
Number of Existing Connections	Capacity	Available
116	116	0

Source: Action Program 9, Tulare County Housing Element. Data current as of January 2011

# ALLENSWORTH HAMLET PLAN

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California Legislature is currently considering enactment of Senate Bill 623 (Monning, Safe Drinking Water Fund) which implements a small new safe drinking water fee (less than \$1 a month for single family home water bills with a low-income rate exemption) that ensure all Californians have access to safe and affordable drinking water. SB 623 bill provides a sustainable source of funding to help subsidize the cost of safe drinking water service for communities like Allensworth. Allensworth CSD passed a letter in support of the bill and will continue to work in tandem with its local and state legislative representation to pass SB 623 or similar legislation in order to secure the funding it needs to provide sustainable safe drinking water at rates that are affordable and reasonable for its residents.

## Sewer

There is no community wide sewer system in Allensworth. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the Allensworth residents are in favor of creating a community wide sewer system.

## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Allensworth does not currently have a storm drainage system.”<sup>9</sup> However, there are ongoing discussions concerning the Angiola Water District in regards utilizing retention basins to reclaim water to the aquifer during flood events and / or recharge back into the canal system.

## Solid Waste

Solid waste disposal services for the Community of Allensworth is provided by Tule Trash, a private company. Solid waste generated in Allensworth can be disposed at Teapot Dome Landfill, located at 21063 Avenue 128, Porterville.

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<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# ALLENSWORTH HAMLET PLAN

## Roads

“There are several roadways in Allensworth that are in need of repair (see **Table 15**). Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

**Table 15 - Roads in Need of Major and Medium Repair in Allensworth**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 39	Road 81 to Road 84	GRX
2	Road 84	Avenue 36 to Avenue 39	GRX
OLAY – overlay resurfacing operation CHIP – chip seal GRX – grind and remix ACST – asphalt reconstruction RCST – cold mix reconstruction			

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

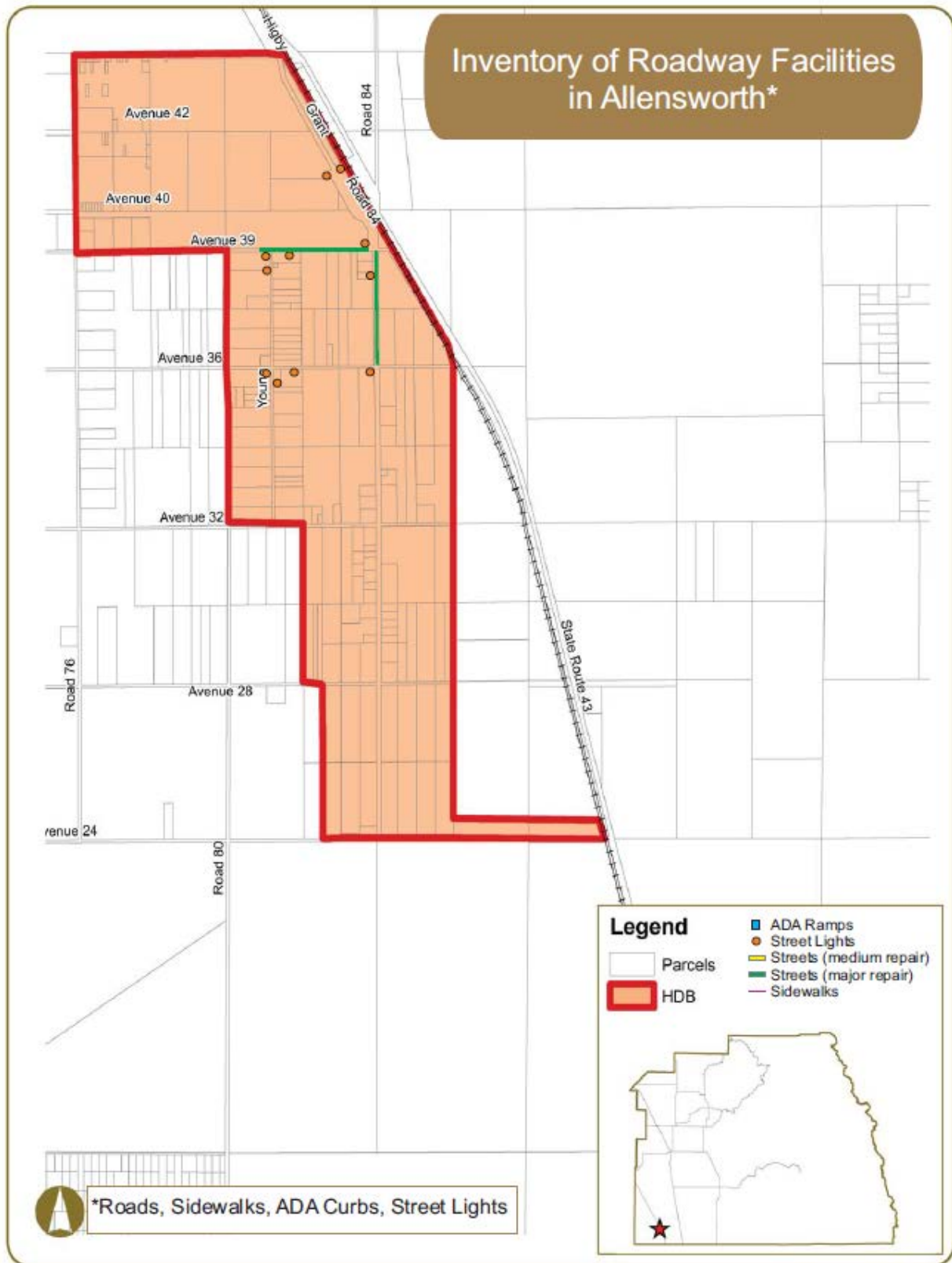
Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>10</sup>

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

# ALLENSWORTH HAMLET PLAN

Figure 7 - Inventory of Roadway Facilities



# ALLENSWORTH HAMLET PLAN

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## Sidewalks

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

There are no sidewalks in Allensworth. However, on September 5, 2017, Allensworth was selected by the California Transportation Commission (CTC) for Active Transportation Program (ATP) Augmentation by staff recommendation for \$313,000, which is one of 2 projects selected by the CTC of 12 projects Tulare County submitted this year. The County has applied for over 30 projects and been selected for 5 to date.

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Allensworth.”<sup>11</sup>

## Street Lights

Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian accidents by increasing the awareness of drivers relative to pedestrians. **Table 16** identifies the location of existing street lights that are maintained by Tulare County, in Allensworth, as well as their specifications. **Figure 7** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.”<sup>12</sup>

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<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element

<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element



# ALLENSWORTH HAMLET PLAN

**Table 16 - Specifications of Existing Street Lights**

Specifications of Existing Street Lights								
No	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 36	Young Road	SW Corner	1343	5800	W	E	SCE
2	Avenue 36	East of Young Road	South Side	1416	5800	W	N	SCE
3	Avenue 36	Road 84	SW Corner	1342	5800	W	E	SCE
4	Avenue 39	Road 84	NW Corner	1401	5800	W	E	PG&E
5	Avenue 39	East of Young Road	South Side	1412	5800	W	N	SCE
6	Avenue 39	Young Road	SW Corner	1413	5800	W	E	SCE
7	Grant Drive	Northwest of Road 84	SW Corner	1410	5800	W	E	SCE
8	Grant Drive	Road 84	West Side	1411	5800	W	E	SCE
9	South of Avenue 36	Young Road	East Side	1515	5800	W	W	SCE
10	South of Avenue 39	Young Road	West Side	1414	5800	W	E	SCE
11	South of Avenue 39	Road 84	West Side	1489	5800	W	E	SCE

(Source: Tulare County Public Works, March 2013)

## Transit

The County of Tulare provides public transportation services to those who are disadvantaged, elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Public transit is currently available in Allensworth. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Allensworth, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified Avenue 56 and State Route 43 as a proposed Class III Bike Project<sup>13</sup>. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are rarely used outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is Avenue 56 and State Route 43.

As stated above, there is \$313,000 in ATP money that has been applied for and \$260,000 awarded to date for sidewalks for and around the school. In addition, the County has developed a complete streets program for Allensworth. In addition, there are plans for a new trail planned from Alpaugh to Allensworth. (See also Complete Streets, Attachment 4).

<sup>13</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# ALLENSWORTH HAMLET PLAN

## Atwell Island-Alpaugh-Allensworth Trail (AAAT) Project

February 2017 Update

**Project Description:** This Project will create a multi-use trail to connect the communities of Alpaugh and Allensworth with one another as well as with two unique and historical attractions; the Bureau of Land Management Atwell Island Project and CA State Parks & Recreation Colonel Allensworth Historic State Park. The AAAT Project will offer recreational and educational opportunities as well as expand access to southwest Tulare County's outstanding birdwatching and wildlife viewing for community members, youth, and visitors alike.

**Location:** Southwest Tulare County, in the Deer Creek sub-watershed. Deer Creek is unique in that it is one of the few undammed streams remaining in California, which enables it to serve the full range of natural ecosystem functions that benefit humans and wildlife. Deer Creek is emblematic of Tulare Valley's native landscape, with riparian corridors adjacent to upland desert scrub habitat. The region has many natural treasures that already call attention to it as a tourist destination, especially among birders. It is also home to many cultural treasures, including Colonel Allensworth Historic State Park and the Native American Yokut heritage found in the vast, historic Tulare Lakebed. There is significant opportunity to invest in the region to elevate it as a tourist destination bringing more wealth to the communities and to invest in youth education and nurture future stewards of the valley.

**Route:** *Under consideration*

**Estimated Funding:** \$230,000 (Phase 1 April 2016 – March 2017)

**Opportunities:** The project will align with other conservation efforts to achieve multiple benefits including ecosystem restoration, land stewardship, environmental education and youth leadership development. It will celebrate and educate the public regarding the cultures of the earliest settlers in the Atwell Island, Alpaugh and Allensworth area; develop capacity for intra-community leadership and collaboration through the Alpaugh & Allensworth Restoration and Conservation Collaborative (AARCC), create tourism opportunities, reduce greenhouse gases due to vehicle travel between communities and enhance quality of life.

**Challenges:** *No insurmountable challenges expected.*

**Partners/participants to date:** Alpaugh and Allensworth citizen advocates and schools, BLM Atwell Island Project, Colonel Allensworth Historic State Park, Friends of Allensworth, Tachi Yokuts Tribe, Tulare Basin Wildlife Partners, Self-Help Enterprises, Deer Creek Storm Water District and local water agencies, Southern California Edison, High Speed Rail, CSET, Tulare County Resources Management Agency and Tulare County Board of Supervisors. Other potential partners include agricultural - philanthropic interests and the Central Valley Community Foundation.



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# ALLENSWORTH HAMLET PLAN



## **Alwell Island Project Work-based Learning (AIP-WBL), Allensworth-Alpaugh-Alwell Island Trail (AAAT), and Allensworth-Alpaugh Restoration and Conservation Collaborative (AARCC) Programs 2017-2019**

**AIP-WBL Program Description:** The Bureau of Land Management (BLM) AIP-WBL Program is envisioned as a permanent four-week summer work-based learning program for 10-30 11th and 12th grade high school students from the rural underserved communities of Allensworth and Alpaugh in SW Tulare County, California. The program begun by BLM in 2016, is conducted at the BLM AIP and contiguous Tonache wetlands with appropriate field trips. The permanent continuation of this program will enable Allensworth and Alpaugh high school students to develop career readiness and the technical skills needed to address regional drought, flood, and other climate change adaptation issues. The 2017-2019 program cost is estimated to be \$260,000.

As soon as funding is available in 2017, the Tulare Basin Wildlife Partners (TBWP)—in its official BLM “Friends of Alwell Island” role—will collaborate with American Conservation Experience (ACE) to hire an Onsite Coordinator to coordinate the program year-round. The Coordinator will work with the Allensworth and Alpaugh School districts and selected teachers as well as with regional partners from key socio-economic sectors to ensure implementation of the program. The Coordinator will be housed at the AIP and will be supported by BLM staff for the equivalent of one day per week.

Students chosen to participate in the summer 2017 program will be given a focus topic upon which to design a region-specific, natural resource-focused project. Focus topic experts will give a series of presentations in classrooms and assemblies throughout the school year, introducing the focus topic from a variety of perspectives.

Possible pathways which students will choose in approaching the focus topic include but are not limited to:

- **Hands-on:** native plant identification, cataloging, and seed collection for the utilization of land restoration
- **Science, Technology, Engineering & Math (STEM):** exploring and examining land use in the Tulare Lake bed area through data accumulation, dissemination, and GIS mapping
- **Historic:** examining and exploring the evolution of the Tulare Lake bed landscape throughout its settlement
- **Artistic:** examining and exploring existing landscapes through photography, drawing, painting, etc.
- **Topics suggested by key partner participants:** key partners include regional water agencies, agricultural entities, environmental justice groups and higher education institutions

**AIP-WBL, AAAT and AARCC Programs:** During the past two years the BLM AIP, Allensworth-Alpaugh community advocates, and TBWP have also collaborated on the Allensworth-Alpaugh-Alwell Island (AAAT) and the Allensworth-Alpaugh Restoration and Conservation Collaborative (AARCC), integrating them with the AIP-WBL program. Together, these three programs comprise a holistic local-regional demonstration project—a model for rural communities throughout the Central Valley.

Under the AAAT and AARCC, with TBWP coordination during the 2016-2019 start-up phase, AARCC community advocates are aiding development of a nature education and recreation trail to connect the communities of Alpaugh and Allensworth with one another and with the two unique local-regional historical attractions—the Alwell Island Project-Tonache Wetlands and Colonel Allensworth State Historic Park (CASHP).

The AAAT will provide opportunities for AIP-WBL participant hands-on project applications, as well as youth and citizen science education to advance drought or flood readiness and climate adaptation. The AAAT will nourish community environmental stewardship, youth leadership development, and youth career readiness. AAAT program coordination by the TBWP in 2017 benefits from partial support from the International Edison Foundation. The AAAT program 2017-2019 cost is approximately \$250,000 in cash and in-kind services value. Significant in-kind value services are provided by the TBWP, BLM, Colonel Allensworth State Historic Park, Deer Creek Storm Water District and the Angiola Water District.

Under the AARCC program, community advocates are preparing to assume management and fiscal responsibility for the WBL and AAAT programs, guided by the highly experienced and trusted retired Tulare County Unified Way Executive Director (Brie, Hernandez) and coordinated by the TBWP. During 2017-2019, the AARCC will develop governance capacity to undertake additional future resource-sharing community benefit projects, enhancing the ecological and economic sustainability of the Allensworth and Alpaugh communities permanently. The Central Valley Community Foundation has offered to hold an endowment for AARCC, providing an important incentive. With the exception of TBWP coordination funded in 2017 by Edison International, costs/expenses under this program are donated by participants.

(February 6, 2017)

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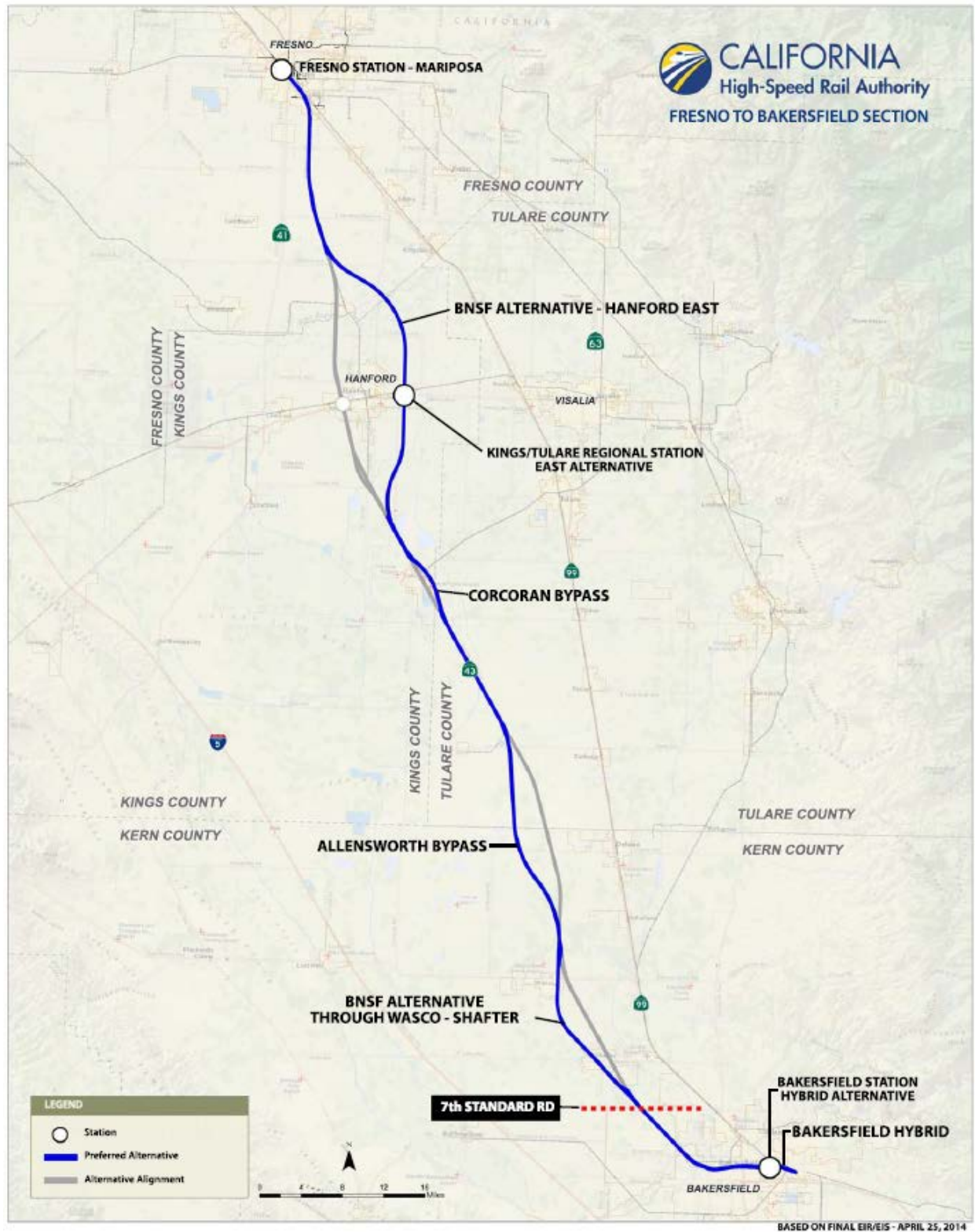
## AMTRAK

The Corcoran AMTRAK station, located 25 miles to the northwest in Kings County, is the closest station to Allensworth providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

## High Speed Rail

High speed rail is anticipated to run adjacent to the SR 43 from stations at Fresno, Hanford and Bakersfield, with two potential bypasses at Allensworth (the “Allensworth Bypass” and Wasco/Shafter. There is potential for construction laydown yards, material and / or equipment storage areas to located in or near Allensworth.

# ALLENSWORTH HAMLET PLAN



# ALLENSWORTH HAMLET PLAN

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## Aviation

The nearest operational general aviation airport is Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 30 miles south of Allensworth and offers direct flights to several destinations.

Fresno Yosemite International Airport (FAT), approximately 70 miles northwest of Allensworth, is the principal passenger and airfreight airport in the central San Joaquin Valley. Visalia Municipal Airport is approximately 40 miles north.

## State Route 99

State Route (SR) 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South.

## State Route 43

State Route (SR) 43 is an arterial route between Kern County line to Kings County line.

## Burlington Northern-Santa Fe Railroad

Allensworth is bound on the east side by the Burlington Northern-Santa Fe Railroad line

## **PUBLIC SERVICES**

### Sheriff

The Tulare County Sheriff Department (TCSO) is responsible for patrolling the community of Allensworth. Police protection services are provided in Allensworth by the Tulare County Sheriff's Department sub-station, located at 161 North Pine Street, in Pixley, approximately 8 miles northeast of Allensworth. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

### Fire

Fire protection and emergency medical services are provided for Allensworth by the Tulare County Fire Department. The community is served by Tulare County Fire Department Station #9, located at 3939 Avenue 54, in Alpaugh. Engine 9 is assigned to this location.

Fifteen fire hydrants (**see Table 17**) are found within Allensworth. These fire hydrants are located within the County right-of-way. **Figure 8** displays Existing Fire Hydrants in Allensworth.

# ALLENSWORTH HAMLET PLAN

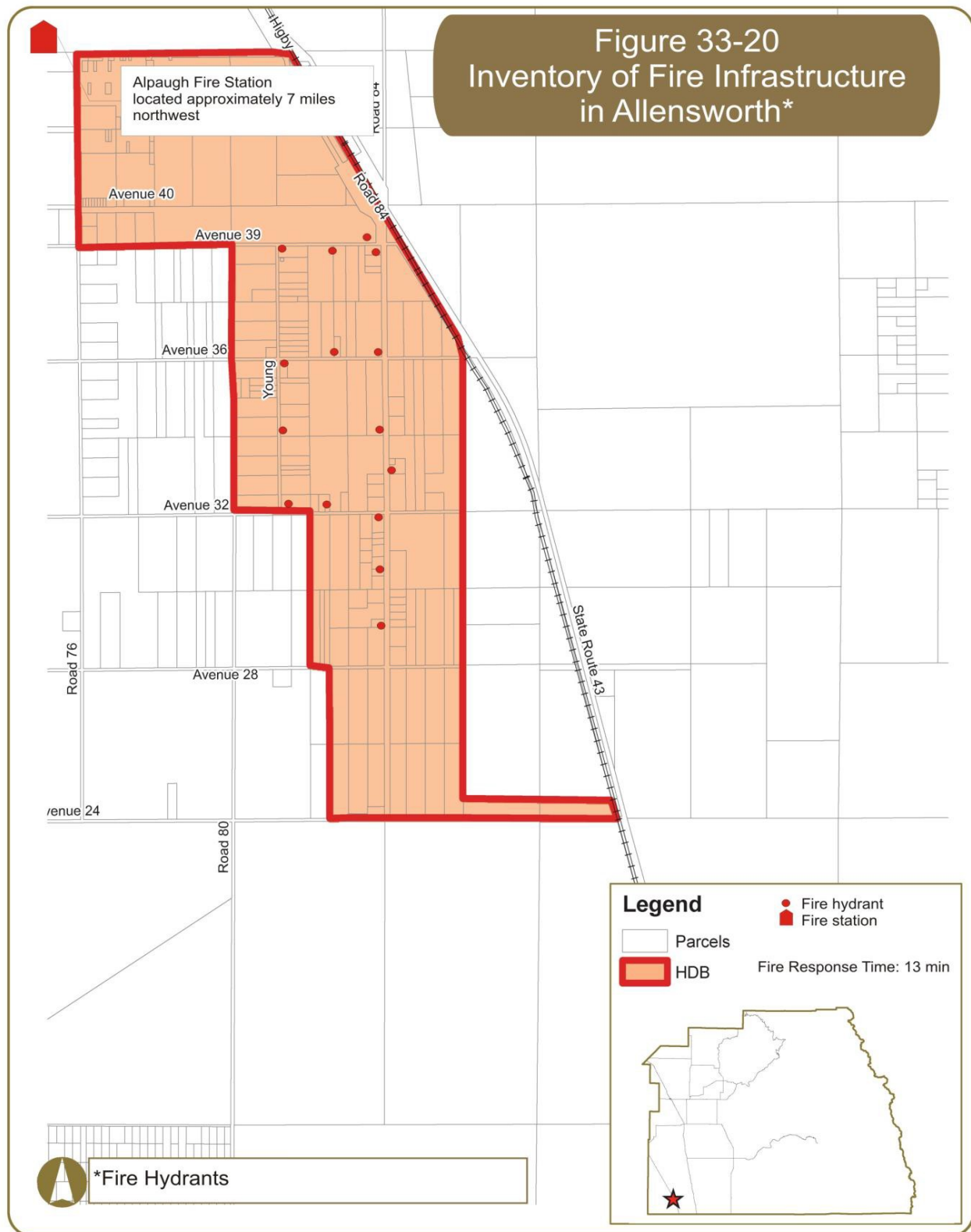
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**Table 17 - Existing Fire Hydrants**

Existing Fire Hydrants	
No.	Location
1	Avenue 39 and Young Road
2	Avenue 39 east of Young Road
3	Avenue 39 west of Road 84
4	Avenue 39 and Road 84
5	Avenue 36 and Young Road
6	Avenue 36 east of Young Road
7	Avenue 36 and Road 84
8	Young Road south of Avenue 36
9	Road 84 south of Avenue 36
10	Road 84 north of Avenue 32
11	Avenue 32 and Young Road
12	Avenue 32 east of Young Road
13	Avenue 32 and Road 84
14	Road 84 south of Avenue 32
15	Road 84 north of Avenue 28

# ALLENSWORTH HAMLET PLAN

Figure 8 - Inventory of Fire Infrastructure in Allensworth





# ALLENSWORTH HAMLET PLAN

## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Allensworth Hamlet Plan Area is within the Allensworth Elementary School District. The District provides K-8 grade education at its Allensworth School. High School students attend high school in Delano. Allensworth is within the Delano Joint Union High School District.

## Library

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>14</sup> The closest library is located in Alpaugh 5 miles away (see Table 18).

Table 18 - Library Location & Hours		
Branch	Address	Service Hours (2016)
Alpaugh	Alpaugh Branch 3816 Avenue 54 Alpaugh, CA 93201	Tuesday & Wednesday: 9:00 am - 1:00 pm 2:00 p.m. – 5:00 p.m.

Library hours current as of September 2017

## Parks

The nearest community park is located in Alpaugh, seven (7) miles away. Alpaugh Park, is a three (3) acre community recreation park is located on Road 38 at the northwest corner of Tule Road and Park Avenue, across from the Alpaugh Memorial Building, is owned and maintained by Tulare County. There are also a number of arbors that are available for rental. No entrance fee.

## Allensworth State Park

“The only State Park in Tulare County is Colonel Allensworth State Historic Park. The park contains a museum and a visitor center addressing the town’s history, as well as camping facilities. Allensworth is the only California town to be founded, financed and governed by African Americans. The small farming community was founded in 1908 by Colonel Allen Allensworth and a group of others dedicated to improving the economic and social status of African Americans.”<sup>15</sup>

“Colonel Allen Allensworth was born a slave. He was sold twice before he escaped. He joined the Union Army to fight against slavery during the Civil War. He became a minister and joined the army again as a chaplain. He rose to the high rank of Colonel.

In 1908 Colonel Allensworth and some friends bought some land along the railroad tracks where African American families could live together in a community and farm to make their living.

<sup>14</sup> General Plan Background Report, page 7-96

<sup>15</sup> General Plan Background Report, page 4-3

# ALLENSWORTH HAMLET PLAN

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By 1914 there was a post office, a school, a church, a general store, and even a county judge. In those days the water came up out of the ground in some places, and it was easy to dig artesian wells with a shovel. But as more people farmed, the water went deeper, and it became hard to farm, and even hard to live there. Most of the people had to move to other places.

In 1974 the California State Park system established Allensworth State Historical Park. Many of the old buildings, including the school, the church, the library, some stores, and the Allensworth home, have been restored and are open for tours. There is a campground and a visitor's center, where a Park Ranger will answer questions about this special place. The Park has annual events every year and is a traveling / camping destination in south west Tulare County.

The modern town of Allensworth is right next to the park. It no longer has a post office or a library of its own.”<sup>16</sup>

## THE ALLENSWORTH HAMLET PLAN

### CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

The 2014 Regional Transportation Plan and projections for Year 2040 traffic volumes do not exceed the LOS “C” threshold. Less than significant impacts will occur to SR 43 as a result of the Project primarily because increased traffic loads on SR 43, and its various entry and exit point, is more associated with growth in the Central Valley and the increased need for Californians to be able to travel north and south. Both the Highway Capacity Manual (HCM) and the Tulare County General Plan (TCGP) LOS remain at B or C through Year 2040 projections, while the build-out time frame of the Community Plan is Year 2030.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>17</sup>

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<sup>16</sup> Tulare County History; <http://tularecountyhistory.org/parks/allensworth-state-park/>

<sup>17</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

# ALLENSWORTH HAMLET PLAN

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## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>18</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation

Allensworth is a small agricultural community located adjacent to SR 43. The major rural collector road is Avenue 24. There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities. (See Sidewalk sections above for recent funding awards and Attachment 4 for the Complete Streets Program for Allensworth). It is noted that Avenue 32, east of Road 84 needs completion, and that Road 24 should be placed as a higher priority on Public Works maintenance schedule.

## Bicycle Facilities

In Allensworth, Class I/II/III facilities are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Future bicycle facilities have been identified through the Bicycle transportation Plan (TCAG – 2010). The Communities of Alpaugh and Allensworth are proposing a Class 1 pedestrian path (See ATIT plans above).

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<sup>18</sup> Tulare County General Plan, Background Report, page 5-7.



# ALLENSWORTH HAMLET PLAN

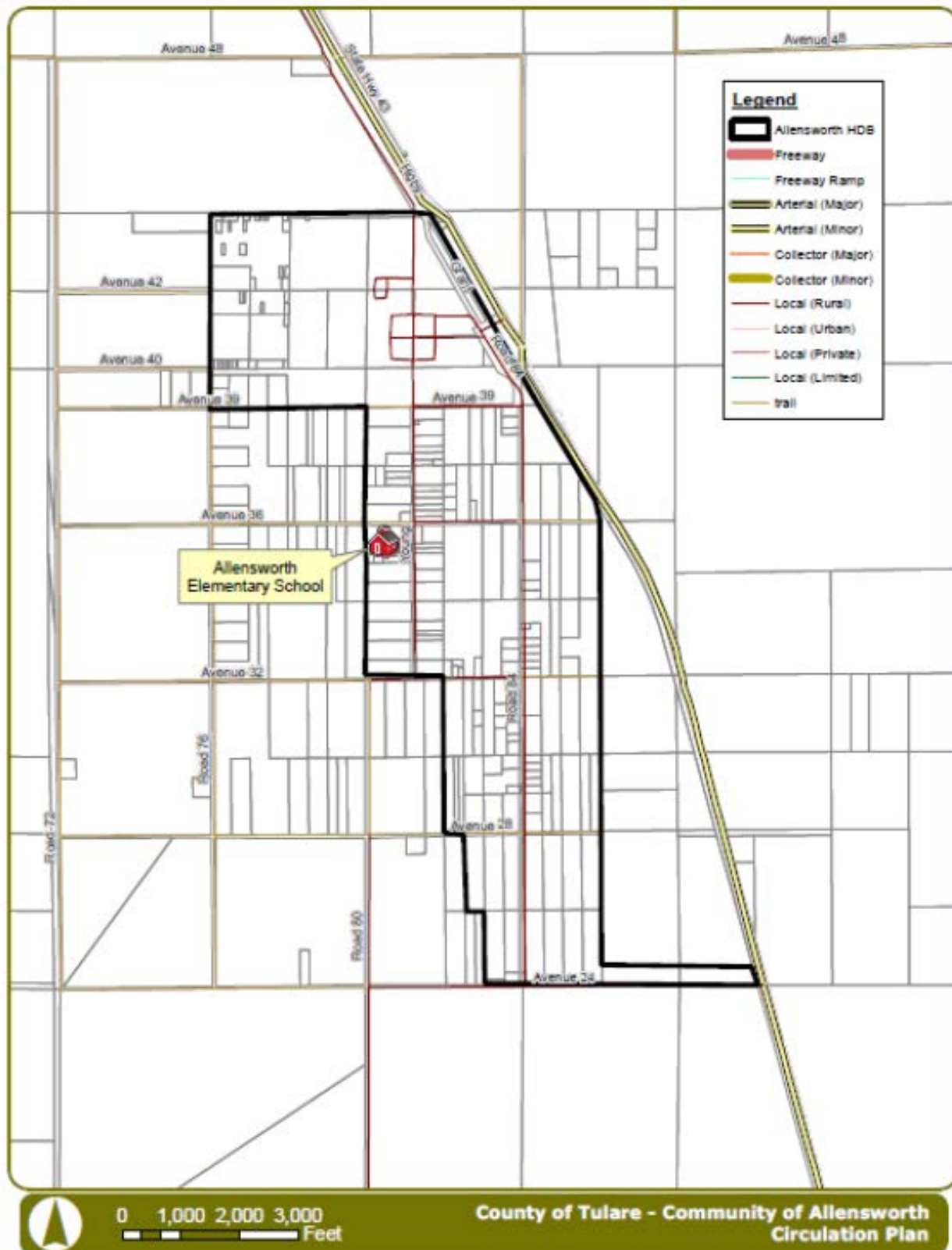
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## Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the Community. Currently, limited continuous sidewalks are provided along major routes in the Community. There are no sidewalks in Allensworth. In addition to connecting available pedestrian resources, the Community has prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks should also be considered in areas where high pedestrian demand occurs (such as to and around the schools). As stated earlier funding has been awarded for the ATP – Safe Routes to Schools Program.

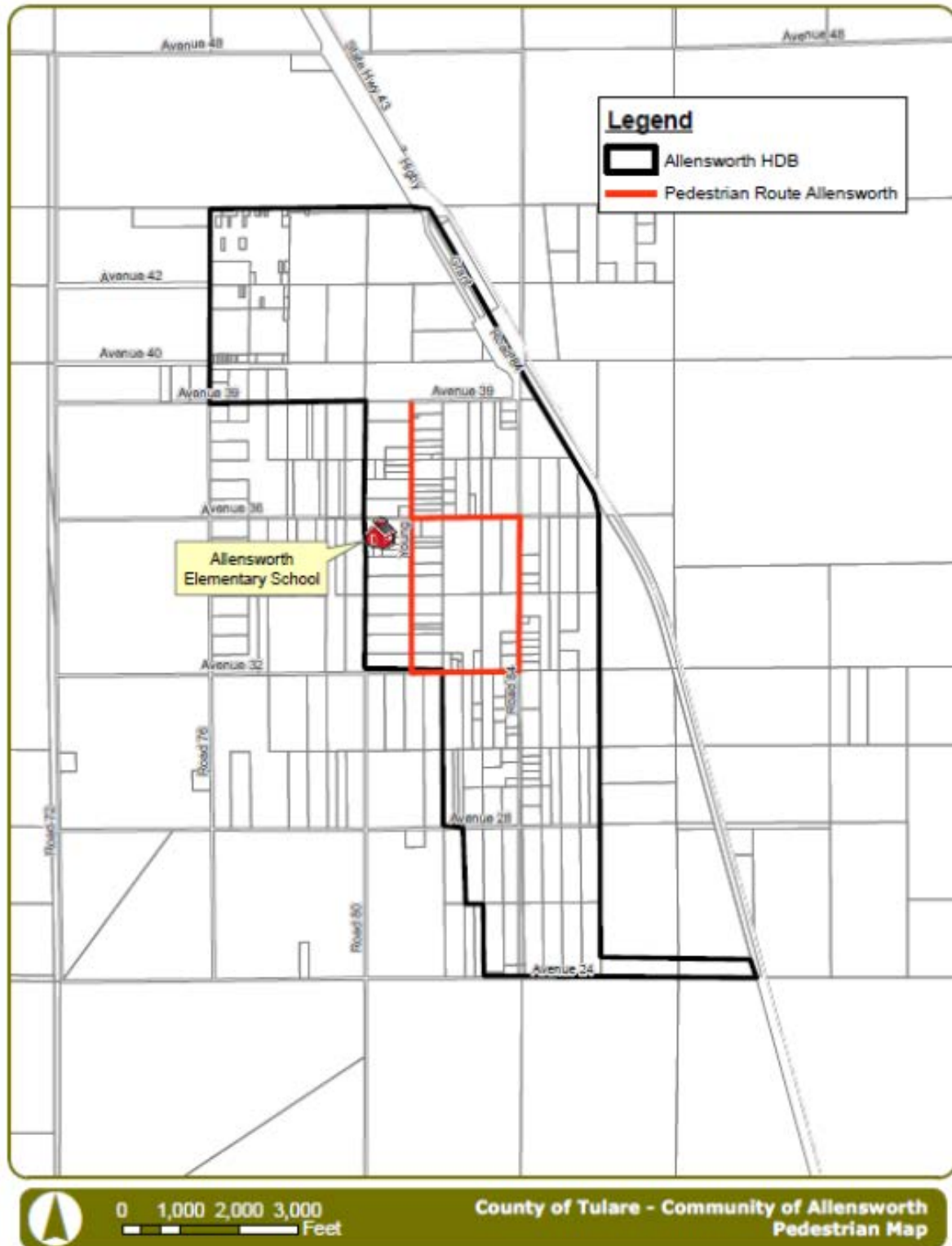
# ALLENSWORTH HAMLET PLAN

Figure 9 - Allensworth Circulation Plan



# ALLENSWORTH HAMLET PLAN

Figure 10 - Pedestrian Routes for Allensworth



# ALLENSWORTH HAMLET PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Allensworth Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Allensworth to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Allensworth Hamlet Plan, and input received from Allensworth citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.

2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
4. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
5. The County shall work with the Schools to provide safe routes to school.
6. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Allensworth Community Plan.
7. The Allensworth Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.
8. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

# ALLENSWORTH HAMLET PLAN

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Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Encourage the County to adopt incentives for rehabilitating substandard housing that such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Allensworth.

**Goal II: Provide quality housing and improved quality of life within the community**

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Allensworth.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of a mobilehome park in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
4. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## Economic Base

**GOAL I: Develop a strong and diversified economy.**

# ALLENSWORTH HAMLET PLAN

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Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access to Highway 43 at Avenue 24 for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

Policies:

1. Encourage Allensworth CSD to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## *Environmental Quality and Public Safety*

**GOAL I: Preserve and enhance the quality of life for present and future generation of Allensworth citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Protect Agricultural Lands:

Land within the respective Allensworth, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate.

When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

# ALLENSWORTH HAMLET PLAN

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The following criteria may be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure are reasonably sufficient to serve urban development either are available or can be made available; and
- At least one of the property boundaries are contiguous on at least one side to existing urban development.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Allensworth.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Allensworth.



# ALLENSWORTH HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 1,051.4 acres of Allensworth Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute of approximately 57 percent of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 565 by 1,051.4 urbanized acres (area within the HDB), which equals 1.8 persons per urbanized acre (**see Tables 19 and 20**) showing population projections). Projecting the population at a 1.3% growth rate adds 118 persons by year 2030.

### Population Growth Forecast

Table 19 - Allensworth Population Projections			
Growth Rate	2015	2020	2030
0.013	565	603	683

Table 20 - Population Projections		
Year	Population	Growth (%)
2015	565	0.013
2016	572	0.013
2017	580	0.013
2018	587	0.013
2019	595	0.013
2020	603	0.013
2021	611	0.013
2022	627	0.013
2023	635	0.013
2024	635	0.013
2025	643	0.013
2026	651	0.013
2027	660	0.013
2028	668	0.013
2029	677	0.013
2030	683	0.013

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The Year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>19</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2013 the community had 132 dwelling units (including vacant dwellings) with a population of 565. At an annual growth rate of 1.3%, the projected housing units are 141 and 160 in years 2020 and 2030, respectively, and projected population is 603 and 683 in Years 2020 and 2030, respectively.

<sup>19</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).



# ALLENSWORTH HAMLET PLAN

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, water service levels need to be expanded, for which grant funding is will be required.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require a discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced. (See Attachment A-1 for a list of "Use Permits").

### Education

Tulare County has five satellite campuses for four-year universities: California State University-Fresno, Cal State Bakersfield, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix.

Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Based on the 2011-2015 American Community Survey (see Table 21), the educational barrier in

Table 21 - Education Attainment			
Educational Attainment	California	Tulare County	Allensworth
Population 25 years and over	25,257,858	260,964	220
Less than 9th grade	10.0%	20.8%	43.2%
9th to 12th grade, no diploma	8.2%	10.9%	20.9%
High school graduate (& equivalency)	20.7%	25.2%	18.6%
Some college, no degree	21.8%	22.3%	16.4%
Associate's degree	7.8%	7.0%	0.9%
Bachelor's degree	19.8%	9.5%	0.0%
Graduate or professional degree	11.6%	4.3%	0.0%

2010-2014 American Community Survey 5-Year Estimates

Allensworth begins in grade school. Of the adults age 25 and older, 43.2% had an educational level of less than 9<sup>th</sup> grade. A lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

### Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are in Earlimart. The Tulare County Family Health Care Network began providing a Mobil Health Unit to Allensworth once a month with comprehensive healthcare services.

# ALLENSWORTH HAMLET PLAN

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 22**) and Zoning Districts (**see Table 23**) have been updated.

Table 22 - Proposed Land Use	
Proposed Land Use	Acres
Mixed Use	1012.4
Right-of Ways	38.9
Allensworth HDB	1051.4

Table 23 - Proposed Zoning Districts	
Proposed Zones Districts	Acres
C-2-MU	39.5
M-1	0.4
O	308.8
P-O	5.3
R-A-M-12.5	658.5
Right-of Ways	38.9
Allensworth HDB	1,051.4

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations contained in the Allensworth Hamlet Plan. The following components comprise the Allensworth Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Allensworth Hamlet Plan, there are several proposed changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Allensworth.

# ALLENSWORTH HAMLET PLAN

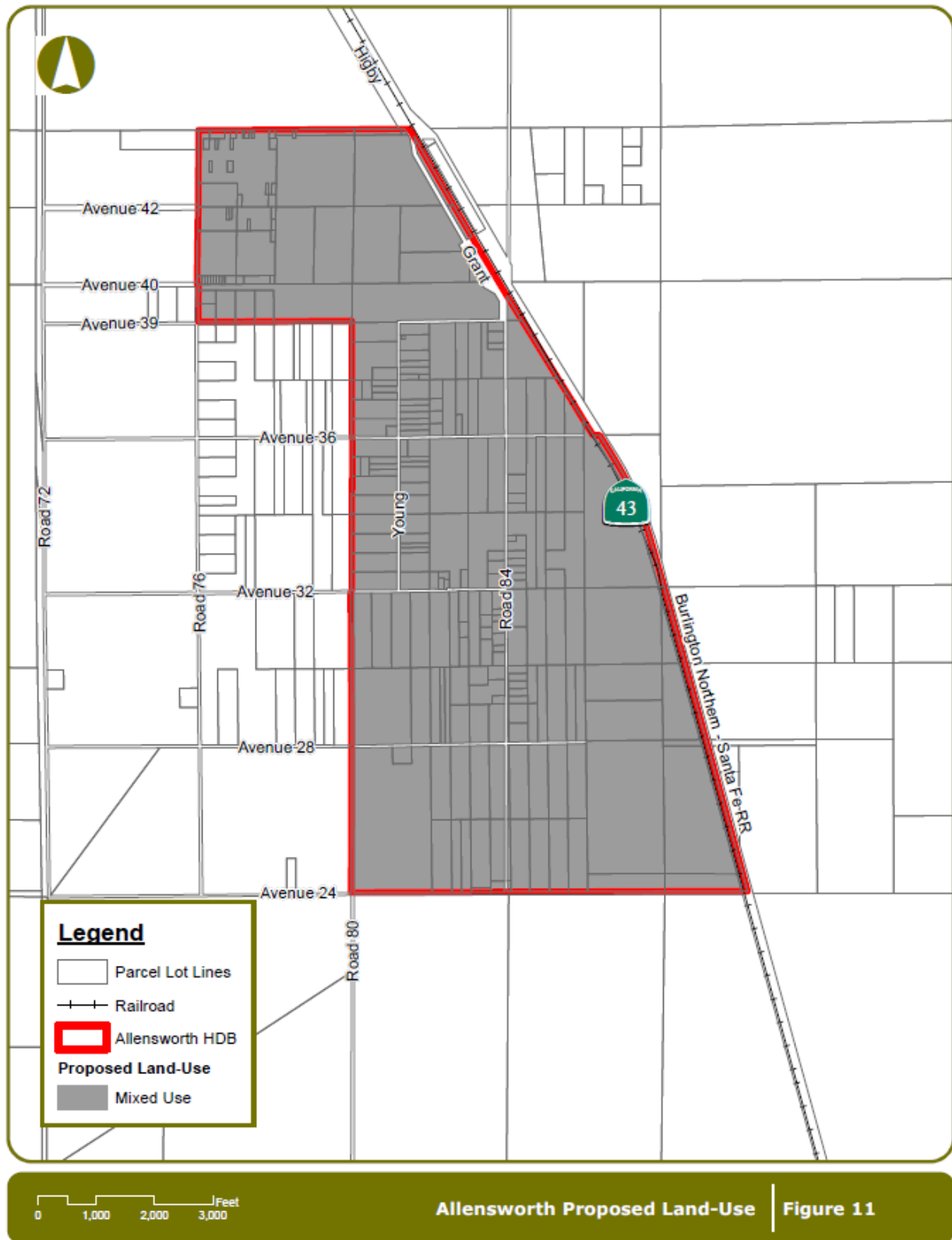
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## Map Updates

The current Zoning Map for Allensworth (**see Figure 12**) will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 11**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 13**) Allensworth Proposed Zoning. **Figure 14** shows Allensworth proposed Hamlet Development Boundary

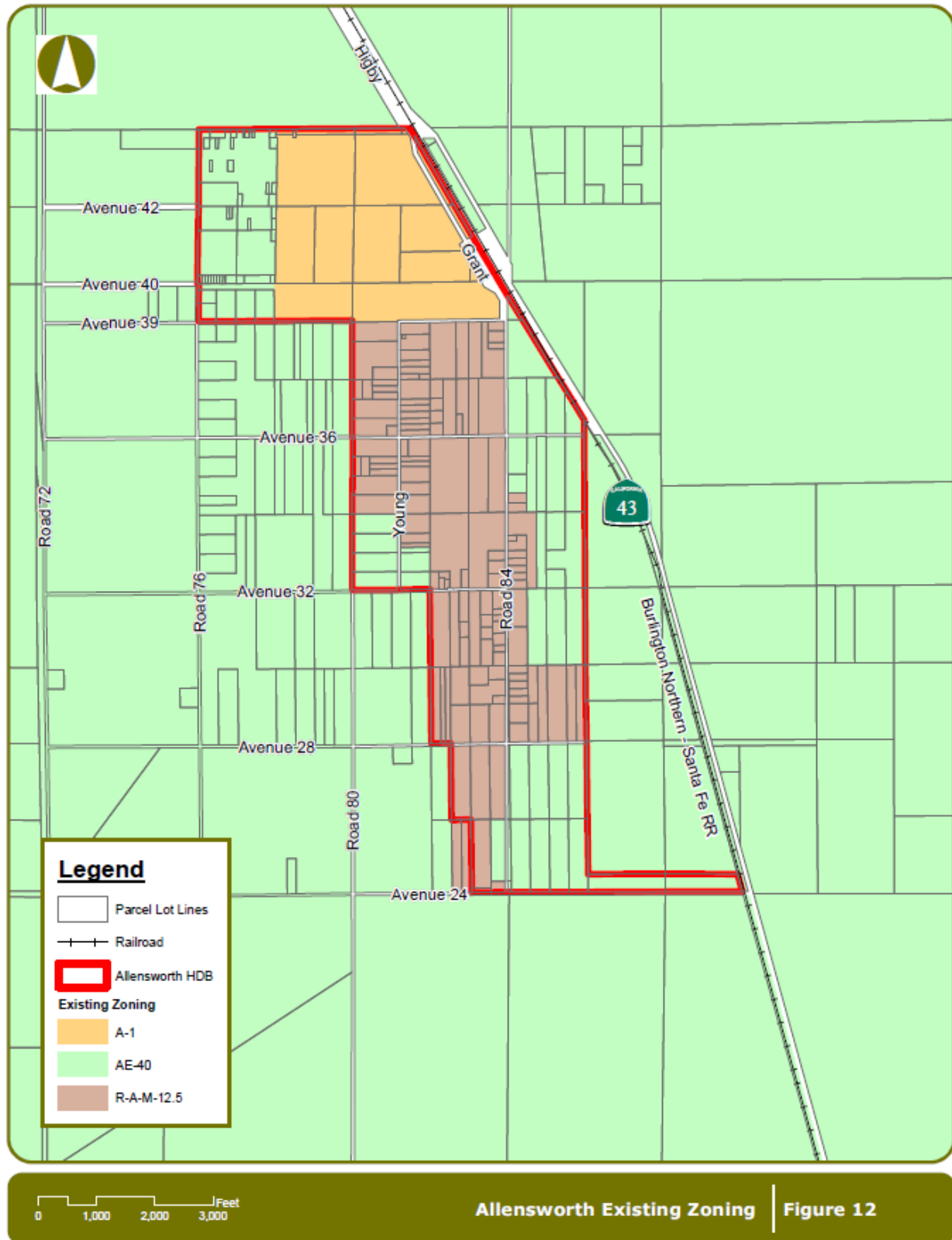
# ALLENSWORTH HAMLET PLAN

Figure 11 - Proposed Land Use Plan



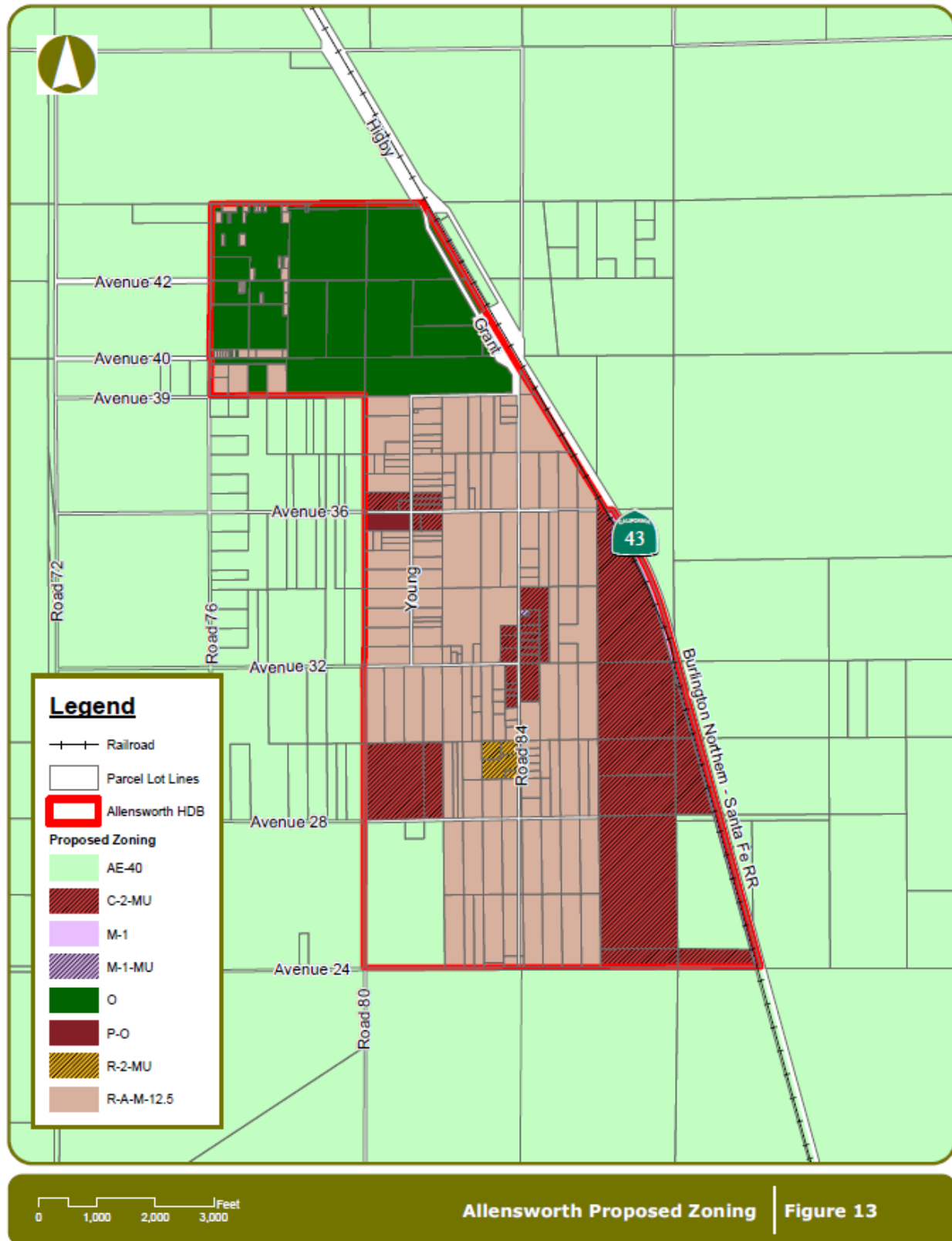
# ALLENSWORTH HAMLET PLAN

Figure 12 - Existing Zoning Districts



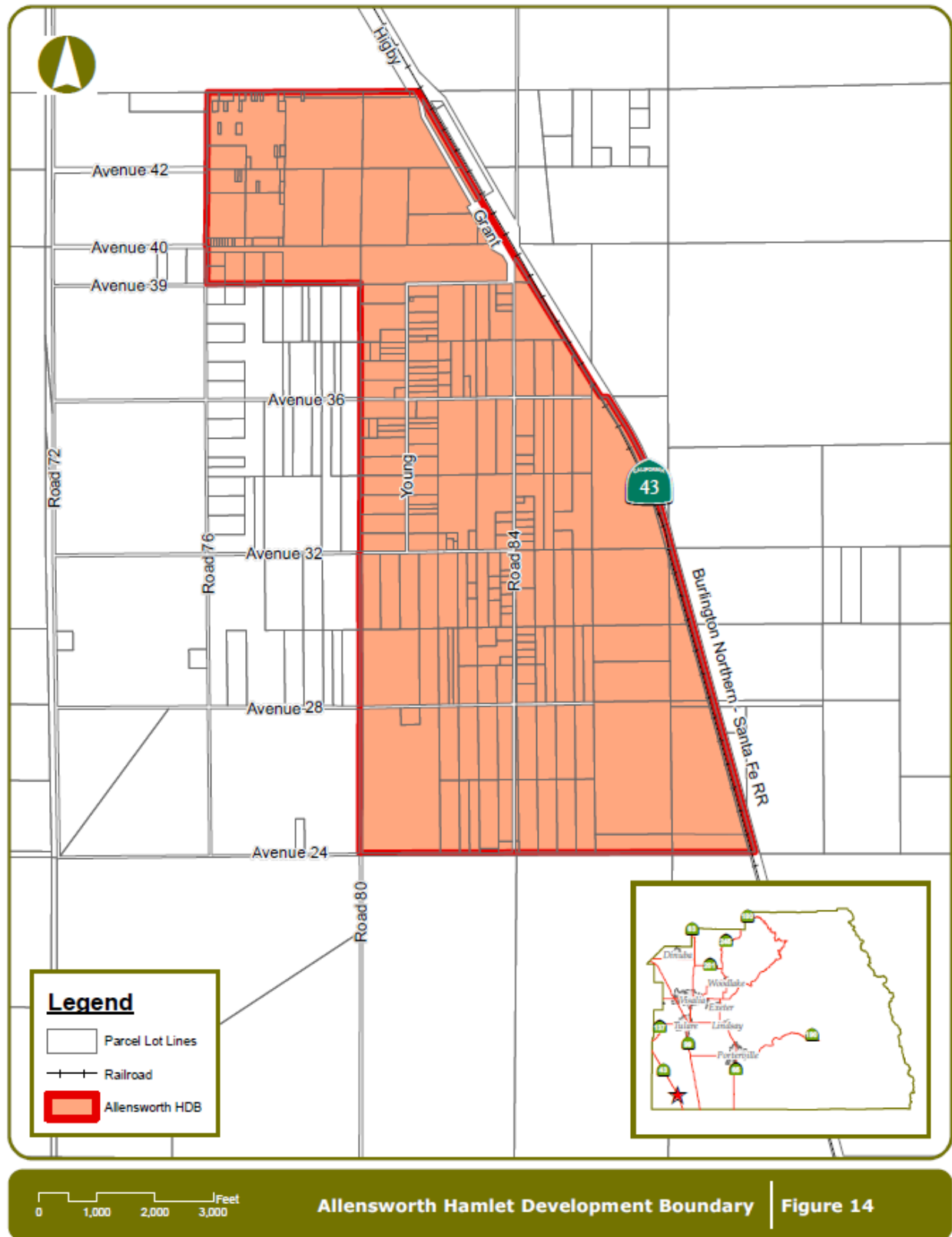
# ALLENSWORTH HAMLET PLAN

Figure 13 - Proposed Zoning Districts



# ALLENSWORTH HAMLET PLAN

Figure 14 - Proposed Allensworth HDB



# ALLENSWORTH HAMLET PLAN

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## ATTACHMENTS

- A-1 – Use Permit Requirement Changes (Zone Change Text)
- A-2 – Mixed Use Overlay District (Zone Change Text)
- A-3 – Development Standards (Mixed Use Zoning Districts)
- A-4-Complete Streets (Complete Streets Program)



# ALLENSWORTH HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1

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Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1

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Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1

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Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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## A-2 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the community of Allensworth, unless otherwise provided in this Ordinance.

**PURPOSE**      **A.**      The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**      **B.**      This overlay zone only applies to the community of Allensworth.

**USE**      **C.**      No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Allensworth.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses:

All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Autowrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport



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gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and

# ALLENSWORTH HAMLET PLAN

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zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Allensworth. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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## A-3 Development Standards (Mixed Use Zoning District)

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To promote Economic Development within the Allensworth Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Allensworth. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Allensworth Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Allensworth. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# ALLENSWORTH HAMLET PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

# ALLENSWORTH HAMLET PLAN

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery

(Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

# ALLENSWORTH HAMLET PLAN

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LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.



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WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.



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SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

# ALLENSWORTH HAMLET PLAN

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# ALLENSWORTH HAMLET PLAN

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## Attachment 4: Complete Streets



# Tulare County Complete Streets Allensworth

## Final



Prepared by:

**Tulare County Resource  
Management Agency**

Paid for by a grant from:



## **Tulare County Complete Streets – Allensworth**

**Prepared for:**

**Tulare County RMA**

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**December 2016**



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## **APPENDIX**

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Appendix B – Circulation Plan
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# Introduction

## Complete Streets Vision

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.”

The Tulare County Resource Management Agency (RMA) is committed to fully integrating modal options in its General Plan and various Community Plans within Tulare County. This includes supporting projects that enhance walking and bicycling infrastructure. Additionally, RMA will improve access to public transportation facilities and services. This includes supporting urban development patterns and Americans with Disabilities Act (ADA) infrastructure that allow for greater accessibility to transit stops and stations. Finally, RMA continues to improve safety for all users and encourages street connectivity to create a comprehensive, integrated and connected circulation network. This is particularly important for those who rely on transportation infrastructure to be physically active and for students who walk or bike to school.


Steady population growth in Tulare County has directly impacted transportation needs. In the past, many of Tulare County’s federal, state, and local funding sources were used to develop new or improved traffic signals, interchanges, provide more travel lanes and to maintain existing roadway facilities. Historically, these funding sources have run well short of what is needed. The typical roadway transportation project that adds capacity and infrastructure is insufficient given these conditions. The RMA must adhere to its vision, which is to “provide a safe, convenient and effective County transportation system that enhances mobility and air quality for residents and visitors.”

Recent RMA and RMA-supported projects have already fulfilled some of these desires. There are already expanded bus transit routes in the County and more are being constructed for implementation in the near future. New transit centers are being placed throughout the County and efforts are underway to add more bicycle lanes and routes. Recent planning studies are looking to improve roadway safety, pedestrian safety, and access management between roadways and building developments. These efforts are consistent with greenhouse gas (GHG) emissions reductions efforts to reduce vehicle miles travelled (VMT) set forth under SB 375.

Promoting Complete Streets projects can offer Tulare County the ability to reduce traffic congestion, improve air quality, and increase the quality of life of residents by providing safe, convenient, and comfortable routes for walking, bicycling, and public transportation. Integration of Complete Streets into Tulare County’s existing policies allows the potential to prevent chronic diseases, reduce motor vehicle related injury and deaths, improve environmental health, stimulate economic development, and ensure access of transportation options for all people in Tulare County.

## Complete Streets Definition

Complete Streets are roadways designed to safely and comfortably accommodate all users, regardless of age, ability or mode of transportation. Users include motorists, cyclists, pedestrians and all vehicle types, including public transportation, emergency responders, and



freight and delivery trucks among others. In addition to providing safety and access for all users, Complete Street design treatments take into account accommodations for disabled persons as required by the ADA. Design considerations for connectivity and access management are also taken into account for non-motorized users of the facility.

Implementation of Complete Street design treatments will be based on whether it connects the networks for all modes, whether it improves the functionality for all users, and whether it is appropriate given the surrounding context of the community. The final elements of a Complete Street roadway will be largely based on these factors. At a minimum, a Complete Street roadway includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

## Complete Streets Attributes

While every street cannot be designed to serve all users equally, there are opportunities to enhance service for all users while maintaining its principal transportation function. Complete Streets incorporate community values and support adjacent land uses while ensuring safety and mobility. Proper applications of Complete Streets concepts support sustainable growth and preservation of scenic, aesthetic and historic resources.


## Report Outcomes

As a part of the Circulation Element for the Community Plan Update, this Complete Streets Report (Implementation Work Plan) and the following Implementation and Policies Section achieved the following outcomes:

- (i) *Addressed* congestion, climate change and oil dependence by shifting to lower-carbon modes;
- (ii) *Improved* safety by addressing shoulders, sidewalks, better bus placement, traffic speed reduction, treatments for travelers with disabilities;
- (iii) *Created* “livable communities” by encouraging walking and bicycling for health, and by providing a safe walking and bicycling environment as an essential part of improving transportation movement and safety within the roadways studied.

These outcomes were achieved by the following:

- (a) *Included all users* namely, pedestrians, bicyclists, transit vehicles and users, and motorists. In drafting this report, all users were invited to comment on how the County could better serve the community. The implementation of complete streets directly shifts the emphasis to lower-carbon (using) modes of transportation. The shift from the gas using automobile to pedestrian and bicycle transport is achieved through the creation of sidewalks, improving sidewalks, and including bike lanes and/or bike routes for a wider range of people to use. The shift to transit is included in improving policies, programs and facilities in the operations of the County’s transit systems.
- (b) *Created a comprehensive integrated and connected network* that supports “livable communities” that promote a safe interwoven fabric are provide for by the Policies



Section using the transportation goals in the 2030 General Plan Circulation Element and by further defining complete streets network (see Appendix C).


- (c) *Emphasized flexibility* recognizing that all streets with these communities are different, and thus, balancing user needs. No one standard was applied to all streets and the street designs were adjusted to existing conditions, differing jurisdictions and the desires of the community.
- (d) *Considered both new and retrofit projects*, including design, planning, maintenance, and operation, for the entire right-of-way within these communities. In addition to the various sections discussed below Appendix A – D include plans that show the plans, designs, and existing and proposed maintenance plans and operations of the Complete Streets Plan.
- (e) *Used the latest and best design standards*. By using newer design standards as represented in the preliminary design plans verses the County’s Roadway Standards the County is able to provide wider sidewalks and include such amenities as traffic calming measures (bulbouts).
- (f) *Conducted extensive public outreach* to ascertain the solutions that best fit within the context of these communities. The community was consulted on January 12, 2016, and provided final feedback on the preliminary designs.

## Conclusions and Future Funding Opportunities

The intended effect of identifying the outcomes and reaching the conclusions in this report is that future funding opportunities will be enhanced because the Community will be supported by fully updated Community Plans. The conclusion to the report includes the Circulation Element of the Community Plan including the policies, and plans. The other conclusion to the report includes preliminary design drawings.

Specifically, the funding sources that are found in the Funding Section will be pursued actively by Tulare County to complete the work identified in the studies include, but are not limited to, the following:

- **Active Transportation Program Funding**
- **Highway Safety Improvement Funds**
- **Federal Transportation Activity Program (TAP) Funds**
- **Federal Transit Funds**
- **Federal Communities Putting Prevention to Work Grant**
- **Federal Highway Administration Pedestrian Safety and Design**
- **Strategic Growth Council**

- 
- **Walkable and Livable Communities Institute**
  - **California's Local Public Health and Built Environment Program**
  - **State Cap and Trade Funding**

## **Complete Street Policies**

### **Complete Street Goals**

The purpose of the RMA Complete Streets Policy is to create a comprehensive and uniform Complete Streets vision and policy for Tulare County. This will allow the implementing entities to incorporate Complete Streets guidelines and standards into both development and redevelopment actions. The County's goals are:

- Tulare County's transportation network will be supported through a variety of feasible transportation choices, which allows for sustainable growth.
- The livability of neighborhoods and commercial centers located along the County's transportation corridors will be enhanced by a safe and inviting pedestrian environment.
- The design of multimodal roadway facilities will not compromise the needs of larger vehicles such as transit vehicles, fire trucks and freight delivery trucks.
- Inclusion of Complete Streets design elements will allow for design flexibility on different street functions and neighborhood contexts.
- Inclusion of Complete Streets design elements will improve the integration of land use and transportation, while encouraging economic revitalization through infrastructure improvements.

### **Complete Streets Objectives**

- To create an integrated and connected transportation network that supports transportation choices and sustainable growth.
- To ensure that all transportation modes are accommodated to the extent possible in all public roadway facilities in the County.
- To develop and use the latest design standards and guidelines in the design of Complete Streets.
- To provide flexibility in the implementation of this policy so that streets chosen for implementation of Complete Streets elements can be developed to fit within the context of their principal purpose and surroundings without compromising the safety of users and needs of larger vehicles.



## Complete Streets Policies

### Tulare County General Plan Policies

The Tulare County General Plan Update (2030) in complying with AB 1358 calls for 4 Complete Streets related principles including:

*Principle 1: County-wide Collaboration*

Support countywide transportation plans that provide choices in travel modes.

*Principle 2: Connectivity*

Emphasize connectivity among cities, communities, and hamlets to ensure County residents have access to jobs and services.

*Principle 3: Community Circulation*

Anticipate and provide transit, traffic, and roadway connections that support the interconnectivity of all communities.

*Principle 4: Pedestrian and Bicycle Facilities*


Plan for the development and expansion of pedestrian paths and bicycle facilities that provide residents with alternative modes of travel.

These principles are expressed mainly in following policies including:

- TC-1.6 Intermodal Connectivity
- TC-1.7 Intermodal Freight Villages
- TC-5.1 Bicycle/Pedestrian Trail System
- TC-5.2 Non-motorized Modes in Planning and Development

### Complete Street Policy Design Criteria

1. Tulare County promotes the incorporation of Complete Streets concepts and design standards in all appropriate new and retrofit County public streets (except State highways and freeways).
2. Tulare County will seek every opportunity to provide funding for the planning, design, and implementation of Complete Streets.
3. New Class I Multi-Use Paths should be a minimum of eight (8) feet wide.
4. New Class II Bike Lanes should be a minimum of five (5) feet wide.
5. New sidewalks should be a minimum of five (5) feet wide.
6. Bulb-outs should be considered in areas of higher speed (35 mph or greater) where sufficient turning radii for trucks is available or as determined by the County Engineer.

- 
7. As determined by the County Engineer, installation of posted speed limit vehicle activated traffic calming signs (VATCS) are encouraged in instances of high speed to promote safety.
  8. Transit shelters and benches are encouraged at all County transit stops if FTA grants are available.
  9. Street lighting and cross walk are encouraged to promote safety if considered feasible by the County Engineer.
  10. Design policies should be consistent with the Tulare County Improvement Standards; other references include existing design guides, such as those issued by Caltrans, AASHTO and the ADA Accessibility Guidelines.
  11. Public streets excluded from this policy include those where:
    - Complete streets concepts are in conflict with existing laws, codes, or ordinances.
    - Compliance with this policy would conflict with goals or physical conditions related to the unique aspects of the location.
  12. Exceptions from Complete Street Policies:
    - Accommodation is not necessary where non-motorized use is prohibited, such as freeways.
    - Cost of accommodation is excessively disproportionate to the need or probable use as determined by the County Engineer.
    - A documented absence of current or future need.

## Complete Street Mobility Plan

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” Essentially, this bill requires a circulation element to plan for multimodal transportation accommodating all modes of transportation where appropriate, including walking, biking, car travel, and transit. The current functional classification system plan is shown in Appendix B (Circulation Plan).

The Complete Streets Act also requires circulation elements to consider the multiple users of the transportation system, including children, adults, seniors, and the disabled. For further clarity, AB 1358 tasks the Governor’s Office of Planning and Research to release guidelines for compliance with this legislation by January 1, 2014. Implementation of complete streets principles should be tailored to the individual jurisdiction and the individual roadway. The Complete Streets Program for Tulare County focuses on a network-based approach that has been tailored to the needs of the Community of Allensworth. Another principle that is being applied is under SB 743, requiring a change to evaluating traffic using Vehicle Miles Traveled (VMT) versus Level of Service under CEQA analysis, and under AB 32 in reducing Green House Gasses.

*Complete Streets:* According to the National Complete Streets Coalition, complete streets are a means by which, “... planners and engineers (can) build road networks that are safer, more livable, and welcoming to everyone.... Instituting a complete streets policy ensures that transportation planners and engineers consistently design and operate the entire roadway with





all users in mind – including bicyclists, public transportation vehicles and riders, and pedestrians of all ages and abilities.”

*Network-Based Complete Streets:* Combines individual travel mode networks into one multimodal transportation system, integrating infrastructure where appropriate, ultimately ensuring that all users can safely and efficiently access their destination.

*Vehicle Miles Traveled (VMT):* Vehicle miles traveled is the metric that identifies the total distance traveled in a car per driver. VMT drives roadway needs (the more people who drive, the more capacity and maintenance are needed on the roadway system). Under the Tulare County Climate Action Plan, in reducing VMT greenhouse gas emissions are reduced, and the County has an overall target of reducing 6% of its greenhouse gas emissions through a reduction in VMT.

Community Plans adopt these principles, which are combined into the following mission statement:

*The Community Complete Streets Network comprises four types of facilities—vehicular, pedestrian, bicycle, and public transit. This complete streets approach will enable residents to choose which travel mode best suits them. It also will ensure that streets are designed with the users in mind—accommodating for businesses, children, the elderly, bicyclists, and transit users.*

## Caltrans and Complete Streets

Under Caltrans District Order 64-R1, Caltrans requires that a Complete Streets Implementation Action Plan be developed and implemented for Caltrans owned and maintained Streets. Their Implementation Action plan provides a background by which the Tulare County Completes Street Plan will be implemented.

### *TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS)*

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Allensworth. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describe bicycle circulation patterns and Pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. The Cutler-Orosi Complete Streets Plan was prepared in 2014 and includes a Class 1 Facility along Avenue 416 through Allensworth. In addition, rail and goods movement is part of the Sustainable Communities Strategy, Regional Transportation Plan (RTP), in lieu of utilizing diesel powered freight trucks.

### *Tulare County Climate Action Plan (CAP)*

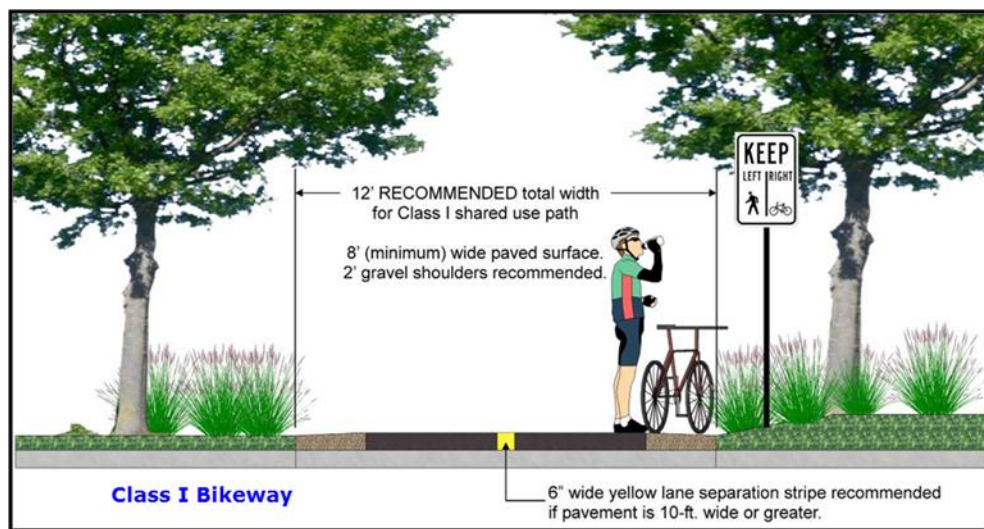
The Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% through a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets.

## Bicycle Facilities

Bicycle facilities consist of Class I, Class II, and Class III facilities as defined below. In Tulare County, this General Plan and the Bicycle Transportation Plan envision a system of bicycle lanes on roadways that will connect the activity centers of the communities to the residents. County has identified pedestrian corridors on the Community of Allensworth Bicycle, Bus and Pedestrian Plan (see Appendix C).

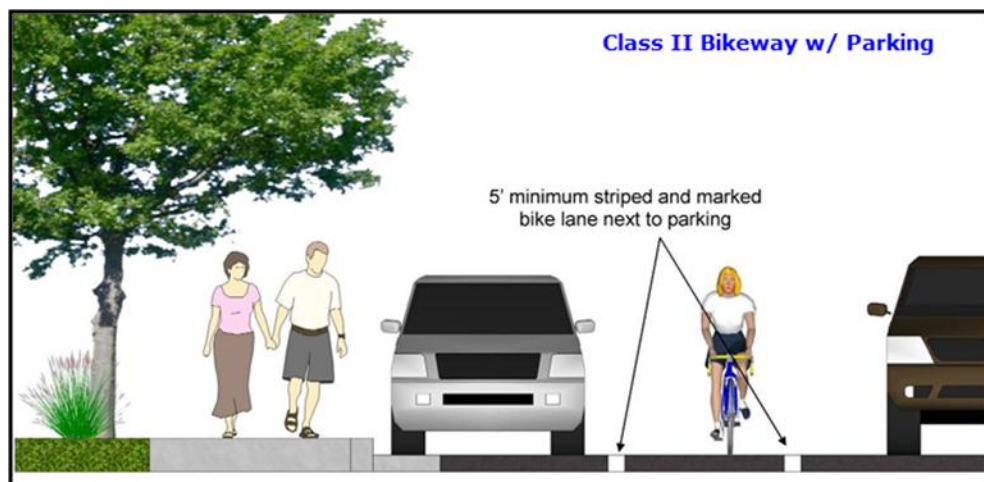
### *Class I*

Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County, Class I facilities will primarily be implemented through TCAG. Future bicycle facilities have also been identified through the *Bicycle Transportation Plan* (TCAG - 2010). The Communities of Alpaugh and Allensworth are proposing a Class 1 / pedestrian path from one community to the other.



### *Class II*

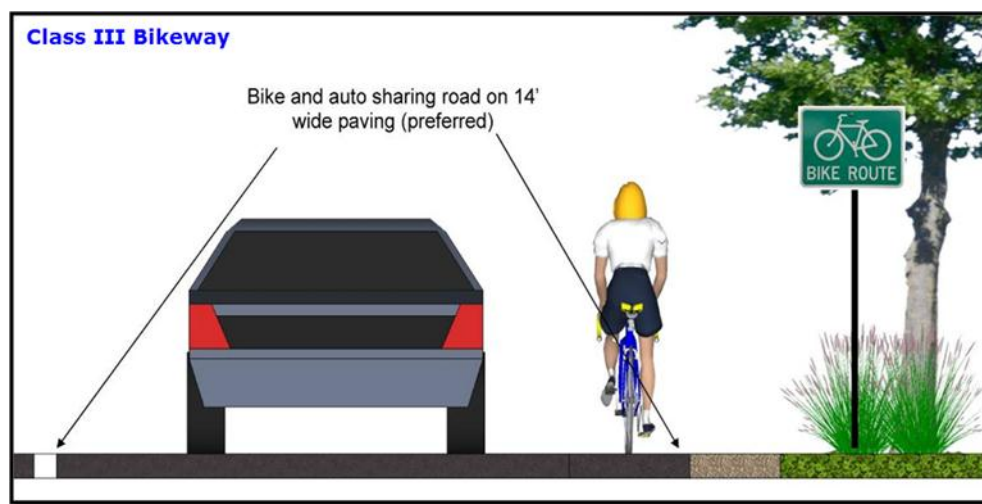
Bikeway that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. For the purposes of Complete Streets, the County is proposing no Class II bicycle facilities in Allensworth.





### *Class III*

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Although not signed on many roads in Allensworth, bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways.



## **Pedestrian Facilities**

### *Pedestrian Paths and Sidewalks*

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Enhanced pedestrian crossings and sidewalks is considered in areas where high pedestrian demand occurs (such as to and around schools).

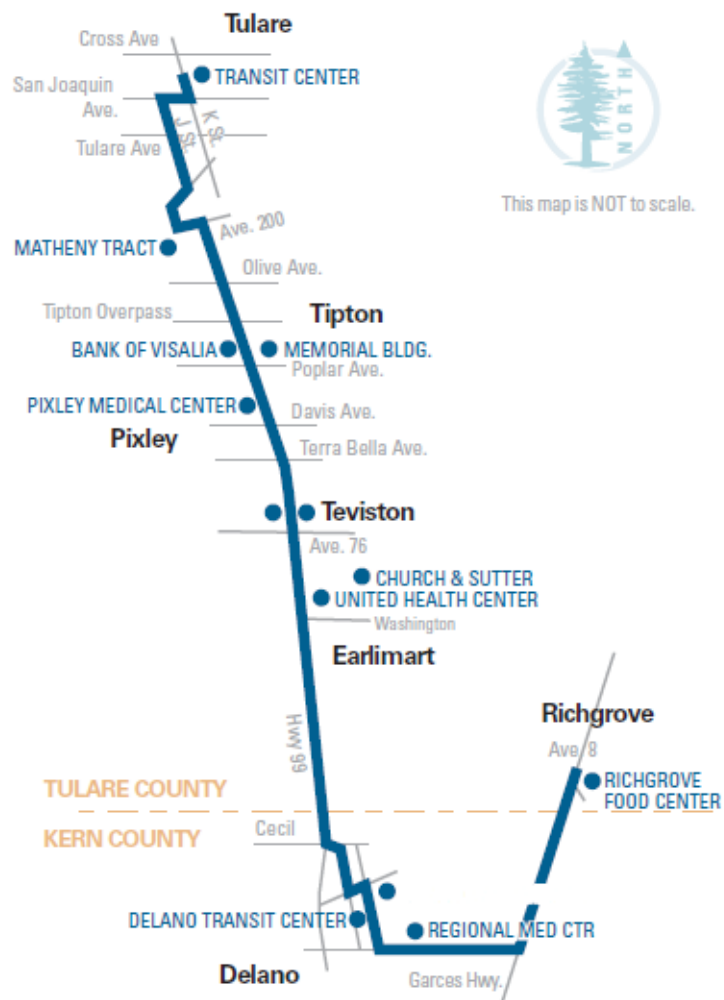
### *Multiuse Trails*

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. No multi use facilities are being considered for the Allensworth Community.

## **Transit Facilities**

Transit options give users the ability to get to a destination without relying on the automobile. This also provides other community benefits, including reduced vehicle miles traveled (VMT). Reducing VMT will help the County achieve their greenhouse gas reduction target,

Public transportation services and facilities in Tulare County consist of public bus service, paratransit service, and could also consider park-and-ride locations.



### Public Bus Service

Public bus service is provided by Tulare County Area Transit (TCaT) in rural areas. Existing transit routes and designated bus stops are shown in the following figures.

Additionally, Tulare County has provided guidance for including transit within facilities. These guidelines should be applied when considering new development to ensure appropriate connectivity and design features to support bus service.

### Paratransit Service

Paratransit is an alternative mode of passenger transportation that does not follow fixed routes or schedules. Typically, vans or minibuses are used to provide paratransit service. Paratransit services vary considerably on the degree of flexibility they provide their customers. The most flexible systems offer on-demand, call-up, door to door service from any origin to any destination in a service area.

### Pass Sales Outlets

Dinuba Transit Center	189 Merced St., Dinuba
Porterville Transit Center	35 W. Oak St., Porterville
Tulare County Government Plaza	5961 S. Mooney Blvd., Visalia
Tulare Transit Center	360 N. 'K' St., Tulare
Visalia Transit Center	Oak & Santa Fe, Visalia

commuters, a park-and-ride location would be best sited near the edges of the Community along Highway 99.

### Park-and-Ride Lots

Park-and-ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the community's population grows and given the large number of

## Cost Benefits Analysis, Implementation, and Funding Mechanisms

Caltrans lists the following benefits of Complete Streets in their implementation plan. They include:

- Increased Transportation Choices: Streets that provide travel choices can give people the option to avoid traffic congestion, and increase the overall capacity of the transportation network.
- Economic Revitalization: Complete streets can reduce transportation costs and travel time while increasing property values and job growth in communities.
- Improved Return on Infrastructure Investments: Integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project spares the expense of retrofits later.
- Quality of Place: Increased bicycling and walking are indicative of vibrant and livable communities.
- Improved Safety: Design and accommodation for bicyclists and pedestrians reduces the incidence of crashes.
- More Walking and Bicycling: Public health experts are encouraging walking and bicycling as a response to the obesity epidemic. Streets that provide room for bicycling and walking help children get physical activity and gain independence.

## Benefits of Complete Streets

The health benefits from walking and bicycle riding include increased overall health, and a reduction in air quality and greenhouse gas emissions. According to the Caltrans accepted, Victoria Transport Policy Institute, walking has a \$.25 per mile health benefit, while the cost of Greenhouse Gas (GHG) reductions is \$23 per ton. According to the Federal Highway Administration, sidewalks reduce incidences to pedestrians over 80%.<sup>1</sup> According to Caltrans, the average costs of highway incidents are stated below.

Cost of Highway Accident	Dollars Per Accident
--------------------------	----------------------

Fatal Accident	\$4,800,000
Injury Accident	\$67,400
Property Damage Only (PDO) Accident	\$10,200
Average Cost per Accident	\$52,500

Cost of an Event	Dollars Per Event
------------------	-------------------

Cost of a Fatality	\$4,400,000
--------------------	-------------

### Cost of an Injury

<sup>1</sup> [http://www.dot.ca.gov/hq/tpp/offices/eab/benefit\\_cost/LCBCA-economic\\_parameters.html](http://www.dot.ca.gov/hq/tpp/offices/eab/benefit_cost/LCBCA-economic_parameters.html)



Level A (Severe)	\$221,400
Level B (Moderate)	\$56,500
Level C (Minor)	\$26,900
Cost of Property Damage	\$2,500

Source: California Department of Transportation

## Community Specific Complete Street Implementation Measures

As part of a network-based approach, the County has identified (and will implement through pursuing further roadway studies and infrastructure design updates) a complete network for pedestrians. The County will also work to deliver infrastructure to support all modes of transportation. In addition to the General Plan Circulation Element Implementation Section, the key implementation measures include:

1. Evaluating Roadways as potential Bike/Pedestrian travel routes,
2. Completing pedestrian infrastructure, as appropriate,
3. Providing safe and accessible pedestrian facilities in high use areas,
4. Designing and building sidewalks for safer routes to school,
5. Designating roadways for bicycle routes that are aligned with the Tulare County comprehensive bicycle network,
6. Coordination with County Transit.
7. Submitting the following list of project and cost to TCAG and Caltrans for consideration under further grant funding opportunities.

## Measure R


Bike/Transit/Environmental Projects (14% of Measure R Funding)

On November 7, 2006, the voters of Tulare County Approved Measure R, imposing a ½ cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

## Active Transportation Program (ATP)

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation ([Senate Bill 99, Chapter 359](#) and [Assembly Bill 101, Chapter 354](#)). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation



Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation. The proposed projects have been included in the County's Active Transportation Plan (2015).

## **Citizen Feedback**

### **Public Outreach Efforts**

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues.

Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Complete Streets and Community Plan Outreach (2015) – is located in the Appendix .

### **Improvement Standards**

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Transportation related facilities for public use are built within existing right of way (R/W) owned by a public agency, e.g., county, city or state. Within this R/W is a standard cross section, which is a term that is used to define the configuration of existing or proposed roadways at right angles to the centerline (CL). Typical sections show the width, thickness and descriptions of the pavement section, as well as the geometrics of the graded roadbed, side improvements and side slopes.

In Tulare County, the two most common cross sections are shown for two or four lane roads, varying in width based upon the number of lanes, parking, sidewalks, shoulders, bike lanes, etc. Figure 1 shows the cross section for two lane roads and Figure 2 identifies a typical four lane cross section.

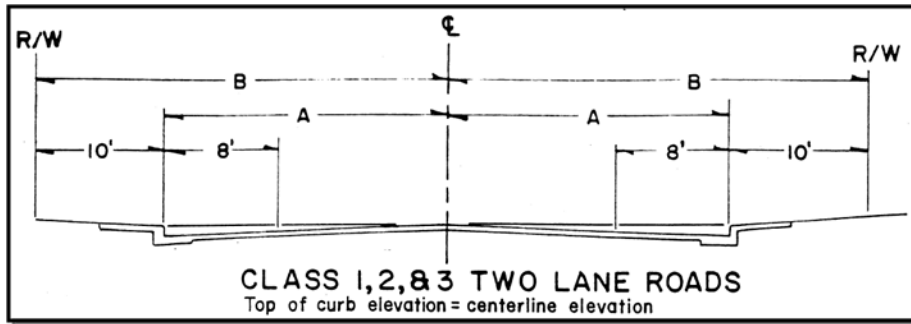


Figure 1 - Tulare County Class 1, 2 & 3 Two Lane Roads

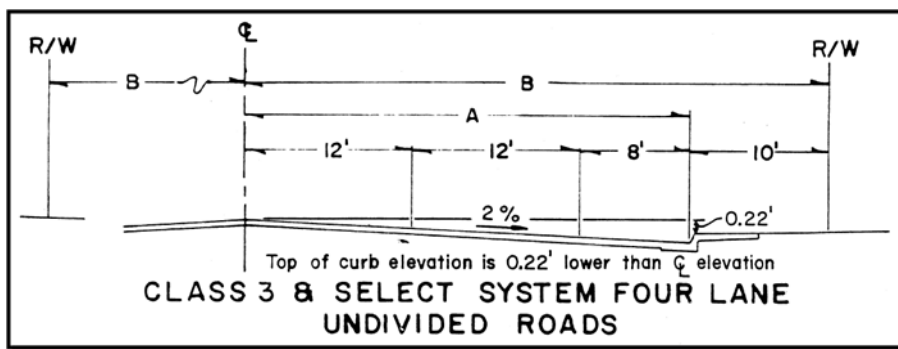


Figure 2 - Tulare County Class 3 Four Lane Road


## Tulare County Pavement Management System

### *Pavement Management*

Pavement management is the process of planning the maintenance and repair of a network of roadways or other paved facilities in order to optimize pavement conditions over the entire network. Pavement management incorporates life cycle costs into a more systematic approach to minor and major road maintenance and reconstruction projects. The needs of the entire network as well as budget projections are considered before projects are executed. Pavement management encompasses the many aspects and tasks needed to maintain a quality pavement inventory, and ensure that the overall condition of the road network can be sustained at desired levels.

### *Pavement Management System*

The Tulare County Pavement Management System (PMS) is a planning tool used to aid pavement management decisions. PMS software programs model future pavement deterioration due to traffic and weather, and recommend maintenance and repairs to the road's pavement based on the type and age of the pavement and various measures of existing pavement quality. Measurements can be made by persons on the ground, visually from a moving vehicle, or using automated sensors mounted to a vehicle. PMS software assists RMA staff to create composite pavement quality rankings based on pavement quality measures on



roads or road sections. Recommendations are usually biased towards preventive maintenance, rather than allowing a road to deteriorate until it needs more extensive reconstruction.

Typical tasks performed by Tulare County PMS include:

- Inventory pavement conditions, identifying good, fair and poor pavements;
- Assign importance ratings for road segments, based on traffic volumes, road functional class, and community demand;
- Schedule maintenance of good roads to keep them in good condition; and,
- Schedule repairs of poor and fair pavements as remaining available funding allows.

Research has shown that it is far less expensive to keep a road in good condition than it is to repair it once it has deteriorated. This is why pavement management systems place the priority on preventive maintenance of roads in good condition, rather than reconstructing roads in poor condition. In terms of lifetime cost and long term pavement conditions, this will result in better system performance.

The County is proposing a Road Maintenance Plan (see Appendix D) for the community of Allensworth that is a result of the PMS.

## Implementation

### Selection of Community Priorities

An effort is under way in Tulare County to implement Complete Streets Policies in the unincorporated communities within Tulare County's boundary. Just as the County updated its General Plan in 2012, many of the Community Plans are going through the update process. As a result of the Community Plan update process, several public meeting have been held in order to garner input from the local residents and business owners. Balancing the needs of what the people want while following local, state and federal policies and laws with a limited amount of available funding is the principal challenge in each community.

Transportation and related infrastructure costs tend to be exceedingly high may take years to implement. For purposes of this Study five transportation corridors were selected within the community (see Appendix A), and two roadway segments in the community were selected to be further evaluated for implementation of Complete Street standards. These roadway segments generally represent the highest volume roadways with a blend of residential and mixed land uses that also provide for regional access.

General themes that were voiced from residents in each community related to transportation included the need for:

- Sidewalks
- Better road conditions
- Safe walking and biking areas
- Street lights
- Pedestrian crossings



- Safe (lower) vehicle speeds
- Improved drainage
- Increased transit stops
- Improved connectivity (railroad crossings)

Given the information provided by the residents and business owners, conceptual layouts and designs based upon the citizens' concerns were presented to collect input. Based upon the community planning process, the following sections identify proposed projects for each community.

## Project Phasing

Generally, Tulare County RMA is proposing two types of projects coming from the community based upon the complexity of the project. The first types of projects are "shovel ready" that could be built immediately. They would be considered Phase 1 Projects and would have only minor needs for storm drain facilities, fence relocations, utility conflicts, etc. Phase 2 Projects are more inclusive and would be classified as medium to long range projects. These projects would need other infrastructure improvements such as storm water basins, major storm drain improvements, utilities to be undergrounded, etc.

Phase 1 Projects	Phase 2 Projects
Curb, gutter & sidewalk (storm drain water into existing system); pedestrian ramps; bulb outs (where appropriate)	Curb, gutter & sidewalk (new drainage system)
Street lights	Major storm drain facilities (new pipelines and storm water basins)
Bus shelters, benches, trash receptacles, etc.	Utility relocations (undergrounding)
Fence relocations	Major land acquisition
Street signage and striping	Railroad crossing improvements
Minor utility conflicts	
Minor land acquisition	





## Outreach: January 12, 2016

SHE conducted outreach on January 12, 2016, the preferred roadways selected were.

- Avenue 32
- Avenue 36
- Road 24
- Road 28
- Road 84

Based on the Community's desired roadway improvements, the Resource Management Agency's Public Works Division devised plans and project descriptions for the following roadways.



## Projects

### Complete Streets Project Plans

The plans and projects in the appendices are identified as part of the complete streets policy to identify corridors for various user types and to demonstrate examples of design policies. These plans and are the result of input obtained through the community outreach process, multiple Tulare County agencies and divisions and professional engineering consultants.

The five projects identified herein represent the priority improvements to the backbone of the complete streets network within the community of Allensworth. Parts of two of these projects Ave 36 and Young Road have been previously developed to a 30% design stage for the 2016 ATP Grants, and the remaining four projects have been preliminarily scoped and budgetary estimates have been prepared. These five projects were developed to provide the County and various funding agencies with a list of projects to move toward funding, design, and ultimately construction.





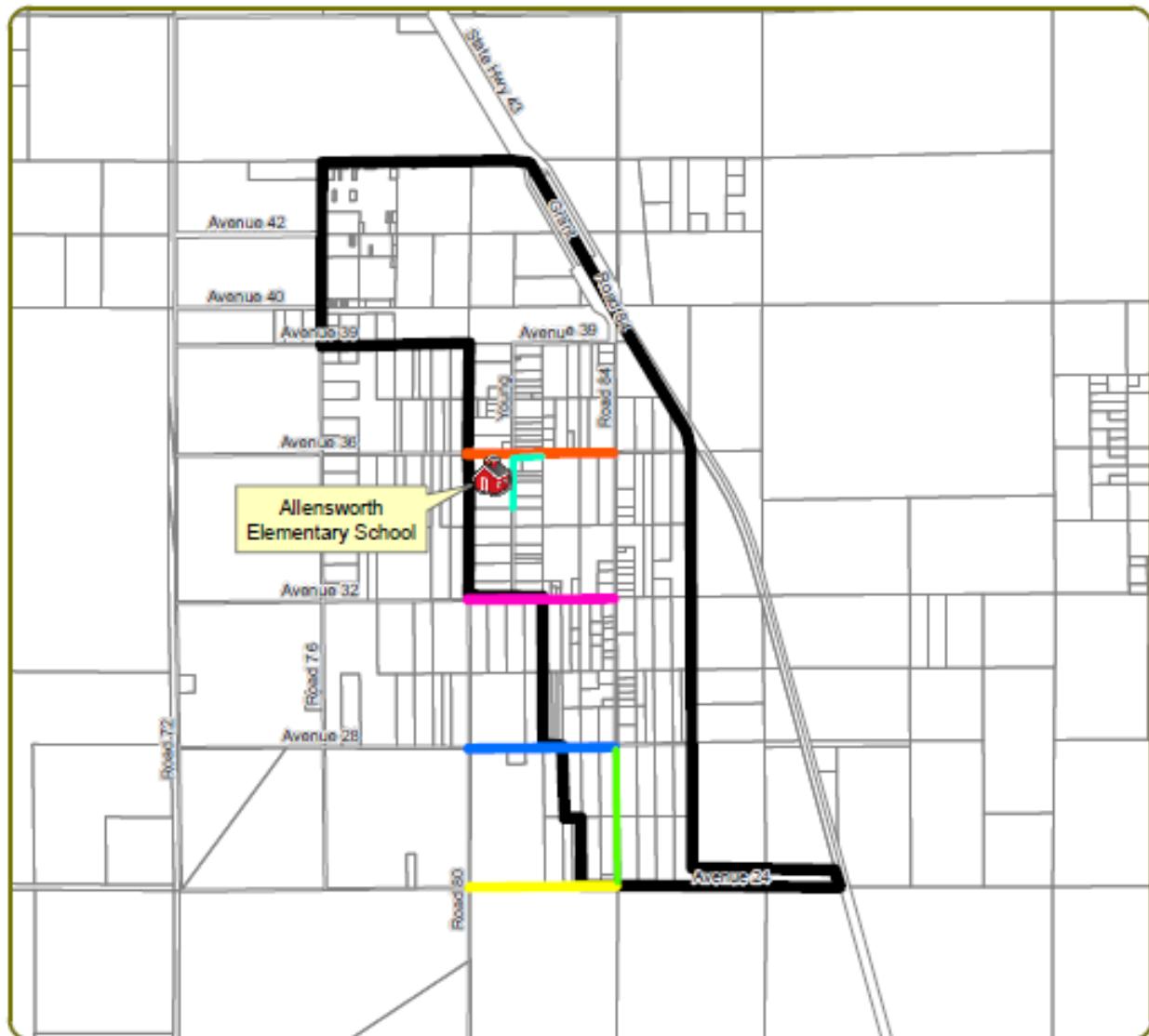
## **Complete Streets Funding Opportunities**

The following sections identify opinions of probable cost estimates for Complete Street transportation related improvements in Allensworth. As shown in the tables, the funding sources include local, state and federal programs. Typically, local matches are required for acquiring state and federal funds. Measure R, a Tulare County sales tax for transportation, is available for such matches.

### **Cost Estimates**

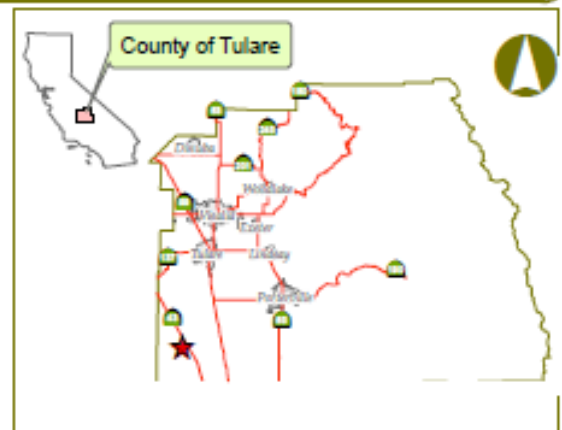
Detailed cost estimates are included in Appendix E.

## Appendix A – Proposed Complete Streets Project



### Legend

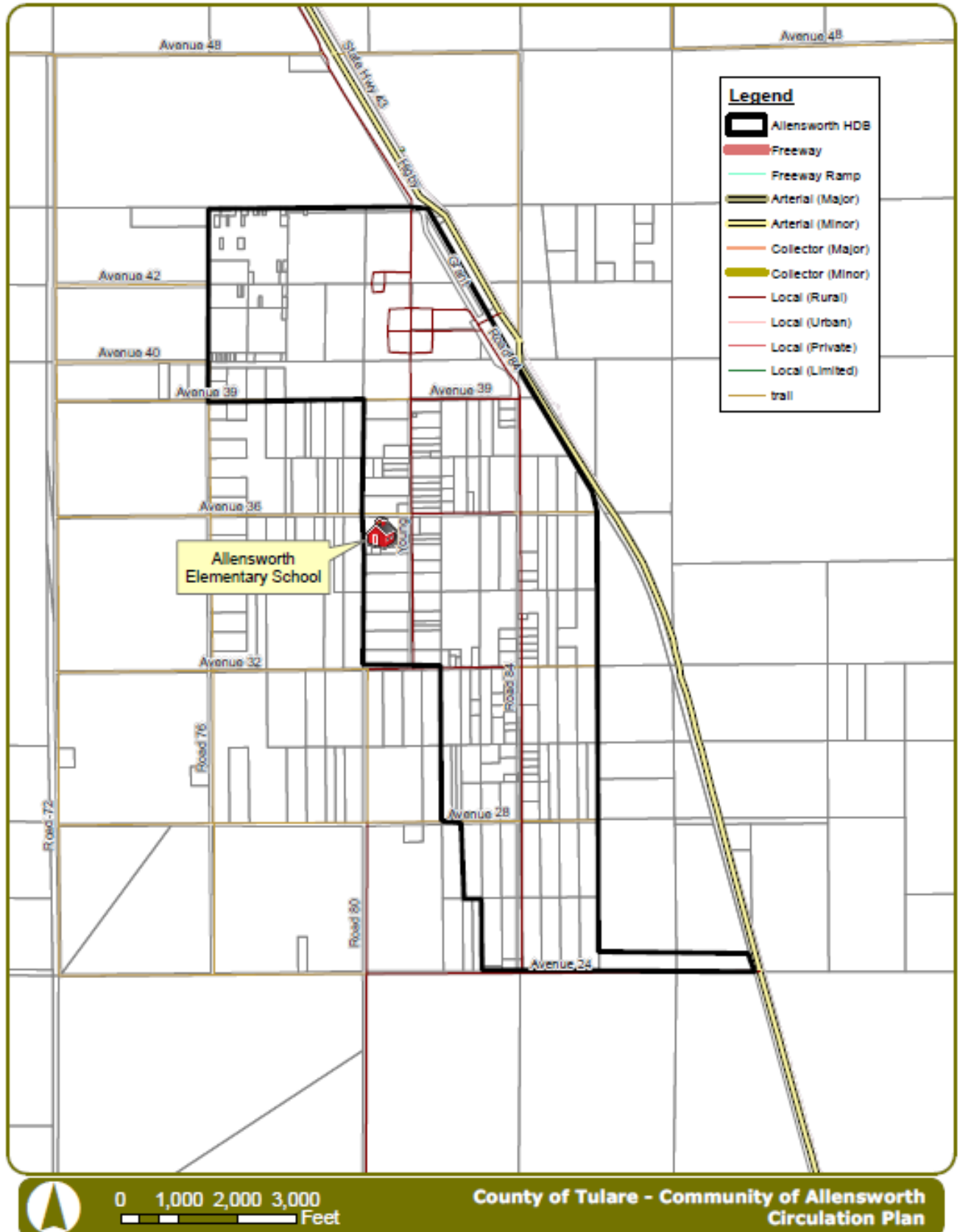
- Avenue 32 (Young Road to Road 84)
- Avenue 38 (Young Road to Road 84)
- Avenue 24 (from Road 84 half mile to the west)
- Avenue 28 (dirt road)
- Road 84 (north of Slowe Ave to Ave 24)
- Young Road (Ave 38 to south end of Allensworth Elementary)



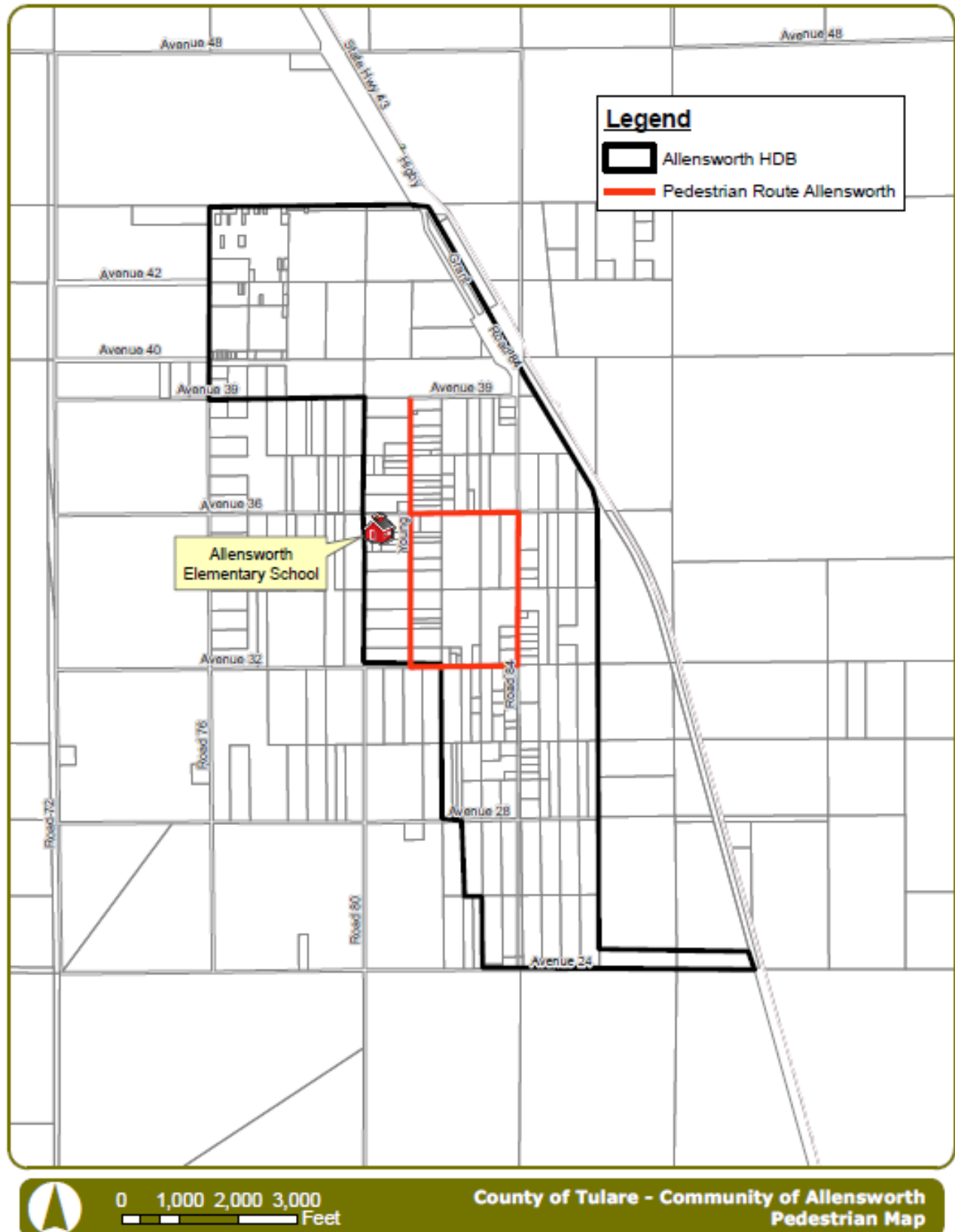
0 0.25 0.5 0.75 1  
Miles

County of Tulare - Community of Allensworth  
Road Identification Map

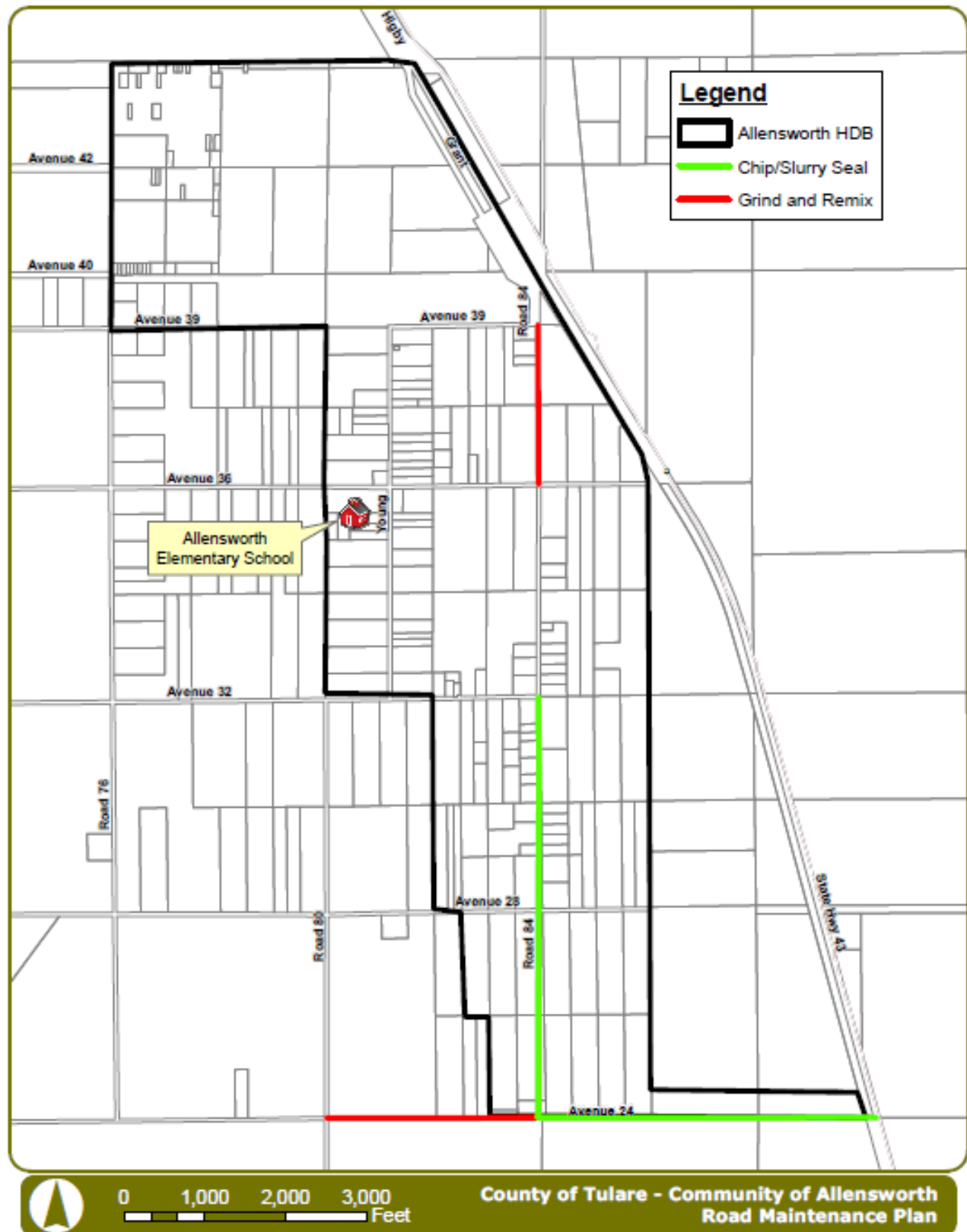
## Appendix B – Circulation Plan



## Appendix C – Bicycle, Bus, and Pedestrian Plan



## Appendix D – Road Maintenance Plan



# Appendix E – Cost Estimates for Allensworth

Detailed Engineer's Estimate						
Agency:		Tulare County Resource Management Agency				
Project Name:		Complete Streets Phase 3 Allensworth				
Project Location:		Allensworth - Young Rd - Ave 36 to Allensworth Elementary				
Date of Estimate:		December 21, 2016				
Prepared by:		Diego Corvera				
Construction Items						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
1	999990	Mobilization	LS	1	\$50,000	\$50,000
2	050126	Construction Staking	LS	1	\$20,000	\$20,000
3	120090	Construction Area Signs	LS	1	\$25,000	\$25,000
4	120100	Traffic control system	LS	1	\$15,000	\$15,000
5	120300	Temporary Pavement Marker (Ref.)	LS	1	\$20,000	\$20,000
6	130200	Prepare Water Pollution Control Program	LS	1	\$10,000	\$10,000
7	220101	Finishing Roadway	LS	1	\$15,000	\$15,000
8	150712	Remove Painted Pavement Markings	SQFT	252	\$5	\$1,260
9	152390	Remove Roadside Sign	EA	4	\$200	\$800
10	152379	Relocate Fence	LF	500	\$30	\$15,000
11(F)	190101	Roadway excavation	CY	1,279	\$100	\$127,870
12(F)	250201	Class 2 Aggregate Base	CY	722	\$95	\$68,611
13	390133	Hot Mix Asphalt (Type B)	TON	780	\$120	\$93,600
14	731504	Minor Concrete (Curb & Gutter)	LF	1220	\$25	\$30,500
15	731521	Minor Concrete (Sidewalk)	SQFT	5200	\$8	\$41,600
16	731623	Minor Concrete (Ramp)	EA	2	\$4,000	\$8,000
17	731516	Minor Concrete (Driveway)	SF	1900	\$15	\$28,500
18	840515	Thermoplastic Pavement Marking (Yellow)	SF	300	\$10	\$3,000
19	152469	Adjust Utility Cover to Grade	EA	3	\$800	\$2,400
20	568023	Install Roadside Sign	EA	6	\$500	\$3,000
21	152370	Relocate Mailboxes	EA	5	\$300	\$1,500
22		Remove Tree	EA	3	\$1,000	\$3,000
					Sub-Total:	\$583,641
					*Contingency:	\$58,364
					Construction Total:	\$642,006
* Up to 10% Contingency may be included in Engineer's Estimate						
(F) = Final Pay Item						
Non-Construction Related Cost						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
23	-	Environmental Clearance	% of CON	5%	\$583,641.48	\$29,182.07
24	-	Preliminary Engineering (PE)	% of CON	10%	\$583,641.48	\$58,364.15
25	-	Construction Engineering (CE)	% of CON	15%	\$583,641.48	\$87,546.22
26		Utility Relocation	LS	1	\$10,000.00	\$10,000.00
27	-	R/W Acquisition	LS	1	\$40,000.00	\$40,000.00
					Total:	\$225,092.44
Total Construction & Non-Construction Items						\$867,098.07







INDEX OF SHEETS

SHEET NO.	PAGE NO.	DESCRIPTION
T1	1	TITLE SHEET
X1	2	TYPICAL CROSS SECTIONS
L1	3	LAYOUT SHEETS

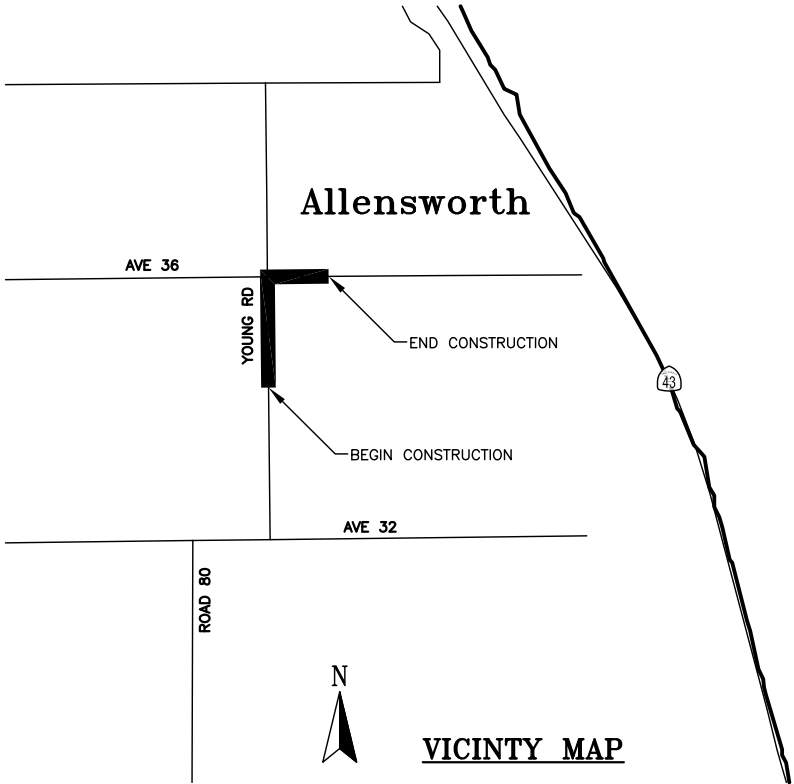
ABBREVIATIONS

AB	AGGREGATE BASE
AC	ASPHALT CONCRETE
AP	ANGLE POINT
BC	BEGIN HORIZONTAL CURVE
CL, $\phi$	CENTER LINE
CONC	CONCRETE
C-C	CENTER TO CENTER
DIA	DIAMETER
DWY	DRIVEWAY
EC	END OF HORIZONTAL CURVE
ETW	EDGE OF TRAVELED WAY
FL	FLOW LINE
GALV	GALVANIZED
GB	GRADE BREAK
HCR	HANDICAP RAMP
LT	LEFT
MAX	MAXIMUM
MIN	MINIMUM
MGS	MIDWEST GUARDRAIL SYSTEM
OG	ORIGINAL GROUND
R	RADIUS
R/W	RIGHT OF WAY
RP	RADIUS POINT
RT	RIGHT
STA	STATION
SW	SIDEWALK
TFC	TOP FACE OF CURB
TYP	TYPICAL
VAR	VARIES

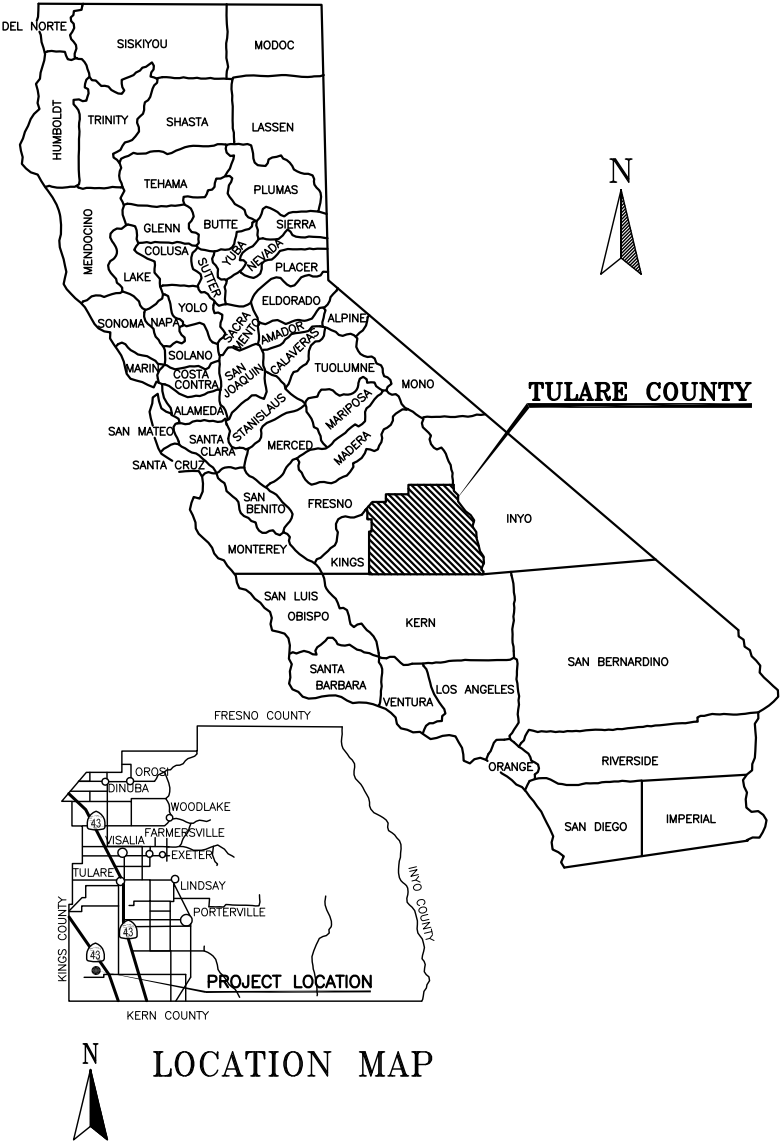
STATE OF CALIFORNIA  
COUNTY OF TULARE

PROJECT PLANS FOR CONSTRUCTION OF  
COMPLETE STREETS PHASE III  
ALLENSWORTH  
IN COUNTY OF TULARE

YOUNG ROAD – AVE 36 TO ALLENSWORTH ELEMENTARY SCHOOL



VICINTY MAP



LOCATION MAP



Know what's below.  
Call before you dig.

Contractor shall call  
Underground Service Alert at  
811 two working days prior  
to excavation

30% SUBMITTAL  
PRELIMINARY, NOT  
FOR CONSTRUCTION

REVISIONS	DATE	BY
No.	DESCRIPTION	

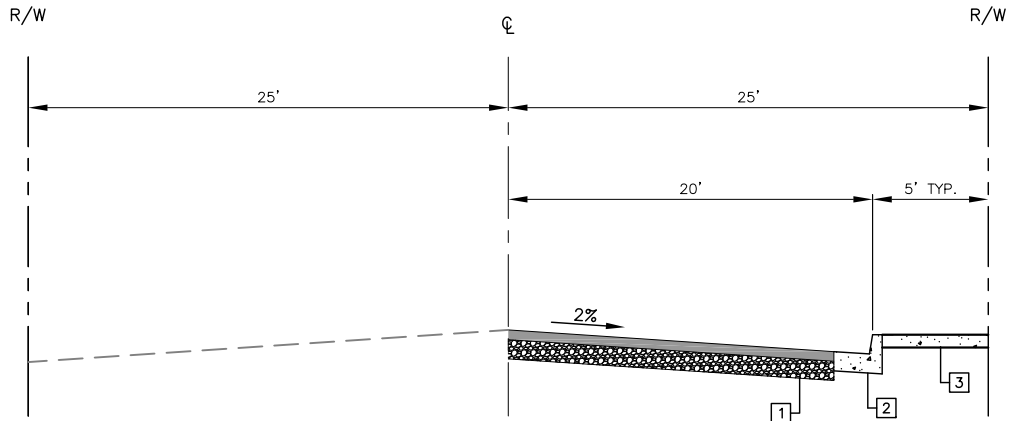
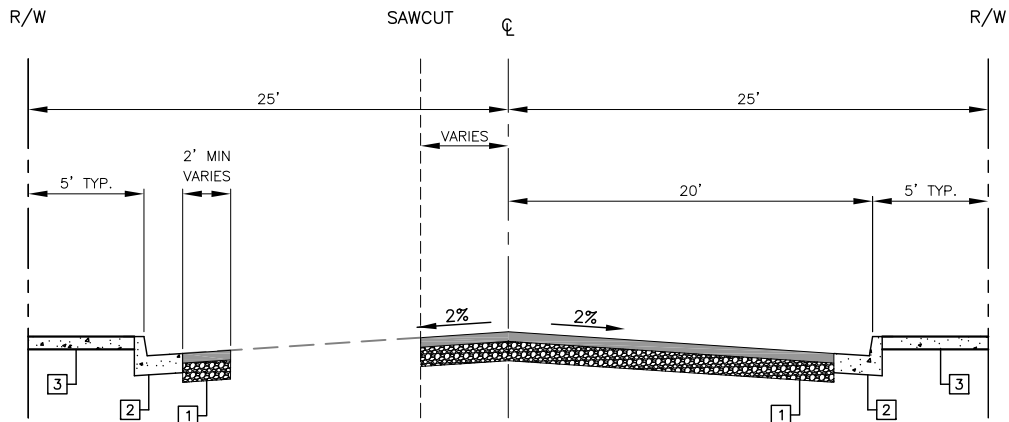
COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA



TITLE SHEET  
COMPLETE STREETS PHASE III  
ALLENSWORTH  
TULARE COUNTY

SCALE	N.T.S.
DIVISION	DESIGN
JOB NO.	16021-1
DESIGNED	DAC
DRAWN	DAC
CHECKED	PAO
FILE	16021-1T001.DWG
DATE	12-20-2016
SHEET No.	

T1  
1 OF 3



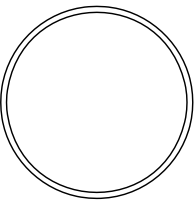
NOTES:

1. FOR EXACT LOCATIONS OF CURB AND GUTTER, CURB, AND SIDEWALK, SEE PLAN SHEETS.
2. ALL MAILBOXES, FENCES, TREES, SIGNS, STRUCTURES, OR OBSTRUCTIONS IN THE PROPOSED SIDEWALK AREA, EITHER EXISTING OR PROPOSED MUST CONFORM TO CURRENT ADA REQUIREMENTS FOR CLEARANCE AND OTHER STATUTES FOR WHEELCHAIR ACCESSIBILITY.
3. ALL EXISTING STRUCTURES, OBJECTS, OR FENCES IN THE CONSTRUCTION AREA OR R/W SHALL BE RELOCATED OR REMOVED UPON CONSULTING THE RESIDENT ENGINEER (RE) OR PROPERTY OWNER. POINT OF REMOVAL OF EXISTING CONCRETE STRUCTURES SHALL BE A CLEAN CUT OR SEPARATION BY SAW-CUT AND DETERMINED BY THE RE.
4. KNOWN UNDERGROUND UTILITIES ARE SHOWN AS APPROXIMATE IN LOCATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND VERIFY ALL UNDERGROUND UTILITIES IN THE CONSTRUCTION AREA BY EITHER CONTACTING USA OR THE UTILITY OWNER.
5. BACK OF WALK GRADING SHALL MATCH EXISTING GROUND AT 10% MAXIMUM SLOPE WITH NATIVE MATERIAL, DG, ASPHALT, CONCRETE OR PRE-EXISTING MATERIAL SUCH THAT A SMOOTH TRANSITION IS PROVIDED. PAYMENT FOR BACK OF WALK GRADING IS INCLUDED IN THE PAYMENT FOR ROADWAY EXCAVATION.
6. PAVING OR PAVE-OUT FROM PROPOSED TO EXISTING ROAD SHALL NOT EXCEED 15% FOR TRANSITION SLOPE OR PER ACCEPTANCE BY THE RE. ALL PAVING TO MATCH EXISTING SHALL HAVE A CLEAN CUT EDGE OR SAW-CUT AND SPRAYED WITH ASPHALT BINDER.

TYPICAL STRUCTURAL SECTIONS

- 1 20-YEAR DESIGN  
0.5' AC (TYPE B)  
1.0' AB (CLASS II)
- 2 BARRIER TYPE CURB AND GUTTER
- 3 CONCRETE SIDEWALK, 4" THICK TYPICAL

30% SUBMITTAL  
PRELIMINARY, NOT  
FOR CONSTRUCTION



REVISIONS		DATE	BY
No.	DESCRIPTION		

COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559)624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA

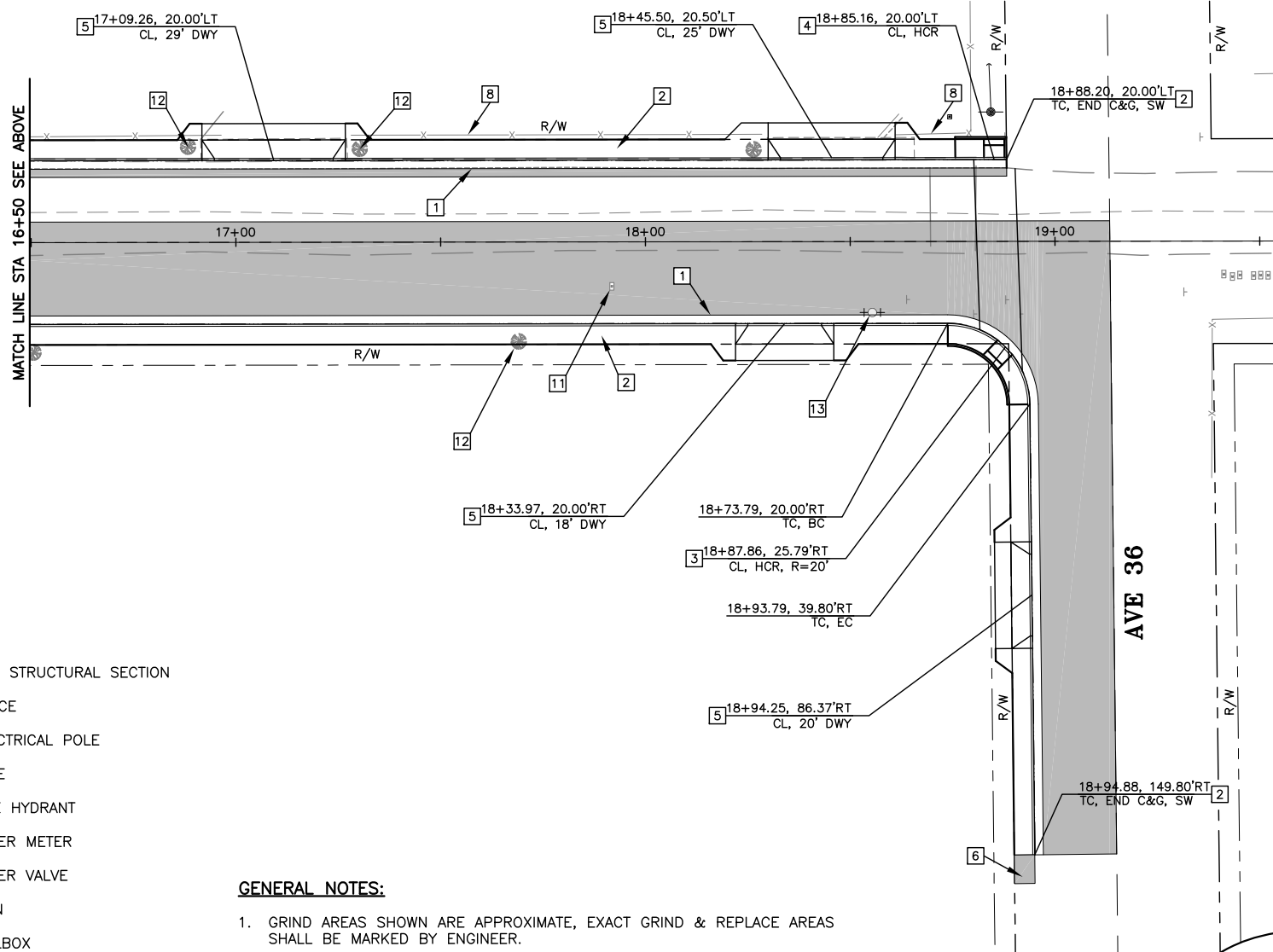
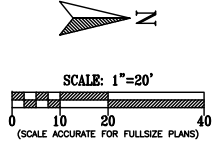
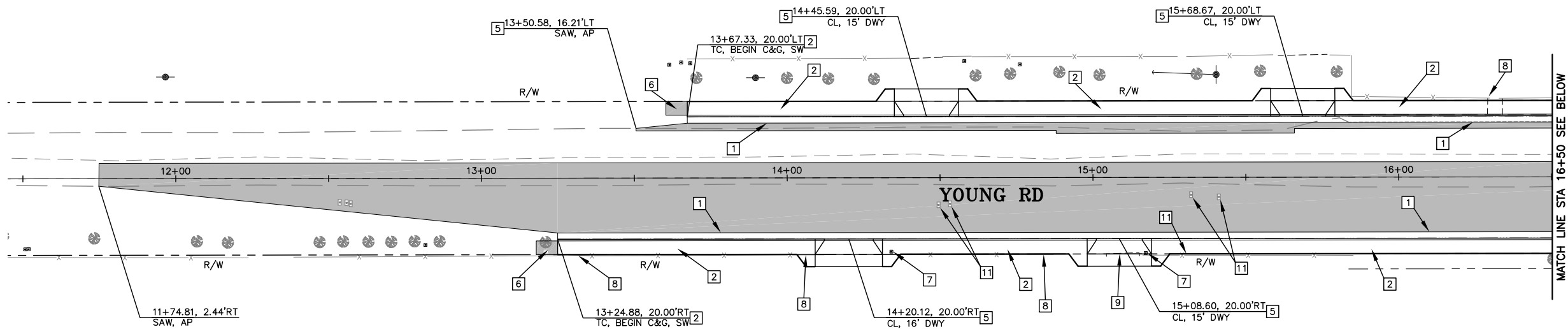


CROSS SECTIONS  
COMPLETE STREETS PHASE III  
ALLENSWORTH  
TULARE COUNTY

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JOB NO.	16021-1
DESIGNED	DAC
DRAWN	DAC
CHECKED	PAO
FILE	16021-1X001.DWG
DATE	12-20-2016
SHEET No.	

X1  
2 OF 3

\\PROJECTS\16021-1\16021-1X001.DWG 12/21/2016 10:44 AM



**CONSTRUCTION NOTES (SHEETS L1 & L2):**

- 1 MINOR CONCRETE (CURB & GUTTER)
- 2 MINOR CONCRETE (SIDEWALK) WIDTH PER PLAN
- 3 MINOR CONCRETE (RAMP) RAMP C
- 4 MINOR CONCRETE (RAMP) MODIFIED RAMP C
- 5 MINOR CONCRETE (DRIVEWAY)
- 6 MISCELLANEOUS AC RAMP
- 7 ADJUST UTILITY TO GRADE
- 8 RELOCATE CHAIN LINK FENCE
- 9 RELOCATE WROUGHT IRON FENCE
- 10 RELOCATE UTILITY POLES (BY OTHERS)
- 11 RELOCATE MAILBOX
- 12 REMOVE TREE
- 13 RELOCATE FIRE HYDRANT

**LEGEND**

- PROPOSED STRUCTURAL SECTION
- EXIST FENCE
- EXIST ELECTRICAL POLE
- EXIST TREE
- EXIST FIRE HYDRANT
- EXIST WATER METER
- EXIST WATER VALVE
- EXIST SIGN
- EXIST MAILBOX
- MONUMENT

**GENERAL NOTES:**

1. GRIND AREAS SHOWN ARE APPROXIMATE, EXACT GRIND & REPLACE AREAS SHALL BE MARKED BY ENGINEER.
2. SAWCUT BOUNDARIES ARE APPROXIMATED FOR THE CONSTRUCTION LIMITS AND SHALL BE DEFINED IN THE FIELD BY THE ENGINEER.

**30% SUBMITTAL  
PRELIMINARY, NOT  
FOR CONSTRUCTION**

REVISIONS	
DATE	BY

COUNTY OF TULARE  
RESOURCE MANAGEMENT AGENCY  
5961 SOUTH MOONEY BLVD.  
VISALIA, CA 93277  
(559) 624-7000  
WWW.TULARECOUNTY.CA.GOV/RMA



LAYOUT SHEET  
**COMPLETE STREETS PHASE III**  
**ALLENSWORTH**  
TULARE COUNTY

SCALE	1"=20'
DIVISION	DESIGN
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DESIGNED	AR
DRAWN	AR
CHECKED	PAO
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SHEET No.	

**L1**  
**3 OF 3**

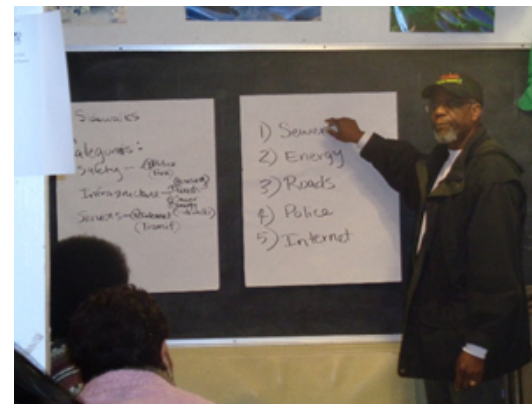
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## Appendix G – Complete Streets Outreach

### Community Meeting

#### Community Meeting

The Strategic Growth Council held a community input meeting in Allensworth on Tuesday January 12, 2016 at 6:00pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held at the Allensworth Community Center located at 8123 Ave 36 Allensworth CA 9321. This is an ideal location because most residents know where the community center is located and they feel welcomed and comfortable there. Over 35 Allensworth residents attended the meeting. 30 surveys were collected. Present were representatives from the Allensworth Community Services District, Allensworth School District and Tulare County Sheriff's Department.





# ALLENSWORTH HAMLET PLAN

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## APPENDICES

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,



Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.



The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT                     )  
TO SECTION 16.H OF ORDINANCE NO. 352            )  
TO ALLOW ADDITIONAL BY-RIGHT USES             )       RESOLUTION NO. 9369  
CONSISTENT WITH THE ADOPTED HAMLET            )  
PLANS, AS PROPOSED IN CHANGE                    )  
OF ZONE NO. PZC 17-039                            )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map

***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

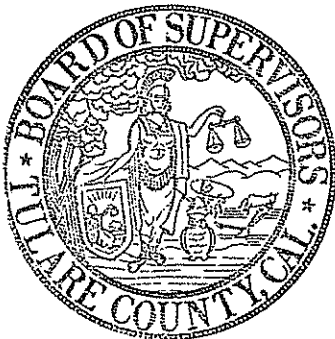
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.



9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **Delft Colony Hamlet Plan**

# DELFT COLONY HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# DELFT COLONY HAMLET PLAN

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# DELFT COLONY HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# DELFT COLONY HAMLET PLAN

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# Delft Colony Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Delft Colony Hamlet Plan: GPA 17-015  
Zoning District Map: PZC 17-015



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# DELFT COLONY HAMLET PLAN

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# DELFT COLONY HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# DELFT COLONY HAMLET PLAN

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# DELFT COLONY HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Mike Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# DELFT COLONY HAMLET PLAN

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# DELFT COLONY HAMLET PLAN

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# DELFT COLONY HAMLET PLAN

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# DELFT COLONY HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Delft Colony is to develop a plan which accurately reflects the needs and priorities of this community. Delft Colony is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (i.e. drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks, etc.) and to stimulate economic development within the community.

### Location

The community of Delft Colony (**see Figure 1**) is located on the east side of the San Joaquin Valley. Delft Colony is a census-designated place located in the northwest portion of Tulare County. It is generally bounded by south of Payne Avenue in the south, Avenue 400 in the north, Road 56 in the west and encompasses 0.07 square miles of land. It is directly served by State Route 201 (**See Figure 2**) roughly five miles southwest of the city of Dinuba in Tulare County. Delft Colony is in Section 26 Township 16 South, Range 23 East, MDB&M, and can be found within Reedley United States Geological Survey 7.5 minute topographic quadrangle. Delft Colony is located at an elevation of 312 feet National Geodetic Vertical Datum. The coordinates of Delft Colony are: Latitude: 36.5119 Longitude: -119.4465.

### Planning Area

The Delft Colony Hamlet Development Boundary (HDB) area consists of 87.3 acres (**see Figure 3**). The Land Uses within the HDB include agricultural activities which currently occupy 45.8% of the 87.3 acres. Urban development, including urbanized uses such as residential and commercial development occupy 40% of the 87.3 acres. The remaining 14 percent are lands dedicated for Right-of-way (**see Table 1**).

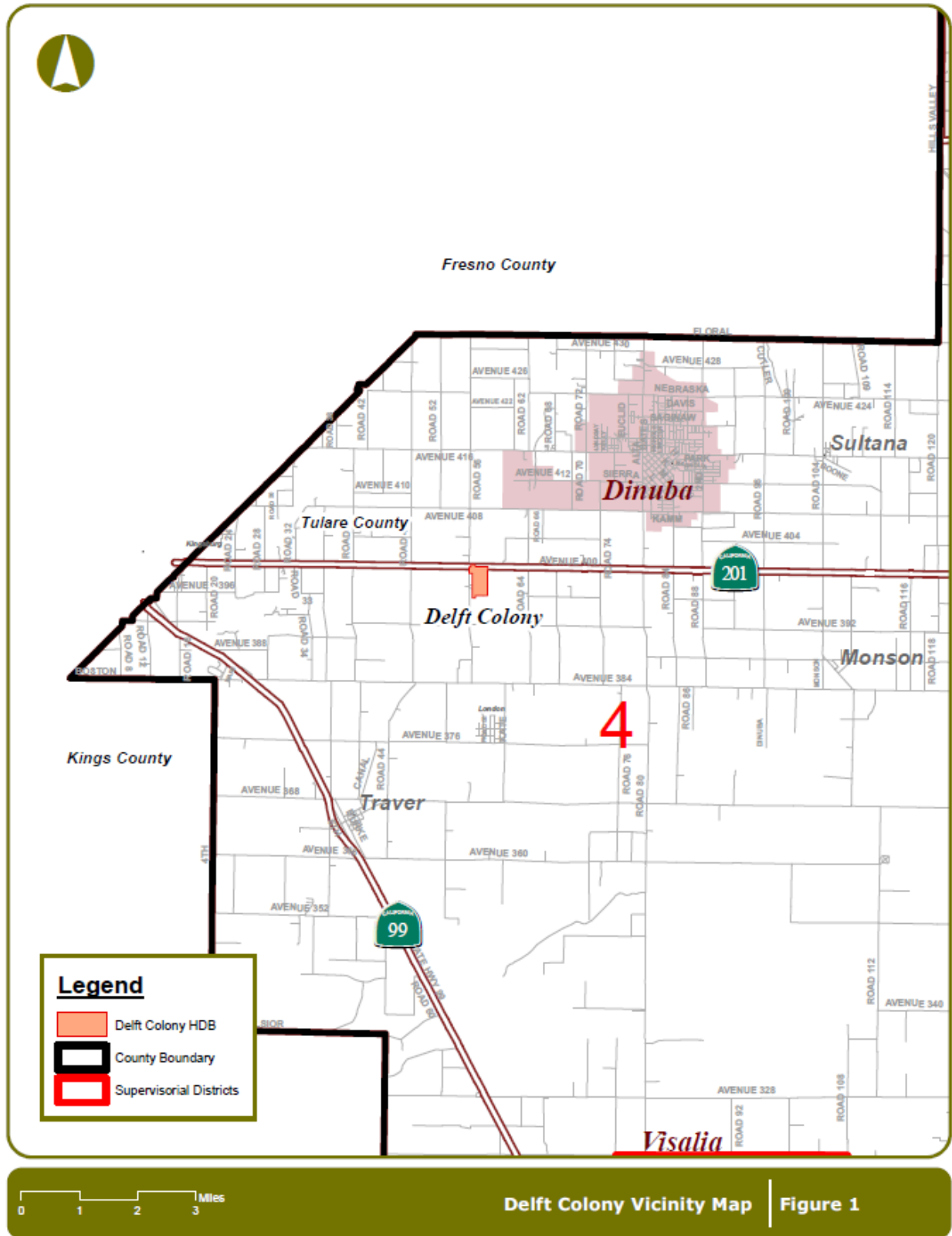
Table 1 - Delft Colony Land Use	
Land Use	Acres
Residential	33.6
Commercial	1.5
Agricultural	40
Rights-of-way	12.2
<b>Total</b>	<b>87.3</b>

Source: Tulare County GIS

There are changes being proposed to the Hamlet Development Boundary to align the northwest corner north of Avenue 400 and west of Road 56, approximately 3.5 acres (**see Figure 13**).

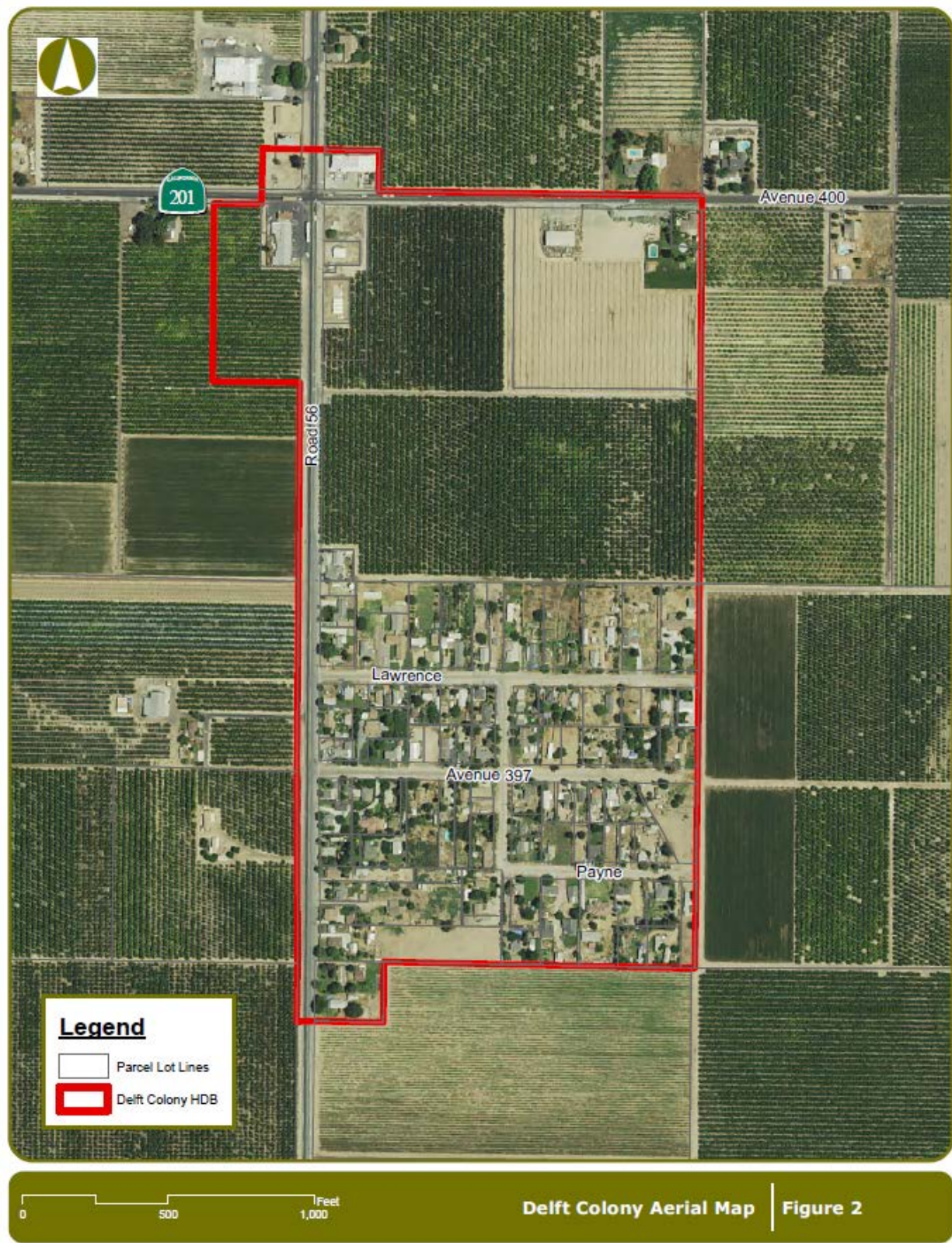
# DELFT COLONY HAMLET PLAN

Figure 1 - Vicinity Map



# DELFT COLONY HAMLET PLAN

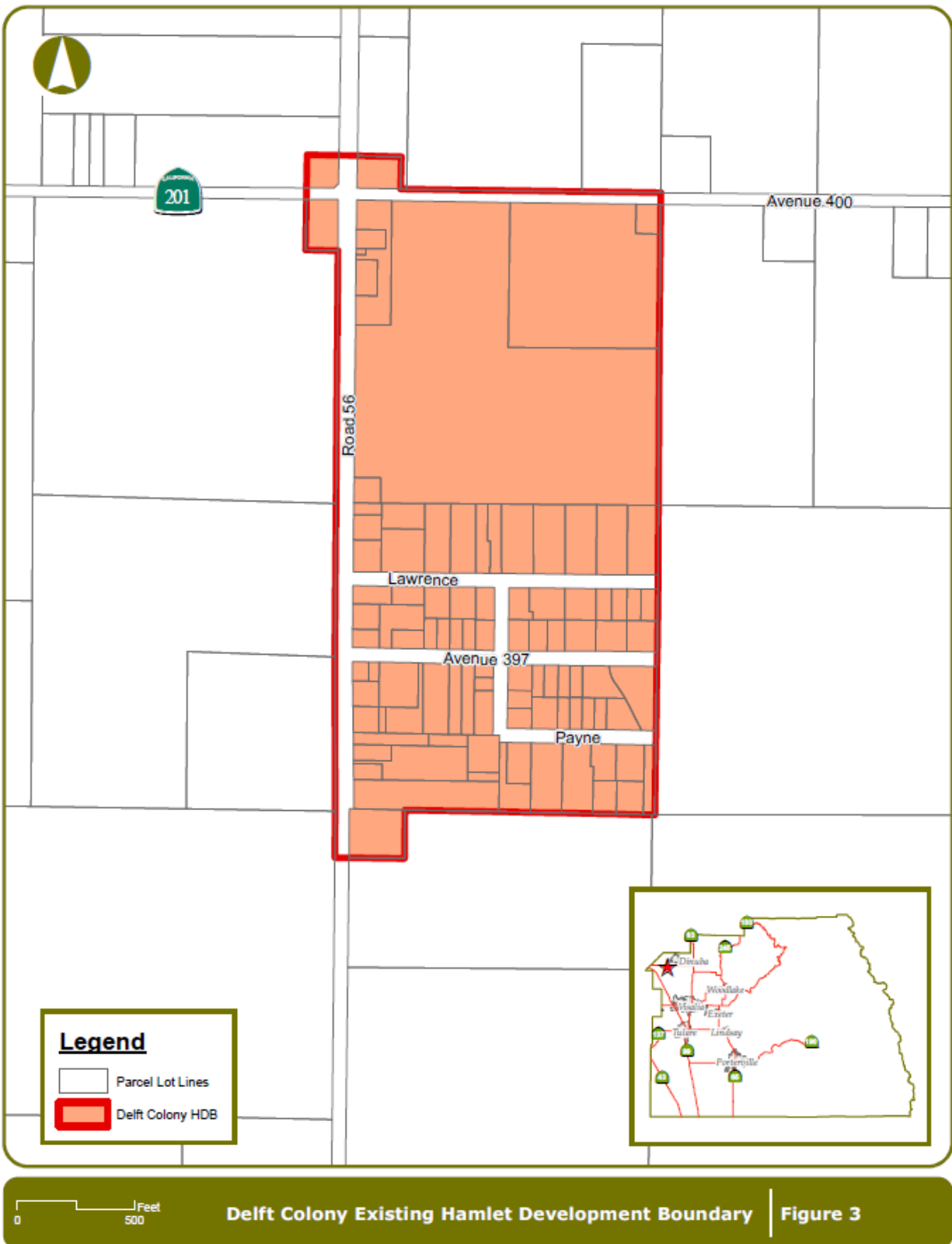
Figure 2 - Aerial - Delft Colony





# DELFT COLONY HAMLET PLAN

Figure 3 – Existing Delft Colony HDB



# DELFT COLONY HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to increase awareness of the Strategic Growth Council (SGC) project, Self Help Enterprises (SHE) went door to door to distribute flyers at every home, nearby businesses and the local school. During this process SHE emphasized the importance of participating in this process. SGC partnered with Tulare County Head Start Preschool to send meeting flyers home with every student at the school, and partnered with a local church to advertise the meeting.

### Community Outreach Steps

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the (SGC) project:

- 1.) SHE staff created an informational flyer.
- 2.) On Saturday, January 30, and Wednesday February 3, two SHE staff went door to door to distribute flyers to homes in Delft Colony.
- 3.) SHE staff partnered with members of A Better Community for Delft, a community service group advocating for local improvements. This group helped to distribute fliers and spread the word throughout the community.
- 4.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance of participating in the SGC community meeting
- 5.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner stores and all businesses.

### Community Meeting

A Strategic Growth Council community input meeting was held in Delft Colony on Thursday, February 4th at 6:00 pm. The meeting was scheduled in the evening to increase the probability that residents could attend.. The meeting was held in the meeting room at the Church of God, 5636 Lawrence Avenue in Delft Colony, a centrally lo



# DELFT COLONY HAMLET PLAN

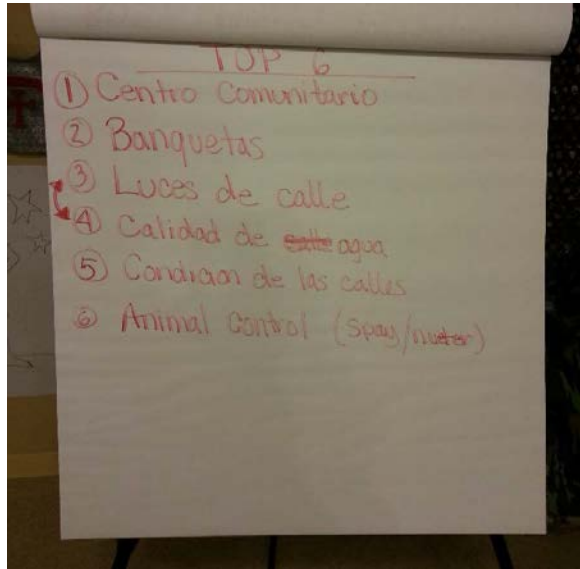
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## Community Survey

A survey was administered to gather specific community information about areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

## Priority Concerns

1. Water Quality
2. Road Conditions
3. Street Lights
4. Sidewalks
5. Community Center
6. Animal Control



## **Water Quality**

Residents expressed concern with their water quality. Most believe that the water is not safe to drink because it is perceived to be contaminated with bacteria. Resident stated that they do not drink their tap water instead they buy bottled water due to the fact that they have received several notices advising them not to drink tap water before boiling it.

A review of the SDWIS website indicates that violations have been issued to the water system due to not properly reporting per Total Coliform Rule requirements, however, there was no actual contamination noted.

## **Road Conditions**

Roads conditions are one of the biggest concerns to the residents of Delft Colony. Residents are concerned with driving conditions and walkability. They describe road conditions as poor and stated that roads have potholes and large cracks. Roads need maintenance, most reported that they are not satisfied with street safety. Fast vehicle traffic is also a concern.

# DELFT COLONY HAMLET PLAN

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## Priority Roads:

Ave 308

Road 52

Ave 309

Road 50

Road 48(Markham)

## **Street Lights**

Residents report dark streets at night due to little or no street lighting, and that they stay indoors because they do not feel safe walking on a dark street. This is a major safety concern for them, especially since according to residents the Sheriff's Department seldom patrols Delft Colony. Residents stated that some streets have light poles but the light bulb is out and other streets have no poles at all.

## **Sidewalks**

Residents feel that their community has too few sidewalks. The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking and for parents who push a stroller through town. During the rainy season, residents and kids are forced to walk in the mud and through big puddles of water. The absence of sidewalks means there is no clear separation between walking paths and the road, creating a safety hazard for pedestrians.

## **Community Resource Center**

The community lacks many resources and needs a central location where residents can go to access information and county resources. People in Delft Colony expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to provide services to Delft Colony residents. This could also be the place where residents access a reliable internet connection.

## **Internet Access**

Most families in Delft Colony do not have internet access at their homes. Available internet service is unreliable and cost prohibitive. Residents report being told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is considered to be a major problem to residents, because without access to the internet, they are unable to do things such as online job hunting, applying for resources, and or provide homework help to their kids. Internet access ranked as a high priority improvement need in Delft Colony.



# DELFT COLONY HAMLET PLAN

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## DELFT COLONY TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Delft Colony.

#### Population

In 2015, the population of Delft Colony was 132 (see Table 2).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Delft Colony</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	85	64.4%
Female	19,334,329	50.3%	226,607	49.9%	47	35.6%
Total	38,421,464		454,033		132	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

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<sup>1</sup> Tulare County Regional Blueprint, page 7

# DELFT COLONY HAMLET PLAN

## Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Delft Colony's population during the Planning Period (see Table 3).

Table 3 - Projected Annual Growth Rate		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

## Median Age

Delft Colony's population with a median age of 16.7 is younger than the median age of the Tulare County and the State of California. (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Delft Colony	16.7

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 0% of Delft Colony's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 0% was two races or more (see Table 5). Approximately 100% was Hispanic (of any race). This statistic is significant because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Delft Colony	%
Total	38,421,464		454,033		132	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	132	100%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	0	0%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%		0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Delft Colony

According to the Tulare County 2030 General Plan Update, Tulare County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see Table 6).

# DELFT COLONY HAMLET PLAN

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According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Delft Colony was about 16.2% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. The 16.2% includes only the employable labor force (that is, not every person of the population) results in about 12 unemployed persons of Delft Colony's person labor force of 47.

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>Delft Colony</b>
Population 16 years & over	30,312,429	322,441	74
In labor force	19,269,449	197,879	47
Civilian labor force	19,137,441	197,506	47
Employed	17,246,360	170,819	35
Unemployed	1,891,081	26,687	12
Armed Forces	132,008	373	0
Not in labor force	11,042,980	124,562	27

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Delft Colony's median household income was \$6,917, compared to \$61,818 for the State of California and \$42,031 for the County of Tulare. (see Table 7)

<b>Table 7 - 2011-2015 American Community Survey Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$30,318
<b>Tulare County</b>	\$42,031	\$44,814	\$17,876
<b>Delft Colony CDP</b>	\$6,917	\$6,917	\$2,345

2011-2015 American Community Survey 5-Year Estimates

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Delft Colony's median household income was \$6,917, whereas the State of California's median household income was \$61,818. Delft Colony's median household income was 11.1% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

# DELFT COLONY HAMLET PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes) According to the California Department of Finance, the 2011-2015 American Community Survey data indicated 100% of the housing units in Delft Colony were renter occupied (see Table 9).

Table 9 - Housing Tenure			
	California	Tulare County	Delft Colony
Occupied housing units	12,717,801	133,570	47
Owner-occupied	6,909,176	75,685	0
Renter-occupied	5,808,625	57,885	47

2011-2015 American Community Survey 5-Year Estimates

Table 8 - Housing Tenure (%)			
	California	Tulare County	Delft Colony
Owner-occupied	54.8%	57.2%	0.0%
Renter-occupied	45.2%	42.8%	100%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Delft Colony	3	9	6	19	6	19	5	6	12	38	32

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Delft Colony indicated that approximately 9% of the housing units were sound. Approximately 44% of Delft Colony's housing units were deteriorated and 38% were dilapidated (see Table 10).

# DELFT COLONY HAMLET PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. **(see Table 11)** The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Delft Colony
Total housing units	13,845,790	144,792	47
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	0
Built 1990 to 1999	1,495,571	21,767	0
Built 1980 to 1989	2,117,819	22,733	0
Built 1970 to 1979	2,503,688	27,111	32
Built 1960 to 1969	1,871,029	15,500	0
Built 1950 to 1959	1,907,512	13,694	15
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	0

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Delft Colony was 2.81 persons per household **(see Table 12)**.

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that

the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Delft Colony	0.0	2.81

2011-2015 American Community Survey 5-Year Estimates

## Vacancy Rate

Vacancy rate provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. Typically, a vacancy rate of about 5-6% is

considered normal; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Delft Colony was 0.0%, which was lower than Tulare County at 4.2% and the State of California at 4.6%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.8% and the State of California at 1.6%, **(see Table 13)**.

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.6%	4.6%
Tulare County	1.8%	4.2%
Delft Colony	0.0%	0.0%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# DELFT COLONY HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016 (**see Figure 4**) indicates there are special status species within the Reedley Quadrant Species List (which includes the Delft Colony Planning Study Area) consisting of one animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

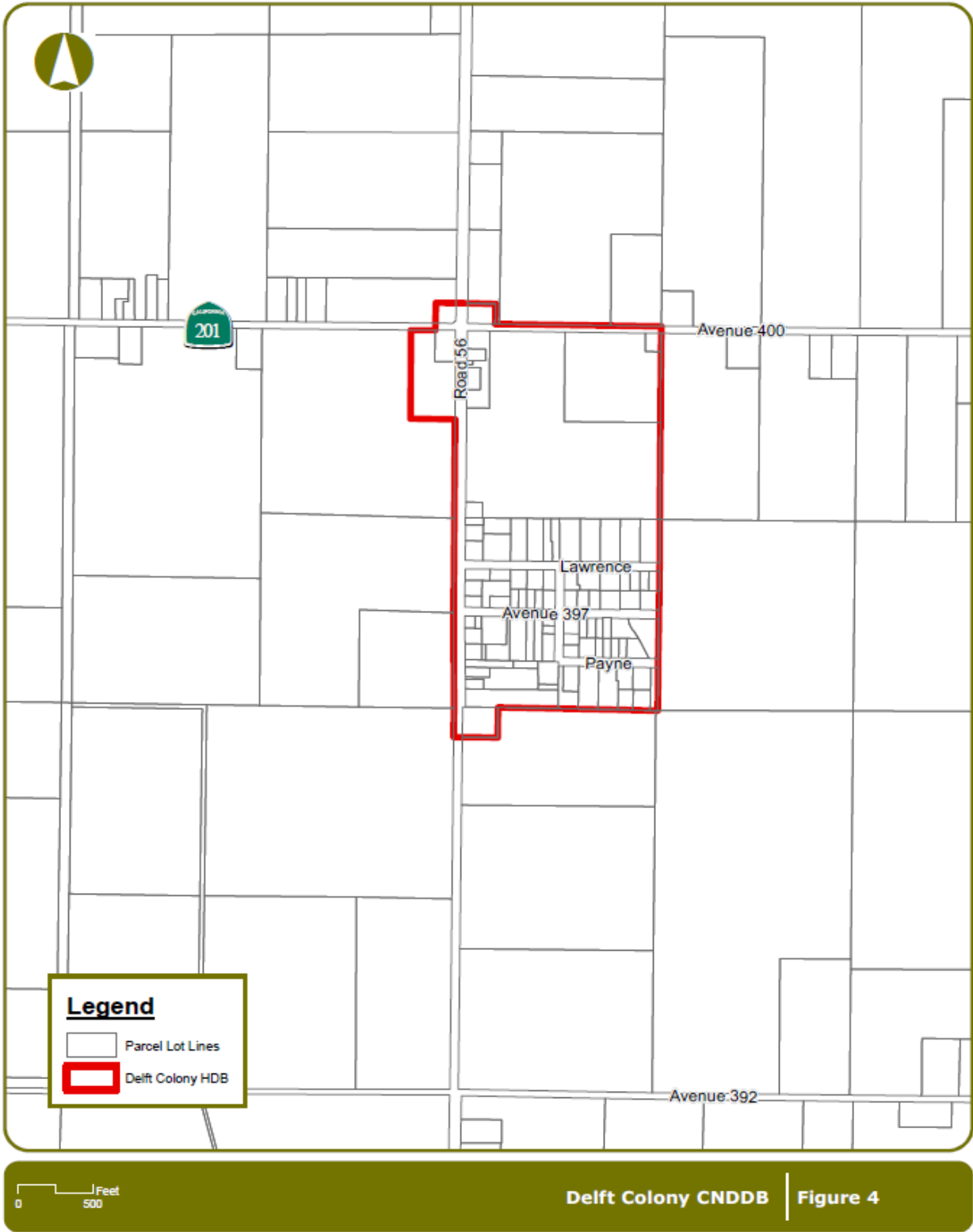
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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. November 1, 2016

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# DELFT COLONY HAMLET PLAN

Figure 4 - Delft Colony CNDDB Map



# DELFT COLONY HAMLET PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Delft Colony are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Delft Colony] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Delft Colony (**see Figure 5**). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**Calgro-Calgro, saline-Sodic, complex, 0 to 2 percent slopes**, Erosion potential is low to moderate and shrink swell potential low.

**Hanford sandy loam, 0 to 2 percent slopes**, is very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains and alluvial fans; negligible to medium runoff; moderately rapid and moderate permeability, however saline-sodic phases and soils with sandy clay loam substratums have moderately slow permeability. This soil is suitable for growing a wide range of fruits, vegetables, and general farm crops. They are also used for urban development and dairies.

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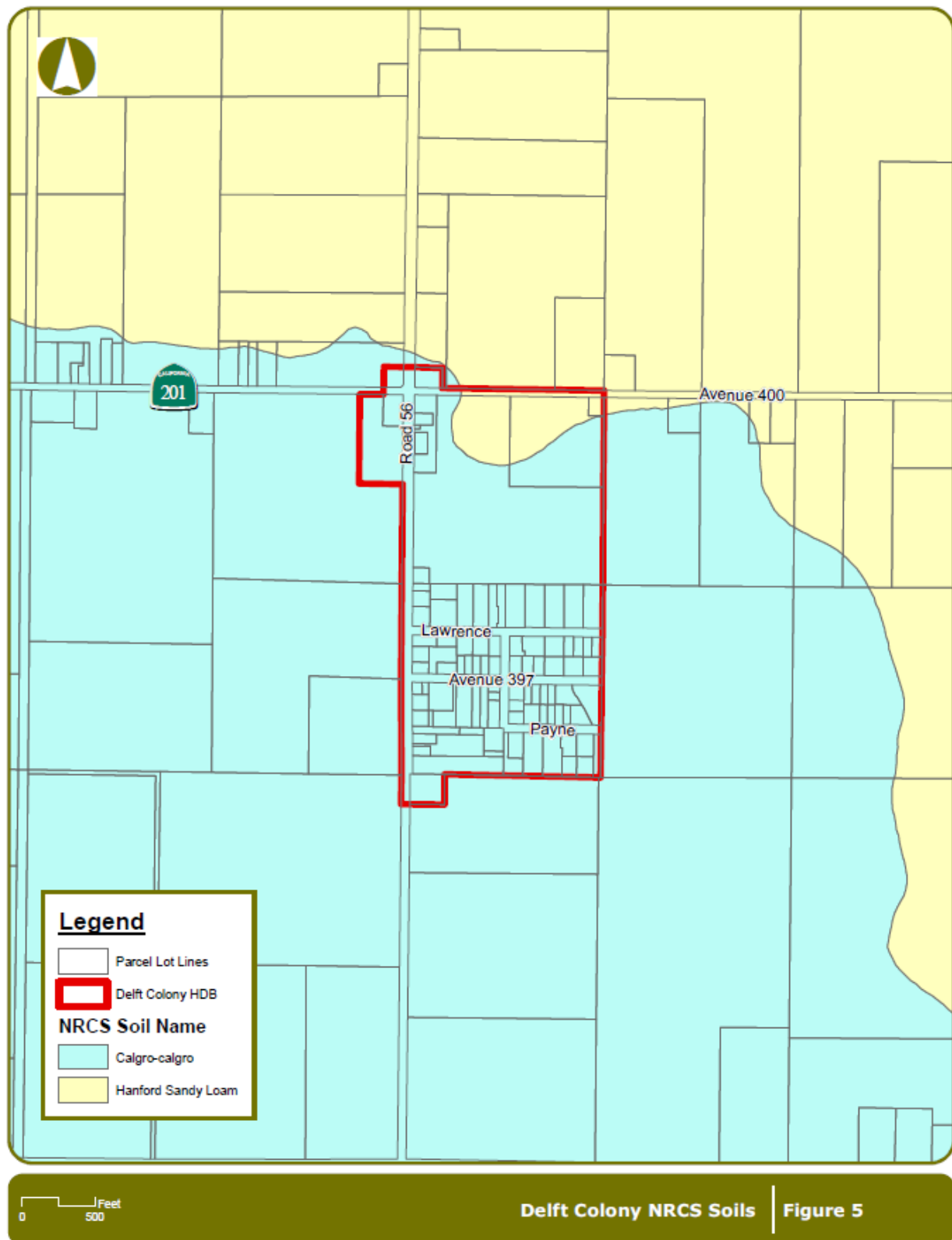
<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database, accessed August 17, 2016.



# DELFT COLONY HAMLET PLAN

Figure 5 - Soils Map – Delft Colony



# DELFT COLONY HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Delft Colony Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Delft Colony is located near the southwestern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely

# DELFT COLONY HAMLET PLAN

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modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Delft Colony include State Highway 201 and agricultural activities.

## Flooding<sup>6</sup>

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0605E (**see Figure 6**), all portions of Delft Colony are located within Flood Zone X, which is defined by FEMA as “Areas determined to be outside the 0.2% annual chance floodplain.” Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

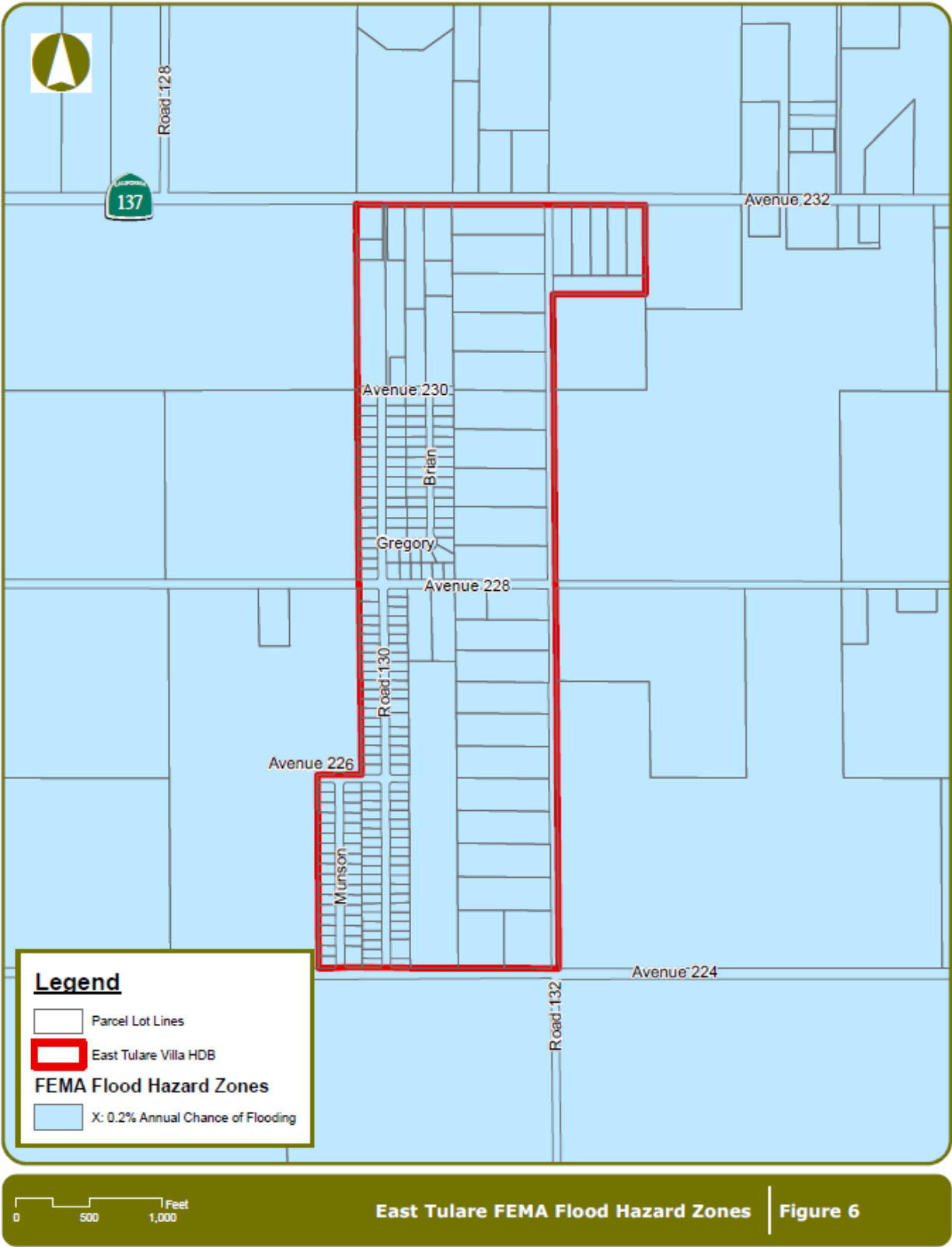
The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

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<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0605E, Panel Number 605 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>. Accessed August 17, 2016.

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Figure 6 - FEMA Flood Map



# DELFT COLONY HAMLET PLAN

## Noise<sup>7</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBa) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. **Table 14** summarizes the daily traffic volumes along State Route (SR) 201 from Fresno County Line to State Route (SR) 63 and Avenue 56 from Avenue 384 to Fresno County Line.

Table 14 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
State Route 201 from Fresno County Line to State Route 63	6,200	41	88	189	407
Avenue 56 from Avenue 384 to Fresno County Line	3,871	28	60	114	277

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.).

### Energy: Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve

<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# DELFT COLONY HAMLET PLAN

capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom SCE has arrangements, and rolling blackouts.

## Domestic Water & Wastewater

Domestic water and sewer service in Delft Colony is provided by Tulare County. **Table 18** shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Tulare County, January 2014). Mapping of the sewer and water systems is currently unavailable.

**Table 15 - Existing Water and Wastewater Connections in Delft Colony**

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
112	112	0	112	142	30

(Data current as of January 2014)

## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Delft Colony does not currently have a storm drainage system.”<sup>8</sup>

## Solid Waste

Solid waste disposal services for the Community of Delft Colony is provided by Pena’s Disposal, a private company. Solid waste generated in Delft Colony can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia.

## Roads

“There are several roadways in Delft Colony that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement

<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

# DELFT COLONY HAMLET PLAN

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may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>9</sup>

**Table 16** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 6** graphically displays this information on a map.

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<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# DELFT COLONY HAMLET PLAN

**Table 16 - Roads in Need of Major and Medium Repair in Delft Colony**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 397	Road 56 to Road 57	CHIP
2	Avenue 397	Road 57 to road 58	CHIP
3	Payne Avenue	Road 57 to Road 58	CHIP
4	Road 56	Avenue 397 to Lawrence Avenue	CHIP

OLAY – overlay resurfacing operation

CHIP – chip seal

GRX – grind and remix

ACST – asphalt reconstruction

RCST – cold mix reconstruction

(Source: County of Tulare Public Works, 2012)

## Street Lights

Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians. **Table 15** identifies the location of existing street lights that are maintained by Tulare County, in Delft Colony, as well as their specifications. **Figure 7** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.”<sup>10</sup>

**Table 17 - Existing Street Lights**

Specifications of Existing Street Lights								
No.	East-West	North-South	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 397	Road 566	NE Corner	1508	9500	W	W	PG&E
2	Avenue 397	Road 57	NE Corner	1507	5800	W	N	PG&E
3	Lawrence Avenue	Road 56	NE Corner	N/A	9500	W	S	SCE
4	Lawrence Avenue	Road 57	NW Corner	1505	5800	W	S	PG&E

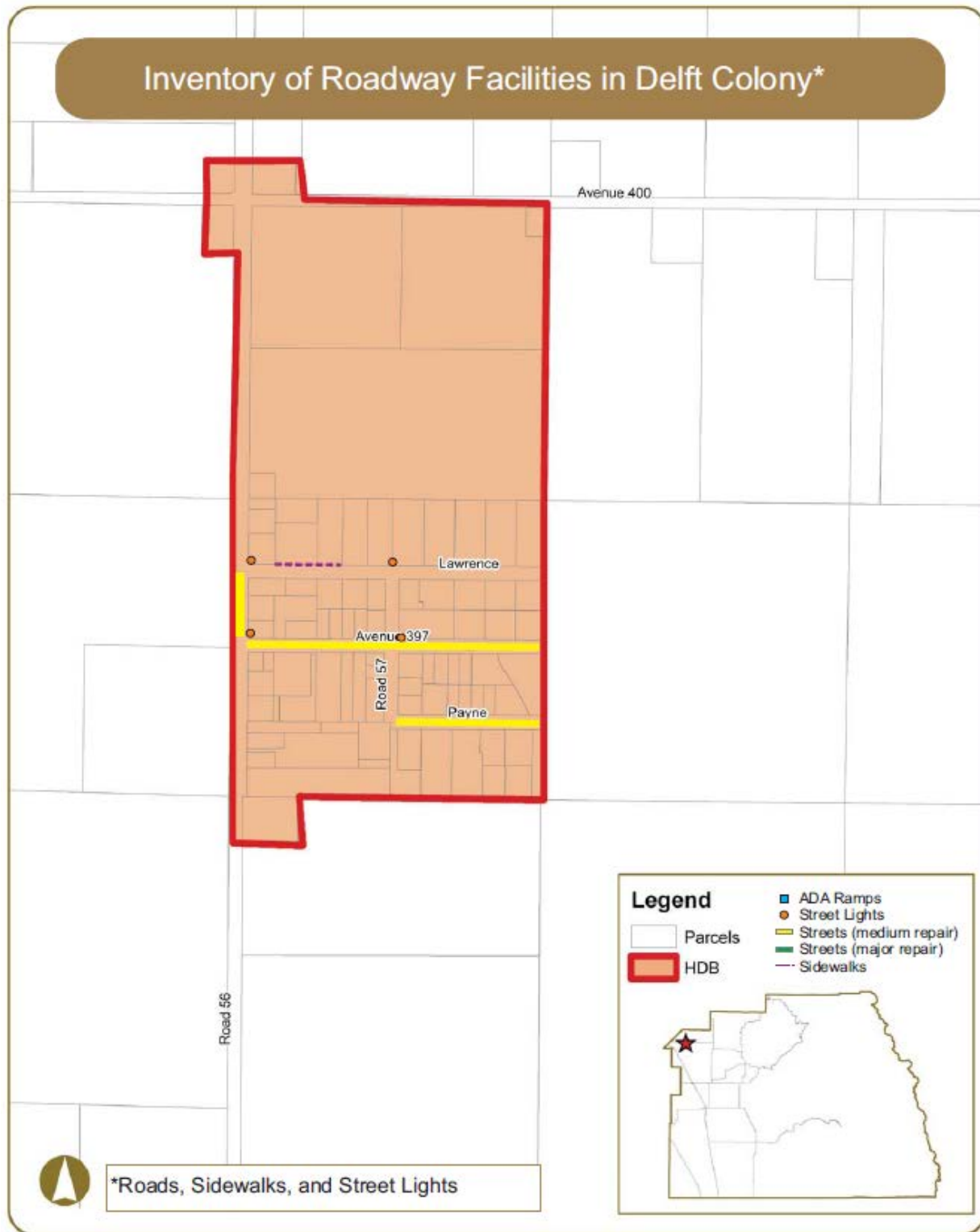
(Source: Tulare County Public Works, March 2013)

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element



# DELFT COLONY HAMLET PLAN

Figure 7 - Delft Colony Inventory of Roadway Facilities



# DELFT COLONY HAMLET PLAN

## Sidewalks

The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking and for parents who push a stroller through town. When it rains, residents and kids must walk in the mud and through puddles of water. The absence of sidewalks means there is a lack of a clear separation between walking paths and the road creating a safety hazard for pedestrians.

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles. (see **Table 18**)

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

**Table 18 - Existing Sidewalks in Delft Colony**

Location of Existing Sidewalks			
No.	Roadway	Limits	Location
1	Lawrence Road	Road 56 to Road 57	North side

(Source: County of Tulare Public Works and VRPA Technologies, February 2014)

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Delft Colony.”<sup>11</sup>

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<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element

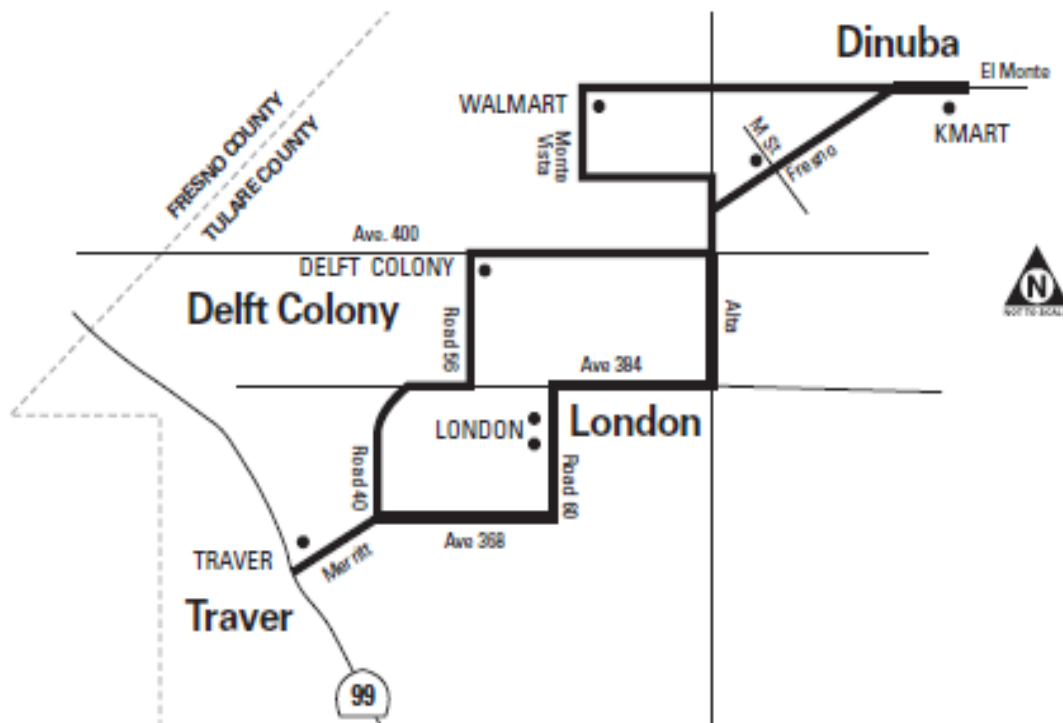
# DELFT COLONY HAMLET PLAN

## Public Transportation

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Delft Colony is currently connected via TCAT Route 50 to Dinuba and its transit center (see **Figure 7**) [www.ridetcats.org](http://www.ridetcats.org). TCAT Route 50 provides service to Dinuba four times per day on weekdays and four times on Saturday. In Dinuba, transfers can be made to connect to Visalia, Tulare, and the remainder of the TCAT public transit system. TCAT vehicles are wheelchair accessible and all full size buses include bike racks. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Delft Colony, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

**Figure 8 – TCAT - Fixed Route Service for Delft Colony – Route 50**



# DELFT COLONY HAMLET PLAN

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## AMTRAK

The Hanford AMTRAK station, located approximately 45 miles to the southwest in Kings County, is the closest station to Delft Colony providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 25 miles south. Fresno Yosemite International Airport (FAT), approximately 40 miles northeast of Delft Colony, is a principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles southeast of Delft Colony and offers direct flights to several destinations.

## PUBLIC SERVICES

### Sheriff

Police protection services are provided in Delft Colony by the Tulare County Sheriff's Department sub-station, located in Visalia, approximately 15 miles southeast of Delft Colony. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

### Fire

Fire protection and emergency medical services are provided for Delft Colony by the Tulare County Fire Department. The community is served by Kings River Fire Station, located at 3811 Avenue 400, in Kingsburg. Engine 2 is assigned to this location.

Eleven fire hydrants are found within Delft Colony (**see Table 19**). These fire hydrants are located within the County right-of-way. **Figure 8** lists existing Fire Hydrants in Delft Colony.

# DELFT COLONY HAMLET PLAN

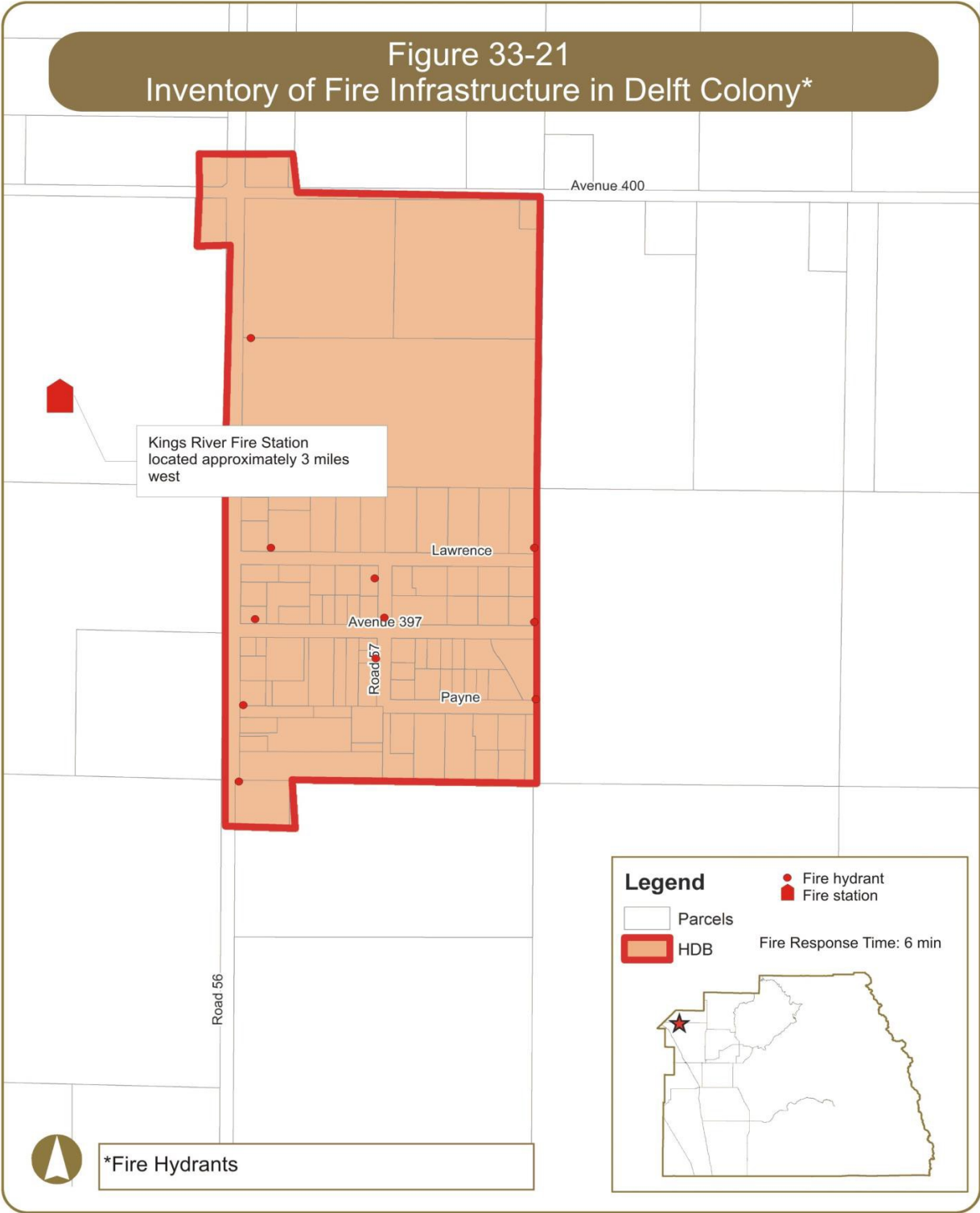
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**Table 19 - Location of Existing Fire Hydrants**

<b>Existing Fire Hydrants</b>	
<b>No.</b>	<b>Location</b>
1	Road 56 south of Avenue 400
2	Lawrence Avenue east of Road 56
3	Lawrence Avenue east of Road 57
4	Road 57 south of Lawrence Avenue
5	Avenue 397 and Road 57
6	Avenue 397 east of Road 56
7	Avenue 397 east of Road 57
8	Road 57 south of Avenue 397
9	Road 56 south of Avenue 397
10	Payne Avenue east of Road 57
11	Road 56 south of Avenue 397

# DELFT COLONY HAMLET PLAN

Figure 9 - Inventory of Fire Infrastructure in Delft Colony



# DELFT COLONY HAMLET PLAN

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## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a significant factor in attracting residential development to certain areas of a community. The Delft Colony Hamlet Plan Area is within the Kings River Elementary School District, located approximately four (4) miles away. The District provides K-8 grade education. High School students attend high school in Dinuba, approximately four (4) miles away.

## Library

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>12</sup> The closest library is located in Dinuba, five miles away. (see Table 20)

Table 20 - Library Location & Hours		
Branch	Address	Service Hours (2016)
Dinuba	Dinuba Branch 150 South I Street Dinuba, CA 93618	Tuesday - Friday: 9:00 a.m. - 1:00 p.m. 2:00 p.m. – 6:00 p.m. *Closed Saturday, Sunday, Monday

Library hours current as of July 2017

## Parks

Presently there are no County owned/operated public parks in Delft Colony. The closest public park to the community is located in Dinuba approximately four (4) miles to the north.

# THE DELFT COLONY HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get

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<sup>12</sup> General Plan Background Report, page 7-96

# DELFT COLONY HAMLET PLAN

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products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>13</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>14</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation and Transportation Conditions

Delft Colony is a small agricultural community located adjacent to SR 201. The major rural collector roads is Avenue 308. There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

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<sup>13</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>14</sup> Tulare County General Plan Background Report, page 5-7.



# DELFT COLONY HAMLET PLAN

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## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified Road 80 as a proposed Class III Bike Project<sup>15</sup> In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is Road 80.

## GOALS, OBJECTIVES AND POLICIES

This Section of the Delft Colony Hamlet Plan describes land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Delft Colony to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Delft Colony Hamlet Plan, and input received from Delft Colony citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.

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<sup>15</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# DELFT COLONY HAMLET PLAN

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3. Locate high density residential uses in close proximity to planned shopping areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
5. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
6. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
7. The County shall work with the Schools to provide safe routes to school.
8. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Delft Colony Community Plan.
9. The Delft Colony Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.
10. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Delft Colony.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Delft Colony.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

# DELFT COLONY HAMLET PLAN

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Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

**GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.

3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

Policies:

1. Encourage the Delft Colony to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## **Environmental Quality and Public Safety**

**GOAL I: Preserve and enhance the quality of life for present and future generation of Delft Colony citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.

# DELFT COLONY HAMLET PLAN

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2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Protect Agricultural Lands:

Land within the respective Delft Colony, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Delft Colony.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Delft Colony.

# DELFT COLONY HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 87.3 acre Delft Colony Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 45 percent of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 132 by 90.3 urbanized acres (area within the HDB), which equals 1.5 persons per urbanized acre (see **Tables 21 and 22**) showing population projections). Projecting the population at a 1.3% growth rate adds 28 persons in Year 2030. Multiplying the 28 persons by the 1/3 ratio suggests an additional 0.36 acres of land will be required to accommodate development by the Year 2030 if projections are realized.

### Population Growth Forecast

<b>Table 21 - Delft Colony Population Projections</b>			
<b>Growth Rate</b>	<b>2015</b>	<b>2020</b>	<b>2030</b>
0.013	<b>132</b>	<b>143</b>	<b>160</b>

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in Year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The Year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>16</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in Year 2015 the community had 47 dwelling units (including vacant dwellings) with a population of 132. At an annual growth rate of 1.3%, the projected housing units are 50 and 57 in Years 2020 and 2030, respectively, and projected population is 141 and 160 in Years 2020 and 2030, respectively.

<b>Table 22 - Population Projects</b>		
<b>Year</b>	<b>Population</b>	<b>Growth (%)</b>
2015	132	0.013
2016	134	0.013
2017	135	0.013
2018	137	0.013
2019	139	0.013
2020	141	0.013
2021	143	0.013
2022	144	0.013
2023	146	0.013
2024	148	0.013
2025	150	0.013
2026	152	0.013
2027	154	0.013
2028	156	0.013
2029	158	0.013
2030	160	0.013

<sup>16</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

# DELFT COLONY HAMLET PLAN

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require a discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Table 23 - Education Attainment			
Educational Attainment	California	Tulare County	Delft Colony
Population 25 years and over	25,257,858	260,964	62
Less than 9th grade	10.0%	20.8%	12
9th to 12th grade, no diploma	8.2%	10.9%	0
High school graduate (& equivalency)	20.7%	25.2%	15
Some college, no degree	21.8%	22.3%	20
Associate's degree	7.8%	7.0%	0
Bachelor's degree	19.8%	9.5%	15
Graduate or professional degree	11.6%	4.3%	0

2011-2015 American Community Survey 5-Year Estimates

Based on the 2011-2015 American Community Survey (see **Table 23**), the educational barrier in Delft Colony begins in grade school. Of the adults age 25 and older, 12% had an educational level of less than 9<sup>th</sup> grade. This lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

### Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are located in Dinuba and Kingsburg.

# DELFT COLONY HAMLET PLAN

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 24**) and Zoning Districts plan (see **Table 25**) have been updated.

<b>Table 25 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	78.6
Right-of Ways	12.2
Delft Colony HDB	90.8

<b>Table 24 - Proposed Zoning Districts</b>	
Proposed Zones Districts	Acres
C-2-MU	7.3
C-0	8.8
R-A	62.5
Right-of Ways	12.2
Delft Colony HDB	90.8

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe the proposed approaches to implement the general plan recommendations contained in the Delft Colony Hamlet Plan. The following components comprise the Delft Colony Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Delft Colony Hamlet Plan, there are a variety of changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise chapter 16 of the zoning code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Delft Colony.

# DELFT COLONY HAMLET PLAN

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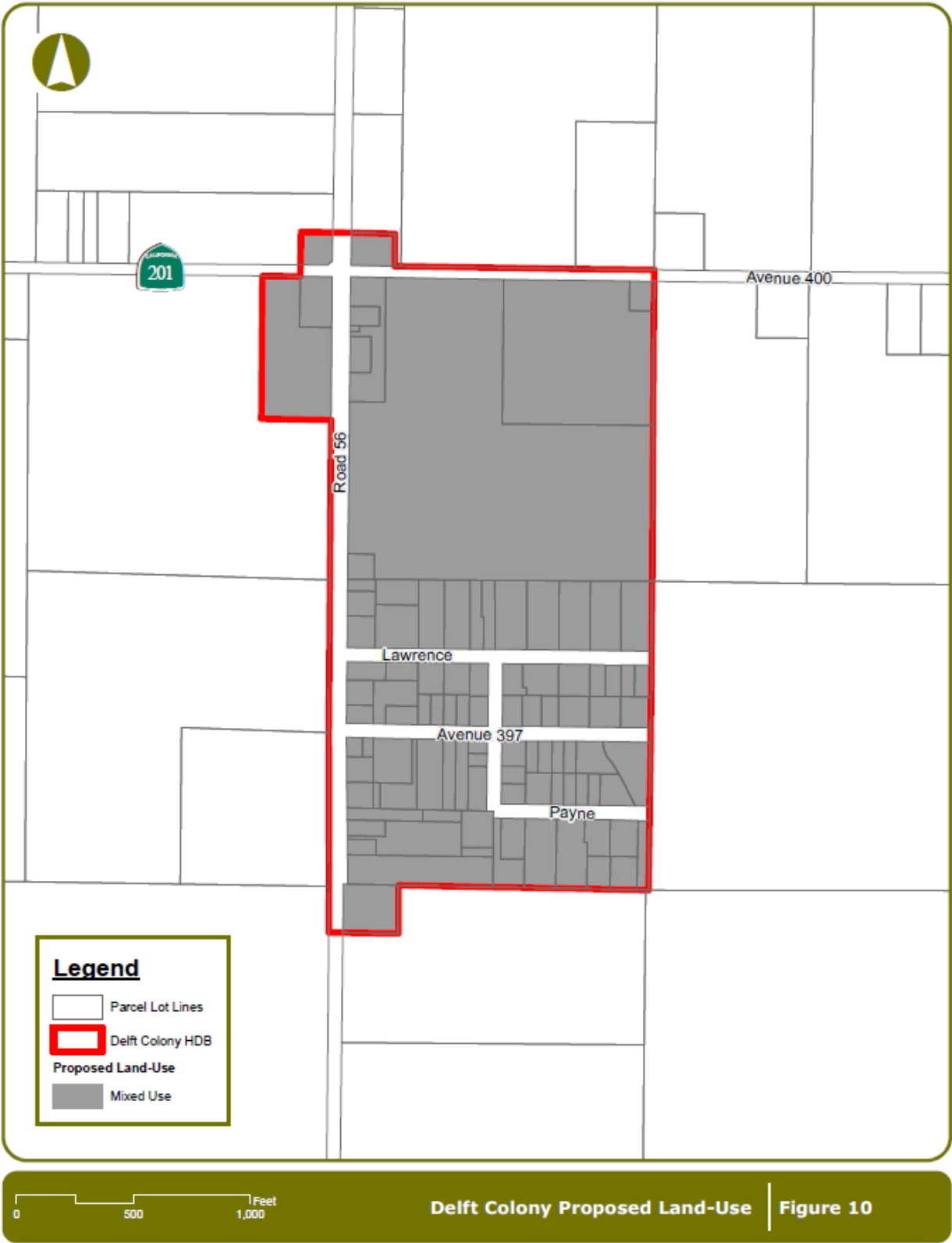
## Zoning Map Update

The current Zoning Map for Delft Colony (**see Figure 11**) will be amended to be compatible with the Land Use Map (**Figure 10**) outlined in the General Plan. There are zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**See Figure 12**) Delft Colony Proposed Zoning Districts).



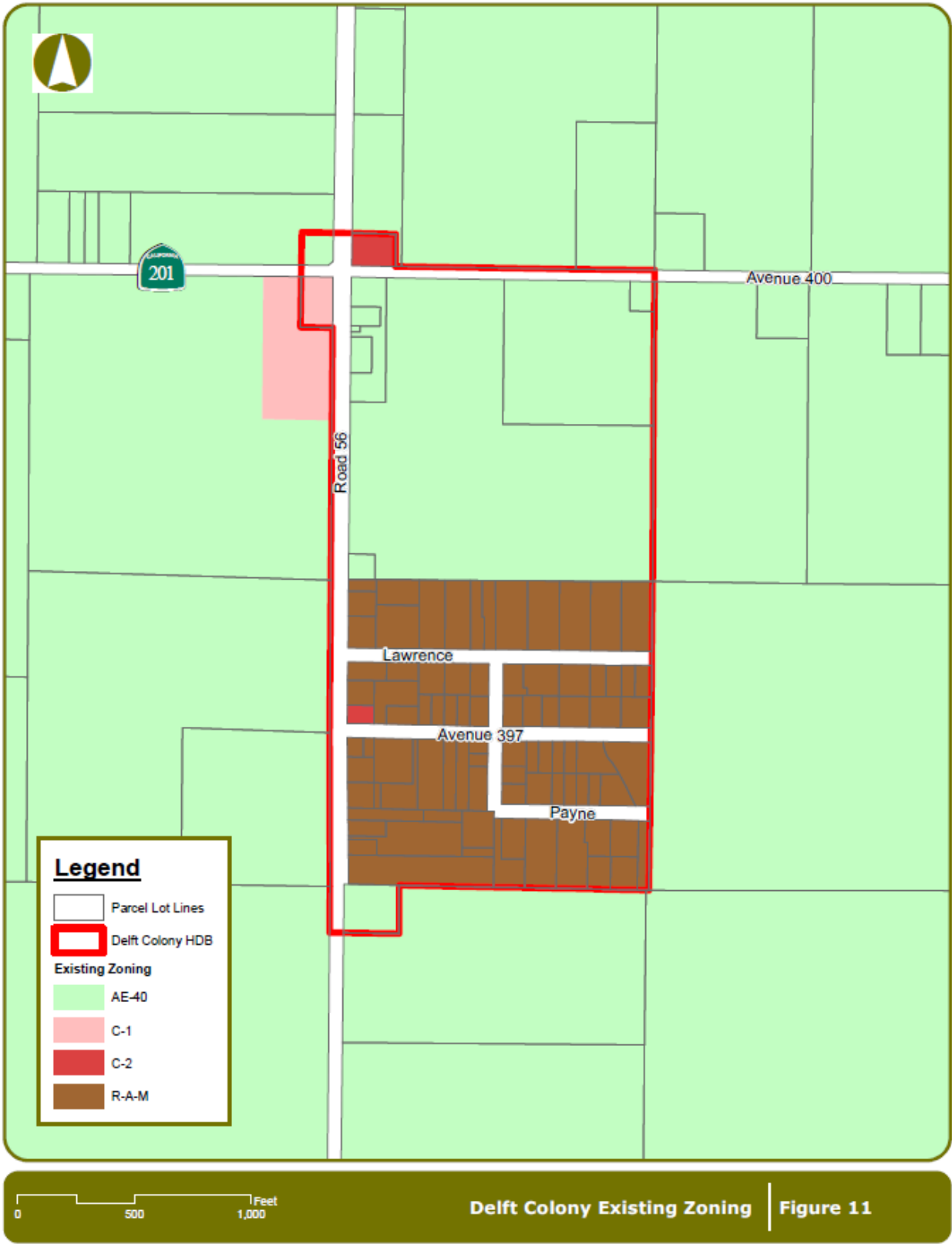
# DELFT COLONY HAMLET PLAN

Figure 10 - Proposed Land Use Plan



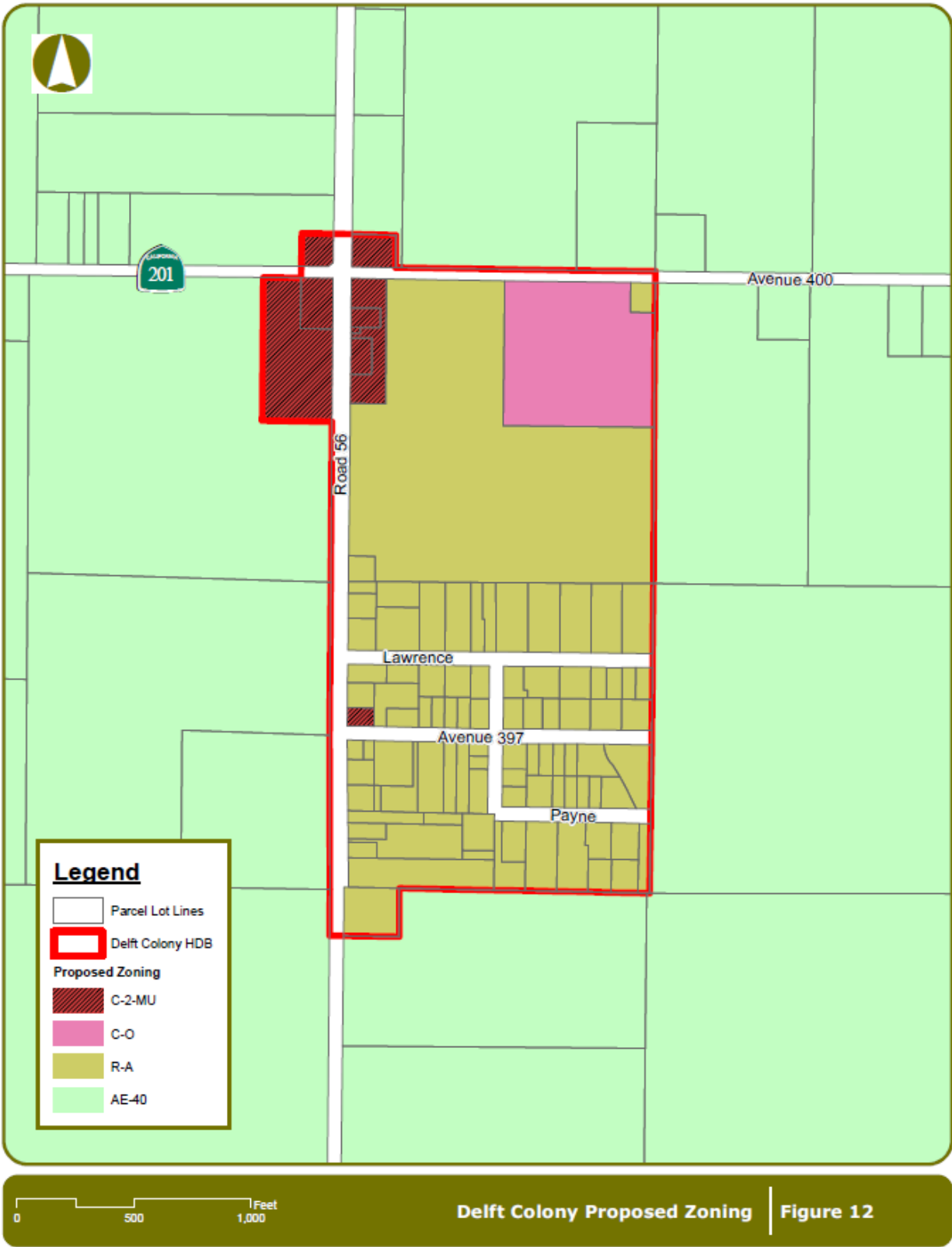
# DELFT COLONY HAMLET PLAN

Figure 11 – Existing Zoning Districts



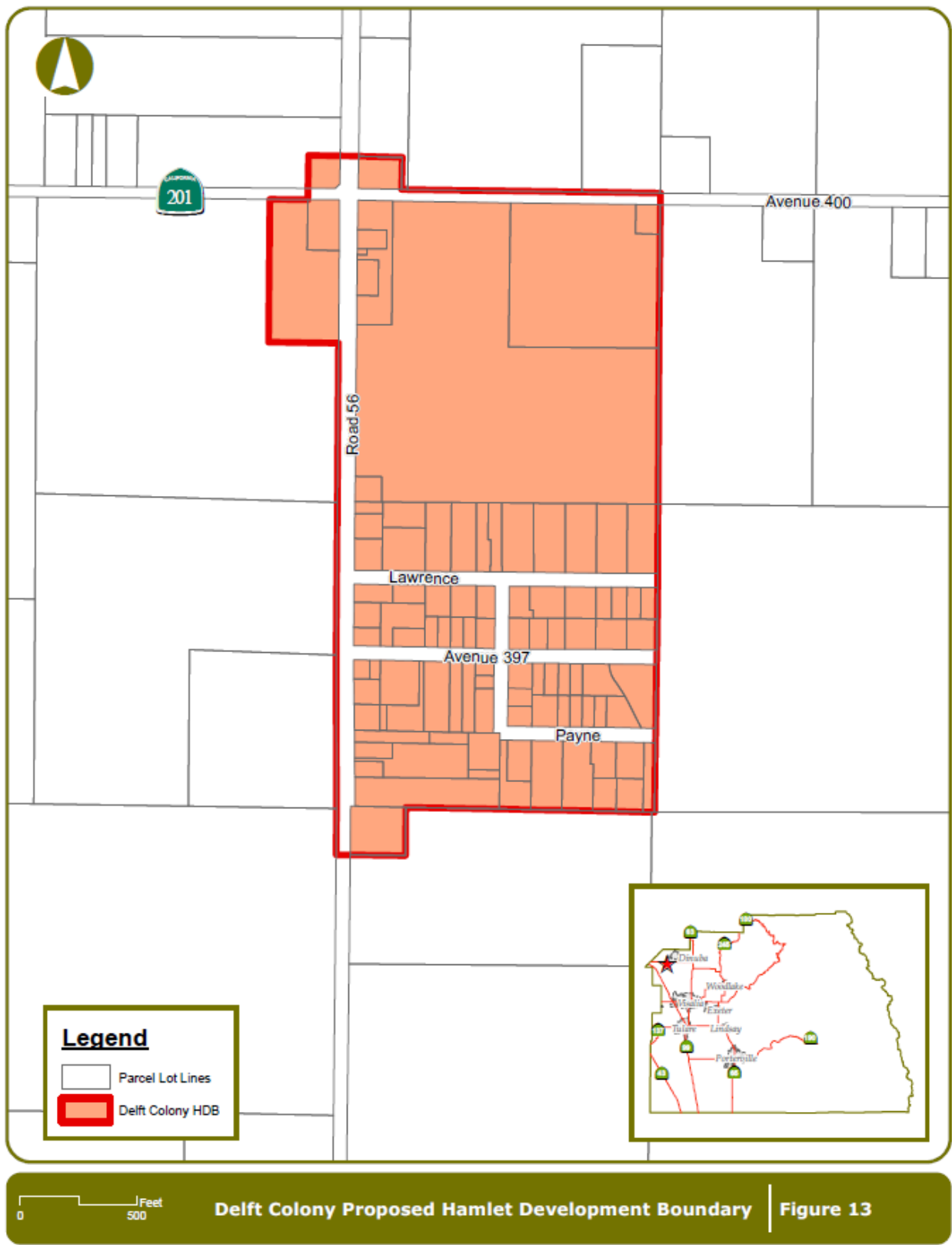
# DELFT COLONY HAMLET PLAN

Figure 12 - Proposed Zoning Districts



# DELFT COLONY HAMLET PLAN

Figure 13 - Proposed Delft Colony HDB



# DELFT COLONY HAMLET PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# DELFT COLONY HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

# DELFT COLONY HAMLET PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	



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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-	R-3, C-1,C-2, C-3,

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	1, R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles	C-2, C-3, M-1, M-2	

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of an airport).		
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1

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Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# DELFT COLONY HAMLET PLAN

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## A-2 *Mixed Use Overlay District (Zone Change Text)*

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The following regulations shall apply in the community of Delft Colony, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Delft Colony.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Delft Colony.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# DELFT COLONY HAMLET PLAN

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

# DELFT COLONY HAMLET PLAN

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Delft Colony. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

# DELFT COLONY HAMLET PLAN

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## A-3 *Development Standards (Mixed Use Zoning District)*

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To promote Economic Development within the Delft Colony Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Delft Colony. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Delft Colony Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Delft Colony. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.



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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# DELFT COLONY HAMLET PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

# DELFT COLONY HAMLET PLAN

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

# DELFT COLONY HAMLET PLAN

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

# DELFT COLONY HAMLET PLAN

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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

# DELFT COLONY HAMLET PLAN

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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.



# DELFT COLONY HAMLET PLAN

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

# DELFT COLONY HAMLET PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.



# DELFT COLONY HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:       None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT                     )  
TO SECTION 16.H OF ORDINANCE NO. 352            )  
TO ALLOW ADDITIONAL BY-RIGHT USES             )       RESOLUTION NO. 9369  
CONSISTENT WITH THE ADOPTED HAMLET            )  
PLANS, AS PROPOSED IN CHANGE                    )  
OF ZONE NO. PZC 17-039                            )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map

***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*



Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

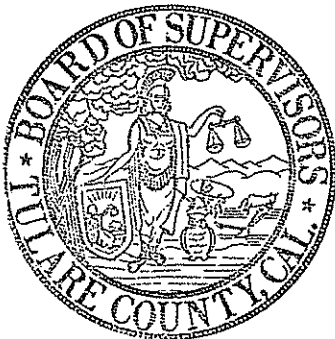
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **East Tulare Villa Hamlet Plan**

# EAST TULARE VILLA HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# EAST TULARE VILLA HAMLET PLAN

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# EAST TULARE VILLA HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.



# EAST TULARE VILLA HAMLET PLAN

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## EAST TULARE VILLA HAMLET PLAN

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# East Tulare Villa Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

East Tulare Villa Hamlet Plan: GPA 17-016  
Zoning District Map: PZC 17-016



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# EAST TULARE VILLA HAMLET PLAN

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# EAST TULARE VILLA HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# EAST TULARE VILLA HAMLET PLAN

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# EAST TULARE VILLA HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Michael Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Hector Guerra, Chief Planner, Environmental Planning

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# EAST TULARE VILLA HAMLET PLAN

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# EAST TULARE VILLA HAMLET PLAN

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# EAST TULARE VILLA HAMLET PLAN

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# EAST TULARE VILLA HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for East Tulare Villa is to develop a plan which can accurately reflect the needs and priorities of this community. East Tulare Villa is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks, etc.) and to stimulate economic development within the community.

### Location

The community of East Tulare Villa (**see Figure 1**) is located on the east side of the San Joaquin Valley and is a census-designated place located in the western portion of Tulare County. It is bounded by Bardsley Avenue in the south, State Route (SR) 137 in the north, Munson Road in the west, and Road 132 in the east and adjacent to the city of Tulare. East Tulare Villa is in Section 08 Township 20 South, Range 25 East, MDB&M, and can be found within the Tulare, United States Geological Survey 7.5 Minute topographic quadrangle. East Tulare Villa is located at an elevation of 302 feet above sea level, National Geodetic Vertical Datum. The coordinates of East Tulare Villa are: Latitude: 36.20356 and Longitude: -119.27762 (**see Figure 2**).

### Planning Area

The East Tulare Villa Hamlet Development Boundary (HDB) area consists of 178.5 acres (**see Figure 1**). The Land Uses within the HDB includes agriculture (orchards and pasture), which currently occupy 54 percent of the 178.5 acres. Urban development, including urbanized uses such as residential and commercial development occupy 18 percent of the 178.5 acres. The remaining 10 percent are lands dedicated for Right-of-way (**See Table 1**).

Table 1 - East Tulare Villa Land Use	
Land Use	Acres
Residential	32.6
Commercial	0.4
Agricultural	97.4
Rights-of-way	18.1
Total	178.5

Source: Tulare County GIS

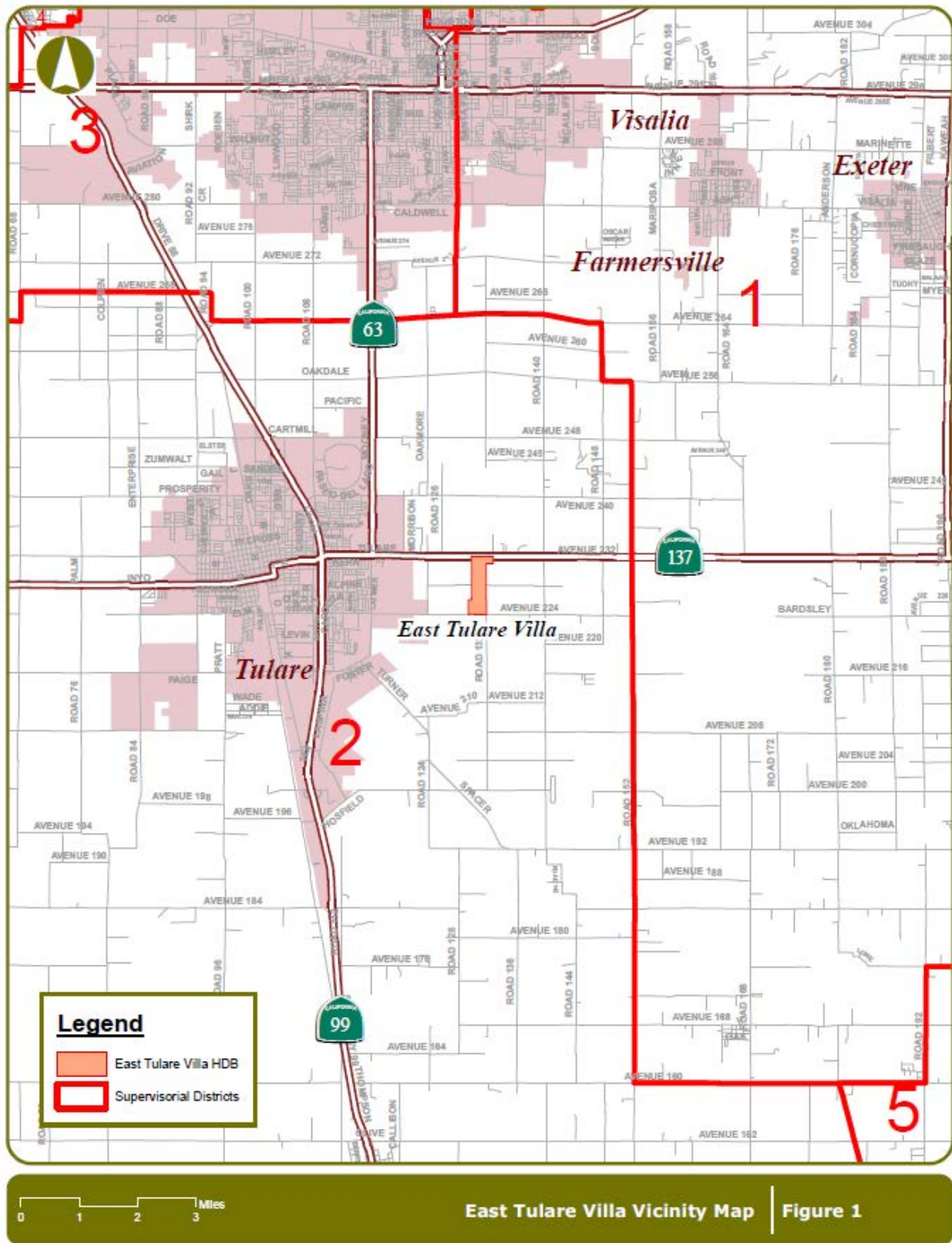
No change is proposed to the Hamlet Development Boundary.

### City of Tulare's Sphere of Influence (SOI)

The community is located adjacent to the Tulare city limits and outside the city's 2035 Urban Development Boundary. Although East Tulare Villa is within the city of Tulare's Sphere of Influence (SOI) (**see Figure 3**), the city's General Plan land use map designates the area Rural Residential 0-2.

# EAST TULARE VILLA HAMLET PLAN

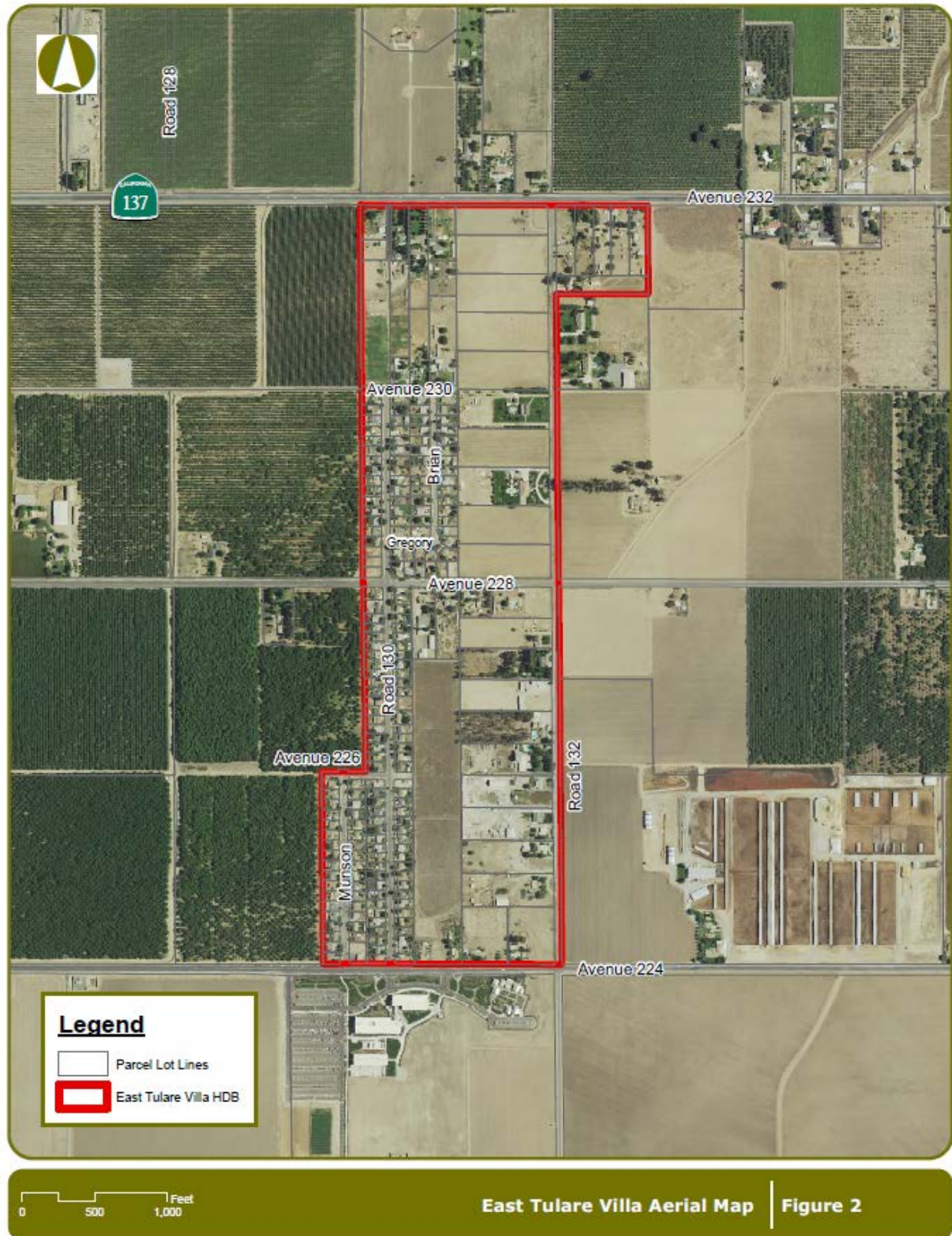
Figure 1 - Vicinity Map





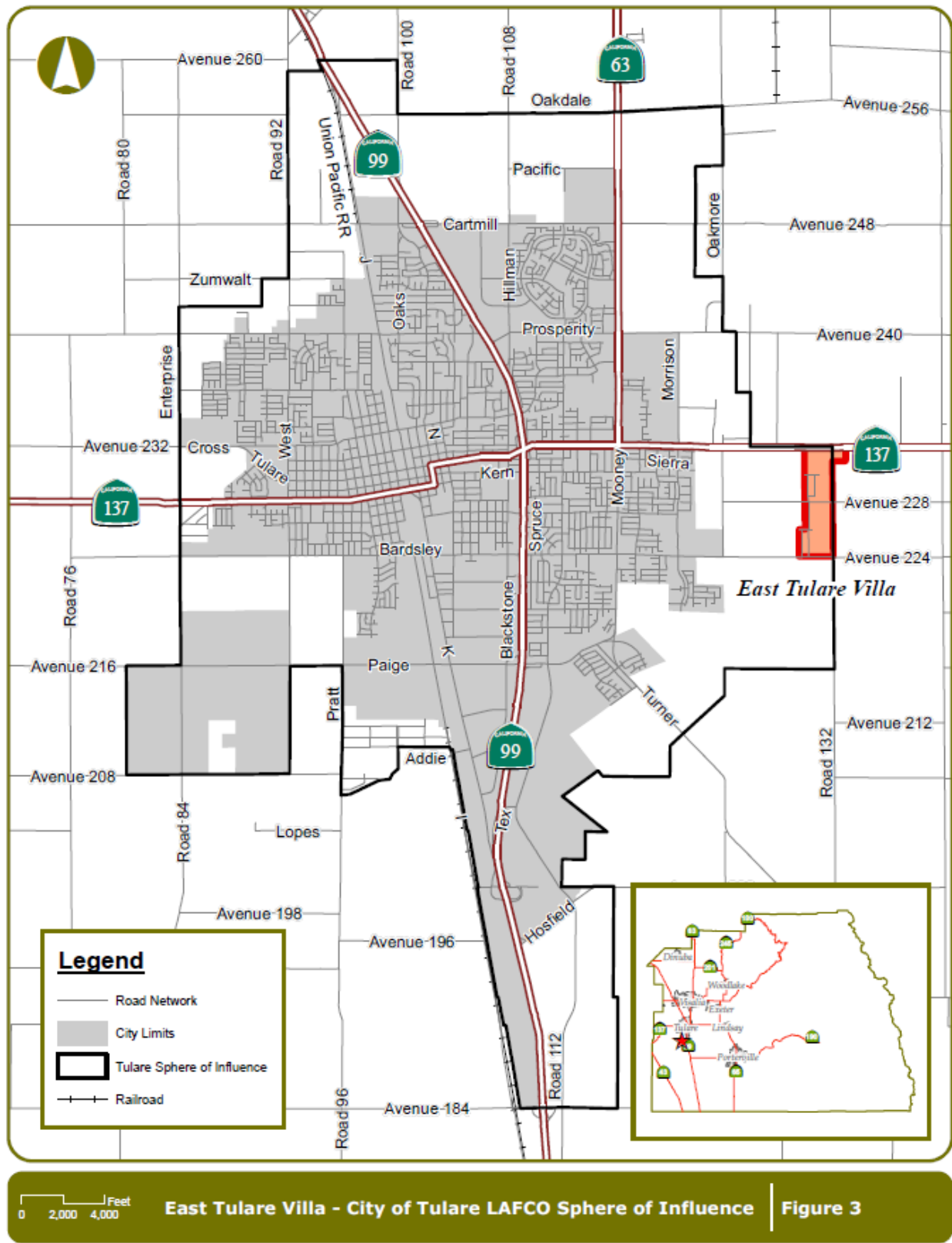
# EAST TULARE VILLA HAMLET PLAN

Figure 2 - Aerial - East Tulare Villa



# EAST TULARE VILLA HAMLET PLAN

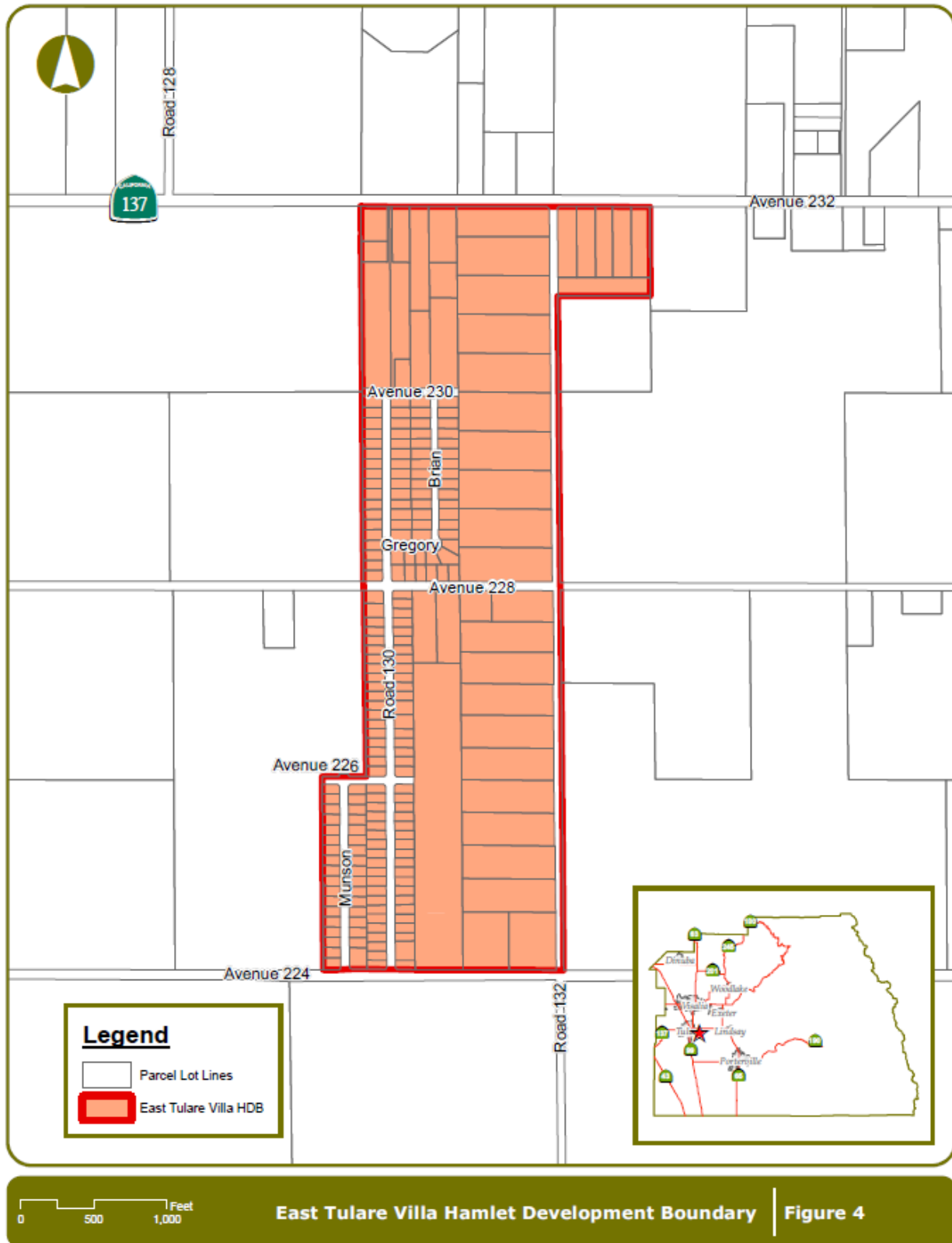
Figure 3 - City of Tulare SOI





# EAST TULARE VILLA HAMLET PLAN

Figure 4 - East Tulare Villa HDB



# EAST TULARE VILLA HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### **Recommendations/request**

1. City Transit
2. Street lights/ Traffic calming/ Road improvements
3. Shopping center (grocery stores)
4. Sewer
5. Public parks

### **Demographics update:**

The Community made a more accurate population would be 900 persons projected in E. Tulare Villa. 45K is too high of a household stated income. A more reasonable number would be 30-35K per household income. Approximate 200 housing units' just on two streets. Majority of people own their homes. There were two ladies who stated they have lived there all their lives and not many people rent in E. Tulare Villa. Average person per household is greater than 3.65. Community laughed and said, "3.65 is far below average. Six people live in my house".

### **City Transit:**

One big concern to the community was City Transit. Community stated that the bus only goes to College of the Sequoias (COS). From COS it goes back to Tulare Station. Every time the bus takes people back to the station, people need to pay again to go to the next location. People would like to be able to go directly where needed. More bus stops would need to be constructed. More shuttles to schools can create more parent involvement in the schools.

### **Septics:**

Community would like to connect to a community sewer. "This shouldn't be complicated because COS just got connected" said a community resident. The distance from Road 130 to COS is about one mile or two. Pipelines are close.

### **Streets / Roads:**

Roads the community really wished would be improved would be Road 137, Road 226 and Road 130 (Allen Rd).

### **Lighting:**

Community all agreed that their community needs more **street lights**. "At night it's too dark and dangerous to walk home from work" said a community resident. The ones that they currently have do not function now.

### **Parks:**

Live Oak and Cypress Park are the two nearest public parks to the community. It's about 6 miles away from the community. Would like a closer park where parents and kids can walk to. This can create a healthy lifestyle.

### **Schools:**

Students attend Tulare Union School and Mission Oak High School

# EAST TULARE VILLA HAMLET PLAN

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## **Markets:**

The only market the community has is the Victorian Market. Community needs an actual grocery store they can purchase for their home needs.

## **Hospital / Fire station:**

Most people go to Visalia if they need to make a hospital visit. Community feels Tulare Hospital doesn't attend to them as they should. The nearest fire station is on Foster Dr.

## **Internet Access**

Most families in East Tulare Villa do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. Without access to the internet, they are unable to do essential things such as online job hunting, applying for resources, and or providing homework help to their kids.

# EAST TULARE VILLA HAMLET PLAN

## EAST TULARE VILLA TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as East Tulare Villa.

#### Population

In 2015, East Tulare Villa's population was 951. (see Table 2)

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>East Tulare Villa</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	954	53.7%
Female	19,334,329	50.3%	226,607	49.9%	512	46.3%
Total	38,421,464		454,033		442	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project East Tulare Villa's population during the Planning Period (see Table 3).

**Table 3 - Projected Annual Growth Rate**

	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7

# EAST TULARE VILLA HAMLET PLAN

## Median Age

East Tulare Villa's population's median age is younger than the median age of both Tulare County, as well as the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.6
Tulare County	30.1
East Tulare	38.1

2011-2015 American Community Survey 5-Year Estimates

## ETHNICITY AND RACE

In 2015, approximately 40.5% of East Tulare Villa's population was White, 0% was African American, 0.3% was Native American, 2.0% were Asian, and 0.2% was two races or more (see Table 5). Approximately 57% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	East Tulare Villa	%
Total	38,066,920		451,108		954	
Hispanic or Latino (of any race)	14,534,449	38.2%	279,022	61.9%	544	57%
White (Not Hispanic)	14,905,601	39.2%	141,322	31.3%	386	40.5%
Black or African American (Not Hispanic)	2,155,929	5.7%	5,997	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	145,736	0.4%	3,241	0.7%	3	0.3%
Asian (Not Hispanic)	5,062,736	13.3%	14,268	3.2%	19	2.0%
Some other race (Not Hispanic)	81,869	0.2%	352	0.1%	0	0%
Two or more races (Not Hispanic)	1,044,136	2.7%	6,444	1.4%	2	0.2%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in East Tulare Villa

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

# EAST TULARE VILLA HAMLET PLAN

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According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for East Tulare Villa was about 1.7% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 1.7% includes only the employable labor force (that is, not every person of the population) results in about 12 unemployed persons of East Tulare Villa's person labor force of 307.

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>East Tulare Villa</b>
Population 16 years & over	30,312,429	325,404	704
In labor force	19,269,449	194,420	307
Civilian labor force	19,137,441	194,102	307
Employed	17,246,360	170,780	295
Unemployed	1,891,081	23,322	12
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	397

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

East Tulare Villa's median household income was \$50,163, compared to \$42,031 for Tulare County and \$61,818 for the State of California (see Table 7).

<b>Table 7 - 2011-2015 American Community Survey: Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$30,318
<b>Tulare County</b>	\$42,031	\$44,814	\$17,876
<b>East Tulare Villa CDP</b>	\$50,163	\$50,435	\$15,859

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, East Tulare Villa's median household income was \$50,163, whereas the State of California's median household income was \$61,818. East Tulare Villa's median household income was 81% of the State of California's median household income, and therefore it is considered a disadvantaged community.

# EAST TULARE VILLA HAMLET PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes). According to the California Department of Finance, the

Table 9 - Housing Tenure			
	California	Tulare County	East Tulare Villa
Occupied housing units	12,717,801	133,570	275
Owner-occupied	6,909,176	75,685	148
Renter-occupied	5,808,625	57,885	127

2011-2015 American Community Survey 5-Year Estimates

Table 8 - Housing Tenure (%)			
	California	Tulare County	East Tulare Villa
Owner-occupied	54.3%	56.7%	53.8%
Renter-occupied	45.7%	43.3%	46.2%

2011-2015 American Community Survey data indicated 53.8% of the housing units in East Tulare Villa was owner-occupied (see Tables 8 & 9). Similarity, 45.7% of housing units were owner occupied in Tulare County and 54.3% of the housing units were owner occupied in the State of California.

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in East Tulare Villa indicated that approximately 37% of the housing units were sound. Approximately 63% of East Tulare Villa's housing units were deteriorated and 2% were dilapidated (see Table 10).

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
East Tulare Villa	26	37	53	63	0	0	0	0	2	2	84

Source: Tulare County 2015 Housing Element

# EAST TULARE VILLA HAMLET PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	East Tulare Villa
Total housing units	13,845,790	144,792	274
Built 2014 or later	10,183	196	
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	19
Built 1990 to 1999	1,495,571	21,767	10
Built 1980 to 1989	2,117,819	22,733	3
Built 1970 to 1979	2,503,688	27,111	72
Built 1960 to 1969	1,871,029	15,500	106
Built 1950 to 1959	1,907,512	13,694	20
Built 1940 to 1949	865,607	7,494	13
Built 1939 or earlier	1,298,438	8,186	31

2010-2014 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in East Tulare Villa was 4.10 persons per household (see Table 12). The average owner occupied household size is 2.93.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
East Tulare Villa	2.93	4.10

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in East Tulare Villa was 0.0%, which was lower than both Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 6.3%, which was higher than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
East Tulare Villa	6.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)



# EAST TULARE VILLA HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

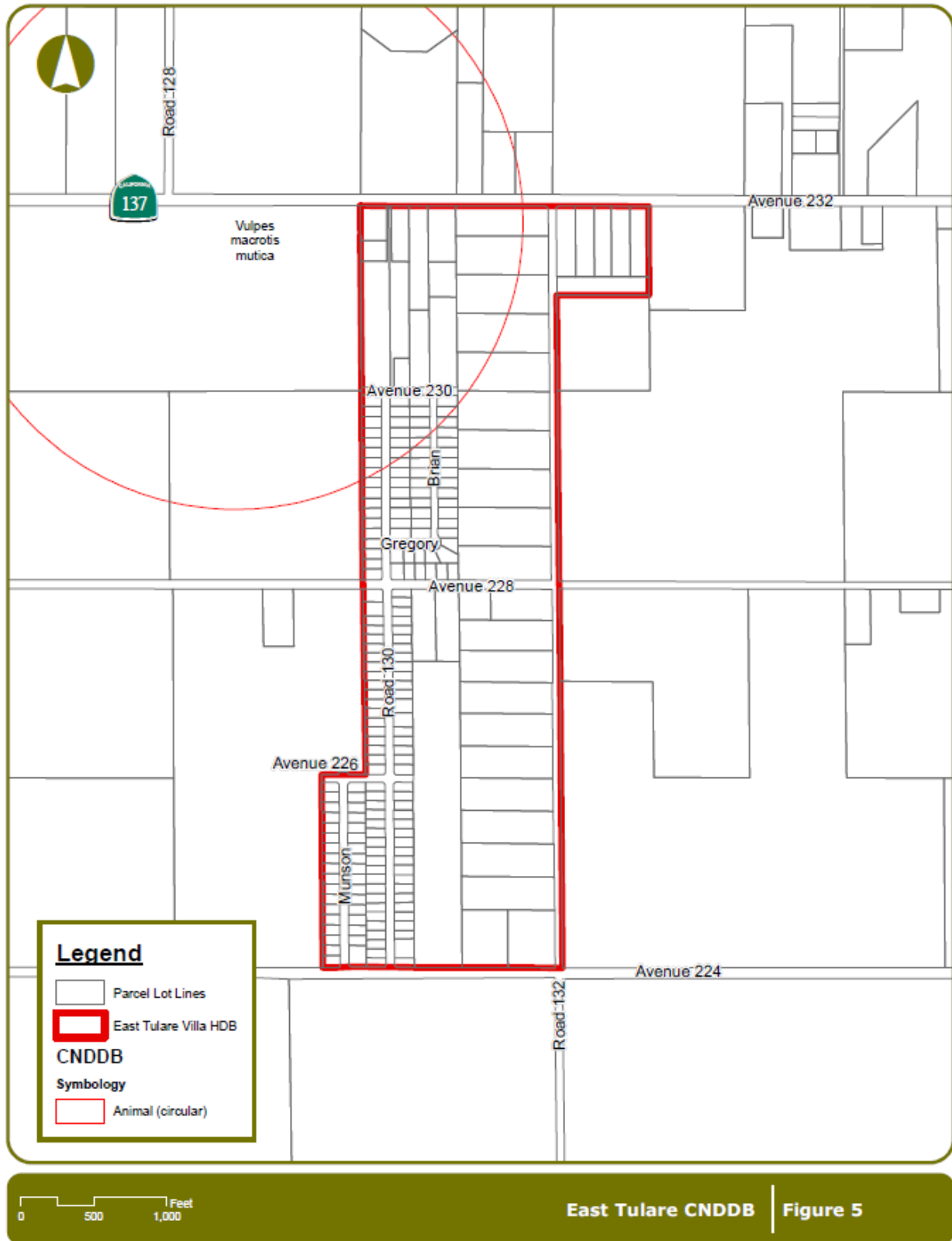
A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016 (**see Figure 5**) indicates there are special status species within the Tulare Quadrant Species List (which includes the East Tulare Villa Planning Study Area) consisting of two animal species and two plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Swainson's hawk (*Buteo Swainsoni*, state threatened); San Joaquin adobe starburst (*Pseudobahia peirsonii*, federal threatened); and California jewelflower (*Caulanthus californicus*, federal and state endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> accessed November 1, 2016

# EAST TULARE VILLA HAMLET PLAN

Figure 5 - East Tulare Villa - CNDDDB Map



# EAST TULARE VILLA HAMLET PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect East Tulare Villa are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [East Tulare Villa] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County (see **Figure 6**), the following soil types are located in East Tulare Villa. Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**Biggriz-Biggriz, saline-Sodic complex, 0 to 2 percent slopes.** The parent material of Biggriz is alluvium derived mainly from granitic rock sources. The soil is somewhat poorly drained. The permeability is moderately slow, the available water capacity is high, and the shrink-swell potential is moderate.

For building site developments, the major management factors are rare flooding hazard and the soil is highly corrosive to steel. The land capability classification: Biggriz- unit, 2s-6, irrigated: subclass, 6s, non-irrigated; Biggriz, saline-sodic--unit, 3s-6, irrigated; subclass, 7s, non-irrigated MLRA: 17.

The areas that are Biggriz soil are considered prime farmland and the areas that are saline-sodic are not considered prime farmland. The map unit is dominantly non-prime farmland.

**Calgro-Calgro, saline-Sodic, complex, 0 to 2 percent slopes,** Erosion potential is low to moderate and shrink swell potential low.

**Tujunga loamy sand, 0 to 2 percent slopes,** very deep, somewhat excessively drained soils that formed in alluvium from granitic sources. The soil is used for grazing, citrus, grapes, other fruits, and urban residential or commercial development

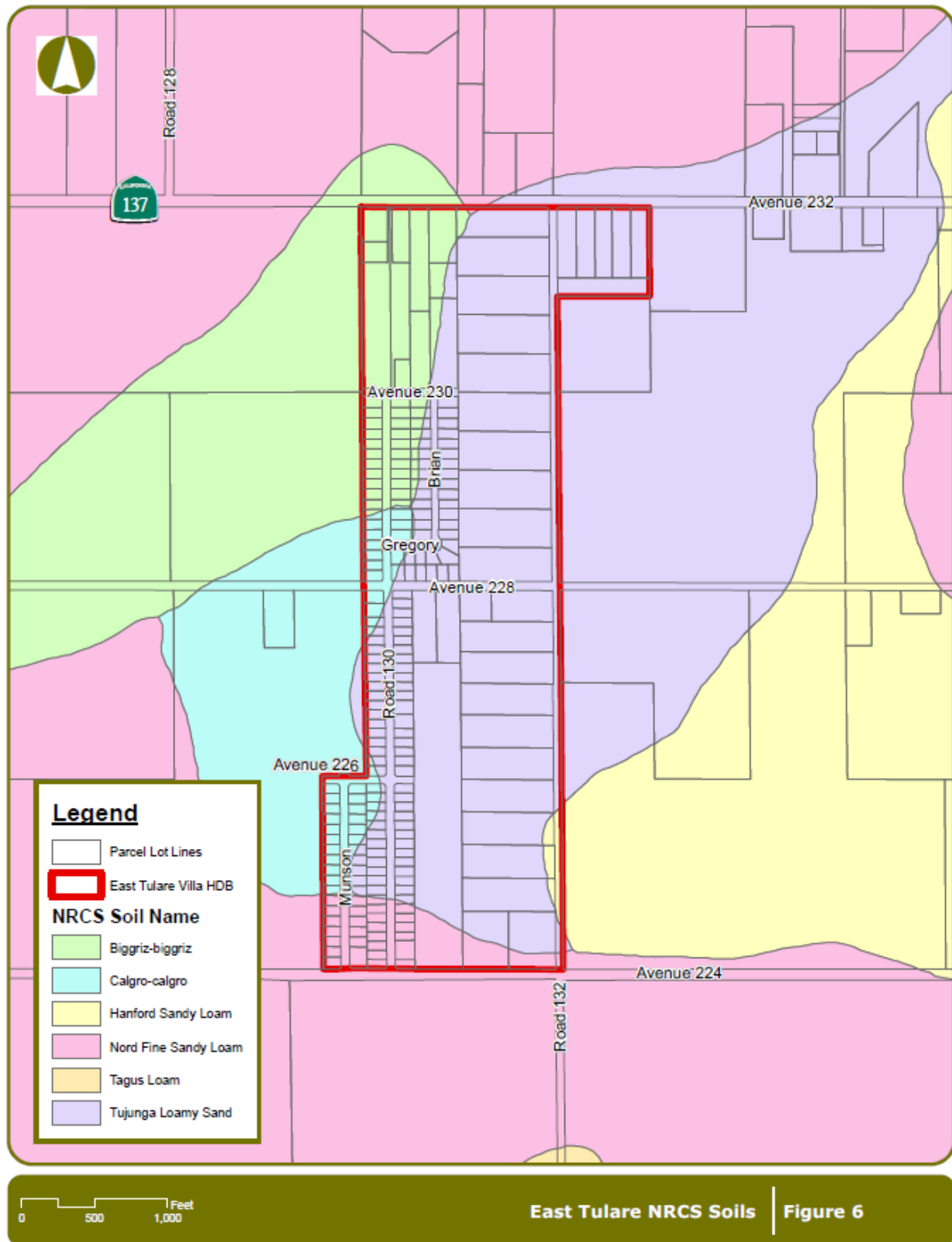
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database August 17,206.

# EAST TULARE VILLA HAMLET PLAN

Figure 6 - Soils Map, East Tulare Villa



# EAST TULARE VILLA HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The East Tulare Villa Planning Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources).<sup>6</sup> The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change

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<sup>6</sup> Tulare County General Plan Background Report, page 6-13.

# EAST TULARE VILLA HAMLET PLAN

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the natural qualities of the air. Because East Tulare Villa is located near the central portion of the Valley with prevailing winds from the northwest, it is susceptible to the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of East Tulare Villa include State Highway 137 and agricultural activities.

## Flooding<sup>7</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C1275E. East Tulare Villa is located within Flood Zone X – Areas of 0.2% annual chance flood; area of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood. East Tulare Villa is also located within the FEMA 500 year flood zone (see **Figure 7**). Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates are not required unless property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within East Tulare Villa there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

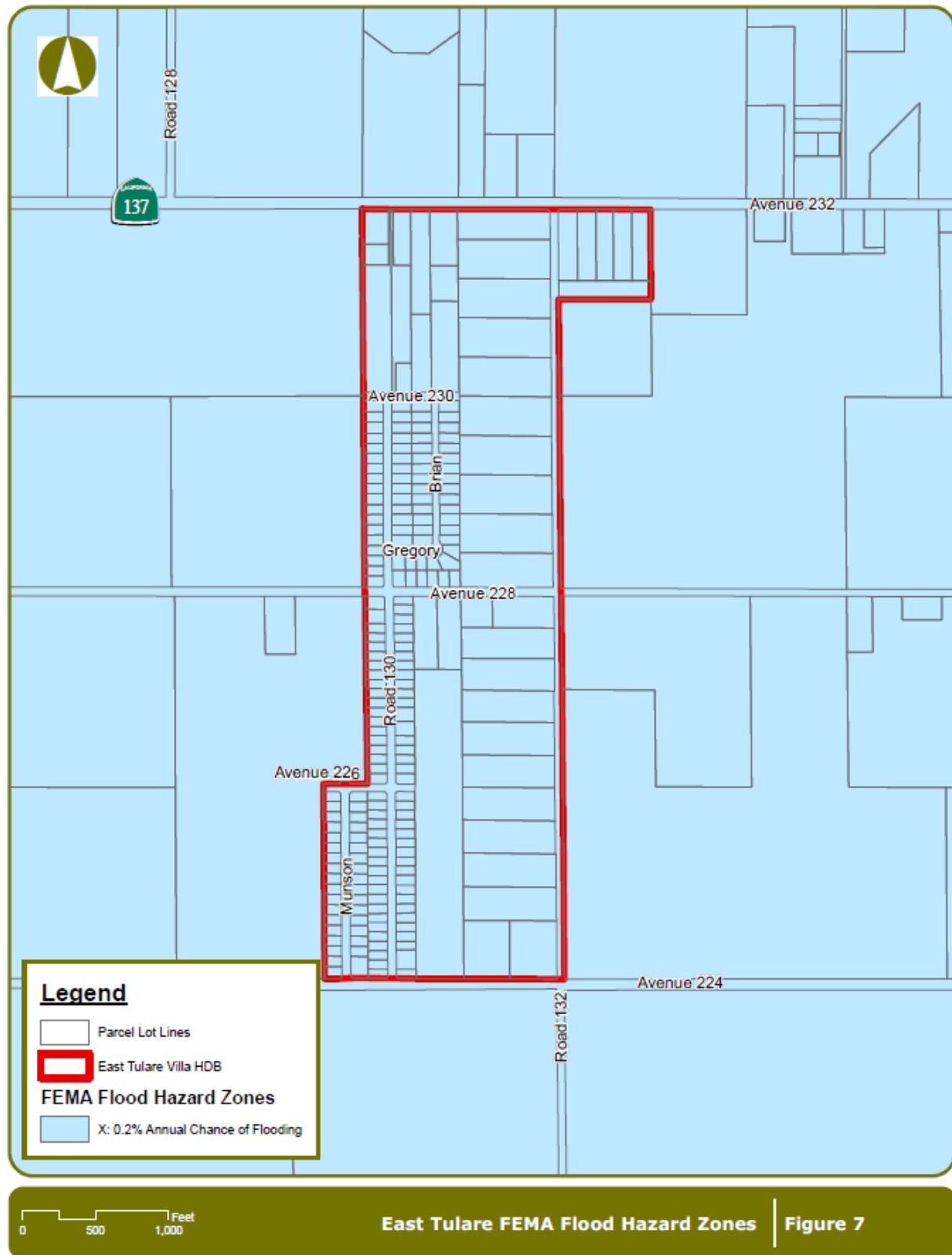
The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <http://msc.fema.gov/portal>.

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<sup>7</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C1275E, Panel Number 1275 of 2550, June 16, 2009. <http://msc.fema.gov/portal/>. Database August 17, 2016.

# EAST TULARE VILLA HAMLET PLAN

Figure 7 - FEMA Flood Map



# EAST TULARE VILLA HAMLET PLAN

## Noise<sup>8</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. **Table 14** summarizes the daily traffic volumes along State Route 137 from the State Route 63 to State Route 65. Together, these noise sources place a portion of East Tulare Villa's urbanized areas within the 60 dB Ldn noise contour. The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise.

Table 14 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
State Route 137 from State Route 63 to State Route 65	11,000	100	215	463	997

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks).

### Energy: Natural Gas/Electricity

Southern California Edison provides service to East Tulare Villa.

### Domestic Water & Wastewater

East Tulare Villa is served by CalWater's Tulco system (which is not connected to Cal Water's Visalia system). The Tulco System's water quality meets State requirements. The system is down to one functional well. CalWater is in the process of linking to the City system so that the City can provide water on an emergency basis if the last well breaks down. City water lines currently run along Bardsley

<sup>8</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)



# EAST TULARE VILLA HAMLET PLAN

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Avenue on the south border of East Tulare Villa. These lines serve the COS campus on the south side of Bardsley.<sup>9</sup>

## Sewer

No public sewer facilities are available in East Tulare Villa. Each land owner relies on septic systems for wastewater. There is no community wide sewer system in East Tulare Villa. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent.

## Storm Drainage

“East Tulare Villa does not currently have a storm drainage system. A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.”<sup>10</sup>

## Solid Waste

Solid waste disposal services for the Community of East Tulare Villa is provided by USA Waste, a private company. Solid waste generated in East Tulare Villa can be disposed of at Road 80 Landfill, located at 8614 Avenue 328.

## Roads

It is reported that road conditions are poor and most roads need work, roads have potholes, cracks and bumps. This makes residents concerned with both driving conditions and walkability. Road conditions are described as poor and most of the roads in town need work. East Tulare Villa roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not repaired.

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<sup>9</sup> City of Tulare Final Municipal Service Review, prepared by Tulare County Local Agency Formation Commission, August 2013.

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

# EAST TULARE VILLA HAMLET PLAN

There are several roadways in East Tulare Villa that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.
- ✓

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>11</sup>

**Table 15 - Roads in Need of Major and Medium Repair in East Tulare Villa**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Allen Road	Bardsley Avenue to Avenue 226	CHIP
2	Allen Road	Avenue 228 to Gregory Avenue	CHIP
3	Avenue 226	Road 129 (End) to Munson Road	CHIP
4	Avenue 226	Munson Road to Allen Road	CHIP
5	Avenue 226	Allen Road to Road 130 (End)	CHIP
6	Avenue 230	Allen Road to Brian Road	CHIP
7	Brian Road	Gregory Road to Avenue 230	CHIP
8	Gregory Avenue	Allen Road to Brian Road	CHIP
9	Munson Road	Bardsley Avenue to Avenue 226	CHIP

(Source: County of Tulare Public Works, 2012)

OLAY – overlay resurfacing operation  
CHIP – chip seal  
GRX – grind and remix

ACST – asphalt reconstruction  
RCST – cold mix reconstruction

<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element

# EAST TULARE VILLA HAMLET PLAN

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## Street Lights

Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

**Table 16** identifies the location of existing street lights that are maintained by Tulare County, in East Tulare Villa, as well as their specifications. Figure 8 also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.”<sup>12</sup>

**Table 16 - Specifications of Existing Street Lights**

Specifications of Existing Street Lights								
No.	East-West	North-South	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 226	Road 130	SE Corner	N/A	N/A	N/A	W	N/A
2	Avenue 228	Brian Road	NW Corner	N/A	5800	W	S	SCE
3	Avenue 228	Road 130	NW Corner	N/A	N/A	N/A	S	N/A
4	Bardsley Ave	Road 130	NW Corner	N/A	N/A	N/A	S	N/A
5	Gregory Ave	Brian Road	SW Corner	N/A	1982247E	W	N	SCE

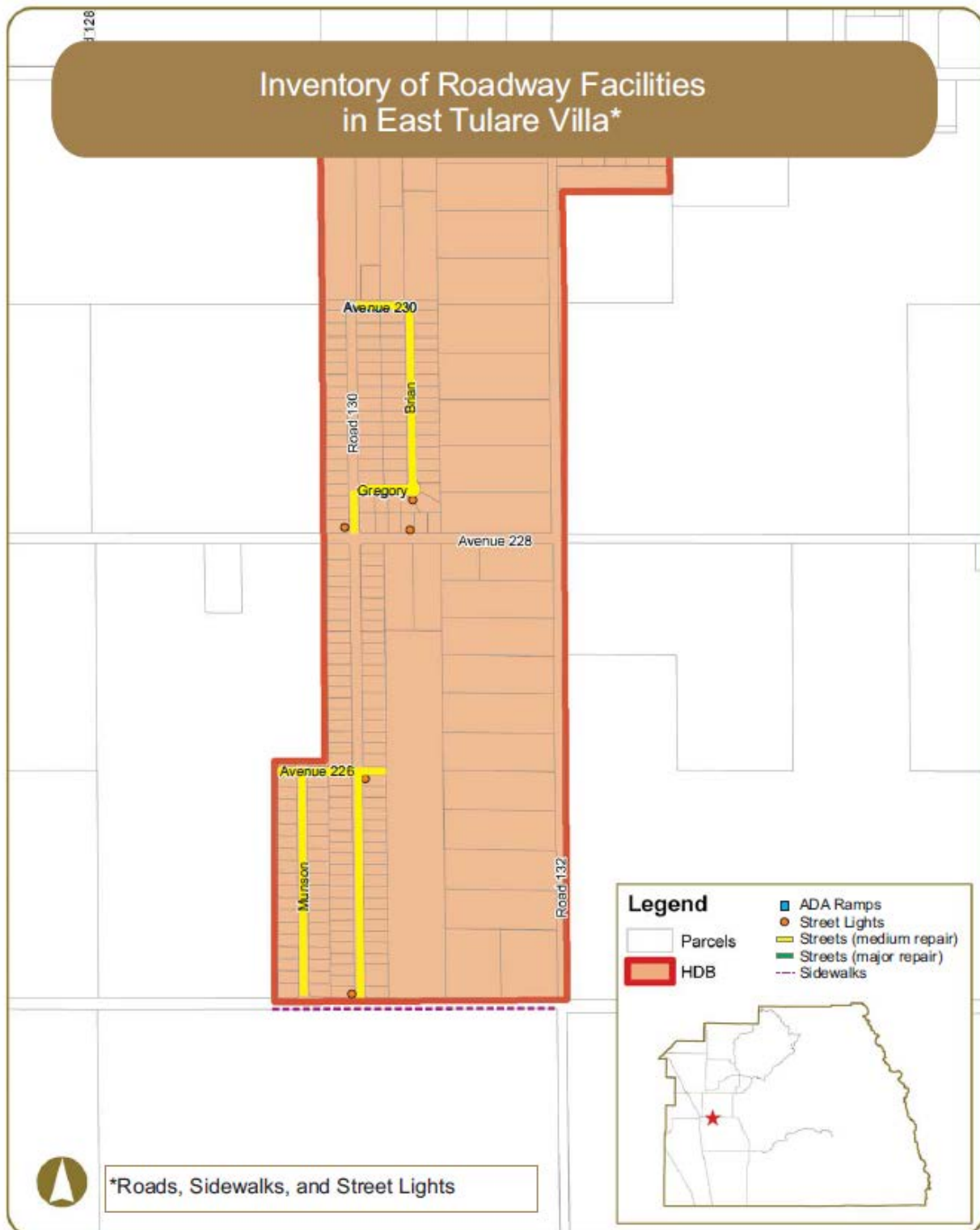
(Source: Tulare County Public Works, March 2013)

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<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element

# EAST TULARE VILLA HAMLET PLAN

Figure 8 - East Tulare Villa Inventory of Roadway Facilities



# EAST TULARE VILLA HAMLET PLAN

## Sidewalks

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

The County and VRPA Technologies surveyed existing sidewalks within the Community. **Table 17** identifies the location of existing sidewalks in East Tulare Villa. **Figure 8** also displays this information graphically. The sidewalks represented in **Table 17** and **Figure 8** do not distinguish between ADA compliant sidewalks and noncompliant sidewalks. The majority of sidewalks represented below were constructed prior to current ADA guidelines and would be considered non ADA compliant facilities. Such noncompliant facilities would require complete reconstruction to be considered ADA compliant.

**Table 17 - Existing sidewalks in East Tulare Villa**

Location of Existing Sidewalks			
No.	Roadway	Limits	Location
1	Bardsley Avenue	West of Munson Road to Road 132	South side

(Source: County of Tulare Public Works and VRPA Technologies, February 2014)

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within East Tulare Villa.”<sup>13</sup>

<sup>13</sup> Action Program 9, Tulare County 2015 Housing Element

# EAST TULARE VILLA HAMLET PLAN

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## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Public transit is not currently available in the East Tulare Villa. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in East Tulare Villa, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified SR 137 as a proposed Class III Bike Project<sup>14</sup>. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are rarely used outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is SR 137.

## AMTRAK

The Hanford AMTRAK station, located approximately 25 miles northwest of East Tulare Villa in Kings County, the closest station providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

## Mefford Field Airport

"The nearest operational General Aviation Airport is Mefford Field located in the City of Tulare approximately five (5) miles south of East Tulare Villa (**see Figure 9**). "Mefford Field is owned and managed by the City of Tulare. The one runway at the airport is 3,900 feet long. It is estimated about 70% of airport operations occur to the northwest on Runway 31. It is also estimated that about 70% of aircraft use the airport during the daytime hours (7:00 a.m. to 7:00 p.m.), 25% during the evening hours (7:00 p.m. to 10:00 p.m. and 5% during the nighttime hours (10:00 p.m. to 7:00 a.m.)"<sup>15</sup> East Tulare Villa is located outside of Mefford field's Airport Safety Zones.

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<sup>14</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

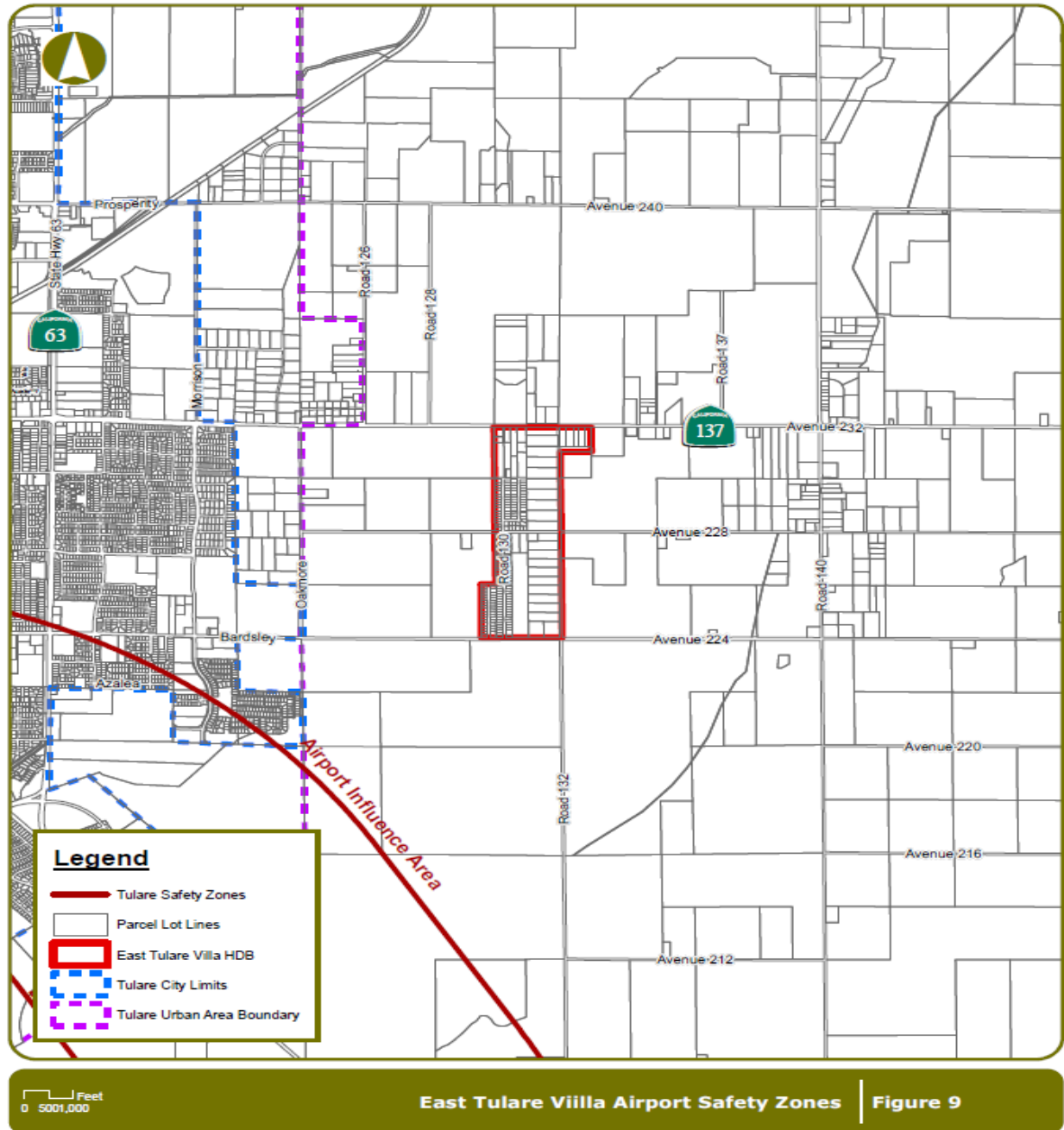
<sup>15</sup> Tulare County General Plan Background Report, page 8-64

# EAST TULARE VILLA HAMLET PLAN

## Aviation

Visalia Municipal Airport, is located approximately 15 miles north of East Tulare Villa. Fresno Yosemite International Airport (FAT), approximately 70 miles northeast of Matheny Tract, is the principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field, Bakersfield's principal commercial airport, is approximately 70 miles southeast of East Tulare Villa and offers direct flights to several destinations.

Figure 9 - Mefford Field Airport





# EAST TULARE VILLA HAMLET PLAN

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## PUBLIC SERVICES

### Community Resource Center

The community lacks resources and needs a facility where residents can access information and county services. People in East Tulare Villa expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used for homework assistance, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service East Tulare Villa. This could also be the place where residents access a reliable internet connection.

### Sheriff

Police protection services are provided in East Tulare Villa by the Tulare County Sheriff's Department main Sheriff's Office located at 2404 W. Burrell Avenue, in Visalia, approximately 10 miles north of East Tulare Villa.

### Fire

No fire hydrants are found in East Tulare Villa. Fire protection and emergency medical services are provided for East Tulare Villa by the Tulare County Fire Department. There are three City fire stations and one County fire station in the Tulare area. County Fire Station #25 is within Tulare City limits. The community is served by Tulare Fire Station #25 located at 2082 Foster Drive, in Tulare. Engine 25, Water Tender 25 and Squad 25 are assigned to this location.

### Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The East Tulare Villa Hamlet Plan Area is within the Tulare City Elementary School District. The District provides K-8 grade education in the western region of Tulare County. High School students attend high school in Tulare. East Tulare Villa is within the Tulare Joint Union High School District.

### Parks

There are no County owned/operated public parks in East Tulare Villa. The closest public park to the community is Mooney Grove Park, located 10 miles north of East Tulare Villa. This facility is operated by the County of Tulare and features picnic areas, playground, and baseball diamonds. The City of Tulare is located one mile west of East Tulare Villa.

### Library

"The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main

**Table 18 - Library Location & Hours**

Branch	Address	Service Hours (2017)
Visalia	Main Branch 200 West Oak Avenue Visalia, CA 93291	Tuesday - Thursday 9:00 am – 8:00 pm Friday: 12:00 pm – 6:00 pm Saturday: 9:00 am – 5:00 pm

Library hours current as of June 2017



# EAST TULARE VILLA HAMLET PLAN

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branch.”<sup>16</sup> The Visalia Main Branch Library is located in Visalia 25 miles away (see Table 18). Tulare Public Library is located five miles to the west, in the City of Tulare.

## THE EAST TULARE VILLA HAMLET PLAN

### CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another. East Tulare Villa is a small agricultural community located adjacent to State Route 137. The major rural collector roads is Avenue 308. There are no proposed major streets as part of this Hamlet Plan.

The 2014 Regional Transportation Plan and projections for Year 2040 traffic volumes do not exceed the LOS “C” threshold. Less than significant impacts will occur to SR 43 as a result of the Project primarily because increased traffic loads on SR 43, and its various entry and exit point, is more associated with growth in the Central Valley and the increased need for Californians to be able to travel north and south. Both the Highway Capacity Manual (HCM) and the Tulare County General Plan (TCGP) LOS remain at B or C through Year 2040 projections while the build-out time frame of the Community Plan is Year 2030.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>17</sup>

#### Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>18</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

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<sup>16</sup> General Plan Background Report, page 7-96

<sup>17</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>18</sup> Tulare County General Plan Background Report, page 5-7.

# EAST TULARE VILLA HAMLET PLAN

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**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

# EAST TULARE VILLA HAMLET PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the East Tulare Villa Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for East Tulare Villa to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

East Tulare Villa Hamlet Plan, and input received from East Tulare Villa citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

##### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

##### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
2. Locate high density residential uses in close proximity to planned shopping areas. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
3. The County shall work with the Schools to provide safe routes to school.
4. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the East Tulare Villa Community Plan.

# EAST TULARE VILLA HAMLET PLAN

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5. The East Tulare Villa Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.
6. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

**Housing**

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.

4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in East Tulare Villa.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in East Tulare Villa.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.

# EAST TULARE VILLA HAMLET PLAN

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4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

#### Policies:

1. Encourage the East Tulare Villa to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

#### Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## **Environmental Quality and Public Safety**

### **GOAL I: Preserve and enhance the quality of life for present and future generation of East Tulare Villa citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

# EAST TULARE VILLA HAMLET PLAN

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## Objective: Protect Agricultural Lands:

Land within the respective East Tulare Villa, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of East Tulare Villa.

## Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of East Tulare Villa.

# EAST TULARE VILLA HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 178.5 acre East Tulare Villa Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 54 percent of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 951 by 178.5 urbanized acres (area within the HDB), which equals 5.3 persons per urbanized acre (**see Tables 19 and 20**) showing population projections). Projecting the population at a 1.3% growth rate adds 203 persons in year 2030. Multiplying the 203 persons by the 1/3 ratio suggests an additional 2.6 acres of land will be required to accommodate development by the year 2030.

### Population Growth Forecast

<b>Growth Rate</b>	<b>2015</b>	<b>2020</b>	<b>2030</b>
0.013	951	1,014	1,154

<b>Year</b>	<b>Population</b>	<b>Growth (%)</b>
2015	951	0.013
2016	963	0.013
2017	976	0.013
2018	989	0.013
2019	1,001	0.013
2020	1,014	0.013
2021	1,028	0.013
2022	1,041	0.013
2023	1,055	0.013
2024	1,068	0.013
2025	1,082	0.013
2026	1,096	0.013
2027	1,110	0.013
2028	1,125	0.013
2029	1,139	0.013
2030	1,154	0.013

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>19</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2015 the community had 275 dwelling units (including vacant dwellings) with a population of 951. At an annual growth rate of 1.3%, the projected housing units are 293 and 334 in years 2020 and 2030, respectively, and projected population is 1,014 and 1,154 in years 2020 and 2030, respectively.

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

<sup>19</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).



# EAST TULARE VILLA HAMLET PLAN

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## Infrastructure

In order for more development to occur, water service levels need to be expanded, for which grant funding will be required.

## Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require a discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

## Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce

Table 21 - Education Attainment			
Educational Attainment	California	Tulare County	East Tulare Villa
Population 25 years and over	24,865,866	258,046	588
Less than 9th grade	10.1%	20.8%	17.7%
9th to 12th grade, no diploma	8.4%	11.2%	13.6%
High school graduate (& equivalency)	20.7%	25.0%	26.9%
Some college, no degree	22.0%	22.3%	34.2%
Associate's degree	7.8%	7.5%	6.6%
Bachelor's degree	19.6%	8.9%	1.0%
Graduate or professional degree	11.4%	4.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates

Development Partners include Proteus Inc., and CSET. Based on the 2011-2015 American Community Survey (see **Table 21**), the educational barrier in East Tulare Villa begins in grade school. Of the adults age 25 and older, 17.7% had an educational level of less than 9<sup>th</sup> grade. A lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

## Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are in the City of Tulare. There are two hospital located in close proximity to East Tulare Villa. Tulare Regional Medical Center located approximately 5 miles to the west, in the City of Tulare and Kaweah Delta located approximately 20 miles to the northeast, in the City of Visalia.



# EAST TULARE VILLA HAMLET PLAN

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 22**) and Zoning Plan District (see **Table 23**) have been updated.

<b>Table 22 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	160.4
Right-of Ways	18.1
East Tulare Villa HDB	178.5

<b>Table 23 - Proposed Zoning Districts</b>	
Proposed Zones	Acres
C-2-MU	17.0
R-1	138.1
R-A-43	12.8
R-A-65	9.25
Right-of Ways	140.80
East Tulare Villa HDB	178.5

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations contained in the East Tulare Villa Hamlet Plan. The following components comprise the East Tulare Villa Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the East Tulare Villa Hamlet Plan, there are several proposed changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of East Tulare Villa.

# EAST TULARE VILLA HAMLET PLAN

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## Zoning and Land Use Map Update

The current Zoning Map for East Tulare Villa (**see Figure 12**) will be amended to be compatible with the Land Use Map (**see Figure 11**) outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 13**).

## City of Tulare's Land Use Designation and Zoning

Currently, the community is located adjacent to the Tulare City Limits and outside the city's 2035 Urban Development Boundary (**see Figure 4 Above**). Although East Tulare Villa is within the city of Tulare's Sphere of Influence (SOI) (**see Figure 3**), the city's General Plan land use map (**see Figure 10**) designates the area Rural Residential 0-2; however, Tulare County has not formally adopted the City's Urban Development Boundary, or zoning, and the Tulare County zoning still applies to the property within East Tulare Villa regardless (and there was no pre-zoning of the East Tulare Villa by the City of Tulare, at the time of this Plan).

# EAST TULARE VILLA HAMLET PLAN

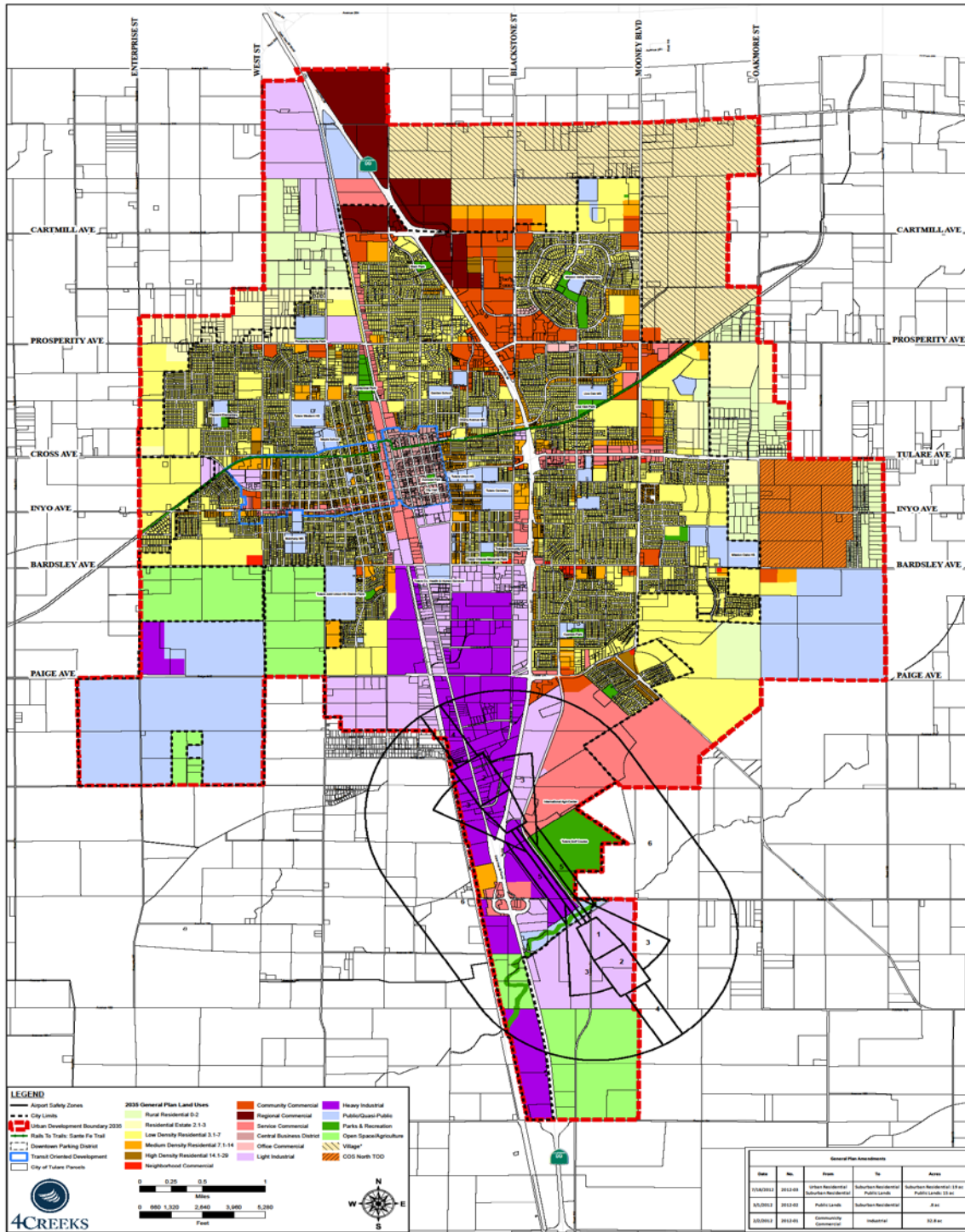
Figure 10 - City of Tulare 2035 General Plan Map



## CITY OF TULARE 2035 General Plan

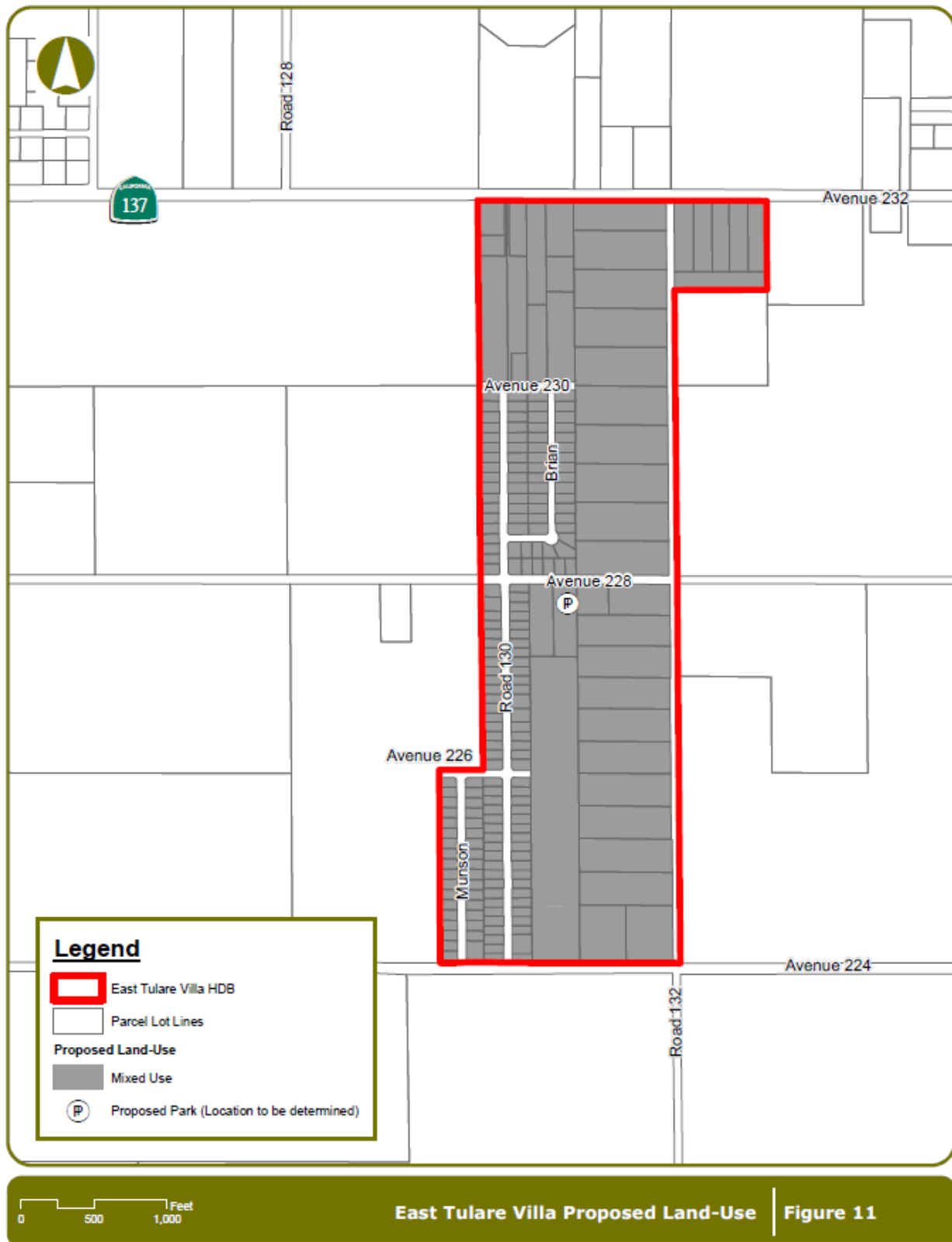
Updated As Of: 4/24/2015

"The General Plan Map was compiled and integrated from the most current and accurate sources generally available at the time of publication. Please contact the Community Development Department for any interpretations or clarifications of zoning boundaries when in question."



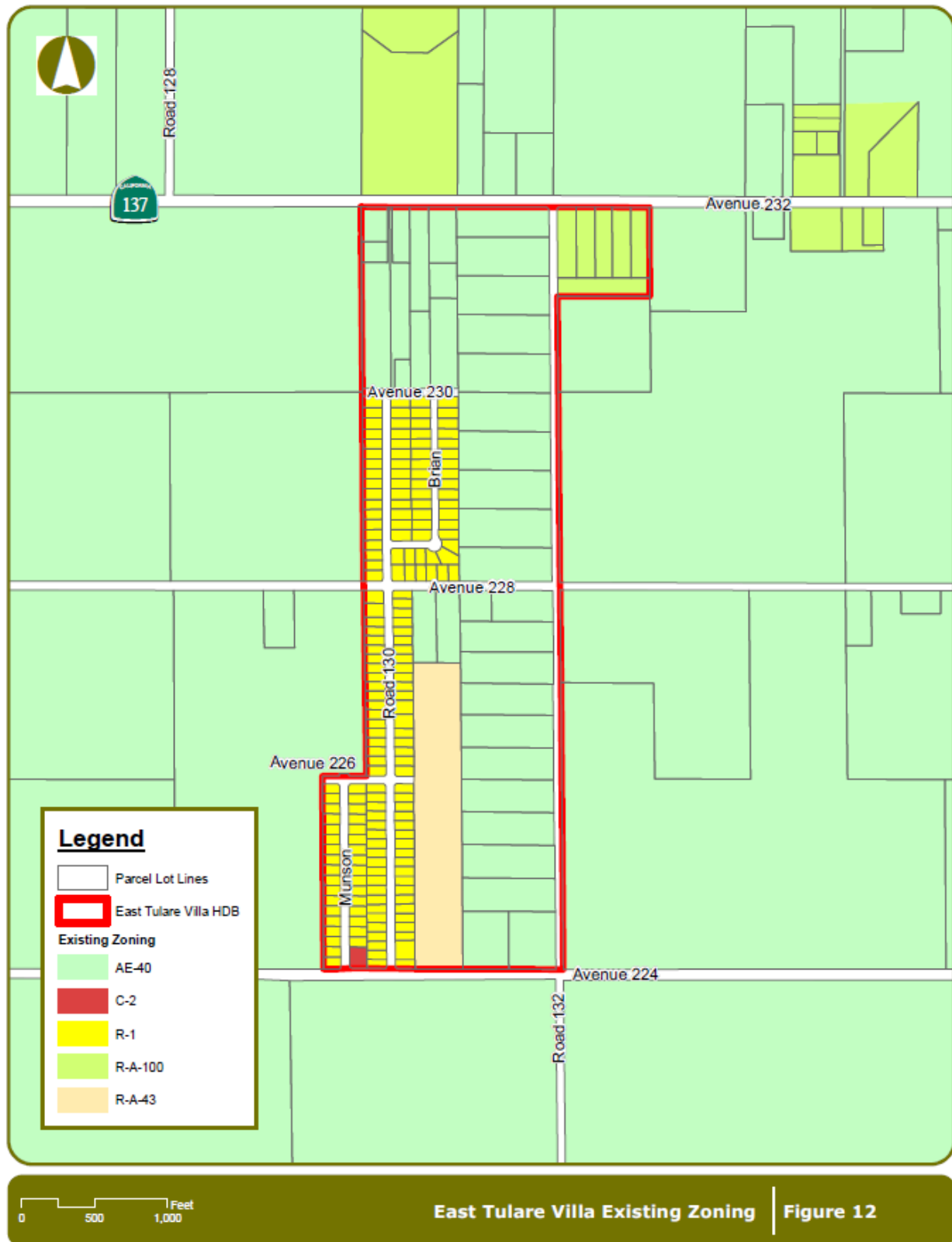
# EAST TULARE VILLA HAMLET PLAN

Figure 11 - Proposed Land Use Plan



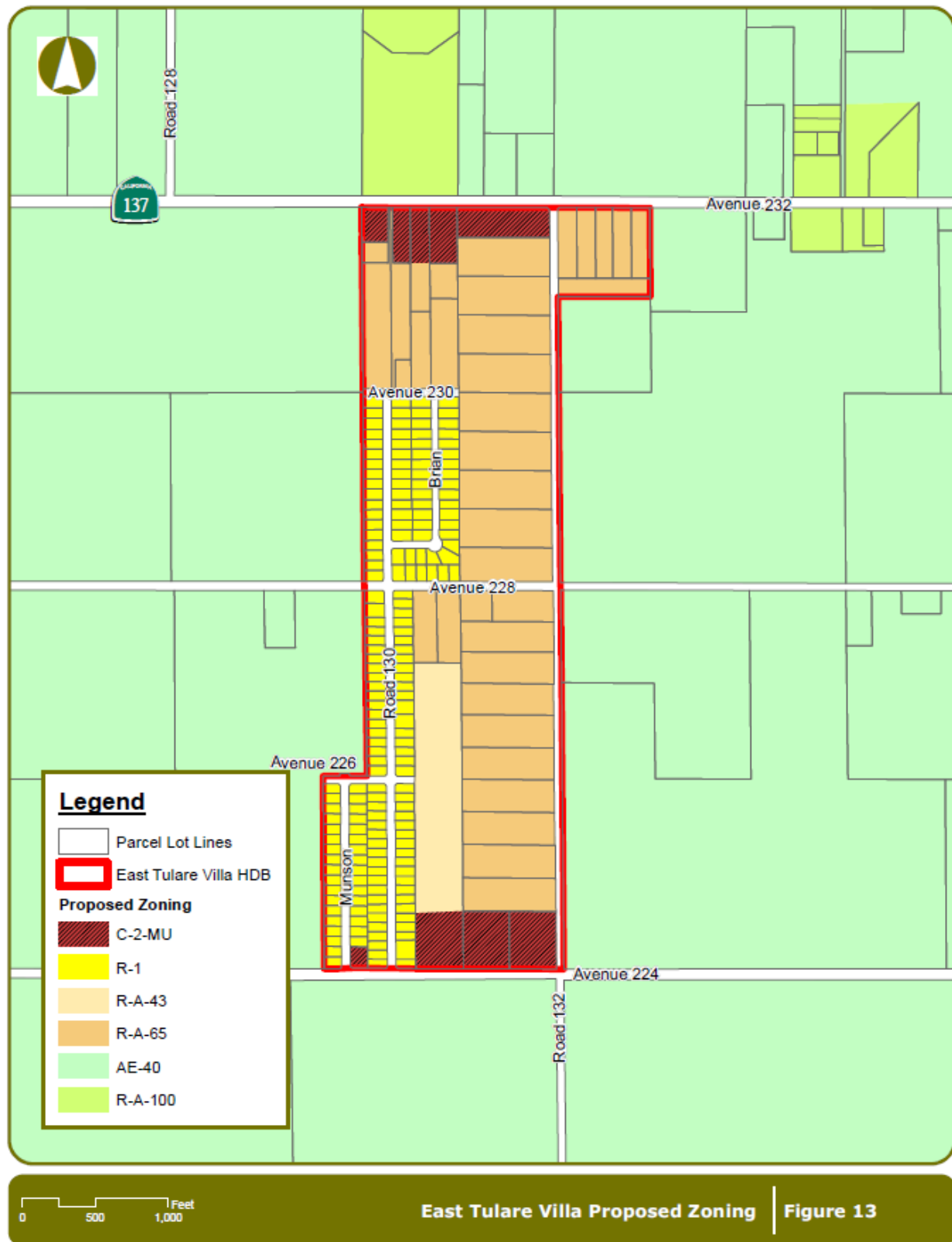
# EAST TULARE VILLA HAMLET PLAN

Figure 12 - Existing Zoning Plan



# EAST TULARE VILLA HAMLET PLAN

Figure 13 - Proposed Zoning Districts



# EAST TULARE VILLA HAMLET PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# EAST TULARE VILLA HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	



# EAST TULARE VILLA HAMLET PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft..	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2,C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2,C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# EAST TULARE VILLA HAMLET PLAN

Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

# EAST TULARE VILLA HAMLET PLAN

Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1

# EAST TULARE VILLA HAMLET PLAN

Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1

# EAST TULARE VILLA HAMLET PLAN

Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1

# EAST TULARE VILLA HAMLET PLAN

Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# EAST TULARE VILLA HAMLET PLAN

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## A-2 *Mixed Use Overlay District (Zone Change Text)*

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The following regulations shall apply in the community of East Tulare Villa, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of East Tulare Villa.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for East Tulare Villa.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# EAST TULARE VILLA HAMLET PLAN

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet



# EAST TULARE VILLA HAMLET PLAN

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for East Tulare Villa. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

# EAST TULARE VILLA HAMLET PLAN

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## A-3 *Development Standards (Mixed Use Zoning District)*

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To promote Economic Development within the East Tulare Villa Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within East Tulare Villa. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the East Tulare Villa Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within East Tulare Villa. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

# EAST TULARE VILLA HAMLET PLAN

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# EAST TULARE VILLA HAMLET PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link

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fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

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WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.



# EAST TULARE VILLA HAMLET PLAN

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SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.



# EAST TULARE VILLA HAMLET PLAN

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

# EAST TULARE VILLA HAMLET PLAN

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# EAST TULARE VILLA HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning



District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:        None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable



Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9369
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-039	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:



AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map

***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## Lindcove Hamlet Plan

# LINDCOVE HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division





# LINDCOVE HAMLET PLAN

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# LINDCOVE HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# LINDCOVE HAMLET PLAN

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# LINDCOVE HAMLET PLAN

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## Lindcove Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Lindcove Hamlet Plan: GPA 17-017  
Zoning District Map: PZC 17-017



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# LINDCOVE HAMLET PLAN

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# LINDCOVE HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# LINDCOVE HAMLET PLAN

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# LINDCOVE HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Michael Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Hector Guerra, Chief Planner, Environmental Planning

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I



# LINDCOVE HAMLET PLAN

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# LINDCOVE HAMLET PLAN

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# LINDCOVE HAMLET PLAN

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# LINDCOVE HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Lindcove is to develop a plan which can accurately reflect the needs and priorities of this community. Lindcove is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding (drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks) and to stimulate economic development within the community.

Lindcove is a census-designated place located in the northern portion of Tulare County. It is bounded by Avenue 312 in the south, Boston Avenue in the north, Road 226 in the west, and Road 228 in the east and encompasses 0.7 square miles of land. It is not directly served by any State Route.

### Location

The community of Lindcove (**see Figure 1**) is located on the east side of the San Joaquin Valley, and a census-designated place located in the central east portion of Tulare County. It is bounded by Avenue 312 in the south, Boston Avenue in the north, and Road 226 in the west, and Road 228 in the east and encompasses 0.7 square miles of land. It is not directly served by any State Route. Lindcove is located roughly five (5) miles northeast of the city of Exeter in Tulare County. Lindcove is located in Section 17 Township 18 South, Range 27 East, MDB&M, and can be found within Rocky Hill United States Geological Survey 7.5 minute topographic quadrangle. Lindcove is located at an elevation of 453 feet National Geodetic Vertical Datum. The coordinates of Lindcove are: Latitude: 36.3595, Longitude: -119.0721 (**see Figure 2**).

### Planning Area

The Lindcove Hamlet Development Boundary (HDB) area consists of 73.3 acres (**See Figure 3**). The Land Uses within the HDB include agricultural activities (orchards and pasture), which currently occupy 76 percent of the 73.3 acres. The remaining 23 percent are lands dedicated for Right-of-way.

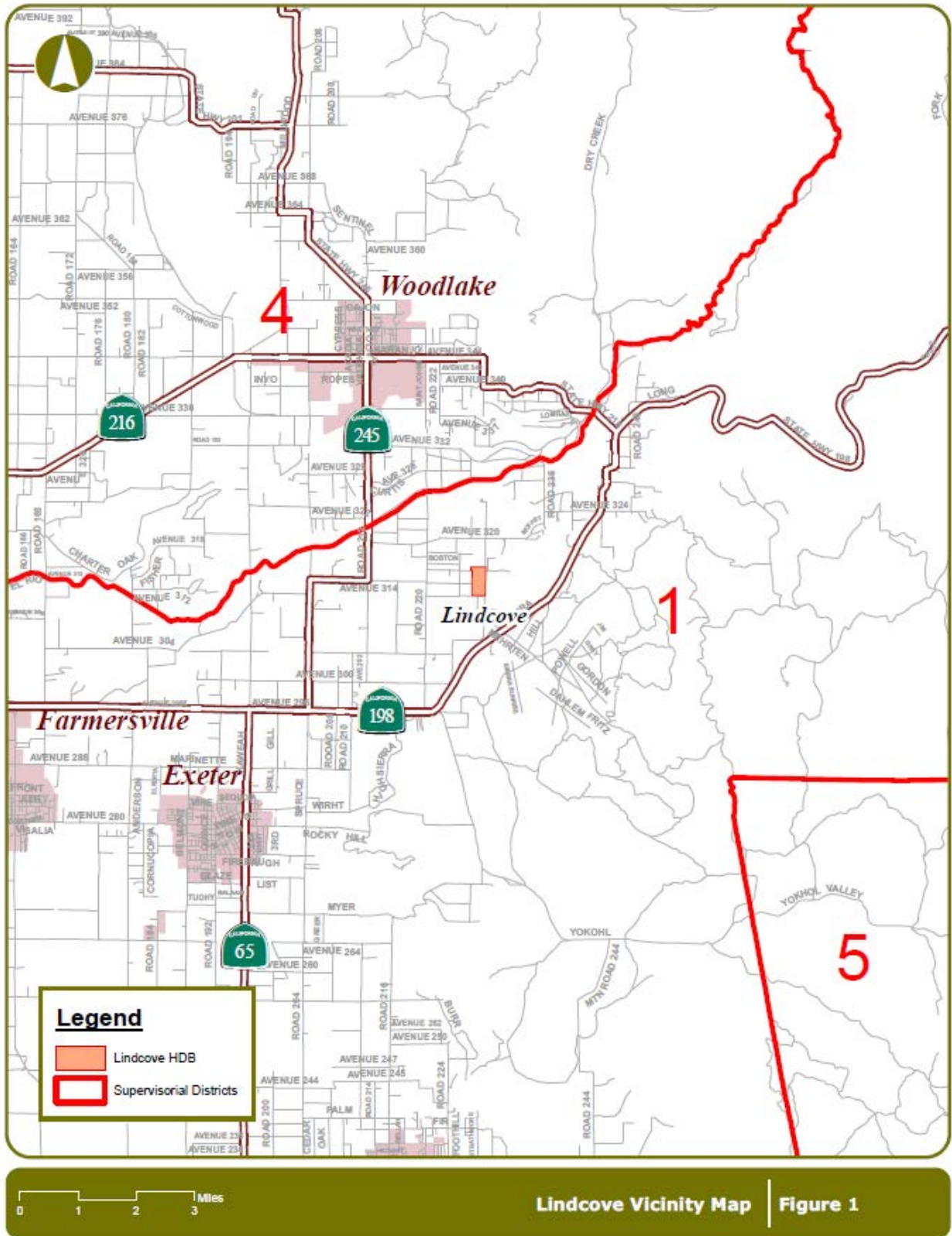
Table 1 - Lindcove Land Use	
Land Use	Acres
Agricultural	55.9
Rights-of-way	17.3
<b>Total</b>	<b>73.3</b>

Source: Tulare County GIS

No change is proposed to the Hamlet Development Boundary.

# LINDCOVE HAMLET PLAN

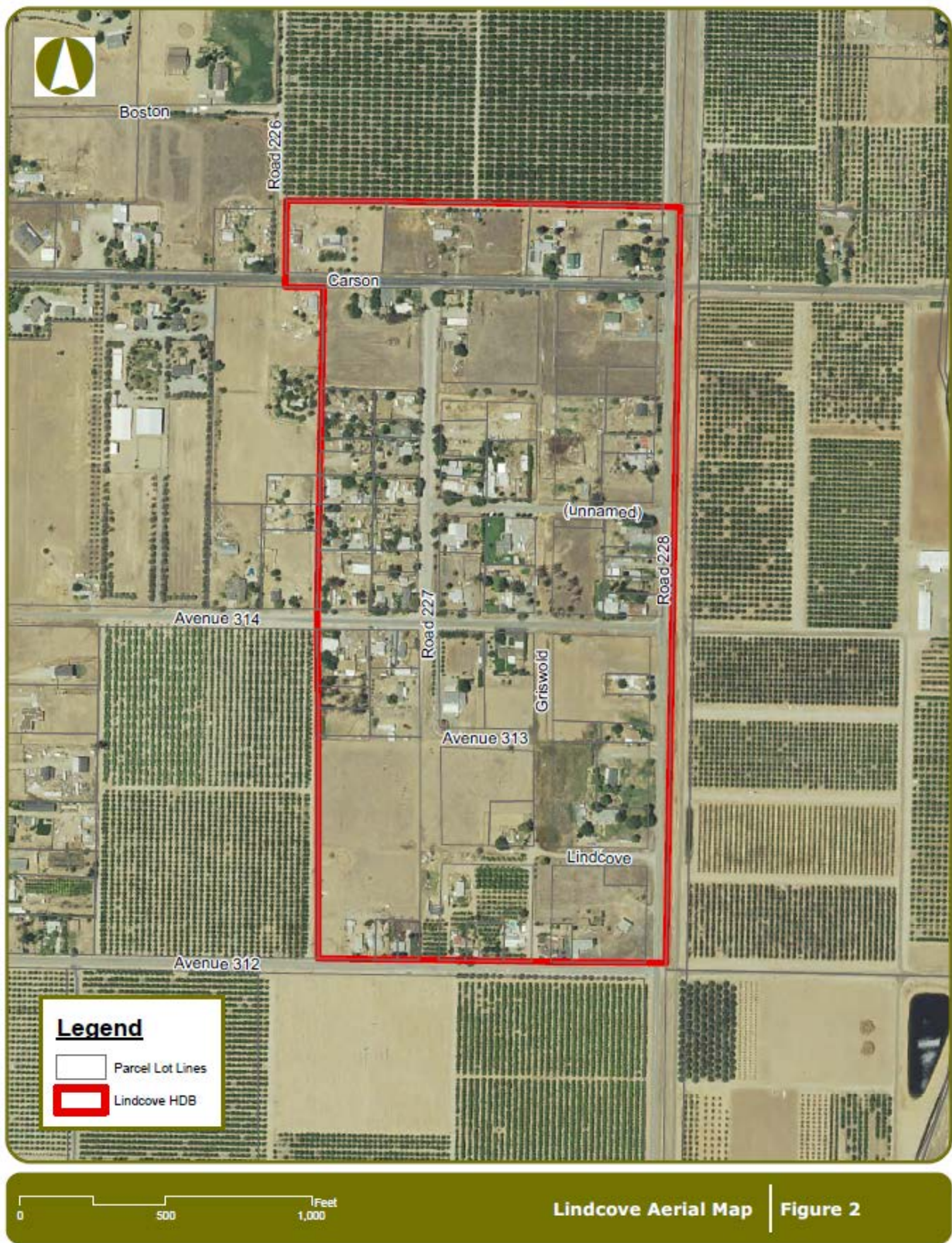
Figure 1 - Vicinity Map





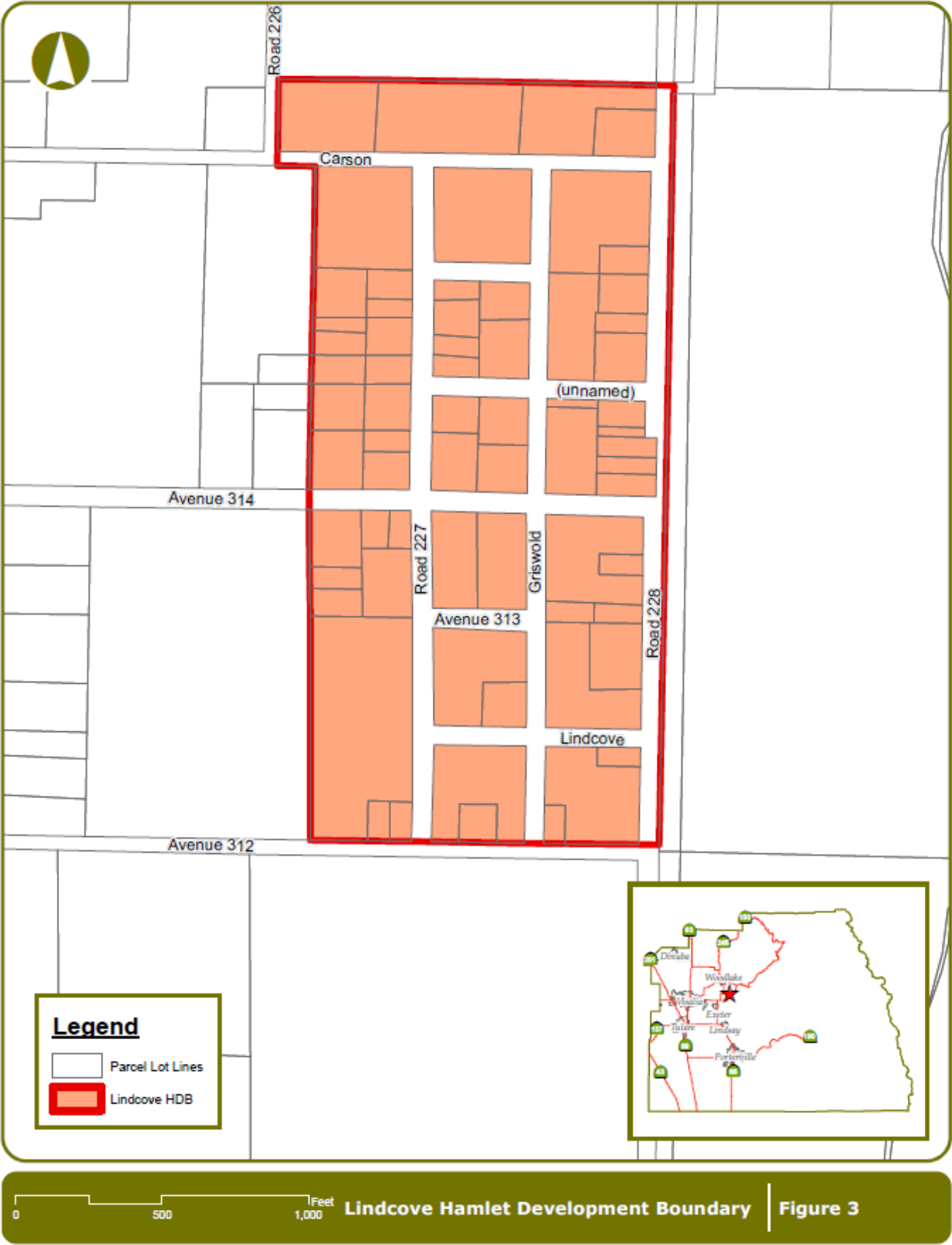
# LINDCOVE HAMLET PLAN

Figure 2 – Aerial - Lindcove



# LINDCOVE HAMLET PLAN

Figure 3 - Lindcove HDB



# LINDCOVE HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Outreach Steps

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) SHE staff made phone calls to local residents to determine who was interested in allowing us to have a community meeting at their home
- 2.) SHE staff visited the community and did outreach in efforts to identify a meeting location.
- 3.) Throughout the community outreach process, SHE staff explained the goals of the project and emphasized the importance and benefits of participating in the SGC project.
- 4.) SHE staff went door to door to complete resident surveys. SHE collected seven surveys in the community of Lindcove.

### Community Survey

A detailed survey was developed in efforts to gather specific community information about different areas of improvement. The survey inquires about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

On Thursday April 7, 2016, SHE staff went door to door in Lindcove to ask residents to participate in a community survey for SGC. Based on the information collected in the surveys, the following is a list of communities' top improvement needs:

### Priority Improvements

- 1) Natural Gas
- 2) Community Water System
- 3) Street Lighting
- 4) Sidewalks
- 5) Public Transportation
- 6) Community Center
- 7) Community Park
- 8) Internet Access

### **Natural Gas**

Lindcove residents report that they do not have natural gas lines and that propane is their source of fuel. Families report a high cost of propane, this cost results in people not heating their homes to a comfortable temperature, and this can lead to increased cold and flu in small children and elderly people.



# LINDCOVE HAMLET PLAN

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## **Water System**

Lindcove is a private well community, residents their own and maintain their own well. Resident expressed that they are interested in exploring their options for connecting to a neighboring community water system, they understand that this may include an initial cost and would result in paying a monthly water bill.

Some residents are concerned with their water quality and perceive their water to be unsafe to drink. Most families do not drink the water from their tap, they either buy bottled water or have a water filtration system. In 2014, SHE tested nine water wells in Lindcove. In summary, four of the nine wells had Total Coliform present, all 9 wells tested over the MCL for Nitrates and four wells exceeded the MCL for 123 TCP.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting and that they stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern, especially since the sheriff's department seldom patrols Lindcove. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Sidewalks**

The community of Lindcove has too few sidewalks. The residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking around town and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Lindcove traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Public Transportation**

Residents expressed their frustration for the lack of public transportation access. The Lindcove residents that do not drive or own a vehicle, find it hard to travel outside of town. Often residents need transportation to go into Visalia or Exeter for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes out through their community. Bus transportation to and from Lindcove would help many residents to travel on a daily basis.

## **Neighborhood Park**

In the community of Lindcove there is no safe green space for children to play and adults to be physically active. The nearest park is located over 3.5 miles away in the city of Exeter. Lindcove residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

# LINDCOVE HAMLET PLAN

## Internet Access

Most families in Lindcove do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. Without access to the internet, Lindcove residents are unable to do things such as online job hunting, apply for resources, or provide homework help to their kids.

## LINDCOVE TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Lindcove.

#### Population

In 2015, the Population for Lindcove was 494 (See Table 2).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Lindcove</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	211	42.7%
Female	19,334,329	50.3%	226,607	49.9%	283	57.3%
Total	38,421,464		454,033		494	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

**Table 3 - Projected Annual Growth Rate**

	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7

# LINDCOVE HAMLET PLAN

## Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Lindcove's population during the Planning Period (see **Table 3**).

## Median Age

Lindcove's population's median age is older than the median age of both the Tulare County and the State of California (see **Table 4**).

<b>Table 4 - Median Age</b>	
<b>Geography</b>	<b>Median Age (years)</b>
California	35.8
Tulare County	30.3
Lindcove	37

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 21.1% of Lindcove's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 5.5% was two races or more (see **Table 5**). Approximately 73.5% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

<b>Table 5 - Race &amp; Ethnicity</b>						
	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Lindcove</b>	<b>%</b>
Total	38,421,464		454,033		494	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	363	73.5%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	104	21.1%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	27	5.5%

2011-2015 American Community Survey 5-Year Estimates

## **ECONOMIC CONDITIONS**

### Employment in Lindcove

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see **Table 6**).

# LINDCOVE HAMLET PLAN

According to the California Department of Finance, the 2010-2014 American Community Survey indicated that the unemployment rate for Lindcove was about 15% while the rate for Tulare County was 13.5%. The unemployment rate for the State of California was 10.1%. Keeping in mind that the 15% includes only the employable labor force (that is, not every person of the population) results in about 64 unemployed persons of Lindcove's person labor force of 248.

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>Lindcove</b>
Population 16 years & over	30,312,429	325,404	400
In labor force	19,269,449	194,420	198
Civilian labor force	19,137,441	194,102	198
Employed	17,246,360	170,780	168
Unemployed	1,891,081	23,322	30
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	202

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Lindcove's median household income was \$43,571, compared to \$61,818 for the State of California (see Table 7).

<b>Table 7 - 2011-2015 American Community Survey: Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$30,318
<b>Tulare County</b>	\$42,031	\$44,814	\$17,876
<b>Lindcove CDP</b>	\$43,571	\$43,393	\$16,573

2011-2015 American Community Survey 5-Year Estimates

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Lindcove's median household income was \$43,571, whereas the State of California's median household income was \$61,818. Lindcove's median household income was 70.8% of the State of California's median household income.

# LINDCOVE HAMLET PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes). According to the California Department of Finance, the 2011-2015 American Community Survey data indicated 51.4% of the housing units in Lindcove were

owner-occupied (see Table 9). Similarly, 57.2% of housing units were owner occupied in Tulare County and 54.8% of the housing units were owner occupied in the State of California.

Table 8 - Housing Tenure			
	California	Tulare County	Lindcove
Occupied housing units	12,717,801	133,570	142
Owner-occupied	6,909,176	75,685	73
Renter-occupied	5,808,625	57,885	69

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Lindcove
Owner-occupied	54.8%	57.2%	51.4%
Renter-occupied	45.2%	42.8%	48.6%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Lindcove	5	14	4	11	17	49	1	3	8	23	35

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Lindcove indicated that approximately 14% of the housing units were sound. Approximately 63% of Lindcove's housing units were deteriorated and 23% were dilapidated (see Table 10).



# LINDCOVE HAMLET PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Lindcove
Total housing units	13,845,790	144,792	142
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	28
Built 1990 to 1999	1,495,571	21,767	3
Built 1980 to 1989	2,117,819	22,733	19
Built 1970 to 1979	2,503,688	27,111	61
Built 1960 to 1969	1,871,029	15,500	6
Built 1950 to 1959	1,907,512	13,694	7
Built 1940 to 1949	865,607	7,494	15
Built 1939 or earlier	1,298,438	8,186	3

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Lindcove was 3.93 persons per household (see Table 12). The average owner occupied household size is 3.05.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Lindcove	3.05	3.93

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Lindcove was 0.0%, which was lower than both Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Lindcove	0.0%	0.0%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# LINDCOVE HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

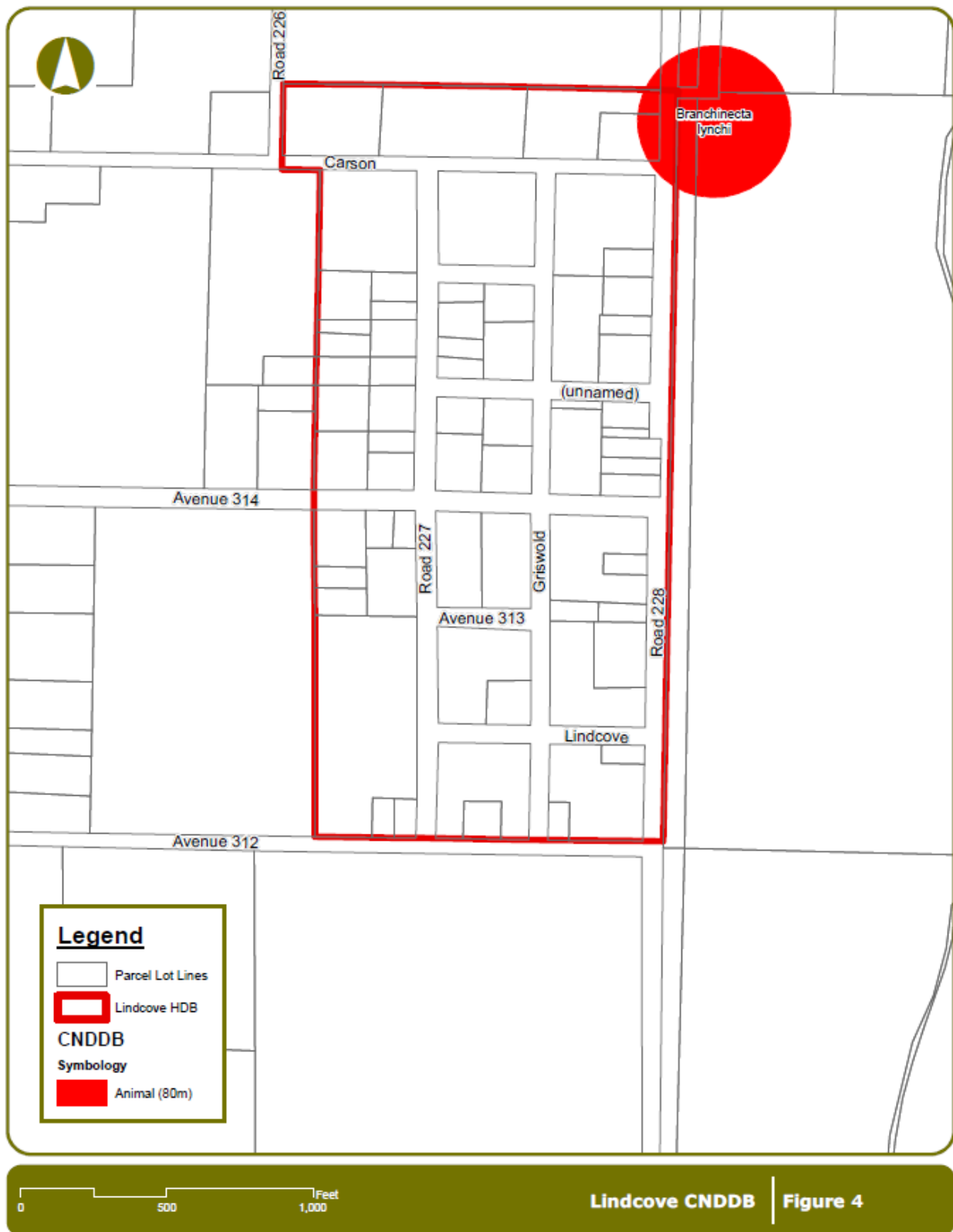
A California Natural Diversity Database (CNDDB) search conducted on November 1, 2016 (**see Figure 4**) indicates there are special status species within the Lindcove Quadrant Species List (which includes the Lindcove Planning Study Area) consisting of one (1) animal species and two (2) plant species: vernal pool fairy shrimp (*Branchinecta lynchi*, federal threatened); Striped adobe-lily (*Fritillaria striata*, state threatened); and San Joaquin adobe sunburst (*Pseudobahia peirsonii*, federal threatened and state endangered).

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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDB/Maps-and-Data> accessed November 1, 2016.

# LINDCOVE HAMLET PLAN

Figure 4 - CNDDDB Map - Lindcove



# LINDCOVE HAMLET PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Lindcove are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Lindcove] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Lindcove (see Figure 5). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**San Joaquin loam, 0 to 2 percent slopes**, is moderately deep to a hardpan, well drained and nearly level. This soil is suitable for orchards, vineyards, and cultivated crops but is somewhat limited by the presence of hardpan which restricts root growth. This problem can be alleviated by ripping and shattering the hardpan. The soil is poorly suited to urban uses because of a high clay content, very slow permeability and a cemented hardpan. Septic tank filter fields are severely limited for these reasons.

**San Joaquin loam, 0 to 9 percent slopes**, is moderately deep to hardpan, well drained and gently rolling. The soil is suited to cultivated crops and orchards. The main limitations are hardpan and a moderate erosion risk on steeper slopes. The soil is also suited for rangeland with erosion a slight limitation. The soil is unsuitable for building sites, roads and septic tank filter fields due to high clay content, very slow permeability and the cemented hardpan. This problem can be overcome by the importation of more suitable soils.

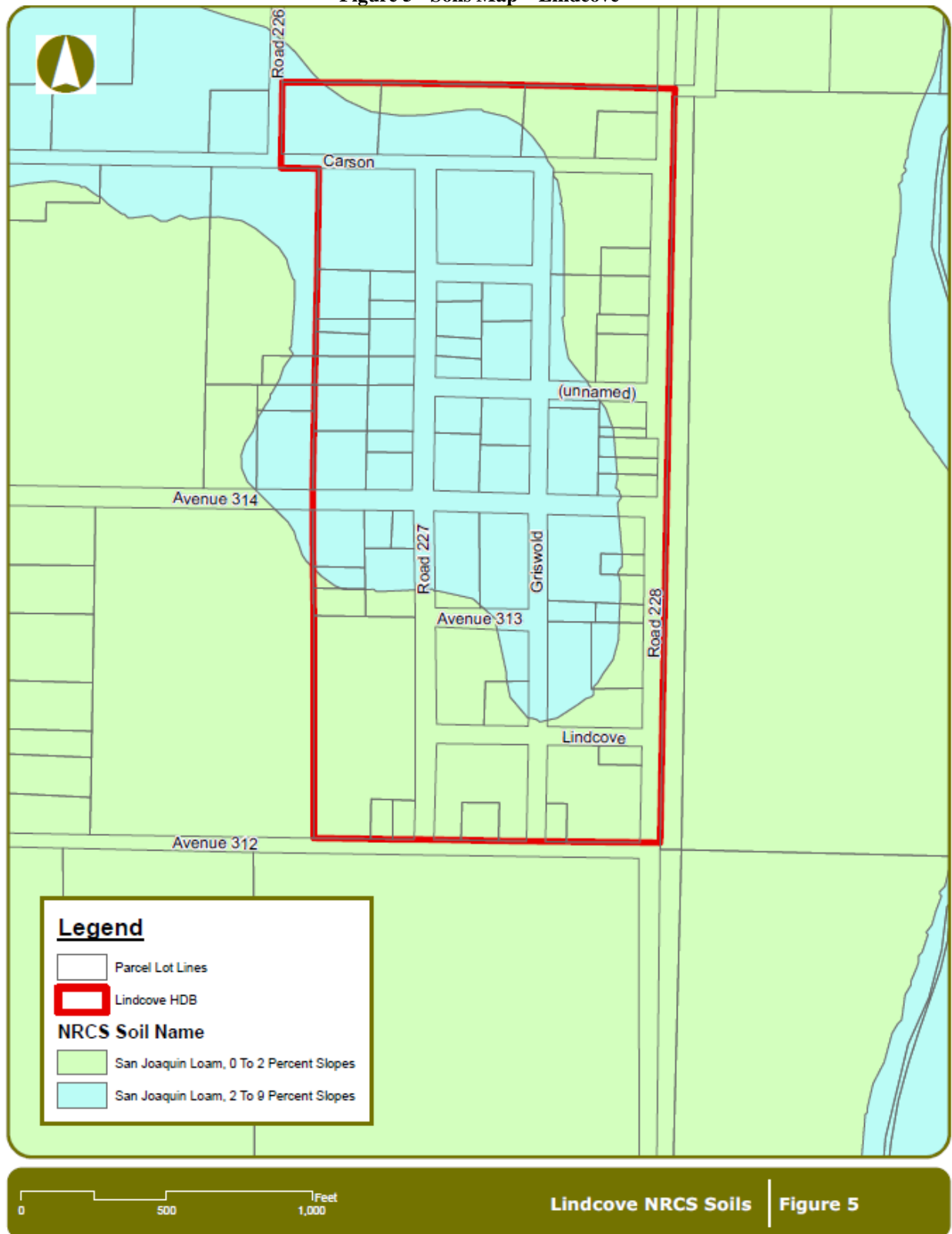
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# LINDCOVE HAMLET PLAN

Figure 5 - Soils Map – Lindcove



# LINDCOVE HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Lindcove Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Lindcove is located near the middle-east end of the Valley with prevailing winds from the northwest, it is susceptible to the accumulation of adversely modified air,

# LINDCOVE HAMLET PLAN

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particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Lindcove include agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0980E (**see Figure 6**), all portions of Lindcove are located within Flood Zone X, which is defined by FEMA as “Areas determined to be outside the 0.2% annual chance floodplain. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each Community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use Planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”<sup>7</sup> Although some areas of Tulare County have experienced major flooding along its major rivers, the Lindcove Plan Area has not. There are portions outside of Lindcove, that are within and adjacent to the FEMA 500 and 100 year flood zones. According to the Tulare County General Plan Update, substantial flooding could occur in Tulare County if the two (2) major dams were to experience failure. The inundation area below the Terminus Dam (on Lake Kaweah) does extend to portions of Lindcove.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Lindcove there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is

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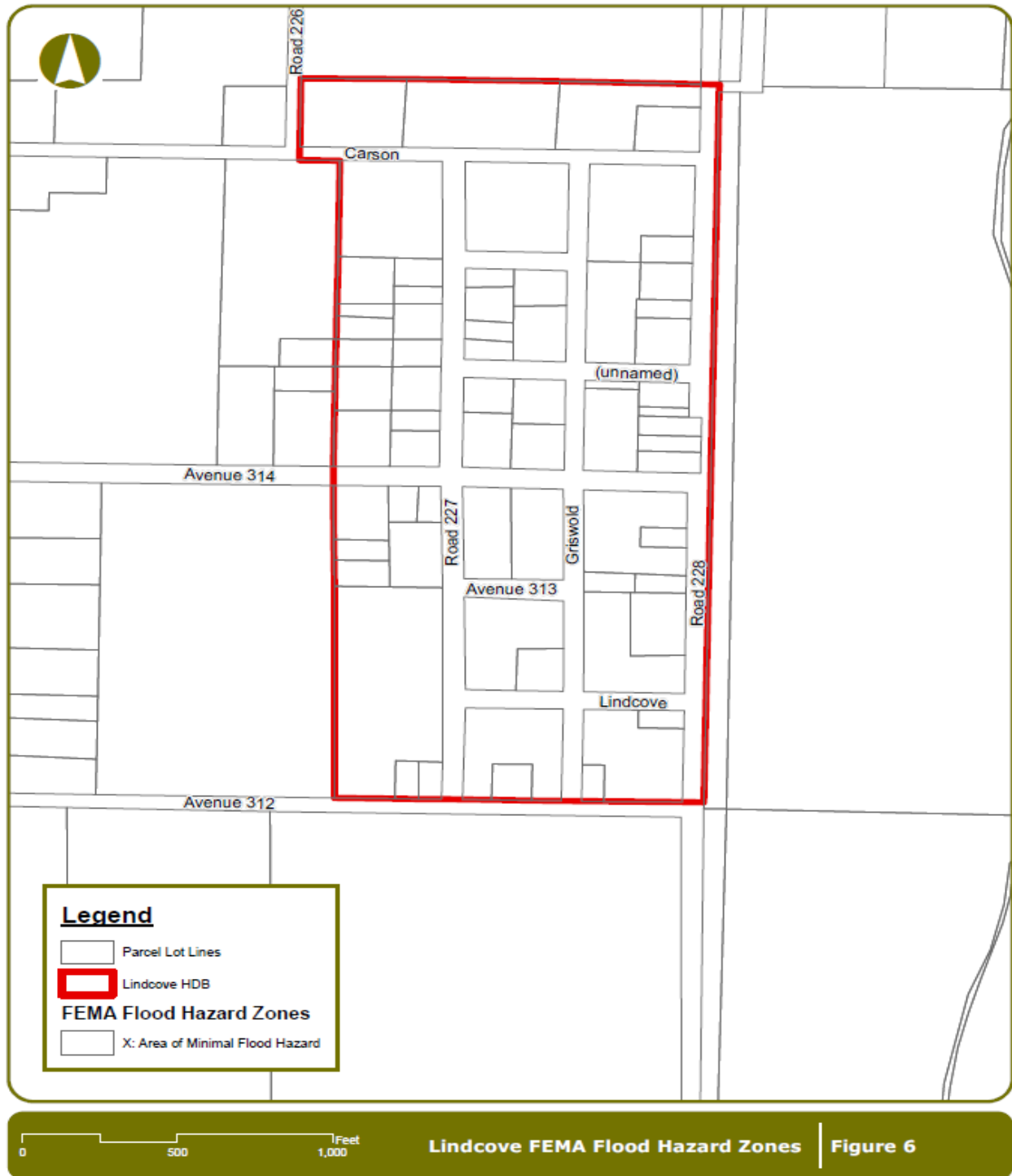
<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C19080E, Panel Number 980 of 2550, June 16, 2009. <https://msc.fema.gov/portal/> Accessed August 17, 2016.

<sup>7</sup> General Plan Background Report, page 8-14

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located in a SFHA by using the following FEMA Map Service Center link as follows:  
<https://msc.fema.gov/portal>.

Figure 6 - FEMA Flood Map - Lindcove





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## Noise<sup>8</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan background Report Lindcove lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks).

### Energy: Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom SCE has arrangements, and rolling blackouts.

### Domestic Water & Wastewater

Lindcove does not currently have any domestic water service. Lindcove also lacks a sanitary sewer service and relies on individual or community septic systems.

### Sewer

No public sewer facilities are available in Lindcove. Each land owner relies on septic systems for wastewater.

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<sup>8</sup> Tulare County General Plan 2030 Update, August 2012.; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# LINDCOVE HAMLET PLAN

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## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Lindcove does not currently have a storm drainage system.”<sup>9</sup>

## Solid Waste

Solid waste disposal services for the Community of Lindcove is provided by USA Waste (Waste Management), a private company. Solid waste generated in Lindcove can be disposed of at Road 80 Landfill, located at 22466 Road 80, Visalia.

## Roads

“There are several roadways in Lindcove that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.

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<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# LINDCOVE HAMLET PLAN

- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads<sup>10</sup>

**Table 14 - Roads in Need of Major and Medium Repair in Lindcove**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 312	Blair Road to Griswold Road	GRX
2	Road 228	Avenue 312 to Carson Avenue	CHIP

(Source: County of Tulare Public Works, 2012)

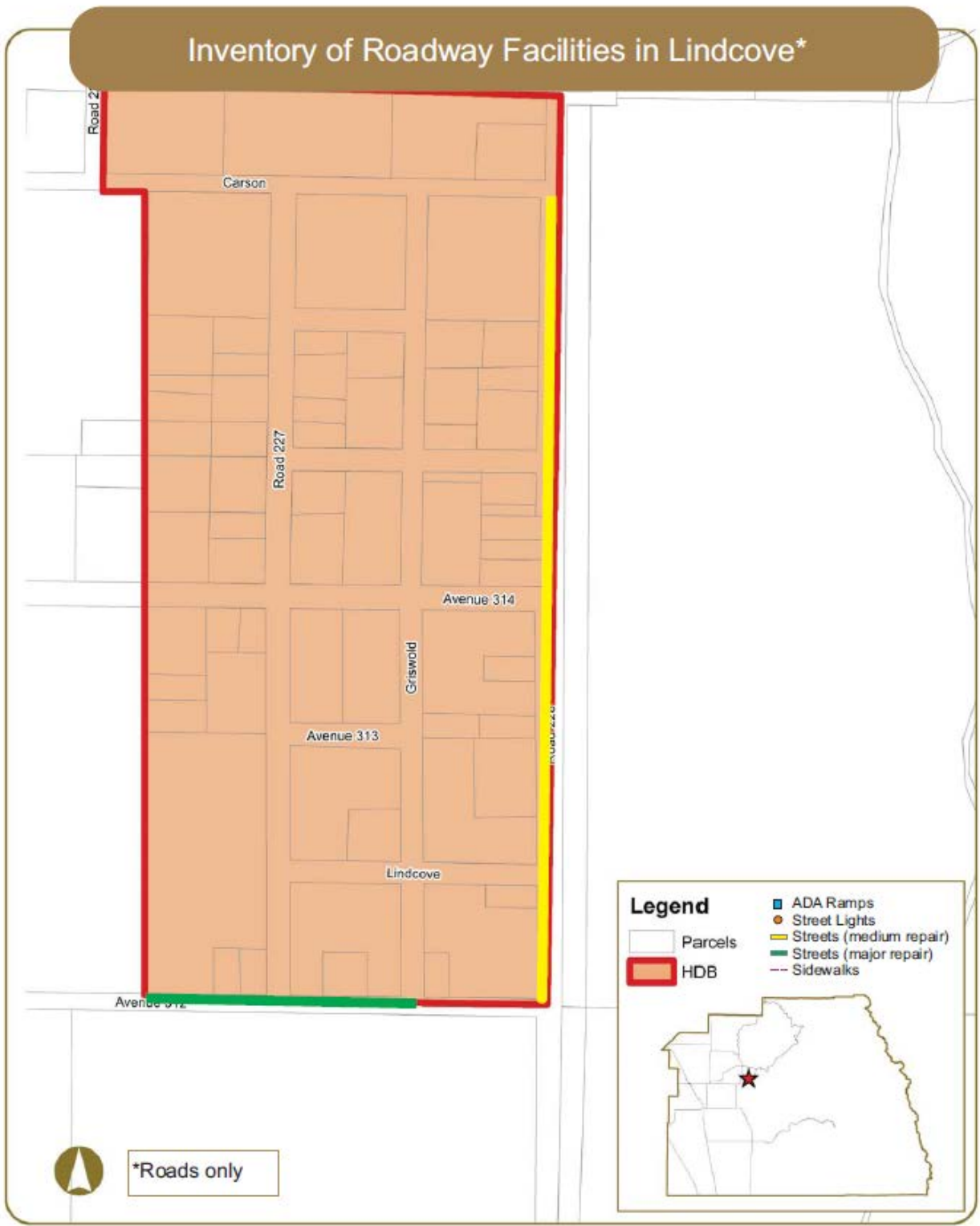
OLAY – overlay resurfacing operation  
 CHIP – chip seal  
 GRX – grind and remix

ACST – asphalt reconstruction  
 RCST – cold mix reconstruction

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

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Figure 7 - Lindcove Inventory of Roadway Facilities



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## Street Lights

There are currently no streetlights located within Lindcove.

## Sidewalks

There are currently no sidewalks located within Lindcove. The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Lindcove.”<sup>11</sup>

## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation. Currently no transit bus stops are located within Lindcove.

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are rarely used outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future.

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<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element

# LINDCOVE HAMLET PLAN

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## AMTRAK

The Hanford AMTRAK station, located approximately 35 miles to the west in Kings County, is the closest station to Lindcove providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 25 miles west. Fresno Yosemite International Airport (FAT), approximately 70 miles northwest of Lindcove is the principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 80 miles southwest of Lindcove and offers direct flights to several destinations.

## **PUBLIC SERVICES**

### Community Resource Center

The community lacks many resources and needs a place where residents can go to access information and county wide resources. People in Lindcove expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used for homework assistance, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Lindcove residents. This could also be the place where residents access a reliable internet connection.

### Sheriff

Police protection services are provided in Lindcove by the Tulare County Sheriff's Department main Sheriff's Office located at 2404 W. Burrell Avenue, in Visalia, approximately 15 miles west of Lindcove.

### Fire

Fire protection and emergency medical services are provided for Lindcove by the Lemon Cove Fire Station located at 32490 Highway 198, in Lemon Cove. Patrol 13, Engine 13 & Water Tender 13 are assigned to this location.

### Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Lindcove Hamlet Plan Area is within the Sequoia Union School District. The District provides K-8 grade education at Sequoia Union School District. High School students attend high school in Exeter. Lindcove is within the Exeter Union High School District.

# LINDCOVE HAMLET PLAN

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## Library

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>12</sup> The closest library is located in Exeter 5 miles away (**see Table 15**).

Table 15 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Exeter	Exeter Branch Library 230 East Chestnut Exeter, CA 93221-1712	Tuesdays & Wednesday 11:00 a.m. – 5:00 p.m., 6:00 p.m. – 8:00 p.m. Thursday & Friday 9:00 a.m. - 1:00 p.m. 2:00 – 6:00 p.m.

Library hours current as of June 2017

## Parks

In the community of Lindcove there is no safe green space for children to play and adults to be physically active. The nearest park is located over 3.5 miles away in the city of Exeter. Lindcove residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

# THE LINDCOVE HAMLET PLAN

## **CIRCULATION AND TRAFFIC**

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”

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<sup>12</sup> General Plan Background Report, page 7-96

# LINDCOVE HAMLET PLAN

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## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>13</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

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<sup>13</sup> Tulare County General Plan Background Report, page 5-7.



# LINDCOVE HAMLET PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Lindcove Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Lindcove to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Lindcove Hamlet Plan, and input received from Lindcove citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.

2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned shopping areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
5. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
6. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
7. The County shall work with the Schools to provide safe routes to school.
8. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Lindcove Community Plan.
9. The Lindcove Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.

# LINDCOVE HAMLET PLAN

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10. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

**Housing**

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Lindcove.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Lindcove.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

# LINDCOVE HAMLET PLAN

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## Economic Base

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

#### Policies:

1. Encourage the Lindcove to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

#### Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.

2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## Environmental Quality and Public Safety

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Lindcove citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

#### Objective: Protect Agricultural Lands:

Land within the respective Lindcove, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

# LINDCOVE HAMLET PLAN

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The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Lindcove.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Lindcove.

# LINDCOVE HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 73.3 acre Lindcove Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 76 percent of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 494 by 73.3 urbanized acres (area within the HDB), which equals 6.8 persons per urbanized acre (see **Tables 16 and 17**) showing population projections). Projecting the population at a 1.3% growth rate adds 106 persons in year 2030. Multiplying the 106 persons by the 1/3 ratio suggests an additional 1.4 acres of land will be required to accommodate development by the year 2030 if projections are realized.

### Population Growth Forecast

Table 16 - Lindcove Population Projections			
Growth Rate	2015	2020	2030
0.013	494	520	600

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the HDB.

Population and Housing Units The year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>14</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2015 the community had 142 dwelling units (including vacant dwellings) with a population of 494. At an annual growth rate of 1.3%, the projected housing units are 527 and 300 in years 2020 and 2030, respectively, and projected population is 527 and 6007 in years 2020 and 2030, respectively.

Table 17 - Population Projections		
Year	Population	Growth (%)
2015	494	0.013
2016	500	0.013
2017	507	0.013
2018	514	0.013
2019	520	0.013
2020	527	0.013
2021	534	0.013
2022	541	0.013
2023	548	0.013
2024	555	0.013
2025	562	0.013
2026	569	0.013
2027	577	0.013
2028	584	0.013
2029	592	0.013
2030	600	0.013

<sup>14</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

# LINDCOVE HAMLET PLAN

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## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, water service levels to be expanded for which Grant funding will be required.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Based on the 2011-2015 American Community Survey (**see Table 18**), the educational barrier in Lindcove begins in grade school. Of the adults age 25 and older, 26.1% had an educational level of less than 9<sup>th</sup> grade. A lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

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Table 18 - Education Attainment			
Educational Attainment	California	Tulare County	Lindcove
Population 25 years and over	24,865,866	258,046	345
Less than 9th grade	10.1%	20.8%	26.1%
9th to 12th grade, no diploma	8.4%	11.2%	25.8%
High school graduate (& equivalency)	20.7%	25.0%	25.2%
Some college, no degree	22.0%	22.3%	15.4%
Associate's degree	7.8%	7.5%	0.9%
Bachelor's degree	19.6%	8.9%	3.2%
Graduate or professional degree	11.4%	4.3%	3.5%

2011-2015 American Community Survey 5-Year Estimates

## Health Care

Health care is important for economic development as businesses need healthy employees. The other nearest medical offices are located in the City of Exeter, approximately five (5) miles to the southwest. The closest hospital is Kaweah Delta District Hospital located in the City of Visalia, approximately 20 miles to the west.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 19**) and Zoning Plan Districts (**see Table 20**) have been updated.

Table 19 - Proposed Land Use	
Proposed Land Use	Acres
Mixed Use	56.0
Right-of Ways	17.3
Lindcove HDB	73.3

Table 20 - Proposed Zoning Districts	
Proposed Zones	Acres
C-2-MU	10.5
R-A-43	45.5
Right-of Ways	17.3
Lindcove HDB	73.3

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations found in the Lindcove Hamlet Plan. The following components comprise the Lindcove Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

# LINDCOVE HAMLET PLAN

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## Zoning Districts Changes

As part of this Implementation Program for the Lindcove Hamlet Plan, there are a several of changes to existing zoning districts. These changes are described below.

## Chapter 16 of the Zoning Code

Revise chapter 16 of the zoning code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

## Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Lindcove.

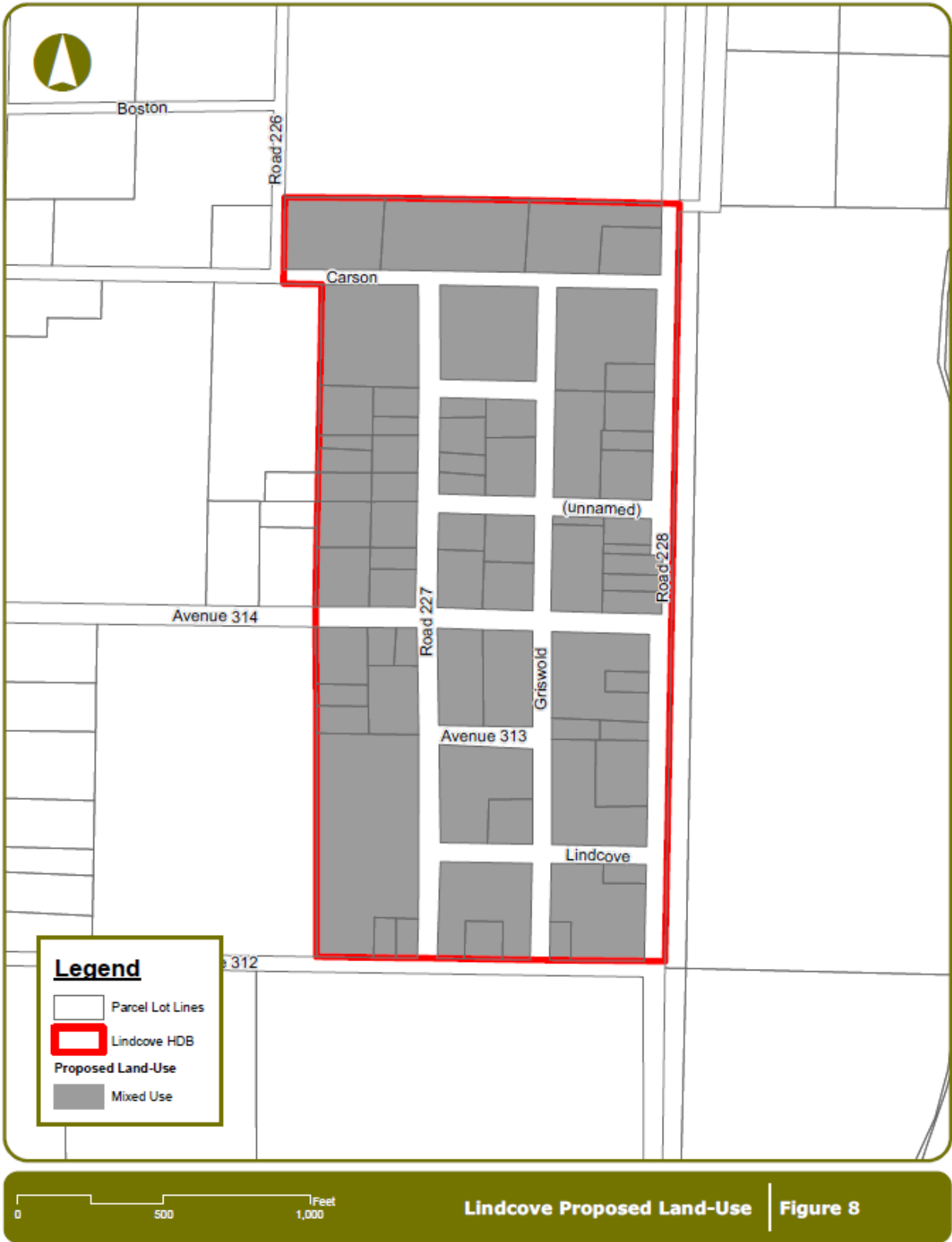
## Zoning Map Update

The current Zoning Districts for Lindcove (**see Figure 8**) will be amended to be compatible with the Land Use Map (**see Figure 7**) outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 9**) Lindcove proposed Zoning District.



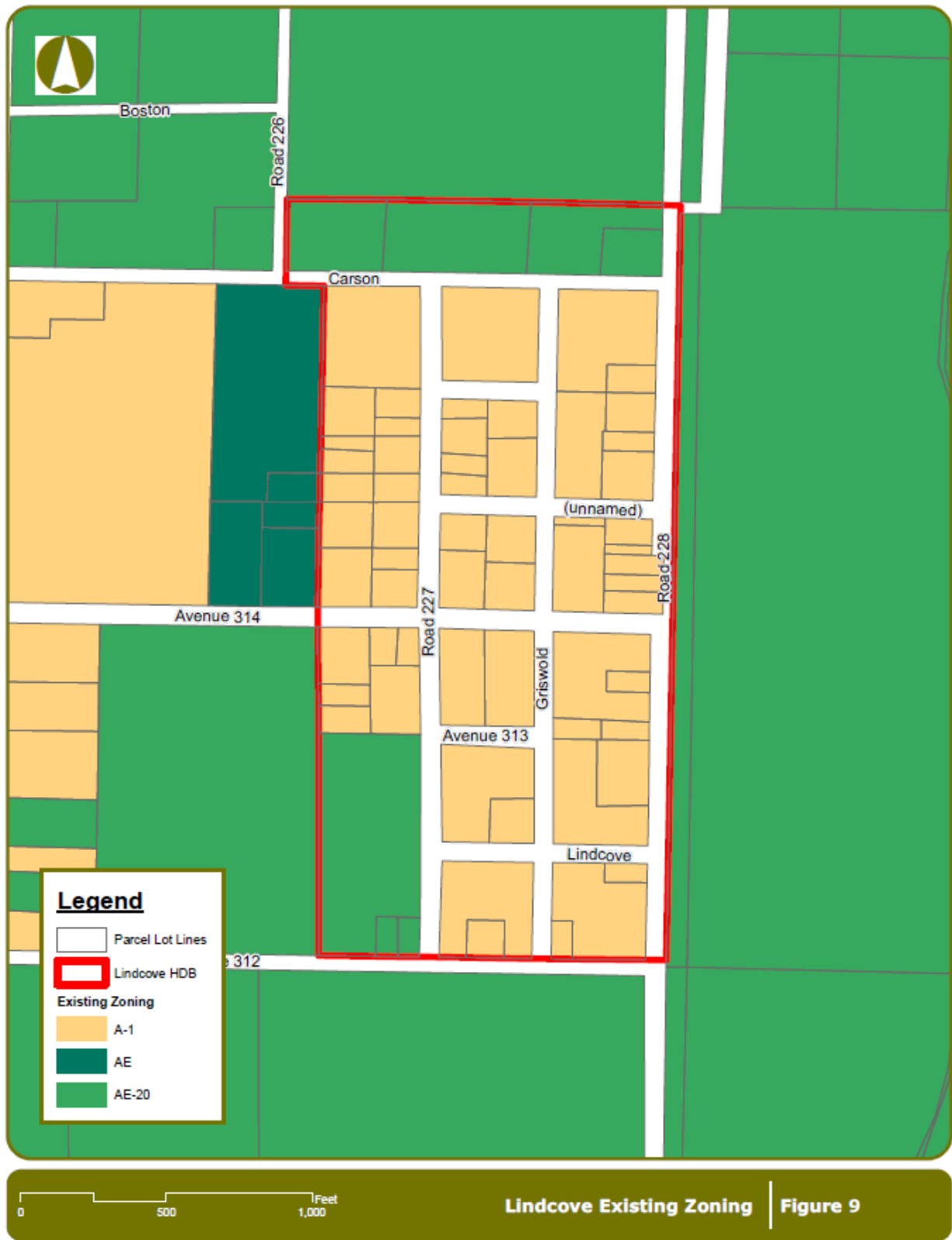
# LINDCOVE HAMLET PLAN

Figure 8 - Proposed Land Use Plan



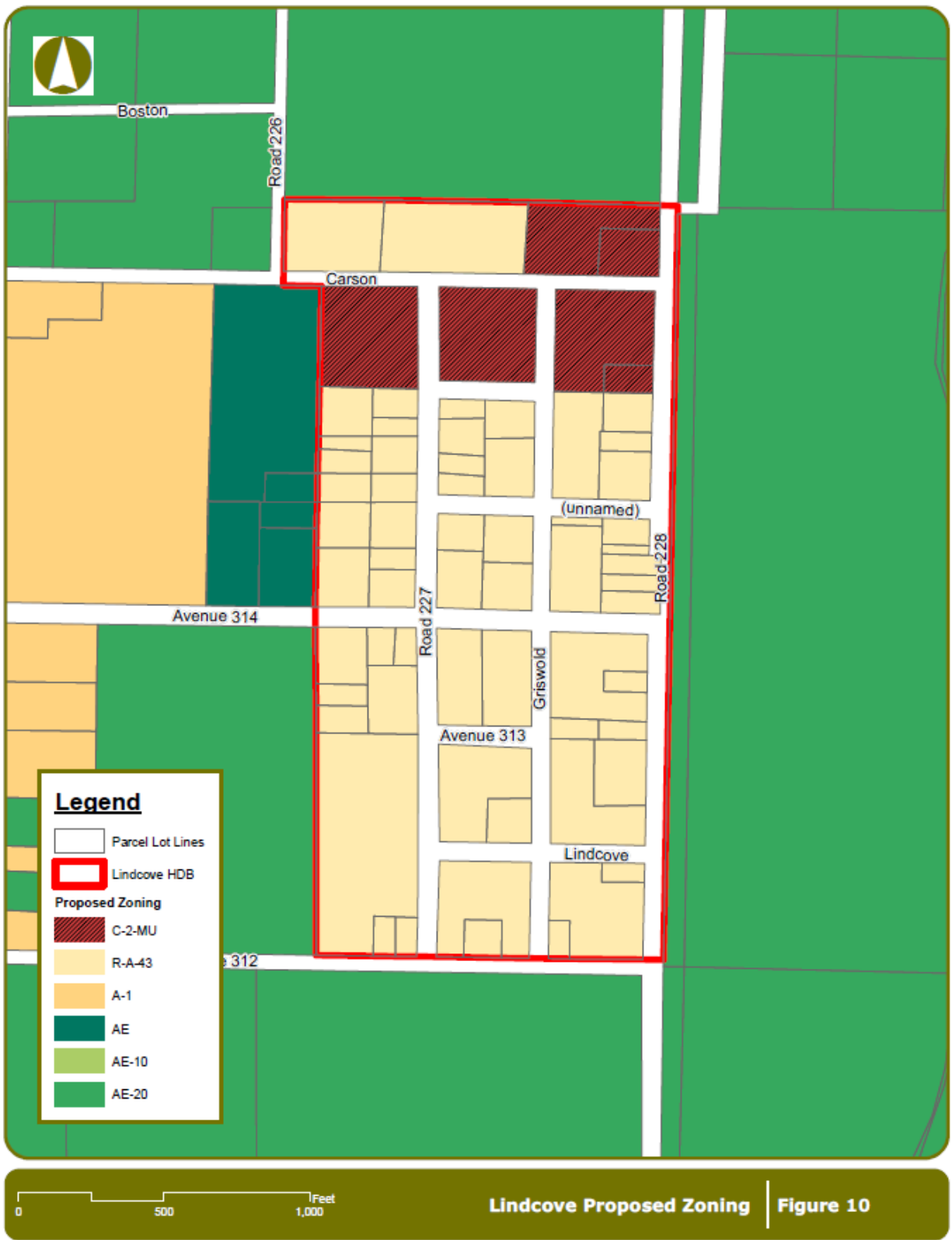
# LINDCOVE HAMLET PLAN

Figure 9 - Existing Zoning Plan



# LINDCOVE HAMLET PLAN

Figure 10 - Proposed Zoning Changes



# LINDCOVE HAMLET PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# LINDCOVE HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

# LINDCOVE HAMLET PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-	R-3, C-1, C-2, C-3,



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	1, R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles	C-2, C-3, M-1, M-2	

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of an airport).		
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1

# LINDCOVE HAMLET PLAN

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Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# LINDCOVE HAMLET PLAN

## A-2 *Mixed Use Overlay District (Zone Change Text)*

The following regulations shall apply in the community of Lindcove, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Lindcove.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Lindcove.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Autowrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planing Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# LINDCOVE HAMLET PLAN

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or

# LINDCOVE HAMLET PLAN

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fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Lindcove. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

# LINDCOVE HAMLET PLAN

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## A-3 *Development Standards (Mixed Use Zoning District)*

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To promote Economic Development within the Lindcove Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Lindcove. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Lindcove Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Lindcove. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

# LINDCOVE HAMLET PLAN

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.



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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.



# LINDCOVE HAMLET PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# LINDCOVE HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution



**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and



WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:        None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9369
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-039	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.



B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map

***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976



# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

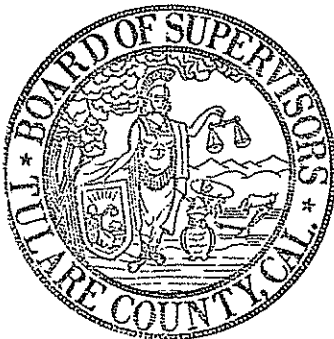
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## Monson Hamlet Plan

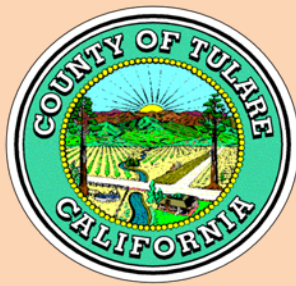
# MONSON HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# MONSON HAMLET PLAN

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# MONSON HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# MONSON HAMLET PLAN

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## MONSON HAMLET PLAN

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# Monson Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Monson Hamlet Plan: GPA 17-018  
Zoning District Map: PZC 17-018



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# MONSON HAMLET PLAN

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# MONSON HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# MONSON HAMLET PLAN

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# MONSON HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Michael Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Hector Guerra, Chief Planner, Environmental Planning

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# MONSON HAMLET PLAN

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# MONSON HAMLET PLAN

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# MONSON HAMLET PLAN

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# MONSON HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Monson is to develop a plan which can accurately reflect the needs and priorities of this community. Monson is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks, etc.) and to stimulate economic development within the community.

### Location

The community of Monson (**see Figure 1**) is located on the east side of the San Joaquin Valley, and is a census-designated place located in the northwest portion of Tulare County. It is bounded by Avenue 384 in the south, Avenue 388 in the north, Sand Creek in the west, and Road 108 roughly three (3) miles southeast of the city of Dinuba in Tulare County (**see Figure 2**). Monson is in Sections 34 & 35 Township 16 South, Range 24 East, MDB&M, and can be found within Monson United States Geological Survey 7.5 minute topographic quadrangle. Monson is located at an elevation of 325 feet National Geodetic Vertical Datum. The coordinates of Monson are: Latitude: 36.4927, Longitude: -119.3357.

### Planning Area

The Monson Hamlet Development Boundary (HDB) area consists of 115 acres (**see Figure 3**). The Land Uses within the HDB include agriculture (orchards and pasture), which currently occupy 82.7 percent of the 115 acres. Urban development, including urbanized uses such as commercial development occupy 0.8 of the 115 acres. The remaining 12.4 percent are lands dedicated for Right-of-way (**See Table 1**).

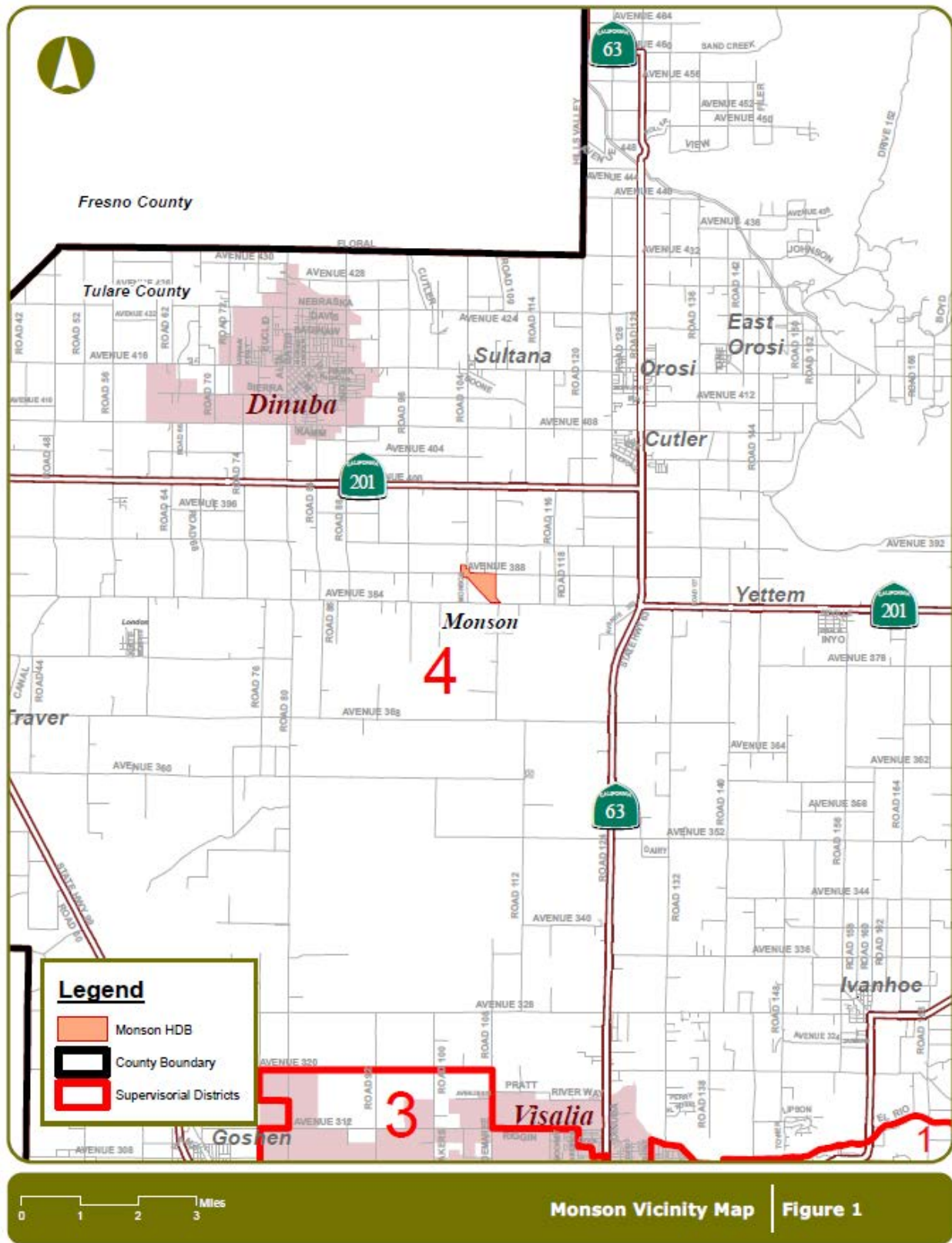
Table 1 - Monson Land Use	
Land Use	Acres
Commercial	1
Agricultural	99
Rights-of-way	14.9 (approx.)
<b>Total</b>	<b>115</b>

Source: Tulare County GIS

There are changes being proposed to the Hamlet Development Boundary to align north of the railroad tracks to the west of Monson Road, approximately 4.8 acres (**see Figure 11**).

# MONSON HAMLET PLAN

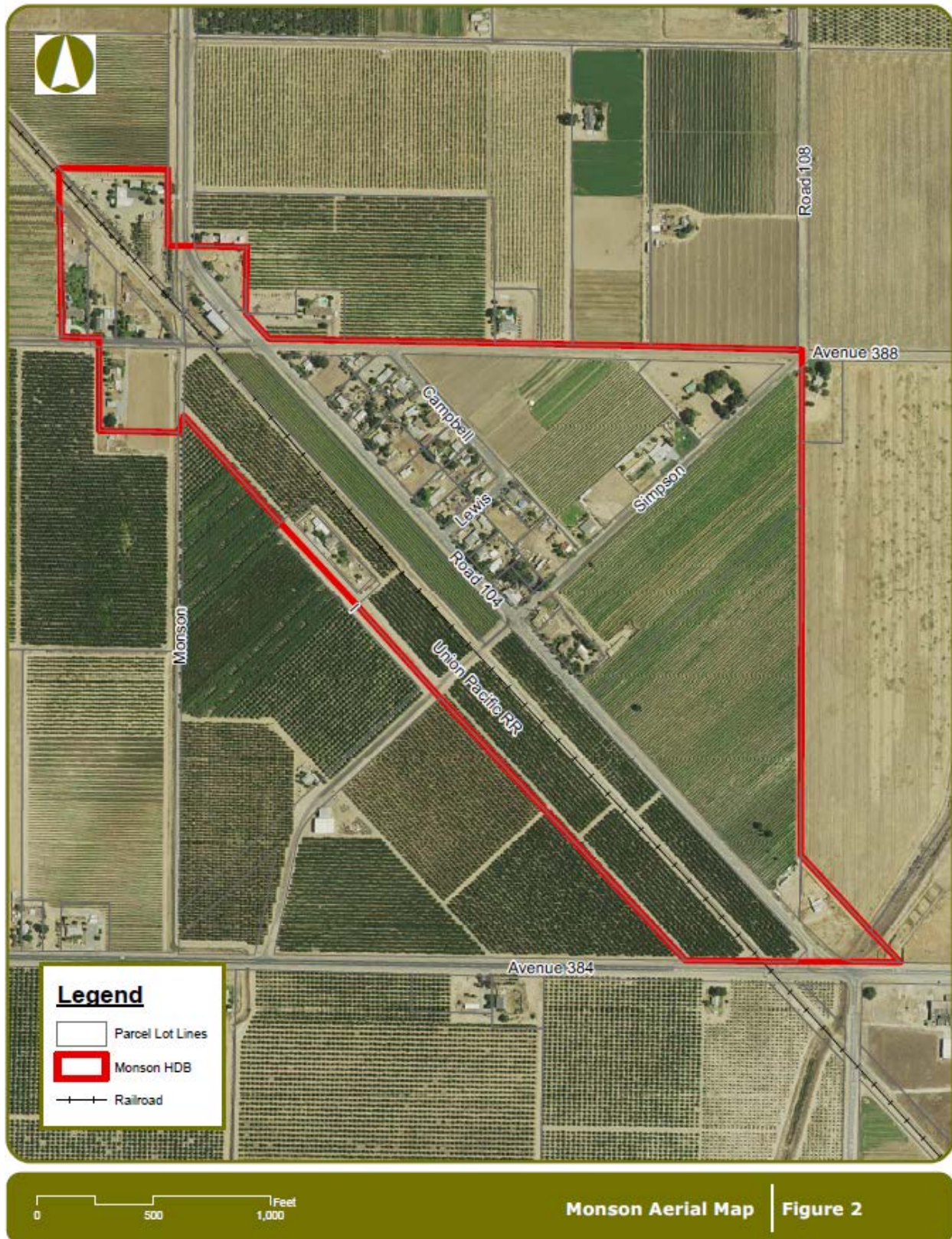
Figure 1 - Vicinity Map





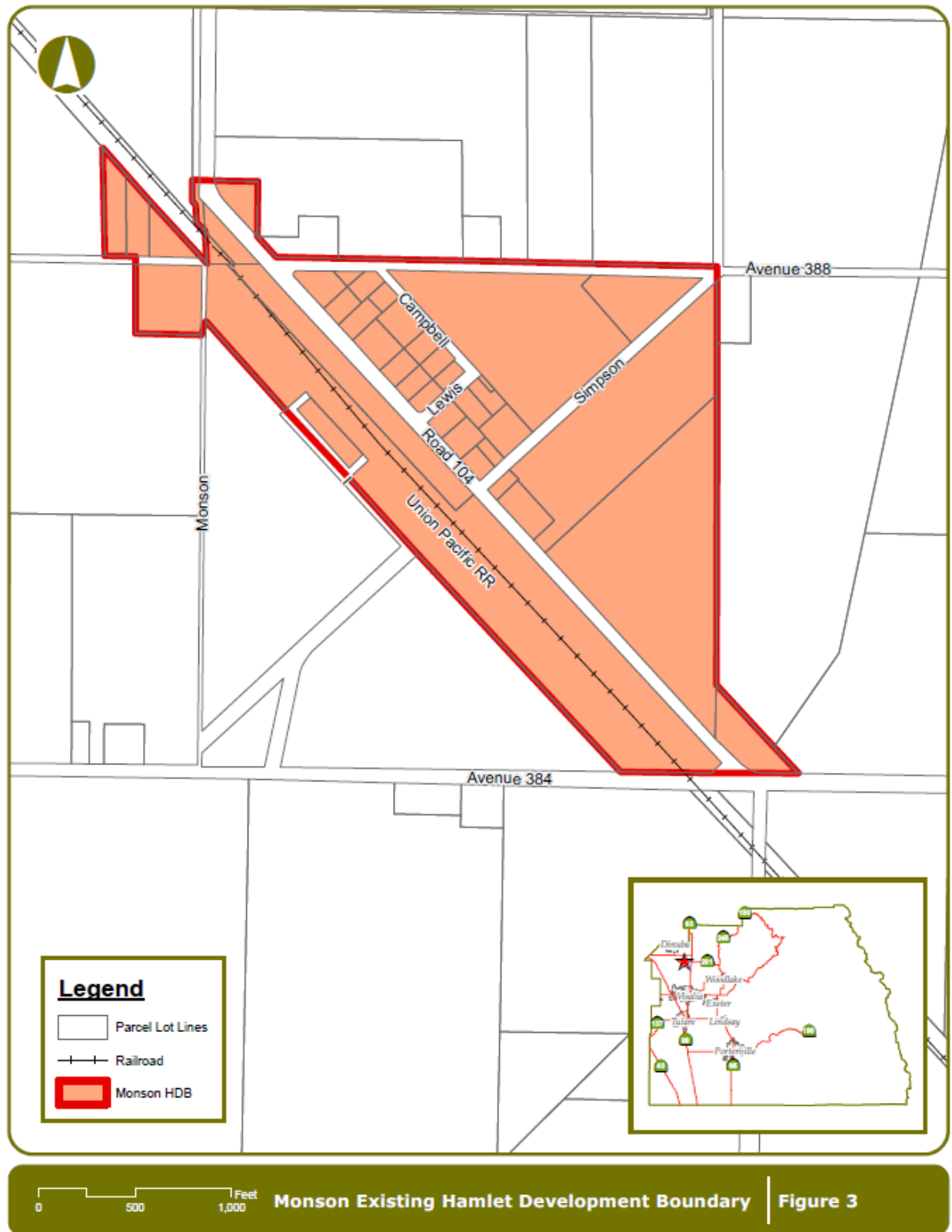
# MONSON HAMLET PLAN

Figure 2 - Monson Aerial Map



# MONSON HAMLET PLAN

Figure 3 – Existing Monson HDB



# MONSON HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises (SHE) went door to door throughout the community of Monson to distribute flyers to homes. SHE emphasized with residents about the importance of participating in this process.

### Community Outreach Steps

Self-Help Enterprises (SHE) utilized various outreach methods to promote community participation and increase awareness of the SGC project:

- 1.) An informational flyer was developed.
- 2.) On Monday August 17 and Wednesday August 19, 2015, SHE staff went door to door to distribute flyers to homes.
- 3.) SHE staff partnered with La Voz de Monson (a local community group of residents that was formed to encourage community involvement, gather support and to spread the word about the importance of attending this meeting.
- 4.) SHE made phone calls to local residents.
- 5.) Throughout the community outreach process, SHE staff explained the goals of the project and emphasized the importance and the community benefits of participating in the SGC community meeting.
- 6.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner store.

### Community Meeting

Self-Help Enterprises conducted a community meeting (with two SHE staff members and one Tulare County RMA employee present) in Monson on Thursday, August 20, 2015 at 6:30 pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at 38660 Monson Drive, the home of a local resident. The purpose of the meeting was to discuss Monson improvement needs, gather community data, and report the findings of the meeting in a final report. More than Twenty Monson residents attended the meeting. Eleven (11) community surveys were collected.



# MONSON HAMLET PLAN

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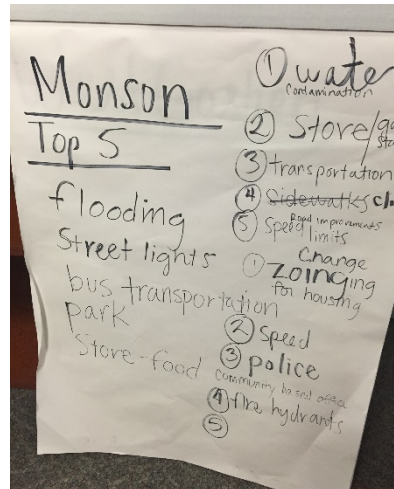
## Community Survey

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Monson SGC community input meeting, residents concluded that the following is a list of priority improvement needs.

## Priority Improvements

1. Water Issues
2. Storm Water Drainage/ Street Flooding
3. Road Improvements
4. Street lights
5. Bus transportation
6. Fire hydrants
7. Community based Officer/Sherriff patrol
8. Community Park
9. Health Care Clinic
10. Zoning changes



# MONSON HAMLET PLAN

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## **Sewer System**

There is no community wide sewer system in Monson. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent.

## **Storm Water Drainage/Flooding**

Monson residents report that the community does not have adequate storm water drainage. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. This is a problem because the stagnant water attracts insects and mosquitos. Residents report that some streets flood during the rainy season, most of the roads in town are dirt roads this makes mud and pot holes. Not having curb and gutters or sidewalks also adds to the problem

## **Road Improvements**

Road conditions are a big concern to Monson residents. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage caused to their vehicles while driving on Monson roads. Residents report that roads are narrow and that two cars barely fit on one road at the same time. According to residents, Monson roads are not regularly maintained.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Monson. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Speed Limit Signs**

During the SGC community meeting residents expressed concern about the speed of traffic in town. Many vehicles travel at a high rate of speed on Monson Drive as well as on the other main roads in town. Residents believe that in order to avoid more car crashes, the County should install a speed limit sign on Monson Drive and speed bumps on residential streets to slow traffic. Residents shared that there is little to zero patrol done by the Sheriff and California Highway Patrol, this adds to the speeding problem.

## **Public Transportation**

Residents expressed their frustration from the lack of public transportation access. The Monson residents that do not drive or own a vehicle, find it hard to travel outside of Monson. Often residents need transportation to go into Visalia or Dinuba for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes out through Monson. Bus transportation to and from Monson would help many residents to travel on a daily basis.

## **Zoning**

During the community meeting residents stated that they are unable to build new homes or start businesses due to the way land is currently zoned in Monson. Residents expressed that zoning changes are needed to allow residents to build new housing in the community. During this meeting, it appeared that multiple residents would be interested in building a home in Monson if zoning laws permitted it.

# MONSON HAMLET PLAN

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## **Community Based Officer**

The Tulare County Sheriff Department is responsible for patrolling the community of Monson. Residents report that the TCSD response time is unacceptable and that there is little to zero Sheriff Patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go un-responded to. Monson residents are asking for increased Sheriff Presence in their community.

## **Fire Hydrants**

Residents are worried because their community does not have fire hydrants. Residents feel vulnerable to fire emergencies. The nearest fire water supply is in Sultana. In case of a fire, Dinuba Fire Department would service the Monson community.

## **Neighborhood Park**

In the community of Monson there is no safe open green space for children to play and adults to be physically active. The nearest park is located 8 miles away in the city of Dinuba, survey results show that in Monson, children play in the street or do not play outside at all. Monson residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

## **Local Market**

The community of Monson is a food desert. This community does not have a local food market that sells fresh produce, meat and dairy. The Monson Market sells some food however, most of it is highly processed, packaged, and not considered nutritious. In addition, the food prices are much higher there than in neighboring communities and the quality of food is not as fresh as it is in other communities. Monson residents regularly travel to Dinuba (8 miles) or Visalia (15 miles) to buy their groceries.

## **Medical Clinic**

Monson residents do not have access to medical care within the community. The nearest medical clinic is approximately 8 miles away in Dinuba. Most residents report traveling to Visalia, 14 miles, Dinuba, 8 miles or Orosi, 8 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. The Tulare County Area Transit does not make regularly scheduled stops in Monson, the TCAT will pick up Monson residents by appointment when calling the day before. Residents report that they are very concerned about the thought of having to travel so far in the case of a medical emergency. The nearest hospital is in Visalia, 15 miles away, and it takes approximately 20 minutes for the nearest ambulance to get to Monson from Visalia.

## **Internet Access**

Most families in Munson do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. Without access to the internet, Munson residents are unable to do things such as online job hunting, apply for resources, or provide homework help to their kids.

# MONSON HAMLET PLAN

## MONSON TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Monson.

#### Population

In 2015, the Population of Monson was 237 (see Table 2)

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Monson</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	128	54%
Female	19,334,329	50.3%	226,607	49.9%	109	46%
Total	38,421,464		454,033		237	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Monson's population during the Planning Period (see Table 3).

<b>Table 3 - Projected Annual Growth Rate</b>		
	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7

# MONSON HAMLET PLAN

## Median Age

Monson's population's median age is younger than the median age of both Tulare County, as well as the State of California (see Table 4).

<b>Table 4 - Median Age</b>	
<b>Geography</b>	<b>Median Age (years)</b>
California	35.8
Tulare County	30.3
Monson	26.4

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 18.1% of Monson's population was White, 4.6% was African American, 0% was Native American, 0% was Asian, and 26.6% was two races or more (see Table 5). Approximately 86.9% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

<b>Table 5 - Race &amp; Ethnicity</b>						
	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Monson</b>	<b>%</b>
Total	38,421,464		454,033		237	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	120	50.6%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	43	18.1%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	11	4.6%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	63	26.6%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Monson

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Monson was about 12.1% while the rate for Tulare County was 13.5%. The unemployment rate for the State of California was 10.1%. Keeping in mind that the 12.1% includes only the employable labor force (that is, not every person of the population) results in about 21 unemployed persons of Monson's person labor force of 131.



# MONSON HAMLET PLAN

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>Monson</b>
Population 16 years & over	30,312,429	325,404	173
In labor force	19,269,449	194,420	131
Civilian labor force	19,137,441	194,102	131
Employed	17,246,360	170,780	110
Unemployed	1,891,081	23,322	21
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	42

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Monson median household income is currently unavailable, however the State of California's median household income is currently \$61,818 (see Table 7)

<b>Table 7 - 2011-2015 American Community Survey Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$29,527
<b>Tulare County</b>	\$42,031	\$44,814	\$17,894
<b>Monson CDP</b>	Not available	Not available	\$18,344

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

<b>Table 8 - Housing Tenure</b>			
	<b>California</b>	<b>Tulare County</b>	<b>Monson</b>
Occupied housing units	12,717,801	133,570	62
Owner-occupied	6,909,176	75,685	49
Renter-occupied	5,808,625	57,885	13

2011-2015 American Community Survey 5-Year Estimates

<b>Table 9 - Housing Tenure (%)</b>			
	<b>California</b>	<b>Tulare County</b>	<b>Monson</b>
Owner-occupied	54.8%	57.2%	62%
Renter-occupied	45.2%	42.8%	79.0%

2011-2015 American Community Survey 5-Year Estimates

# MONSON HAMLET PLAN

## Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Monson indicated that approximately 14% of the housing units were sound. Approximately 64% of Monson's housing units were deteriorated and 23% were dilapidated (see Table 10).

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Monson	3	14	3	14	7	32	4	18	5	23	22

Source: Tulare County 2015 Housing Element

## Age of Structures

Most of the homes in Tulare County were built between 2000 and 2013. (see Table 11) The total number of housing units in Monson is 62.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Monson
Total housing units	13,845,790	144,792	62
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	19
Built 2000 to 2009	1,646,490	25,997	24
Built 1990 to 1999	1,495,571	21,767	0
Built 1980 to 1989	2,117,819	22,733	0
Built 1970 to 1979	2,503,688	27,111	3
Built 1960 to 1969	1,871,029	15,500	4
Built 1950 to 1959	1,907,512	13,694	0
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	12

2011-2015 American Community Survey 5-Year Estimates

# MONSON HAMLET PLAN

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Monson was 5.23 persons per household (see Table 12). The average owner occupied household size is 3.45.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Monson	3.45	5.23-

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Monson was 0.0%, which was lower than both Tulare County at 4.2% and the State of California at 4.6%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Monson	0.0%	0.0%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)



# MONSON HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

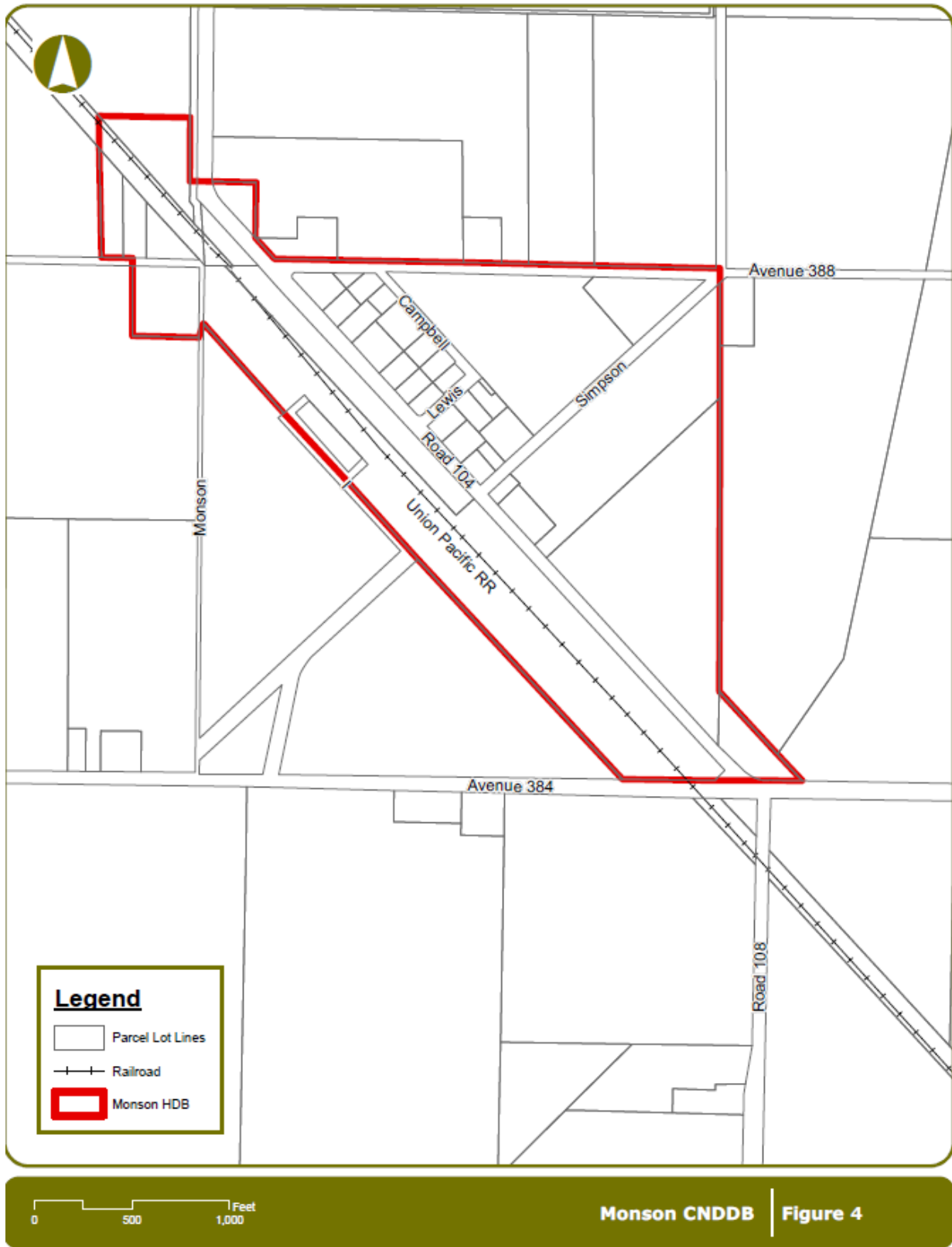
A California Natural Diversity Database (CNDDB) search conducted on November 1, 2016 (**see Figure 4**), indicates there are special status species within the Monson Quadrant Species List (which includes the Monson Planning Study Area) consisting of three (4) animal species and two (2) plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); California tiger salamander (*Ambystoma californiese*, federal and state threatened); Vernal pool fairy shrimp (*Branchinecta lynchi*, federal threatened); Vernal pool tadpole shrimp (*Lepidurus packardi*, federal endangered), Hoover's spurge (*Euphorbia hooveri*, federal endangered); and San Joaquin Valley Orcutt grass (*orcuttia inaequalis*, federal threatened and state endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDB/Maps-and-Data> accessed November 1, 2016.

# MONSON HAMLET PLAN

Figure 4 - CNDDDB Map



# MONSON HAMLET PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Monson are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, Monson is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Monson (see Figure 5). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**Calgro- Calgro, saline-Sodic, complex, 0 to 2 percent slopes,** Erosion potential is low to moderate and shrink swell potential low.

**Crosscreek-kai association, 0 to 2 percent slopes,** is formed by the chemical and mechanical alteration of the Kai soil that originally formed in alluvium derived from granitic rock sources. The soil is well drained, and the permeability is moderately slow above the duripan due to sodicity and very slow in the duripan. The available water capacity is moderate to high and the shrink-swell potential is moderate.

The parent material of Kai Soil is alluvium derived mainly from granitic rock sources.

The soil is moderately well drained and the permeability is moderately slow above the duripan and very slow in the duripan. The available water capacity is low to moderate and the shrink-swell potential is moderate.

Building site development management factors include very slow permeability of the duripan, moderate to moderately rapid permeability below the duripan, high corrosivity to steel in both soils and high corrosivity to concrete in the Crosscreek soil. The land capability classification: Crosscreek-unit, 2s-6, irrigated; subclass, 6s, non-irrigated; Kai- unit, 3s-6 irrigated; subclass, 7s, non- irrigated, MLRA17. This is not considered to be prime farmland.

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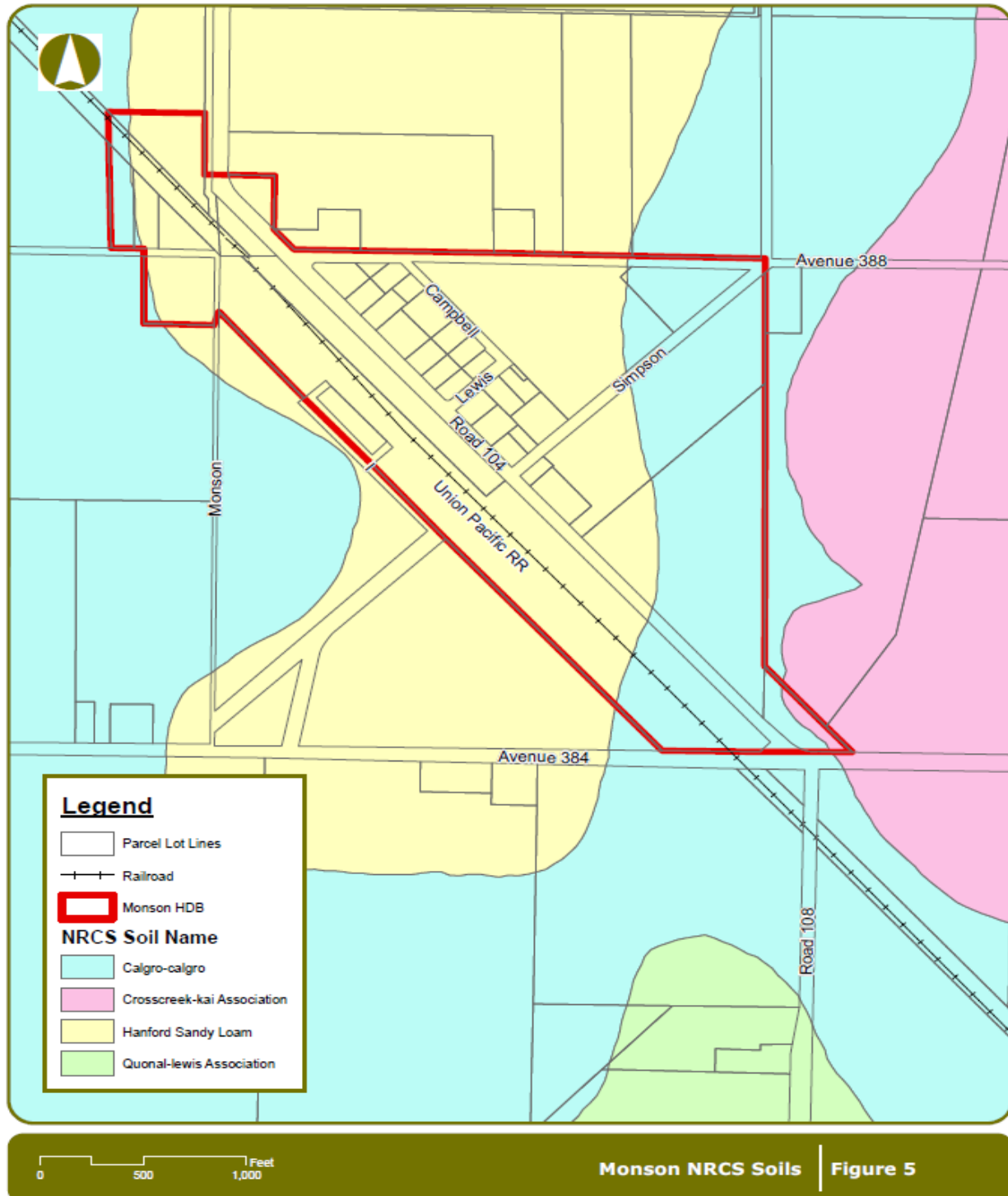
<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 – accessed August 17, 2016.

# MONSON HAMLET PLAN

**Hanford sandy loam, 0 to 2 percent slopes,** consists of very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains, and alluvial fans. Hanford soils are used for growing a wide range of fruits, vegetables, general farm crops, dairies and for urban development.

**Figure 5 - NCRS Soil Map**



# MONSON HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Monson Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Monson is located near the northwest end of the Valley with prevailing winds from the northwest, it is susceptible to accumulate adversely modified air, particularly

# MONSON HAMLET PLAN

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when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Monson including agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0630E (**see Figure 6**), Monson is located within Flood Zone X, – Areas of 0.2% annual chance flood; area of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood. Monson is also located within the FEMA 500 year flood zone. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). “Floodplain” or “flood-prone area” means any land area susceptible to being inundated by water from any source. “Base Flood” is the flood having a one percent chance of being equaled or exceeded in any given year. “One-hundred-year flood” or “100 year flood” has the same meaning as “Base Flood.” “Special Flood Hazard Area” is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. “Floodway” means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

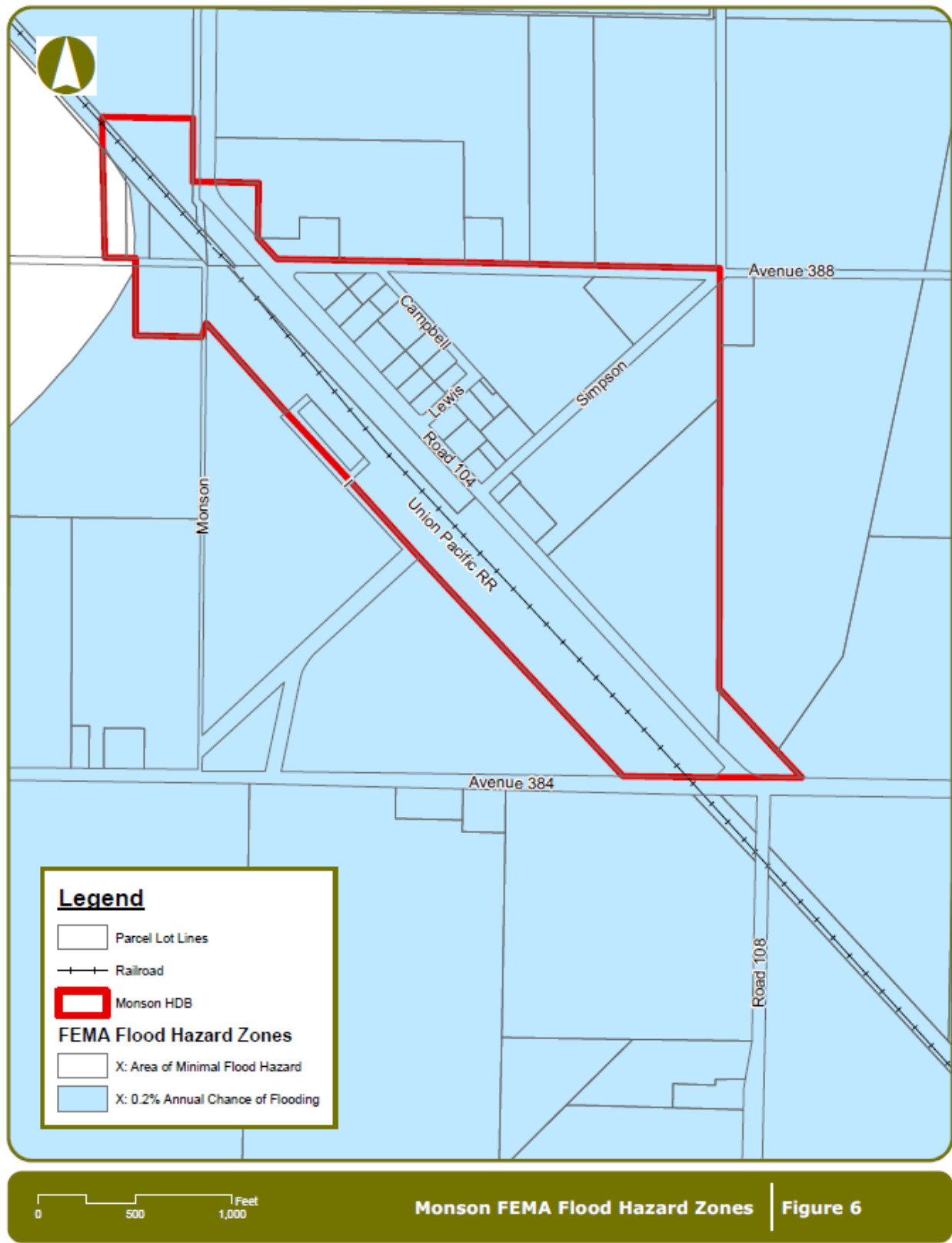
The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

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<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0630E, Panel Number 630 of 2550, June 16, 2009. <https://msc.fema.gov/portal/> accessed August 17, 2016.

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Figure 6 - FEMA Flood Map





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## Noise<sup>7</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. **Table 14** summarizes the daily traffic volumes along Avenue 384 from Road 80 to State Route (SR) 63.

Table 14 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
Avenue 384 from Road 80 to State Route 63	3,530	25	55	118	253

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks).

### Energy: Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom SCE has arrangements, and rolling blackouts.

<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)



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## Water Quality

Monson until recently was not served by a public water system. Approximately 40 homes in the community of Monson obtained their drinking water from private domestic water wells, many of which are shared between multiple households. Nitrate levels in the area's groundwater have long been known to be high. In 2008, testing of a dozen domestic wells was conducted in the community of Monson. Testing results revealed nitrate levels between 46 and 130 parts per million (ppm), which are all over the EPA and California Maximum Contaminant level (MCL) of 45 ppm. Furthermore, the average level of nitrate contamination among the wells tested was 105 ppm which is over twice the MCL.

The community had been concerned about the quality and safety of their water, it is hard to know how to solve the community's water problems. SGC survey results show that 82% of people (that attended the SGC meeting) would like to be connected to a community water system, 18% did not respond to the question.

Monson is comprised of approximately 37 lots with approximately 40 residential units and a local store. All the residential lots had been served by a private water well and septic tanks. The local store was also served by an individual private water well but is still regulated as Transient Non-Community Water System (TNCWS).

Almost all of the wells had been shallow. Typical wells were 80 feet deep. Data indicate that between Fall of 2011 and Spring of 2014 water levels in one of the wells declined from 19 feet below ground surface to 49 feet - a drop of 30 feet and water levels in well in other wells have declined from 27 feet below ground surface to 63 feet a drop of 36 feet and this drop in the water table is accelerating as the drought continues. In May 2014 and July 2014, the depth to water in a single domestic well in Monson was measured. Results of the measurements show the water level dropped from 54 feet to 63 feet (9 feet) between May and July of 2014.

In 2015-16 funding was obtained by the County and a project was designed (**see Figure 7**) and installed in 2017, for Monson the project consisted of developing a well site to include a well, 50,000-gallon water storage tank, installation of approximately 32 water services (more services were added beyond those originally indicated on the sheet) and the associated distribution piping to serve residents. A layout of the system and the services is shown on the attached drawing. The project was funded by the United States Department of Agriculture Emergency Community Water Assistance Grant, a grant from the State Water Resources Control Board and a grant from the Department of Water Resources. Construction began in March of 2017 and was completed in September of 2017, Sultana CSD will manage the system.



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## Sewer

No public sewer facilities are available in Monson. Each land owner relies on septic systems for wastewater.

## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration

Monson does not currently have a storm drainage system.”<sup>8</sup>

## Solid Waste

Solid waste disposal services for the Community of Monson is provided by Pena’s Disposal, a private company. Solid waste generated in Monson can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia.

## Roads

There are several roadways in Monson that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

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<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

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Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>9</sup>

**Table 15** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 7** graphically displays this information on a map.

**Table 15 - Roads in Need of Major and Medium Repair in Monson**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 388	Monson Drive to Road 108	CHIP
2	Monson Drive	Simpson Drive to Road 104	CHIP
3	Monson Drive	Avenue 384 to Simpson Drive	GRX
4	Road 104	Avenue 388 to Monsoon Drive	CHIP
5	Simpson Drive	Avenue 384 to Monson Drive	CHIP

OLAY – overlay resurfacing operation	ACST – asphalt reconstruction
CHIP – chip seal	RCST – cold mix reconstruction
GRX – grind and remix	

(Source: County of Tulare Public Works, 2012)

## Street Lights

**Table 16** identifies the location of existing street lights that are maintained by Tulare County, in Monson, as well as their specifications. **Figure 8** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.

**Table 16 - Specifications of Existing Street Lights**

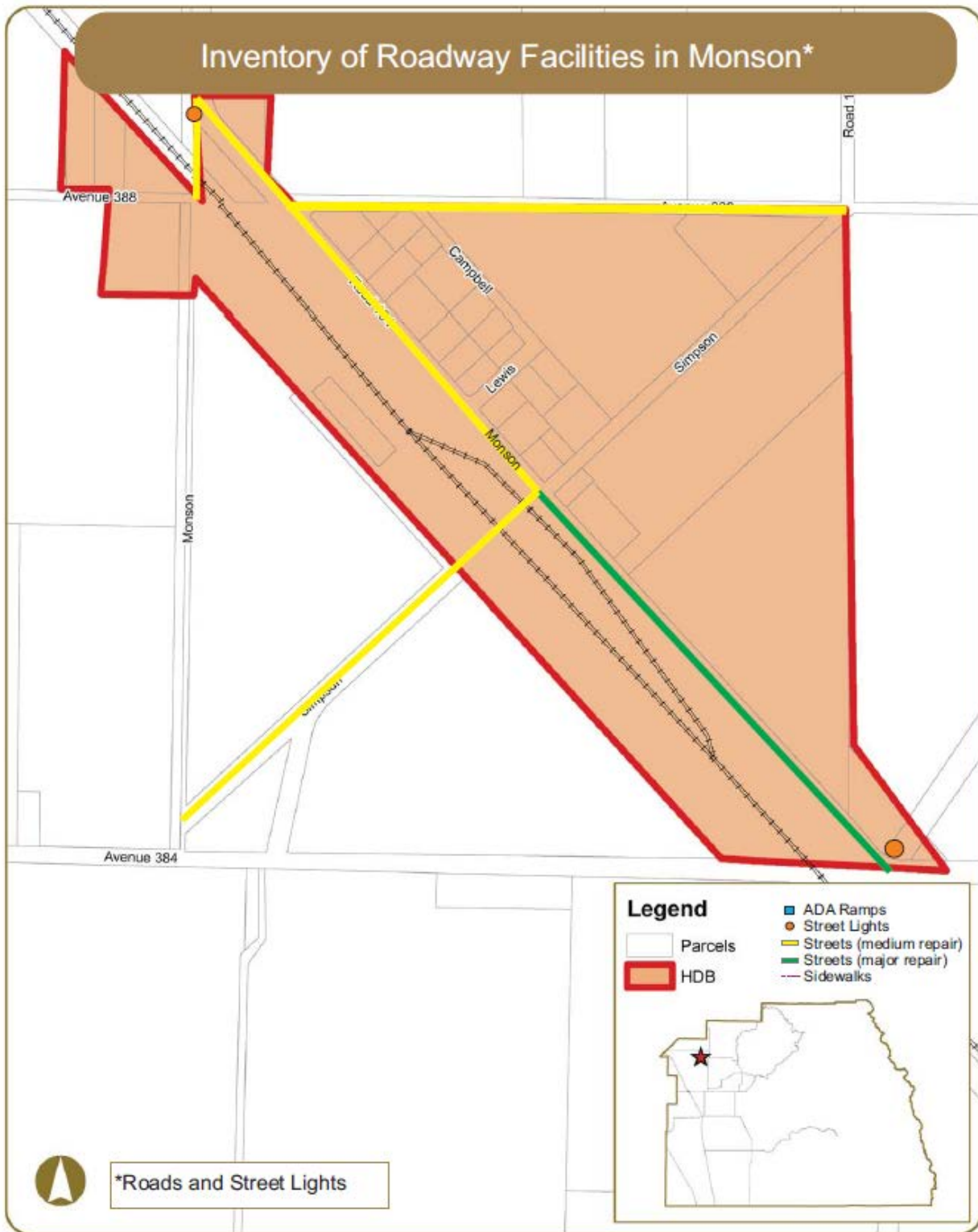
Specifications of Existing Street Lights								
No.	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 384	Monson Drive	NE Corner	5	9500	W	S	PG&E
2	Road 104	Monson Drive	NE Corner	2454	9500	W	E	PG&E

(Source: Tulare County Public Works, March 2013)

<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

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Figure 8 - Monson Inventory of Roadway Facilities



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## Sidewalks

There are currently no sidewalks located within Monson.”<sup>10</sup> Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Monson.”<sup>11</sup>

## Transit and Bus Stops

County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare’s urban transit systems. Public transit is currently not available in Monson. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents.

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<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element



# MONSON HAMLET PLAN

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The low level of auto congestion in Monson, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified Road 80 as a proposed Class III and State Route 63 as a proposed Class II Bike Projects<sup>12</sup>. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are rarely used outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. There are two bike route near the community of Monson, Road 80 and State Route 63.

## AMTRAK

The Hanford AMTRAK station, located approximately 45 miles to the southwest in Kings County, is the closest station to Monson providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport is approximately 25 miles south. Fresno Yosemite International Airport (FAT), approximately 40 miles northeast of Monson, is a principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles southeast of Monson and offers direct flights to several destinations

## **PUBLIC SERVICES**

### Community Resource Center

The community lacks many resources and needs a place where residents can go to access information and county wide resources. People in Monson expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Monson residents. This could also be the place where residents access a reliable internet connection.

### Sheriff

Police protection services are provided in Monson by the Tulare County Sheriff's Department sub-station, located 40765 Road 128, in Culter, approximately five (5) miles northeast of Monson. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

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<sup>12</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

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## Fire

There are currently no Fire Hydrants in Monson. Fire protection and emergency medical services are provided for Monson by the Tulare County Fire Department. The community is served by Dinuba Station #3, located at 40404 Road 80, in Dinuba. Parade 1, Engine 3 and Water Tender 3 is assigned to this location.

## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Monson Hamlet Plan Area is within the Monson-Sultana Joint Union School District. The District provides K-8 grade education at its Monson-Sultana School four (4) miles away. High School students attend high school in Dinuba. Monson is within the Dinuba Union Joint High School District.

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>13</sup> The closest library is located in Dinuba, approximately eight (8) miles to the northwest (see Table 17).

Table 17 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Dinuba	Dinuba Branch Library 150 South I Street Dinuba, CA 93618-2399 (559) 591-5829	Tuesday to Friday: 9:00 a.m. – 1:00 p.m., 2:00 p.m. – 6:00 p.m.

Library hours current as of September 2017

## Parks

Presently there are no County owned/operated public parks in Monson. The closest public park to the community is Dinuba Community Park, located approximately eight (8) miles to the northwest.

# THE MONSOON HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies

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<sup>13</sup> General Plan Background Report, page 7-96



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the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>14</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>15</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation and Transportation Conditions

Monson is a small agricultural community located near State Route 201. The major rural collector roads is Avenue 308. There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

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<sup>14</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>15</sup> Tulare County General Plan Background Report, page 5-7.

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## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

## GOALS, OBJECTIVES AND POLICIES

This Section of the Monson Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Monson to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan, Monson Hamlet Plan, and input received from Monson citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned shopping areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.

# MONSON HAMLET PLAN

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5. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
6. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
7. The County shall work with the Schools to provide safe routes to school.
8. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Monson Community Plan.
9. The Monson Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.
10. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Monson.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Monson.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

# MONSON HAMLET PLAN

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## Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

## Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

## Policies:

1. Encourage the Monson to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

## Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## **Environmental Quality and Public Safety**

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Monson citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

## Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

# MONSON HAMLET PLAN

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Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Protect Agricultural Lands:

Land within the respective Monson, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate.

When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Monson.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Monson.

# MONSON HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 115 acre Monson Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 86.0% of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 237 by 3.89 urbanized acres (area within the HDB), which equals 61 persons per urbanized acre (**see Tables 18 and 19**) showing population projections). Projecting the population at a 1.3% growth rate adds 51 persons by year 2030.

### Population Growth Forecast

**Table 18 - Monson Population Projections**

Growth Rate	2015	2020	2030
0.013	237	253	288

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The Year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>16</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2015 the community had 62 dwelling units (including vacant dwellings) with a population of 237. At an annual growth rate of 1.3%, the projected housing units are 66 and 75 in years 2020 and 2030, respectively, and projected population is 263 and 288 in years 2020 and 2030, respectively.

**Table 19 - Population Projections**

Year	Population	Growth (%)
2015	237	0.013
2016	240	0.013
2017	243	0.013
2018	246	0.013
2019	250	0.013
2020	253	0.013
2021	256	0.013
2022	259	0.013
2023	263	0.013
2024	266	0.013
2025	270	0.013
2026	273	0.013
2027	277	0.013
2028	280	0.013
2029	284	0.013
2030	288	0.013

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

<sup>16</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

# MONSON HAMLET PLAN

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## Infrastructure

In order for more development to occur, water service levels need to be expanded, for which grant funding will be required.

## Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require a discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

## Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET

Based on the 2011-2015 American Community Survey (**see Table 20**), the educational barrier in Monson begins in grade school. Of the adults age 25 and older, 20.5% had an educational level of less than 9<sup>th</sup> grade. A lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should to begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

<b>Table 20 - Education Attainment</b>			
<b>Educational Attainment</b>	<b>California</b>	<b>Tulare County</b>	<b>Monson</b>
Population 25 years and over	25,257,858	260,964	127
Less than 9th grade	10.0%	20.8%	20.5%
9th to 12th grade, no diploma	8.2%	10.9%	7.9%
High school graduate (& equivalency)	20.7%	25.2%	37.8%
Some college, no degree	21.8%	22.3%	0.0%
Associate's degree	7.8%	7.0%	7.1%
Bachelor's degree	19.8%	9.5%	23.6%
Graduate or professional degree	11.6%	4.3%	3.1%

2011-2015 American Community Survey 5-Year Estimates



# MONSON HAMLET PLAN

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 21**) and Zoning Districts (**see Table 22**) have been updated.

<b>Table 21 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	104.8
Right-of Ways	14.9
Monson HDB	119.7

<b>Table 22 - Proposed Zoning District</b>	
Proposed Zones	Acres
AE-10	43.9
C-2-MU	19.1
M-1	31.6
R-A-43	10.2
Right-of Ways	14.9
Monson HDB	119.7

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations contained in the Monson Hamlet Plan. The following components comprise the Monson Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Monson Hamlet Plan, there are several proposed changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the Uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Monson.



# MONSON HAMLET PLAN

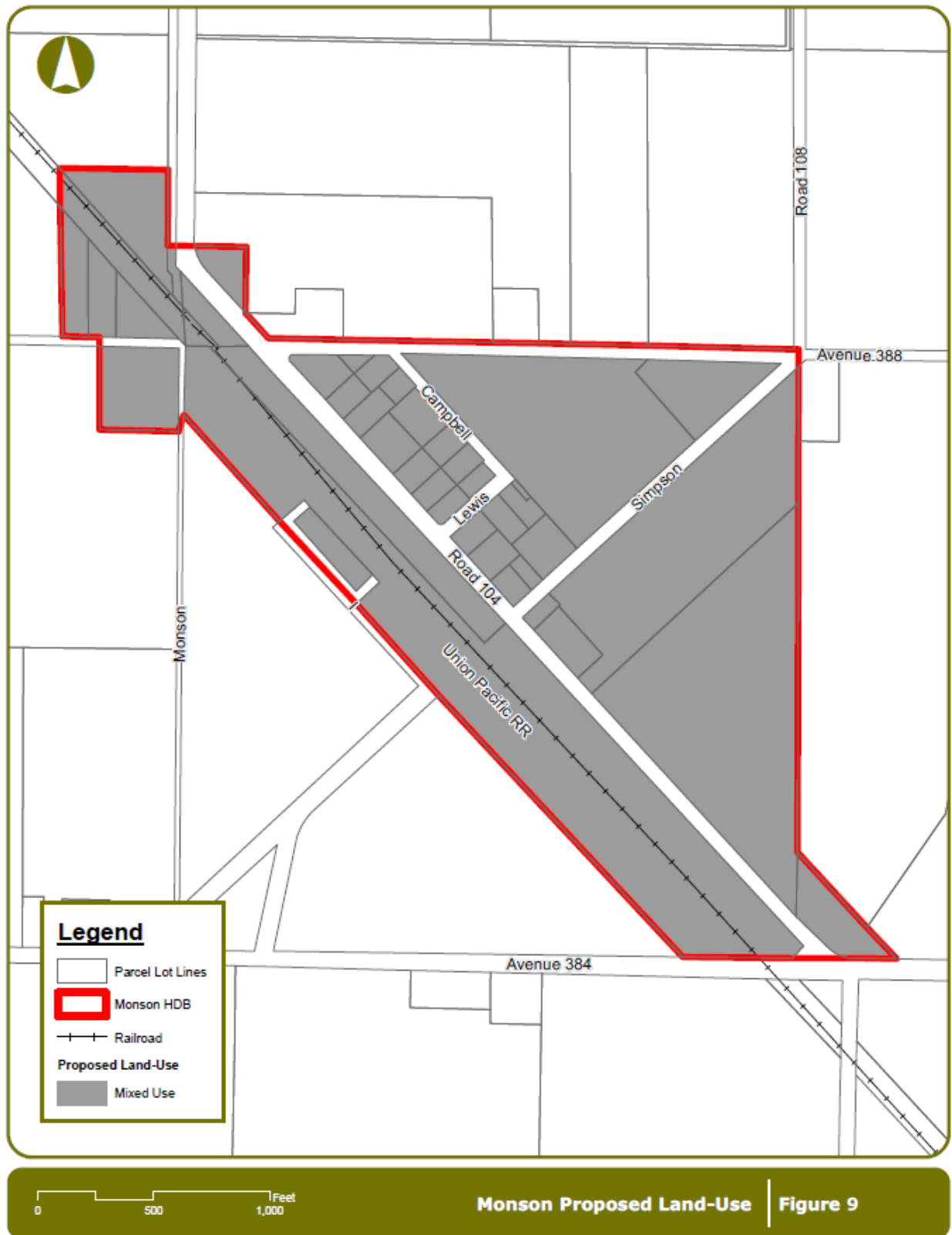
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## Zoning Map Update

The current Zoning Map for Monson (**see Figure 10**) will be amended to be compatible with the Land Use Map (**see Figure 9**) outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance (**see Figure 11**) to be in conformity with each other Monson Proposed Zoning. **Figure 12** show the proposed Monson Urban Development Boundary.

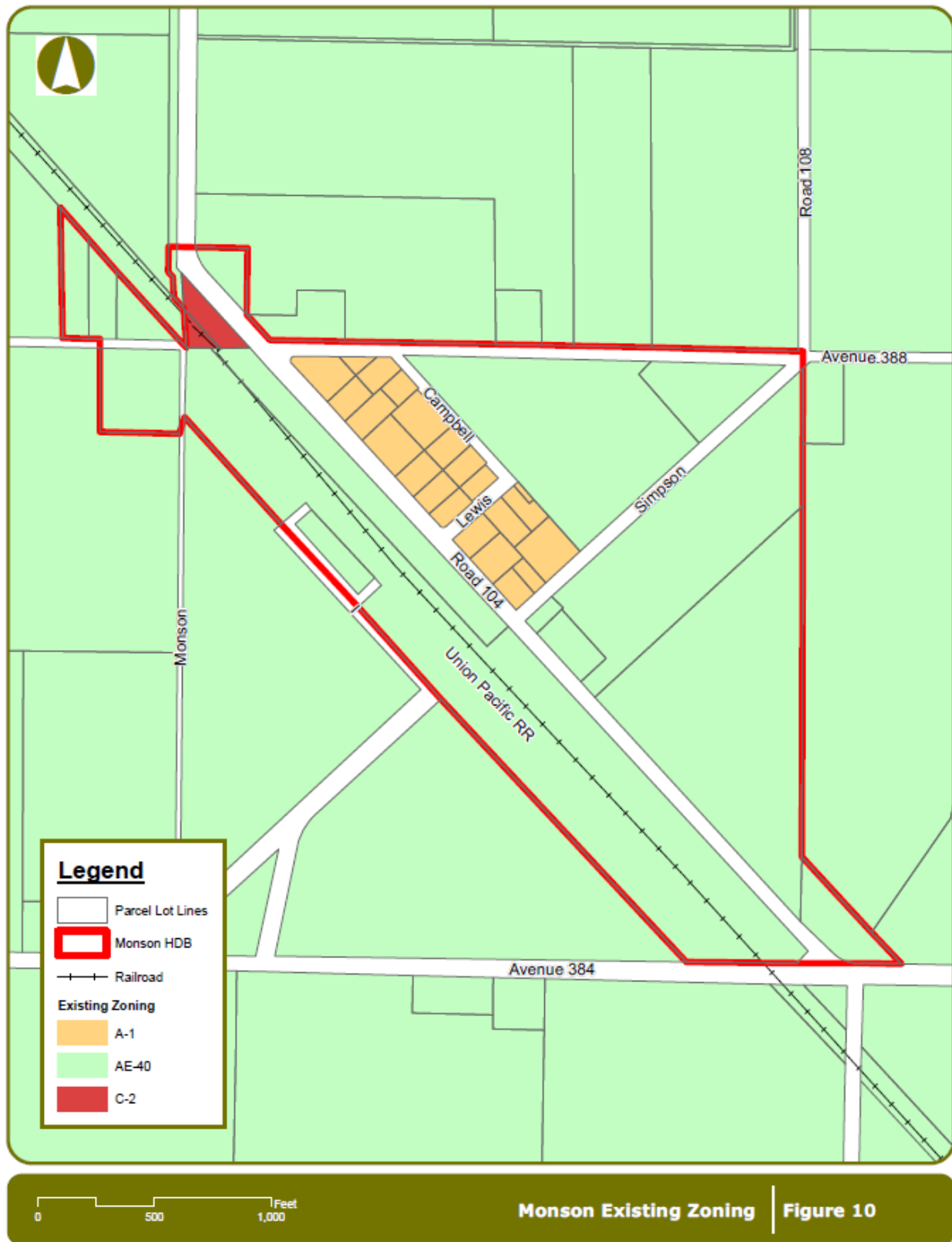
# MONSON HAMLET PLAN

Figure 9 - Proposed Land Use Plan



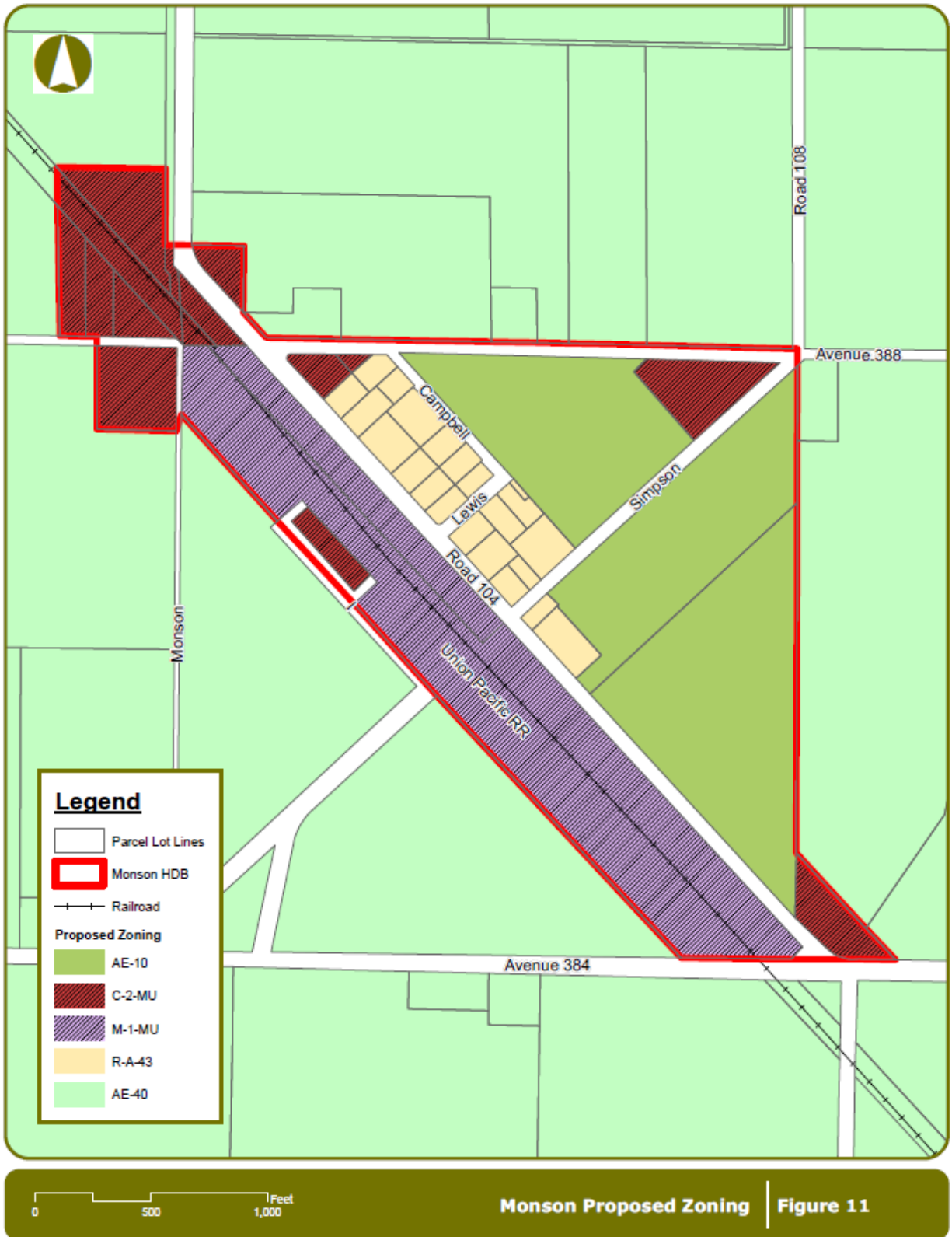
# MONSON HAMLET PLAN

Figure 10 - Existing Zoning Districts



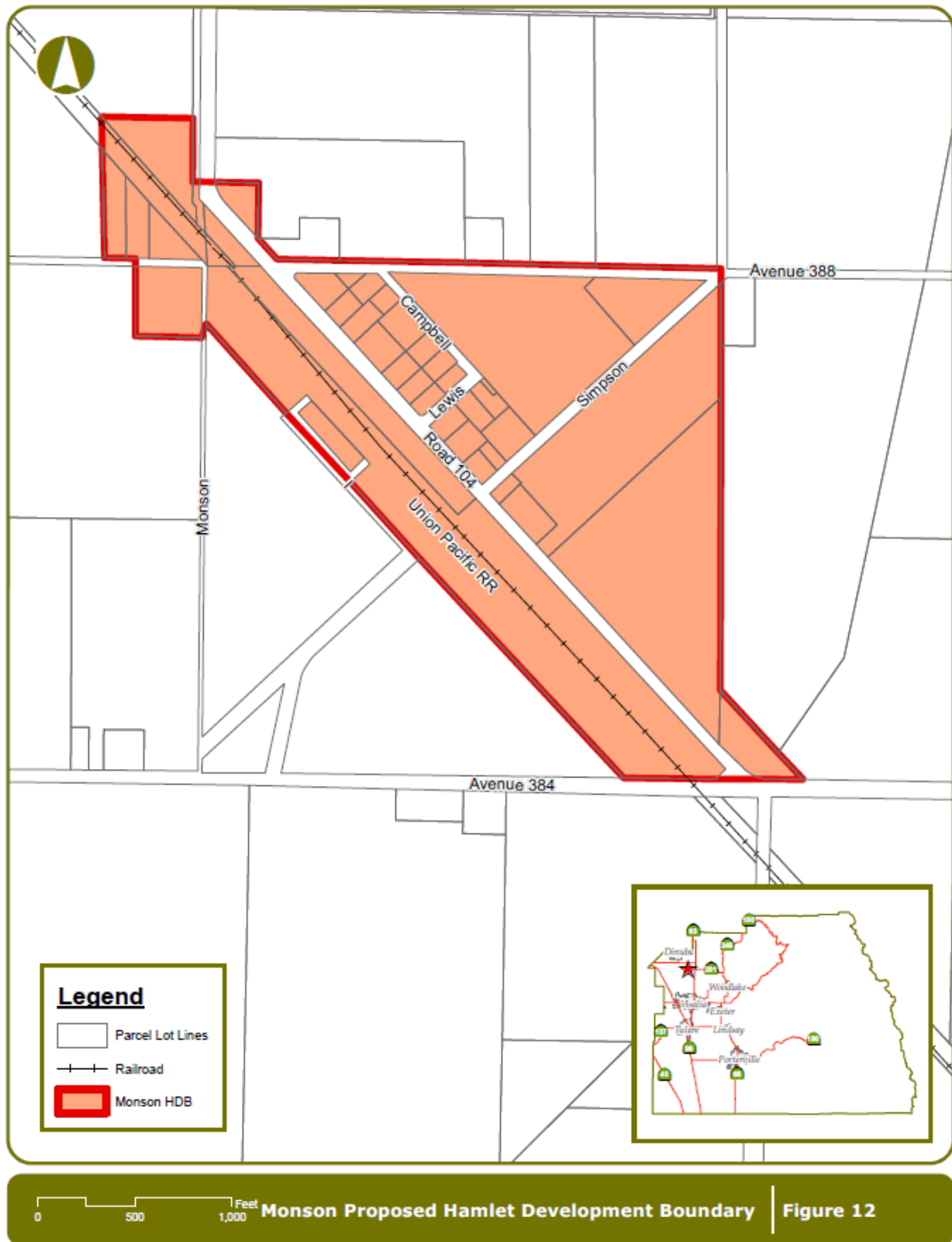
# MONSON HAMLET PLAN

Figure 11 - Proposed Zoning Districts



# MONSON HAMLET PLAN

Figure 12 - Proposed Monson HDB



# MONSON HAMLET PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning District)

# MONSON HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc.) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	
Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1

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Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1



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Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone

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Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1

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Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	

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Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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## A-2 *Mixed Use Overlay District (Zone Change Text)*

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The following regulations shall apply in the community of Monson, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Monson.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Monson.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Monson. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.



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## **A-3     *Development Standards (Mixed Use Zoning District)***

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To promote Economic Development within the Monson Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Monson. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Monson Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Monson. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1     Entries to buildings should be individualized and clearly identifiable.

A-2     Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3     Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4     The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5     Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6     Each building should have a defined base, body, and cap segment

A-7     Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8     Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9     Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10    Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11    Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.



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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

# MONSON HAMLET PLAN

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.



# MONSON HAMLET PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# MONSON HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.



B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:       None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.



The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

## COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT )  
TO SECTION 16.H OF ORDINANCE NO. 352 )  
TO ALLOW ADDITIONAL BY-RIGHT USES ) RESOLUTION NO. 9369  
CONSISTENT WITH THE ADOPTED HAMLET )  
PLANS, AS PROPOSED IN CHANGE )  
OF ZONE NO. PZC 17-039 )

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map



***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

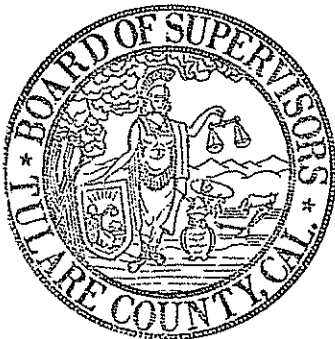
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## Seville Hamlet Plan

# SEVILLE HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# SEVILLE HAMLET PLAN

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# SEVILLE HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# SEVILLE HAMLET PLAN

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## SEVILLE HAMLET PLAN

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# Seville Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Seville Hamlet Plan: GPA 17-019  
Zoning District Map: PZC 17-019



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# SEVILLE HAMLET PLAN

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# SEVILLE HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# SEVILLE HAMLET PLAN

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# SEVILLE HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Michael Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Hector Guerra, Chief Planner, Environmental Planning

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# SEVILLE HAMLET PLAN

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# SEVILLE HAMLET PLAN

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# SEVILLE HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Seville is to develop a plan which can accurately reflect the needs and priorities of this community. Seville is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks, etc.) and to stimulate economic development within the community.

### Location

Seville is a census-designated place located in the northwest portion of Tulare County (see **Figure 1**). It is generally bounded by Inyo Avenue in the south, Avenue 384 in the north, Road 152 in the west, and east of Road 156, roughly 8.5 miles southwest of Cutler in Tulare County. Seville is in Sections 01, 02 & 35. Townships 16 & 17 South, Range 25 East, MDB&M, and can be found within Ivanhoe United States Geological Survey 7.5 minute topographic quadrangle. Seville is located at an elevation of 354 feet National Geodetic Vertical Datum. The coordinates of Seville are: Latitude: 36.4857, Longitude: -119.2248. Seville is directly served by State Route (SR) 201 (see **Figure 2**).

### Planning Area

The Seville Hamlet Development Boundary (HDB) area consists of 178.5 acres. The Land Uses within the HDB include agriculture (orchards and pasture), which currently occupy 21.5 percent of the 178.5 acres. Urban development, including urbanized uses such as residential, commercial, public and quasi-public facilities, as well as industrial development occupy 67.7 percent of the 178.5 acres. The remaining 21.4 percent are lands dedicated for Right-of-way (See **Table 1**).

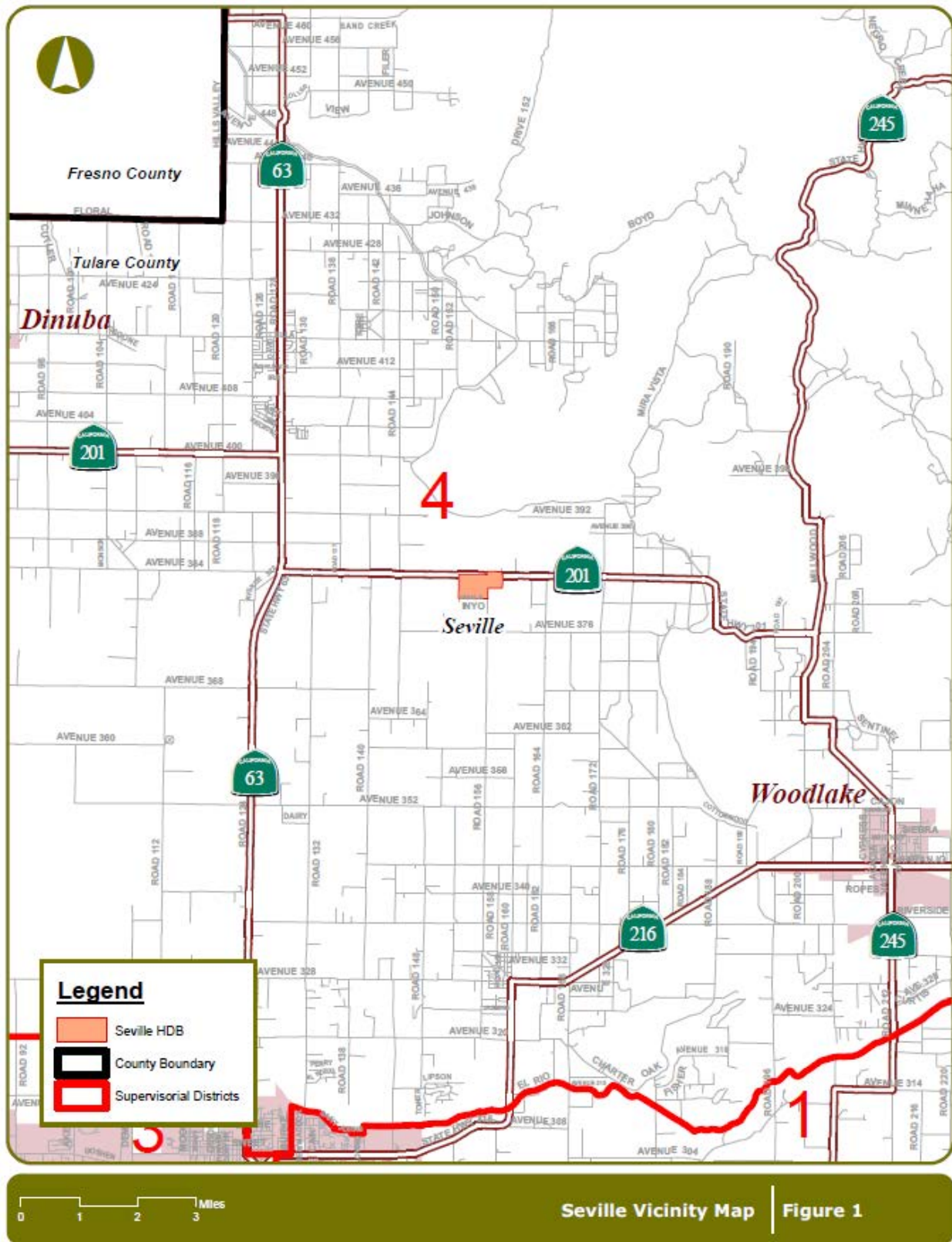
Table 1 - Seville Land Use	
Land Use	Acres
Residential	90.5
Commercial	7.6
Industrial	15.3
Agricultural	38.5
Public/Institutional	7.6
Rights-of-way	38.2
Total	178.5

Source: Tulare County GIS

No change is proposed to the Hamlet Development Boundary (see **Figure 3**).

# SEVILLE HAMLET PLAN

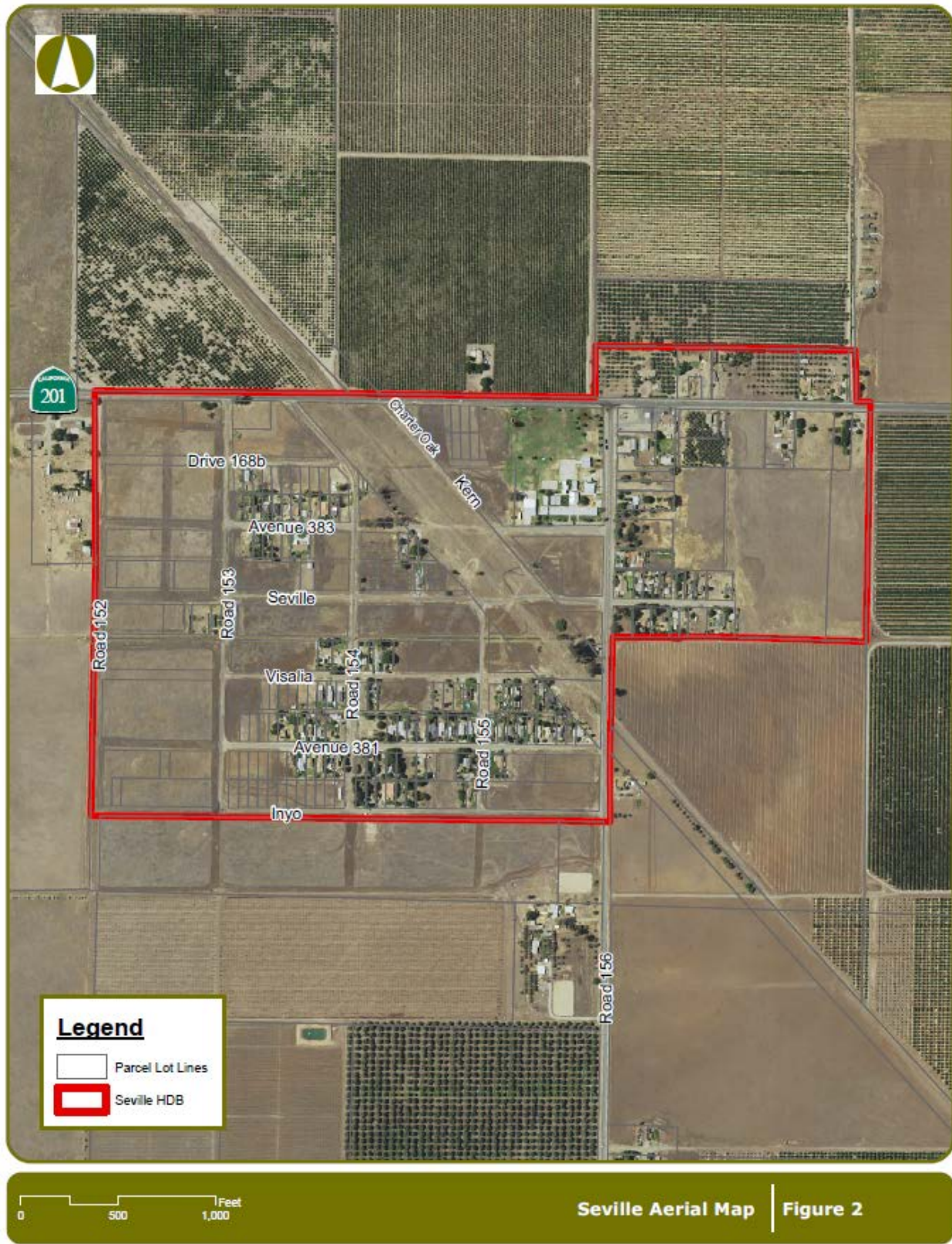
Figure 1 - Vicinity Map





# SEVILLE HAMLET PLAN

Figure 2 – Aerial Seville





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# SEVILLE HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises (SHE) went door to door throughout the community of Seville to distribute flyers at every home, business and school. SHE emphasized to residents about the importance of participating in this process. SGC partnered with the Stone Corral School and sent meeting flyers home with every student in the school district. Meeting invitation were sent to the members of the Stone Corral School Board Members. Fliers were posted at the local store.

### Community Meeting

The Strategic Growth Council held a community input meeting in Seville on Wednesday, December 9, 2015 at 5:30 pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Stone Corral School located at 15590 Ave 383 in Seville. More than 8 surveys and 9 Community Support Petitions were collected in Seville.



### Community Survey

A survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

### Priority Concerns

1. Reliable Internet Service
2. Road Conditions
3. Natural Gas Lines
4. Side Walks

# SEVILLE HAMLET PLAN

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5. Community Park
6. Street Lights
7. Grocery Store

## **Natural Gas**

Residents report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that residents rather have natural gas but cannot afford the cost of extending gas lines to their homes. Seville residents are requesting assistance from the County to help with the cost of extending natural gas lines to Seville. This is the most important issue to all residents that were surveyed.

## **Road Improvements**

Road conditions are a big concern to Seville residents. It is reported that road conditions are poor and some roads need work, streets have potholes, cracks and bumps. Drivers feel unsafe and are worried about damage to their vehicles while driving on most roads. According to residents, Seville roads are not regularly maintained. The following roads reported by the residents to be top priority:

- Inyo Ave
- Road 154
- Ave 383
- Ave 381

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Seville. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Side Walks**

The community of Seville has too few sidewalks. The residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids while walking and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk around town in the mud and through big puddles of water. The absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Reliable Internet Access**

Most families in Seville do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. Without access to the internet, Seville residents are unable to do things such as online job hunting, apply for resources, or provide homework help to their kids.

# SEVILLE HAMLET PLAN

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## **Community Park**

In the community of Seville there is no safe public green space for children to play and adults to be physically active. The nearest park is located 8 miles away in the city of Oroquieta. Residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and concerned for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. Residents express that having a community park is a major priority for the

## **Local Market**

The community of Seville is a food desert. This community does not have a local food market that sells fresh produce, meat and dairy. There is one corner gas station that sells mostly highly processed, packaged food that is not considered nutritious. Seville residents regularly travel to Oroquieta (8 miles) or Visalia (14 miles) to buy their groceries.

# SEVILLE HAMLET PLAN

## SEVILLE TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Seville.

#### Population

In 2015, Seville Population was 545. (See Table 2)

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Seville</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	303	55.6%
Female	19,334,329	50.3%	226,607	49.9%	242	44.4%
Total	38,421,464		454,033		545	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Seville's population during the Planning Period (see Table 3).

<b>Table 3 - Projected Annual Growth Rate</b>		
	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7

# SEVILLE HAMLET PLAN

## Median Age

Seville's population's median age is younger than the median age of both Tulare County, as well as the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Seville	24.3

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 2.8% of Seville's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 0% was two races or more (see Table 5). Approximately 97.2% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community. (see Table 5)

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Seville	%
Total	38,421,464		454,033		545	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	530	97.2%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	15	2.8%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Seville

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Seville was about 9.9% while the rate for Tulare County was 13.5%. The unemployment rate for the State of California was 10.1%. Keeping in mind that the 9.9% includes only the employable labor force (that is, not every person of the population) results in about 36 unemployed persons of Seville's person labor force of 227.

# SEVILLE HAMLET PLAN

Table 6 - Employment Status			
Employment Status	California	Tulare County	Seville
Population 16 years & over	30,312,429	325,404	365
In labor force	19,269,449	194,420	227
Civilian labor force	19,137,441	194,102	227
Employed	17,246,360	170,780	191
Unemployed	1,891,081	23,322	36
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	138

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Seville's median household income was \$ 23,750, whereas the State of California's median household income was \$61,818 and \$42,031 for Tulare County. (see Table 7)

Table 7 - 2011-2015 American Community Survey Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Seville CDP	\$23,750	\$19,957	\$6,829

2011-2015 American Community Survey 5-Year Estimates

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Seville's median household income was \$23,750, whereas the State of California's median household income was \$61,818. Seville's median household income was 38.4% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes) (see Tables 8 and 9).

Table 8 - Housing Tenure			
	California	Tulare County	Seville
Occupied housing units	12,717,801	133,570	111
Owner-occupied	6,909,176	75,685	31
Renter-occupied	5,808,625	57,885	80

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Seville
Owner-occupied	54.3%	56.7%	27.9%
Renter-occupied	45.7%	43.3%	72.1%

2011-2015 American Community Survey 5-Year Estimates



# SEVILLE HAMLET PLAN

## Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Seville	11	33	6	18	8	24	4	12	4	12	33

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Seville indicated that approximately 33% of the housing units were sound. Approximately 54% of Seville's housing units were deteriorated and 12% were dilapidated (**see Table 10**).

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009 (**see Table 11**). The total number of housing units in Tulare County is 144,792.

<b>Table 11 - Year Structure Built</b>			
Year Structure Built	California	Tulare County	Seville
Total housing units	13,845,790	144,792	129
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	17
Built 1990 to 1999	1,495,571	21,767	13
Built 1980 to 1989	2,117,819	22,733	24
Built 1970 to 1979	2,503,688	27,111	23
Built 1960 to 1969	1,871,029	15,500	12
Built 1950 to 1959	1,907,512	13,694	19
Built 1940 to 1949	865,607	7,494	21
Built 1939 or earlier	1,298,438	8,186	0

2011-2015 American Community Survey 5-Year Estimates



# SEVILLE HAMLET PLAN

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Seville was 50.8 persons per household (see Table 12). The average owner occupied household size is 4.48.

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Seville	4.48	5.08

2011-2015 American Community Survey 5-Year Estimates

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Seville was 10.1%, which was lower than both Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0.0%, which was lower than Tulare County at 1.7% and the State of California at 1.4% (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Seville	0.0%	10.1%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# SEVILLE HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

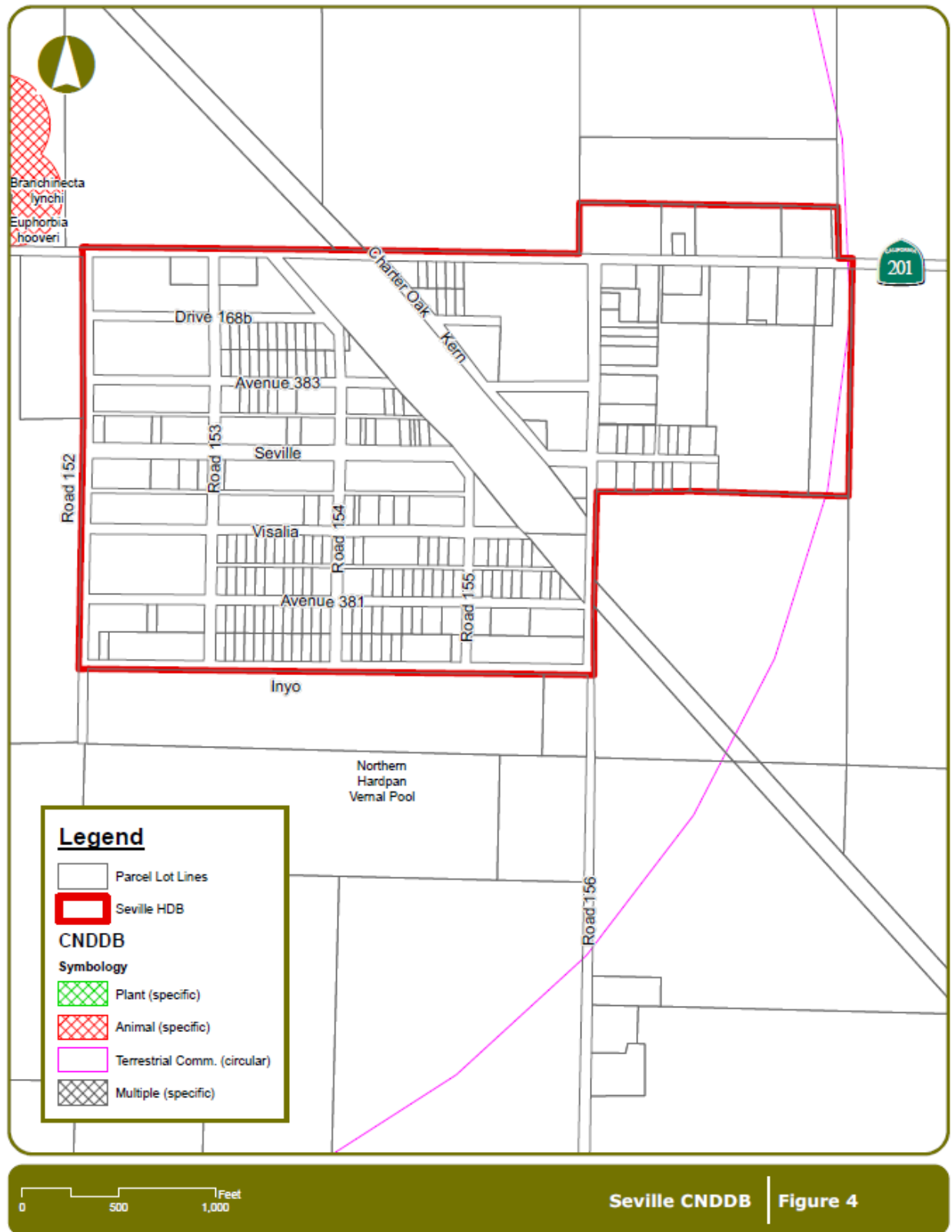
A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016 (**see Figure 4**) indicates there are special status species within the Ivanhoe Quadrant Species List (which includes the Seville Planning Study Area) consisting of four animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); California tiger salamander (*Ambystoma Californiense*, federal and stated threatened); vernal pool fairy shrimp (*Branchinecta lynchi*, federal threatened); vernal pool tadpole shrimp (*Lepidurus packardii*, federal endangered); San Joaquin Valley Orcutt grass (*orcuttia inequalis*, federal threatened and state endangered); Hoover's spurge (*Euphorbia hooveri*, federal threatened). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975, while the adobe starburst is has been determined to be "extirpated" (that is, "A species no longer surviving in regions that were once part of their range") and is no longer within the Seville project area.

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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> accessed November 1, 2016

# SEVILLE HAMLET PLAN

Figure 4 - CNDDDB Map



# SEVILLE HAMLET PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Seville are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Seville] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), (see Figure 3) and the Soil Survey of Tulare County, the following soil types are located in Seville. The following descriptions are provided for the above soil types: Accessed on August 17, 2016

**Centerville clay, 0 to 2 percent slopes**, is moderately deep and well drained. The soil is suitable for orchards, cultivated crops and rangeland. It is not considered suitable for buildings, roads and septic tanks due to the high clay content, slow permeability and inability to support heavy loads.

**Seville clay, 0 to 2 percent slopes, moderately deep**, moderately well drained soils; runoff is ponded to medium; slow permeability. Vegetation is dryland crops, irrigated pasture, row and irrigated field crops.

**Porterville clay, 0 to 2 percent slopes** deep well drained soils that formed in fine textured alluvial material from basic and metabasic igneous rock. Porterville soils are on fans and foothills, used mostly for range pasture. Vegetation is annual grasses, burclove, herbs and widely spaced shrubs. Most cultivated areas are irrigated and planted to oranges, lemons, olive, figs, and some grapes.

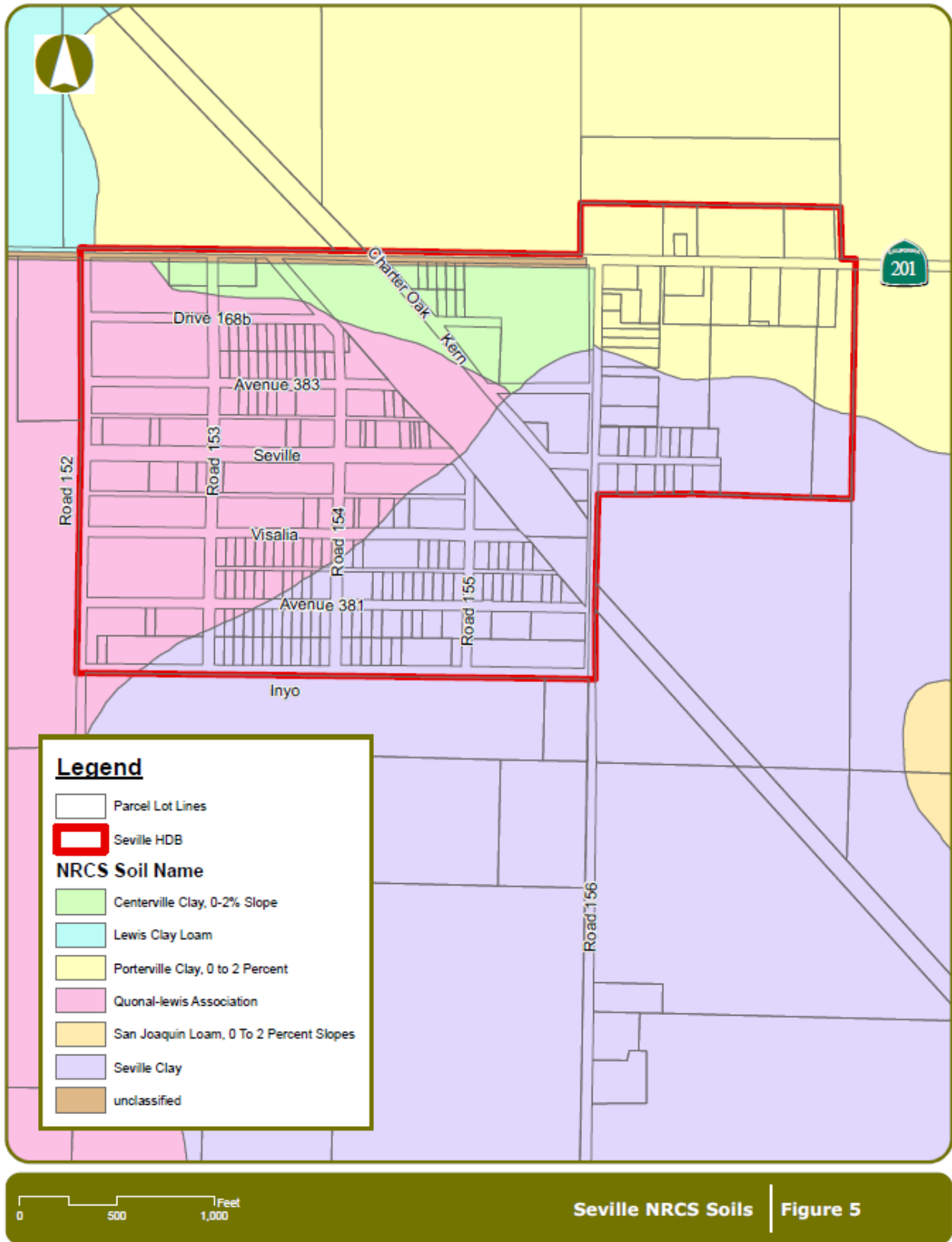
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# SEVILLE HAMLET PLAN

Figure 5 - NRCS Soils Map



# SEVILLE HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Seville Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Seville is located near the southwestern end of the Valley with prevailing winds from the northwest, it is susceptible to accumulate adversely modified air, particularly

# SEVILLE HAMLET PLAN

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when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Seville and within the community itself include State Highway 201 and agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0655E (**see Figure 6**), Seville is within Flood Zone X, areas of 0.2% chance flood; areas of 1 % annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance of flood. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each Community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use Planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”<sup>7</sup> Although some areas of Tulare County have experienced major flooding along its major rivers, the Seville Plan area has not. There are portions of Seville, however, that are within and adjacent to the FEMA 500 year flood zones.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Seville there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in Special Flood Hazard Areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

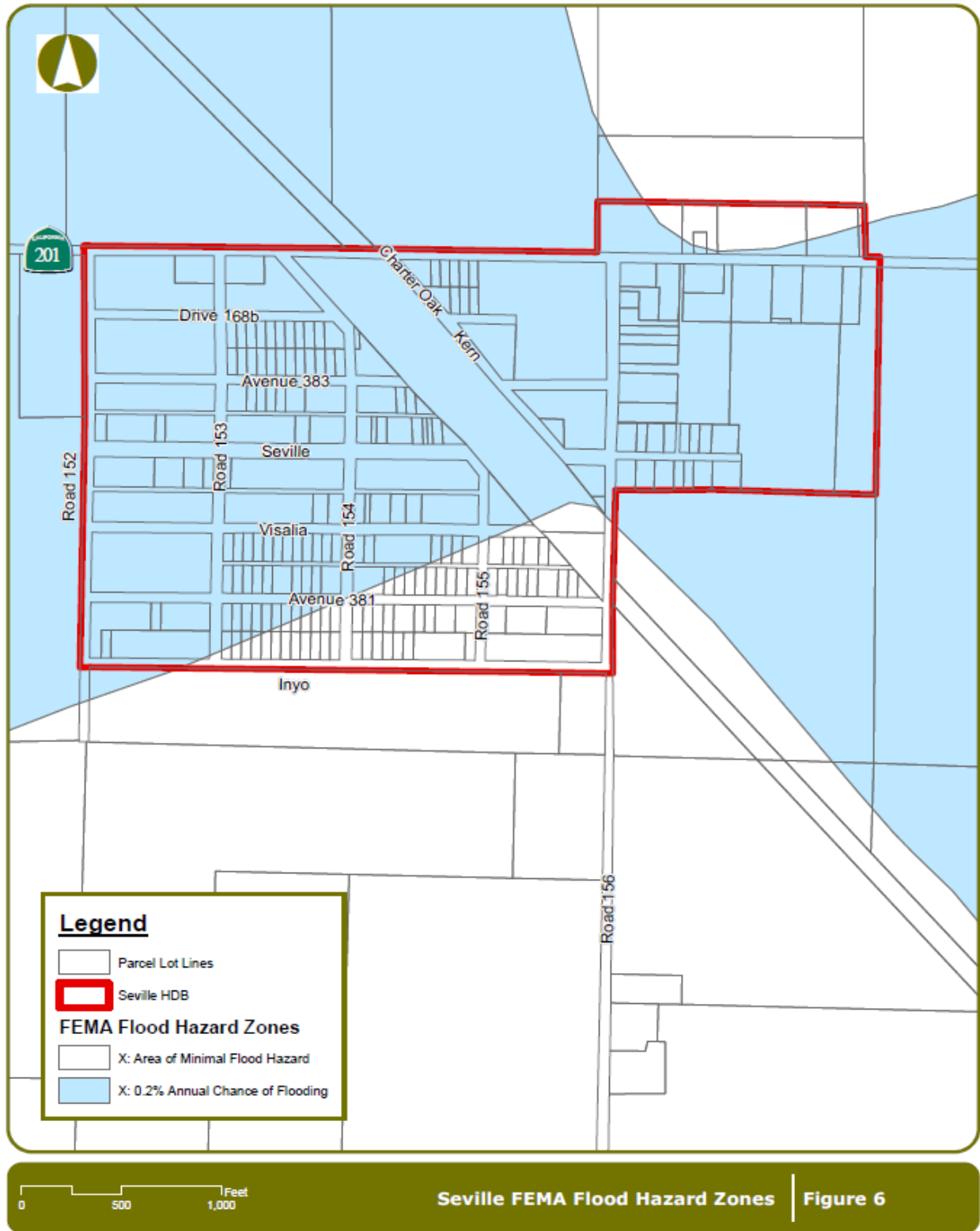
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<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0655E, Panel Number 655 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

<sup>7</sup> General Plan Background Report, page 8-14

# SEVILLE HAMLET PLAN

Figure 6 - FEMA Flood Map





# SEVILLE HAMLET PLAN

## Noise<sup>8</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan background Report Seville lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks.

### Energy: Natural Gas/Electricity

Seville energy source is served by PG&E.

### Domestic Water & Wastewater

Domestic water and sewer service in Seville is provided by Tulare County. **Table 14** shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Tulare County, January 2014). Currently, there are plans to hook up Seville to a new pump in Yettem, the plans are contained in **Figure 7**.

**Table 14 - Description of Existing Infrastructure**

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
114	114	0	99	103	4

(\* Data current as of January 2014)

### **Seville's Water Challenges and History of Project**

Most Seville residents get their water supply from the Seville water system (previously the privately-owned Seville Water Company), which has been operated by the County of Tulare since 2009 under a court receivership order. Most of the distribution system is over 50 years old and needs to be fully replaced. The pipes have exceeded their useful life and were not built to current standards. The wells supplying water to the entire community are unable to meet current water demand.

<sup>8</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# SEVILLE HAMLET PLAN

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## **Water Systems Improvements**

The objectives of the Seville-Yettem Water Systems Improvement Project are to:

- Secure a safe and reliable source of drinking water for Seville and Yettem
- Replace old and failing infrastructure in Seville
- Promote water conservation via the installation and use of water meters in both communities
- Address governance challenges and improve economies of scale
- Reduce water systems' vulnerabilities

## **Project Objectives**

To accomplish these objectives, the project includes the following water Systems Improvement:

- Build a completely new water distribution system in Seville
- Drill a New Well (the County of Tulare has purchased property in Yettem for the future well)
- Install Water Meters in Yettem and Seville
- Interconnect the Yettem and Seville Systems

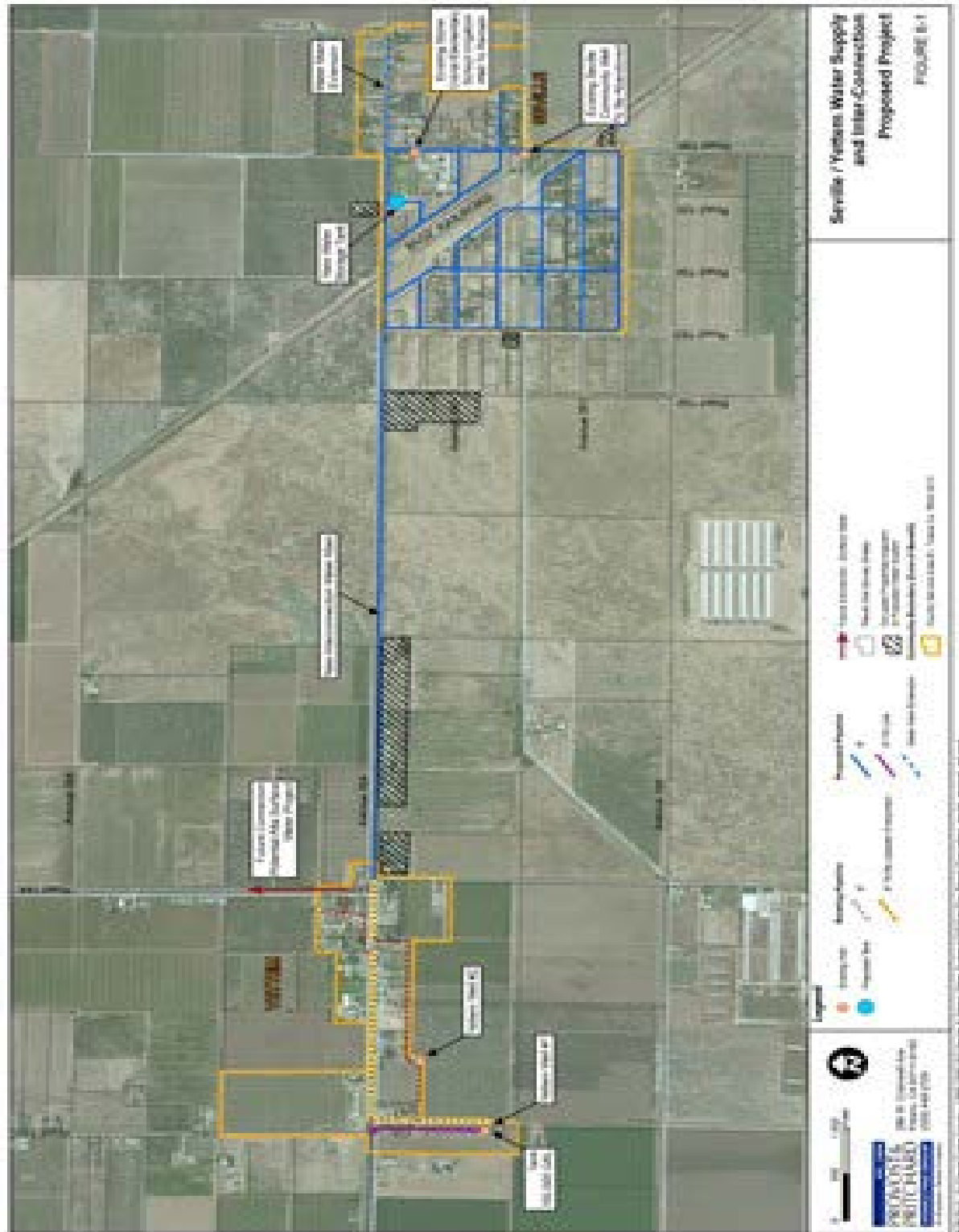
In 2012, the County of Tulare obtained grant funding under Proposition 84 from the State of California, to complete a feasibility study for the Seville water system. The study determined that a new water source for the community was needed, evaluated the consolidation of the Yettem and Seville water systems and funded the preparation of plans, specifications, environmental documents and construction costs estimates for the proposed water systems improvements.

## **Governance Formation**

Self-Help Enterprises and Tulare County will be working with Seville and Yettem to form a Community Services District for both communities to manage the connected systems (formation to be completed November 2018). A detailed formation schedule is available upon request.

# SEVILLE HAMLET PLAN

Figure 7 - Seville - Yettem Water Systems Improvements Fact Sheet



# SEVILLE HAMLET PLAN

Table 15 - Project Phasing

PROJECT PHASES	
To expedite the construction of proposed water systems improvements and improve water access in Seville, the project has been split into two phases.	
PHASE 1: Seville Water Distribution System	PHASE 2: Seville-Yettem Intertie
<p>Includes the construction of a completely new water distribution system in Seville, installation of water meters in Seville and, if enough funding is available, a water storage tank.</p> <p><u>Anticipated Project Schedule</u> Design, Plans, and Specifications and Environmental Documents</p> <ul style="list-style-type: none"> <li>Water System Design and Environmental documents have been completed</li> <li>Project Plans and Specifications are expected to be completed by Fall 2017</li> </ul> <p><u>Construction</u></p> <ul style="list-style-type: none"> <li>Begin Construction by early 2018</li> <li>Complete Construction by the end of 2018</li> </ul> <p><u>Estimated Project Cost</u></p> <ul style="list-style-type: none"> <li>Phase 1: \$5 million</li> <li>Lateral Connections : \$199,300</li> </ul> <p><u>Funding Secured to Date</u></p> <ul style="list-style-type: none"> <li>\$5 million – State Water Resources Control Board for Phase 1</li> <li>\$199,300 – Department of Water Resources for Laterals Connections</li> </ul>	<p>Includes the connection (intertie) of the Seville and Yettem water systems, water meters in Yettem, drilling of the new well in Yettem and the storage tank if it is not part of the Phase 1.</p> <p><u>Anticipated Project Schedule</u> Design, Plans, Specifications, and Environmental Documents to be completed by 2019.</p> <p><u>Construction</u></p> <ul style="list-style-type: none"> <li>To begin in late 2019</li> </ul> <p><u>Estimated Project Cost</u></p> <ul style="list-style-type: none"> <li>Phase 2: \$3.5 million</li> </ul>
PROJECT TEAM	

## Sewer

Domestic sewer service in Seville is provided by Tulare County. **Table 14** shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Tulare County, January 2014). Mapping of the sewer systems is currently unavailable.

## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and

# SEVILLE HAMLET PLAN

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cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Seville does not currently have a storm drainage system.”<sup>9</sup>

## Solid Waste

Solid waste disposal services for the Community of Seville is provided by Pena’s Disposal, a private company. Solid waste generated in Seville can be disposed of at the Visalia Landfill, located at 22466 Road 80, Visalia.

## Roads

“There are several roadways in Seville that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt

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<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# SEVILLE HAMLET PLAN

- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads”<sup>10</sup>

**Table 16 - Roads in Need of Major and Medium Repair in Seville**

<b>Road Maintenance Strategies</b>			
<b>No.</b>	<b>Roadway</b>	<b>Limits</b>	<b>Repair Code</b>
1	Avenue 383	Road 153 to Road 155	CHIP
2	Inyo Avenue	Road 153 to Road 156	CHIP
3	Kern Road	Seville Avenue to Avenue 384	CHIP
4	Road 153	Inyo avenue to Avenue 381	GRX
5	Road 153	Visalia Avenue to Seville Avenue	GRX
6	Road 153	Seville Avenue to Avenue 384	CHIP
7	Road 155	Inyo Avenue to Seville Avenue	CHIP
8	Seville Avenue	Road 153 to Kern Road	CHIP
9	Visalia Avenue	Road 153 to road 156 (End)	GRX

OLAY – overlay resurfacing operation

CHIP – chip seal

GRX – grind and remix

ACST – asphalt reconstruction

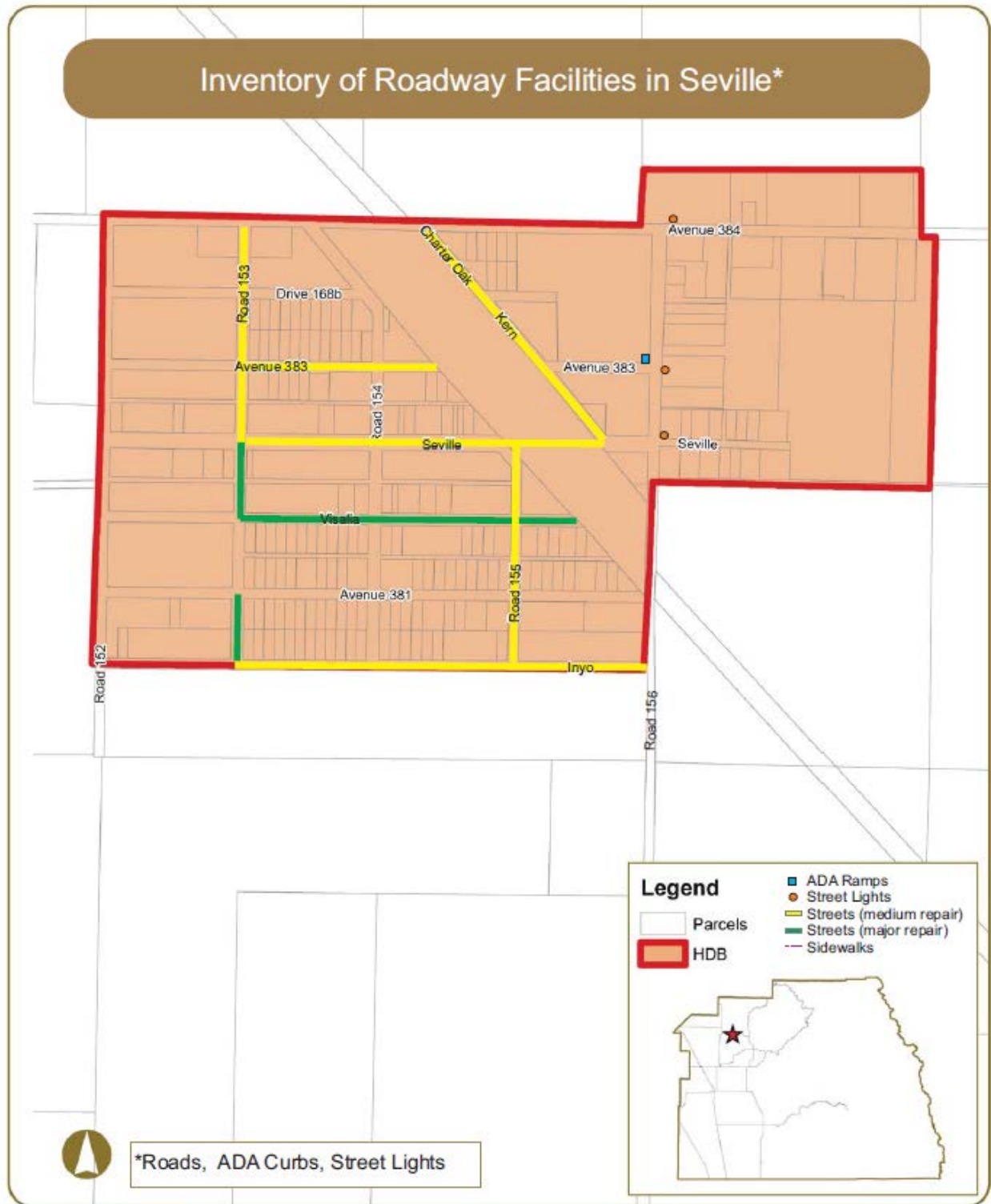
RCST – cold mix reconstruction

(Source: County of Tulare Public Works, 2012)

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

# SEVILLE HAMLET PLAN

Figure 8 - Inventory of Roadway Facilities in Seville





# SEVILLE HAMLET PLAN

## Street Lights

**Table 17** identifies the location of existing street lights that are maintained by Tulare County, in Seville, as well as their specifications. **Figure 8** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type "W" represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete."<sup>11</sup>

**Table 17 - Specifications of Existing Street Lights**

Specifications of Existing Street Lights								
No.	East-West	North-South	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 383	Road 156	NE Corner	2	5800	W	W	PG&E
2	Seville	Road 156	NE Corner	N/A	5800	W	N	PG&E
3	SR 201	Road 156	NE Corner	11	9500	W	S	PG&E

(Source: Tulare County Public Works, March 2013)

## Sidewalks

There are currently no sidewalks located within Seville. Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

"The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element



# SEVILLE HAMLET PLAN

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there is one ADA compliant curb ramps located within Seville and are listed in **(Table 18)** and displayed in **(Figure 8)**.<sup>12</sup>

**Table 18 - Location of Existing ADA Ramps**

Location of Existing ADA Ramps			
No.	East-West Roadway	North-South Roadway	Location
1	Avenue 383	Road 156	NW Corner

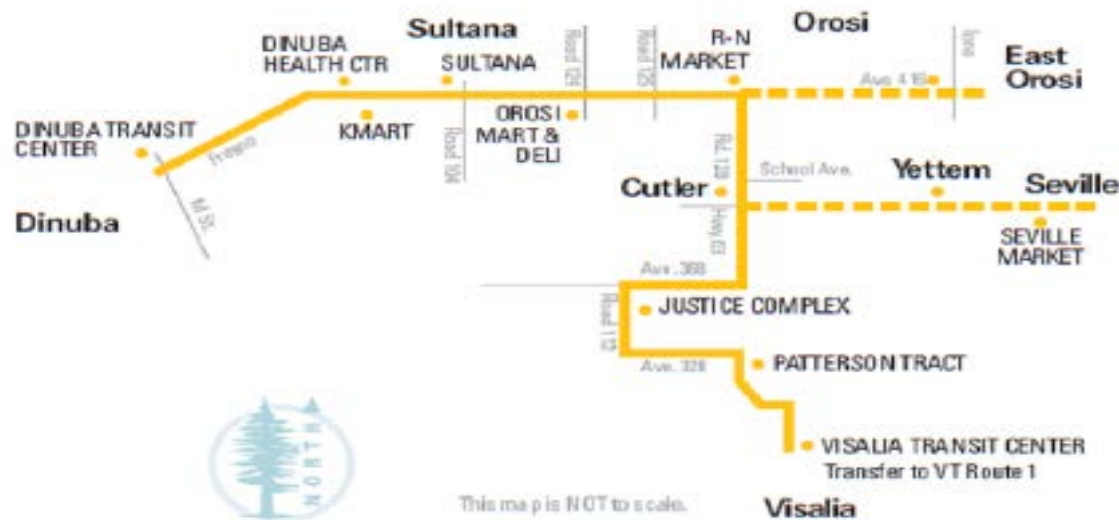
(Source: County of Tulare Public Works, August 2013)

## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Seville is currently connected via TCAT Route 10/North County (Seville Market) to Visalia and Dinuba's transit centers **(see Figure 9)**. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Seville, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

**Figure 9 - TCAT - Fixed Route Service for Seville**



<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element

# SEVILLE HAMLET PLAN

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## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified State Route (SR) 63 as a proposed Class III Bike Project<sup>13</sup>. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are rarely used outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is SR 63.

## AMTRAK

The Hanford AMTRAK station, located approximately 60 miles to the southwest in Kings County, is the closest station to Seville providing passenger rail service. The San Joaquin Joint Powers Authority (SJPPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport is approximately 30 miles south. Fresno Yosemite International Airport (FAT), approximately 45 miles northwest of Seville, is a principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles southeast of Seville and offers direct flights to several destinations

## **PUBLIC SERVICES**

### Sheriff

Police protection services are provided in Seville by the Tulare County Sheriff's Department sub-station, located 40765 Road 128, in Culter, approximately 6.5 miles northwest of Seville. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

### Fire

No fire Hydrants are found in Seville. Fire protection and emergency medical services are provided for Seville by the Tulare County Fire Department. The community is served by Cutler-Orosi Fire Station #4, located at 40779 Road 128, in Culter. Patrol 4, Engine 4 and Engine 204 are assigned to this location.

### Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Seville Hamlet Plan Area is within the Stone Corral Elementary School District. The District provides K-8 grade education in Seville. High School students attend high school in Visalia. Seville is within the Visalia Unified School District.

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<sup>13</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# SEVILLE HAMLET PLAN

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>14</sup> The closest library is located in Orosi, four miles to the northeast and the Ivanhoe Branch is four miles to the south (see Table 19)

Table 19 - Library Location & Hours		
Branch	Address	Service Hours (2016)
Orosi	Orosi Branch 12646 Avenue 416 Orosi, CA 936471	Wednesday, Thursday, Friday 9:00 am - 1:00 pm 2:00 p.m. – 6:00 p.m.
Ivanhoe	Ivanhoe Branch 15964 Heather Ivanhoe, CA 93235	Tuesday and Friday 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 6:00 p.m.

Library hours current as of September 2017

## Parks

In the community of Seville there is no safe public green space for children to play and adults to be physically active. The nearest park is located 8 miles away in the city of Orosi. Residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and concerned for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. Residents expresses that having a community park is a major priority for them.

Presently there are no County owned/operated public parks in Seville. The closest public park to the community is Ledbetter Park, located one mile northwest of Culter on Road 124/State Route 63.

# THE SEVILLE HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get

<sup>14</sup> General Plan Background Report, page 7-96

# SEVILLE HAMLET PLAN

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products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>15</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>16</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation and Transportation Conditions

Seville is a small agricultural community located near State Route 63 and 201. The major rural collector roads is Avenue 308. There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

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<sup>15</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>16</sup> Tulare County General Plan Background Report, page 5-7.

# SEVILLE HAMLET PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Seville Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Seville to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Seville Hamlet Plan, and input received from Seville citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.

2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned shopping areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
5. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
6. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.  
The County shall work with the Schools to provide safe routes to school.
7. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Seville Community Plan.
8. The Seville Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.

# SEVILLE HAMLET PLAN

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9. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

**Housing**

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Seville.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Seville.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.



# SEVILLE HAMLET PLAN

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## Economic Base

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

#### Policies:

1. Encourage the Seville to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

#### Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.

2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## Environmental Quality and Public Safety

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Seville citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

#### Objective: Protect Agricultural Lands:

Land within the respective Seville, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate.

# SEVILLE HAMLET PLAN

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When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Seville.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Seville.



# SEVILLE HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 178.5 acres Seville Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute % of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 545 by .25 urbanized acres (area within the HDB), which equals 117 persons per urbanized acre (**see Tables 20 and 21**) showing population projections). Projecting the population at a 1.3% growth rate adds 145 persons by year 2030.

### Population Growth Forecast

Table 20 - Seville Population Projections			
Growth Rate	2015	2020	2030
0.013	545	597	662

Table 21 - Population Projections		
Year	Population	Growth (%)
2015	545	0.013
2016	552	0.013
2017	559	0.013
2018	567	0.013
2019	574	0.013
2020	581	0.013
2021	589	0.013
2022	597	0.013
2023	604	0.013
2024	612	0.013
2025	620	0.013
2026	628	0.013
2027	636	0.013
2028	645	0.013
2029	653	0.013
2030	662	0.013

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The Year 2015 baseline population and was determined by projecting the 2013 American Community Survey (Survey)<sup>17</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in Year 2015 the community had 129 dwelling units (including vacant dwellings) with a population of 545. At an annual growth rate of 1.3%, the projected housing units are 138 and 662 in years 2020 and 2030, respectively, and projected population is 597 and 662 in years 2020 and 2030, respectively.

The total land area developed in these existing residential units was provided by the Tulare County Resource Management Agency Geographic Information System (GIS) division. There are currently approximately 178.5 acres within the HDB that are developed for residential uses.

<sup>17</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

# SEVILLE HAMLET PLAN

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, water service levels need to be expanded, for which grant funding will be required.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Based on the 2011-2015 American Community Survey (see Table 22), the educational barrier in Seville begins in grade school. Of the adults age 25 and older,

20.8% had an educational level of less than 9<sup>th</sup> grade. A lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

Table 22 - Education Attainment			
Educational Attainment	California	Tulare County	Seville
Population 25 years and over	25,257,858	260,964	250
Less than 9th grade	10.0%	20.8%	40.8%
9th to 12th grade, no diploma	8.2%	10.9%	20.8%
High school graduate (& equivalency)	20.7%	25.2%	24.8%
Some college, no degree	21.8%	22.3%	10.0%
Associate's degree	7.8%	7.0%	2.4%
Bachelor's degree	19.8%	9.5%	0.8%
Graduate or professional degree	11.6%	4.3%	0.4%

2011-2015 American Community Survey 5-Year Estimates

### Health Care

Health care is important for economic development as businesses need healthy employees. The other nearest medical offices are in Porterville.

# SEVILLE HAMLET PLAN

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 23**) and Zoning Plan map (see **Table 24**) have been updated.

<b>Proposed Land Use</b>	
<b>Table 23 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	140.3
Unclassified (Right-of Ways)	38.2
Seville HDB	178.5

<b>Table 24 - Proposed Zoning</b>	
Proposed Zones	Acres
C-2-MU	26.9
M-1-MU	15.3
P-O	7.6
R-1	63.2
R-A-12.5	27.3
Unclassified (Right-of Ways)	38.2
Seville HDB	178.5

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations contained in the Seville Hamlet Plan. The following components comprise the Seville Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Seville Hamlet Plan, there are several proposed changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the Uses that require a Use Permit. As part of the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

# SEVILLE HAMLET PLAN

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## Mixed Use Overlay District

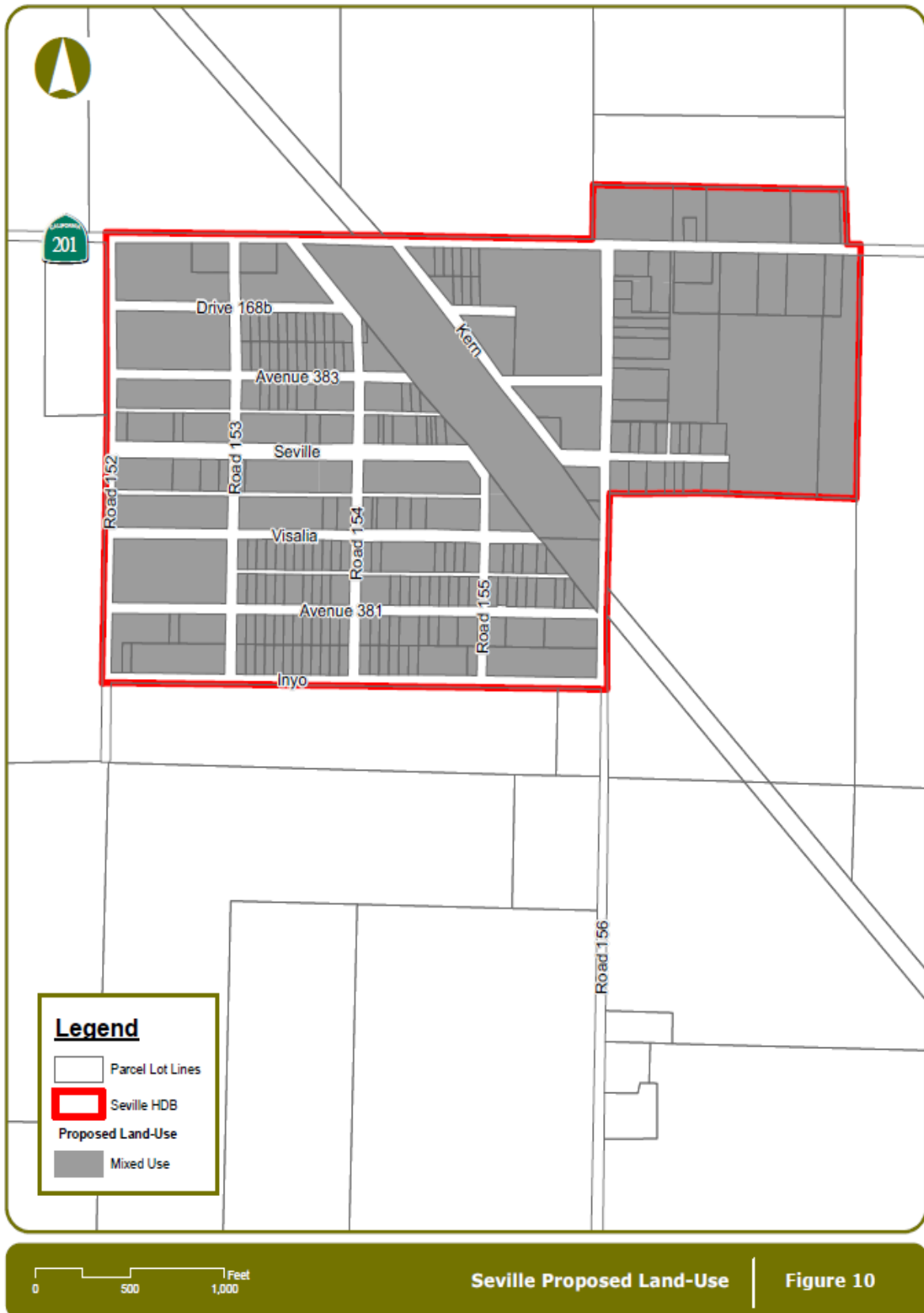
This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Seville.

## Zoning Map Update

The current Zoning Map for Seville (**see Figure 11**) will be amended to be compatible with the Land Use Map (**see Figure 10**) outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance (**see Figure 12**) to be in conformity with each other Seville Proposed Zoning.

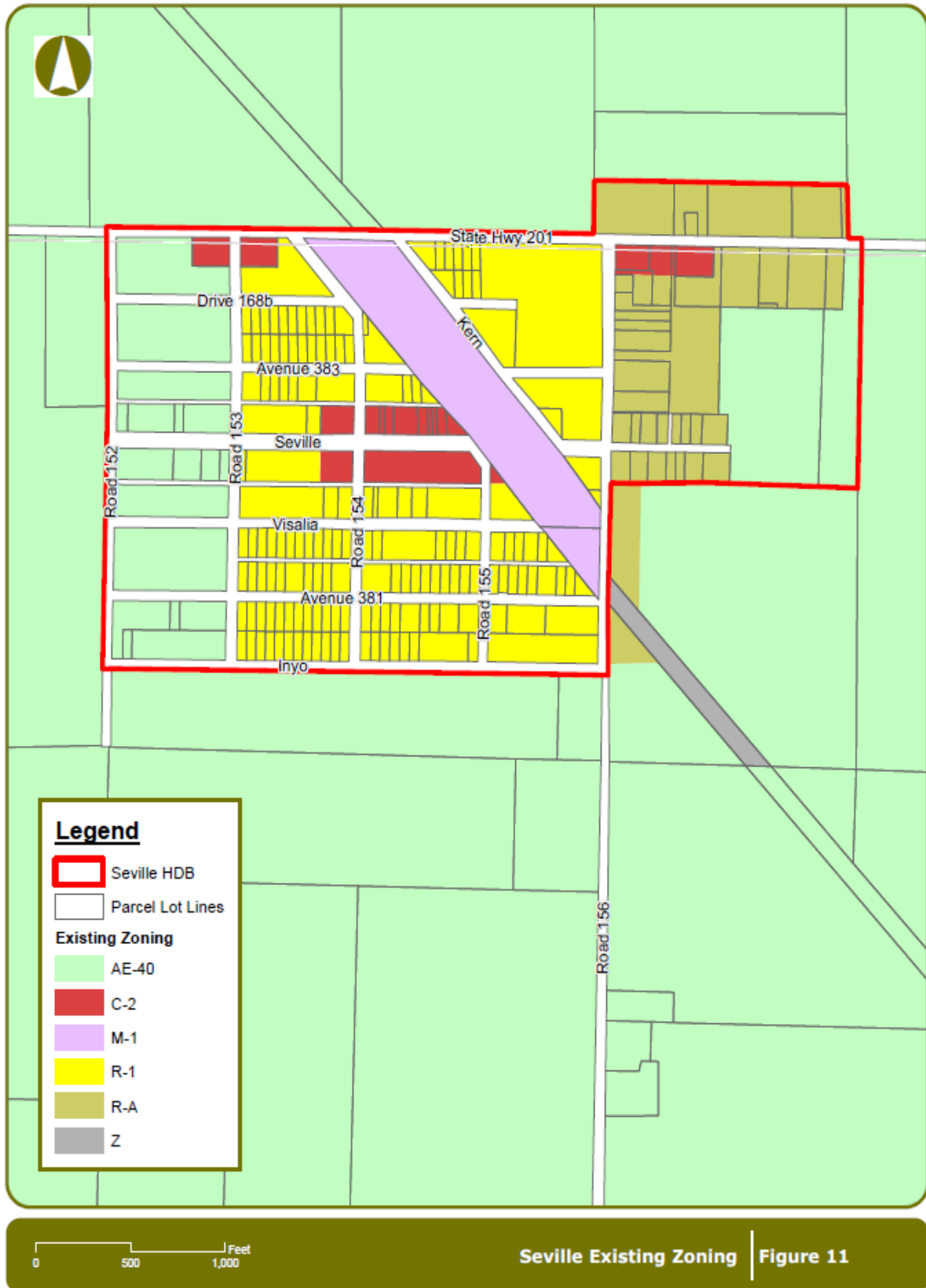
# SEVILLE HAMLET PLAN

Figure 10- Proposed Land Use Plan



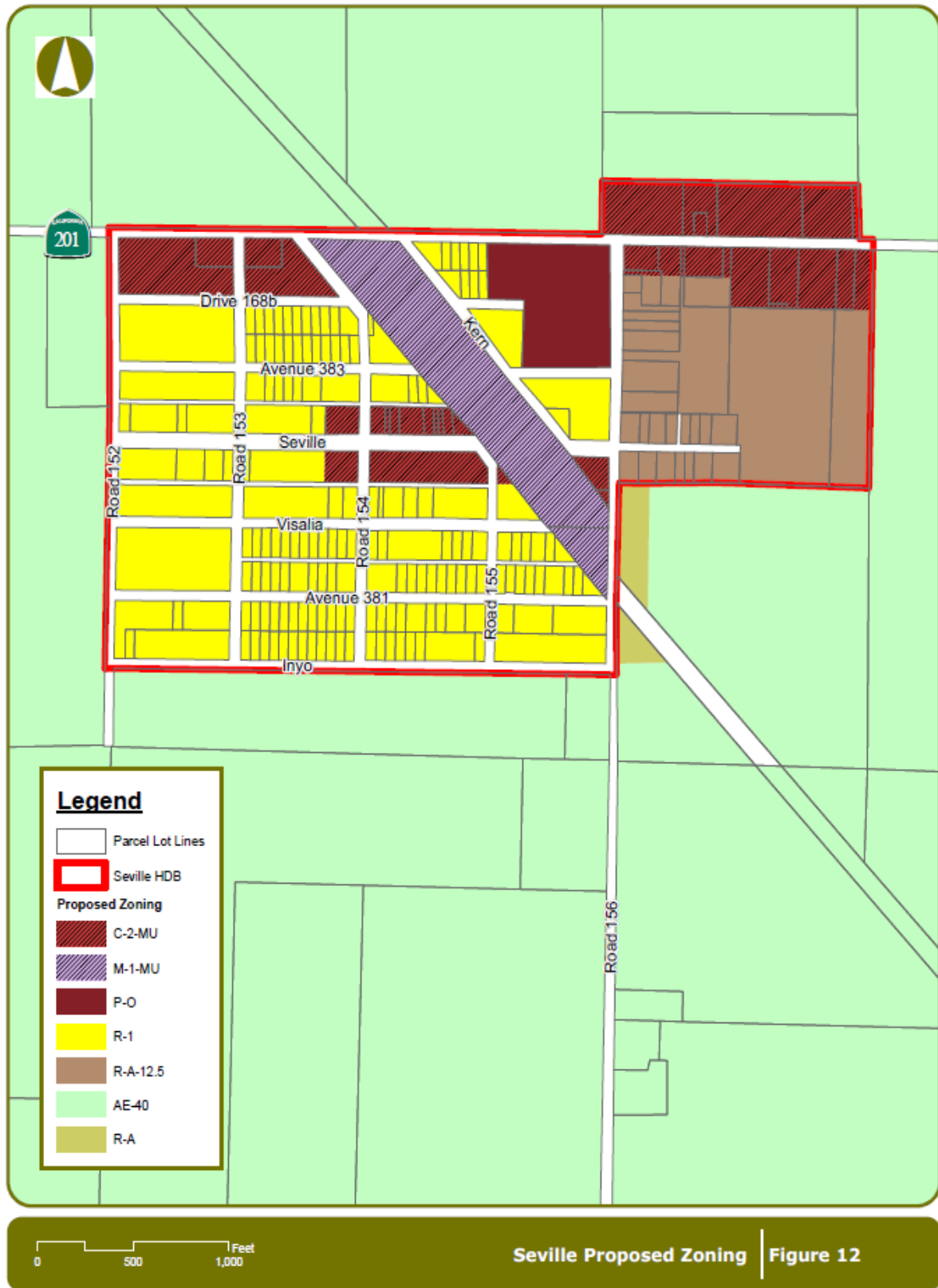
# SEVILLE HAMLET PLAN

Figure 11 - Existing Zoning Plan



# SEVILLE HAMLET PLAN

Figure 12 - Proposed Zoning Changes



# SEVILLE HAMLET PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)



# SEVILLE HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

# SEVILLE HAMLET PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-	R-3, C-1, C-2, C-3,

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	1, R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles	C-2, C-3, M-1, M-2	

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of an airport).		
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1

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Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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## A-2 *Mixed Use Overlay District (Zone Change Text)*

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The following regulations shall apply in the community of Seville, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Seville.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Seville.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Autowrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planing Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport



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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Seville. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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## ***A-3 Development Standards (Mixed Use Zoning District)***

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To promote Economic Development within the Seville Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Seville. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Seville Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Seville. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.



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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.



# SEVILLE HAMLET PLAN

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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

# SEVILLE HAMLET PLAN

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

# SEVILLE HAMLET PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# SEVILLE HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;



- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:       None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is



applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9369
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-039	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;



3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map

***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

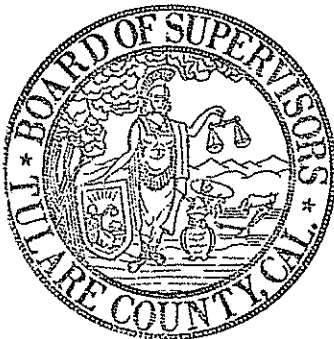
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **Teviston Hamlet Plan**



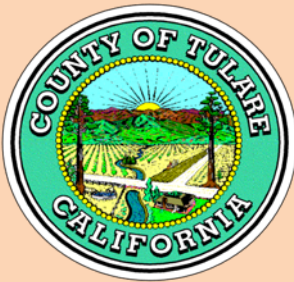
# TEVISTON HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch,  
and Planning Processing Division



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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# TEVISTON HAMLET PLAN

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## Teviston Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Teviston Hamlet Plan: GPA 17-020  
Zoning District Map: PZC 17-020



Tulare County Resource Management Agency  
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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

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# TEVISTON HAMLET PLAN

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# TEVISTON HAMLET PLAN

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# TEVISTON HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Teviston is to develop a plan which can accurately reflect the needs and priorities of this community. Teviston is currently designated as a Hamlet in the 2030 Tulare County (the County) General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks) and to stimulate economic development within the community.

### Location

The community of Teviston (**see Figure 1**) is located in Tulare County on the eastern side of the San Joaquin Valley and along State Route 99. Teviston is a census-designated place located in the southwest portion of the County, southwest of Porterville. Teviston (**see Figure 2**) is bounded by south of Avenue 72 in the south, Avenue 84 in the north, Road 126 in the west, and Road 136 in the east and encompasses 2.2 square miles of land. Teviston is an agriculturally oriented service community surrounded on the north, west and south by lands in agricultural production and on the east by scattered rural residential, agricultural, and vacant land. Recently, the northern Urban Development Boundary for the Community of Earlimart was adopted along Deer Creek just south of Teviston's southern Hamlet Development Boundary. In 2015, the Population of Teviston was 1,623. Medium Household Income is \$24,267, and is comprised of approximately 424 residences.

Teviston is located in Section 08, 09, 16, 17, 21, 22 Township 23 South, Range 25 East, MDB&M, and can be found within Pixley United States Geological Survey 7.5 minute topographic quadrangle. Teviston is located at an elevation of 272 feet National Geodetic Vertical Datum. The coordinates of Teviston are: Latitude: 36°55'44"N and Longitude: 119°16'42"W.

### Planning Area

The Teviston Hamlet Development Boundary (HDB) was approved in the 2012 Tulare County General Plan with an area that consists of 1,443.2 acres (**See Figure 3**). The Land Uses within the HDB are Mixed Use activities that occupy 1,312 acres of the buildable land. The remaining 130.8 acres of lands are dedicated for Right-of-Way (**See Table 1**).

Table 1 - Teviston Land Use	
Land Use	Acres
Mixed Use	1,312.4
Rights-of-way	130.8
<b>Total</b>	<b>1,443.2</b>

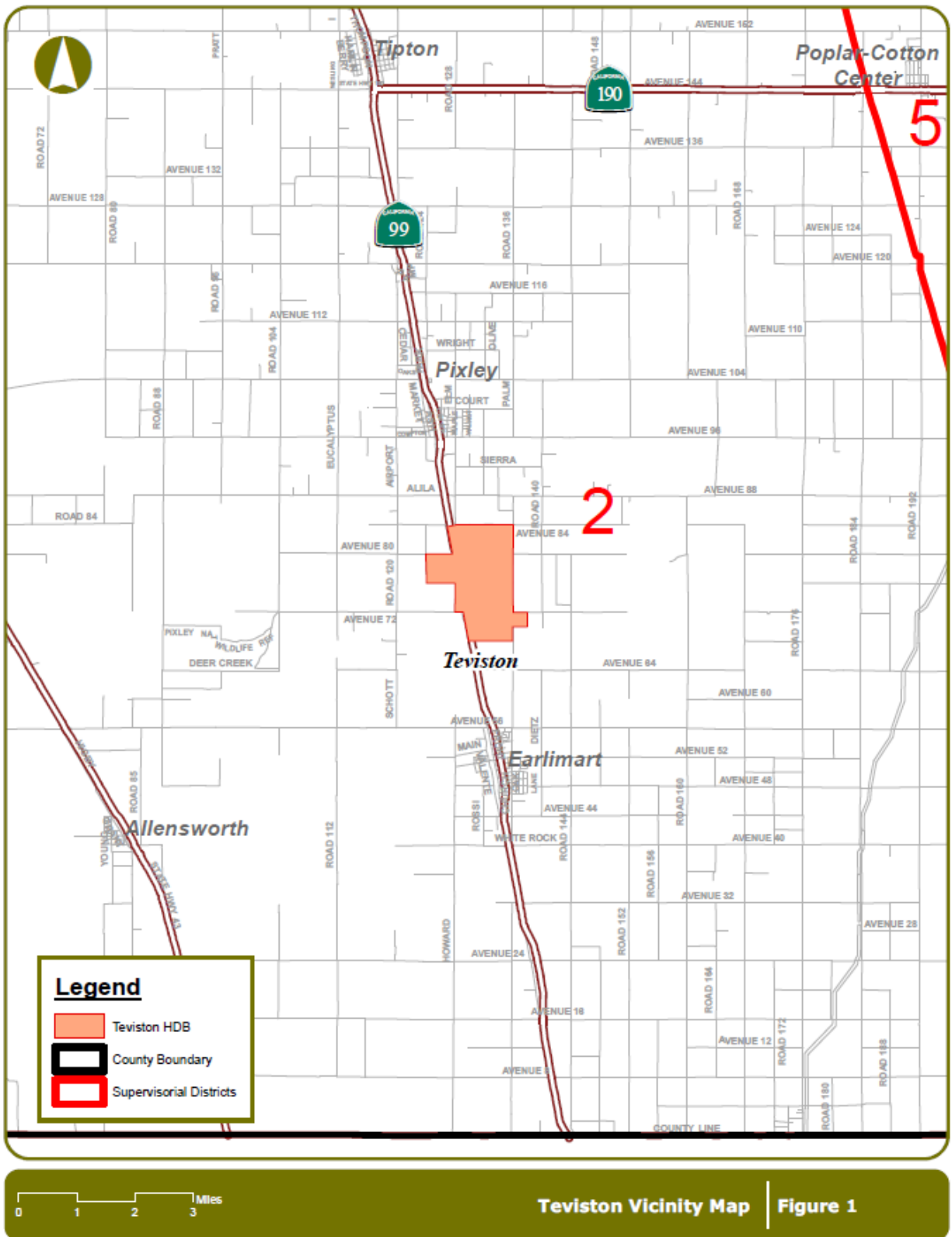
Source: Tulare County GIS

No change is proposed to the Hamlet Development Boundary.



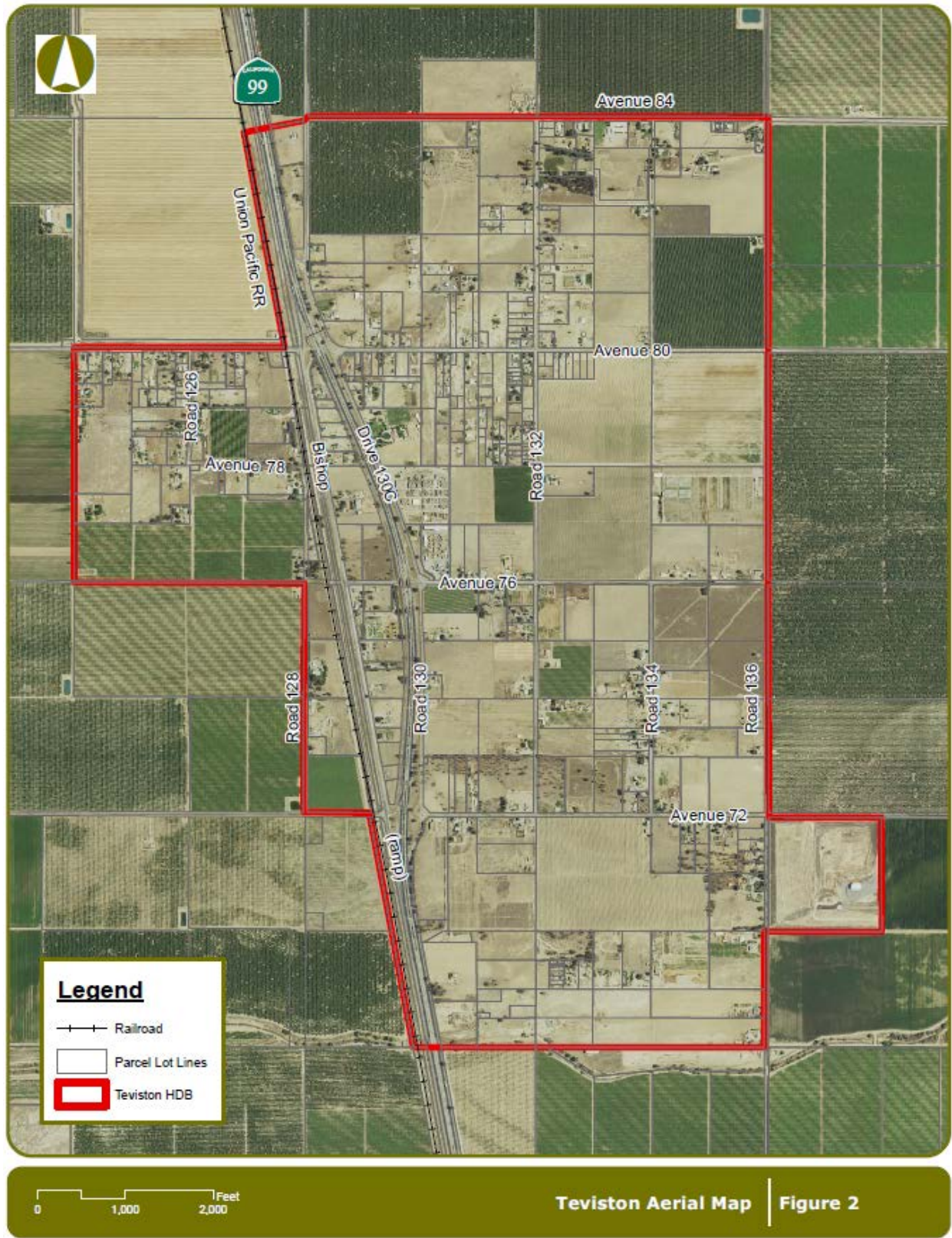
# TEVISTON HAMLET PLAN

Figure 1 - Vicinity Map



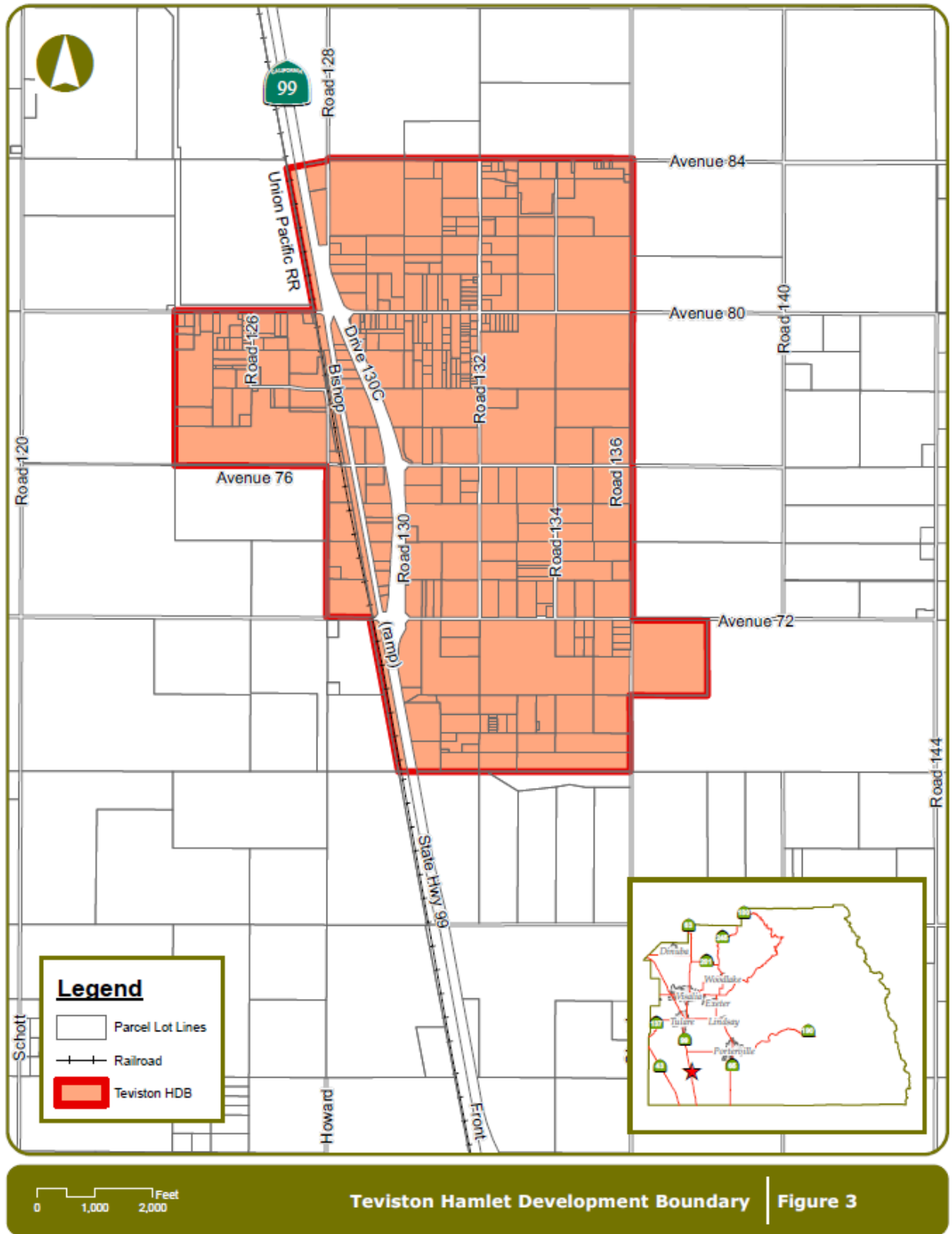
# TEVISTON HAMLET PLAN

Figure 2 – Aerial Teviston



# TEVISTON HAMLET PLAN

Figure 3 - Teviston HDB





# TEVISTON HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY

### OUTREACH REPORT

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises (SHE) went door to door, throughout the community of Teviston to distribute flyers at homes and businesses. SHE improvised the importance of participating in this process.

#### Community Outreach and Invitation Process

Self-Help Enterprises (SHE) performed various outreach methods to promote community participation and create awareness of the (SGC) project.

- 1.) An informational flyer was developed.
- 2.) SHE staff went door to door to distribute flyers to every home and business. Door to door outreach was done on Wednesday December 2 and Tuesday December 8, 2015
- 3.) SHE staff distributed flyers at local churches
- 4.) SHE partnered with Teviston Betterment Association to do outreach for our community meeting.
- 5.) Throughout the community outreach process, SHE staff explained the goals of the project and emphasized the importance and benefits of participating in the SGC community meeting.

#### Community Meetings

Self-Help Enterprises held a community meeting with two staff in Teviston on Thursday December 10, 2015 at 5:30 pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Teviston Community Center 12934 Ave 80 in Teviston. The purpose of the meeting was to discuss Teviston improvement needs, gather community data, and report the findings of the meeting in a final report. More than 30 residence attended the meeting and 22 community surveys were collected. Present were representatives from the Teviston Betterment Association were present.



A second meeting was held on December 8, 2016 at 6:30. More than 20 residents attended the meeting, and the community survey results were reported back to them. In addition, the community planning process was discussed and several residents expressed their desire for the opportunity to change the

# TEVISTON HAMLET PLAN

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zoning plan for the community. At that time, Teviston did not have any water capacity to provide will serve letters for new projects. At the meeting, the Chairperson for the Teviston Community Services District stated they had received a grant to “sink a new well” and that the water well would provide more capacity than they had previously. Although still high on the priority for the community, sewer issues were discussed, but the residents also stated they were very much concerned about the conditions of the roads and lighting. Residents generally agreed with the remaining survey report findings, and one resident wanted to verify that the environmental process would be completed. County staff assured them that it would.



## Community Survey

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Teviston SGC community input meeting, residents concluded that the following is a list of Teviston priority improvement needs:

## The Community's Priority Improvements

1. Sewer
2. Affordable Housing
3. Home Repairs
4. New Housing
5. Road Conditions and Street Safety
6. Street Lights
7. Public Transportation
8. Renovate the Community Center

## **Sewer**

There is no community wide sewer system in Teviston. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC

# TEVISTON HAMLET PLAN

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survey results indicate that the Teviston residents are in favor of creating a community wide sewer system.

## **Affordable Housing**

Teviston residents want new affordable housing to be built in town. Many people expressed that they would buy a new home in town if it were available to them. Residents would like to see the town grow. Residents feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Tulare and Delano.

## **Home Repairs**

Teviston residents are concerned because many homes in the community need of significant repairs. It was reported that some of the community members are living in houses and trailers that are substandard and are not up to building code. Some old homes have deteriorated and are facing situations such as: bad flooring, no functioning heat or cooling system, and no running water or sewer. Home owners and landlords are requesting resources that will help with the home repair costs.

## **Road Conditions and Street Safety**

It is reported that road conditions in Teviston are poor and most roads need work, as they have potholes, cracks and bumps. Drivers report feeling unsafe and are worried about damage caused to their vehicles while driving on the roads. According to residents, Teviston roads are not regularly maintained and the few times that roads have been serviced, they considered the repair work to be poor because the roads were patched and not actually repaired.

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols Teviston. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Public Transportation**

Teviston lacks access to public transportation. Residents that do not drive find it hard to travel outside of town. Often Teviston residents need to travel to Delano or Tulare for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes through Teviston, this would help many residents with their daily travel needs. The nearest bus stops are three miles away in Earlimart and Pixley.

## **Renovate Community Center**

In Teviston, there is a building that is used as the local community center. The building is old and in need of repairs. If repaired, this community center is an ideal location for a multi-purpose center that could be used by kids for homework help, afterschool programs and as a computer lab, it also be available for use by outside organizations that need office space to service residents. This could also be the place where residents access a reliable internet connection. Teviston residents see this as a

# TEVISTON HAMLET PLAN

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priority need and are asking for the County's assistance in identifying resources to improve and repair their local community center.

## **Internet Access**

Most families in Teviston do not have internet access at their homes. Any available internet service is unreliable and cost prohibited. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. Without access to the internet, they are unable to do online job hunting, applying for resources, or provide homework help to their kids.

## TEVISTON HAMLET TECHNICAL BACKGROUND REPORT

### **DEMOGRAPHICS**

An important part of planning is having information that describes the characteristics of a Hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Hamlet Plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of Hamlet such as Teviston.

### Population

In 2015, the Population of Teviston was 1,623. (See Table 2)

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Teviston</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	734	45.2%
Female	19,334,329	50.3%	226,607	49.9%	889	54.8%
Total	38,421,464		454,033		1,623	

2011-2015 American Community Survey 5-Year Estimates

# TEVISTON HAMLET PLAN

## Growth Rate

As noted in the (2010) General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Teviston's population during the Planning Period (see Table 3). This would result in a total increase of 325 persons by 2030 (21 to 25 persons per year). Note: however, there are recent uncertified surveys that show a lower growth rate, or even a net migration out of the unincorporated County, but those numbers have not been verified.

Table 3 - Projected Annual Growth Rate		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.4%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

## Median Age

Teviston's population's median age is younger than the median age of both Tulare County and the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Teviston	18.5

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 14.5% of Teviston's population was White, 4.4% was African American, 0% was Native American, 0.7% was Asian, and 0% was two races or more (see Table 5). Approximately 80.3% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Teviston	%
Total	38,421,464		454,033		1623	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	1,304	80.3%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	236	14.5%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	71	4.4%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	12	0.7%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates



# TEVISTON HAMLET PLAN

## Employment in Teviston

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Teviston was about 9.5% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 9.5% includes only the employable labor force (that is, not every person of the population) results in about 89 unemployed persons of Teviston's person labor force of 429.

Table 6 - Employment Status			
Employment Status	California	Tulare County	Teviston
Population 16 years & over	30,312,429	325,404	938
In labor force	19,269,449	194,420	429
Civilian labor force	19,137,441	194,102	429
Employed	17,246,360	170,780	340
Unemployed	1,891,081	23,322	89
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	509

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Teviston's median household income was \$24,181 compared to \$61,818 for the State of California and \$42,031 for Tulare County. (see Table 7)

Table 7 - 2011-2015 American Community Survey			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Teviston CDP	\$24,181	\$22,740	\$9,098

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Teviston's median household income was \$24,181, whereas the State of California's median household income was \$61,818. Teviston's median household income was 39% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.

# TEVISTON HAMLET PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

Table 8 - Housing Tenure			
	California	Tulare County	Teviston
Occupied housing units	12,717,801	133,570	363
Owner-occupied	6,909,176	75,685	153
Renter-occupied	5,808,625	57,885	210

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Teviston
Owner-occupied	54.3%	56.7%	42.1%
Renter-occupied	45.7%	43.3%	57.9%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Teviston	2	13	1	7	3	20	4	27	5	33	15

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Teviston indicated that approximately 13% of the housing units were sound. Approximately 54% of Teviston's housing units were deteriorated and 33% were dilapidated (see Table 10).

# TEVISTON HAMLET PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Teviston
Total housing units	13,845,790	144,792	424
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	86
Built 1990 to 1999	1,495,571	21,767	28
Built 1980 to 1989	2,117,819	22,733	117
Built 1970 to 1979	2,503,688	27,111	126
Built 1960 to 1969	1,871,029	15,500	42
Built 1950 to 1959	1,907,512	13,694	0
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	25

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Teviston was 4.90 persons per household (see Table 12). The average owner occupied household size is 3.88.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Teviston	3.88	4.90

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Teviston was 0%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Teviston	0%	0%

2011-2015 American Community Survey 5-Year Estimates

<sup>1</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# TEVISTON HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>2</sup>

A California Natural Diversity Database (CNDDB) search conducted on November 1, 2016 (**see Figure 4**) indicates there are special status species within the Pixley Quadrant Species List (which includes the Teviston Planning Study Area) consisting of four animal species and two plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Tipton kangaroo rat (*Dipodomys nitratoideus*, federal endangered); Blunt-nosed leopard lizard (*Gambelia sila*, federal endangered); Vernal pool fairy shrimp (*Branchinecta lynchi*, federal threatened); San Joaquin woollythreads (*Monolopia congdonii* federal endangered) and Kern mallow (*Eremalche kernensis*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975, while the adobe starburst is has been determined to be “extirpated” (that is, “A species no longer surviving in regions that were once part of their range”) and is no longer within the Teviston project area.

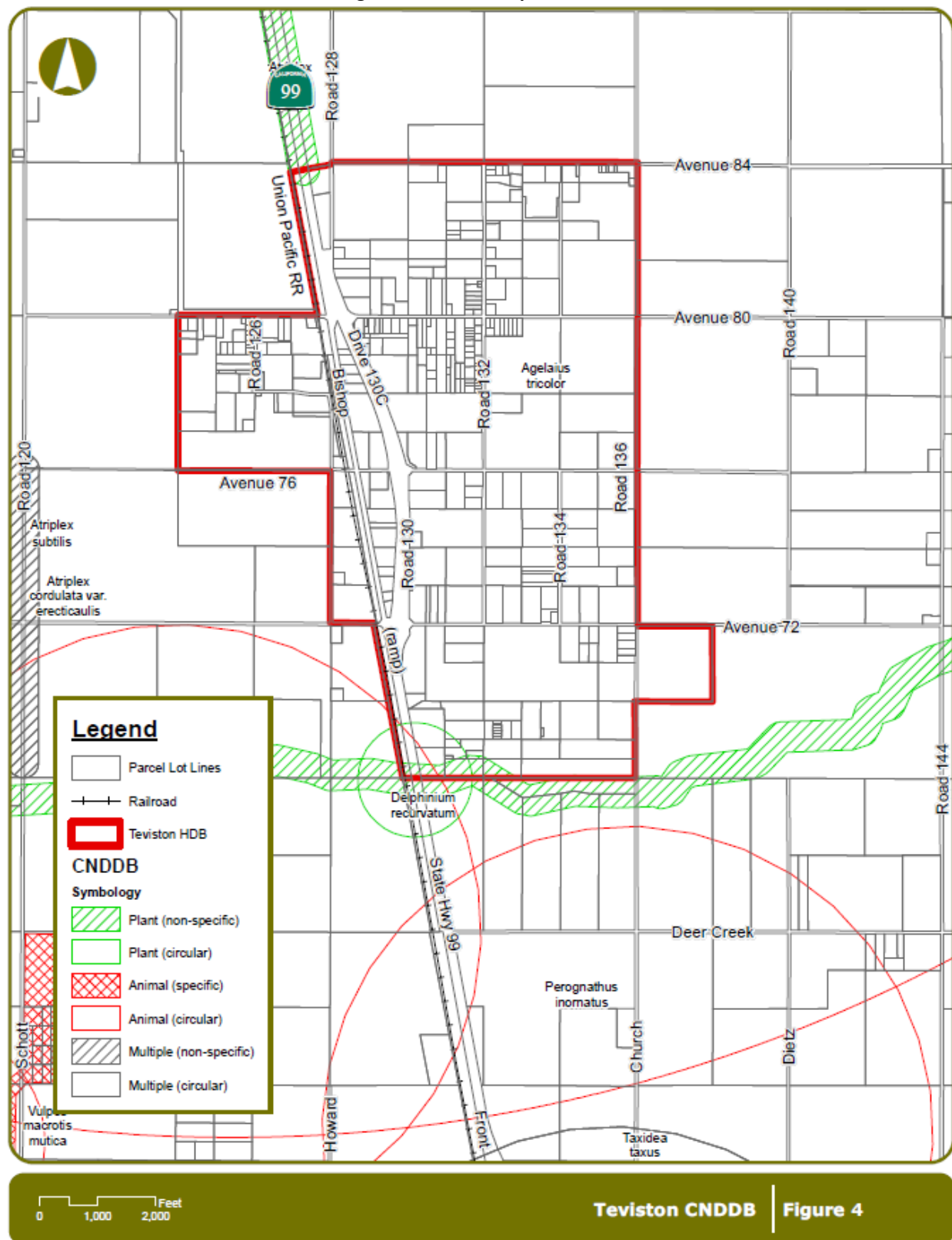
The Pixley National Wildlife Refuge was established in 1959 by executive order to provide wetland habitat for migratory waterfowl and shorebirds. Of the 6,939 acres that comprise the Refuge, approximately 5,350 is upland habitat made up of grassland, alkali playa, and vernal pool habitat, 755 acres consists of seasonal wetlands, and 15 acres consist of riparian habitat. The Pixley National Wildlife Refuge is located 3 miles to the northwest.

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<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDB/Maps-and-Data> November 1, 2016

# TEVISTON HAMLET PLAN

Figure 4 - CNDDDB Map - Teviston



# TEVISTON HAMLET PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Teviston are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Teviston] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>4</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Teviston (see Figure 5). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**Akers-Akers, saline-Sodic, complex, 0 to 2 percent slopes**, is very deep, well drained soils formed in alluvium derived from granitic rock. Well drained; negligible runoff; moderate permeability. Saline-sodic phases have moderately slow permeability. This soil is used for irrigated cropland to grow cotton, corn, wheat, table grapes, walnuts, plums, and alfalfa. It is also used for dairy and cattle production and building site development.

### **Dumps – Not rated**

**Gareck-Garces association, 0 to 2 percent slopes**, is very deep and well drained, medium or high runoff, some areas are ponded; very slow permeability. The soil is suitable for livestock grazing. Vegetation is scattered saltbush with annual grasses and forbs.

**Hanford sandy loam, 0 to 2 percent slopes**, consists of very deep, well drained soils that formed in moderately coarse textured alluvium dominantly from granite. Hanford soils are on stream bottoms, floodplains, and alluvial fans. Hanford soils are used for growing a wide range of fruits, vegetables, and general farm crops. They are also used for urban development and dairies.

### **Riverwash – Not rated**

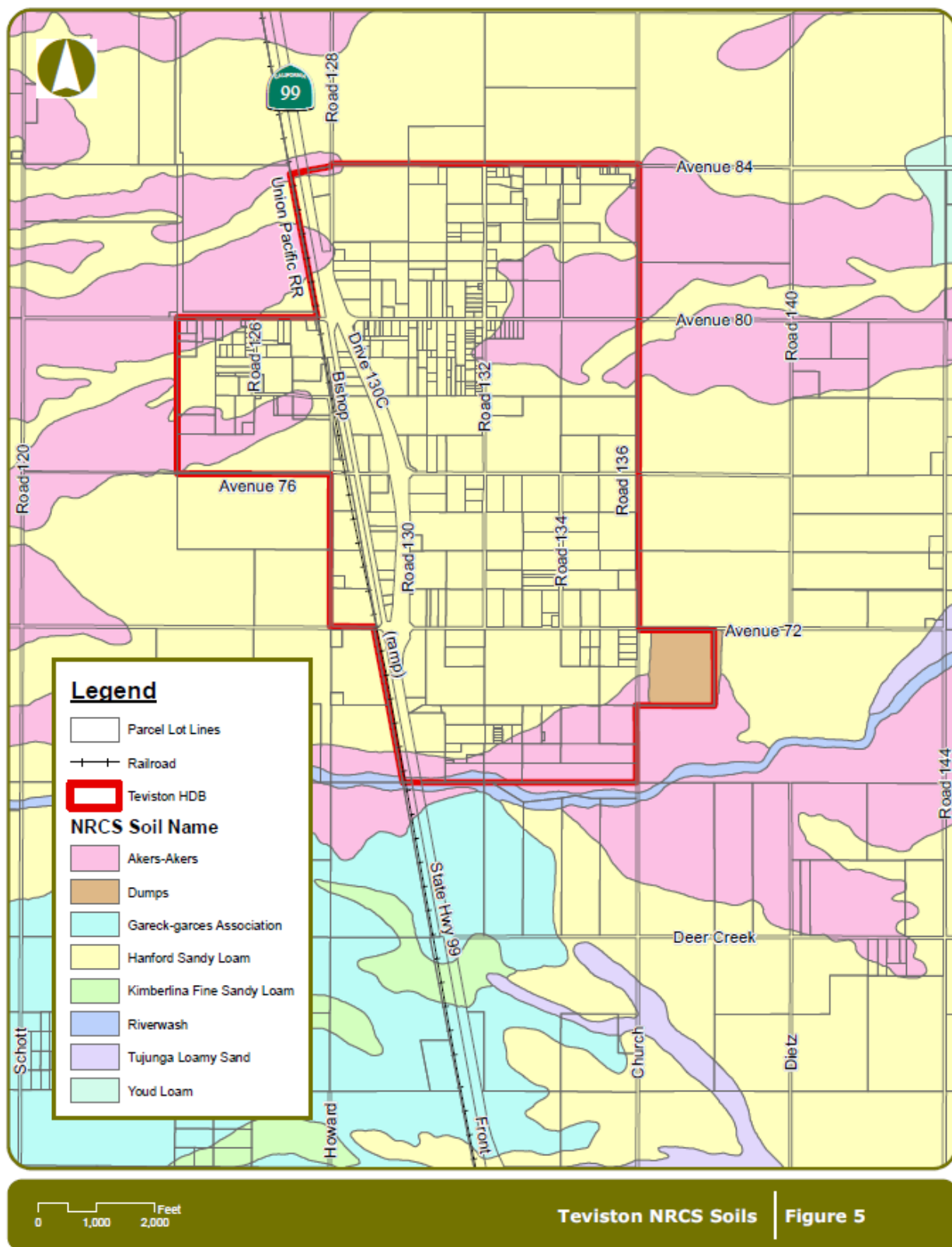
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<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>4</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# TEVISTON HAMLET PLAN

Figure 5 - NRCS Soils Map





# TEVISTON HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Teviston Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between land use and air quality; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Teviston is located near the southwestern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position to accommodate adversely



# TEVISTON HAMLET PLAN

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modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of Teviston and within the community itself include State Highway 99 and industrial uses emitting dust and odors, and agricultural activities.

## Flooding<sup>5</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C1925E, 100 year flood zone meanders along the east portion of Teviston (see **Figure 6**). The expected source of this flooding hazard is Deer Creek along the southern boundary with flooding begin caused northward. Zone A areas are subject to inundation by the 1-percent-annual-chance flood event generally determined using approximate methodologies. Because detailed hydraulic analyses have not been performed, no Base Flood Elevations (BFEs) or flood depths are shown. Mandatory flood insurance purchase requirements and floodplain management standards apply. The remaining areas are within Flood Zone X. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). “Floodplain” or “flood-prone area” means any land area susceptible to being inundated by water from any source. “Base Flood” is the flood having a one percent chance of being equaled or exceeded in any given year. “One-hundred-year flood” or “100 year flood” has the same meaning as “base flood.” “Special flood hazard area” is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. “Floodway” means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Teviston there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set

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<sup>5</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C1925E, Panel Number 1925 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

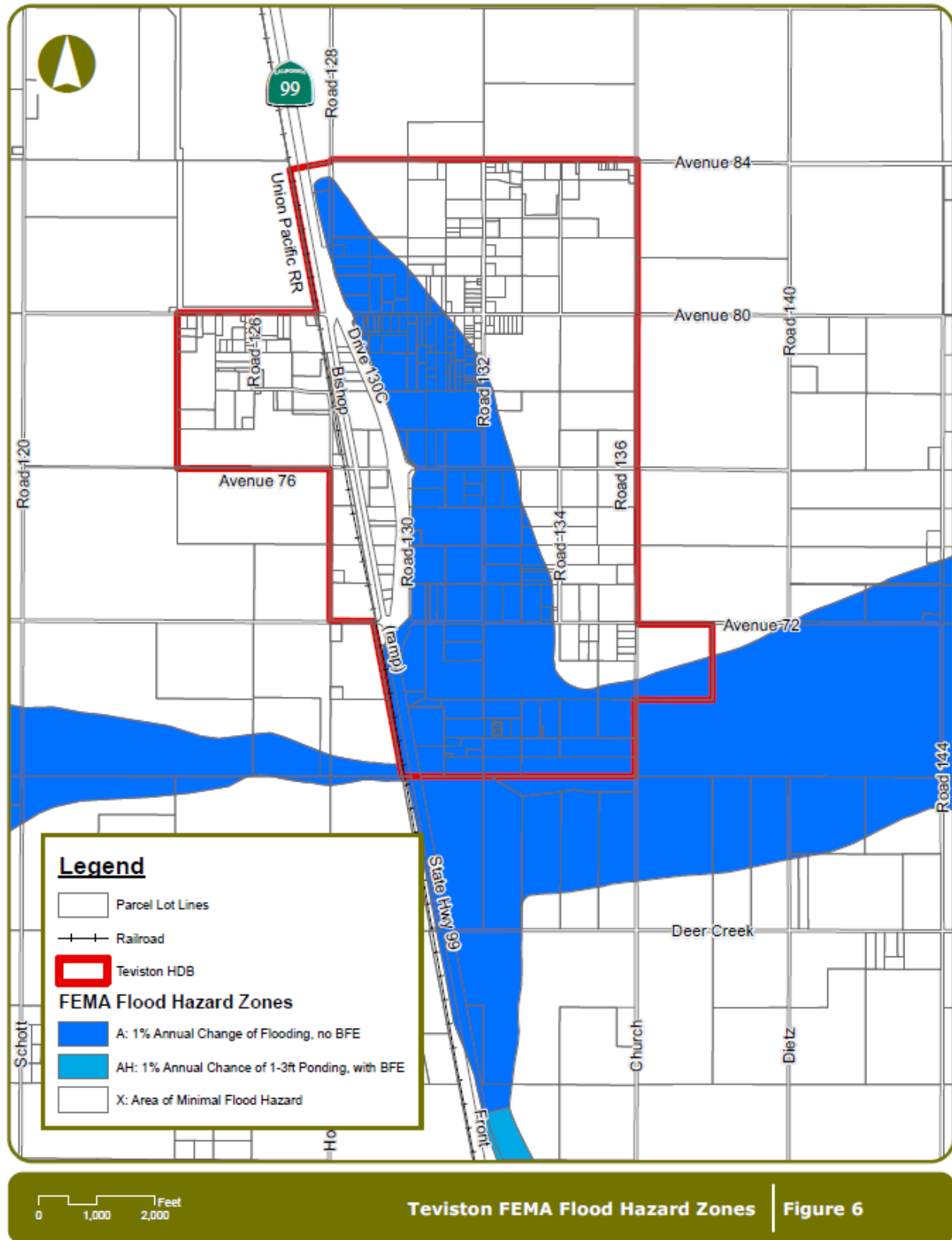
# TEVISTON HAMLET PLAN

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forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

# TEVISTON HAMLET PLAN

Figure 6 - FEMA Flood Map



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## Noise<sup>6</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBa) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy summarizes the daily traffic volumes along State Route (SR) 99 from Avenue 76 to Avenue 96 (see Table 14).

Table 14 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
State Route 99 from Avenue 76 to Avenue 96	38,500	324	698	1,503	3,239

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan serves to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks).

### Energy: Natural Gas/Electricity

Southern California Edison provides service to Teviston.

### Domestic Water & Wastewater

There is no community wide sewer system in Teviston. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the Teviston residents are in favor of creating a community wide sewer system.

<sup>6</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# TEVISTON HAMLET PLAN

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“Domestic water service in Teviston is provided by the Teviston Community Services District (CSD), which was formed in November 1956. Teviston lacks a sanitary sewer system and is served by individual or community septic systems. Table 16 shows the number of existing water connections, the capacity of the system, and the number of additional connections the system can accommodate for new development (Housing Element, May 2012). Mapping of the water system is currently unavailable.

According to the Municipal Service Review 2006 (MSR), Teviston’s water supply is derived from two existing deep underground wells that provide an ample clean water supply requiring no chlorination or treatment. The two wells have a total maximum production efficiency of approximately 900 gallons per minute (GPM).

The Teviston CSD water system supports 105 total connections including 99 residential connections, 4 church connections, 1 school connection, and 1 connection to the community center. In 1998, the District completed several improvements to its water system including replacing old deteriorating water lines, construction of new water lines, installation of fire hydrants throughout the system, installation of meters for all connections, and improvements to the north well site.

The *Preliminary Engineering Report Water System Rehabilitation Project* (Roberts Engineering, November 1995) estimates that the two wells have adequate water supply to support a population of approximately 460 residents, or approximately 125 equivalent dwelling units (EDUs) at a dwelling unit occupancy rate of 3.7 persons per household. It is recommended that the District plan for future water system improvements as the current system reaches its capacity, perhaps through a meter plan, or updated water system study. Potential funding sources should also be identified during the planning process.

Assuming 105 EDUs, in order to meet Tulare County Improvement Standards the Teviston CSD water system would need to be capable of delivering a combined flow rate (from all source and storage facilities) of 857 GPM (500 GPM fire flow and 357 GPM domestic demand) for a period of two hours while maintaining a minimum pressure of 25 pounds per square inch (PSI) to each lot served. The District’s water system is capable of delivering a source flow of 900 GPM, and includes pneumatic pressure tanks for storage, indicating that the system currently meets the requirements of the Tulare County Improvement Standards.”<sup>7</sup> As of the date of this plan, there were no connections available, but it was understood that the Teviston CSD had received a grant and were expanding their capacity. Once installed, the County will know what the capacity available is.

Table 15 - Existing Water Connections		
Number of Existing Connections	Capacity	Available
105	125	20

Source: Action Program 9, Tulare County Housing Element. Data current as of January 2011

## Sewer

There is no community wide sewer system in Teviston. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent.

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<sup>7</sup> Action Program 9, Tulare County 2015 Housing Element

# TEVISTON HAMLET PLAN

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## Storm Drainage

Teviston does not currently have a storm drainage system. A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

## Solid Waste

Solid waste disposal services for the Community of Teviston is provided by Tule Trash, a private company. Solid waste generated in Teviston can be disposed of at Teapot Dome Landfill, located at 21063 Avenue 128, Porterville, California.

## Roads

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used.

# TEVISTON HAMLET PLAN

It is commonly used as patching material and on lower volume service roads”<sup>8</sup>

**Table 16 - Roads in need of Major and Medium Repair**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 72	Road 128 to Bishop Drive	CHIP
2	Avenue 72	Road 130 to Road 136	CHIP
3	Avenue 76	Road 132 to Road 134	CHIP
4	Avenue 76	Road 134 to Road 136	RCST
5	Avenue 78	Road 126 to Road 128	CHIP
6	Avenue 80	Road 126 to Bishop Drive	GRX
7	Avenue 80	SR 99 to Road 131	GRX
8	Avenue 80	Road 131 to Road 132	CHIP
9	Avenue 80	Road 132 to Road 136	GRX
10	Avenue 84	Road 128 to Road 132	GRX
11	Avenue 84	Road 132 to Road 136	CHIP
12	Bishop Drive	Avenue 72 to Avenue 80	CHIP
13	Elm Street	SR 99 to Avenue 84	CHIP
14	Road 126	Avenue 78 to Avenue 80	CHIP
15	Road 128	Avenue 72 to Avenue 78	CHIP
16	Road 130	Avenue 72 to Avenue 76	CHIP
17	Road 131	Williams Avenue to Avenue 80	GRX
18	Road 132	Avenue 72 to Williams Avenue	CHIP
19	Road 132	Avenue 80 to Avenue 84	CHIP
20	Road 134	Avenue 72 to Avenue 76	CHIP
21	Road 134	Avenue 80 to Avenue 84 (End)	CHIP
22	Toad 136	Avenue 72 to Avenue 84	CHIP
23	SR 99	Avenue 76 to Elm Street	CHIP

OLAY – overlay resurfacing operation  
CHIP – chip seal  
GRX – grind and remix

ACST – asphalt reconstruction  
RCST – cold mix reconstruction

## Street Lights

Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

**Table 17** identifies the location of existing street lights that are maintained by Tulare County, in Teviston, as well as their specifications. **Figure 7** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light).

<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

# TEVISTON HAMLET PLAN

The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete.”<sup>9</sup>

**Table 17 - Specifications of Existing Street Lights**

Specifications of Existing Street Lights								
No.	East-West	North-South	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 70	Road 132	NW Corner	1316608E	5800	W	S	SCE
2	Avenue 80	Road 126	SE Corner	854236e	5800	W	N	SCE
3	Avenue 80	Road 131	North Side	Removed	5800	W	S	SCE
4	Avenue 80	Road 132	NW Corner	4477708E	5800	W	S	SCE
5	Avenue 80	Between Road 130 and 131	North Side	854393E	5800	W	S	SCE
6	Avenue 80	Between Road 130 and 131	North Side	1316587E	5800	W	S	SCE
7	Avenue 84	Avenue 76	NW Corner	686466E	5800	W	S	SCE
8	Bishop Drive	Avenue 80	SE Corner	1349358E	5800	W	N/A	SCE
9	Bishop Drive	Avenue 80	Pedestrian Overcrossing (NE Corner)	C/T	9500	S	E	SCE
10	Bishop Drive	Avenue 80	Pedestrian Overcrossing (NE corner)	C/T	5800	S	S	SCE
11	SR 99	Avenue 80	Pedestrian Overcrossing (NE Corner)	N/A	9500	S	W	SCE
12	SR 99	Avenue 80	Pedestrian Overcrossing (NE Corner)	N/A	16000	S	W	SCE
13	Williams	Road 132	SW Corner	1706286E	5800	W	N/A	SCE

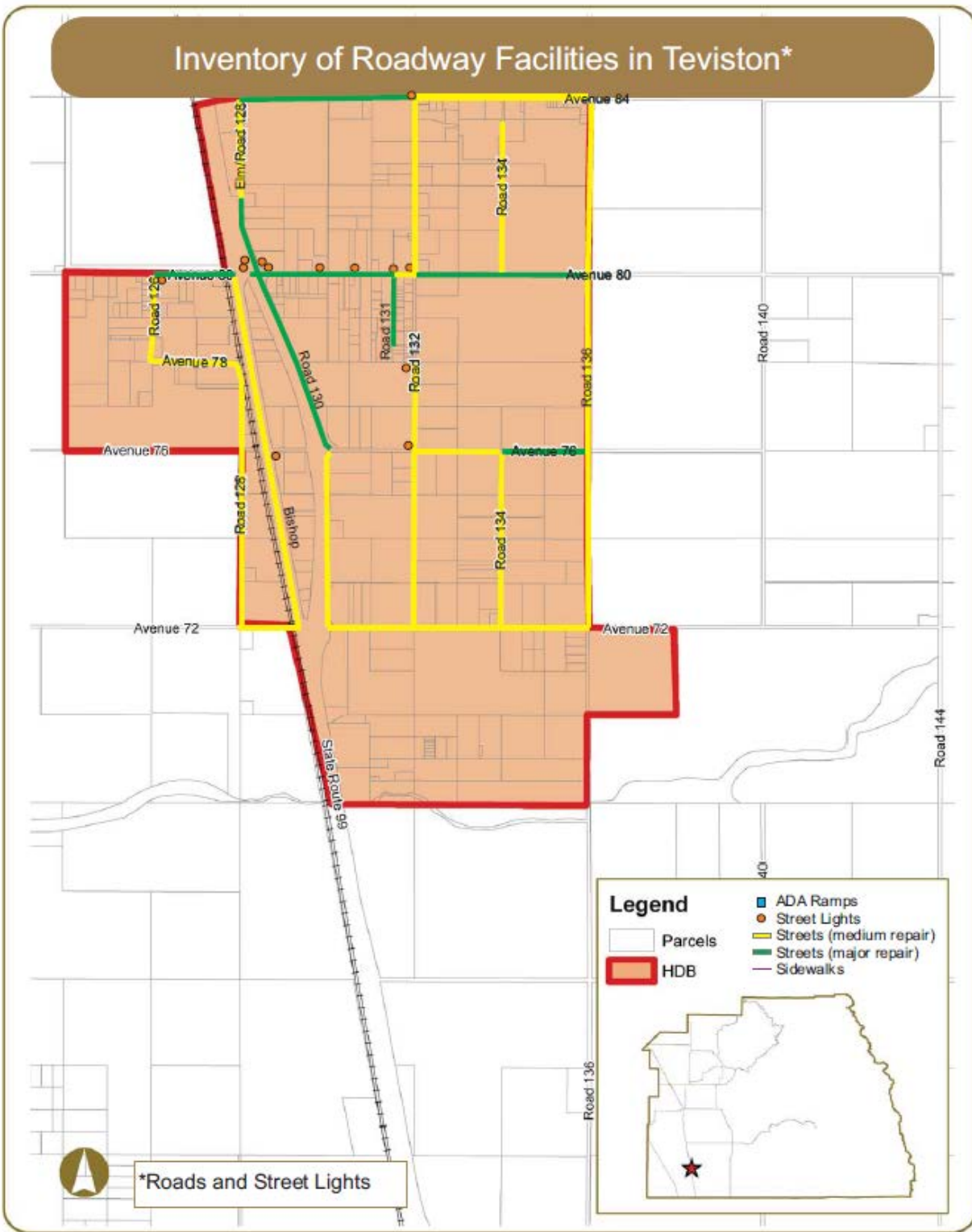
(Source: Tulare County Public Works, March 2013)

<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element



# TEVISTON HAMLET PLAN

Figure 7 - Teviston Inventory of Roadway Facilities



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## Sidewalks

There are currently no sidewalks located within Teviston. Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Teviston.”<sup>10</sup>

## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

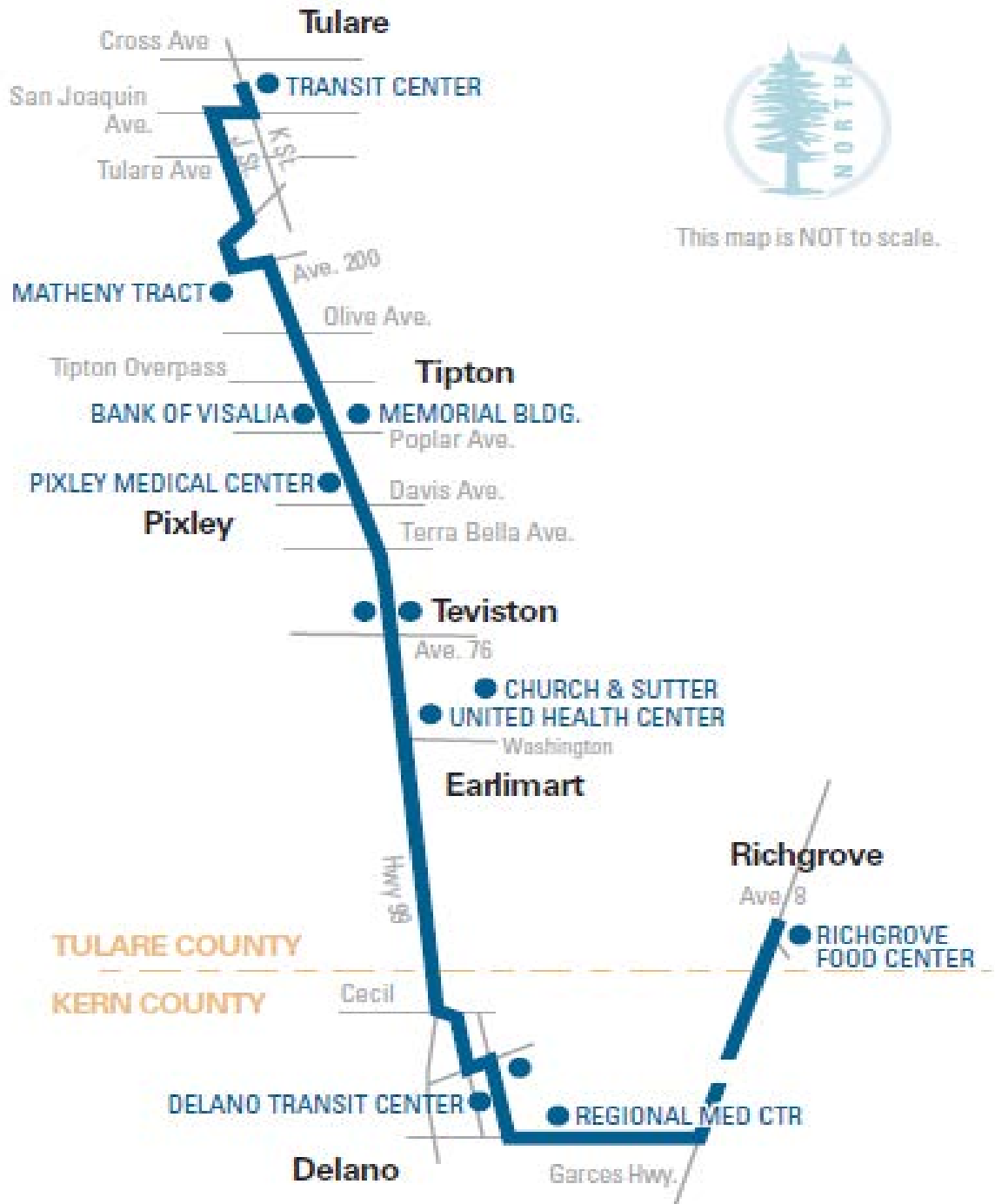
The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare’s urban transit systems. Teviston (Ave. 76 stop) is currently connected via TCAT Route 20/South Valley County to Tulare and Delano transit centers (**see Figure 8**). Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Teviston, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

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<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

# TEVISTON HAMLET PLAN

Figure 8 - TCAG (TCAT) Transit



# TEVISTON HAMLET PLAN

Figure 9 - Transit Southbound/Northbound

Southbound											
WEEKDAY SERVICE											
TULARE o Transit Center	MATHENY TRACT Wade & Canal	TIPTON Bank of Visalia	TIPTON Memorial Building	PIXLEY Pixley Med Center	TEVISTON Avenue 76	EARLMART Church & Sutter	EARLMART United Health Center	DELANO Transit Center	DELANO Vallarta Mkt.	DELANO Regional Med Center	RICHGROVE Richgrove Food Center
5:45	5:56	6:09	6:11	6:21	6:26	6:32	6:35	ARR	DEP	—	—
6:45	6:56	7:09	7:11	7:21	7:26	7:32	7:35	7:48	7:53	7:56	—
7:27	7:38	7:51	7:53	8:03	8:08	8:14	8:17	8:30	8:35	8:38	—
9:04	9:15	9:28	9:30	9:40	9:45	9:51	9:54	10:07	10:12	10:15	—
10:41	10:52	11:05	11:07	11:17	11:22	11:28	11:31	11:44	11:49	11:52	11:57
12:18	12:29	12:42	12:44	12:54	12:59	1:05	1:08	1:21	1:26	1:29	12:12
1:43	1:54	2:07	2:09	2:19	2:24	2:30	2:33	2:46	2:51	2:54	2:59
2:37	2:48	3:01	3:03	3:13	3:18	3:24	3:27	3:40	3:45	3:48	3:14
4:53	5:04	5:17	5:19	5:29	5:34	5:40	5:43	5:56	6:01	6:04	—
5:51	6:02	6:15	6:17	6:27	6:32	6:38	6:41	6:54	6:59	7:02	—
WEEKEND SERVICE											
8:30	8:41	8:54	8:56	9:06	9:11	9:17	9:20	9:33	9:38	9:41	—
10:53	11:04	11:17	11:19	11:29	11:34	11:40	11:43	11:56	12:01	12:04	—
2:06	2:17	2:30	2:32	2:42	2:47	2:53	2:56	3:09	3:14	3:17	—
4:29	4:40	4:53	4:55	5:05	5:10	5:16	5:19	5:32	5:37	5:40	—

Northbound										
WEEKDAY SERVICE										
DELANO Regional Med Center	DELANO Vallarta Mkt.	DELANO Transit Center	EARLMART Church & Sutter	EARLMART United Health Center	TEVISTON Avenue 76	PIXLEY Pixley Med Center	TIPTON Bank of Visalia	TIPTON Memorial Building	MATHENY TRACT Pratt & Addie	TULARE o Transit Center
—	—	—	—	6:35	6:41	6:46	6:56	6:58	7:12	7:22
—	—	7:53	8:09	8:12	8:18	8:23	8:33	8:35	8:49	8:59
—	—	8:35	8:51	8:54	9:00	9:05	9:15	9:17	9:31	9:41
—	—	10:12	10:28	10:31	10:37	10:42	10:52	10:54	11:08	11:18
12:27	12:32	12:35	12:48	12:51	12:57	1:02	1:12	1:14	1:28	1:38
—	—	1:26	1:42	1:45	1:51	1:56	2:06	2:08	2:22	2:32
3:29	3:34	3:37	3:50	3:53	4:10	4:15	4:25	4:27	4:41	4:51
—	—	3:45	4:01	4:04	4:26	4:31	4:41	4:43	4:57	5:07
—	—	6:01	6:17	6:20	6:26	6:31	6:41	6:43	6:57	7:07
—	—	6:59	7:15	7:18	7:24	7:29	7:39	7:41	7:55	8:05
WEEKEND SERVICE										
—	—	9:38	9:53	9:56	10:02	10:07	10:17	10:19	10:33	10:43
—	—	12:01	12:16	12:19	12:25	12:30	12:40	12:42	12:56	1:06
—	—	3:14	3:29	3:32	3:38	3:43	3:53	3:55	4:09	4:19
—	—	5:37	5:52	5:55	6:01	6:06	6:16	6:18	6:32	6:42

## Bicycle Facilities:

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified Road 120 as a proposed Class II Bike Project<sup>11</sup>. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is Road 120.

<sup>11</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# TEVISTON HAMLET PLAN

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## AMTRAK

The Corcoran AMTRAK station, located 25 miles to the northwest in Kings County, is the closest station to Teviston providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

## Aviation

The nearest operational general aviation airport is Meadows Field, Bakersfield's principal commercial airport, is approximately 30 miles south of Teviston and offers direct flights to several destinations.

Fresno Yosemite International Airport (FAT), approximately 70 miles northwest of Teviston, is the principal passenger and airfreight airport in the central San Joaquin Valley. Visalia Municipal Airport, approximately 40 miles north.

## State Route 99

State Route (SR) 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South.

## **PUBLIC SERVICES**

### Community Resource Center

In Teviston, there is a building used as the Community Center at 12934 Avenue. 80 that is used as the local community center.

# TEVISTON HAMLET PLAN

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## Sheriff

The Tulare County Sheriff's Department has a Pixley Substation covering 552 square miles and serving a rural population to include the unincorporated communities of Alpaugh, Allensworth, Teviston, Earlimart, Pixley and Tipton which holds approximately 45,000 people. The Substation runs a four shift operation which includes 16 deputies, five sergeants and one lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. The substation is open for walk-ins from 8:00 am to 5:00 pm. After hours, there is a phone provided outside the substation that calls directly into our dispatch center.

## Fire

Fire protection and emergency medical services are provided for Teviston by the Earlimart Fire Station Tulare County Fire Department. The community is served by Tulare County Fire Department Station #27, located at 200 N. Park Drive, in Earlimart. Patrol 27, Engine 27 and Water Tender 27 are assigned to this location. Nineteen fire hydrants (**see Table 18**) are found within Teviston. These fire hydrants are located within the County right-of-way. **Figure 10** display Existing Fire Hydrants in Teviston.

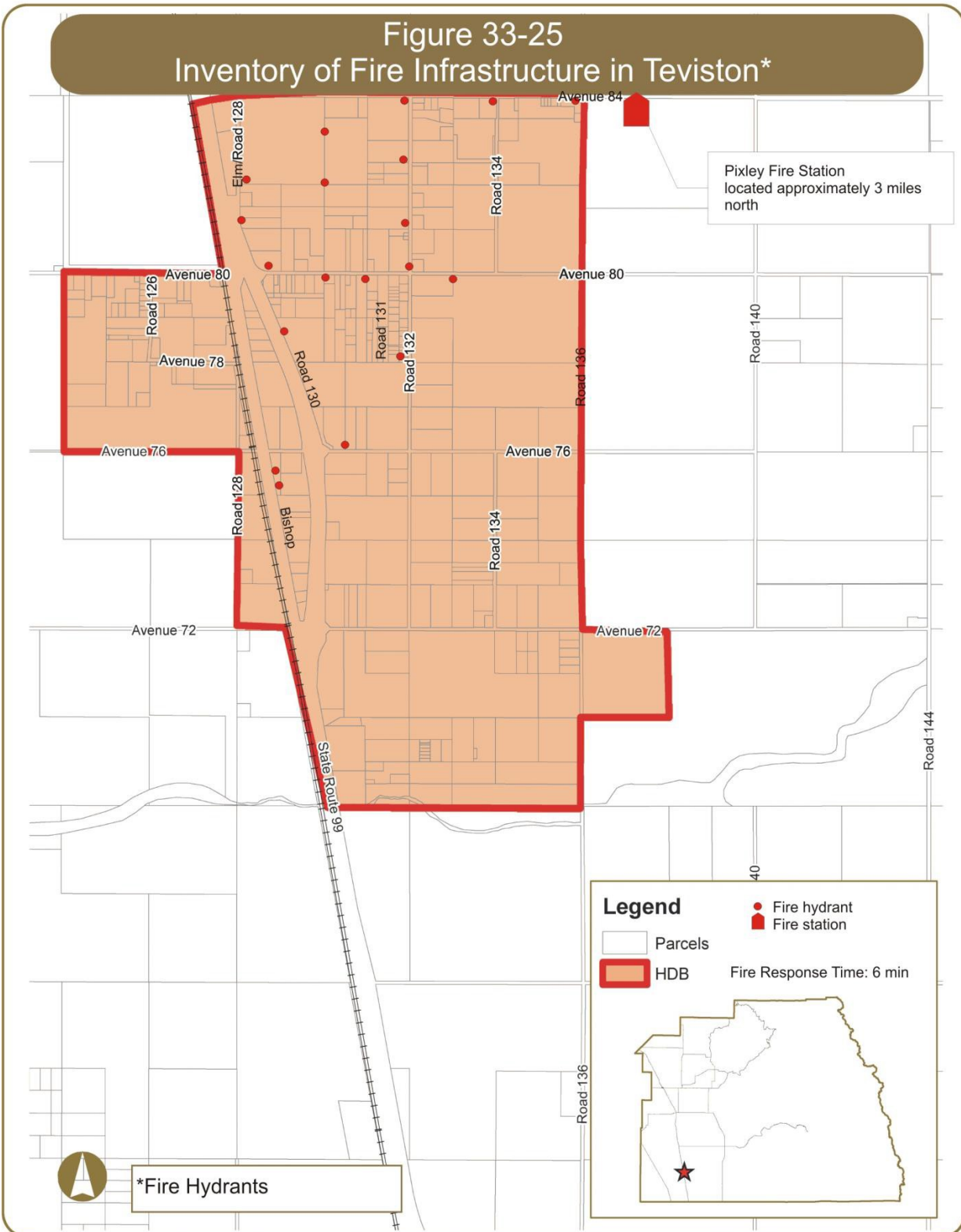
**Table 18 - Existing Fire Hydrants**

Existing Fire Hydrants	
No.	Location
1	Avenue 84 and Road 132
2	Avenue 84 and Road 134
3	Avenue 84 and Road 136
4	Private Road south of Avenue 84 btw Road 128 and Road 132
5	Road 132 south of Avenue 84
6	Road 128 south of Avenue 84
7	Private Road south of Avenue 84 btw Road 128 and Road 132
8	Road 128 north of Avenue 80
9	Road 132 north of Avenue 80
10	Road 128/130 and Avenue 80
11	Avenue 80 east of Road 128/130
12	Avenue 80 west of Road 132
13	Avenue 80 and Road 132
14	Avenue 80 east of Road 132
15	Road 130 south of Avenue 80
16	Road 132 south of Avenue 80
17	Avenue 76 east of Road 130
18	Bishop Street south of Avenue 76
19	Avenue 84 and Road 132



# TEVISTON HAMLET PLAN

Figure 10 - Inventory of Fire Infrastructure in Teviston



# TEVISTON HAMLET PLAN

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## Schools

The Teviston Hamlet Plan Area is within two school districts: 1) North of Avenue 80, the Pixley Union Elementary provides K-8 grade education and high school students attend high school in the Tulare Joint High School District; and 2) south of Avenue 80, Earlimart Elementary provides K-8 grade education and high school students attend high school in the Delano Joint Union High School District.

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>12</sup> The closest library is located in Earlimart 2 miles away. (see Table 19)

Table 19 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Earlimart	Earlimart Branch 780 East Washington Earlimart, CA 93219	Tuesday – Friday: 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 5:00 p.m.

Library hours current as of September 2017

## Parks

Presently there are no County owned/operated public parks in Teviston. The closest public park to the community is Pixley County Park, located three miles north of Teviston. This facility is operated by the County of Tulare and features large grassy areas with shade trees. There are also parks in the Community of Earlimart, located three miles south of Teviston.

# THE TEVISTON HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

The 2014 Regional Transportation Plan and projections for Year 2040 traffic volumes do not exceed the LOS “C” threshold. Less than significant impacts will occur to SR 43 as a result of the Project primarily because increased traffic loads on SR 43, and its various entry and exit point, is more associated with growth in the Central Valley and the increased need for Californians to be able to travel north and south. Both the Highway Capacity Manual (HCM) and the Tulare County General Plan (TCGP) LOS remain at B or C through Year 2040 projections while the build-out time frame of the Community Plan is Year 2030.

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<sup>12</sup> General Plan Background Report, page 7-96



# TEVISTON HAMLET PLAN

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“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>13</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>14</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation

Teviston is a small agricultural community located near Highway 99. The major rural collector roads are Avenue 80 and Road 130/Ave 76. There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

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<sup>13</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>14</sup> Tulare County General Plan, Background Report, page 5-7.

# TEVISTON HAMLET PLAN

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## County Complete Streets / ATP Program

The County has two Programs for Complete Streets for implementing multi-modal (all forms of transportation for all type of users) transportation policies for the Tulare County General Plan and for gaining Active Transportation Project funding from the State and Federal Programs. The County has received over \$6 Million in funding through these programs to date.

## GOALS, OBJECTIVES AND POLICIES

This Section of the Teviston Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Teviston to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Teviston Hamlet Plan, and input received from Teviston citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Create a mix of uses with easy access to major roads**

Objective: Promote mixed use zoning and land uses.

#### Policies:

1. Locate high density residential uses near planned commercial areas.
2. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
3. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
4. The County shall work with the Schools to provide safe routes to school.
5. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Teviston Hamlet Plan.

# TEVISTON HAMLET PLAN

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6. The Teviston Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

**Housing**

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Teviston.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate-income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Teviston.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

**Economic Base**

**GOAL I: Develop a strong and diversified economy.**

# TEVISTON HAMLET PLAN

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Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

Policies:

1. Encourage the Teviston to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## *Environmental Quality and Public Safety*

**GOAL I: Preserve and enhance the quality of life for present and future generation of Teviston citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

## Objective: Protect Agricultural Lands:

Land within the respective Teviston, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

# TEVISTON HAMLET PLAN

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The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Teviston.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Teviston.

# TEVISTON HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 19.5 acres of Teviston Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute of approximately 57 percent of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 565 by 1,051.4 urbanized acres (area within the HDB), which equals 1.8 persons per urbanized acre (**see Tables 20 and 21**) showing population projections). Projecting the population at a 1.3% growth rate adds 118 persons in Year 2030.

### Population Growth Forecast

**Table 20 - Teviston Population Projections**

Growth Rate	2015	2020	2030
0.013	1,623	1,731	1,970

**Demand Forecast** To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in Year 2030 were compared to the vacant land available within the HDB.

**Population and Housing Units** The Year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>15</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in Year 2013 the community had 424 dwelling units (including vacant dwellings) with a population of 1,623. At an annual growth rate of 1.3%, the projected housing units are 603 and 683 in Years 2020 and 2030, respectively, and projected population is 603 and 683 in years 2020 and 2030, respectively.

<b>Table 21 - Population Projections</b>		
Year	Population	Growth (%)
2015	1,623	0.013
2016	1,644	0.013
2017	1,665	0.013
2018	1,687	0.013
2019	1,709	0.013
2020	1,731	0.013
2021	1,754	0.013
2022	1,777	0.013
2023	1,800	0.013
2024	1,823	0.013
2025	1,847	0.013
2026	1,871	0.013
2027	1,895	0.013
2028	1,920	0.013
2029	1,945	0.013
2030	1,970	0.013

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, water service levels need to be expanded, for which Grant funding is needed.

<sup>15</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

# TEVISTON HAMLET PLAN

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## Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

## Education

Tulare County has five satellite campuses for four-year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

## Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are in the Communities of Pixley or Earlimart and the Cities of Tulare and Delano.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 22**) and Zoning Districts Plan (**see Table 23**).

<b>Table 22 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	17.1
Right-of Ways	2.4
Teviston HDB	19.5

<b>Table 23 - Proposed Zoning</b>	
Proposed Zones	Acres
C-2-MU	2.5
R-A-43	14.7
Right-of Ways	2.4
Teviston HDB	19.5

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations found in the Teviston Hamlet Plan. The following components comprise the Teviston Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Teviston Hamlet Plan, there are a several changes to existing zoning districts. These changes are described below.

# TEVISTON HAMLET PLAN

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## Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision-making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

## Mixed Use Overlay District

This alternative involves the creation of a Mixed-Use Zoning Designation for the Community of Teviston.

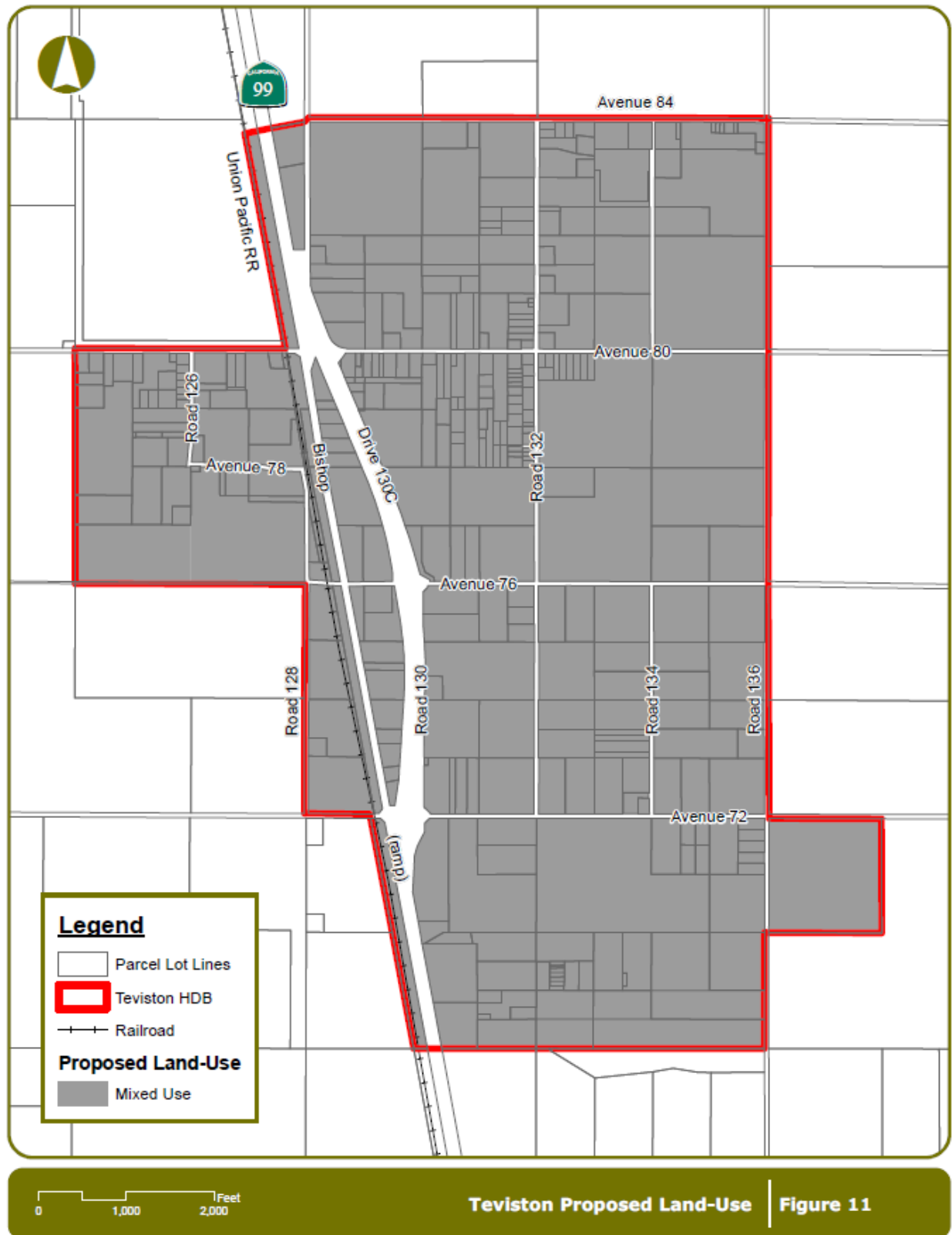
## Zoning Map Update

The current Zoning Map (**see Figure 12**) for Teviston will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 11**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 13**) Teviston proposed Zoning.



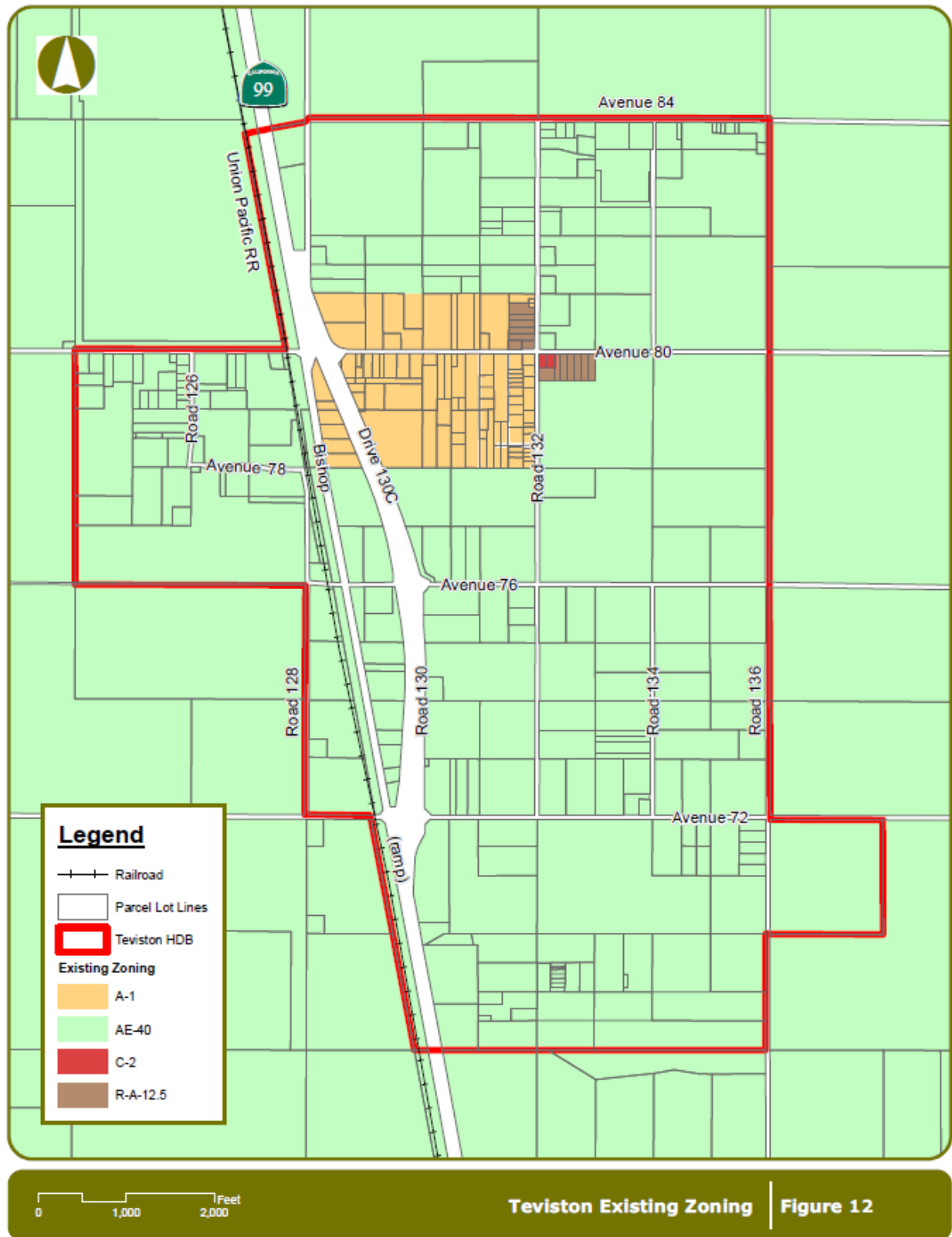
# TEVISTON HAMLET PLAN

Figure 11 - Proposed Land Use Plan



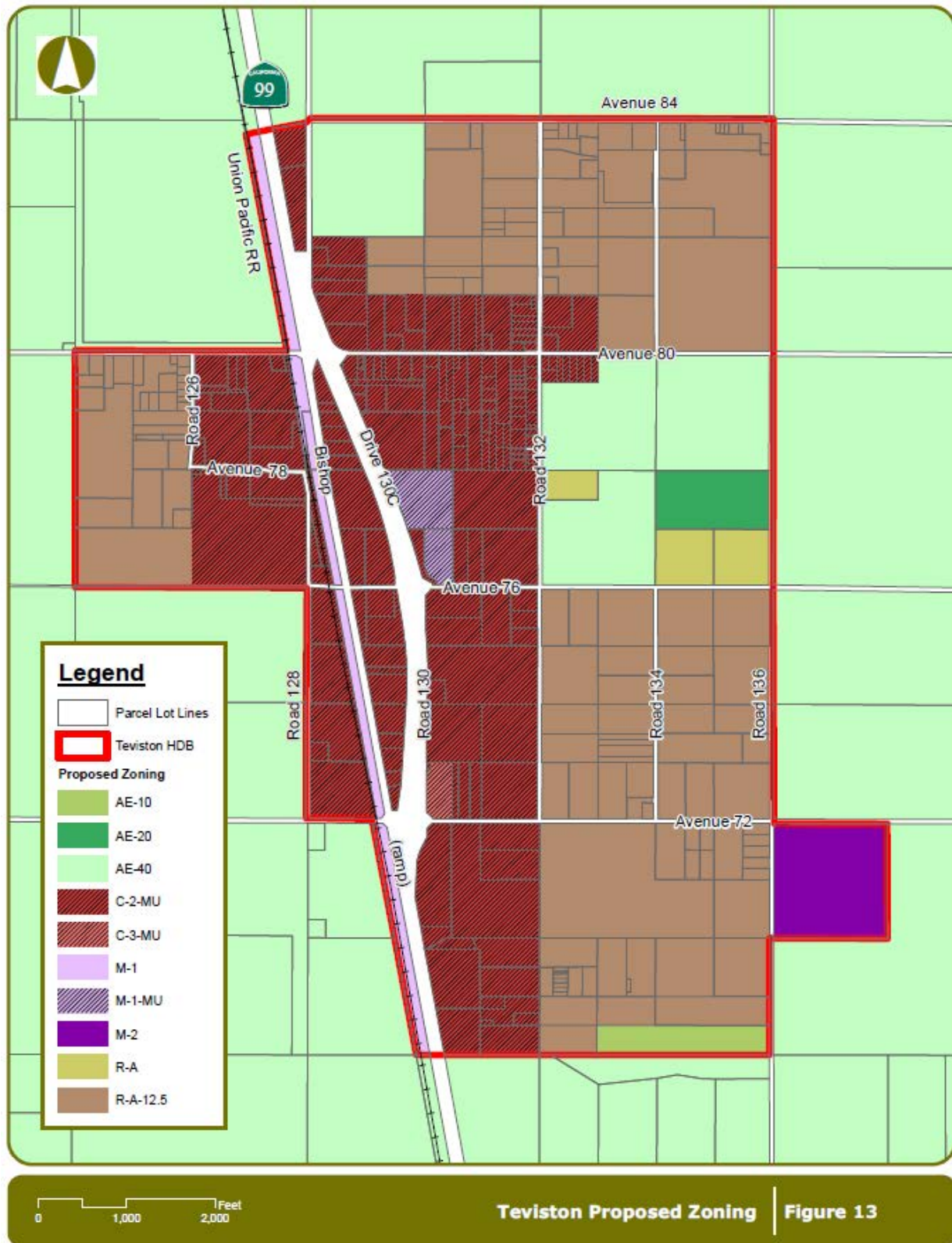
# TEVISTON HAMLET PLAN

Figure 12 - Existing Zoning District



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Figure 13 - Proposed Zoning District



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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# TEVISTON HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	



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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-	R-3, C-1, C-2, C-3,



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	1, R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles	C-2, C-3, M-1, M-2	

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of an airport).		
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1

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Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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## A-2 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the community of Teviston, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Teviston.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Teviston.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Autowrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

# TEVISTON HAMLET PLAN

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Teviston. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

# TEVISTON HAMLET PLAN

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## A-3 Development Standards (Mixed Use Zoning District)

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To promote Economic Development within the Teviston Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Teviston. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Teviston Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Teviston. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.



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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.



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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

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## **APPENDICES**

Appendix A: Planning Commission Resolutions  
Appendix B: Board of Supervisors Resolution



**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and



WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:       None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9369
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-039	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.



B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map

***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976



# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **Tonyville Hamlet Plan**

# TONYVILLE HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# TONYVILLE HAMLET PLAN

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# TONYVILLE HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# TONYVILLE HAMLET PLAN

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# TONYVILLE HAMLET PLAN

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## Tonyville Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Tonyville Hamlet Plan: GPA 17-021  
Zoning District Map: PZC 17-021



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# TONYVILLE HAMLET PLAN

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# TONYVILLE HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# TONYVILLE HAMLET PLAN

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# TONYVILLE HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Mike Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# TONYVILLE HAMLET PLAN

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# TONYVILLE HAMLET PLAN

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# TONYVILLE HAMLET PLAN

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# TONYVILLE HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Tonyville is to develop a plan which can accurately reflect the needs and priorities of this community. Tonyville is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

### Location

The community of Tonyville (**see Figure 1**) is located on the east side of the San Joaquin Valley and is a census-designated place located in the western portion of Tulare County. It is bounded by Avenue 252 in the south, Avenue 254 in the north, and Road 216 in the west and encompasses 0.05 square miles of land. It is not directly served by any State Route (**See Figure 2**). Tonyville is in Section 30 Township 19 South, Range 27 East, MDB&M, and can be found within Rocky Hill United States Geological Survey 7.5 minute topographic quadrangle. Tonyville is located at an elevation of 361 feet National Geodetic Vertical Datum. The coordinates of Tonyville are: Latitude: 36.248561 Longitude: -119.090660.

### Planning Area

The Tonyville Hamlet Development Boundary (HDB) was approved in the 2012 Tulare County General Plan with an area that consists of 34.2 acres (**See Figure 3**). The Land Uses within the HDB are Mixed Use which occupy 29.7 acres of the buildable land. The remaining 4.5 acres are lands dedicated for Right-of-way (**See Table 1**).

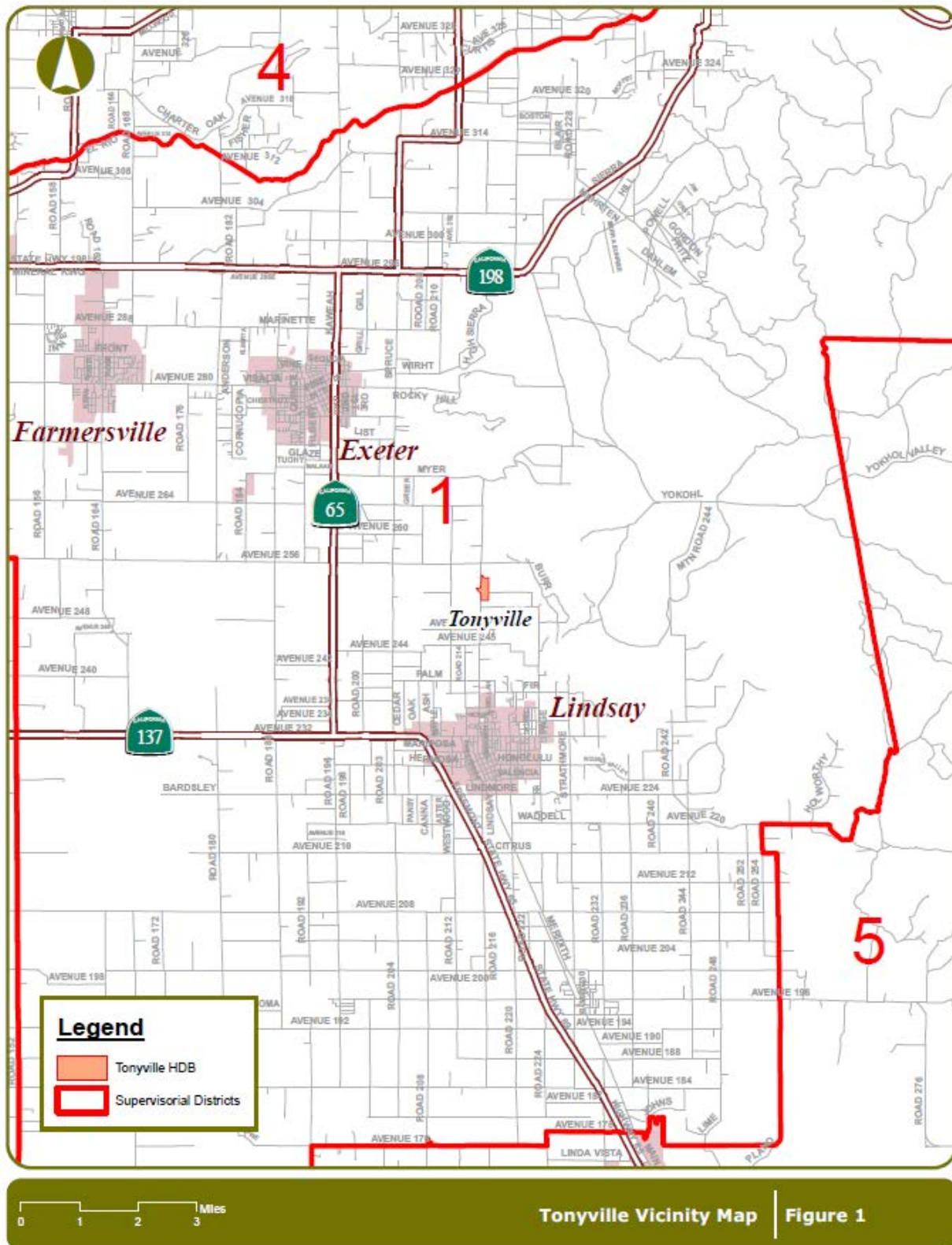
No change is proposed to the Hamlet Development Boundary.

<b>Table 1 - Tonyville Land Use</b>	
<b>Existing Land-Use</b>	<b>Acres</b>
Mixed Use	29.7
Unclassified (Right-of-Way)	4.5
<b>TOTAL</b>	<b>34.2</b>

Source: Tulare County GIS

# TONYVILLE HAMLET PLAN

Figure 1 - Vicinity Map





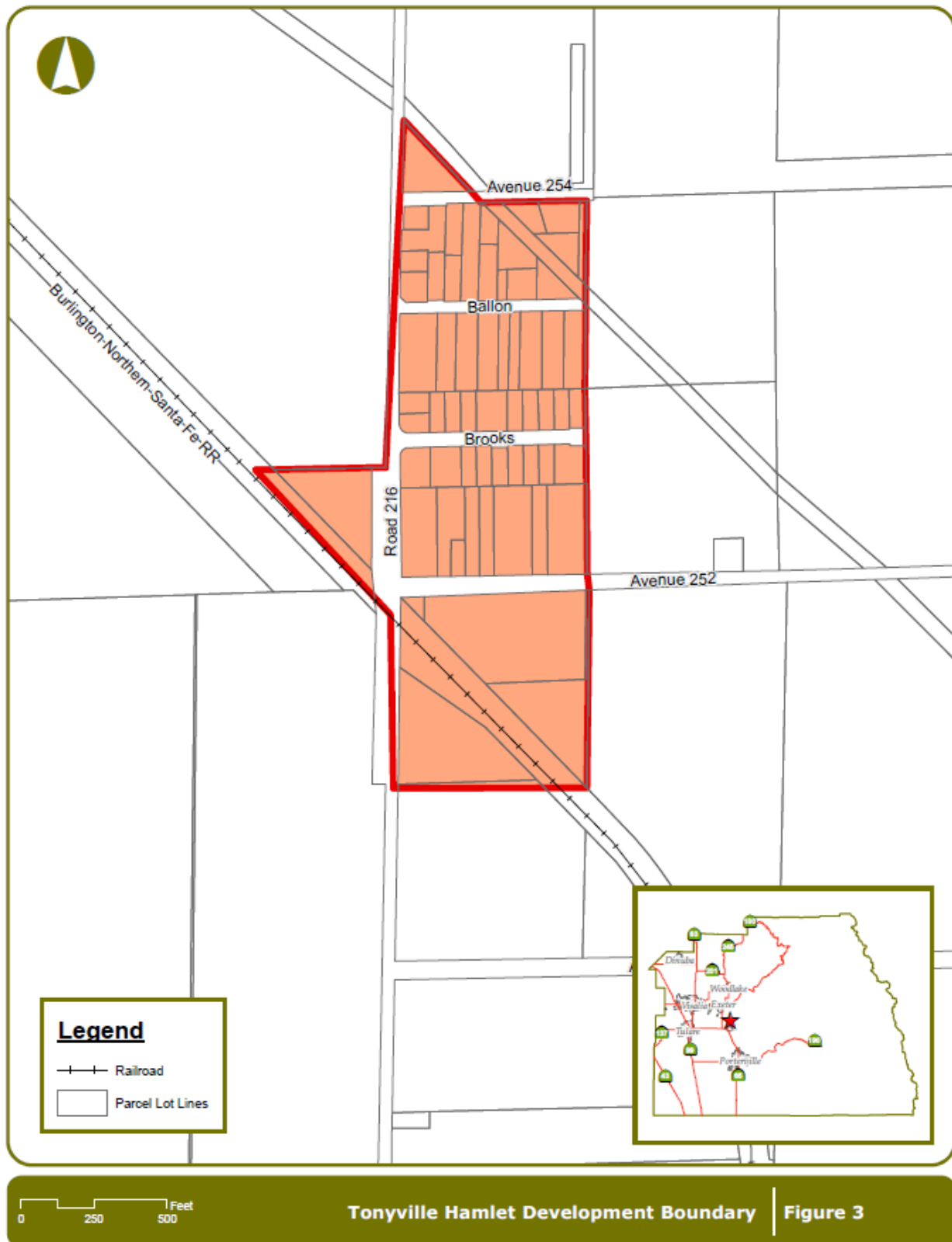
# TONYVILLE HAMLET PLAN

Figure 2 - Aerial Map - Tonyville



# TONYVILLE HAMLET PLAN

Figure 3 - Tonyville HDB



# TONYVILLE HAMLET PLAN

## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Outreach and Invitation Process

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to invite and make local residents aware of the SGC project, Self-Help Enterprises went door to door throughout the community of Tonyville to distribute flyers to homes, businesses, and schools. Fliers were posted at local stores and gas stations.

### Community Outreach Steps

The Leadership Council and SHE performed various outreach methods to promote community participation and increase awareness of the SGC project:

1. An informational flyer was developed.
2. To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner store and all businesses.

### Internet Access

Most families in Tonyville do not have internet access at their homes. Any available internet service is unreliable and cost prohibited. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. Without access to the internet, Tonyville residents are unable to go online job hunting, applying for resources, or provide homework help to their kids.

**Leadership Council for Justice & Accountability and TULARE COUNTY**  
invites you to participate in a  
**COMMUNITY MEETING**  
to  
**DISCUSS IMPROVEMENT NEEDS**  
in  
**Tonyville**

The Sustainable Communities Strategy Project (SCS) seeks to document the following needs: Sewer, Water, Water Quality & Quantity, Storm Water Drainage, Fire, Access to Transportation, Housing, Schools, Libraries, Parks, Access to Healthy Foods and Shopping Opportunities, Access to Medical Facilities, Internet Access and more.

The information that you provide at this meeting will help Tulare County create a plan and identify resources for future infrastructure improvements, land use changes, and zoning changes where possible.

**When:** February 4th, 2016  
**Location:** St. Anthony Mission Church  
21631 Brooks Ave  
Tonyville, CA 93274  
**Time:** 6:00pm - 7:00pm



**For more info please call: Merced at 310-499-8034 or Ashley at 415-686-1368**

The work upon which this publication is based was funded in whole or in part through a grant awarded by the California Strategic Growth Council. Statements and conclusions of this report are those of the County of Tulare and do not necessarily represent the California Strategic Growth Council or the California Department of Conservation, or its employees. The California Strategic Growth Council and the California Department of Conservation make no warranty, express or implied, and assume no liability for the information contained in this text.



**El Consejo de Liderazgo y el condado de Tulare**  
les invita a una  
**JUNTA COMUNITARIA**  
para  
**discutir necesidades de infraestructura**  
en  
**Tonyville**

El Proyecto de Compendio de Comunidades Sostenibles (SCS) está documentando las siguientes necesidades: alcantarillado, agua, calidad y cantidad del agua, drenaje pluvial, agua, fuego, el acceso al transporte, vivienda, escuelas, bibliotecas, parques, acceso a alimentos y tiendas, acceso a servicios médicos, acceso al internet y mucho más!

La información que usted proporcione en esta junta ayudará al condado a crear un plan e identificar recursos para mejorar la infraestructura de La Villa donde sea posible. También ayudará a planificar el uso de la tierra y los cambios de zonificación.

**Quando:** Febrero 4, 2016  
**Lugar:** St. Anthony Mission Church  
21631 Brooks Ave  
Tonyville, CA 93274  
**Horas:** 6:00pm - 7:00pm



**Para más información llame a: Merced al 310-499-8034 o Ashley al 415-686-1368**

El trabajo sobre el que se basa esta publicación fue financiado en su totalidad o en parte a través de una subvención otorgada por el Consejo de Crecimiento Estratégico de California. Los hechos de esta publicación y las conclusiones de esta información son los del condado de Tulare y no necesariamente los del Consejo de Crecimiento Estratégico de California o el Departamento de Conservación de California, o de sus empleados. El Consejo de Crecimiento Estratégico de California y el Departamento de Conservación de California hacen algunas promesas, pero no se garantiza su responsabilidad por la información contenida en este texto.







# TONYVILLE HAMLET PLAN

Self Help Enterprises and Tulare County invites you  
to a community meeting to review the  
**Tonyville Community Plan**  
**Wednesday, February 22, 2017 at 5:00pm at**  
**21626 Brookside Ave, Tonyville CA**

A few months ago Tulare County and Self Help Enterprises came to your community to seek input on a planning analysis of your community. Surveys were distributed and collected. Tulare County invites you to attend a community meeting to see the findings from the study. A thorough examination and inventory of existing land uses will suggest infrastructure improvements, land use changes and possibly zoning changes, where feasible.

For more information, call: Abigail at 559-802-1659

The work upon which this publication is based was funded in whole or in part through a grant awarded by the California Strategic Growth Council.

**Disclaimer:** The statements and conclusions of this report are those of the GRANTEE and/or subcontractor and not necessarily those of the California Strategic Growth Council of the California Department of Conservation, or its employees. The California Strategic Growth Council and the California Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text



Self Help Enterprises y El Condado de Tulare  
le invita a la Junta Comunitaria para revisar  
**El Plan Comunitario de Tonyville**  
**Miercoles 22 de febrero 2017 a las 5 de la tarde**  
**21626 Brookside Ave, Tonyville CA**

Hace unos meses, el Condado de Tulare y Self Help Enterprises vino a su comunidad para buscar la opinión de la comunidad sobre un análisis de planificación de su comunidad. Encuestas fueron distribuidas y recolectadas. El Condado de Tulare le invita a asistir a una junta comunitaria para ver los resultados del estudio. Un examen completo y la organización de los usos existentes de la tierra sugerirán mejoras en la infraestructura, cambios en el uso de la tierra y posiblemente cambios de zonificación, donde sea factible.

Para más información llame a: Abigail 559-802-1659

El trabajo sobre el que se basa esta publicación fue financiado en su totalidad o en parte, a través de una beca concedida por el Consejo de Crecimiento Estratégico de California. **Descargo de responsabilidad:** Las declaraciones y conclusiones de este informe son las del concesionario y/o subcontratista y no necesariamente

# TONYVILLE HAMLET PLAN

## TONYVILLE HAMLET TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Tonyville.

#### Population

In 2015, the Population for Tonyville was 572 (see Table 2).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Tonyville</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	387	67.7%
Female	19,334,329	50.3%	226,607	49.9%	185	32.3%
Total	38,421,464		454,033		572	

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Tonyville's population during the Planning Period (see Table 3). This would result in a total increase of 122 persons by 2030 (7 to 9 persons per year). Note: however, there are recent uncertified surveys that show a lower growth rate, or even a net migration out of the unincorporated County, but those numbers have not been verified.

**Table 3 - Projected Annual Growth Rate**

	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.4%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

# TONYVILLE HAMLET PLAN

## Median Age

Tonyville's population's median age is younger than the median age of both Tulare County and the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Tonyville	23.0

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 0% of Tonyville's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 0% was two races or more (see Table 5). Approximately 100% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Tonyville	%
Total	38,421,464		454,033		572	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	572	100%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	0	%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Tonyville

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

According to the California Department of Finance, the 2011-2015 American Community Survey the unemployment rate for Tonyville was about 9.7% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 9.7% includes only the employable labor force (that is, not every person of the population) results in about 42 unemployed persons of Tonyville's person labor force of 306.

# TONYVILLE HAMLET PLAN

Table 6 - Employment Status			
Employment Status	California	Tulare County	Tonyville
Population 16 years & over	30,312,429	325,404	435
In labor force	19,269,449	194,420	306
Civilian labor force	19,137,441	194,102	306
Employed	17,246,360	170,780	264
Unemployed	1,891,081	23,322	42
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	129

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

In 2015, Tonyville's median household income was not available.

Table 7 - 2011-2015 American Community Survey Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Tonyville CDP	(not available)	\$53,189	\$11,614

## Severely Disadvantaged Community

Tonyville is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

Table 8 - Housing Tenure			
	California	Tulare County	Tonyville
Occupied housing units	12,717,801	133,570	96
Owner-occupied	6,909,176	75,685	28
Renter-occupied	5,808,625	57,885	68

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Tonyville
Owner-occupied	54.3%	56.7%	29.2%
Renter-occupied	45.7%	43.3%	70.8%

2011-2015 American Community Survey 5-Year Estimates

# TONYVILLE HAMLET PLAN

## Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Tonyville	3	13	0	0	9	38	2	8	10	42	24

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Tonyville indicated that approximately 13% of the housing units were sound. Approximately 46% of Tonyville's housing units were deteriorated and 42% were dilapidated (see Table 10).

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Tonyville
Total housing units	13,845,790	144,792	96
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	0
Built 1990 to 1999	1,495,571	21,767	0
Built 1980 to 1989	2,117,819	22,733	34
Built 1970 to 1979	2,503,688	27,111	12
Built 1960 to 1969	1,871,029	15,500	0
Built 1950 to 1959	1,907,512	13,694	30
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	20

2011-2015 American Community Survey 5-Year Estimates



# TONYVILLE HAMLET PLAN

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Tonyville was 5.72 persons per household (see Table 12). The average owner occupied household size is 6.54.

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Tonyville	6.54	5.72

2011-2015 American Community Survey 5-Year Estimates

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% reflect affordability or housing conditions. In 2015, the rental vacancy rate in Tonyville was 0%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. This is probably not an accurate reflection of the Community's vacancy rates. The homeowner vacancy rate was 0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Tonyville	0%	0%

2011-2015 American Community Survey 5-Year Estimates

## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>2</sup>

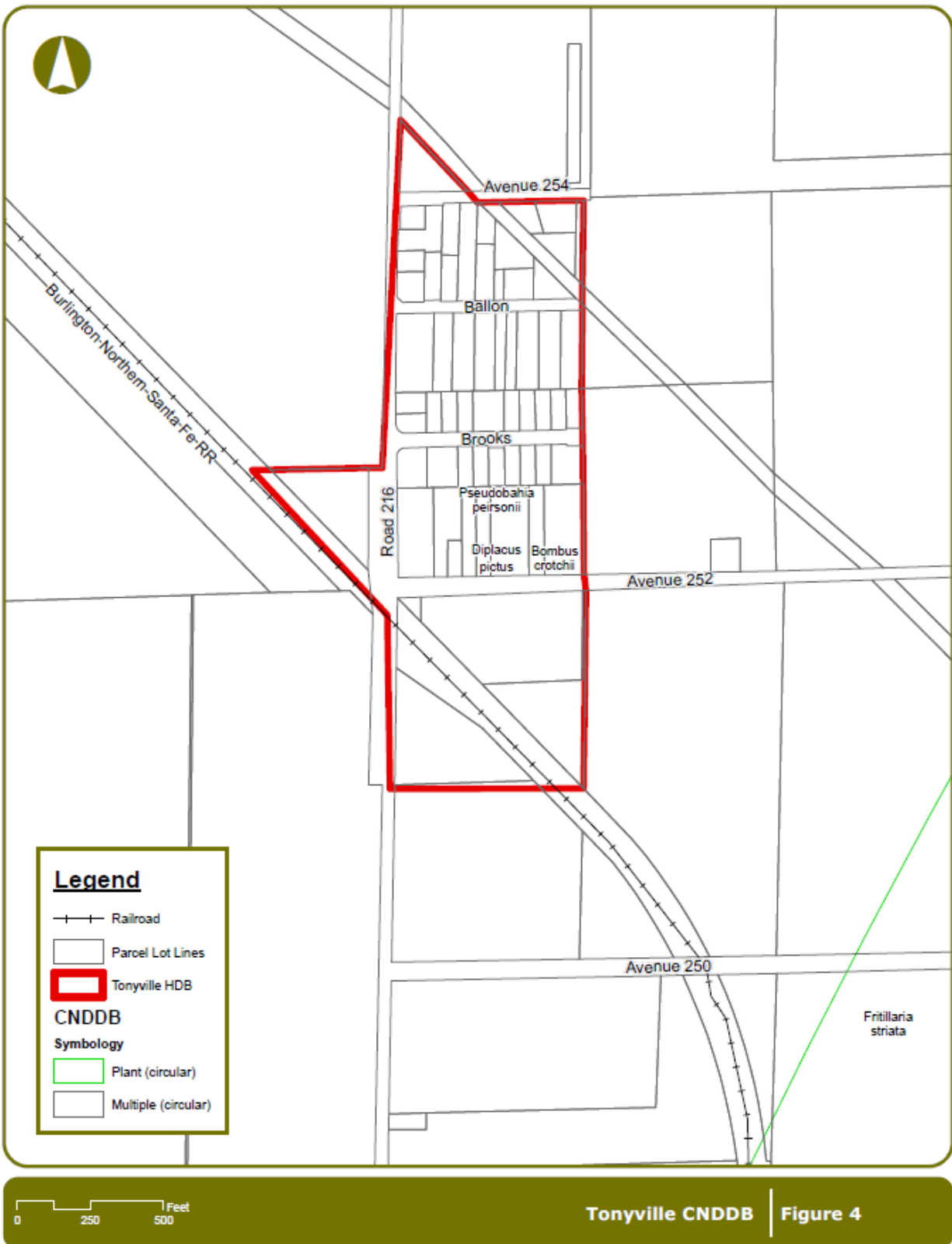
A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016 (see Figure 4) indicates there are special status species within the Rocky Hill Quadrant Species List (which includes the Tonyville Planning Study Area) consisting of one animal species and two plant species: Vernal Pool fairy shrimp (*Branchinecta lynchi*, federal threatened, Striped adobe-lily (*Fritillaria striata*, state threatened, and San Joaquin adobe sunburst, *pesudobahia peirsonii*, federal threatened and state endangered).

<sup>1</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <http://www.dfg.ca.gov/biogeodata/cnddb/mapsanddata.asp>; California Department of Fish and Wildlife. California Natural Diversity Database. November 1, 2016.

# TONYVILLE HAMLET PLAN

Figure 4 - Tonyville CNDDB Map





# TONYVILLE HAMLET PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Tonyville are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Tonyville] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>4</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County (see **Figure 5**), the following soil types are located in Tonyville. Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**Exeter loam, 0 to 2 percent slopes**, moderately deep to a duripan, moderately well drained, very slow to medium runoff, moderately slow permeability above the duripan. The soils that formed in alluvium mainly from granitic sources. Exeter soils are on alluvial fans and stream terraces. The soil is used for irrigated cropland growing oranges, olives and deciduous orchards, vineyards and row crops.

**Porterville clay, 0 to 2 percent slopes**, deep well drained soils that formed in fine textured alluvial material from basic and metabasic igneous rock. Porterville soils are on fans and foothills, used mostly for range pasture. Vegetation is annual grasses, burclove, herbs and widely spaced shrubs. Most cultivated areas are irrigated and planted to oranges, lemons, olive, figs, and some grapes.

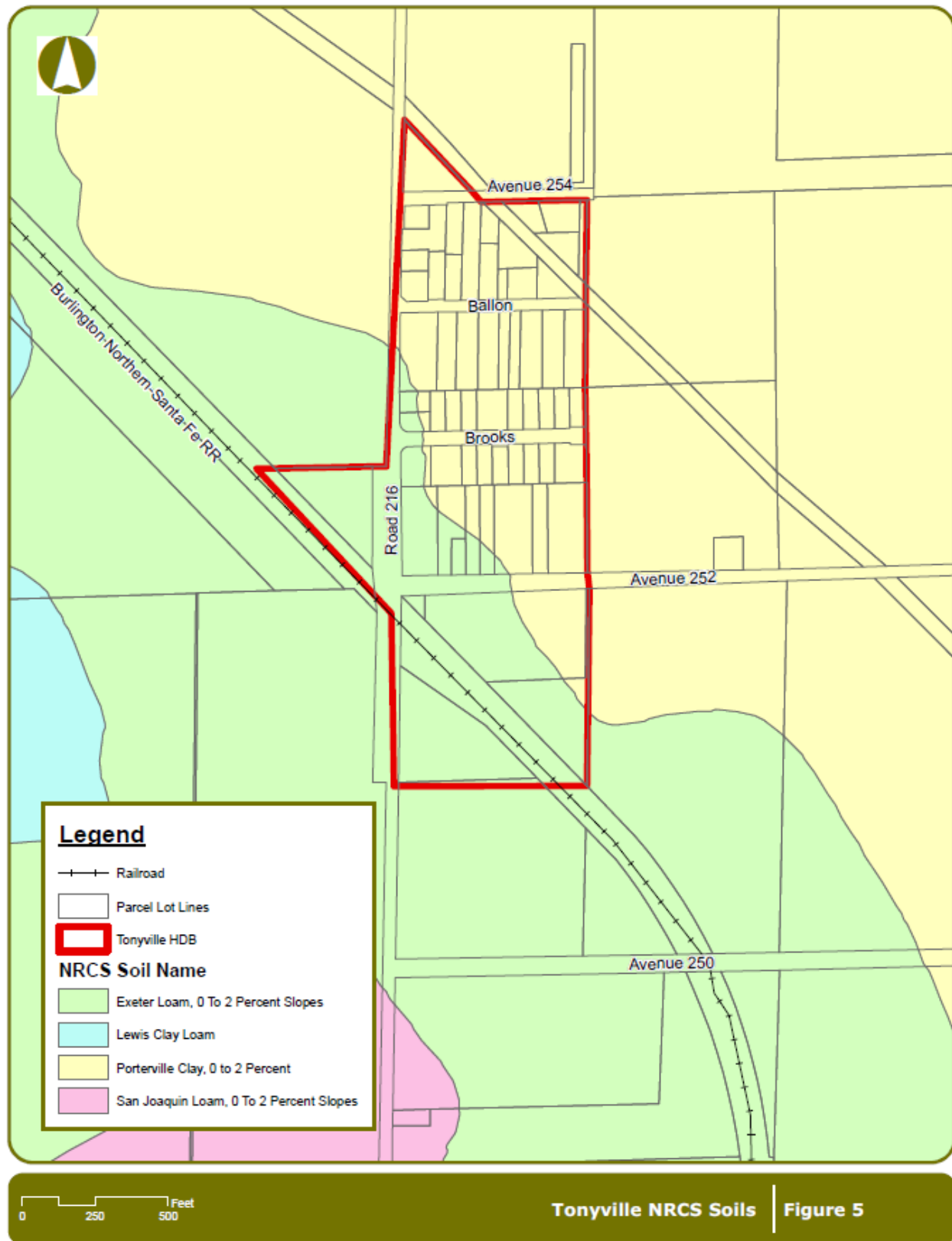
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<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>4</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# TONYVILLE HAMLET PLAN

Figure 5 - Soils Map for Tonyville



# TONYVILLE HAMLET PLAN

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## Physical Conditions

### Air Quality

The Tonyville Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies
- Adopt Local Air Quality Mitigation Fee Program
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations
- Fugitive dust regulation enforcement coordinated with SJVUAPCD
- Energy efficiency incentive programs
- Local alternative fuels programs
- Coordinate location of land uses to separate odor generators and sensitive receptors

There is a correlation between land use and air quality; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Tonyville is located near the southeast end of the Valley with prevailing winds from the northwest, it is in a vulnerable position to accumulate of adversely modified

# TONYVILLE HAMLET PLAN

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air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Tonyville include agricultural activities.

## Flooding<sup>5</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C1305E, show Tonyville (**see Figure 6**) within Flood Zoned X, areas of 0.2% chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance of flood. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy. The areas of 2%, or 500-year, flood events it is suggested that Structures located in a shaded X zone are recommended to be elevated one foot above natural ground.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each Community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use Planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”<sup>6</sup> Although some areas of Tulare County have experienced major flooding along its major rivers, the Tonyville Plan area has not. There are portions of Tonyville, however, that are within and adjacent to the FEMA 500 year flood zones.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Tonyville there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in Special Flood Hazard Areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows:

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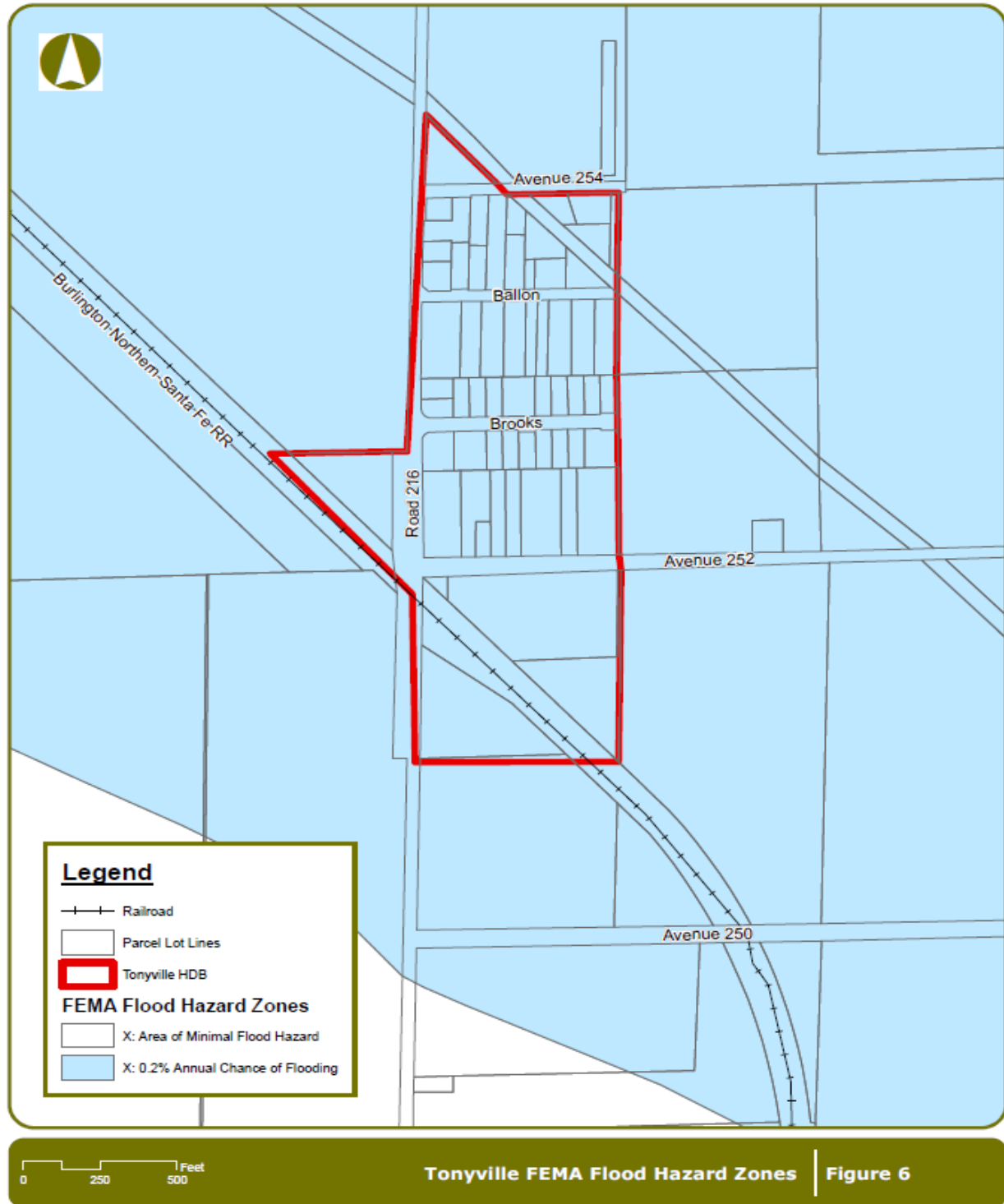
<sup>5</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C1958E, Panel Number 1958 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

<sup>6</sup> General Plan Background Report, page 8-14

# TONYVILLE HAMLET PLAN

<https://msc.fema.gov/portal>.

Figure 6 - FEMA Flood Map



# TONYVILLE HAMLET PLAN

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## Noise<sup>7</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan Background Report Tonyville lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks).

### Energy: Natural Gas/Electricity

Southern California Edison provides service to Tonyville.

### Domestic Water & Wastewater

Domestic water service in Tonyville is provided by the Lindsay-Strathmore Irrigation District (LSID) and sanitary sewer service is provided by Tulare County. **Table 14** shows the number of existing sewer connections, the capacity of the system, and the number of additional connections the system can accommodate for new development (Housing Element, May 2012). The capacity and number of connections for the water system is unavailable. **Figure 7** graphically displays the approximate location of water wells and water lines.

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<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

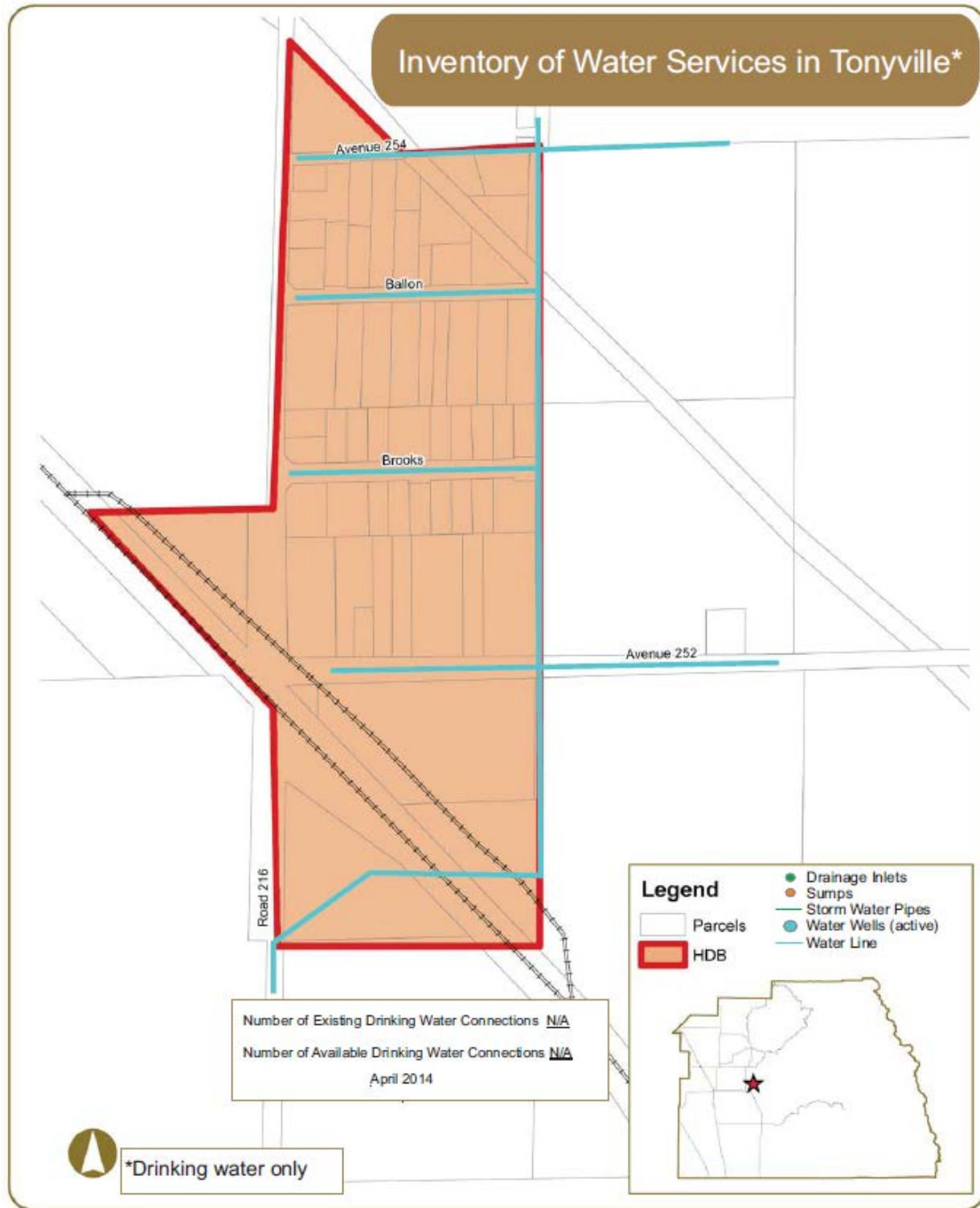
[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)



# TONYVILLE HAMLET PLAN

Figure 7 - Inventory of Water Services in Tonyville



# TONYVILLE HAMLET PLAN

**Table 14 - Existing Water & Wastewater Connections**

Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
N/A	N/A	N/A	79	170	91

\* Data current as of May 2012

## Sewer

Domestic sewer service in Tonyville is provided by Tulare County. **Table 14** shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Tulare County, January 2014). Mapping of the sewer and water systems is currently unavailable.

## Storm Drainage

Tonyville does not currently have a storm drainage system. A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

## Solid Waste

Solid waste disposal services for the Community of Tonyville is provided by USA Waste, a private company. Solid waste generated in Tonyville can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia.

## Roads

“There are several roadways in Tonyville that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.



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If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>8</sup>

**Table 15** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 8** graphically displays this information on a map.

**Table 15 - Road Maintenance Strategies**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 254	Parkside Avenue to Road 217 (End)	CHIP
2	Parkside Avenue	Brooks Avenue to Avenue 39	CHIP

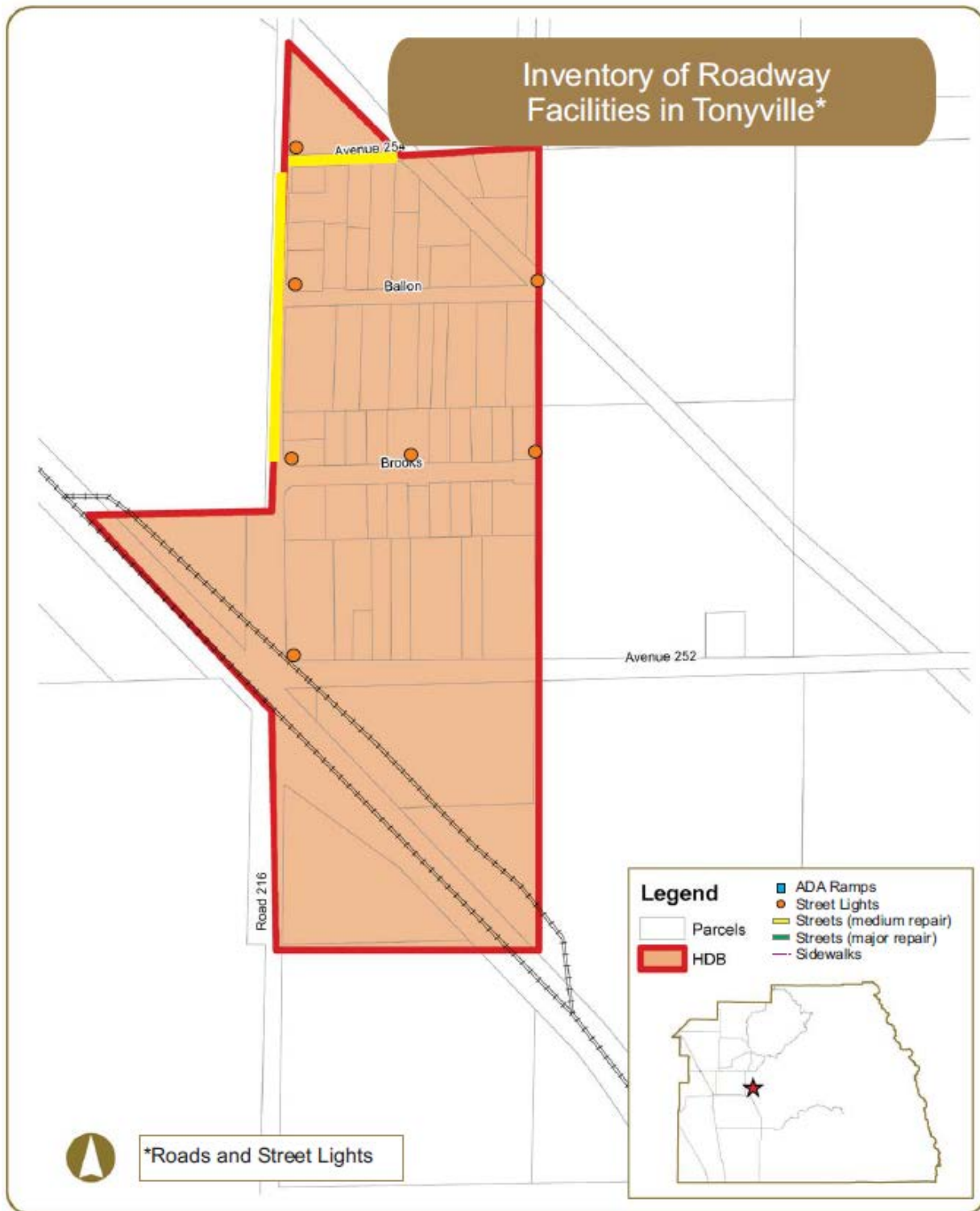
OLAY – overlay resurfacing operation  
CHIP – chip seal  
GRX – grind and remix

ACST – asphalt reconstruction  
RCST – cold mix reconstruction

<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

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Figure 8 - Tonyville Inventory of Roadway Facilities



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## Street Lights

Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

**Table 16** identifies the location of existing street lights that are maintained by Tulare County, in Tonyville, as well as their specifications. **Figure 8** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type "W" represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete.”<sup>9</sup>

**Table 16 - Specifications of Existing Street Lights**

Specifications of Existing Street Lights								
No.	East-West	North-South	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Avenue 252	Road 216	NW Corner	227280E	5800	W	W	SCE
2	Avenue 254	Road 216	NE Corner	N/A	5800	W	W	SCE
3	Ballon Avenue	Road 216	NE Corner	562195E	5800	W	W	SCE
4	Ballon Avenue	Road 217	East dead end (N Side)	570287E	5800	W	S	SCE
5	Brooks Avenue	Road 216	NE corner	2272807E	5800	W	W	SCE
6	Brooks Avenue	Between Road 216 and 217	North Side	1428827E	5800	W	S	SCE
7	Brooks Avenue	Road 217	East dead end (N Side)	722203E	5800	W	S	SCE

(Source: Tulare County Public Works, March 2013)

## Sidewalks

There are currently no sidewalks located within Tonyville. The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# TONYVILLE HAMLET PLAN

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Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Tonyville.”<sup>10</sup>

## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare’s urban transit systems. Public transit is currently not available in Tonyville. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Tonyville, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified SR 65 as a proposed Class III Bike Project<sup>11</sup>. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is State Route 65.

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<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

<sup>11</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# TONYVILLE HAMLET PLAN

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## PUBLIC SERVICES

### Sheriff

Police protection services are provided in Tonyville by the Tulare County Sheriff's Department main Sheriff's Office located at 2404 W. Burrel Avenue, in Visalia, approximately 20 miles west of Tonyville.

### Fire

Fire protection and emergency medical services are provided for Tonyville by the Lindsay Fire Station [fire hydrants]. The Community is served by Tulare County Fire Department Station #15, located at 19603 Avenue 228, in Lindsay. Engine 15 is assigned to this location. Fire Station #16 is located in Strathmore. Fire Station #11 is located in Exeter.

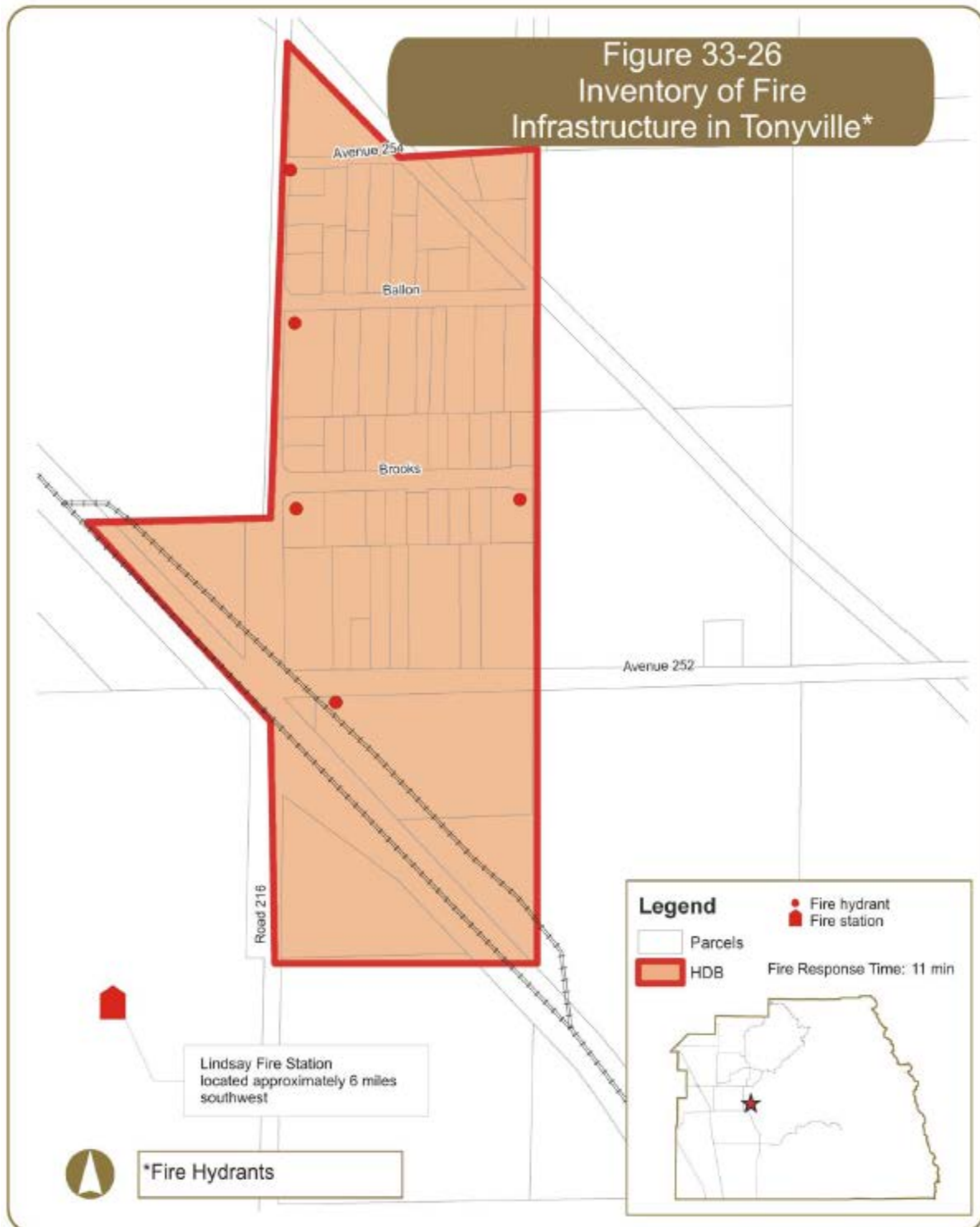
Five fire hydrants (**see Table 17**) are found within Tonyville. These fire hydrants are located within the County right-of-way. **Figure 9** displays Existing Fire Hydrants in Tonyville.

**Table 17 - Existing Fire Infrastructure in Tonyville**

Existing Fire Hydrants	
No.	Location
1	Avenue 254 and Road 216
2	Ballon Avenue and Road 216
3	Brooks Avenue and Road 216
4	Brooks Avenue east of Road 216
5	Avenue 252 east of Road 216

# TONYVILLE HAMLET PLAN

Figure 9 - Inventory of Fire Infrastructure in Tonyville



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## Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Tonyville Hamlet Plan Area is within the Lindsay Unified School District. The District provides K-8 grade education in Lindsay. High School students attend high school in Lindsay. Tonyville is within the Lindsay Unified High School District.

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>12</sup> The closest library is located in Lindsay, approximately three (3) miles to the south and the Exeter Branch is located approximately four (4) miles to the northwest. (see Table 17)

Table 18 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Lindsay	Lindsay Branch 165 N. Gale Hill Street Lindsay, CA 93247	Tuesday & Thursday: 11:00 a.m. - 5:00 pm Wednesday & Friday 9:00 a.m. – 6:00 p.m.
Exeter	Exeter Branch 230 E Chestnut Exeter, CA 93221	Tuesday & Wednesday 11:00 a.m. – 5:00 p.m. 6:00 p.m. – 8:00 p.m. Thursday – Friday 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 6:00 p.m.

Library hours current as of September 2017

## Parks

Presently there are no County owned/operated public parks in Tonyville. The closest public park to the community is Mooney Grove Park, located in Visalia. There are also parks in the City of Lindsay located three miles to the south.

# THE TONYVILLE HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

The 2014 Regional Transportation Plan and projections for Year 2040 traffic volumes do not exceed the LOS “C” threshold. Less than significant impacts will occur to SR 43 as a result of the Project primarily because increased traffic loads on SR 43, and its various entry and exit point, is more associated with growth in the Central Valley and the increased need for Californians to be able to travel north and south. Both the Highway Capacity Manual (HCM) and the Tulare County General Plan (TCGP) LOS remain at B or C through Year 2040 projections while the build-out time frame of the Community Plan is Year 2030.

<sup>12</sup> General Plan Background Report, page 7-96



# TONYVILLE HAMLET PLAN

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“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>13</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>14</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation

Tonyville is a small agricultural community located along the Orange Belt Highway and north of SR 137, near Road 256 and Road 204. The major rural collector road is Avenue 308. There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

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<sup>13</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>14</sup> Tulare County General Plan, Background Report, page 5-7.



# TONYVILLE HAMLET PLAN

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## County Complete Streets / ATP Program

The County has two Programs for Complete Streets for implementing multi-modal (all forms of transportation for all type of users) transportation policies for the Tulare County General Plan and for gaining Active Transportation Project funding from the State and Federal Programs. The County has received over \$6 Million in funding through these programs to date.

## **GOALS, OBJECTIVES AND POLICIES**

This Section of the Tonyville Hamlet Plan prescribes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Tonyville to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Tonyville Hamlet Plan, and input received from Tonyville citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### **Community Development**

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Create a mix of uses with easy access to major roads**

Objective: Promote mixed use zoning and land uses.

### **Policies:**

1. Locate high density residential uses near planned commercial areas.
2. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
3. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
4. The County shall work with the Schools to provide safe routes to school.
5. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Tonyville Hamlet Plan.
6. The Tonyville Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.

# TONYVILLE HAMLET PLAN

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7. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Tonyville Community Plan.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Tonyville.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Tonyville.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

# TONYVILLE HAMLET PLAN

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## Economic Base

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

#### Policies:

1. Encourage the Tonyville to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

#### Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## Environmental Quality and Public Safety

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Tonyville citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

#### Objective: Protect Agricultural Lands:

Land within the respective Tonyville, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County

# TONYVILLE HAMLET PLAN

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General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Tonyville.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Tonyville.

# TONYVILLE HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Tonyville Hamlet Development Boundary consists of 34.2. The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 572 by urbanized acres (area within the HDB), which equals 19 persons per urbanized acre (see **Tables 19 and 20**) showing population projections). Projecting the population at a 1.3% growth rate adds 122 persons by year 2030.

### Population Growth Forecast

**Table 19 - Tonyville Population Projections**

Growth Rate	2015	2020	2030
0.013	572	610	694

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>15</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in Year 2013 the community had 96 dwelling units (including vacant dwellings) with a population of 572. At an annual growth rate of 1.3%, the projected housing units are 102 and 117 in years 2020 and 2030, respectively, and projected population is 610 and 694 in years 2020 and 2030, respectively.

Table 20 - Population Projections		
Year	Population	Growth (%)
2015	572	0.013
2016	579	0.013
2017	587	0.013
2018	595	0.013
2019	602	0.013
2020	610	0.013
2021	618	0.013
2022	626	0.013
2023	634	0.013
2024	643	0.013
2025	651	0.013
2026	659	0.013
2027	668	0.013
2028	677	0.013
2029	685	0.013
2030	694	0.013

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, water service levels need to be expanded, for which Grant funding is needed.

<sup>15</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

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## Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require a discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

## Education

Tulare County has five satellite campuses for four-year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

## Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices is in the City of Exeter.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 21**) and Zoning Districts Plan (**see Table 22**).

<b>Table 21 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	29.7
Right-of Ways	4.5
Tonyville HDB	34.2

<b>Table 22 - Proposed Zoning</b>	
Proposed Zones	Acres
C-2-MU	8.6
M-1	2.9
M-1-MU	4.2
R-A	14.0
Right-of Ways	4.5
Tonyville HDB	34.2

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations found in the Tonyville Hamlet Plan. The following components comprise the Tonyville Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overly District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Tonyville Hamlet Plan, there are a several of changes to existing zoning districts. These changes are described below.

# TONYVILLE HAMLET PLAN

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## Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

## Mixed Use Overlay District

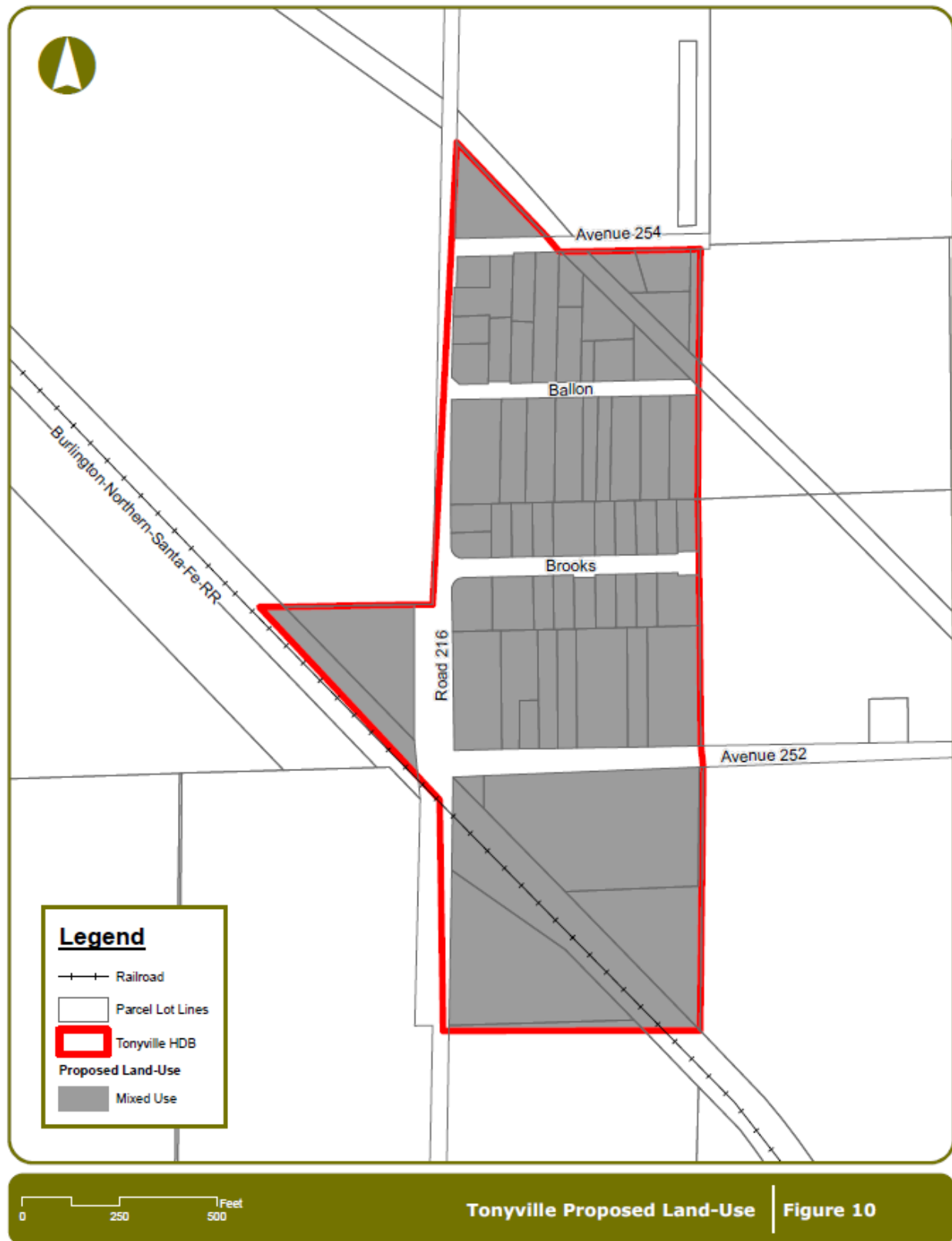
This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Tonyville.

## Zoning Map Update

The current Zoning Map (**see Figure 11**) for Tonyville will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 10**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 12**) Tonyville proposed Zoning.

# TONYVILLE HAMLET PLAN

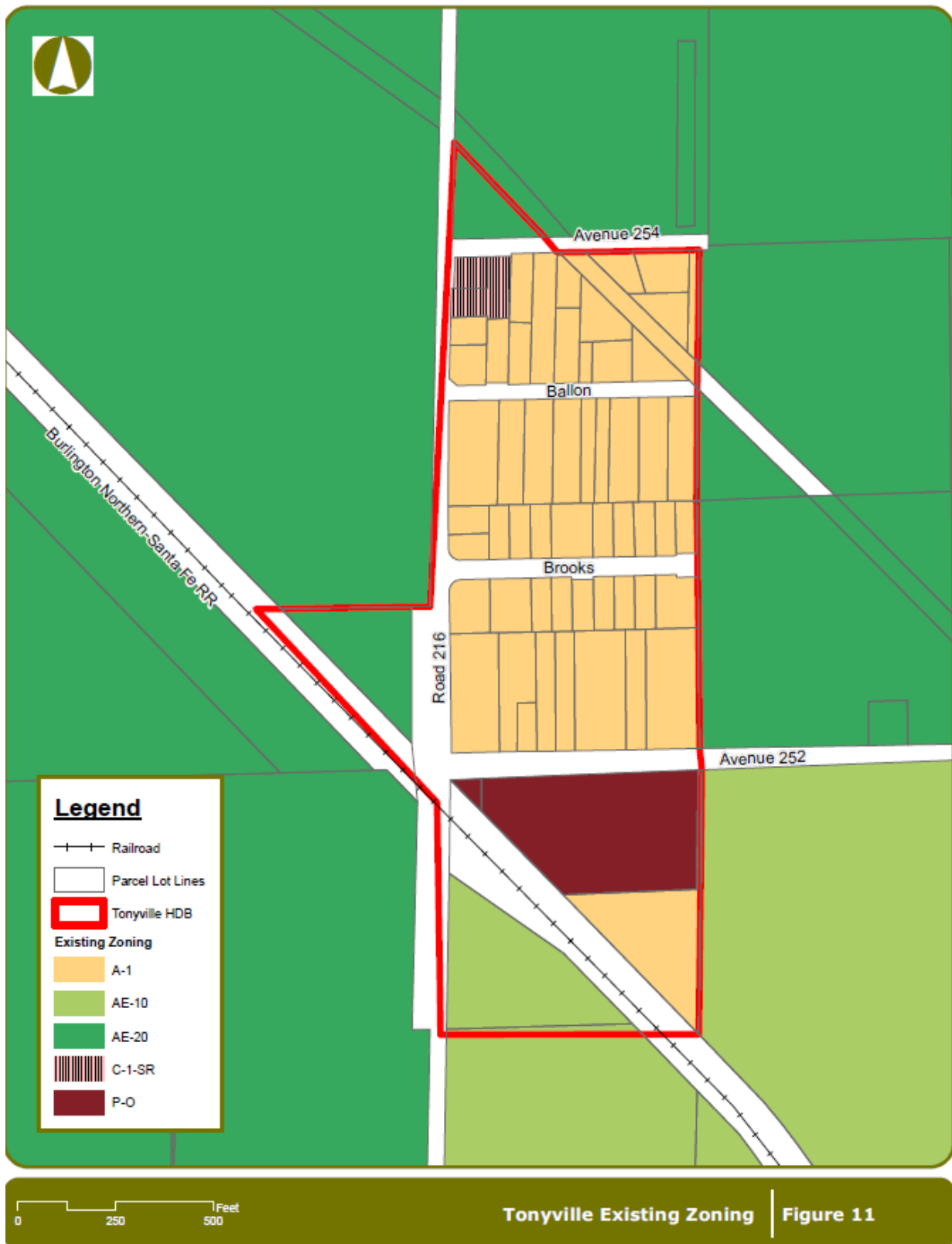
Figure 10 - Proposed Land Use Plan





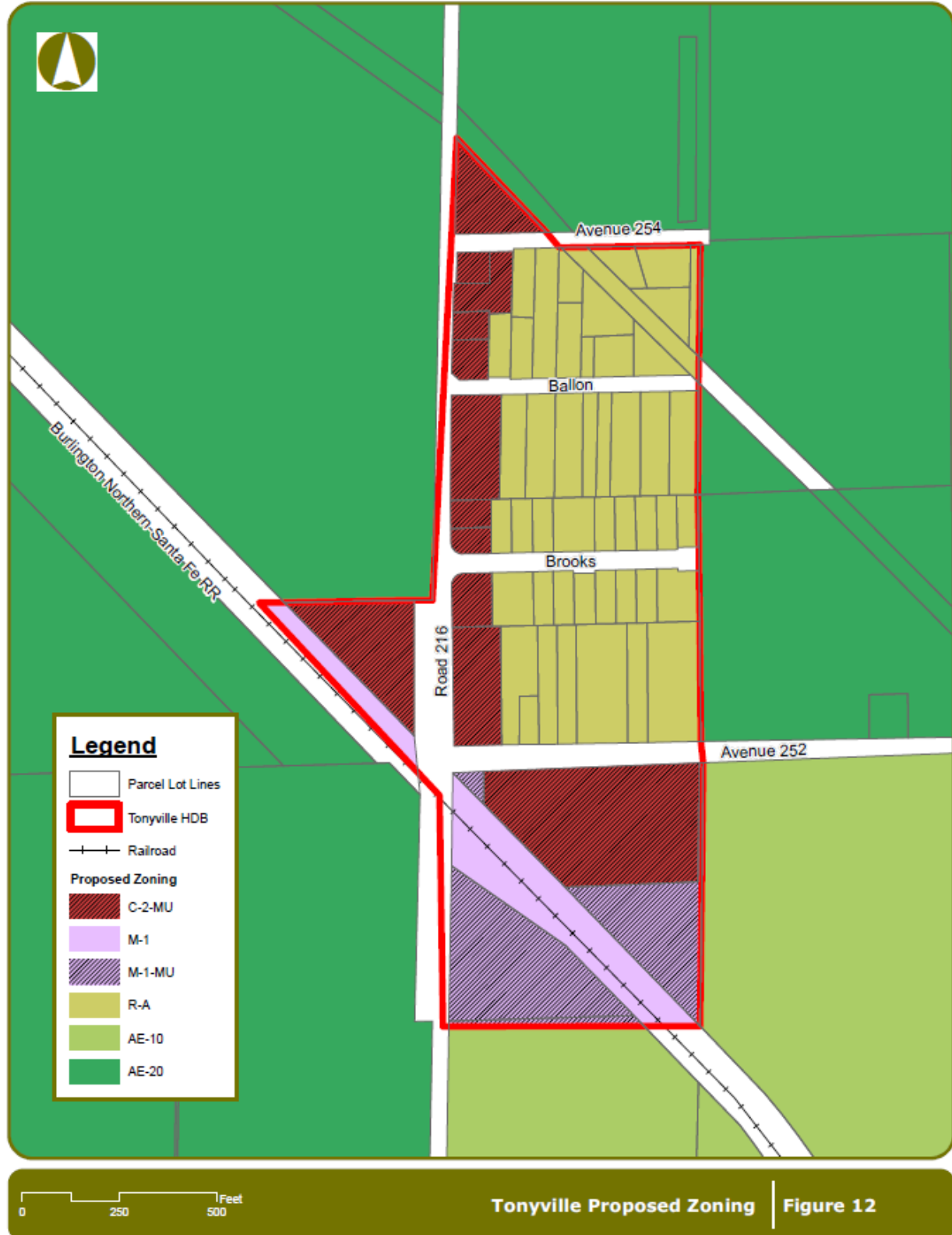
# TONYVILLE HAMLET PLAN

Figure 11 - Existing Zoning Plan



# TONYVILLE HAMLET PLAN

Figure 12 - Proposed Zoning Changes



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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

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## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-	R-3, C-1, C-2, C-3,

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	1, R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles	C-2, C-3, M-1, M-2	



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of an airport).		
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1

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Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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## A-2 *Mixed Use Overlay District (Zone Change Text)*

The following regulations shall apply in the community of Tonyville, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Tonyville.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Tonyville.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

# TONYVILLE HAMLET PLAN

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Tonyville. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

# TONYVILLE HAMLET PLAN

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## **A-3     *Development Standards (Mixed Use Zoning District)***

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To promote Economic Development within the Tonyville Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Tonyville. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Tonyville Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Tonyville. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1     Entries to buildings should be individualized and clearly identifiable.

A-2     Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3     Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4     The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5     Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6     Each building should have a defined base, body, and cap segment

A-7     Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8     Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9     Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10    Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11    Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

# TONYVILLE HAMLET PLAN

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# TONYVILLE HAMLET PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.



# TONYVILLE HAMLET PLAN

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

# TONYVILLE HAMLET PLAN

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

# TONYVILLE HAMLET PLAN

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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

# TONYVILLE HAMLET PLAN

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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

# TONYVILLE HAMLET PLAN

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

# TONYVILLE HAMLET PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# TONYVILLE HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

## Waukena Hamlet Plan



# WAUKENA HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# WAUKENA HAMLET PLAN

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# WAUKENA HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# WAUKENA HAMLET PLAN

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## WAUKENA HAMLET PLAN

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# Waukena Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Waukena Hamlet Plan: GPA 17-022  
Zoning District Map: PZC 17-022



Tulare County Resource Management Agency  
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# WAUKENA HAMLET PLAN

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## **Tulare County Board of Supervisors**

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Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

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Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# WAUKENA HAMLET PLAN

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# WAUKENA HAMLET PLAN

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# WAUKENA HAMLET PLAN

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# WAUKENA HAMLET PLAN

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# WAUKENA HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Waukena is to develop a plan which can accurately reflect the needs and priorities of this community. Waukena is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks) and to stimulate economic development within the community.

### Location

The community of Waukena (**see Figure 1**) is located on the east side of the San Joaquin Valley and is a census-designated place located in the southwest portion of Tulare County on California State Route 137, approximately 4 miles northeast of Corcoran and approximately 13.5 miles southwest of Tulare. It is bounded by Road 24 in the west, Curti Road in the east, and north and south of Avenue 192 and encompasses 0.9 square miles of land. It is directly served by State Route (SR) 137 (**see Figure 2**). Waukena is in Section 31 and 32 Township 20 South, Range 23 East, and Section 05 Township 21 South, Range 23 East MDB&M, and can be found within Waukena United States Geological Survey 7.5 minute topographic quadrangle. Waukena is located at an elevation of 226 feet National Geodetic Vertical Datum. The coordinates of Waukena are: Latitude: 36.138238 Longitude: -119.509886.

### Planning Area

The Waukena Hamlet Development Boundary (HDB) area consists of 119.3 acres (**see Figure 3**). The Land Uses within the HDB are Mixed Use activities that occupy 104.4 acres. The remaining 14.9 percent are lands dedicated for Right-of-way (**see Table 1**).

No change is proposed to the Hamlet Development Boundary.

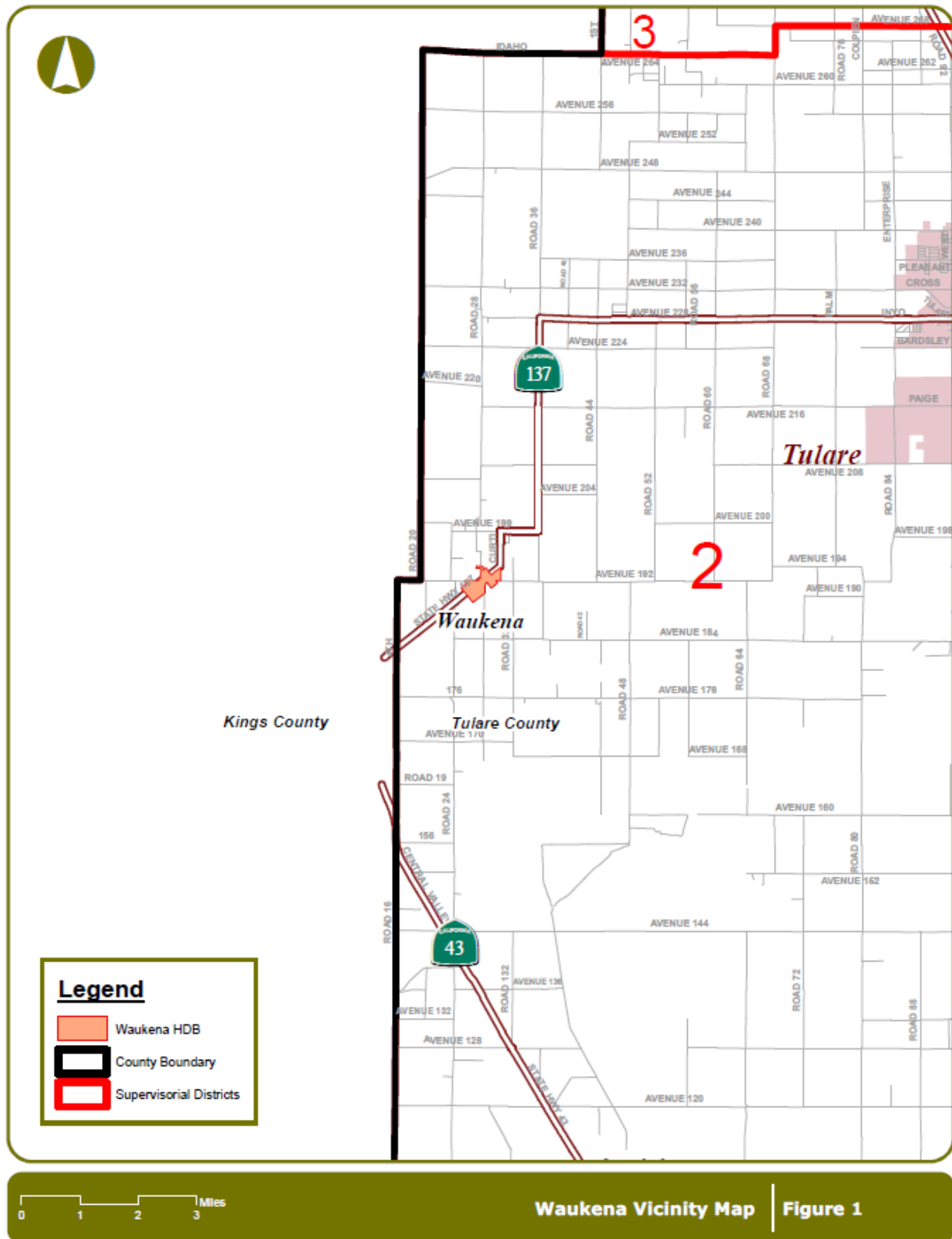
Table 1 - Waukena Land Use	
Land Use	Acres
Mixed Use	104.4
Rights-of-way	14.9
Total	119.3

Source: Tulare County GIS



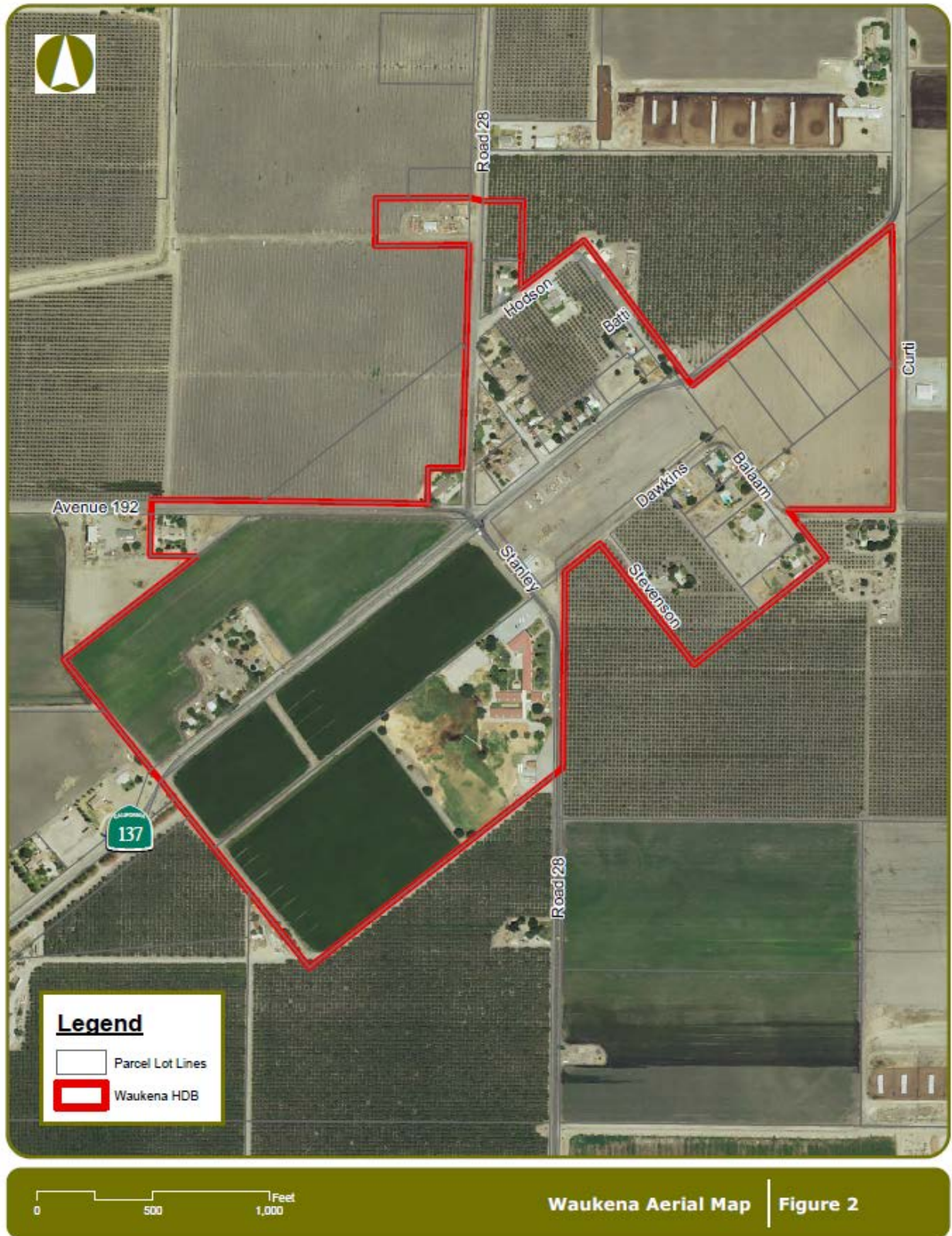
# WAUKENA HAMLET PLAN

Figure 1 - Vicinity Map



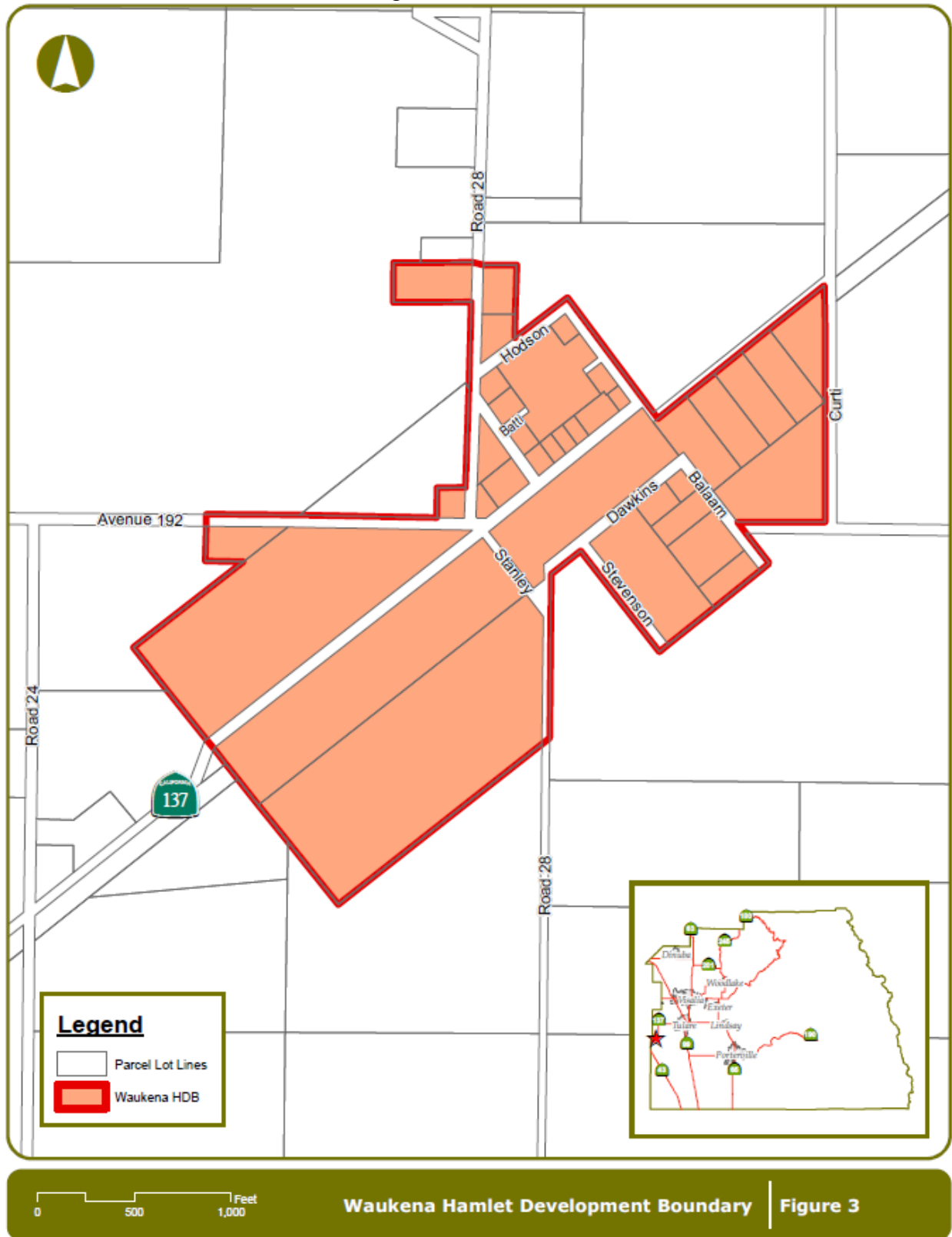
# WAUKENA HAMLET PLAN

Figure 2 – Aerial – Waukena



# WAUKENA HAMLET PLAN

Figure 3 - Waukena HDB



# WAUKENA HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Outreach and Invitation Process

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the Strategic Growth Council (SGC) project.

- 1.) An informational flyer was developed.
- 2.) SHE staff went door to door to distribute flyers to every home and business in Waukena during the day and in the evening on Saturday February 13, 2016 and Thursday February 18, 2016
- 3.) SHE partnered with the Waukena School District (WSD) to distribute flyers to their students. Meeting flyers were sent home from school with each of the 250 students of the WSD.
- 4.) Throughout the community outreach process, SHE staff explained the goals of the project and expressed the importance and the community benefits of participating in the SGC community meeting.
- 5.) Flyers were posted at the local corner store and the local post office.

### Community Meeting

Self-Help Enterprises conducted a community meeting in Waukena on Saturday February 20, 2016, at 3:00 pm. The meeting was scheduled during the weekend to make it easier for working residents to attend. The meeting was held at Waukena School. The purpose of the meeting was to discuss Waukena improvement needs, gather community data, and report the findings of the meeting in a final report. Ten residents attended the meeting. Nine (9) community surveys were collected.



### Community Survey

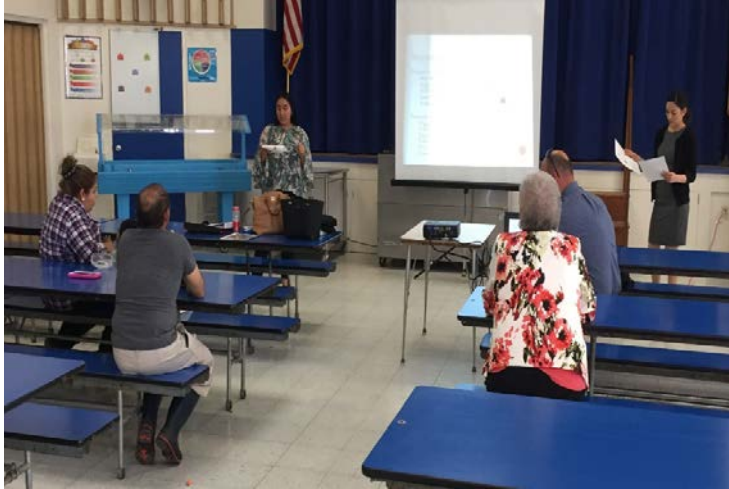
A survey was developed as a tool to gather a variety of community information about multiple topics. The survey inquires about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.



# WAUKENA HAMLET PLAN

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Tulare County Staff has conducted two meetings at Waukena in regard to the Community Plan and Active Transportation Plan, since the SHE conducted their meeting. The first meeting was completed on April 20, 2017, and the second follow up meeting was on Monday, May 15, at the School Board Meeting to present the Zoning Plan, and to see how the County could incorporate the school's planning efforts. The County was informed there would be traffic calming lights installed by the end of the 2017 summer south of and north of the school on Ave. 28.

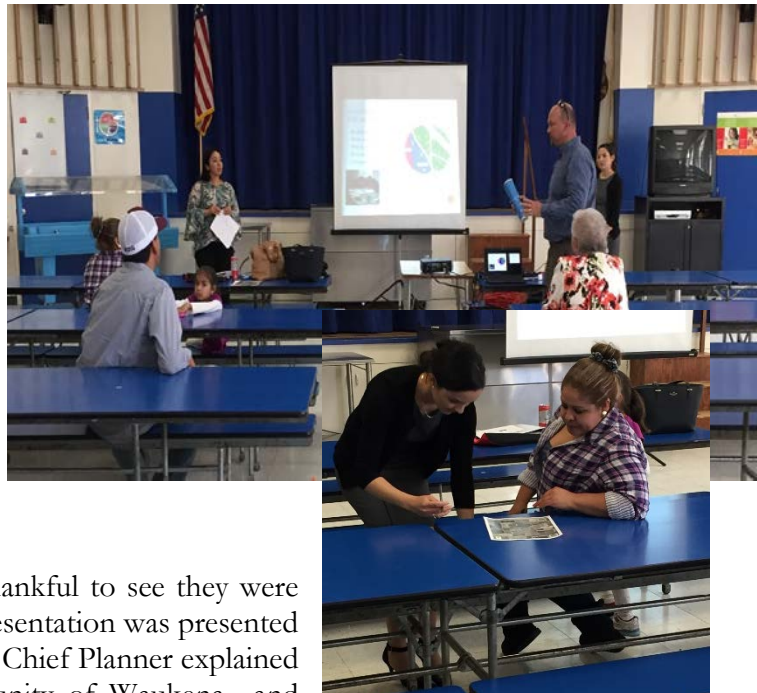


On April 20, 2017 a community meeting was held in the Community of Waukena. Approximately 4 members of the community attended and voiced their concerns with routes regarding safety improvements. Approximately 250 English and Spanish flyers were distributed to Waukena Joint Union School students to take home to their guardians, notifying them of the meeting that was going to take place. Translation services were provided at the meeting. Power Point presentation was held in English and Spanish.

## Map Assessments

County staff created writable maps of each community in Tulare County Being assessed for the plan. The maps were distributed at the community meetings. Residents were asked to draw areas they felt needed improvements in their communities. County staff translated the maps in English and Spanish and made user friendly by adding school locations on the maps. The maps were taken from Google Earth and formatted to have street locations making it easier for residents to identify their place of residence and future improvements.

Some of the Communities concerns were vehicle's drove past speed limit on road 173. A "slow down" blinker was suggested on highway 137 and Rd 28 when school bus has their flashing red lights on, vehicles still don't stop. A stop sign was highly recommended by the community.



The Waukena Community was very thankful to see they were included in the plan. A PowerPoint presentation was presented in English and Spanish and the County Chief Planner explained the Land use and Zoning for community of Waukena and assisted Waukena community members on the Map survey

# WAUKENA HAMLET PLAN

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After careful discussion at the Waukena SGC community input meeting, residents concluded that the following is a list of priority improvements Waukena needs:

1. Waukena School Improvements
2. School Traffic Safety
3. Waukena Schools
4. Road Conditions
5. Public Services

## **Waukena School Improvements**

The residents expressed concern for the condition of the Waukena School and said that repairing the school is the priority improvement need in town. Waukena School is a small K-8 school district with approximately 260 students, residents said that the school desperately needs repairs and modernization. Additionally, the building has an asbestos problem. The community is looking for funds to help the school because the district lacks the resources and funding opportunities to cover the cost of repairs. The residents in attendance agreed that the safety and condition of the school for their kids is a priority.

## **School Traffic Safety**

Parents expressed concern for the speed of traffic that travels in front of the school house. Residents are requesting assistance to slow down the traffic, whether it be a speed bump, flashing lights or caution signs warning drivers of the school zone and children crossing, parents are concerned for the safety of their students.

## **Waukena Schools**

The Waukena School District water well has continuously exceeded the MCL for Nitrates, the district has had to provide bottled water to students for many years. Currently they are receiving grant funds to cover the costs of the bottle water through a grant for 36 months grant.

## **Road Conditions**

Residents expressed a need for improvement to Waukena residential roads, the main road gets repaired but the other roads are over looked. The residents are requesting repairs to the following streets:

- Road 28
- Road 36
- Road 42
- Road 32
- Avenue 176
- Avenue 192

## **Public Services**

- Sheriff – Waukena is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The nearest Sheriff sub-station is in Pixley.
- Fire –Tulare County Fire
- Schools –Waukena School District (K-8)
- Libraries – The nearest Library is Corcoran
- Parks – The nearest Park is in Corcoran

# WAUKENA HAMLET PLAN

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## **Community Resource Center**

The community lacks many resources and needs a place where residents can go to access information and county wide resources. People in Waukena expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service Waukena residents. This could also be the place where residents access a reliable internet connection.

Please join us for the Community Safe Routes to School meeting. The meeting will dive deeper into the goals of the Safe Routes to School plan. We encourage all the members of the community to attend. We seek and welcome your input!

**Highlights:** What is Safe Routes to School, the need for the safe routes to school plan, identification of project areas, discussion of future funding and more!

**Date: Thursday, April 20, 2017**

**Time: 5:00p.m.**

**Location: Waukena Joint Union – Cafeteria  
19113 Road 28 Tulare, CA 93274**

For more information call: Rosa Sanchez at 559-624-7144



Por favor, acompáñenos a la reunión comunitaria para informarse acerca del programa "Safe Routes to School" (Rutas Seguras a la Escuela). La reunión se enfocará en los objetivos y metas que tiene el plan "Safe Routes to School." Todos los miembros de la comunidad están invitados a asistir. ¡Agradecemos su ayuda y opinión!

**Aspectos destacados:** ¿Que es "Safe Routes to School"?, la necesidad de rutas seguras a la escuela, la identificación de áreas de proyectos, un análisis de finanzas futuras y mucho más!

**Fecha: Jueves, 6 de Abril del 2017**

**Hora: 5:00p.m.**

**Localidad: Waukena Joint Union – Cafetería  
19113 Road 28 Tulare, CA 93274**

Para más información llame a: Rosa Sanchez al 559-624-7144

# WAUKENA HAMLET PLAN

## WAUKENA HAMLET TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Waukena.

#### Population

In 2010, the Population in Waukena was 108 (see Table 2).

Table 2 - Population						
	California	%	Tulare County	%	Waukena	%
Male	19,087,135	49.7%	227,426	50.1%	60	60%
Female	19,334,329	50.3%	226,607	49.9%	48	44.4%
Total	38,421,464		454,033		108	

2010 Census

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns is how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Waukena's population during the Planning Period (see Table 3).

Table 3 - Projected Annual Growth Rate		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7



# WAUKENA HAMLET PLAN

## Median Age

Waukena's population's median age is younger than the median age of both Tulare County and the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Waukena	20.4

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 56.5% of Waukena's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 1.0% was two races or more (see Table 4). Approximately 42.4% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues, which may affect their community (see Table 5).

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Waukena	%
Total	38,421,464		454,033		191	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	81	42.4%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	108	56.5%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	2	1.0%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Waukena

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production.

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Waukena was about 1.6% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 1.6% includes only the employable labor force (that is, not every person of the population), results in about 2 unemployed people of Waukena's person labor force of 56 (see Table 6).

# WAUKENA HAMLET PLAN

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Table 6 - Employment Status			
Employment Status	California	Tulare County	Waukena
Population 16 years & over	30,312,429	325,404	122
In labor force	19,269,449	194,420	56
Civilian labor force	19,137,441	194,102	56
Employed	17,246,360	170,780	54
Unemployed	1,891,081	23,322	2
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	66

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Median income in Waukena was lower than the State of California and the County of Tulare. Waukena's median household income was \$36,750, compared to \$61,818 for the State of California and \$42,031 for Tulare County (see Table 7).

Table 7 - 2011-2015 American Community Survey Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Waukena CDP	\$36,750	\$45,750	\$16,062

## Severely Disadvantaged Community

Waukena is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, Waukena's median household income was \$36,750, whereas the State of California's median household income was \$61,818. Waukena's median household income was 59% of the State of California's median household income, therefore it is considered a severely disadvantaged community.

# WAUKENA HAMLET PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, to allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes) (see Tables 8 and 9).

Housing Tenure Table 8 - Housing Tenure			
	California	Tulare County	Waukena
Occupied housing units	12,717,801	133,570	52
Owner-occupied	6,909,176	75,685	18
Renter-occupied	5,808,625	57,885	34

2011-2015 American Community Survey 5-Year Estimates

Table 9 - Housing Tenure (%)			
	California	Tulare County	Waukena
Owner-occupied	54.3%	56.7%	34.6%
Renter-occupied	45.7%	43.3%	65.4%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in; without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Waukena	3	18	5	29	5	29	3	18	1	6	17

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Waukena indicated that approximately 18% of the housing units were sound. Approximately 76% of Waukena's housing units were deteriorated and 6% were dilapidated (see Table 10).

### Age of Structures

According to the U.S. Census, the 2011 – 2015 Community Survey noted that most of the housing structures in Waukena were built between 1950 and 2009, (see Table 11). In 2015, the total number of housing units in Tulare County were 144,792.

# WAUKENA HAMLET PLAN

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Waukena
Total housing units	13,845,790	144,792	59
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	11
Built 1990 to 1999	1,495,571	21,767	12
Built 1980 to 1989	2,117,819	22,733	3
Built 1970 to 1979	2,503,688	27,111	2
Built 1960 to 1969	1,871,029	15,500	0
Built 1950 to 1959	1,907,512	13,694	23
Built 1940 to 1949	865,607	7,494	2
Built 1939 or earlier	1,298,438	8,186	6

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015, the average renter occupied household size in Waukena was 3.36 persons per household (see Table 12). The average owner occupied household size is 3.41.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
Waukena	4.33	3.32

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition.

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Waukena	0%	0%

2011-2015 American Community Survey 5-Year Estimates

In 2015, the rental vacancy rate in Waukena was 0%, which was lower than Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# WAUKENA HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

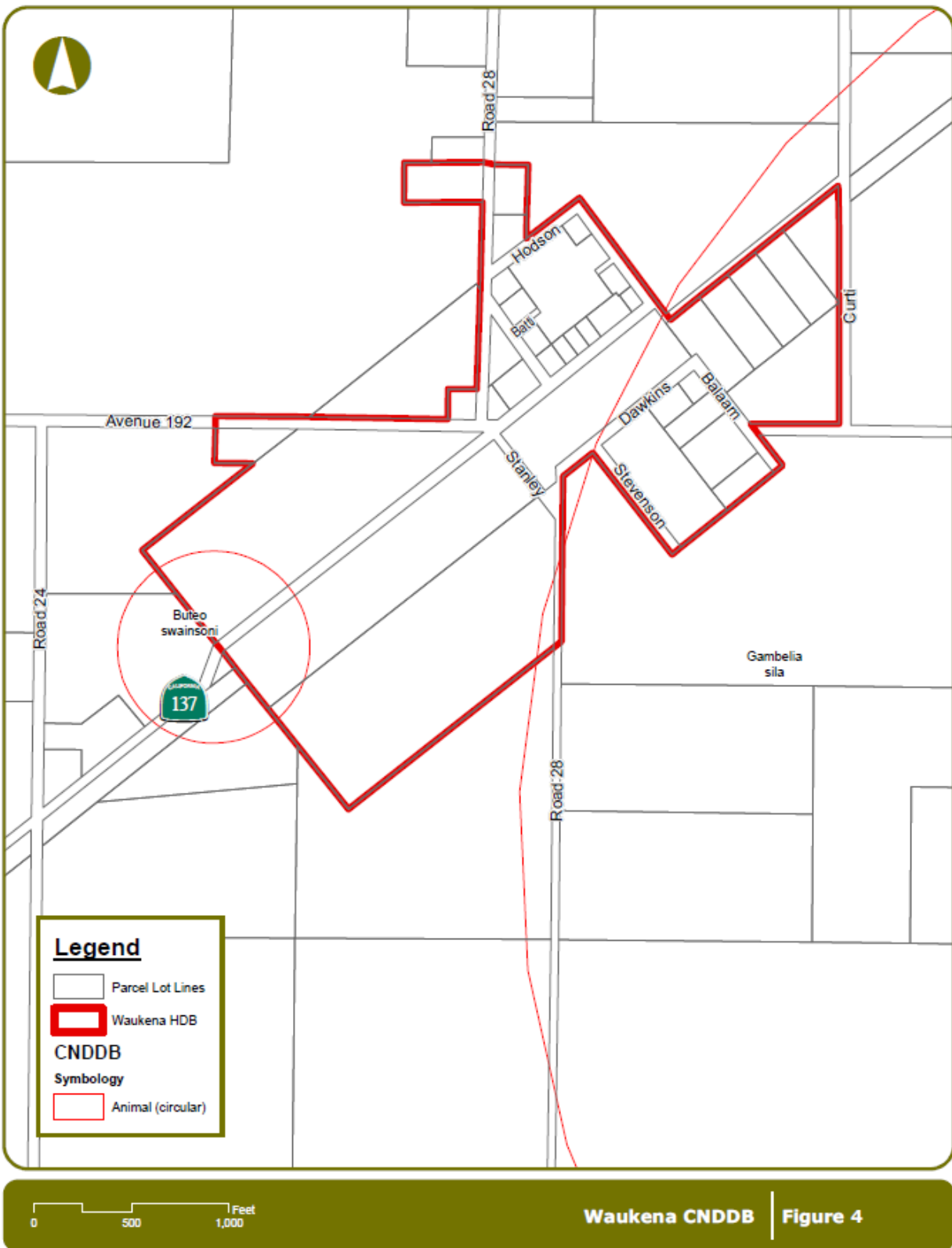
A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016, (**see Figure 4**) indicates there are special status species within the Waukena Quadrant Species List (which includes the Waukena Planning Study Area) consisting of three animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Swainson's hawk (*Buteo swainsoni*, state threatened); and Blunt-nosed leopard lizard (*Gambelia sila*, federal endangered). The California Department of Fish and Wildlife (CDFW)

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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> accessed November 1, 2016

# WAUKENA HAMLET PLAN

Figure 4 - CNDDDB Map



# WAUKENA HAMLET PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Waukena are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest).

According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Waukena] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the Soil Survey of Tulare County, California, Western Part (2003), prepared by the United States Department of Agriculture (USDA), Soil Conservation Service, (**see Figure 5**), the following soil type is located in Waukena:

**Colpien loam, 0 to 2 percent slopes**, is very deep and well drained, medium or high runoff, some areas are ponded; very slow permeability. The soil is suitable for livestock grazing. Vegetation is scattered saltbush with annual grasses and forbs.

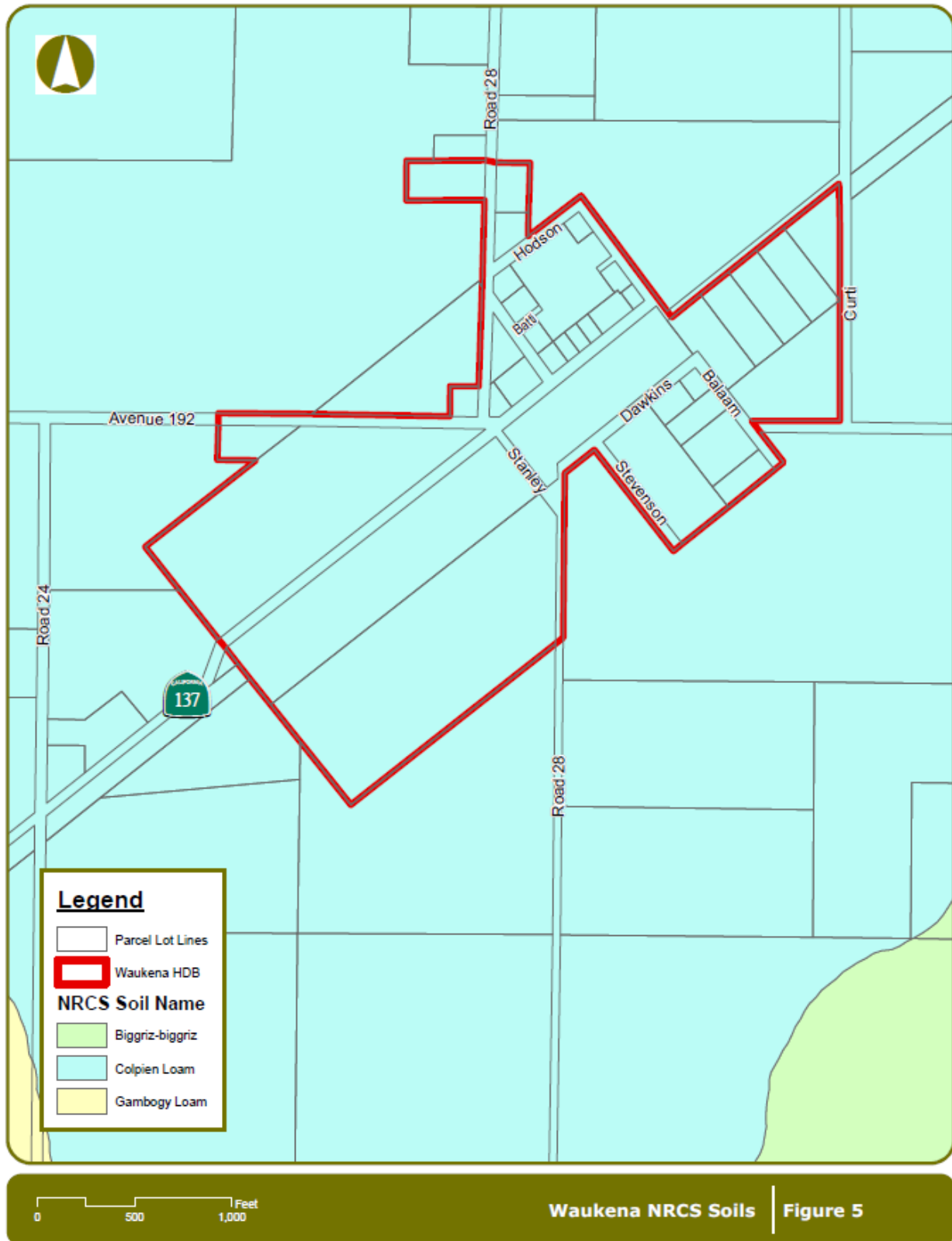
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# WAUKENA HAMLET PLAN

Figure 5 - Soils Map





# WAUKENA HAMLET PLAN

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## Physical Conditions

### Air Quality

The Waukena Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Waukena is located near the southwestern end of the Valley with prevailing winds from the northwest, it is susceptible to the accumulation of adversely modified

# WAUKENA HAMLET PLAN

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air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Waukena include agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C1225E, most of Waukena is in flood zone A. The 100 year flood zone meanders along the southern portion and the northeast portion of Waukena (**see Figure 6**). The expected source of this flooding hazard is Cameron Creek and its distributary channels. FEMA requires development in Flood Zone A to be constructed so that a building's ground floor elevation is above the flood contour line existing in the flood area. In addition, there are portions of Waukena, within the FEMA 500 year flood zone.

"Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA)." "Floodplain" or "flood-prone area" means any land area susceptible to being inundated by water from any source. "Base Flood" is the flood having a one percent chance of being equaled or exceeded in any given year. "One-hundred-year flood" or "100 year flood" has the same meaning as "Base Flood." "Special Flood Hazard Area" is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. "Floodway" means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Waukena there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in Special Flood Hazard Areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Future flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood->

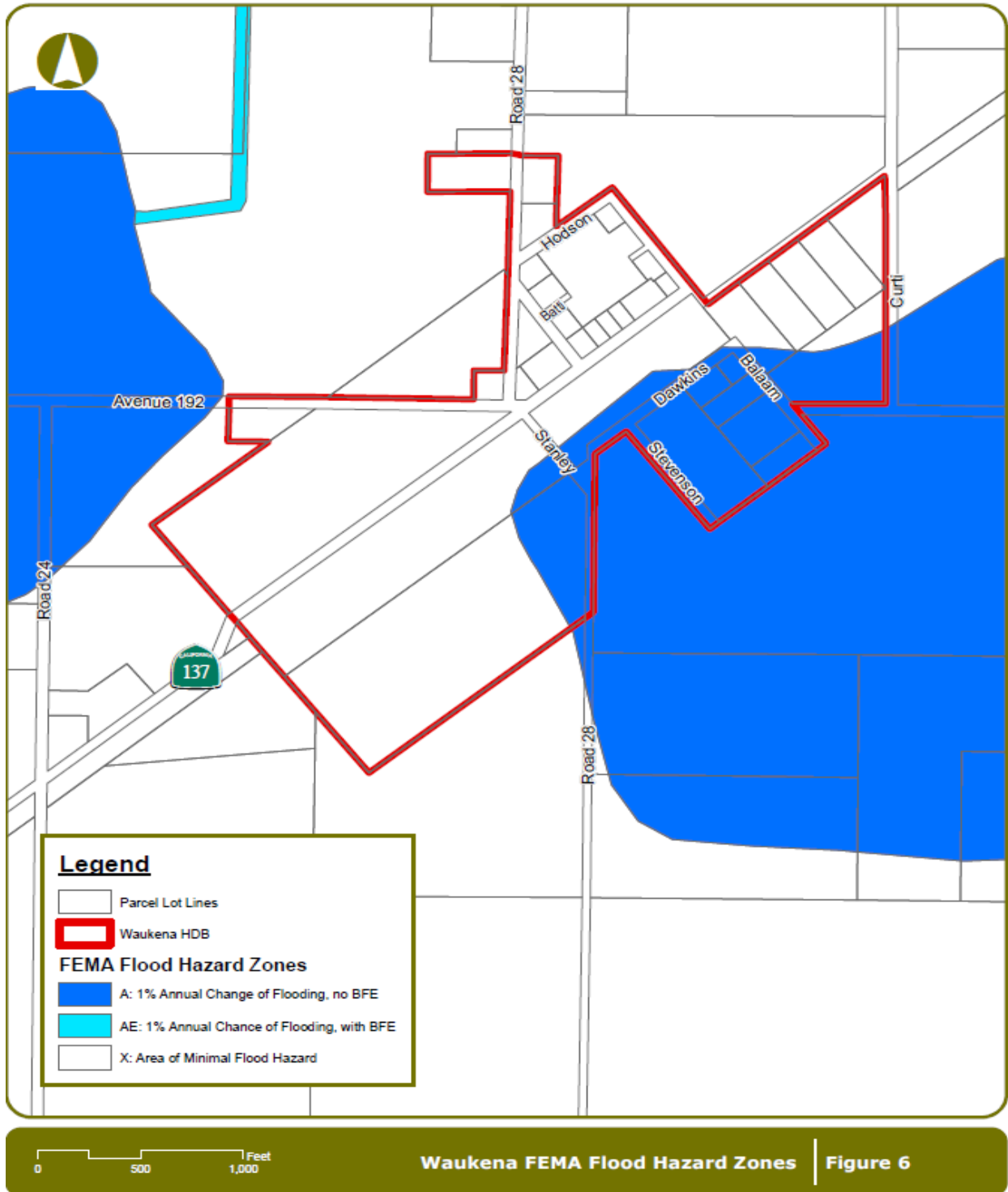
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<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C1225E, Panel Number 1225 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

# WAUKENA HAMLET PLAN

[hazard-information/flood-control-information/](http://hazard-information/flood-control-information/). On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

Figure 6 - FEMA Flood Waukena



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## Noise

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. **Table 14** summarizes the daily traffic volumes along State Route 137 from the Kings County Line to Road 68. Together, these noise sources place a portion of Waukena's urbanized areas within the 60 dB Ldn noise contour. The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise.<sup>7</sup>

Table 14 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
State Route 137 from King County Line to Road 68	3,350	38	81	175	376

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks). The prioritize concerns of the Community include:

### Natural Gas/Electricity

PG&E provides service to Waukena.

### Water

Domestic water service in Waukena is provided by individual wells.

### Sewer

There is no community wide sewer system in Waukena. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. No public sewer facilities are available in Waukena, each land owner relies on septic systems for wastewater.

<sup>7</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

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## Storm Water Drainage

Waukena does not currently have a storm drainage system. “A storm drainage system is designed to drain excess rain and groundwater from (roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.”<sup>8</sup>

## Solid Waste

Solid waste disposal services for the Community of Waukena is provided by Tule Trash, a private company. Solid waste generated in Waukena can be disposed at Road 80 Landfill, located at 8614 Avenue 328 in Visalia, California.

## Roads

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting

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<sup>8</sup> Action Program 9, Tulare County 2015 Housing Element

# WAUKENA HAMLET PLAN

- rock beneath the roadway, and placing and compacting hot mix asphalt
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. “It is commonly used as patching material and on lower volume service roads”<sup>9</sup>

**Table 15** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 8** graphically displays this information on a map.

**Table 15 - Roads in Need of Major and Medium Repair**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Balaam Drive	Dawkins Drive to Avenue 192	GRX
2	Dawkins Drive	Stanley Drive to Balaam Drive	CHIP
3	Road 28	Avenue 192 to Stevenson Drive	GRX

OLAY – overlay resurfacing operation  
CHIP – chip seal  
GRX – grind and remix

ACST – asphalt reconstruction  
RCST – cold mix reconstruction

(Source: County of Tulare Public Works, 2012)

## Street Lights

Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian accidents by increasing the awareness of drivers relative to pedestrians.

**Table 16** identifies the location of existing street lights that are maintained by Tulare County, in Waukena, as well as their specifications. Figure 6 also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type “W” represents a wood post for which the light is commonly shared with a Utility provider. Similarly, “M” represents metal and “C” represents concrete.”<sup>10</sup>

**Table 16 - Specifications of Existing Street Lights**

No.	East-West	North-South	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	Dawkins	Balaam Drive	SW Corner	1486	9500	W	N	PG&E

(Source: Tulare County Public Works, March 2013)

**Figure 7 - Waukena Inventory of Roadway Facilities**

<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element



**Inventory of Roadway Facilities in Waukena\***

Map showing the Inventory of Roadway Facilities in Waukena. The map displays a central area outlined in red, representing the HDB. Within this area, various streets are labeled, including State Route 137, Avenue 192, Road 24, Road 28, Road 32, and several local streets like Hotson, Banti, Stevenson, Dawkins, and Curti. A legend in the bottom right corner identifies symbols for ADA Ramps (blue square), Street Lights (orange circle), Streets (medium repair) (yellow line), Streets (major repair) (green line), Sidewalks (purple line), Parcels (white area), and HDB (red outline). An inset map in the bottom right corner shows the location of Waukena within a larger regional context, marked with a red star.

**Legend**

- ADA Ramps
- Street Lights
- Streets (medium repair)
- Streets (major repair)
- Sidewalks
- Parcels
- HDB

\*Roads, Sidewalks, ADA Curbs, Street Lights

# WAUKENA HAMLET PLAN

## Sidewalks

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

The County and VRPA Technologies surveyed existing sidewalks within the Community. **Table 17** identifies the location of existing sidewalks in Waukena. **Figure 8** also displays this information graphically. The sidewalks represented in **Table 17** and **Figure 8** do not distinguish between ADA compliant sidewalks and noncompliant sidewalks.

**Table 17 - Existing Sidewalks**

Location of Existing Sidewalks			
No.	Roadway	Limits	Location
1	Harmon Road	Stanley Drive to 300' south	West side
2	Stanley Drive	Dawkins Drive to Harmon Road	South side

(Source: County of Tulare Public Works and VRPA Technologies, February 2014)

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there is one ADA compliant curb ramp located within Waukena and is listed in **Table 18** and displayed in **Figure 8**.<sup>11</sup>

**Table 18 - Location of Existing ADA Ramps**

Location of Existing ADA Ramps		
East-West Roadway	North-South Roadway	Location
South of Stanley Drive	Harmon Road	West Side

(Source: County of Tulare Public Works, August 2013)

<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element



# WAUKENA HAMLET PLAN

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## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Public transit is currently not available in Tonyville. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Tonyville, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. In Tulare County, bicycle travel is not a major mode of transportation and bicycles are not seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. Currently no bike routes are within or near the community of Waukena.

## AMTRAK

The Hanford AMTRAK station, located approximately 10 miles to the northwest in Kings County, is the closest station to Waukena providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 15 miles northeast. Fresno Yosemite International Airport (FAT), approximately 70 miles northeast of Waukena is the principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 80 miles southeast of Waukena and offers direct flights to several destinations.

# WAUKENA HAMLET PLAN

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## PUBLIC SERVICES

### Sheriff

Police protection services are provided in Waukena by the Tulare County Sheriff's Department sub-station, located at 161 North Pine Street, in Pixley, approximately 20 miles southwest of Waukena. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

### Fire

Fire protection and emergency medical services are provided for Waukena by the Tulare County Fire Department. The community is served by Tulare County Fire Department Station #25, located at 2082 Foster Drive, in Tulare, California. Engine 25 is assigned to this location. The fire department personnel also provide emergency medical aid.

There are currently no fire hydrants in Waukena.

### Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Waukena Hamlet Plan Area is within the Waukena Joint School District. The District provides K-8 grade education at its Waukena School. High School students attend Tulare Joint Union High School District located ten (10) miles east.

### Library

"The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch."<sup>12</sup> Waukena does not have a public library; the nearest library is located in Corcoran, Kings County, California, approximately five (5) miles southwest of Waukena. Tulare Public Library is located approximately ten (10) miles to the northeast, in the City of Tulare,

### Parks

There are no County owned/operated public parks in Waukena. The closest public is located in Kings County, approximately five (5) miles southwest in the City of Corcoran. A number of parks are located in the City of Tulare and are operated by the City.

## THE WAUKENA HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

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<sup>12</sup> General Plan Background Report, page 7-96

# WAUKENA HAMLET PLAN

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“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>13</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>14</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing circulation and Transportation Conditions

Waukena is a small agricultural community, it is directly served by State Route 137. There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

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<sup>13</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>14</sup> Tulare County General Plan Background Report, page 5-7.

# WAUKENA HAMLET PLAN

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## County Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

## **GOALS, OBJECTIVES AND POLICIES**

This Section of the Waukena Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Waukena to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Based on the Waukena Hamlet Plan, and input received from Waukena citizens during the public outreach process, the goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Create a mix of uses with easy access to major roads**

Objective: Promote mixed use zoning and land uses.

#### Policies:

1. Locate high density residential uses near planned commercial areas.
2. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
3. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.

# WAUKENA HAMLET PLAN

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4. The County shall work with the Schools to provide safe routes to school.
5. The Teviston Legacy Plan should be reviewed every five years to determine if amendments are appropriate.
6. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Teviston Community Plan.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Waukena.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Waukena.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

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## Economic Base

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

#### Policies:

1. Encourage the Waukena to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

#### Policies:

1. Assure that commercial and industrial

developments are designed so that traffic will not impact upon residential areas.

2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## Environmental Quality and Public Safety

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Waukena citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

#### Objective: Protect Agricultural Lands:

Land within the respective Waukena, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the

# WAUKENA HAMLET PLAN

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reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.

1. Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Waukena.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Waukena.



# WAUKENA HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 119.3 acre Waukena Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 88 percent of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 108 by 119.3 urbanized acres (area within the HDB), which equals 0.9 persons per urbanized acre (see **Tables 16 and 17**) showing population projections). Projecting the population at a 1.3% growth rate adds 23 persons in year 2030. Multiplying the 23 persons by the 1/3 ratio suggests an additional 0.3 acres of land will be required to accommodate development by the year 2030.

### Population Growth Forecast

**Table 19 - Waukena Population Projects**

Growth Rate	2015	2020	2030
0.013	108	115	131

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in Year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>15</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2015 the community had 52 dwelling units (including vacant dwellings) with a population of 108. At an annual growth rate of 1.3%, the projected housing units are 55 and 63 in years 2020 and 2030, respectively, and projected population is 115 and 131 in years 2020 and 2030, respectively.

Table 20 - Population Projects		
Year	Population	Growth (%)
2015	108	0.013
2016	109	0.013
2017	111	0.013
2018	112	0.013
2019	114	0.013
2020	115	0.013
2021	117	0.013
2022	118	0.013
2023	120	0.013
2024	121	0.013
2025	123	0.013
2026	124	0.013
2027	126	0.013
2028	128	0.013
2029	129	0.013
2030	131	0.013

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

<sup>15</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).



# WAUKENA HAMLET PLAN

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## Infrastructure

In order for more development to occur, water service level needs to be expanded, for which grant funding will be required.

## Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require a discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

## Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

## Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are in the City of Tulare.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 21**) and Proposed Zoning Plan map (see **Table 22**) have been updated.

<b>Table 21 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	104.04
Right-of Ways	14.9
Waukena HDB	119.3

<b>Table 22 - Proposed Zoning</b>	
Proposed Zones	Acres
A-40	16.3
C-2-MU	30.5
P-O	24.0
R-A-12.5	33.7
Right-of Ways	14.9
Waukena HDB	119.3

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## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the Waukena Legacy Plan. The following components comprise the Waukena Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Waukena Legacy Plan, there are a variety of changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

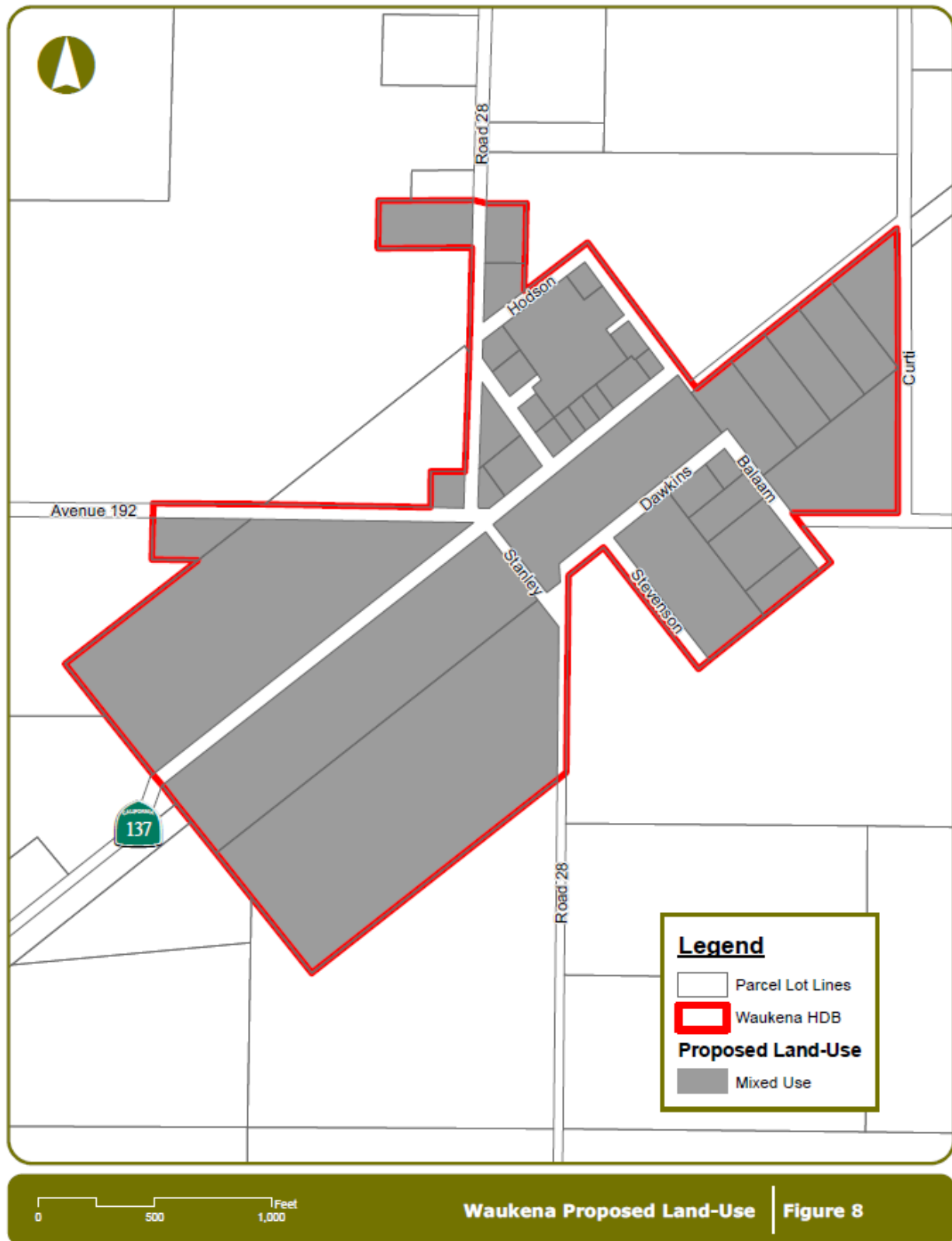
This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Waukena.

### Zoning and Land Use Maps Updated

The current Zoning Map (**see Figure 9**) for Waukena will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 8**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 10**) Waukena Proposed Zoning District.

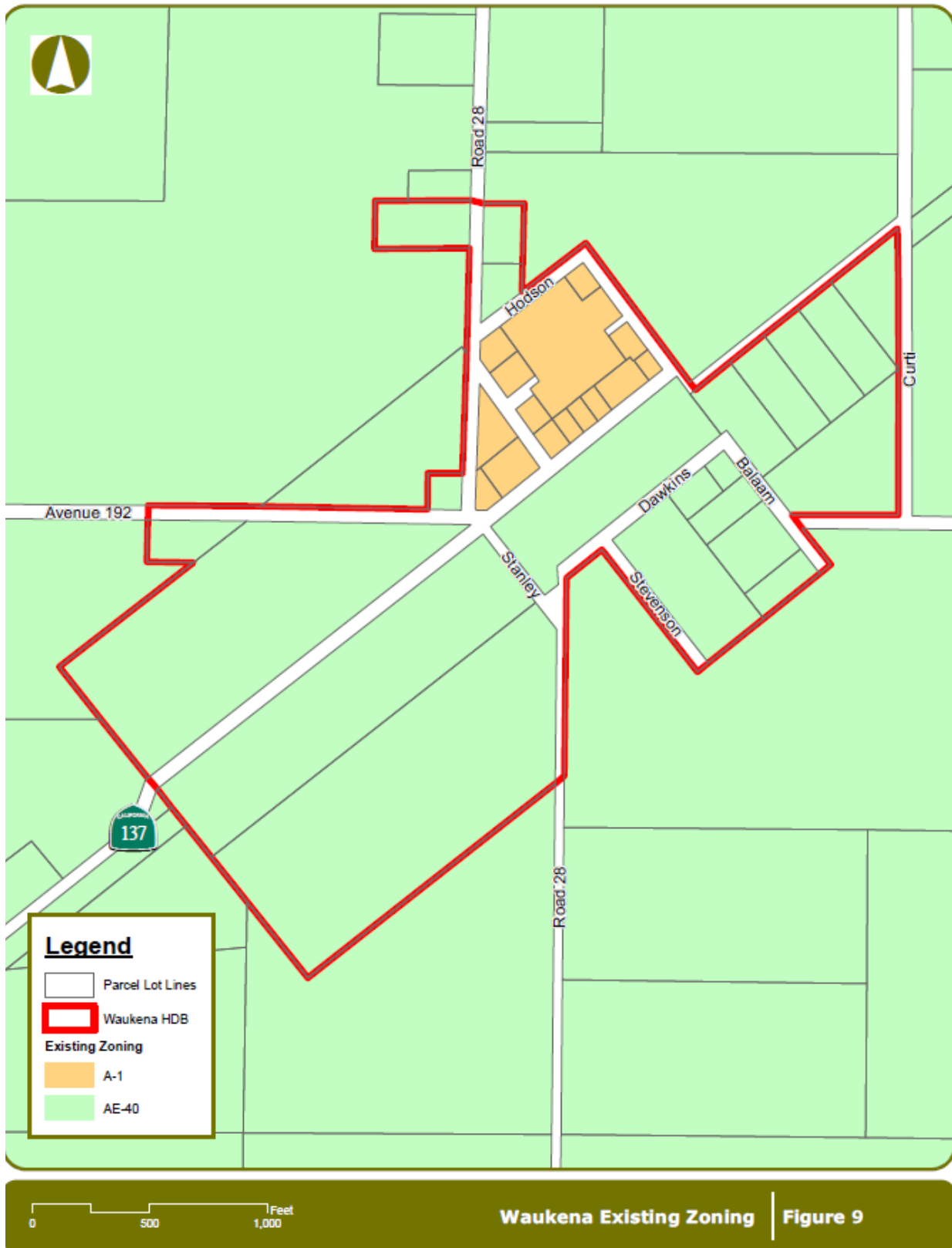
# WAUKENA HAMLET PLAN

Figure 8 - Proposed Land Use Plan



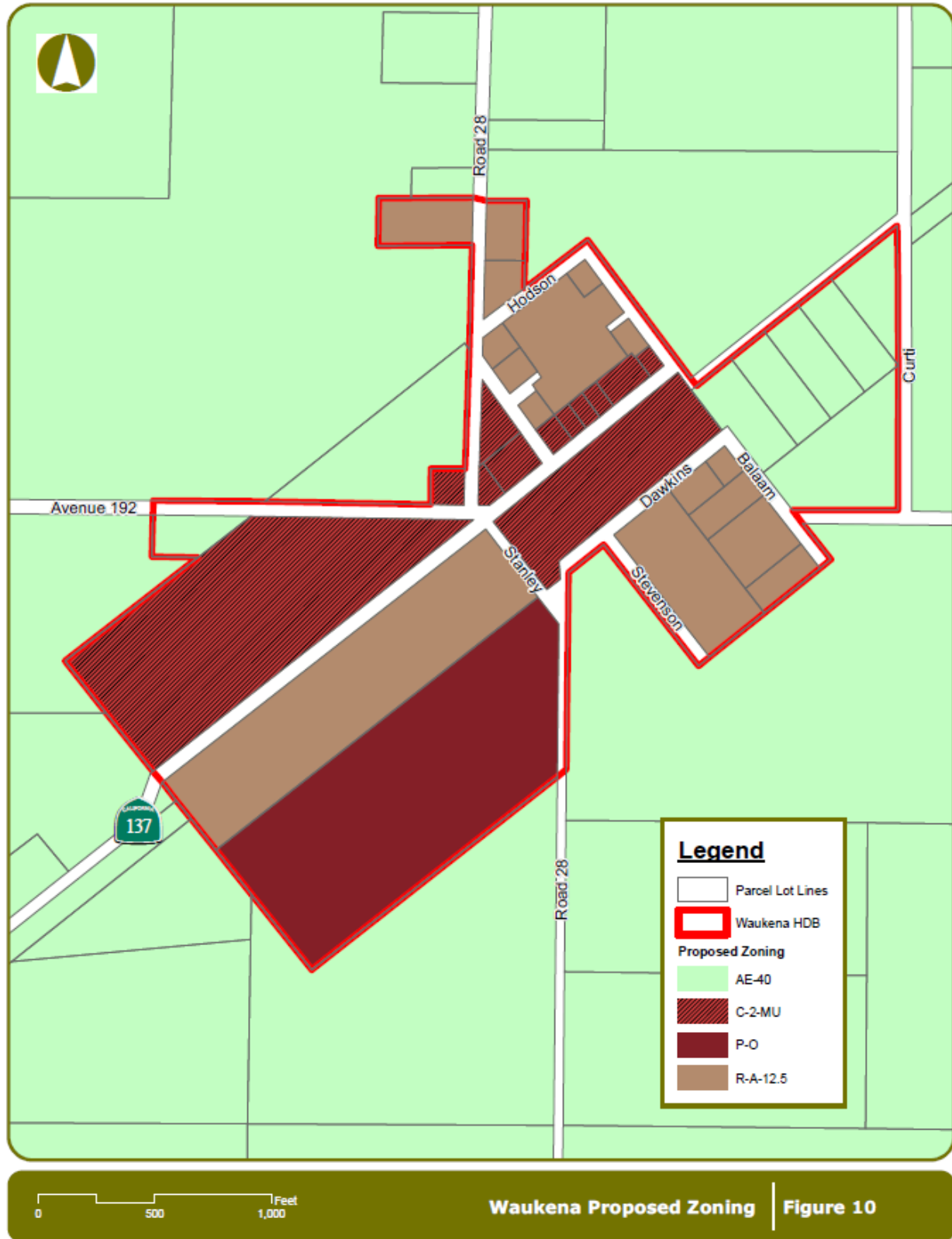
# WAUKENA HAMLET PLAN

Figure 9 - Existing Zoning Plan



# WAUKENA HAMLET PLAN

Figure 10 - Proposed Zoning Changes



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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# WAUKENA HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1



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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-	R-3, C-1, C-2, C-3,

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	1, R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles	C-2, C-3, M-1, M-2	

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of an airport).		
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1

# WAUKENA HAMLET PLAN

Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# WAUKENA HAMLET PLAN

## A-2 *Mixed Use Overlay District (Zone Change Text)*

The following regulations shall apply in the community of Waukena, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Waukena.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Waukena.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# WAUKENA HAMLET PLAN

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

# WAUKENA HAMLET PLAN

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Waukena. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.



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## **A-3     *Development Standards (Mixed Use Zoning District)***

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To promote Economic Development within the Waukena Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Waukena. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Waukena Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Waukena. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1     Entries to buildings should be individualized and clearly identifiable.

A-2     Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3     Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4     The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5     Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6     Each building should have a defined base, body, and cap segment

A-7     Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8     Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9     Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10    Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11    Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.



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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.



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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# WAUKENA HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.



B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:       None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.



The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT                     )  
TO SECTION 16.H OF ORDINANCE NO. 352            )  
TO ALLOW ADDITIONAL BY-RIGHT USES             )       RESOLUTION NO. 9369  
CONSISTENT WITH THE ADOPTED HAMLET            )  
PLANS, AS PROPOSED IN CHANGE                    )  
OF ZONE NO. PZC 17-039                            )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map



***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

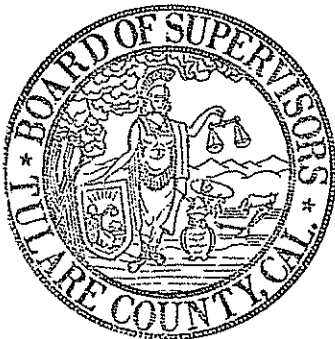
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017

## **West Goshen Hamlet Plan**

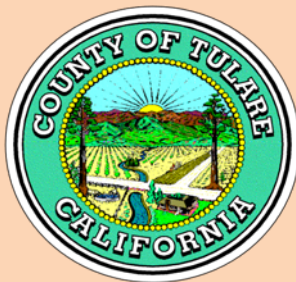
# WEST GOSHEN HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
Planning Processing Division



# WEST GOSHEN HAMLET PLAN

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# WEST GOSHEN HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# WEST GOSHEN HAMLET PLAN

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## WEST GOSHEN HAMLET PLAN

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# West Goshen Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

West Goshen Hamlet Plan: GPA 17-023  
Zoning District Map: PZC 17-023



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# WEST GOSHEN HAMLET PLAN

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# WEST GOSHEN HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# WEST GOSHEN HAMLET PLAN

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# WEST GOSHEN HAMLET PLAN

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## **County Administrative Office**

Michael C. Spata, CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director

Mike Washam, Associate Director

Aaron Bock, Chief Planner, Project Processing

Dave Bryant, Chief Planner, Special Projects

Susan Simon, Planner III

Velma Quiroz, Planning Tech II

Johnson Vang, Engineer I

Alexandra J. Vander Poel, Grant Specialist I

# WEST GOSHEN HAMLET PLAN

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# WEST GOSHEN HAMLET PLAN

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# WEST GOSHEN HAMLET PLAN

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for West Goshen is to develop a plan which can accurately reflect the needs and priorities of this community. West Goshen is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [water distribution piping, and storage tanks], curbs, gutters, and sidewalks) and to stimulate economic development within the community.

### Location

The community of West Goshen (**see Figure 1**) is located on the east side of the San Joaquin Valley and is a census-designated place located approximately 1.5 miles of the west of the community of Goshen along Ave. 308, just east of Kings County. It is bounded by Avenue 304 in the south, railroad tracks in the north, 1st Avenue in the west, and west of Markham Road in the east and encompasses 1.2 square miles of land. It is not directly served by any State Route. West Goshen is situated approximately 1.5 miles west of Highway 99 and 1-mile north of Highway 198 (**see Figure 2**). West Goshen is located in Section 22 and 23 Township 18 South, Range 23 East, MDB&M, and can be found within Goshen United States Geological Survey 7.5 minute topographic quadrangle. West Goshen is located at an elevation of 443 feet National Geodetic Vertical Datum. The coordinates of West Goshen are: Latitude: 36°20'57"5119 Longitude: 119°27'27".

### Planning Area

The West Goshen Hamlet Development Boundary (HDB) area consists of 477.2 acres (**see Figure 3**). The Land Uses within the HDB are described include agricultural activities (orchards and pasture), which currently occupy 92 percent of the 477.2 acres. Urban development, including urbanized uses such as residential development occupy 9.6 percent of the 477.2 acres. The remaining 5.7 percent are lands dedicated for Right-of-way (**see Table 1**).

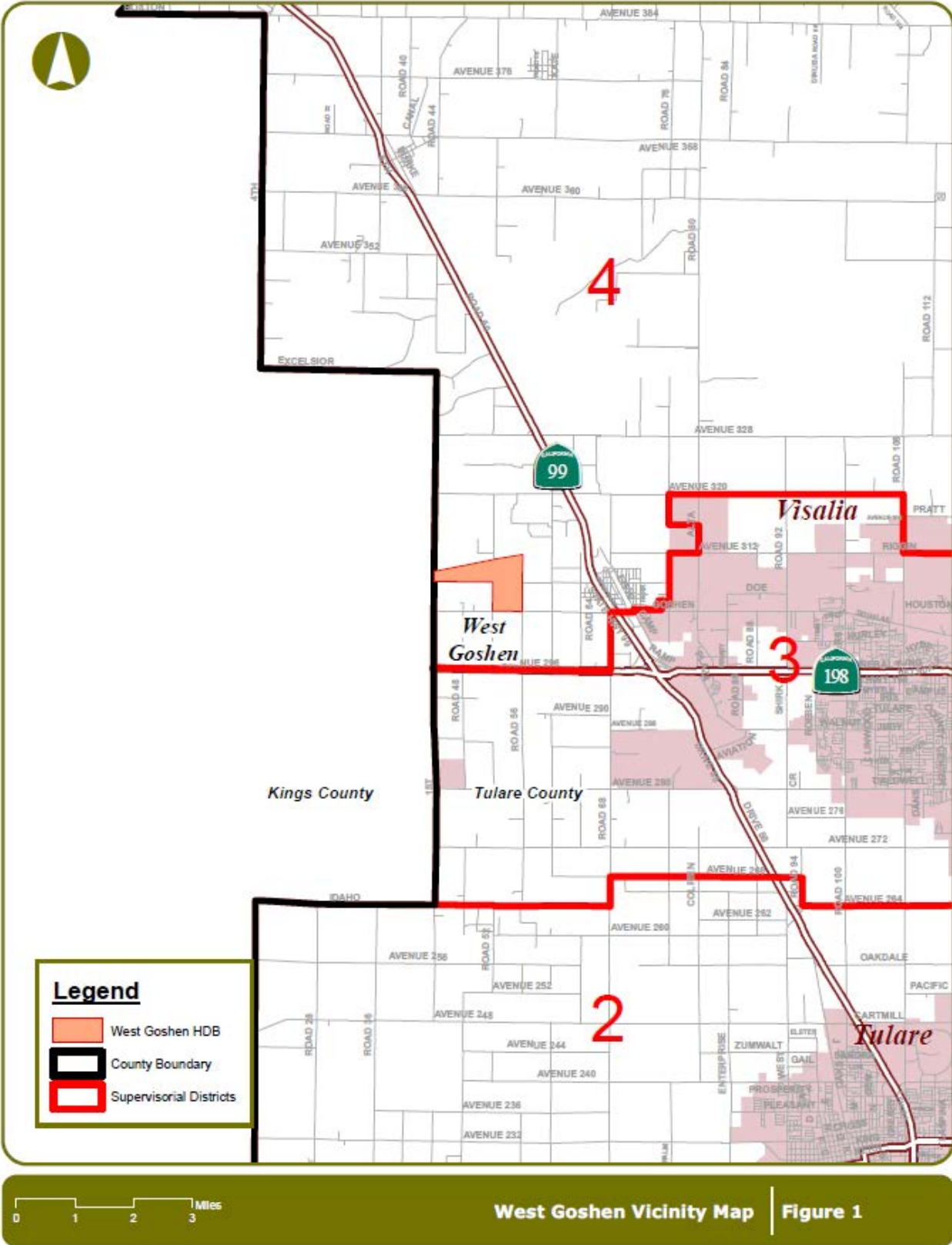
Table 1 - West Goshen Land Use	
Land Use	Acres
Agricultural	440.0
Residential	9.6
Rights-of-way	27.6
Total	477.2

Source: Tulare County GIS

No change is proposed to the Hamlet Development Boundary.

# WEST GOSHEN HAMLET PLAN

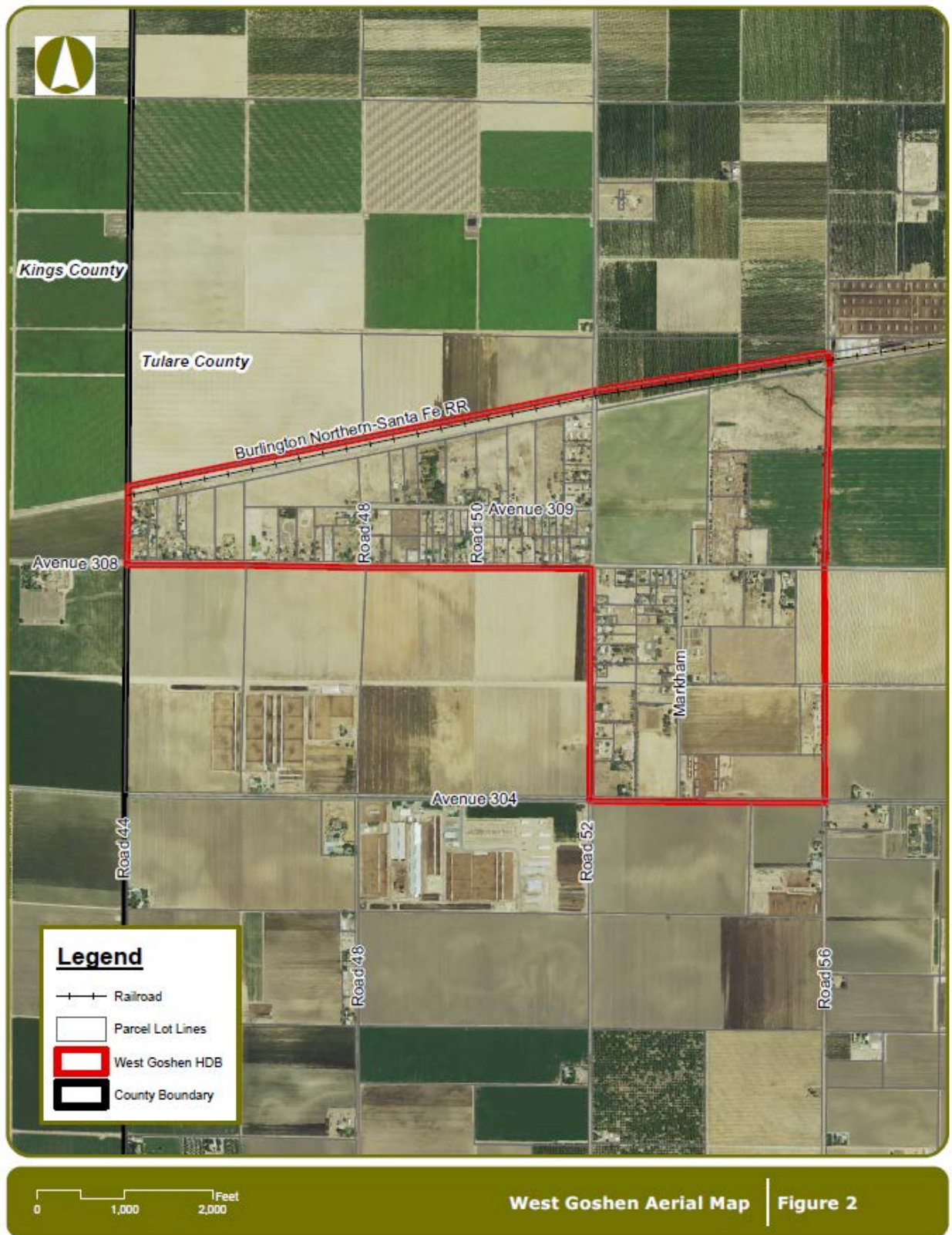
Figure 1 - Vicinity Map





# WEST GOSHEN HAMLET PLAN

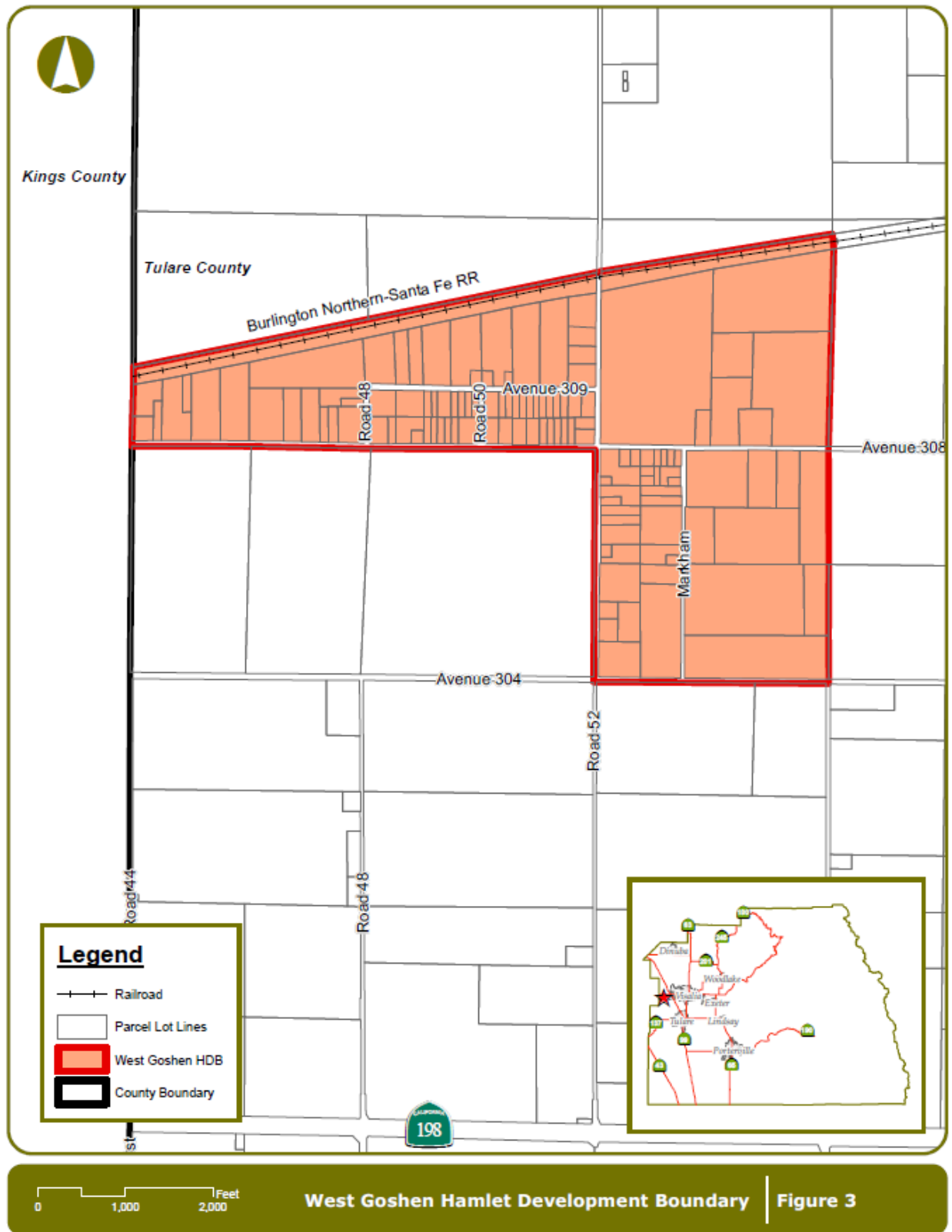
Figure 2 - West Goshen - Aerial





# WEST GOSHEN HAMLET PLAN

Figure 3 - West Goshen HDB



# WEST GOSHEN HAMLET PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Outreach and Invitation Process

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to invite and make local residents aware of the SGC project, Self Help Enterprises (SHE) went door to door throughout the community of West Goshen to distribute flyers at every homes, businesses, and schools. SHE emphasized with residents about the importance of participating in this process. SGC partnered with the Goshen School to send flyers home with every student. Meeting invitations were sent to the members of the West Goshen Mutual Water Company.

### Community Outreach Steps

Self-Help Enterprises (SHE) utilized various outreach methods to promote community participation and increase awareness of the SGC project:

1. An informational flyer was developed.
2. On September 13 and 14, 2015 SHE staff went door to door to distribute flyers to homes.
3. SHE staff partnered with members of the West Goshen Water Mutual Company to encourage community involvement. This group distributed fliers on September 15 and 16, 2015
4. SHE made phone calls to local residents.
5. Throughout the community outreach process, SHE staff explained the goals of the project and emphasized the importance benefits of participating in the SGC community meeting.
6. To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local corner store and all businesses.

### Community Meeting

The Strategic Growth Council held a community input meeting in W. Goshen on Thursday, September 17, 2015 at 6:00 pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Goshen school located at 6505 Ave 308 Visalia CA. More than 21 West Goshen residents attended the meeting. 20 surveys were collected.



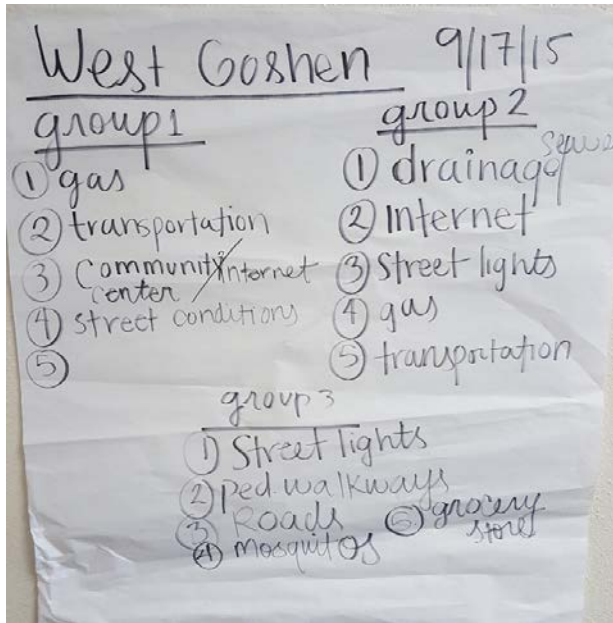
# WEST GOSHEN HAMLET PLAN

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## Community Survey

A detailed survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

A second meeting was held on November 21, 2016 in Goshen at the Elementary School. More than 16 West Goshen residents attended. Both SHE and Tulare County staff were present to discuss the results of the survey and to explain the Community Planning process.



1. Natural Gas
2. Sewer
3. Street Conditions
4. Street lights
5. Transportation/Bus Stops
6. Internet
7. Community Center

## **Natural Gas**

Some residents report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that the residents that do not have natural gas would like to have it but cannot afford the cost of extending gas lines to their homes. Residents are requesting assistance from Tulare County to access resources to help them cover the cost of extending gas lines to their homes.

# WEST GOSHEN HAMLET PLAN

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## **Water**

West Goshen Mutual Water Company, a small mutual made up of approximately 90 water customers is in the process of dissolving as they are now fully consolidated with Calwater. A few private wells in the area were also able to join in on the consolidation.

## **Sewer**

There is no community wide sewer system in West Goshen. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the West Goshen residents are in favor of creating a community wide sewer system.

## **Storm Water Drainage**

Residents report that the community does not have adequate storm water drainage. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season especially Road 308 and Road 309. The stagnant water attracts insects and mosquitos which adds another problem to the issue.

## **Road Conditions**

Roads conditions are a huge concern to the residents of W. Goshen. It is reported that road conditions are poor and most roads need work. The roads have potholes, cracks and bumps. This makes residents concerned about both driving conditions and walkability. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving. West Goshen roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not repaired. The following roads are a priority to the residents:

- Avenue 308
- Avenue 309
- \*Road 52
- Road 50
- Road 48
- Markham

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department seldom patrols West Goshen. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Sidewalks**

The community of West Goshen has too few sidewalks. Residents that were surveyed reported that the streets they live on do not have sidewalks. People report that this is a problem for kids and parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk in the mud and through big puddles of water. West Goshen traffic travels at a high rate of speed (see next section), and the absence of

# WEST GOSHEN HAMLET PLAN

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sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Traffic Rules Enforcement to Slow Traffic**

The area of West Goshen has a problem with speeding traffic. Residents expressed that many vehicles travel at an unsafe rate of speed and there are no speed limit signs to enforce the speed limit. Residents believe that by installing speed limit signs to clearly display the legal speed limit, this issue could be improved. A speed limit sign is especially needed on West Goshen's main road, Road 9. Along with speed limit signs, residents feel traffic law enforcement is needed. The California Highway Patrol and the Tulare County Sheriff's Department are rarely seen in West Goshen, the result of this is that people are rarely cited for violating traffic laws.

## **Internet Access**

Most families in West Goshen do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. Without access to the internet, they are unable to do things such as online job hunting, applying for resources, and or provide homework help to their kids.

## **Community Resource Center**

The community lacks resources and needs a facility where residents can access information and county services. People in West Goshen expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used for homework assistance, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service West Goshen residents. This could also be the place where residents access a reliable internet connection.

## **Community Resources**

Sheriff –West Goshen is in the jurisdiction of Tulare County Sheriff's Department (TCSD). The TCSD is responsible for patrolling the area of West Goshen.

Fire –Tulare County Fire

Schools – Goshen School is part of the Visalia Unified School District.

# WEST GOSHEN HAMLET PLAN

## WEST GOSHEN TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as West Goshen.

#### Population

In 2015, the population of West Goshen was 675 (see Table 2).

Table 2 - Population						
	California	%	Tulare County	%	West Goshen	%
Male	19,087,135	49.7%	227,426	50.1%	305	45.2%
Female	19,334,329	50.3%	226,607	49.9%	370	54.8%
Total	38,421,464		454,033		675	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project West Goshen's population during the Planning Period (see Table 3).

Table 3 - Projected Annual Growth Rates		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7



# WEST GOSHEN HAMLET PLAN

## Median Age

West Goshen's median age of 23.2 is younger than the median age of both Tulare County and of the State of California (see Table 4).

<b>Table 4 - Median Age</b>	
<b>Geography</b>	<b>Median Age (years)</b>
California	35.8
Tulare County	30.3
West Goshen	23.2

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 21.3% of West Goshen's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 0% was two races or more (see Table 5). Approximately 78.7% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

<b>Table 5 - Race &amp; Ethnicity</b>						
	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>West Goshen</b>	<b>%</b>
Total	38,421,464		454,033		675	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	531	78.7%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	144	21.3%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in West Goshen

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see Table 6).

# WEST GOSHEN HAMLET PLAN

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for West Goshen was about 3.6% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 3.6% includes only the employable labor force (that is, not every person of the population) results in about 16 unemployed persons of West Goshen's person labor force of 275.

<b>Table 6 - Employment Status</b>			
<b>Employment Status</b>	<b>California</b>	<b>Tulare County</b>	<b>West Goshen</b>
Population 16 years & over	30,312,429	325,404	441
In labor force	19,269,449	194,420	275
Civilian labor force	19,137,441	194,102	275
Employed	17,246,360	170,780	259
Unemployed	1,891,081	23,322	16
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	166

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

West Goshen's median household income was \$20,700, compared to \$42,708 for Tulare County and \$61,818 for the State of California (see Table 7).

<b>Table 7 - 2011-2015 American Community Survey: Income</b>			
<b>Geography</b>	<b>Median household income (dollars)</b>	<b>Median family income (dollars)</b>	<b>Per capita income (dollars)</b>
<b>California</b>	\$61,818	\$70,720	\$30,318
<b>Tulare County</b>	\$42,031	\$44,814	\$17,876
<b>West Goshen CDP</b>	\$20,700	\$14,756	\$20,808

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

In 2015, West Goshen's median household income was \$20,700, whereas the State of California's median household income was \$61,818. West Goshen's median household income was 33.4% of the State of California's median household income, and therefore it is considered a severely disadvantaged community.



# WEST GOSHEN HAMLET PLAN

## HOUSING CHARACTERISTICS

### Tenure

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the percentage of housing units that were owner occupied was 22.7% in West Goshen, 56.7% in Tulare County, and 54.3% in the State of California (see Tables 8 and 9).

Table 9 - Housing Tenure			
	California	Tulare County	West Goshen
Occupied housing units	12,717,801	133,570	172
Owner-occupied	6,909,176	75,685	39
Renter-occupied	5,808,625	57,885	133

2011-2015 American Community Survey 5-Year Estimates

Table 8 - Housing Tenure (%)			
	California	Tulare County	West Goshen
Owner-occupied	54.3%	56.7%	22.7%
Renter-occupied	45.7%	43.3%	77.3%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
West Goshen	10	23%	11	26%	22	51%	0	0%	0	0%	43

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in West Goshen indicated that approximately 23% of the housing units were sound. Approximately 77% of West Goshen's housing units were deteriorated and 0% were dilapidated (see Table 10).

# WEST GOSHEN HAMLET PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009 (see Table 11). The total number of housing units in West Goshen is 179.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	West Goshen
Total housing units	13,845,790	144,792	179
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	8
Built 2000 to 2009	1,646,490	25,997	25
Built 1990 to 1999	1,495,571	21,767	0
Built 1980 to 1989	2,117,819	22,733	50
Built 1970 to 1979	2,503,688	27,111	22
Built 1960 to 1969	1,871,029	15,500	0
Built 1950 to 1959	1,907,512	13,694	57
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	17

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in West Goshen was 3.83 persons per household (see Table 12). The average owner occupied household size is 4.26.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	3.00	2.91
Tulare County	3.24	3.50
West Goshen	4.26	3.83

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in West Goshen was 0%, which was lower than both Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate was 0%, which was lower than Tulare County at 1.7% and the State of California at 1.4%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
West Goshen	0%	0%

2011-2015 American Community Survey 5-Year Estimates

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# WEST GOSHEN HAMLET PLAN

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## ENVIRONMENTAL CONDITIONS

### Wildlife:<sup>3</sup>

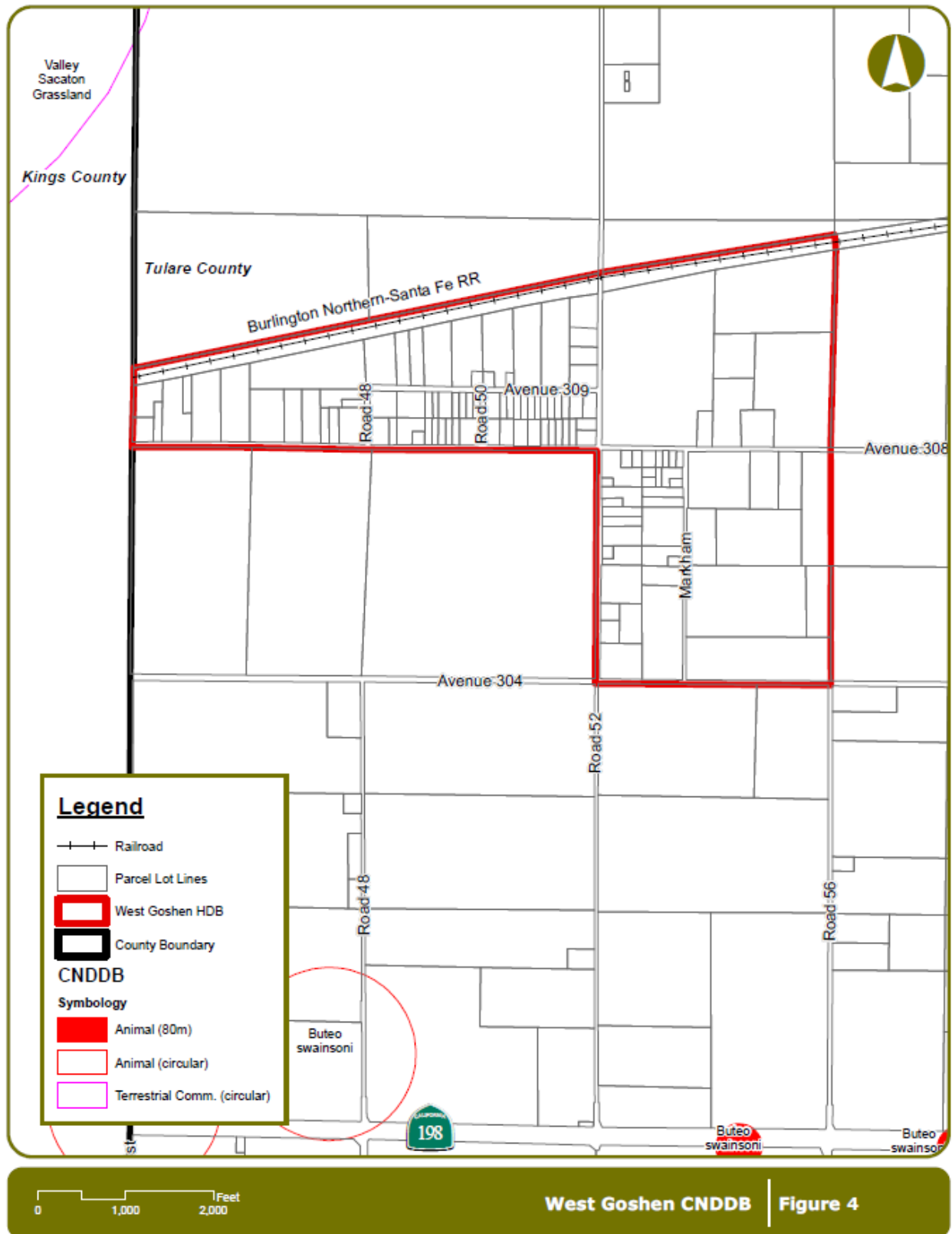
A California Natural Diversity Database (CNDDDB) search conducted on November 3, 2016, (**see Figure 4**) indicates there are special status species within the Goshen Quadrant Species List (which includes the West Goshen Planning Study Area) consisting of three animals species and one plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Swainson's hawk (*Buteo swainsoni*, state endangered); vernal pool fairy shrimp (*Branchinecta lynchi*, federal threatened), Valley Sacaton Grassland. The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

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<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> accessed November 3, 2016

# WEST GOSHEN HAMLET PLAN

Figure 4 - West Goshen CNDDb Map



# WEST GOSHEN HAMLET PLAN

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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect West Goshen are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [West Goshen] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in West Goshen (**see Figure 5**). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Hamlet Development Boundary (HDB). The following descriptions are provided for the above soil types:

**Colpien Loam, 0 to 2 percent slopes**, the parent material is alluvium derived from granitic rock sources. The soil is moderately well drained and the permeability is moderately slow. The available water capacity is very high and the shrink-swell potential is moderate. The soil is suitable for irrigated crops, building site development and farmstead dairies. For building site development the major limitation is rare flooding hazard as well as a high corrosivity to steel. The land capability classification: Class, 1, irrigated; subclass, 4c, non-irrigated MLRA: 17. This is considered prime farmland.

**Akers-Akers, saline-Sodic, complex, 0 to 2 percent slopes**, The parent material of the Akers soil is alluvium derived from granitic rock sources. The soil is very deep and well drained. The available water capacity is high and the shrink-swell potential is low.

For building site development, the soil is highly corrosive to steel and concrete. The land capability classification is: Akers-1, irrigated, and 4c, non-irrigated; Akers, saline-sodic -2s-6, irrigated, and 4s-6, non-irrigated. The areas not saline-sodic are considered prime farmland.

**Calgro-Galgro, saline-Sodic, complex, 0 to 2 percent slopes**, shrink swell potential low, erosion potential low to moderate.

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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# WEST GOSHEN HAMLET PLAN

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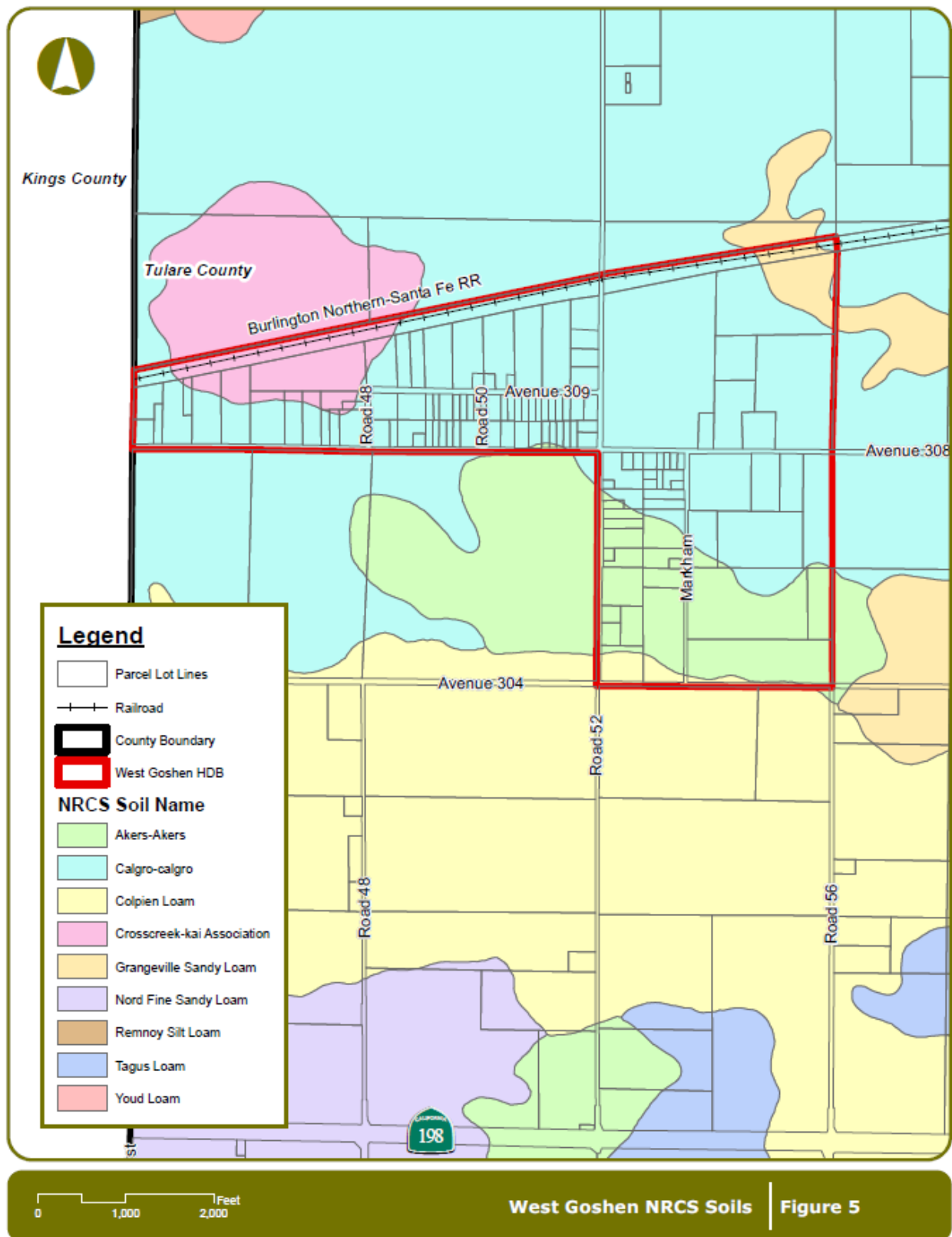
**Crosscreek-Kai association, 0 to 2 percent slopes,** is formed by the chemical and mechanical alteration of the Kai soil that originally formed in alluvium derived from granitic rock sources. The soil is well drained, and the permeability is moderately slow above the duripan due to sodicity and very slow in the duripan. The available water capacity is moderate to high and the shrink-swell potential is moderate. The parent material of Kai Soil is alluvium derived mainly from granitic rock sources. The soil is moderately well drained and the permeability is moderately slow above the duripan and very slow in the duripan. The available water capacity is low to moderate and the shrink-swell potential is moderate.

Building site development management factors include very slow permeability of the duripan, moderate to moderately rapid permeability below the duripan, high corrosivity to steel in both soils and high corrosivity to concrete in the Crosscreek soil. The land capability classification: Crosscreek-unit, 2s-6, irrigated; subclass, 6s, non-irrigated; Kai- unit, 3s-6 irrigated; subclass, 7s, non- irrigated, MLRA17. This is not considered to be prime farmland.

**Grangeville sandy loam drained, 0 to 2 percent slopes,** prime farmland if irrigated shrink swell potential low, erosion potential moderate

# WEST GOSHEN HAMLET PLAN

Figure 5 - Soils Map



# WEST GOSHEN HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The West Goshen Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because West Goshen is located near the west side of the Valley with prevailing winds from the northwest, it is susceptible to the accumulation of adversely modified air,



# WEST GOSHEN HAMLET PLAN

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particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of West Goshen include agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C905E, A substantial portion of the planning area in West Goshen is subject to Flood Zone A, which meanders through West Goshen (**see Figure 6**). The majority of the Community is within flood zone X. The expected source of this flooding hazard is Kaweah River and its distributary channels, located about five miles south of West Goshen. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each Community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use Planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”<sup>7</sup> Although some areas of Tulare County have experienced major flooding along its major rivers, the West Goshen Plan Area has not. There are portions outside of West Goshen, that are within and adjacent to the FEMA 500 and 100 year flood zones. According to the Tulare County General Plan Update, substantial flooding could occur in Tulare County if the two (2) major dams were to experience failure.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within West Goshen there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new

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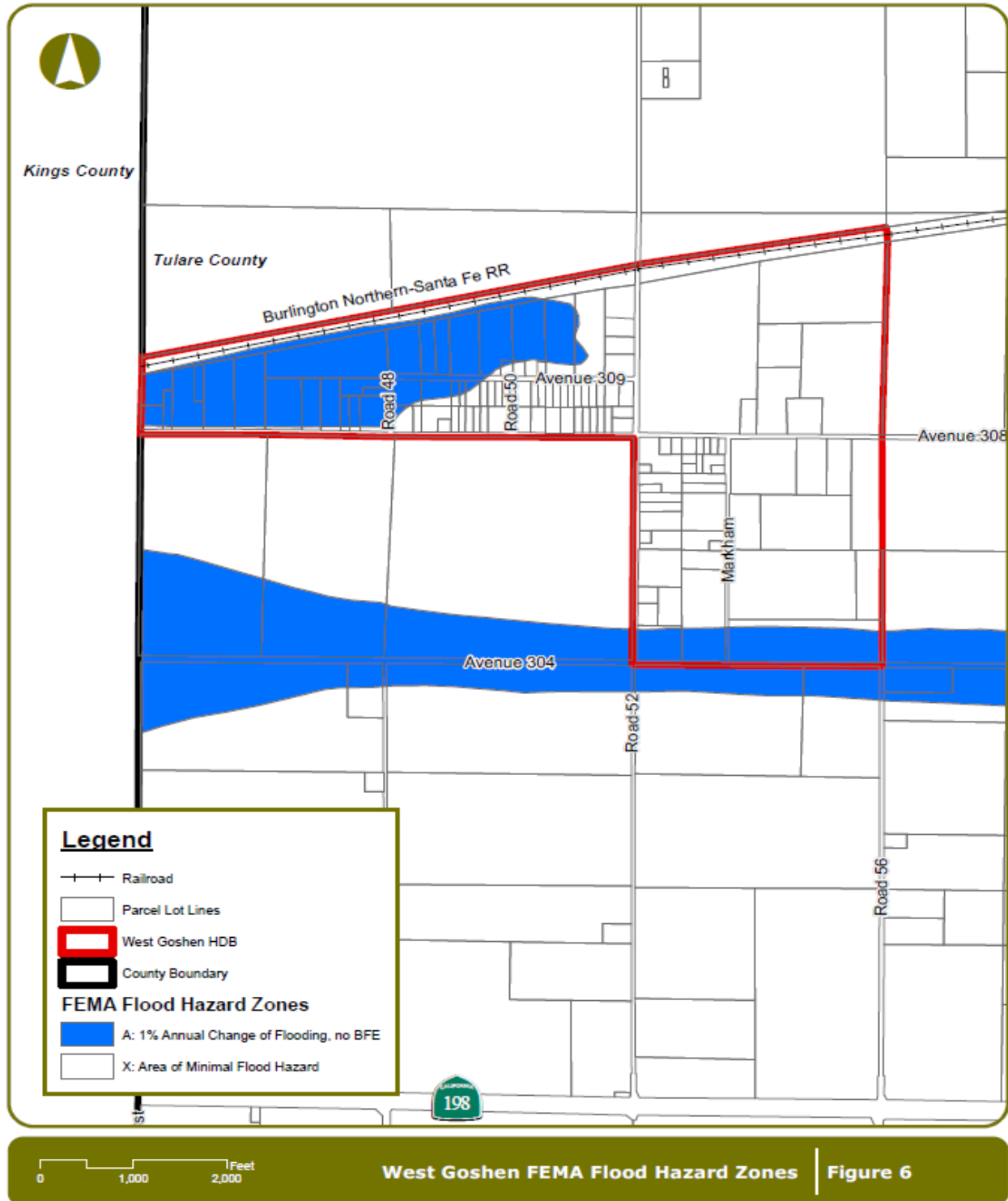
<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C905E, Panel Number 905 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

<sup>7</sup> General Plan Background Report, page 8-14

# WEST GOSHEN HAMLET PLAN

Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

Figure 6 - FEMA Flood Map



# WEST GOSHEN HAMLET PLAN

## Noise<sup>8</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan background Report West Goshen lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks). The prioritized concerns of the Community include:

### Energy: Natural Gas/Electricity

Southern California Edison provides service to West Goshen.

### Water

Domestic water service in West Goshen is provided by the California Water Service Company (Cal Water). “Cal Water's Visalia District was formed in 1926 with the purchase of the Visalia Water Company. We proactively maintain and upgrade our facilities to ensure a reliable, high-quality supply.”<sup>9</sup>

Table 14 - Existing Water Connections		
Number of Existing Connections	Capacity	Available
88	70	0

Source: Action Program 9, Tulare County Housing Element. Data current as of January 2014

**Table 15** shows the number of existing water connections, the capacity of the system, and the number of additional connections the system can accommodate for new development (Tulare County, January 2014). Mapping of the water system is currently unavailable.”<sup>10</sup>

### Sewer

There is no community wide sewer system in West Goshen. The community depends on individual on-site septic tank systems for wastewater disposal. No public sewer facilities are available in West Goshen. Each land owner relies on septic systems for wastewater.

<sup>8</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

<sup>9</sup> <https://www.calwater.com/about/district-information/vis/>

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

# WEST GOSHEN HAMLET PLAN

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## Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

West Goshen does not currently have a storm drainage system.”<sup>11</sup>

## Solid Waste

Solid waste generated in West Goshen can be disposed of at Visalia Landfill, located at 8614 Avenue 328. The landfill opened in 1950 and the estimated closure date was anticipated in 2012. The design capacity is 6.5 million cubic yards. Future development to accommodate projected growth as proposed in the Community Plan, including commercial and industrial development within the Project Area, will increase the amount of waste requiring disposal at landfills. To meet the required 50% diversion rate required by the State of California, each community will need to implement further programs, such as composting or establishing a location for the further separation and recovery of recyclable and other materials from the waste stream.

## Roads

“There are several roadways in West Goshen that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

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<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element

# WEST GOSHEN HAMLET PLAN

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>12</sup>

**Table 14** lists the roadways in need of repair, the limits, and type of maintenance strategy proposed. **Figure 7** graphically displays this information on a map.

**Table 15 - Road Maintenance Strategies**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Avenue 308	Road 48 to Road 52	GRX
2	Road 48	Avenue 308 to Avenue 309	GRX
3	Road 52	Avenue 308 to Avenue 309	CHIP

OLAY – overlay resurfacing operation

ACST – asphalt reconstruction

CHIP – chip seal

RCST – cold mix reconstruction

(Source: County of Tulare Public Works, 2012)

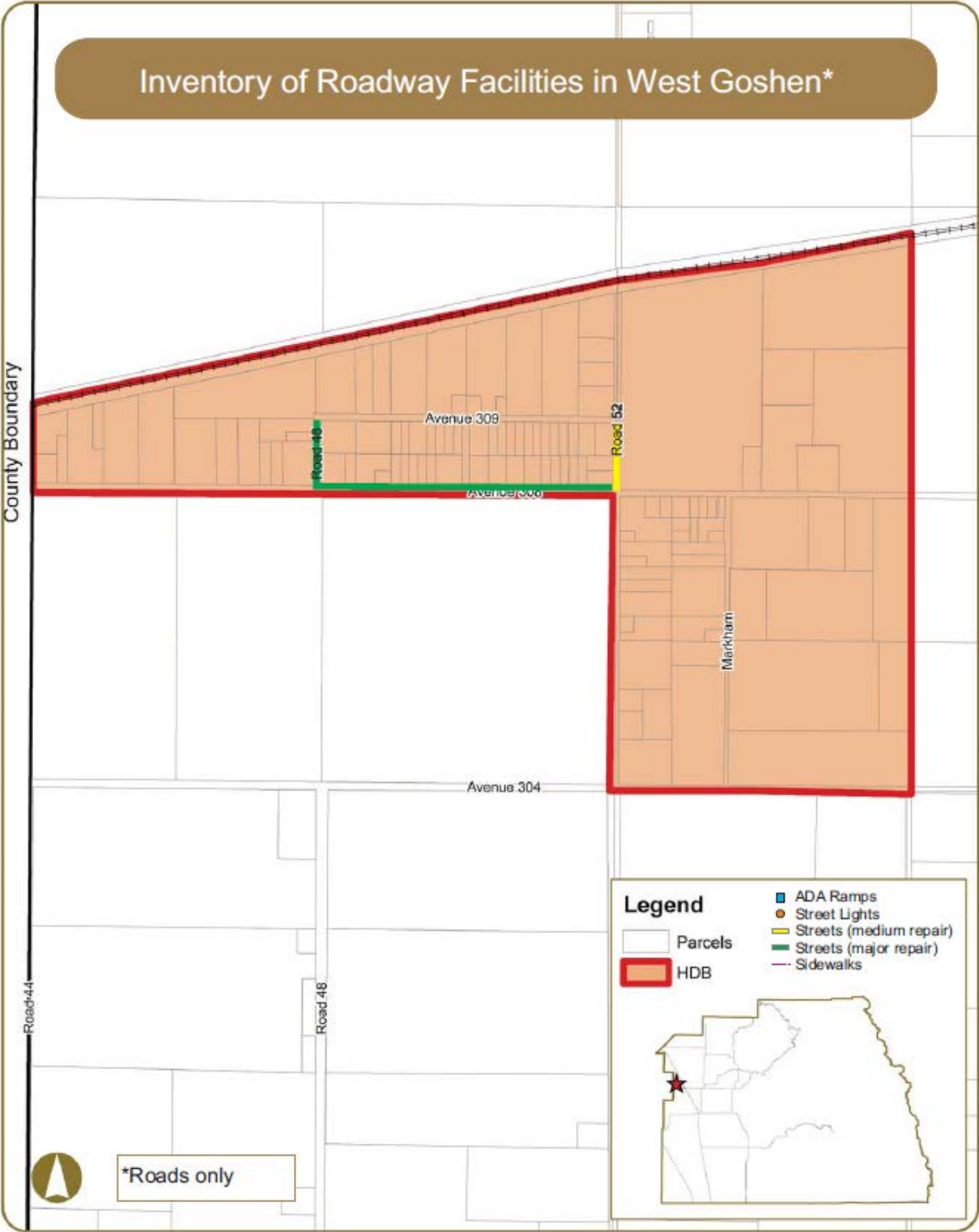
## Street Lights

There are currently no street lights located within West Goshen. Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element

# WEST GOSHEN HAMLET PLAN

Figure 7 - Inventory of Roadway Facilities in West Goshen



# WEST GOSHEN HAMLET PLAN

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## Sidewalks

There are currently no sidewalks located within West Goshen. The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within West Goshen.”<sup>13</sup>

## Transit and Bus Stops

Recognizing the need to provide transportation services to those who are disadvantaged, the City of Visalia provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation. Public transit is currently not available in West Goshen. However, the City of Visalia operates fixed-route services currently available Sunday through Saturday on a fixed route bus service. City of Visalia provides round-trip transportation for Goshen residents to Visalia via the Number 6 – Downtown, Visalia Medical Clinic, Goshen Route at Frontage Road/Betty Drive.

## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future.

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<sup>13</sup> Ibid



# WEST GOSHEN HAMLET PLAN

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## AMTRAK

The Hanford AMTRAK station, located approximately 10 miles to the west in Kings County, is the closest station to West Goshen providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 10 miles southeast. Fresno Yosemite International Airport (FAT), approximately 65 miles northwest of West Goshen, is the principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 75 miles southwest of West Goshen and offers direct flights to several destinations.

## Burlington Northern-Santa Fe Railroad

West Goshen is bound on the west side by the Burlington Northern-Santa Fe Railroad line running through Tulare County.

## **PUBLIC SERVICES**

### Community Resource Center

The community lacks sources and needs a facility where residents can access information and county services. People in West Goshen expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used for homework assistance, afterschool programs and as a computer lab. It was also suggested that the center should also be available for use by outside organizations that need a place to service West Goshen residents. This could also be the place where residents access a reliable internet connection.

### Sheriff

Police protection services are provided in West Goshen by the Tulare County Sheriff's Department main Sheriff's Office located at 2404 W. Burrell Avenue, in Visalia, approximately 10 miles east of West Goshen.

### Fire

There are currently no Fire Hydrants in West Goshen. Fire protection and emergency medical services are provided for West Goshen by the Tulare County Fire Department. The community is served by Tulare County Fire Department Station #7, located at 30901 Road 67 in Goshen. The station is staffed by patrol 7 and Engine 7.

### Schools

Schools are an essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The West Goshen Hamlet Plan Area is within the Visalia Unified School District Area 4 (8 miles away). The nearest elementary school (K-6) is in Goshen. Students in Junior High and High School attend schools in Visalia.



# WEST GOSHEN HAMLET PLAN

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## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>14</sup> The closest library is located in Visalia 8 miles away. (see Table 16).

Table 16 - Library Location & Hours		
Branch	Address	Service Hours (2003)
Visalia	Main Branch 200 West Oak Avenue Visalia, CA 93291-4993	Tuesday: 9 am - 8 pm Wednesday: 9 am - 8 pm Thursday: 9 am - 8 pm Friday: 12 pm - 6 pm Saturday: 9 am - 5 pm

Library hours current as of February 2010 Source: General Plan Background Report

## Parks

There are no County owned/operated public parks in West Goshen.

# THE WEST GOSHEN HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

Tulare County’s relationship to the State Route system, nearby counties, cities, and communities

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

West Goshen is a small agricultural community located near Highway 99 and 198. The major rural collector roads is Avenue 308. There are no proposed major streets as part of this Hamlet Plan. The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>15</sup>

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<sup>14</sup> General Plan Background Report, page 7-96

<sup>15</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

# WEST GOSHEN HAMLET PLAN

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## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>16</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation and Transportation Conditions

West Goshen is a small agricultural community located east of the Burlington Northern Santa Fe Railroad and west of Avenue 304. There are no proposed major streets as part of this Legacy Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

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<sup>16</sup> Tulare County General Plan Background Report, page 5-7.

# WEST GOSHEN HAMLET PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the West Goshen Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for West Goshen to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

West Goshen Hamlet Plan, and input received from West Goshen citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### **Community Development**

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Create a mix of uses with easy access to major roads**

Objective: Promote mixed use zoning and land uses.

Policies:

1. Locate high density residential uses near planned commercial areas.

2. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
3. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
4. The County shall work with the Schools to provide safe routes to school.
5. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Hypericum Legacy Plan.
6. The Teviston Legacy Plan should be reviewed every five years to determine if amendments are appropriate.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.

# WEST GOSHEN HAMLET PLAN

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2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

### Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in West Goshen.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

### Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in West Goshen.

3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

### Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## Economic Base

**GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

# WEST GOSHEN HAMLET PLAN

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## Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

## Policies:

1. Encourage the West Goshen to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

## Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## **Environmental Quality and Public Safety**

**GOAL I: Preserve and enhance the quality of life for present and future generation of West Goshen citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

## Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

## Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

## Objective: Protect Agricultural Lands:

Land within the respective West Goshen, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;

# WEST GOSHEN HAMLET PLAN

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- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of West Goshen.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of West Goshen.

# WEST GOSHEN HAMLET PLAN

## ASSESSMENT OF LAND NEEDS

Within the existing 477.2 acre West Goshen Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute 93 percent of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 675 by 242.1 urbanized acres (area within the HDB), which equals 2.78 persons per urbanized acre (see **Tables 17 and 18** showing population projections). Projecting the population at a 1.3% growth rate adds 144 persons in Year 2030. Multiplying the 144 persons by the 1/3 ratio suggests an additional 1.87 acres of land will be required to accommodate development by the year 2030 if projections are realized.

### Population Growth Forecast

**Table 17 - West Goshen Population Projections**

<b>Growth Rate</b>	<b>2015</b>	<b>2020</b>	<b>2030</b>
0.013	633	720	819

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The Year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>17</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in year 2015 the community had 175 dwelling units (including vacant dwellings) with a population of 675. At an annual growth rate of 1.3%, the projected housing units are 187 and 212 in years 2020 and 2030, respectively, and projected population is 729 and 819 in years 2020 and 2030, respectively.

**Table 18 - Population Projections**

<b>Year</b>	<b>Population</b>	<b>Growth (%)</b>
2015	675	0.013
2016	684	0.013
2017	693	0.013
2018	702	0.013
2019	711	0.013
2020	720	0.013
2021	729	0.013
2022	739	0.013
2023	748	0.013
2024	758	0.013
2025	768	0.013
2026	778	0.013
2027	788	0.013
2028	798	0.013
2029	809	0.013
2030	819	0.013

<sup>17</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).



# WEST GOSHEN HAMLET PLAN

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## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, water service levels to be expanded, for which grant funding will be required.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily require a discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Table 19 - Educational Attainment			
Educational Attainment	California	Tulare County	West Goshen
Population 25 years and over	25,257,858	260,964	316
Less than 9th grade	10.0%	20.8%	44.3%
9th to 12th grade, no diploma	8.2%	10.9%	6.6%
High school graduate (& equivalency)	20.7%	25.2%	13.9%
Some college, no degree	21.8%	22.3%	30.4%
Associate's degree	7.8%	7.0%	2.2%
Bachelor's degree	19.8%	9.5%	2.5%
Graduate or professional degree	11.6%	4.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates

Based on the 2011-2015 American Community Survey (**see Table 19**), the educational barrier in West Goshen begins in grade school. Of the adults age 25 and older, 44.3% had an educational level of less than 9<sup>th</sup> grade. A lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

### Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are located in Visalia.



# WEST GOSHEN HAMLET PLAN

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 20**) and Zoning Districts Plan (see **Table 21**) have been updated.

<b>Table 20 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	449.6
Right-of Ways	27.6
West Goshen HDB	477.2

<b>Table 21 - Proposed Zoning Districts</b>	
Proposed Zones	Acres
A-40	136.2
C-2-MU	34.4
M-1	36.8
R-A-12.5	242.1
Right-of Ways	27.6
West Goshen HDB	477.2

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the General Plan recommendations contained in the West Goshen Legacy Plan. The following components comprise the West Goshen Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the West Goshen Legacy Plan, there are a several proposed changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of West Goshen.

# WEST GOSHEN HAMLET PLAN

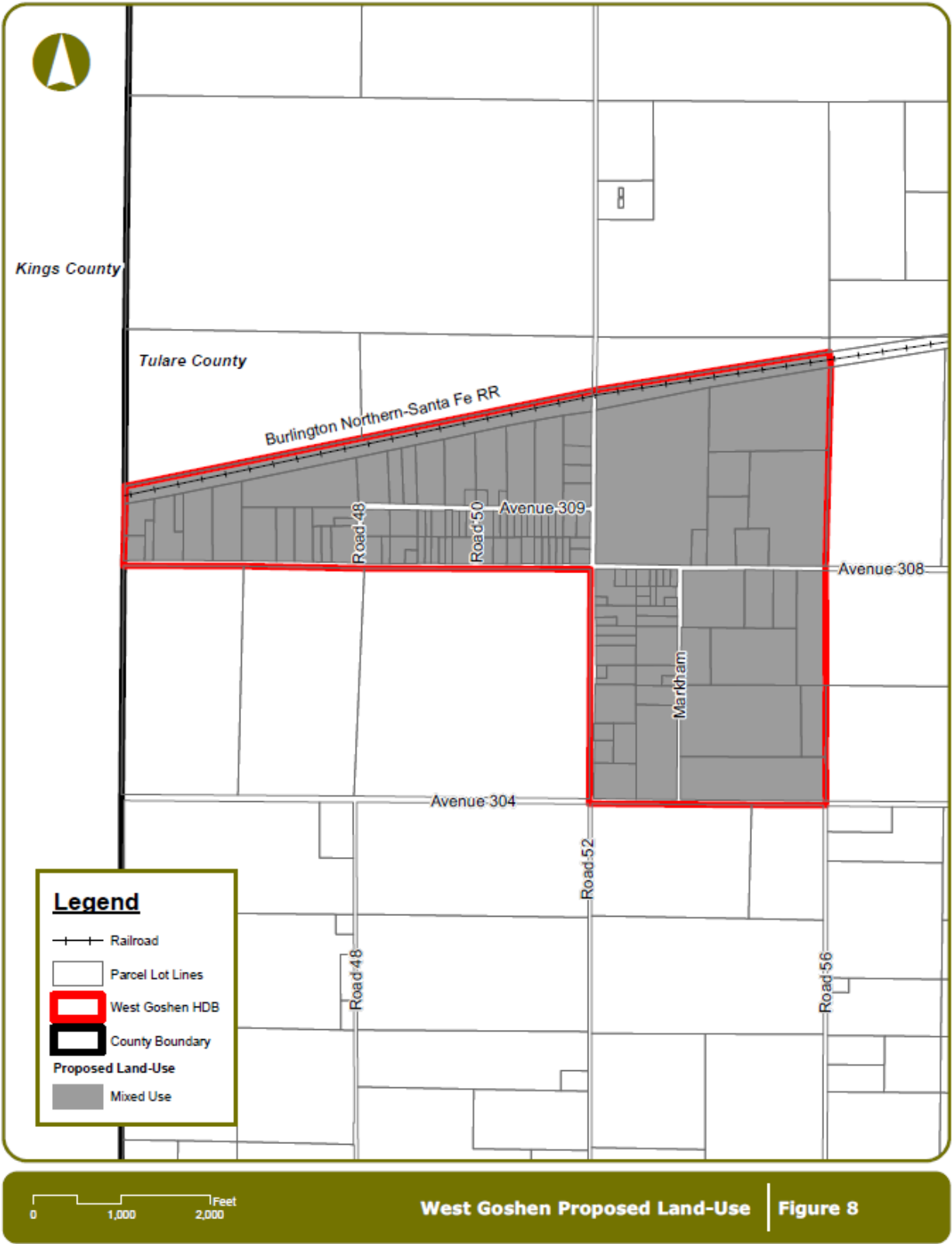
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## Zoning Map Update

The current Zoning Map (**see Figure 9**) for West Goshen will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 8**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 10**) West Goshen Proposed Zoning).

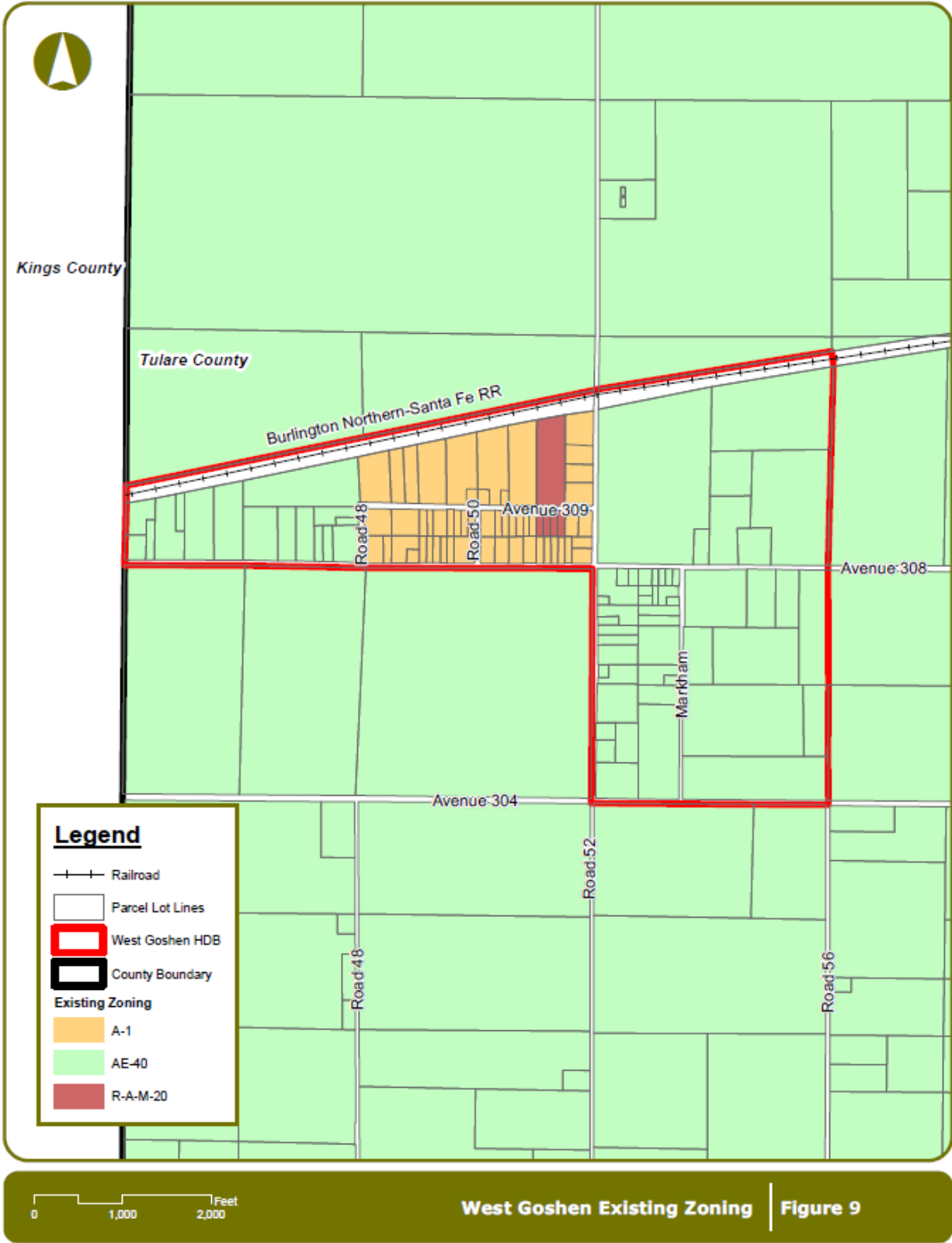
# WEST GOSHEN HAMLET PLAN

Figure 8 - Proposed Land Use Plan



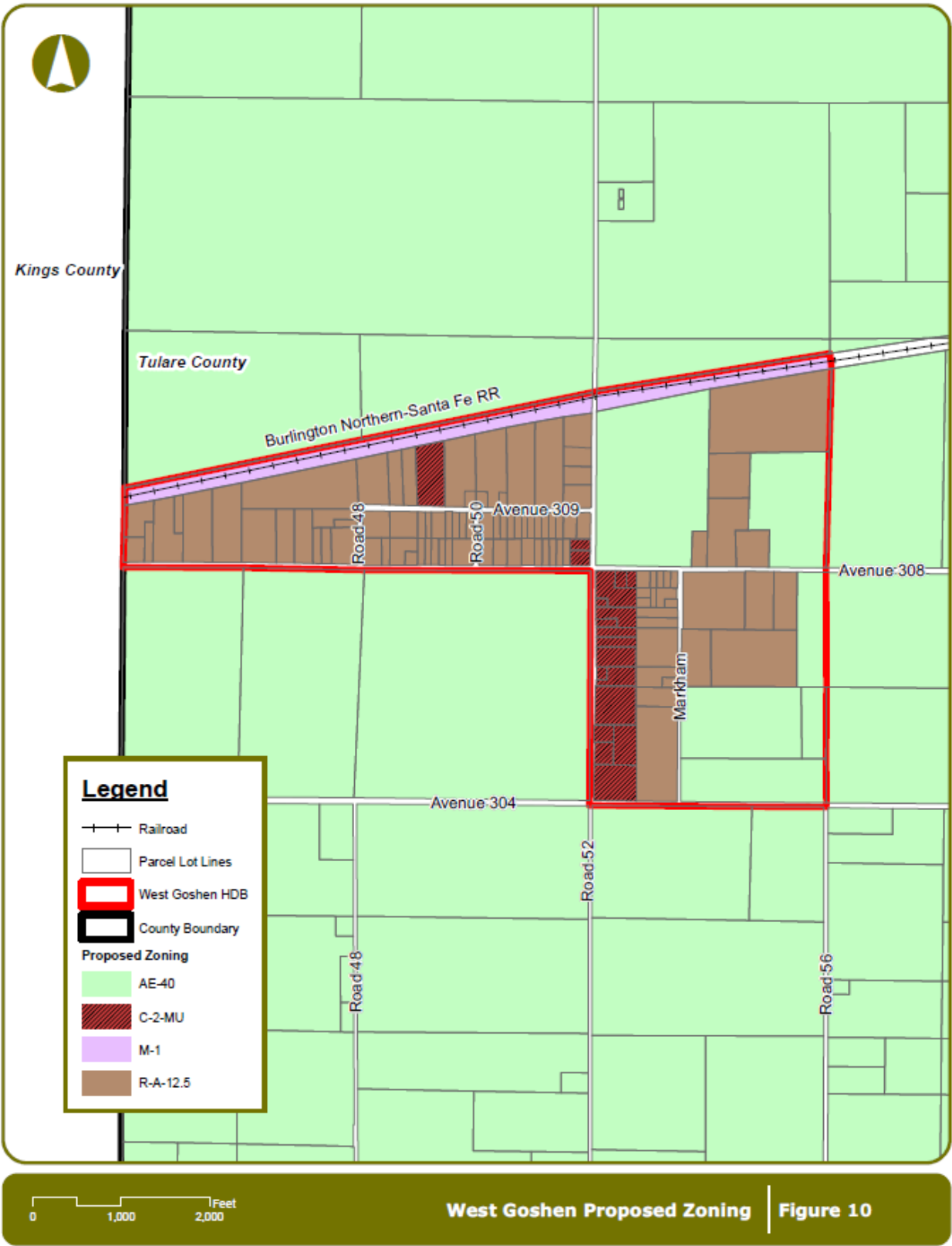
# WEST GOSHEN HAMLET PLAN

Figure 9 - Existing Zoning Districts Plan



# WEST GOSHEN HAMLET PLAN

Figure 10 - Proposed Zoning Districts Plan



# WEST GOSHEN HAMLET PLAN

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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

# WEST GOSHEN HAMLET PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	
Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1

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Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft..	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	



# WEST GOSHEN HAMLET PLAN

Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone

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Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3, *C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1,	R-3, C-1, C-2, C-3,

# WEST GOSHEN HAMLET PLAN

	R-3	M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1

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Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1

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Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# WEST GOSHEN HAMLET PLAN

## A-2 *Mixed Use Overlay District (Zone Change Text)*

The following regulations shall apply in the community of West Goshen, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**    The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**    This overlay zone only applies to the community of West Goshen.

**USE**                   **C.**    No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for West Goshen.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the

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neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for

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buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for West Goshen. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.



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## **A-3     *Development Standards (Mixed Use Zoning District)***

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To promote Economic Development within the West Goshen Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within West Goshen. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the West Goshen Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within West Goshen. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1     Entries to buildings should be individualized and clearly identifiable.

A-2     Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3     Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4     The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5     Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6     Each building should have a defined base, body, and cap segment

A-7     Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8     Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to

sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9     Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10    Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11    Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12    Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

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A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public

areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

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RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

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SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

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LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

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WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.



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SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

# WEST GOSHEN HAMLET PLAN

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.



# WEST GOSHEN HAMLET PLAN

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## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# WEST GOSHEN HAMLET PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning

District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.



B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:       None

TULARE COUNTY PLANNING COMMISSION

  
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Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.



The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT                     )  
TO SECTION 16.H OF ORDINANCE NO. 352            )  
TO ALLOW ADDITIONAL BY-RIGHT USES                )       RESOLUTION NO. 9369  
CONSISTENT WITH THE ADOPTED HAMLET             )  
PLANS, AS PROPOSED IN CHANGE                     )  
OF ZONE NO. PZC 17-039                             )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map



***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

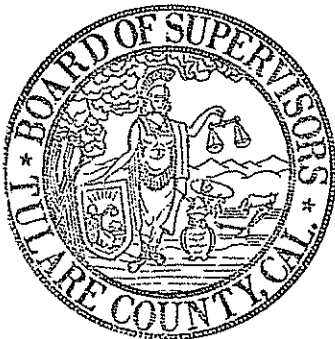
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

## Yetteem Hamlet Plan

# YETTEM HAMLET PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development, Planning Branch, and  
Planning Processing Division



# YETTEM HAMLET PLAN

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# YETTEM HAMLET PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# YETTEM HAMLET PLAN

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# YETTEM HAMLET PLAN

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## Yettem Hamlet Plan

Adopted: December 5, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0976

Tulare County Planning Commission  
Recommendations: November 8, 2017  
Resolutions No. 9366, 9367, 9368, 9369, and 9370

General Plan Amendment: GPA 17-035  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-039  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-038

Yettem Hamlet Plan: GPA 17-024  
Zoning District Map: PZC 17-024



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# YETTEM HAMLET PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

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## **Tulare County Resource Management Agency**

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## INTRODUCTION

The primary objective in preparing a Hamlet Plan for Yettem is to develop a plan which can accurately reflect the needs and priorities of this community. Yettem is currently designated as a Hamlet in the 2030 Tulare County General Plan (2012). A more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [wells, water distribution piping, and storage tanks], curbs, gutters, and sidewalks, etc.) and to stimulate economic development within the community.

### Location

The community of Yettem (**see Figure 1**) is located on the east side of the San Joaquin Valley and is a census-designated place located in the northeast portion of Tulare County. It is bounded by Road 140 in the west, Road 144 in the east, and north and south of Avenue 384 and encompasses 0.2 square miles of land. Yettem is directly served by State Route 201 (**see Figure 2**). Yettem is location in Section 34 and 35 Township 16 South, Range 25 East, MDB&M, and can be found within Reedley United States Geological Survey 7.5 minute topographic quadrangle. Yettem is located at an elevation of 348 feet National Geodetic Vertical Datum. The coordinates of Yettem are: Latitude: 36.486313 Longitude: -119.259026.

### Planning Area

The Yettem Hamlet Development Boundary (HDB) area consists of 59.9 acres (**see Figure 3**). The Land Uses within the HDB are Mixed Use activities which occupy 55.1 acres. The remaining 1 percent are lands dedicated for Right-of-way (**See Table 1**).

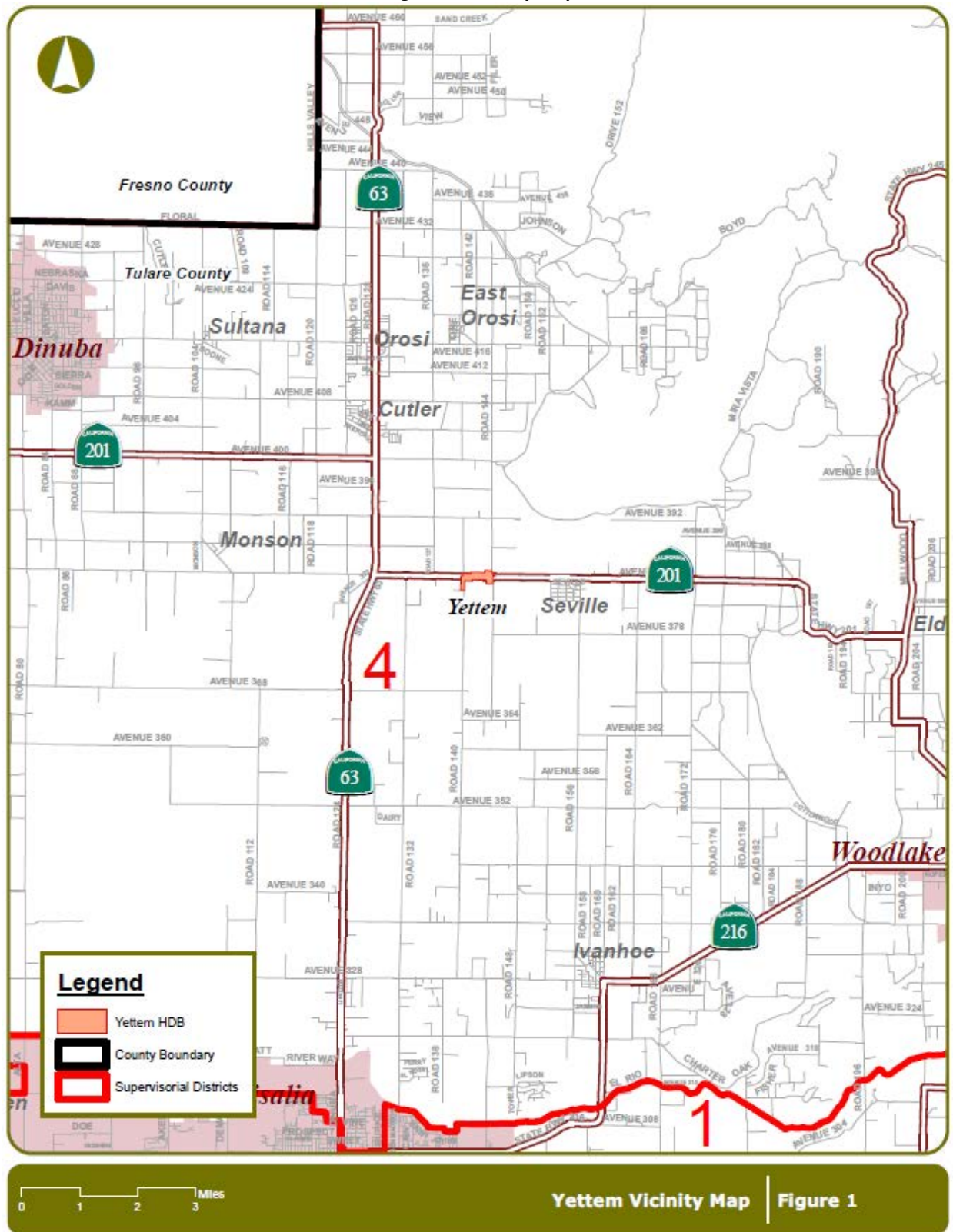
No change is proposed to the Hamlet Development Boundary.

Table 1 - Yettem Land Use	
Land Use	Acres
Mixed Use	55.1
Rights-of-way	4.8
Total	59.9

Source: Tulare County GIS

# YETTEM HAMLET PLAN

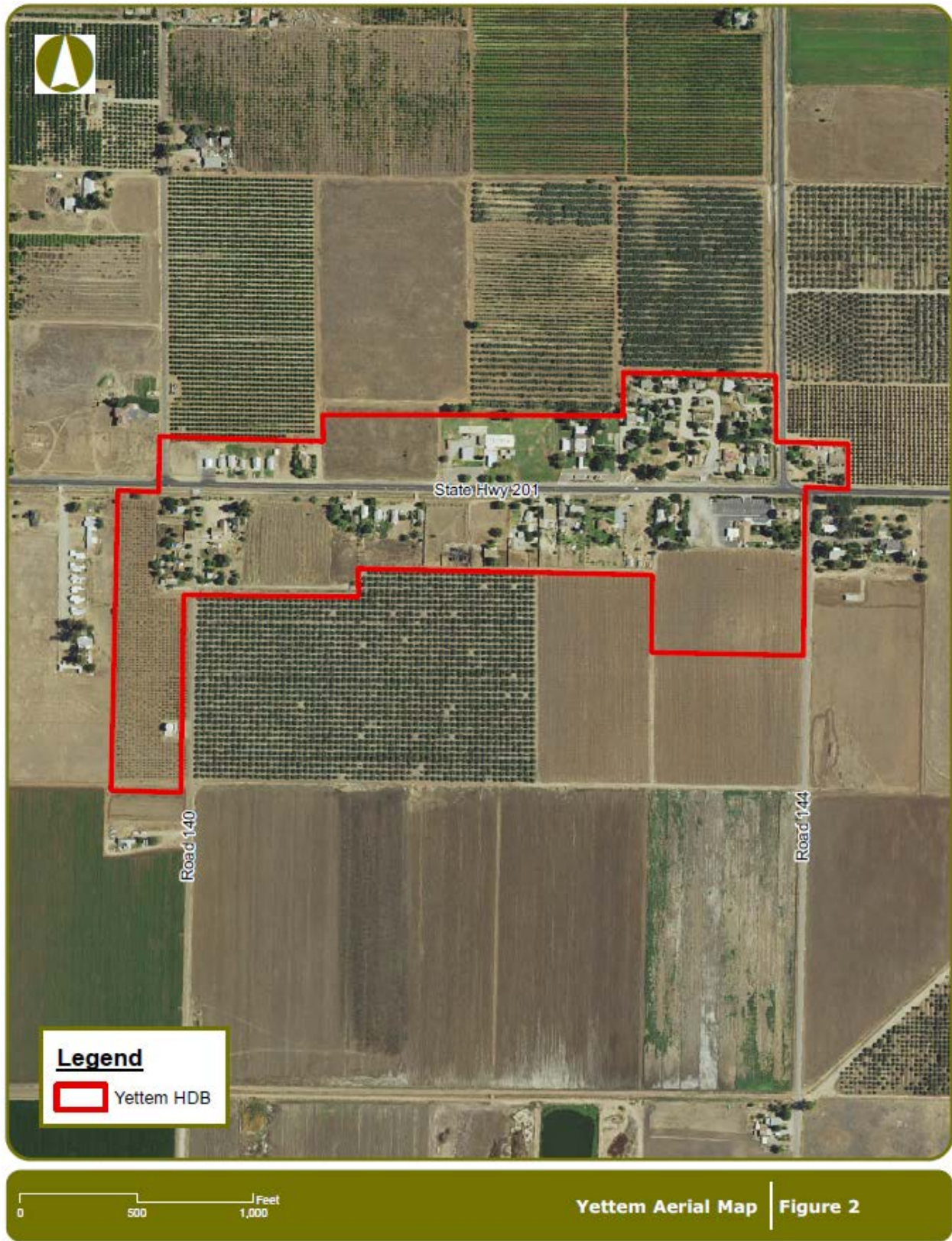
Figure 1 - Vicinity Map





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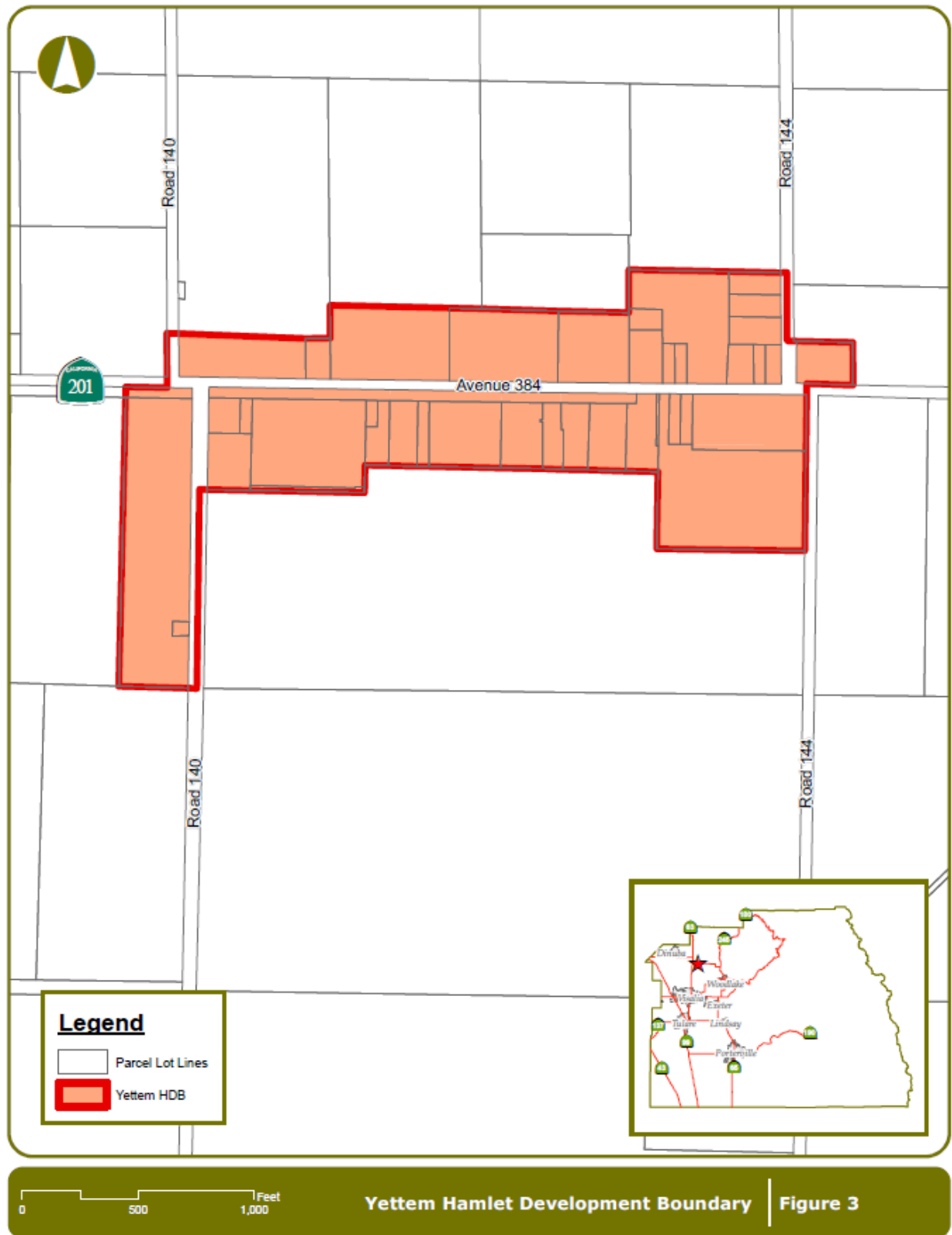
Figure 2 - Aerial Yettem





# YETTEM HAMLET PLAN

Figure 3 - Yettem HDB



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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

The County of Tulare obtained a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Hamlet Plan. In efforts to invite and make local residents aware of the Strategic Growth Council (SGC) project, Self Help Enterprises (SHE) went door to door throughout the community of Yettem to gather the information needed for this study, SHE conducted door to door surveys throughout the community of Yettem. Nine (9) surveys were collected.

### Community Outreach Steps

Self-Help Enterprises (SHE) utilized various outreach methods to promote community participation and increase awareness of the SGC project:

- 1.) An informational flyer was developed.
- 2.) On October 1, October 3 and October 5, 2015 SHE staff went door to door to distribute flyers to homes.
- 3.) Throughout the community outreach process, SHE staff explained the goals of the project and emphasized the importance and benefits of participating in the SGC community meeting.
- 4.) SHE partnered with the Stone Corral, located in neighboring Seville to spread the word about the meeting; meeting flyers were sent home from school with each student including those students that live in Yettem.
- 5.) To ensure that residents had multiple opportunities to be made aware of the meeting, flyers were posted at the local stores and local businesses.

### Community Meeting

The Strategic Growth Council held a community for input meeting for Yettem and Seville residents in Seville on Wednesday, December 9, 2015 at 5:30 pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held in the cafeteria of the Stone Corral School located at 15590 Ave 383 in Seville. One Yettem resident attended this meeting.

### Community Survey

A survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Multimodal Opportunities, and Priority Improvements.

### Priority Concerns

1. Road Conditions
2. Street lighting
3. Sidewalks
4. Speed Bumps
5. Public Transportation

# YETTEM HAMLET PLAN

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6. Internet Access
7. Community resource center
8. Community Park

## **Road Conditions**

Yettem survey results show that most residents are not satisfied with road conditions. It is reported that road conditions are poor and most roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving on their roads. Yettem roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not actually repaired. The following roads reported by the residents to be a top priority:

- Road 383
- Road 153
- Road 142

## **Street Lights**

Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's Department seldom patrols Yettem. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

## **Side Walks**

The community of Yettem has too few sidewalks. The residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Yettem traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

## **Speed Bumps**

Residents reported that they are concerned for the safety of their children while they are outside because traffic travels at a high speed in remote areas of the County such as Yettem. Parents are requesting that the County install speed bumps to help slow traffic.

## **Additional Bus Stop**

Residents report that the TCAT has one bus stop in Yettem. This location is too far for some people to walk to. Residents are requesting an additional bus stop location in town.

## **Community Resource Center**

Yettem does not have a community hub. The community lacks many resources and needs a place where residents can access information and county resources. People surveyed in Yettem, expressed the need for a Community Resource Center. This center would be a multi-purpose building that could be used by kids for homework help, afterschool programs and as a computer lab. It was also

# YETTEM HAMLET PLAN

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suggested that the center should also be available for use by outside organizations that need a place to service Yettem residents. This could also be the place where residents access a reliable internet connection.

## **Community Park**

In the community of Yettem there is no safe public green space for children to play and adults to be physically active. The nearest park is located 6 miles away in the city of Orosi. Residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and concerned for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. Residents expresses that having a community park is a major priority for them.

## **Internet Access**

Most families in Yettem do not have internet access at their homes. Any available internet service is unreliable and cost prohibitive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or expensive compared to what it costs in nearby cities. Without access to the internet, Yettem residents are unable to do things such as online job hunting, apply for resources, or provide homework help to their kids.

# YETTEM HAMLET PLAN

## YETTEM TECHNICAL BACKGROUND REPORT

### DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a hamlet's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a hamlet plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a hamlet allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a hamlet. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller hamlet such as Yettem.

#### Population

In 2015, the population of Yettem was 300 (see Table 2).

**Table 2 - Population**

	<b>California</b>	<b>%</b>	<b>Tulare County</b>	<b>%</b>	<b>Yettem</b>	<b>%</b>
Male	19,087,135	49.7%	227,426	50.1%	300	53.7%
Female	19,334,329	50.3%	226,607	49.9%	161	46.3%
Total	38,421,464		454,033		139	

2011-2015 American Community Survey 5-Year Estimates

#### Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."<sup>1</sup>

#### Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate has been used to project Yettem's population during the Planning Period (see Table 3).

<b>Table 3 - Projected Annual Growth Rate</b>		
	<b>Historic Growth Rates 1990-2007</b>	<b>Projected Growth Rates 2007-2030</b>
<b>County Total</b>	1.9%	2.4%
<b>Incorporated</b>	2.8%	2.9%
<b>Unincorporated</b>	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

<sup>1</sup> Tulare County Regional Blueprint, page 7

# YETTEM HAMLET PLAN

## Median Age

Yettem's population's median age is younger than the median age of both Tulare County, as well as the State of California (see Table 4).

Table 4 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Yettem	22.0

2011-2015 American Community Survey 5-Year Estimates

## Ethnicity and Race

In 2015, approximately 0% of Yettem's population was White, 0% was African American, 0% was Native American, 0% was Asian, and 0% was two races or more. Approximately 100% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community. (see Table 5)

Table 5 - Race & Ethnicity						
	California	%	Tulare County	%	Yettem	%
Total	38,421,464		454,033		300	
Hispanic or Latino (of any race)	14,750,686	38.4%	283,533	62.4%	300	100%
White (Not Hispanic)	14,879,258	38.7%	139,581	30.7%	0	0%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5%	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Yettem

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production. (see Table 6)

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Yettem was about 0.0% while the rate for Tulare County was 7.813.5%. The unemployment rate for the State of California was 10.1%. Keeping in mind that the 0.0% includes only the employable labor force (that is, not every person of the population) results in about 0 unemployed persons of Yettem's person labor force of 149.

# YETTEM HAMLET PLAN

Table 6 - Employment Status			
Employment Status	California	Tulare County	Yetttem
Population 16 years & over	30,312,429	325,404	220
In labor force	19,269,449	194,420	149
Civilian labor force	19,137,441	194,102	149
Employed	17,246,360	170,780	149
Unemployed	1,891,081	23,322	0
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	71

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Although Yetttem's median household income is unavailable the Per Capita income was \$8,544 for 2015. Whereas the Per Capita income for both Tulare County and the State of California was higher (see Table 7).

Table 7 - 2011-2015 American Community Survey Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Yetttem CDP	-	-	\$8,544

## Severely Disadvantaged Community

Public Resources Code 75005. (g) states that a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom), and allow various housing types (such as mobile homes).

Table 8 - Housing Tenure			
	California	Tulare County	Yetttem
Occupied housing units	12,617,280	132,706	61
Owner-occupied	6,908,925	75,974	0
Renter-occupied	5,708,355	56,732	61

2011-2015 American Community Survey 5-Year Estimates

# YETTEM HAMLET PLAN

Tulare County has an estimated 131,642 occupied housing units. Within the communities along Highway 99, the number of occupied housing units ranges from 208 in Traver to 1,891 in Earlimart. The total occupied housing units are lower than the total built housing units, as not all housing units are occupied.

Table 9 - Housing Tenure (%)			
	California	Tulare County	Yettem
Owner-occupied	54.8%	57.2%	0%
Renter-occupied	45.2%	42.8%	100%

2011-2015 American Community Survey 5-Year Estimates

## Housing Conditions

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

Table 10 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Yettem	2	10	0	0	2	10	4	20	12	60	20

Source: Tulare County 2015 Housing Element

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Yettem indicated that approximately 10% of the housing units were sound. Approximately 30% of Yettem's housing units were deteriorated and 60% were dilapidated (see Table 10).

## Age of Structures

Most of the homes in Tulare County were built between 1950 and 2009. (see Table 11) The total number of housing units in Tulare County is 144,792.

Table 11 - Year Structure Built			
Year Structure Built	California	Tulare County	Yettem
Total housing units	13,845,790	144,792	80
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	0
Built 1990 to 1999	1,495,571	21,767	0
Built 1980 to 1989	2,117,819	22,733	24
Built 1970 to 1979	2,503,688	27,111	56
Built 1960 to 1969	1,871,029	15,500	0
Built 1950 to 1959	1,907,512	13,694	0
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	0

2011-2015 American Community Survey 5-Year Estimates



# YETTEM HAMLET PLAN

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Yettem was 4.66 persons per household (see Table 12).

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>2</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

Table 12 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	2.99	2.01
Tulare County	3.23	3.52
Yettem	0	4.66

2011-2015 American Community Survey 5-Year Estimates

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. A vacancy rate of about 5-6% is considered typical; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2014, the rental vacancy rate in Yettem was 0%, which was lower than both Tulare County at 4.2% and the State of California at 4.6%. The homeowner vacancy rate was 0%, which was lower than Tulare County at 1.8% and the State of California at 81.6%, (see Table 13).

Table 13 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.6%	4.6%
Tulare County	1.8%	4.2%
Yettem	0%	0%

2011-2015 American Community Survey 5-Year Estimates

## ENVIRONMENTAL CONDITIONS

### Wildlife<sup>3</sup>

A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016 (see Figure 4) indicates there are special status species within the Ivanhoe Quadrant Species List (which includes the Yettem Planning Study Area) consisting of four animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Tipton kangaroo rat (*Dipodomys nitratoideus*, federal endangered); Nelson antelope squirrel (*Ammospermophilus nelsoni*, state threatened); and Blunt-nosed leopard lizard (*Gambelia sila*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

<sup>2</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See:

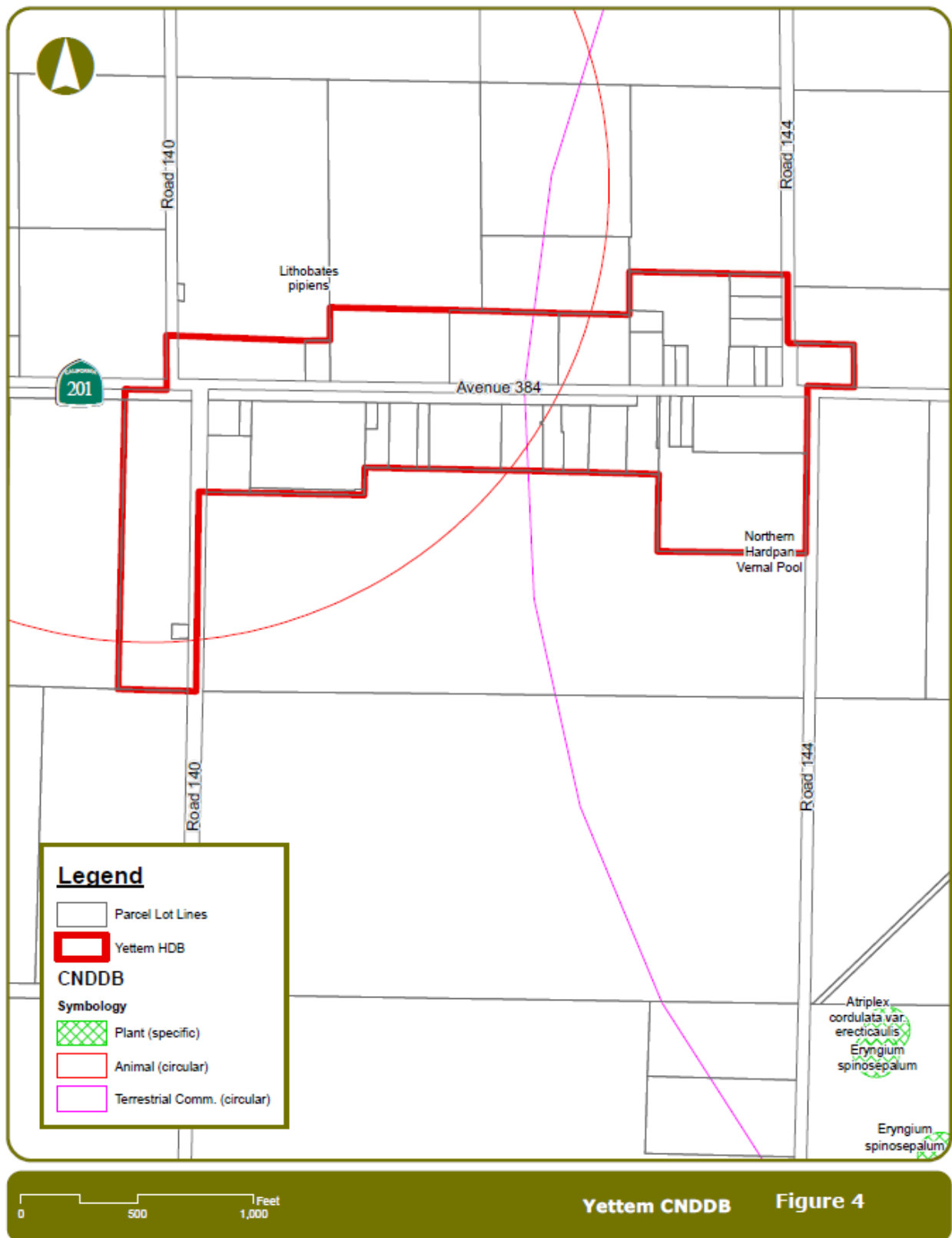
[http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

<sup>3</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS).

<http://www.dfg.ca.gov/biogeodata/bios/>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> accessed November 1, 2016

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Figure 4 - CNDDDB Map



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## Geology & Seismic Hazards<sup>4</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Yettem are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Yettem] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>5</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County (see Figure 3 ). The following descriptions are provided for the below soil types:

**Seville clay, 0 to 2 percent slopes,** is very deep and well drained, medium or high runoff, some areas are ponded; very slow permeability. The soil is suitable for livestock grazing. Vegetation is scattered saltbush with annual grasses and forbs.

**San Joaquin loam, 0 to 2 percent slopes,** is moderately deep to a hardpan, well drained and nearly level. This soil is suitable for orchards, vineyards, and cultivated crops but is somewhat limited by the presence of hardpan which restricts root growth. This problem can be alleviated by ripping and shattering the hardpan. The soil is poorly suited to urban uses because of a high clay content, very slow permeability and a cemented hardpan. Septic tank filter fields are severely limited for these reasons.

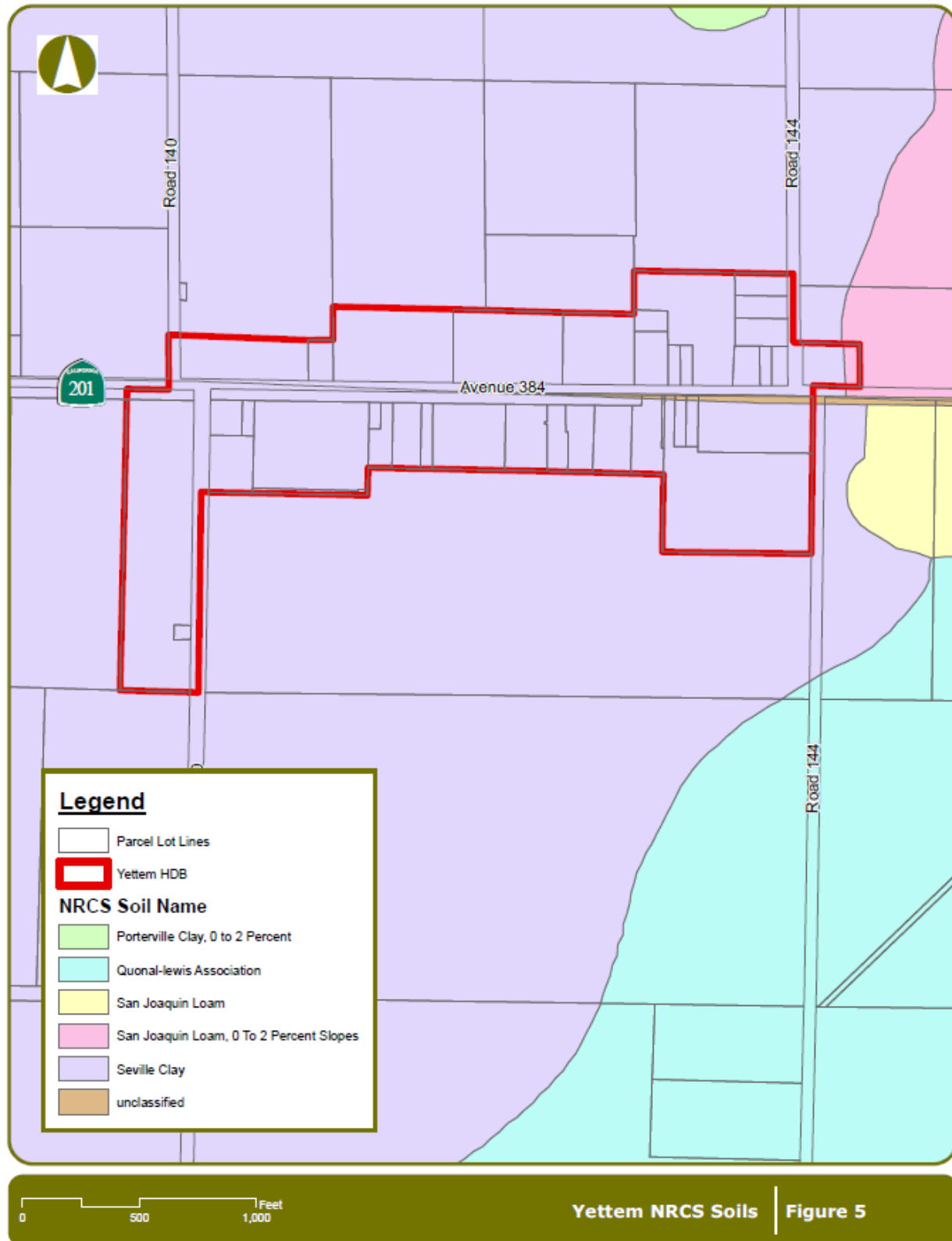
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<sup>4</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>5</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009

# YETTEM HAMLET PLAN

Figure 5 - NRCS Soils Map



# YETTEM HAMLET PLAN

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## PHYSICAL CONDITIONS

### Air Quality

The Yettem Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

There is a correlation between air quality and land use. It is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Yettem is located near the southwestern end of the Valley with prevailing winds from the northwest, it is susceptible to accumulate adversely modified air, particularly

# YETTEM HAMLET PLAN

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when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Yettem and within the community itself include State Highway 201 and agricultural activities.

## Flooding<sup>6</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0635E, Yettem is within Flood Zone X (see **Figure 6**). The west portion of Yettem is within Flood Zone X, an area of 0.2% chance flood; areas of 1 % annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance of flood. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each Community, known as the Flood Insurance Rate Map (FIRM). A 100-year flood is considered for purposes of land use Planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms.”<sup>7</sup> Although some areas of Tulare County have experienced major flooding along its major rivers, the Yettem Plan area has not. There are portions of Yettem, however, that are within and adjacent to the FEMA 500 year flood zones.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Yettem there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand storm water drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), by agreeing to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in Special Flood Hazard Areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available at the County of Tulare Resource Management Agency at the following website: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

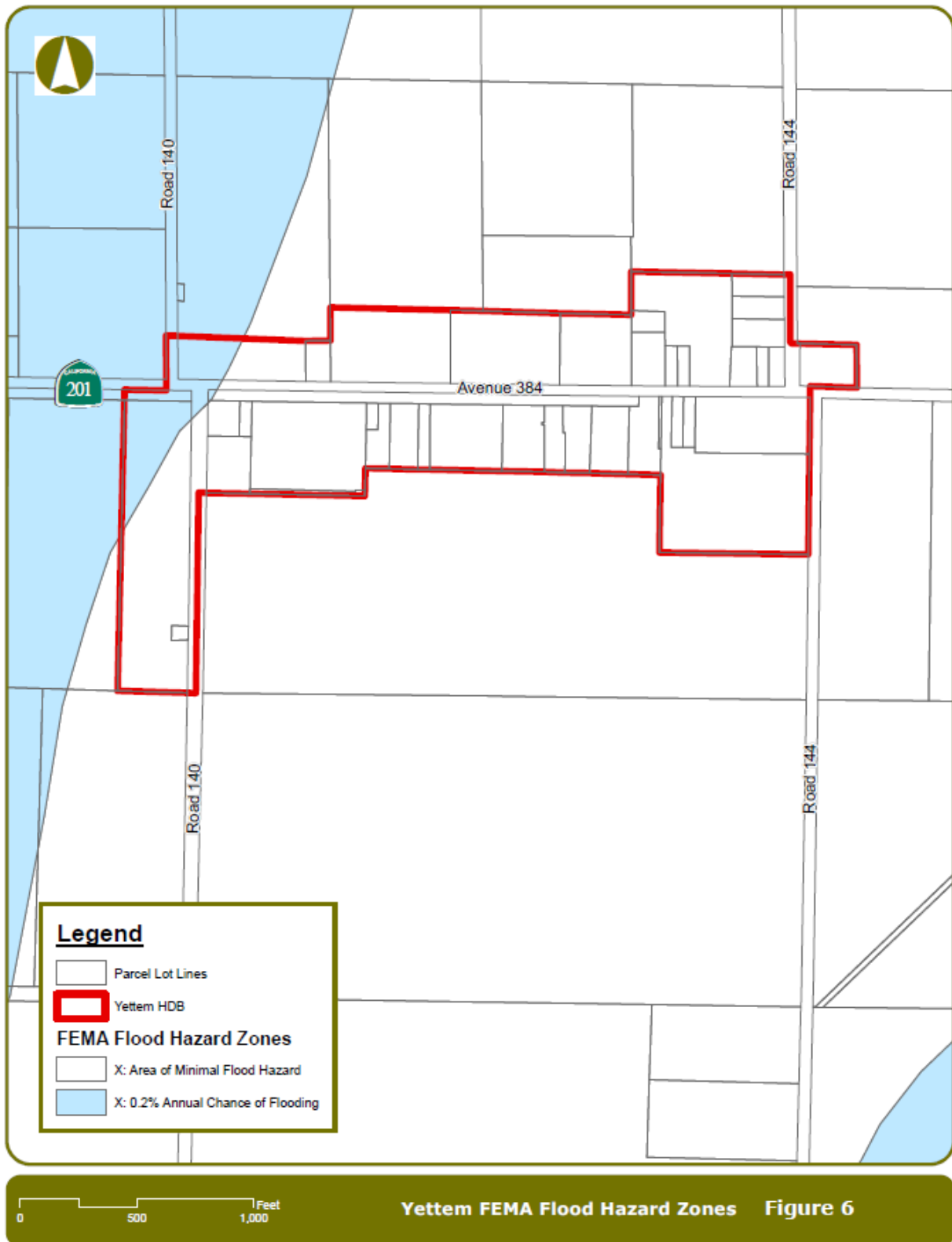
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<sup>6</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0635E, Panel Number 635 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

<sup>7</sup> General Plan Background Report, page 8-14

# YETTEM HAMLET PLAN

Figure 6 - FEMA Flood Map



# YETTEM HAMLET PLAN

## Noise<sup>8</sup>

Tulare County adopted a noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan background Report Yettem lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” This Hamlet Plan seeks to address deficiencies and the need for improvements to the drinking water system improvements, (wells, water distribution piping, and storage tanks), curbs, gutters, streets, and sidewalks).

### Energy: Natural Gas/Electricity

PG&E provides service to Yettem.

### Domestic Water & Wastewater

Domestic water and sewer service in Yettem is provided by Tulare County. **Table 14** shows the number of existing water and sewer connections, the capacity of each system, and the number of additional connections the systems can accommodate for new development (Tulare County, January 2014 and Housing Element, May 2012). Currently, there are plans to hook up Seville to a new pump in Yettem, the plans are contained in **Figure 7**.

**Table 14 - Existing Water & Wastewater Connections in Yettem**

Description of Existing Infrastructure					
Drinking Water			Waste Water*		
No. of Existing Connections	Capacity	Available	No. of Existing Connections	Capacity	Available
69	596	527	69	193	124

\* Data current as of January 2014

\*\* Data current as of May 2012

<sup>8</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[http://www.fta.dot.gov/documents/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](http://www.fta.dot.gov/documents/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)



# YETTEM HAMLET PLAN

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## **Seville's Water Challenges and History of Project**

Most Seville residents get their water supply from the Seville water system (previously the privately-owned Seville Water Company), which has been operated by the County of Tulare since 2009 under a court receivership order. Most of the distribution system is over 50 years old and needs to be fully replaced. The pipes have exceeded their useful life and were not built to current standards. The wells supplying water to the entire community are unable to meet current water demand.

## **Water Systems Improvements**

The objectives of the Seville-Yetter Water Systems Improvement Project are to:

- Secure a safe and reliable source of drinking water for Seville and Yettem
- Replace old and failing infrastructure in Seville
- Promote water conservation via the installation and use of water meters in both communities
- Address governance challenges and improve economies of scale
- Reduce water systems' vulnerabilities

## **Project Objectives**

To accomplish these objectives, the project includes the following Water Systems Improvements:

- Build a completely new water distribution system in Seville
- Drill a New Well (the County of Tulare has purchased property in Yettem for the future well)
- Install Water Meters in Yettem and Seville
- Interconnect the Yettem and Seville Systems

In 2012, the County of Tulare obtained grant funding under Proposition 84 from the State of California, to complete a feasibility study for the Seville water system. The study determined that a new water source for the community was needed, evaluated the consolidation of the Yettem and Seville water systems and funded the preparation of plans, specifications, environmental documents and construction costs estimates for the proposed water systems improvements.

## **Governance Formation**

Self-Help Enterprises and Tulare County will be working with Seville and Yettem to form a Community Services District for both communities to manage the connected systems (formation to be completed November 2018). A detailed formation schedule is available upon request.



# YETTEM HAMLET PLAN

Table 15 - Project Phasing

PROJECT PHASES	
To expedite the construction of proposed water systems improvements and improve water access in Seville, the project has been split into two phases.	
<b><u>PHASE 1: Seville Water Distribution System</u></b>	<b><u>PHASE 2: Seville-Yettem Intertie</u></b>
<p>Includes the construction of a completely new water distribution system in Seville, installation of water meters in Seville and, if enough funding is available, a water storage tank.</p> <p><b><u>Anticipated Project Schedule</u></b> Design, Plans, and Specifications and Environmental Documents</p> <ul style="list-style-type: none"> <li>• Water System Design and Environmental documents have been completed</li> <li>• Project Plans and Specifications are expected to be completed by Fall 2017</li> </ul> <p><b><u>Construction</u></b></p> <ul style="list-style-type: none"> <li>• Begin Construction by early 2018</li> <li>• Complete Construction by the end of 2018</li> </ul> <p><b><u>Estimated Project Cost</u></b></p> <ul style="list-style-type: none"> <li>• Phase 1: \$5 million</li> <li>• Lateral Connections : \$199,300</li> </ul> <p><b><u>Funding Secured to Date</u></b></p> <ul style="list-style-type: none"> <li>• \$5 million – State Water Resources Control Board for Phase 1</li> <li>• \$199,300 – Department of Water Resources for Laterals Connections</li> </ul>	<p>Includes the connection (intertie) of the Seville and Yettem water systems, water meters in Yettem, drilling of the new well in Yettem and the storage tank if it is not part of the Phase 1.</p> <p><b><u>Anticipated Project Schedule</u></b> Design, Plans, Specifications, and Environmental Documents to be completed by 2019.</p> <p><b><u>Construction</u></b></p> <ul style="list-style-type: none"> <li>• To begin in late 2019</li> </ul> <p><b><u>Estimated Project Cost</u></b></p> <ul style="list-style-type: none"> <li>• Phase 2: \$3.5 million</li> </ul>
PROJECT TEAM	

# YETTEM HAMLET PLAN

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## Sewer

Domestic sewer service in Yettem is provided by Tulare County (**see Table 14**) show the existing sewer connection, the capacity of each system, and the number of additional connections the system can accommodate for new development (Tulare County; January 2014 and Housing Element, May 2012). Mapping of the sewer systems is currently unavailable.

## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Yettem does not currently have a storm drainage system.”<sup>9</sup>

## Solid Waste

Solid waste disposal services for the Community of Yettem is provided by Pena’s Disposal, a private company. Solid waste generated in Yettem can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia

## Roads

“There are several roadways in Yettem that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

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<sup>9</sup> Action Program 9, Tulare County 2015 Housing Element

# YETTEM HAMLET PLAN

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>10</sup>

**Table 16 - Roads in Need of Major and Medium Repair in Yettem**

Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Road 140	Avenue 380 to SR 210	RCST
2	Road 140	SR 201 to Avenue 392	GRX

OLAY – overlay resurfacing operation  
CHIP – chip seal  
GRX – grind and remix

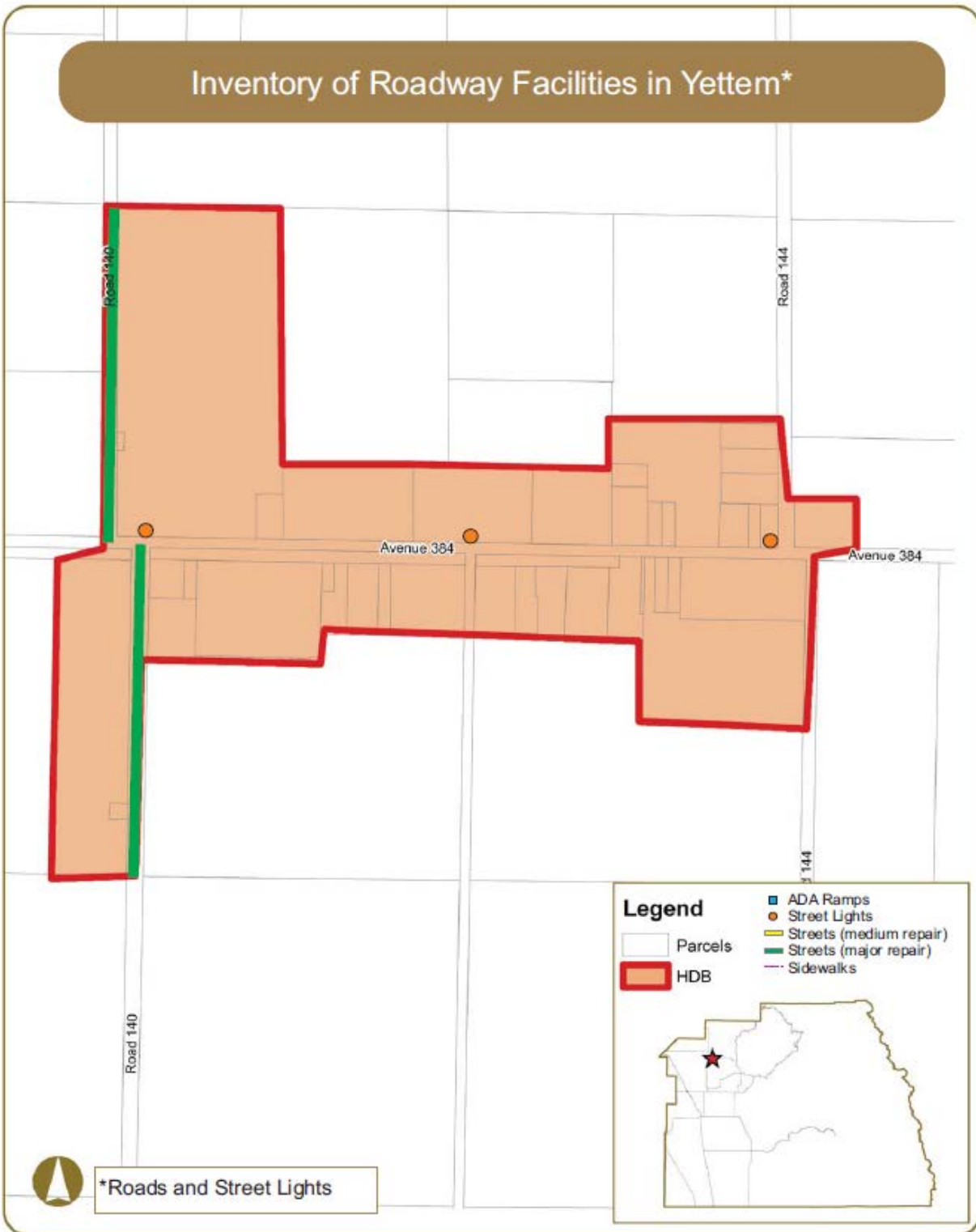
ACST – asphalt reconstruction  
RCST – cold mix reconstruction

(Source: County of Tulare Public Works, 2012)

<sup>10</sup> Action Program 9, Tulare County 2015 Housing Element

# YETTEM HAMLET PLAN

Figure 8 - Yettem Inventory of Roadway Facilities



# YETTEM HAMLET PLAN

## Street Lights

**Table 17** identifies the location of existing street lights that are maintained by Tulare County, in Yettem, as well as their specifications. **Figure 8** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type "W" represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete."<sup>11</sup>

**Table 17 - Existing Street Lights**

<b>Specifications of Existing Street Lights</b>								
No.	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole Type	Arm Direction	Utility
1	SR 201	Road 149	North Side	1460	9500	W	S	PG&E
2	SR 201	Road 142	North Side	N/A	9500	W	S	PG&E
3	SR 201	Road 144	NW corner	1459	9500	W	S	PG&E

(Source: Tulare County Public Works, March 2013)

## Sidewalks

There are currently no sidewalks located within Yettem. The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

## ADA Curb Ramps

"The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities. The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Yettem."<sup>12</sup>

<sup>11</sup> Action Program 9, Tulare County 2015 Housing Element

<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element

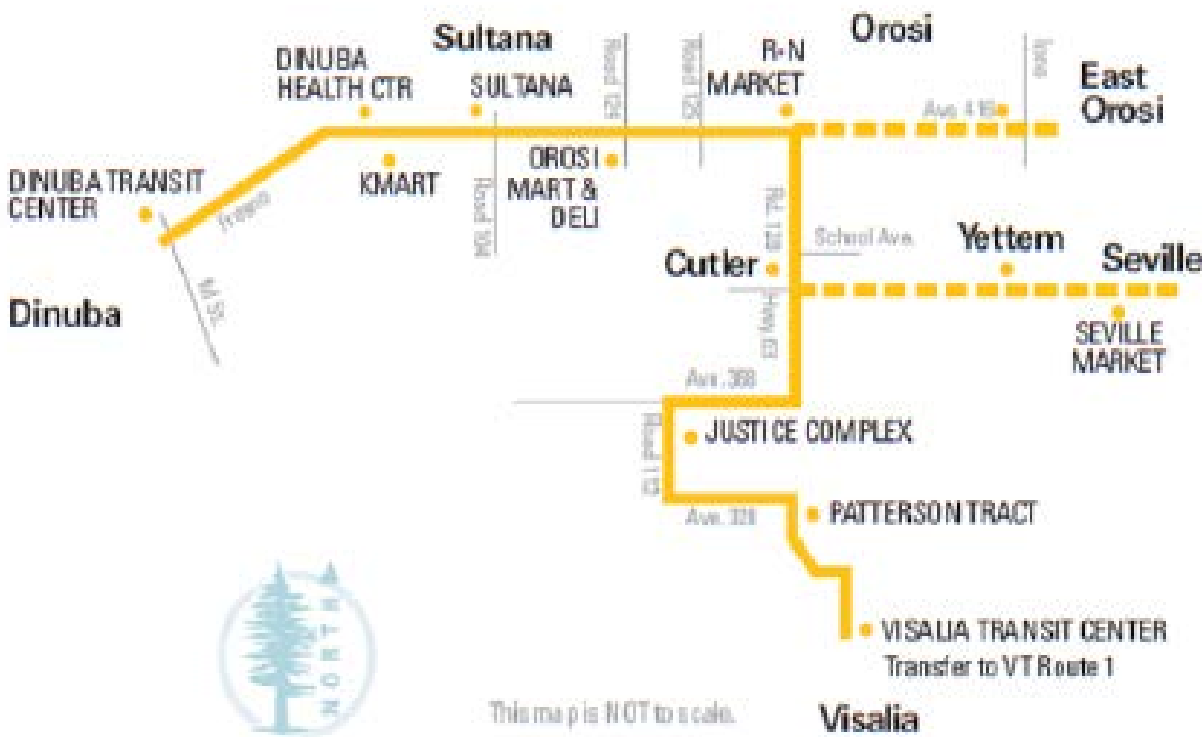
# YETTEM HAMLET PLAN

## Transit and Bus Stops

The County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Yettem is currently connected via TCAT Route 10/North County (locate just east at the Seville Market) to the City of Dinuba and the City of Visalia transit centers (**see Figure 9**). Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday.

**Figure 9 - TCAT - Fixed Route Service for Yettem**



Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Yettem, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.



# YETTEM HAMLET PLAN

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## Bicycle Facilities

The 2014 Regional Transportation Plan (adopted June 30, 2014) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. The 2014 RTP identified State Route 63 as a proposed Class II Bike Project<sup>13</sup>. In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are used seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future. The only bike route within or near the community is SR 63.

## AMTRAK

The Hanford AMTRAK station, located approximately 60 miles to the southwest in Kings County, is the closest station to Yettem providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 30 miles south. Fresno Yosemite International Airport (FAT), approximately 45 miles northwest of Yettem, is a principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field (BFL), Bakersfield's principal commercial airport, is approximately 90 miles southeast of Yettem and offers direct flights to several destinations

## State Route 99

State Route (SR) 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South. Yettem is located approximately 15 miles east of SR 99

## State Route 201

State Route (SR) 201 is a state highway in the Central Valley of California, United States. It connects SR 99 in Kingsburg with State Route (SR) 63 and SR 245 in Elderwood. SR 201 traverse Yettem east to west.

## **PUBLIC SERVICES**

### Sheriff

Police protection services are provided in Yettem by the Tulare County Sheriff's Department substation, located at 40765 Road 128, in Culter, approximately 8 miles northwest of Yettem. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, CA.

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<sup>13</sup> TCAG Action Element, TCAG 2014 Regional Transportation Plan. Page 3-89.

# YETTEM HAMLET PLAN

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## Fire

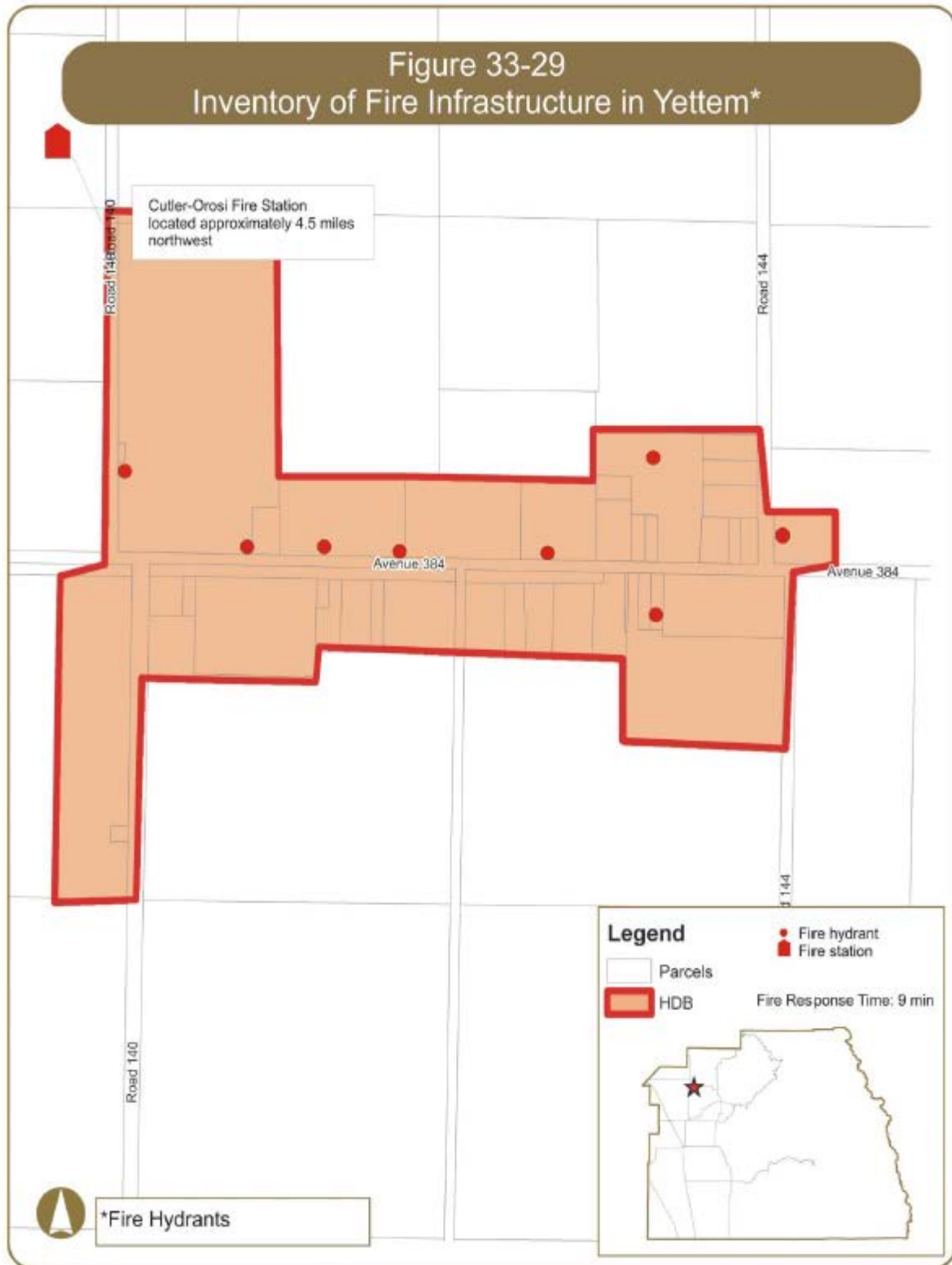
Fire protection and emergency medical services are provided for Yettem by the Tulare County Fire Department. The community is served by Tulare Fire Station #4, located at 40779 Road 128, in Culter. Patrol 4, Engine 4 and Engine 204 is assigned to this location.

Eight fire hydrants are found within Yettem. These fire hydrants are located within the County right-of-way. (see **Table 18**). **Figure 10** displays Existing Fire Hydrants in Yettem.

<b>Table 18 - Existing Fire Hydrants</b>	
<b>No.</b>	<b>Location</b>
1	Road 140 north of Avenue 384
2	Avenue 384 east of Road 140
3	Avenue 384 west of Road 144
4	Road 144 north of Avenue 384

# YETTEM HAMLET PLAN

Figure 10 - Inventory of Fire Infrastructure In Yettem



# YETTEM HAMLET PLAN

## Schools

Schools are essential component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Yettem Hamlet Plan Area is within the Stone Corral Elementary School District. The District provides K-8 grade education in Seville. High School students attend high school in Delano. Yettem is within the Visalia Unified School District.

## Libraries

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>14</sup> The closest library is located in Orosi, four miles to the northeast and the Ivanhoe Branch, four miles to the south. (see Table 19)

Table 19 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Orosi	Orosi Branch 12646 Avenue 416 Orosi, CA 93647	Wednesday, Thursday, Friday 9:00 am - 1:00 pm 2:00 p.m. – 6:00 p.m.
Ivanhoe	Ivanhoe Branch 15964 Heather Ivanhoe, CA 93235	Tuesday and Friday 9:00 a.m. – 1:00 p.m. 2:00 p.m. – 6:00 p.m.

Library hours current as of September 2017

## Parks

In the community of Yettem there is no safe public green space for children to play and adults to be physically active. The nearest park is located 6 miles away in the city of Orosi. Residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and concerned for the health of their children from lack of physical activity that comes from not having a safe place to be physically active. Residents expresses that having a community park is a major priority for them.

Presently there are no County owned/operated public parks in Yettem. The closest public park to the community is Ledbetter Park, located one mile northwest of Culter on Road 124/State Route 63.

# THE YETTEM HAMLET PLAN

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets

<sup>14</sup> General Plan Background Report, page 7-96

# YETTEM HAMLET PLAN

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and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>15</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>16</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation and Transportation Conditions

Yettem is a small agricultural community located near State Route 63 and 201. The major rural collector road is Avenue 384 (SR 201). There are no proposed major streets as part of this Hamlet Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

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<sup>15</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG ), June 2014. Page 3-54.

<sup>16</sup> Tulare County General Plan Background Report, page 5-7.

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## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

## **GOALS, OBJECTIVES AND POLICIES**

This Section of the Yettem Hamlet Plan describes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Hamlet Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Yettem to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Yettem Hamlet Plan, and input received from Yettem citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### **Community Development**

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned shopping areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.

# YETTEM HAMLET PLAN

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5. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
6. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
7. The County shall work with the Schools to provide safe routes to school.
8. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Yettem Community Plan.
9. The Yettem Hamlet Plan should be reviewed every five years to determine if amendments are appropriate.
10. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to initiate home maintenance/repair programs in Yettem.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Yettem.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.



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## Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

## Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

## Policies:

1. Encourage the Yettem to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

## Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## **Environmental Quality and Public Safety**

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Yettem citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

## Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.



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Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Protect Agricultural Lands:

Land within the respective Yettem, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.

The following criteria shall be used to determine when conversion to urban use is appropriate:

- The property is not subject to an agricultural preserve contract;
- Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and

- At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
- Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Yettem.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Yettem.

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## ASSESSMENT OF LAND NEEDS

Within the existing 312 acre Yettem Hamlet Development Boundary, agricultural activities, such as orchards and pasture, currently constitute (7%) of the HDB Area.

The ratio of urbanized acres per person is calculated by dividing the year 2015 population of 300 by 55.9 urbanized acres (area within the HDB), which equals 5.3 persons per urbanized acre (see **Tables 20 and 21**) showing population projections). Projecting the population at a 1.3% growth rate adds 64 persons by year 2030.

### Population Growth Forecast

Table 20 - Yettem Population Projections			
Growth Rate	2015	2020	2030
0.013	300	320	364

### Demand Forecast

To determine whether there is enough land within the exiting HDB to accommodate anticipated growth within the community, the population growth and land use projections in Year 2030 were compared to the vacant land available within the HDB.

### Population and Housing Units

The year 2015 baseline population and was determined by projecting the 2015 American Community Survey (Survey)<sup>17</sup> data population by an annual growth rate of 1.3% annually. The Survey indicated that in Year 2015 the community had 80 dwelling units (including vacant dwellings) with a population of 300. At an annual growth rate of 1.3%, the projected housing units are 85 and 97 in years 2020 and 2030, respectively, and projected population is 324 and 364 in years 2020 and 2030, respectively.

Table 21 - Population Projections		
Year	Population	Growth (%)
2015	300	0.013
2016	304	0.013
2017	308	0.013
2018	312	0.013
2019	316	0.013
2020	320	0.013
2021	324	0.013
2022	328	0.013
2023	333	0.013
2024	337	0.013
2025	341	0.013
2026	346	0.013
2027	350	0.013
2028	355	0.013
2029	359	0.013
2030	364	0.013

The total land area developed in these existing residential units was provided by the Tulare County Resource Management Agency Geographic Information System (GIS) division. There are currently approximately 312 acres within the HDB that are developed for residential uses. Assuming that the land area needed to accommodate future population increases is consistent with annual population growth projections, the land use growth projections are also assessed at an annual growth rate of 1.3%.

<sup>17</sup> See: [http://factfinder.census.gov/faces/nav/jsf/pages/community\\_facts.xhtml](http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml).

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## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

### Infrastructure

In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

### Use Permits

There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

### Education

Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Table 22 - Education Attainment			
Educational Attainment	California	Tulare County	Yettem
Population 25 years and over	25,257,858	260,964	149
Less than 9th grade	10.0%	20.8%	63.8%
9th to 12th grade, no diploma	8.2%	10.9%	16.1%
High school graduate (& equivalency)	20.7%	25.2%	20.1%
Some college, no degree	21.8%	22.3%	0.0%
Associate's degree	7.8%	7.0%	0.0%
Bachelor's degree	19.8%	9.5%	0.0%
Graduate or professional degree	11.6%	4.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates

Based on the 2011-2015 American Community Survey (see Table 22), the educational barrier in Yettem begins in grade school. Of the adults age 25 and older, 63.8% had an educational level of less than 9<sup>th</sup> grade. A lack of education could limit the types of jobs that these adults would be qualified to undertake. Improving educational attainment should begin in elementary school. As part of the Safe Routes to Schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This would provide a safer and easier route for children to get to and from school.

### Health Care

Health care is important for economic development as businesses need healthy employees. The nearest medical offices are located in Cutler/Orosi.

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

Based on the forecasted growth and the recommended Hamlet Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (see **Table 23**) and Zoning Plan map (see **Table 24**) have been updated.

<b>Table 23 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	55.1
Right-of Ways	4.8
Yettem HDB	59.9

<b>Table 24 - Proposed Zoning Districts</b>	
Proposed Zones	Acres
A-40	6.5
C-2-MU	29.9
P-O	5.5
R-A	13.1
Unclassified (right-of-Way)	
Yettem HDB	59.9

## IMPLEMENTATION STRATEGY

The purpose of this section is to describe a proposed approach to implement the general plan recommendations contained in the Yettem Hamlet Plan. The following components comprise the Yettem Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

### Zoning District Changes

As part of this Implementation Program for the Yettem Hamlet Plan, there are several proposed changes to existing zoning districts. These changes are described below.

### Chapter 16 of the Zoning Code

Revise Chapter 16 of the zoning code to limit the uses that require a use permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

### Mixed Use Overlay District

This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Yettem.

# YETTEM HAMLET PLAN

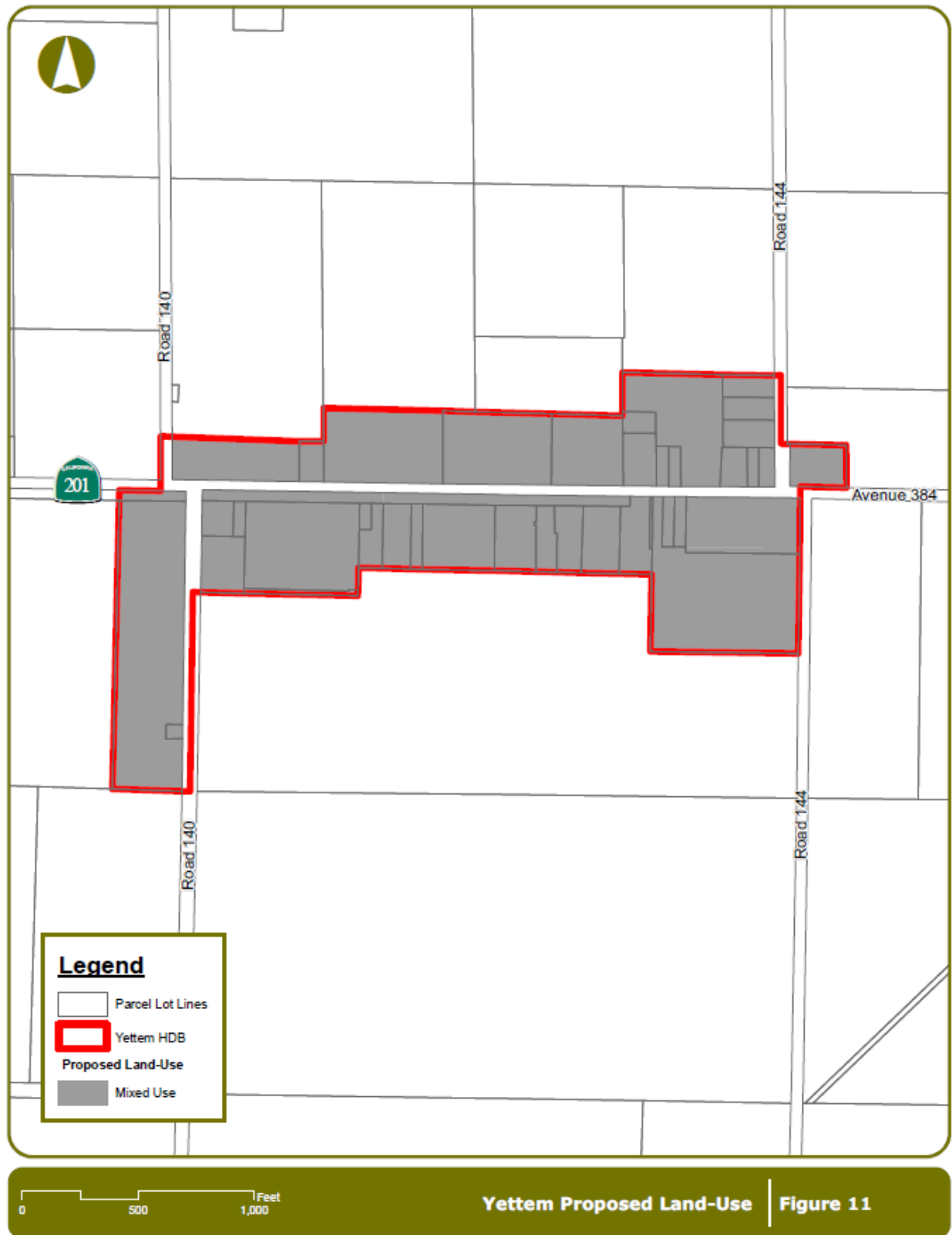
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## Zoning Map Update

The current Zoning Map for Yettem (**see Figure 12**) will be amended to be compatible with the Land Use Map (**see Figure 11**) outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**See Figure 13**) Yettem Proposed Zoning Districts.

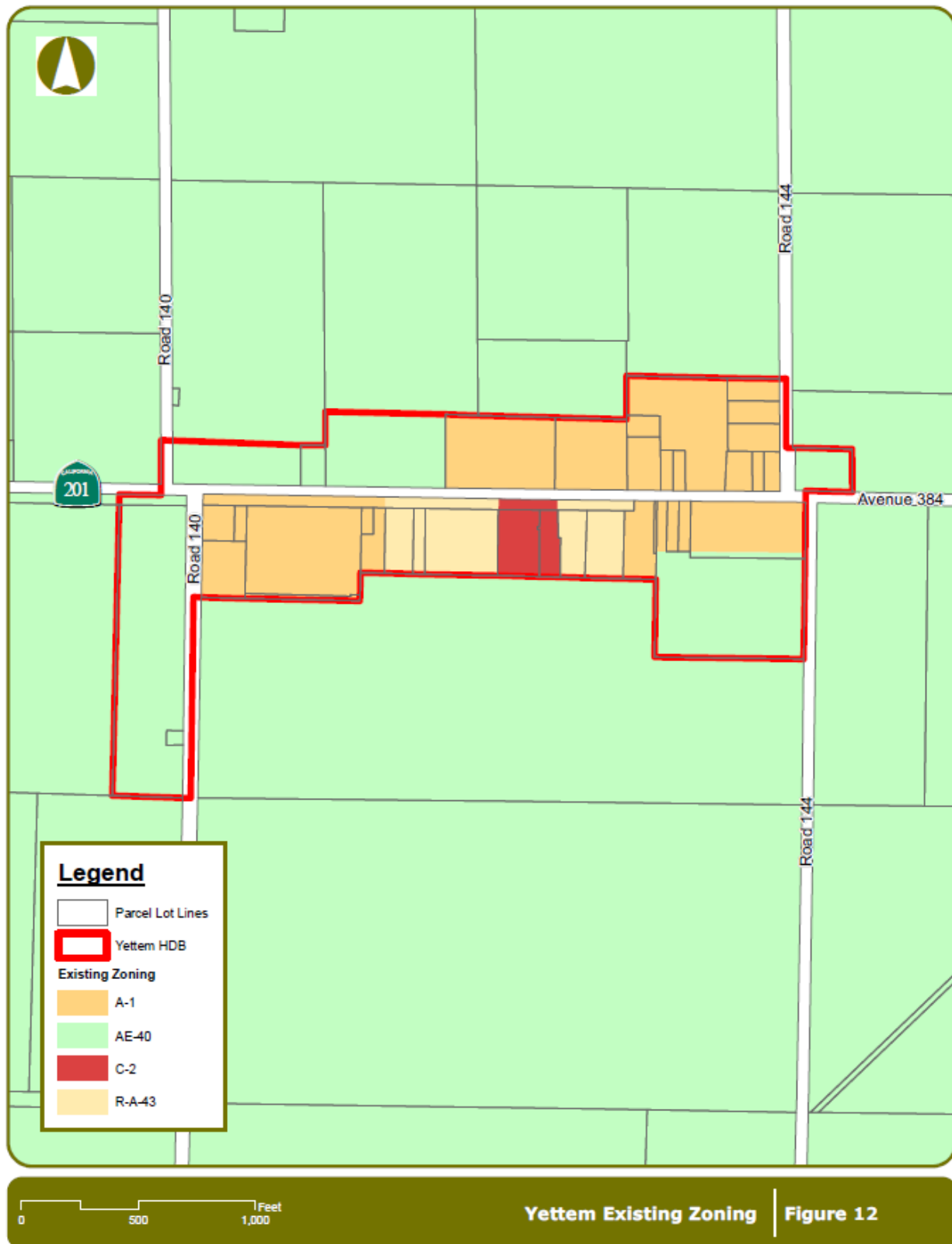
# YETTEM HAMLET PLAN

Figure 11 - Proposed Land Use Plan



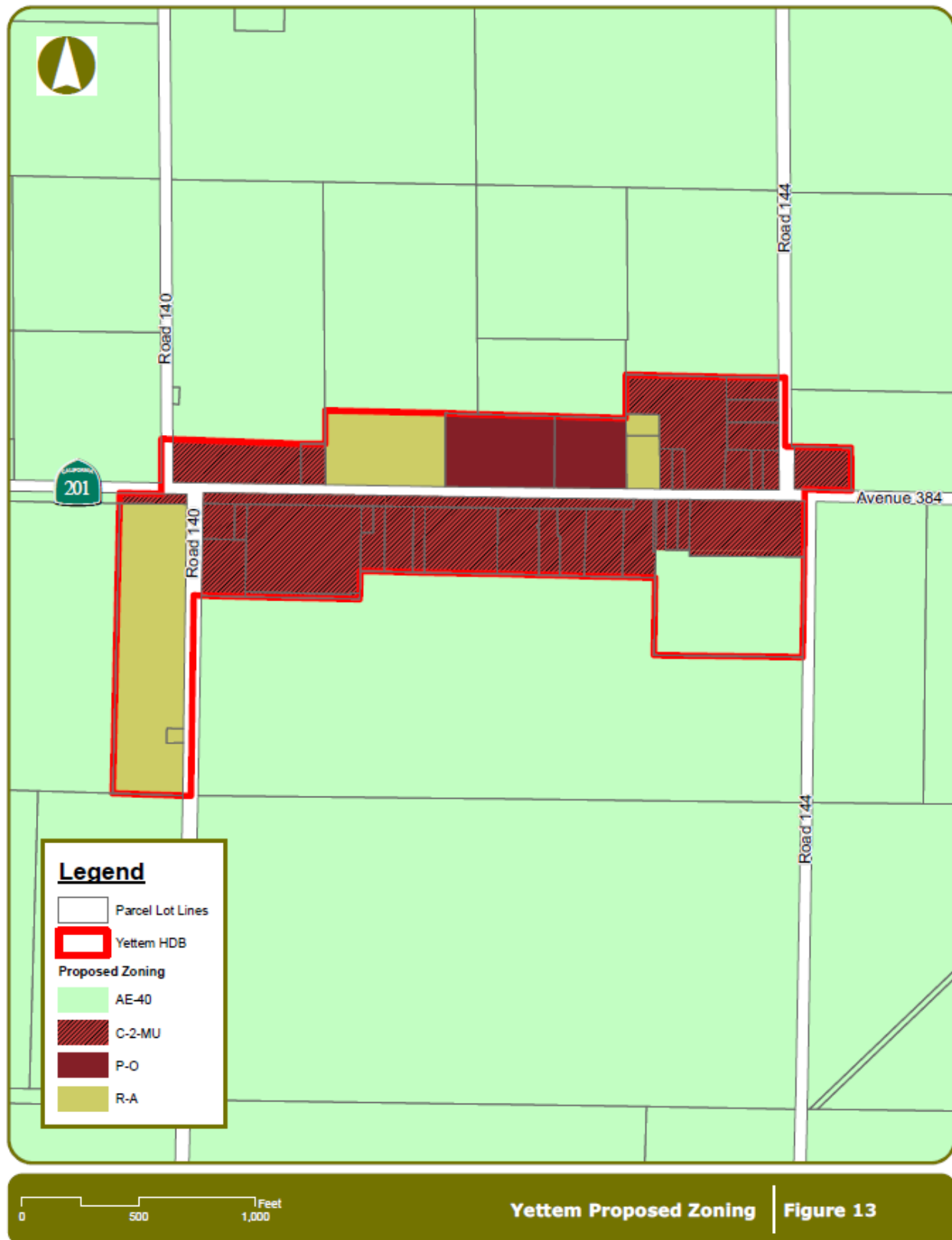
# YETTEM HAMLET PLAN

Figure 12 – Existing Zoning Districts



# YETTEM HAMLET PLAN

Figure 13 - Proposed Zoning Districts





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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Development Standards (Mixed Use Zoning Districts)

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## A-1 Use Permit Requirement Changes (Zone Change Text)

### H. Permitted Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	

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Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1

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Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1

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Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1

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Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1



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## A-2 *Mixed Use Overlay District (Zone Change Text)*

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The following regulations shall apply in the community of Yettem, unless otherwise provided in this Ordinance.

**PURPOSE**            **A.**     The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

**APPLICATION**       **B.**     This overlay zone only applies to the community of Yettem.

**USE**                   **C.**     No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Yettem.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust,

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## DEVELOPMENT

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet

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in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Yettem. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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## **A-3     *Development Standards (Mixed Use Zoning District)***

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To promote Economic Development within the Yettem Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Yettem. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district. To promote Economic Development within the Yettem Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Yettem. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed use overlay district.

### **ARCHITECTURE**

A-1     Entries to buildings should be individualized and clearly identifiable.

A-2     Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3     Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4     The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5     Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6     Each building should have a defined base, body, and cap segment

A-7     Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8     Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9     Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10    Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11    Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

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A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance.

Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.



# YETTEM HAMLET PLAN

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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.



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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

# YETTEM HAMLET PLAN

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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

# YETTEM HAMLET PLAN

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SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

# YETTEM HAMLET PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

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## APPENDICES

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9366

General Plan Amendment GPA 17-035-Resolution No. 9367

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9368

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9369

Zoning District Map-Resolution No. 9370

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE HAMLET PLANS 2017 ) RESOLUTION NO. 9366  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, AND PART III )  
AMENDMENTS (GPA 17-035) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Hamlet Plans 2017 Update which consists of General Plan Amendment No. GPA 17-035 amendment to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), and Transportation and Circulation, and Part III amending the Tulare County General Plan to adopt Hamlet Plans and Hamlet Development Boundaries, (Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024), Change of Zones (Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map), for an amendment PZC 17-039 to Section 16 of Ordinance No. 352, to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundary, and Zone Ordinance Amendment No. PZC 17-038 establishing a Mixed Use Combining Zone in Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem (incorporated by reference herein). The unincorporated Hamlet Communities are located within the Mount Diablo Base & Meridian as follows: Allensworth, Sections, 4, 5, 8, 9, 15, & 16, Township 24S, Range 25E, Delft Colony, Sections 22, 23, 26, & 27 Township 16S, Range 23E, East Tulare Villa, Sections 8, & 9, Township 20S, Range 25E, Lindcove, Section 17, Township 18S, Range 27E, Monson Sections 34, & 35, Township 16 south, Range 24 East. Seville, Sections 1, 2 & 35, Township 17S, Range 25E,

Teviston, Sections 8, 9, 16, 17, 21, & 22, Township 23S, Range 25E, Tonyville, Section 30, Township 23S, Range 25E, Waukena, Sections 5, 31, & 32 Township 21S, Range 23E, West Goshen Sections 22 & 23, Township 18 south, Range 23 East, Yettem, Sections 3, 4, 33, & 34 Townships, 16S & 17S Range 25E; and,

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element (Open Space), and Transportation & Circulation Element; (2) add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Hamlet Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Hamlet Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-035 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management (Open Space) and the Transportation and Circulation Element; Change of Zone No. PZC 17-038 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for Allensworth Change of Zone No. PZC 17-037 (Zoning District Map), Delft Colony Change of Zone No. PZC 17-015 (Zoning District Map), East Tulare Villa Change of Zone No. PZC 17-016 (Zoning District Map), Lindcove Change of Zone No. PZC 17-017 (Zoning District Map), Monson Change of Zone No. PZC 17-018 (Zoning District Map), Seville Change of Zone No. PZC 17-019 (Zoning District Map), Teviston Change of Zone No. PZC 17-020 (Zoning District Map), Tonyville Change of Zone No. PZC 17-021 (Zoning District Map), Waukena Change of Zone No. PZC 17-022 (Zoning District Map), West Goshen Change of Zone No. PZC 17-023 (Zoning



District Map, Yettem Change of Zone No. PZC 17-024 (Zoning District Map) within the respective Hamlet Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-039 an amendment to Section 16 to allow additional by-right uses in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Hamlet Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Hamlet Plans;
  - b. Encourage infill development within Hamlet Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Hamlet Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The Hamlet planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;

- b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on October 26, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Hamlet Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Hamlet Plans 2017 Update General Plan Amendment 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014; Change of Zone No. PZC 17-037 (Zoning District Map); Delft Colony General Plan Amendment No. GPA 17-015; Change of Zone No. PZC 17-015 (Zoning District Map); East Tulare Villa General Plan Amendment No. GPA 17-016; Change of Zone No. PZC 17-016 (Zoning District Map); Lindcove General Plan Amendment No. GPA 17-017; Change of Zone No. PZC 17-017 (Zoning District Map); Monson General Plan Amendment No. GPA 17-018; Change of Zone No. PZC 17-018 (Zoning District Map); Seville General Plan Amendment No. GPA 17-019; Change of Zone No. PZC 17-019 (Zoning District Map); Teviston General Plan Amendment No. GPA 17-020; Change of Zone No. PZC 17-020 (Zoning District Map); Tonyville General Plan Amendment No. GPA 17-021; Change of Zone No. PZC 17-021 (Zoning District Map); Waukena General Plan Amendment No. GPA 17-022; Change of Zone No. PZC 17-022 (Zoning

District Map); West Goshen General Plan Amendment No. GPA 17-023; Change of Zone No. PZC 17-023 (Zoning District Map); Yettem General Plan Amendment No. GPA 17-024; Change of Zone No. PZC 17-024 (Zoning District Map); Change of Zone No. PZC 17-038 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-039 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Planning Framework, Environmental Resources Management (Open Space), Transportation & Circulation, and Part III (Hamlet Plans) has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,

2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,
4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit “B” that are restrictive and applied only to the Hamlet Plans 2017 Update. Therefore, the public will benefit from the Hamlet Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Gong, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE )  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT, PART III HAMLET PLANS, OF THE ) RESOLUTION NO. 9367  
TULARE COUNTY GENERAL PLAN FOR THE )  
HAMLET PLANS 2017 UPDATE GPA 17-035 )  
INCLUSIVE OF GPA's GPA 17-014, GPA 17-015, GPA 17-016, )  
GPA 17-017, GPA 17-018, GPA 17-019, GPA 17-020, )  
GPA 17-021, GPA 17-022, GPA 17-023, GPA 17-024 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Framework Element; Land Use Elements, , Environmental Resources Management (Open Space) Transportation & Circulation and Part III Hamlet Plan of the Tulare County General Plan as set forth in Exhibits "A to K" for the Hamlet Plans 2017 Update General Plan Amendment No. GPA 17-035; inclusive of Allensworth General Plan Amendment No. GPA 17-014, Delft Colony General Plan Amendment No. GPA 17-015, East Tulare Villa General Plan Amendment No. GPA 17-016, Lindcove General Plan Amendment No. GPA 17-017, Monson General Plan Amendment No. GPA 17-018, Seville General Plan Amendment No. GPA 17-019, Teviston General Plan Amendment No. GPA 17-020, Tonyville General Plan Amendment No. GPA 17-021, Waukena General Plan Amendment No. GPA 17-022, West Goshen General Plan Amendment No. GPA 17-023, Yettem General Plan Amendment No. GPA 17-024 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Hamlet communities as part of the Hamlet Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on October 26, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Hamlet Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017 and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation & Circulation and Part III Hamlet Plans of the Tulare County General Plan for the Hamlet Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Hamlet Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Hamlet Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to adopt Hamlet Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;
4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the

adoption of Hamlet development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;

5. The Amendment to Part I Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Hamlet Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the Hamlet community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Planning Framework, Land Use, Environmental Resources Management, Transportation & Circulation and Public Facilities & Services Elements, and Part III Hamlet Plans of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-035 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-035 the Hamlet Plans 2017 Update set forth in attached Exhibits "A-K" which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Millies, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES:           Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES:           None  
ABSTAIN:       None  
ABSENT:       None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

- Exhibit "A" – 2017 Allensworth Hamlet Plan (Part III)
- Exhibit "B" – 2017 Delft Colony Hamlet Plan (Part III)
- Exhibit "C" – 2017 East Tulare Villa Hamlet Plan (Part III)
- Exhibit "D" – 2017 Lindcove Hamlet Plan (Part III)
- Exhibit "E" – 2017 Monson Hamlet Plan (Part III)
- Exhibit "F" – 2017 Seville Hamlet Plan (Part III)
- Exhibit "G" – 2017 Teviston Hamlet Plan (Part III)
- Exhibit "H" – 2017 Tonyville Hamlet Plan (Part III)
- Exhibit "I" – 2017 Waukena Hamlet Plan (Part III)
- Exhibit "J" – 2017 West Goshen Hamlet Plan (Part III)
- Exhibit "K" – 2017 Yettem Hamlet Plan (Part III)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO )  
SECTION 18.9 "MU" MIXED USE COMBINING )  
ZONE OF ORDINANCE NO. 352 CONSISTENT ) RESOLUTION NO. 9368  
WITH THE ADOPTED HAMLET PLANS 2017 )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE )  
NO. PZC 17-038 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella, Ducor, El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable

Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT                     )  
TO SECTION 16.H OF ORDINANCE NO. 352            )  
TO ALLOW ADDITIONAL BY-RIGHT USES             )       RESOLUTION NO. 9369  
CONSISTENT WITH THE ADOPTED HAMLET            )  
PLANS, AS PROPOSED IN CHANGE                    )  
OF ZONE NO. PZC 17-039                            )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem, consistent with the adopted Hamlet Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Hamlet Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem ;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the Hamlet Communities of Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem , consistent with the adopted Hamlet Plans 2017 Update, consistent with the Hamlet Communities 2017.

The foregoing was adopted upon motion of Commissioner Gong, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot

NOES: None

ABSTAIN: None

ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	
IN THE HAMLET DEVELOPMENT BOUNDARIES	)	
OF ALLENSWORTH, DELFT COLONY,	)	
EAST TULARE VILLA, LINDCOVE, MONSON,	)	
SEVILLE, TEVISTON, TONYVILLE, WAUKENA,	)	
WEST GOSHEN, AND YETTEM	)	RESOLUTION NO. 9370
CONSISTENT WITH THE ADOPTED HAMLET	)	
PLANS 2017 UPDATE (GPA 17-035), AS	)	
PROPOSED IN CHANGE OF ZONE NO. PZC's	)	
PZC 17-037, PZC 17-015, PZC 17-016, PZC 17-017,	)	
PZC 17-018, PZC 17-019, PZC 17-020, PZC 17-021,	)	
PZC 17-022, PZC 17-023, AND PZC 17-024	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" Allensworth Rezoning Plan (PZC 17-037), to include the Hamlet Community of Allensworth, consistent with the adopted Allensworth Hamlet Plan 2017 Update, Exhibit "E" Delft Colony Rezoning Plan (PCZ 17-015), to include the Hamlet Community of Delft Colony, consistent with the adopted Delft Colony Hamlet Plan 2017 Update, Exhibit "H" East Tulare Villa Rezoning Plan (PCZ 17-016), to include the Hamlet Community of East Tulare Villa, consistent with the adopted East Tulare Villa Hamlet Plan 2017 Update, Exhibit "K" Lindcove Rezoning Plan (PCZ 17-017), to include the Hamlet Community of Lindcove, consistent with the adopted Lindcove Hamlet Plan 2017 Update, Exhibit "N" Monson Rezoning Plan (PCZ 17-018), to include the Hamlet Community of Monson, consistent with the adopted Monson Hamlet Plan 2017 Update, Exhibit "Q" Seville Rezoning Plan (PZC 17-019), to include the Hamlet Community of Seville, consistent with the adopted Seville Hamlet Plan 2017 Update, Exhibit "T" Teviston Rezoning Plan (PCZ 17-020), to include the Hamlet Community of Teviston, consistent with the adopted Teviston Hamlet Plan 2017 Update, Exhibit "W" Tonyville Rezoning Plan (PCZ 17-021), to include the Hamlet Community of Tonyville, consistent with the adopted Tonyville Hamlet Plan 2017 Update, Exhibit "Z" Waukena Rezoning Plan (PCZ 17-022), to include the Hamlet Community of Waukena, consistent with the adopted Waukena Hamlet Plan 2017 Update, Exhibit "CC" West Goshen Rezoning Plan (PCZ 17-023), to include the Hamlet Community of West Goshen, consistent with the adopted West Goshen Hamlet Plan 2017 Update, Exhibit "FF" Yettem Tract Rezoning Plan (PCZ 17-024), to include the Hamlet Community of Yettem, consistent with the adopted Yettem Hamlet Plan 2017 Update, and

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and



WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on October 26, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on November 8, 2017, and

WHEREAS, at the Tulare County Planning Commission meeting, a public hearing was held and public testimony was presented by two Allensworth residents and a representative from Leadership Council who supported the project was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Hamlet Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Hamlet Plans 2017 Update (GPA 17-035) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Communities;

3. The proposed changes in zone district designations identified in the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the Allensworth Change of Zone No. PZC 17-037, Delft Colony Change of Zone No. PZC 17-015, East Tulare Villa Change of Zone No. PZC 17-016, Lindcove Change of Zone No. PZC 17-017, Monson Change of Zone No. PZC 17-018, Seville Change of Zone No. PZC 17-019, Teviston Change of Zone No. PZC 17-020, Tonyville Change of Zone No. PZC 17-021, Waukena Change of Zone No. PZC 17-022, West Goshen Change of Zone No. PZC 17-023, Yettem Change of Zone No. PZC 17-024 an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" "N" "Q" "T" "W" "Z" "CC" "FF" Rezoning Plans consistent with the adopted Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Plans 2017 Update (GPA 17-035).

The foregoing was adopted upon motion of Commissioner Aguilar, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on November 8, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano, Elliot  
NOES: None  
ABSTAIN: None  
ABSENT: None

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

Attachment 1

***Allensworth Change of Zone No. PZC 17-037***

Exhibit "A" – Zoning District Ordinance Map

Exhibit "B" – Zoning Map

Exhibit "C" – List of Affected Properties/Map

***Delft Colony Change of Zone No. PZC 17-015***

Exhibit "D" – Zoning District Ordinance Map

Exhibit "E" – Zoning Map

Exhibit "F" – List of Affected Properties/Map

***East Tulare Villa Change of Zone No. PZC 17-016***

Exhibit "G" – Zoning District Ordinance Map

Exhibit "H" – Zoning Map

Exhibit "I" – List of Affected Properties/Map

***Lindcove Change of Zone No. PZC 17-017***

Exhibit "J" – Zoning District Ordinance Map

Exhibit "K" – Zoning Map

Exhibit "L" – List of Affected Properties/Map

***Monson Change of Zone No. PZC 17-018***

Exhibit "M" – Zoning District Ordinance Map

Exhibit "N" – Zoning Map

Exhibit "O" – List of Affected Properties/Map

***Seville Change of Zone No. PZC 17-019***

Exhibit "P" – Zoning District Ordinance Map

Exhibit "Q" – Zoning Map

Exhibit "R" – List of Affected Properties/Map

***Teviston Change of Zone No. PZC 17-020***

Exhibit “S” – Zoning District Ordinance Map

Exhibit “T” – Zoning Map

Exhibit “U” – List of Affected Properties/Map

***Tonyville Change of Zone No. PZC 17-021***

Exhibit “V” – Zoning District Ordinance Map

Exhibit “W” – Zoning Map

Exhibit “X” – List of Affected Properties/Map

***Waukena Change of Zone No. PZC 17-022***

Exhibit “Y” – Zoning District Ordinance Map

Exhibit “Z” – Zoning Map

Exhibit “AA” – List of Affected Properties/Map

***West Goshen Change of Zone No. PZC 17-023***

Exhibit “BB” – Zoning District Ordinance Map

Exhibit “CC” – Zoning Map

Exhibit “DD” – List of Affected Properties/Map

***Yettem Change of Zone No. PZC 17-024***

Exhibit “EE” – Zoning District Ordinance Map

Exhibit “FF” – Zoning Map

Exhibit “GG” – List of Affected Properties/Map

**Attachment 2**

***Hamlet Plan Rezoning Full Ordinance***

Exhibit A - *Hamlet Plan Rezoning Full Ordinance*

Appendix B – Board of Supervisors Resolution No. 2017-0976

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF HAMLET AND	)	
COMMUNITY PLANS 2017 UPDATE	)	
(GENERAL PLAN AMENDMENT 17-035)	)	Resolution No. <u>2017-0976</u>
AND AMENDMENTS TO THE TULARE	)	Ordinance No. <u>3515, 3516, 3517,</u>
ZONING ORDINANCE NO. 352	)	<u>3518, 3519, and 3520</u>

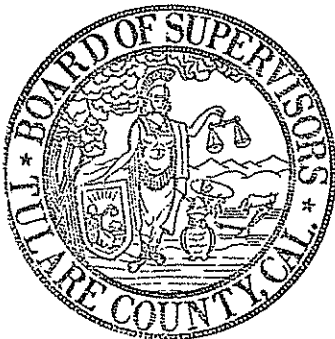
UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD DECEMBER 5, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS

NOES: NONE

ABSTAIN: NONE

ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:

  
Deputy Clerk

\* \* \* \* \*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Hamlet and Community Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Hamlet and Community Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Hamlet and Community Plans 2017 Update (GPA 17-035); and

3. Adopted one General Plan Amendment No. GPA 17-035, for the Hamlet and Community Plans 2017 Update inclusive of the Allensworth Hamlet Plan (GPA 17-014), Delft Colony Hamlet Plan (GPA 17-015), East Tulare Villa Hamlet Plan (GPA 17-016), Lindcove Hamlet Plan (GPA 17-017), Monson Hamlet Plan (GPA 17-018), Seville Hamlet Plan (GPA 17-019), Teviston Hamlet Plan (GPA 17-020), Tonyville Hamlet Plan (GPA 17-021), Waukena Hamlet Plan (GPA 17-022), West Goshen Hamlet Plan (GPA 17-023), Yettem Hamlet Plan (GPA 17-024), Alpaugh Community Plan (GPA 17-004), East Orosi Community Plan (GPA 17-034), London Community Plan (GPA 17-008), Richgrove Community Plan (GPA 17-011), Sultana Community Plan (GPA 17-012), and Amendments to Part III of the Tulare County General Plan to adopt Hamlet and Community Plans, and to Part I General Plan Amendment to the Planning Framework, Land Use, Environmental Resources Management (Open Space), Transportation and Circulation Elements, consistent with the Hamlet Plans 2017 Update; and
4. Waived the final reading and adopt Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Hamlet and Community Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-038) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-039) to Section 16 to allow additional by-right uses within the Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, and Yettem Hamlet Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Allensworth (PZC 17-037), Delft Colony (PZC 17-015), East Tulare Villa (PZC 17-016) Lindcove (PZC 17-017), Monson (PZC 17-018), Seville (PZC 17-019), Teviston (PZC 17-020), Tonyville (PZC 17-021), Waukena (PZC 17-022), West Goshen (PZC 17-023), Yettem (PZC 17-024), consistent with the Hamlet Plans 2017 Update.
  - D. Zone Ordinance Amendment (PZC 17-041) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - E. Zone Ordinance Amendment (PZC 17-042) to Section 16 to allow additional by-right uses within the Alpaugh, East Orosi, London, Richgrove, and Sultana Urban Development Boundaries.
  - F. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for Alpaugh (PZC 17-004), East Orosi (No. PZC 17-005), London (PZC 17-008), Richgrove (PZC 17-011), Sultana (PZC 17-012), consistent with the Community Plans 2017 Update; and
5. Adopted the findings of approval set forth for Hamlets in Planning Commission Resolution No. 9366 (Addendum to EIR), Planning Commission Resolution No.

9367 (Hamlet Plans 2017 Update), Planning Commission Resolution No. 9368 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9369 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9370 (Zoning District Ordinance Maps), and Communities in Planning Commission Resolution No. 9371 (Addendum to EIR), Planning Commission Resolution No. 9372 (Community Plans 2017 Update), Planning Commission Resolution No. 9373 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9374 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9375 (Zoning District Ordinance Maps); and

6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add Allensworth, Delft Colony, East Tulare Villa, Lindcove, Monson, Seville, Teviston, Tonyville, Waukena, West Goshen, Yettem, Alpaugh, East Orosi, London, Richgrove, and Sultana to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. Seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Hamlet and Community Plans, and associated project documents; and
10. Adopted the findings of approval set forth in Planning Commission Resolution 9376 and approved the Strategic Growth Council Final Report; and
11. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

RMA

HAR  
12/05/2017



## **Appendix D: Legacy Plans**

**El Monte Mobile Village**

**Hypericum**

**Jovista**

**Matheny Tract**

**Tooleville**

## **El Monte Mobile Village Legacy Plan**

# EL MONTE MOBILE VILLAGE LEGACY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development and Planning Branch



# EL MONTE MOBILE VILLAGE LEGACY PLAN

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# EL MONTE MOBILE VILLAGE LEGACY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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# El Monte Mobile Village Legacy Plan

Adopted: October 17, 2017

Tulare County Board of Supervisors

Resolution No. 2017-0868

GPA 2017-A

PZC 17-032

GPA 17-025

PZC 17-025



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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# EL MONTE MOBILE VILLAGE LEGACY PLAN

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Michael C. Spata, CAO  
Eric Coyne, Deputy CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director  
Michael Washam, Associate Director  
Aaron Bock, Chief Planner, Project Processing  
Dave Bryant, Chief Planner, Special Projects  
Susan Simon, Planner III  
Velma Quiroz, Planning Tech II  
Johnson Vang, Engineer I

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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# EL MONTE MOBILE VILLAGE LEGACY PLAN

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# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## INTRODUCTION

An important objective in preparing a Legacy Plan for the El Monte Mobile Village is to develop a plan which can accurately reflect the needs and priorities of the El Monte Mobile Village community. El Monte Mobile Village was designated in the 2030 Tulare County General Plan (2012) through the SB 244 Study under GPA 2015-D. It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [for example wells, water distribution piping, and storage tanks], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

### Location

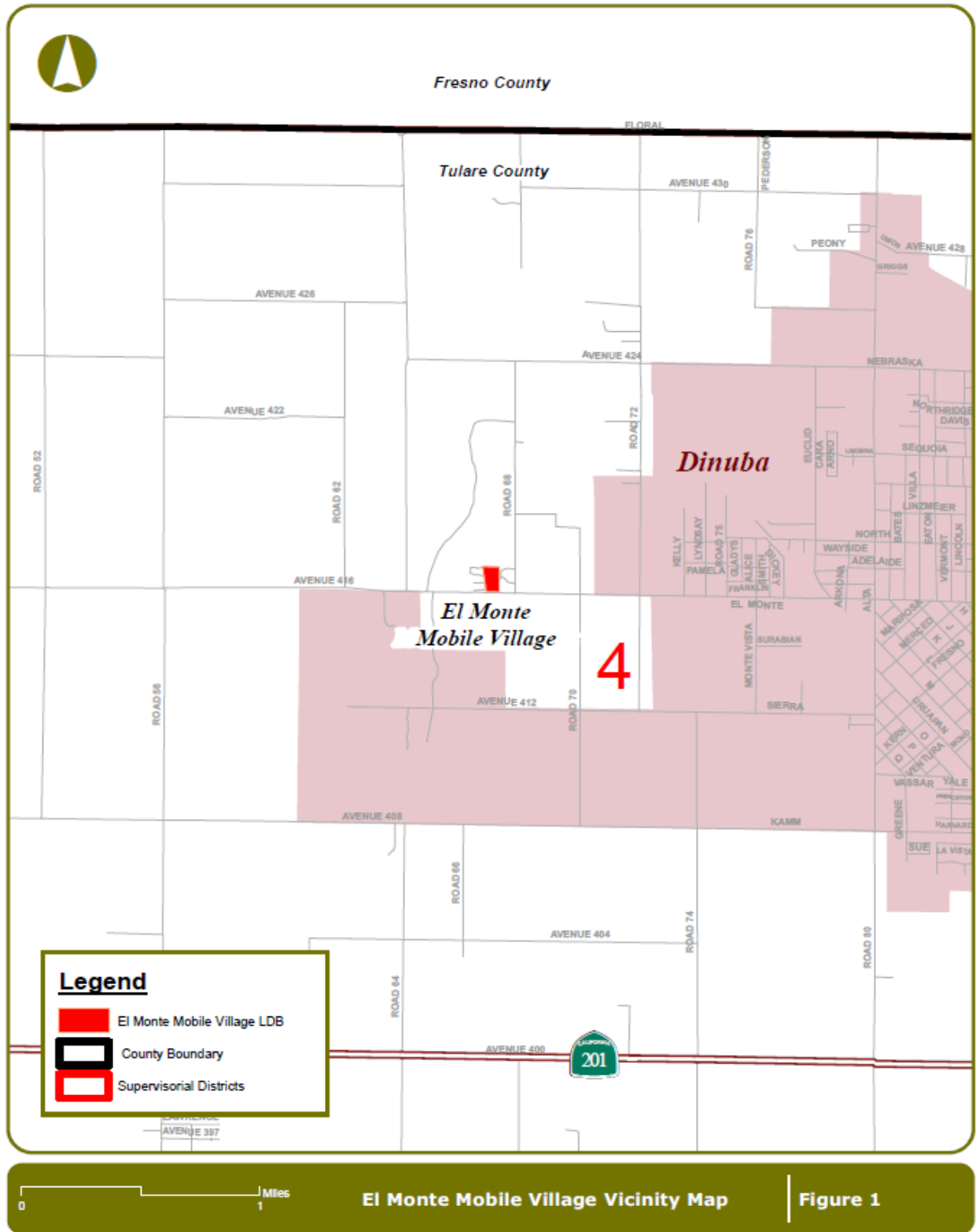
El Monte Mobile Village (**see Figure 1**) is Located on the northern part of Tulare County on the valley floor. El Monte Mobile Village is a small community comprised of mobilehomes located on Avenue 416 approximately one (1) mile west of the City of Dinuba. El Monte Mobile Village (**see Figure 2**) is in Section 12, Township 16 South, Range 23 East, MDB&M, and can be found within the Reedley, United States Geological Survey 7.5 minute topographic quadrangle. El Monte Mobile Village is located at an elevation of 337 feet above sea level, National Geodetic Vertical Datum. The coordinates of El Monte Mobile Village are: Latitude: 26.547156, Longitude: -119.424884. El Monte Mobile Village is located in Census Tract 3.01, Block Group 6 and is divided into three Census Blocks (6039, 6040 and 6041). The population is approximately 155 and is comprised of approximately 47 mobilehomes.

### City of Dinuba Sphere of Influence (SOI)

El Monte Mobile Village is located just outside the City Limits of Dinuba but within the Sphere of Influence (SOI) (**see Figure 3**). The City of Dinuba Urban Boundary Element defines the limits for extending city services and infrastructure so as to accommodate new development anticipated within the time frame of the General Plan. The element includes a 20-year Urban Development Boundary which is the city's ultimate physical boundary and service area for the next 20-years (2006-2026); a 10-year Urban Development Boundary which is the urbanizable area within which a full range of urban services will need to be extended to accommodate urban development. El Monte Mobile Village is within the city's 10 and 20-year Urban Development Boundary (UDB), the city's General Plan map designates the site as Residential – High Density.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 1 - Vicinity Map



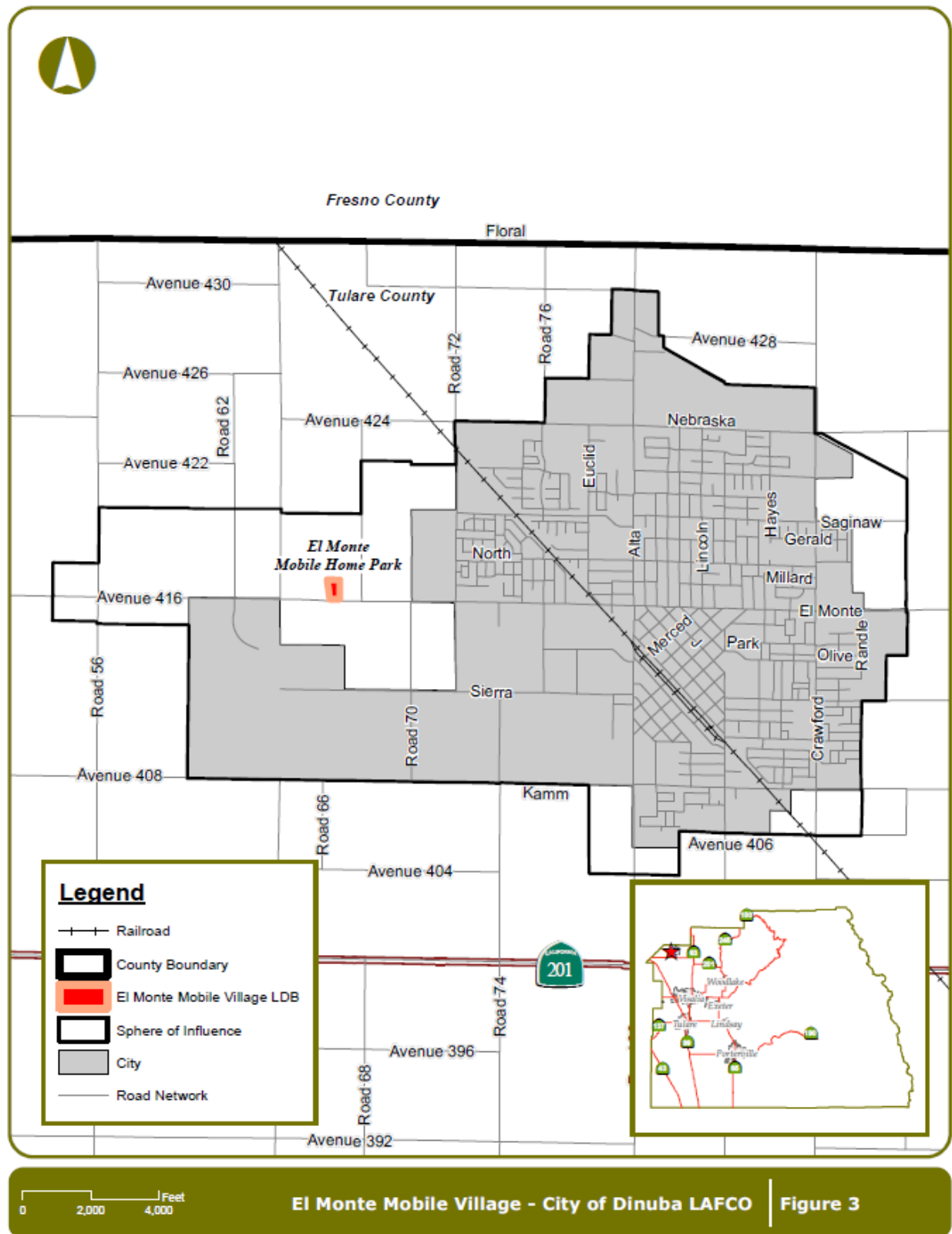
# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 2 - El Monte Mobile Village Aerial Map



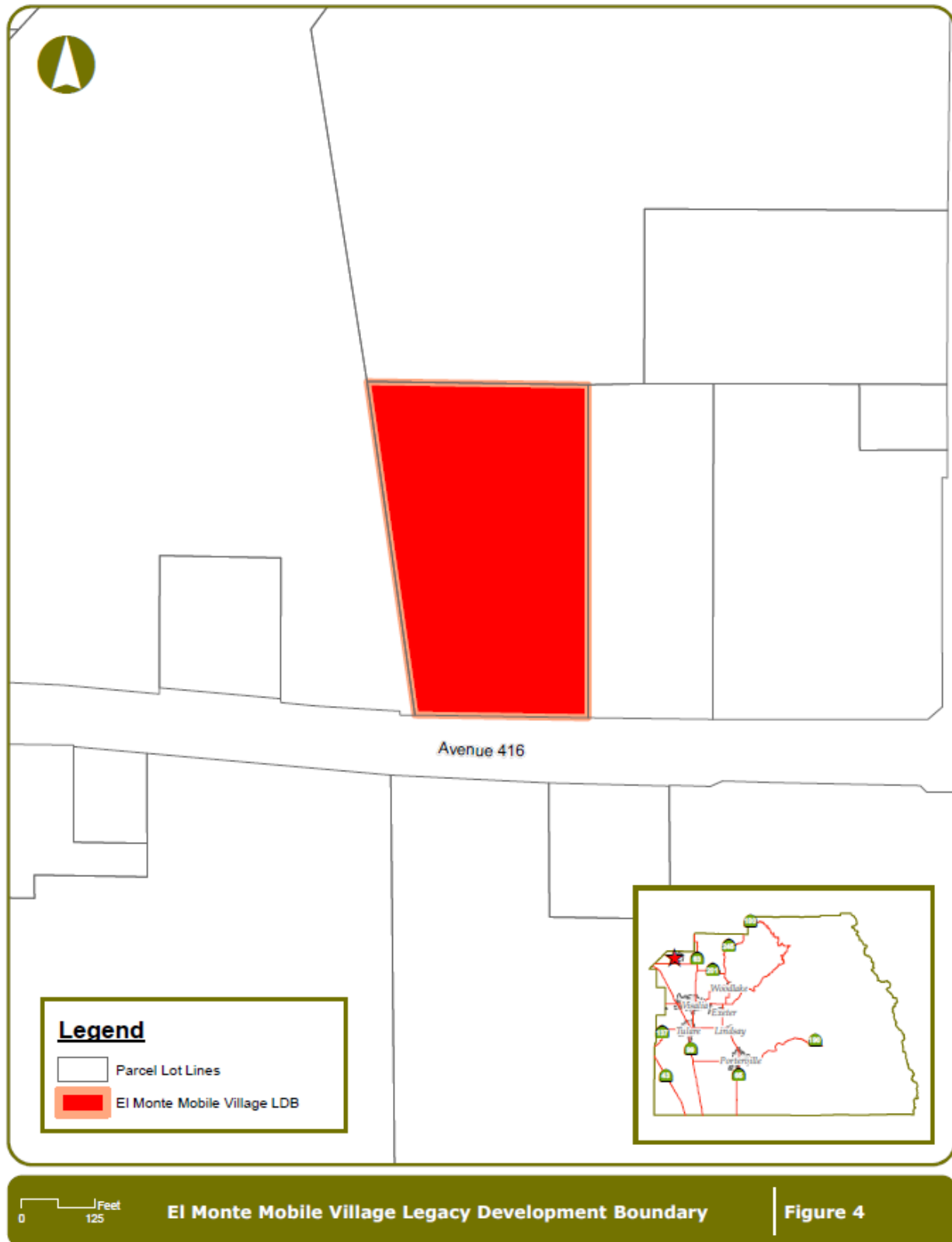
# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 3 - City of Dinuba Sphere of Influence



# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 4 - El Monte Mobile Village LDB





# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Legacy's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Legacy plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Legacy allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Legacy. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller Legacy such as El Monte Mobile Village.

### Population

El Monte Mobile Village is located within Census Tract 3.01, Block Group 6 and includes three separate Census Blocks (6039, 6040 and 6041). The Population for Census Tract 3.01 was 6,901 with 155 people living in El Monte Mobile Village. **(Census data: ACS 2015 5-year)**

### Median Age

El Monte Mobile Village's median age of 34.4 is lower than the median age of the State of California **(Census data: ACS 2015 5-year)** (See Table 1).

Table 1 - Median Age	
Geography	Median Age (years)
California	35.6
Tulare County	30.1
El Monte Mobile Village	34.4

## ECONOMIC CONDITIONS

### Employment in El Monte Mobile Village

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production.

### Median Household Income

In 2015, El Monte Mobile Village's median household income was \$44,569 **(Census data: ACS 2015 5)** whereas the State of California's median household income was \$61,818 **(see Table 2)**. However, the Medium Household Income of \$44,569 for El Monte Mobile Village is based on Census Tract 3.01 (a large geographical area) and is approximately 20% higher than the \$36,860 in the City of Dinuba.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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Table 2 - Median Household Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17, 876
City of Dinuba	\$36,860	\$36,707	\$13,221
El Monte Mobile Village	\$44,569		\$22,872

## Severely Disadvantaged Community

El Monte Mobile Village is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

## Household Size (Overcrowding)

In 2015 the average housing units and occupancy were 3.37 persons per household in Tulare County. **(Census data: ACS 2015 5-year).**

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## ENVIRONMENTAL CONDITIONS

### Natural Conditions

#### Wildlife<sup>2</sup>

A California Natural Diversity Database (CNDDDB) (see **Figure 5**) search conducted on May 24, 2017, indicates there are special status species within the Reedley Quadrant Species List (which includes the El Monte Mobile Village Planning Study Area) consisting of no animal species and one plant species: Valley Elderberry Longhorn Beetle (*Desmocerus californicus dimorphus*), which has been removed from the Federal/State list for Tulare County.

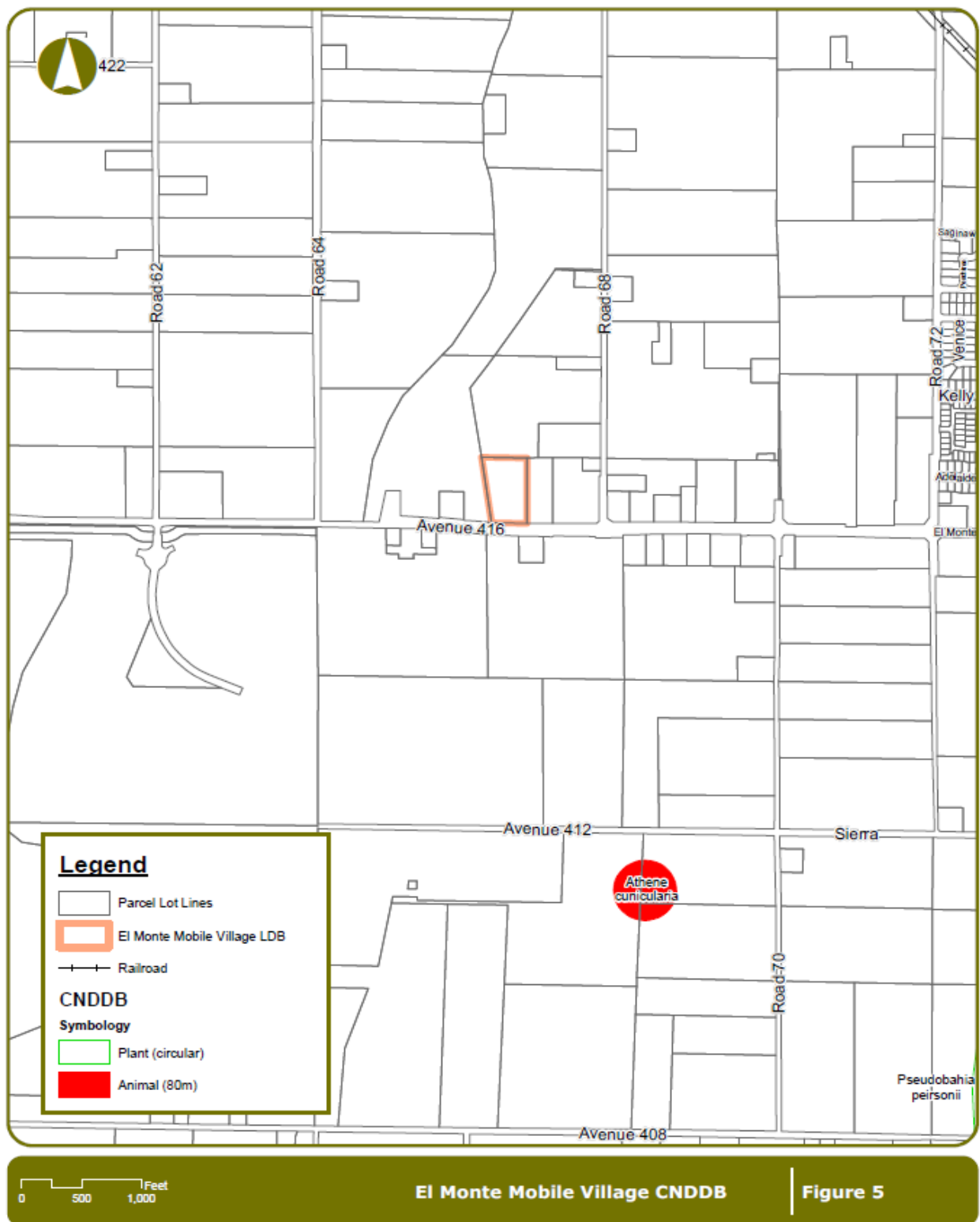
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<sup>1</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> accessed November 1, 2016

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 5 - CNDDDB Map Reedley Quad





# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect El Monte Mobile Village are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [El Monte Mobile Village] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>4</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County (**see Figure 6**), the following soil types are located throughout El Monte Mobile Village Legacy Development Boundary (LDB). The following description is provided for the above soil type:

**Delhi sandy loam, 0 to 2 percent slopes.** Consists of very deep, somewhat excessively drained soil; negligible to slow runoff, rapid permeability. Delhi soil is used for such crops as grapes, peaches, truck crops, alfalfa, and for homesites.

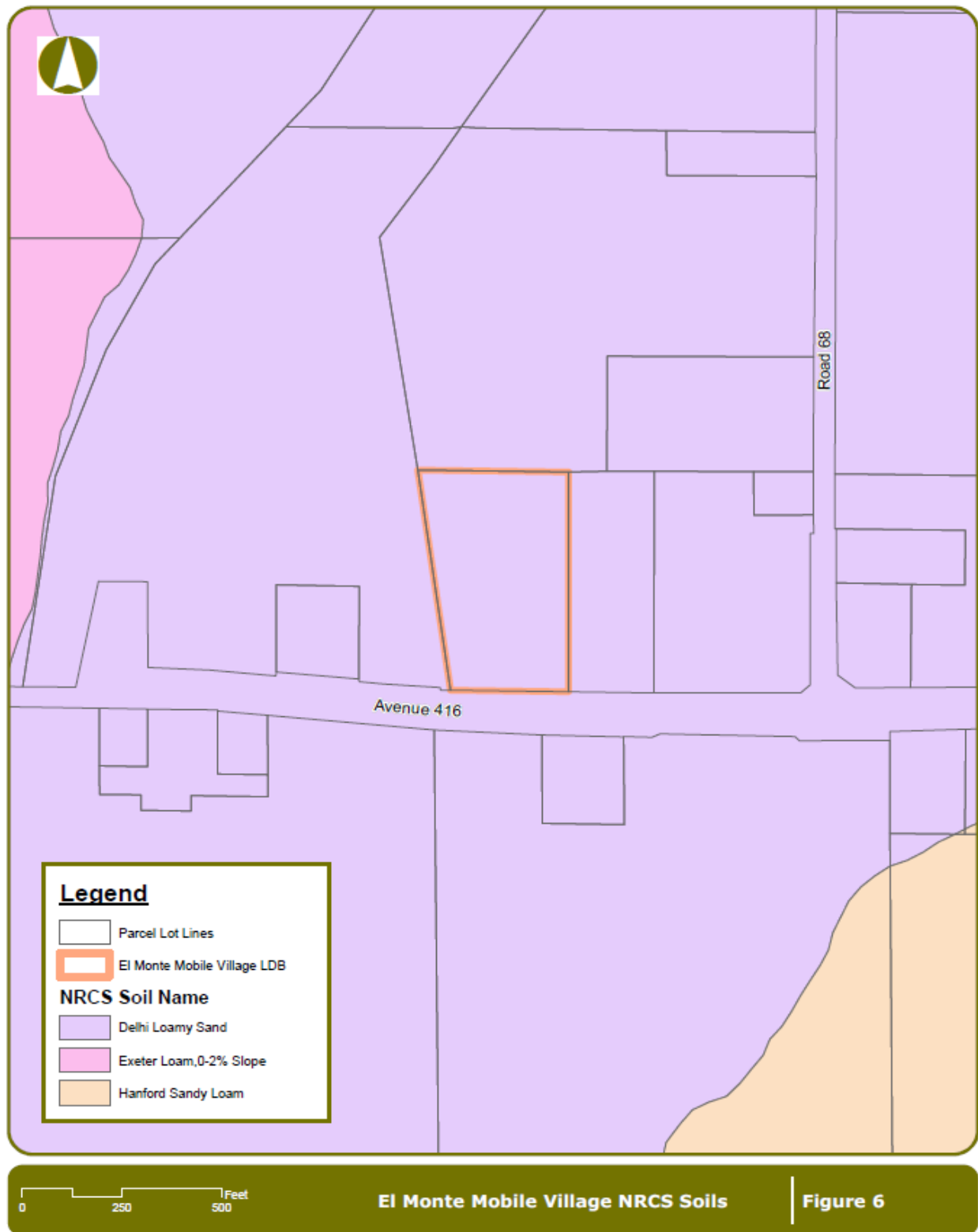
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<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>4</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database August 17, 2006.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 6 - NRCS Soils Map



# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Physical Conditions

### Air Quality

The El Monte Mobile Village Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because El Monte Mobile Village is located near the central portion of the Valley

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of El Monte Mobile Village include Avenue 416 and agricultural activities.

## Flooding<sup>5</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0320E (**see Figure 7**). El Monte Mobile Village is located within Flood Zone X – Areas of 0.2% annual chance flood; area of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates are not required unless property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

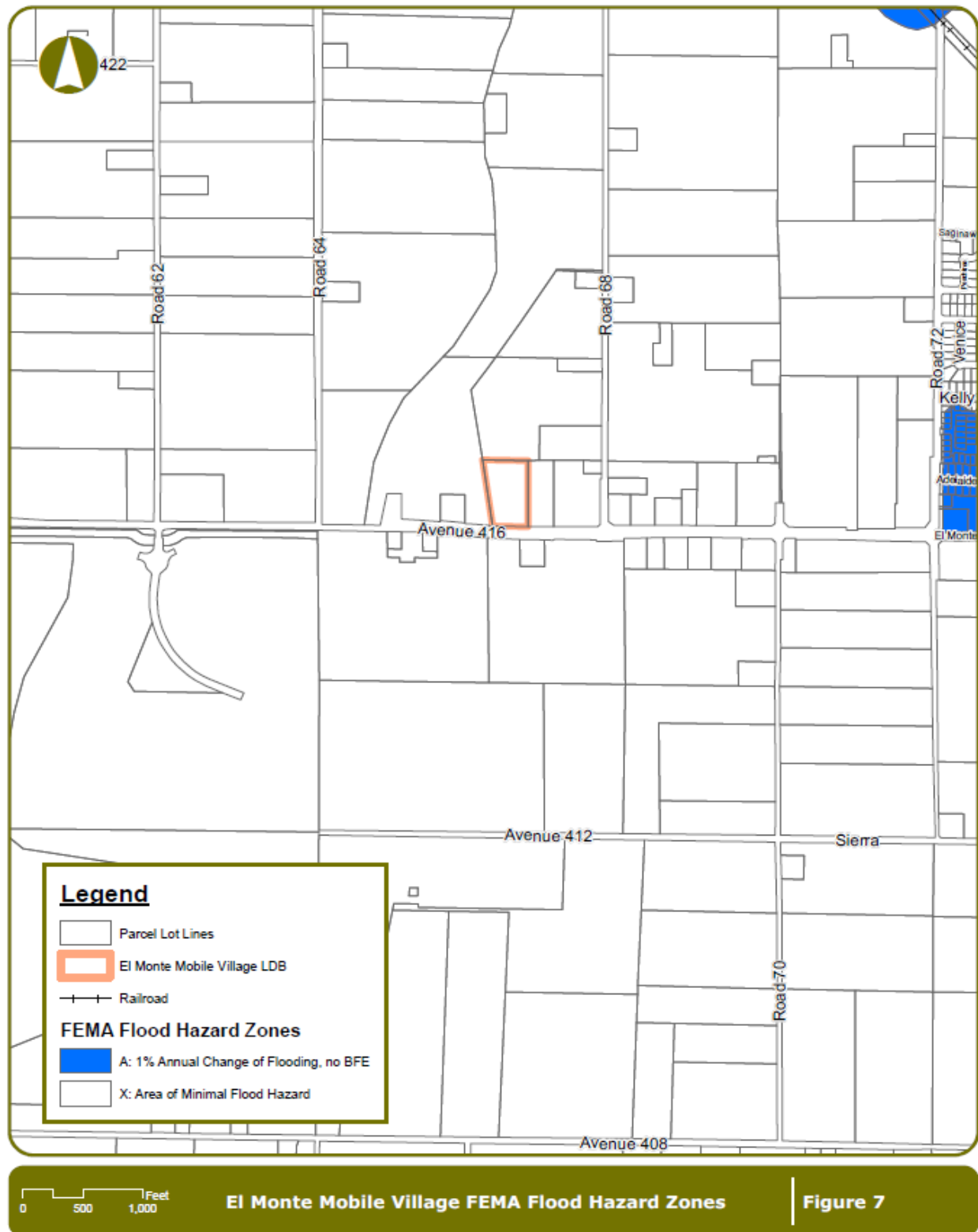
The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), which means the County of Tulare agreed to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available on Tulare County Resource Management Agency website at: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

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<sup>5</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0320E, Panel Number 320 of 2550, June 16, 2009. <https://msc.fema.gov/portal/> accessed May 23, 2017

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 7 - FEMA Flood Map



# EL MONTE MOBILE VILLAGE LEGACY PLAN

## Noise<sup>6</sup>

Tulare County adopted a new noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. (**Table 3**) summarizes the daily traffic volumes along Avenue 416 from State Route 99 to Road 72. Together, these noise sources place a portion of El Monte Mobile Village's urbanized areas within the 60 dB Ldn noise contour. The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise.

Table 3 - Noise Levels - Avenue 416					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
Avenue 416 from State Route 99 to Road 72	9,830	42	91	196	422

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to El Monte Mobile Village, this Legacy Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.). The prioritized concerns of the Community include:

1. Energy - Natural Gas/Electricity
2. Water Supply
3. Sewer
4. Storm Water Drainage
5. Solid Waste
6. Street Conditions

<sup>6</sup> Tulare County General Plan 2030 Update, August 2012.; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# EL MONTE MOBILE VILLAGE LEGACY PLAN

7. Roads
8. Internet
9. Public Transportation

Figure 8 - Inventory of Services in El Monte Mobile Village



# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Energy: Natural Gas/Electricity

Pacific Gas & Electric (PG&E) provides electric and gas power to El Monte Mobile Village.

## Water Supply

The El Monte Mobile Village provides water service to the El Monte Mobile Village. El Monte Village has 49 connections serving 100 residence. The system has one water well that supply the community.

“The drinking water services are provided by the El Monte Village, according to the Environmental Working Group National Drinking Water [https://sdwis.waterboards.ca.gov/PDWW/JSP/WaterSystemDetail.jsp?tinwsys\\_is\\_number=7522&tinwsys\\_st\\_code=CA&counter=0](https://sdwis.waterboards.ca.gov/PDWW/JSP/WaterSystemDetail.jsp?tinwsys_is_number=7522&tinwsys_st_code=CA&counter=0) Database with services for 100 people. Testing conducted between 2004 and 2009 and provided to the EWG by the California Department of Public Health did indicate nitrite and nitrate levels over the legal and health limits, as well as alpha particle activity, lead, 1,2-dibromo-3-chloropropane, and arsenic over the health limit. Environmental Protection Agency (EPA) violations were noted for nitrate levels over the MCL (2007-2008), failure to report information to the public or state agency in the Consumer Confidence Report (2004), and failure to regularly monitor nitrate (2007).”<sup>7</sup>

Well 02 – Online 6-8-15

Well 01 – Dry (6-2015)

## Sewer

“Wastewater treatment is provided by septic systems and information is unavailable regarding any potential leaching and potential deficiencies may exist.”<sup>8</sup>

## Storm Water Drainage

El Monte Mobile Village does not currently have a storm drainage system.

## Solid Waste

Solid waste disposal services for the Community of El Monte Mobile Village is provided by Pena’s Disposal, a private company. Solid waste generated in El Monte Mobile Village can be disposed of at Visalia Landfill, located at 22466 Road 80, Visalia.

## Roads and Circulation

The community of El Monte Mobile Village is comprised of a private internal roads.

## Street Lights

El Monte Mobile Village has street lighting at two (2) locations **(See Table 4)**. “Streetlights are provided in some areas of the community but are lacking in others, therefore the community is deficient in these areas **(see Figure 8)**.”<sup>9</sup>

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<sup>7</sup> SB244: Part II Disadvantaged Unincorporated Communities Assessment, October 2015, Page 3-8

<sup>8</sup> Id

<sup>9</sup> Id



# EL MONTE MOBILE VILLAGE LEGACY PLAN

Table 4 - Streetlight Inventory

Streetlight Locations			
Community	Main Road	Crossroad	Side
El Monte Mobile Home	Ave 416	west of Mobile Home Park entrance	N
El Monte Mobile Home	Inside Park	--	W

## Sidewalks

There are currently no sidewalks located within El Monte Mobile Village.

## ADA Curb Ramps

The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

There are currently no ADA compliant curb ramps found in El Monte Mobile Village.

## **Public Transportation**

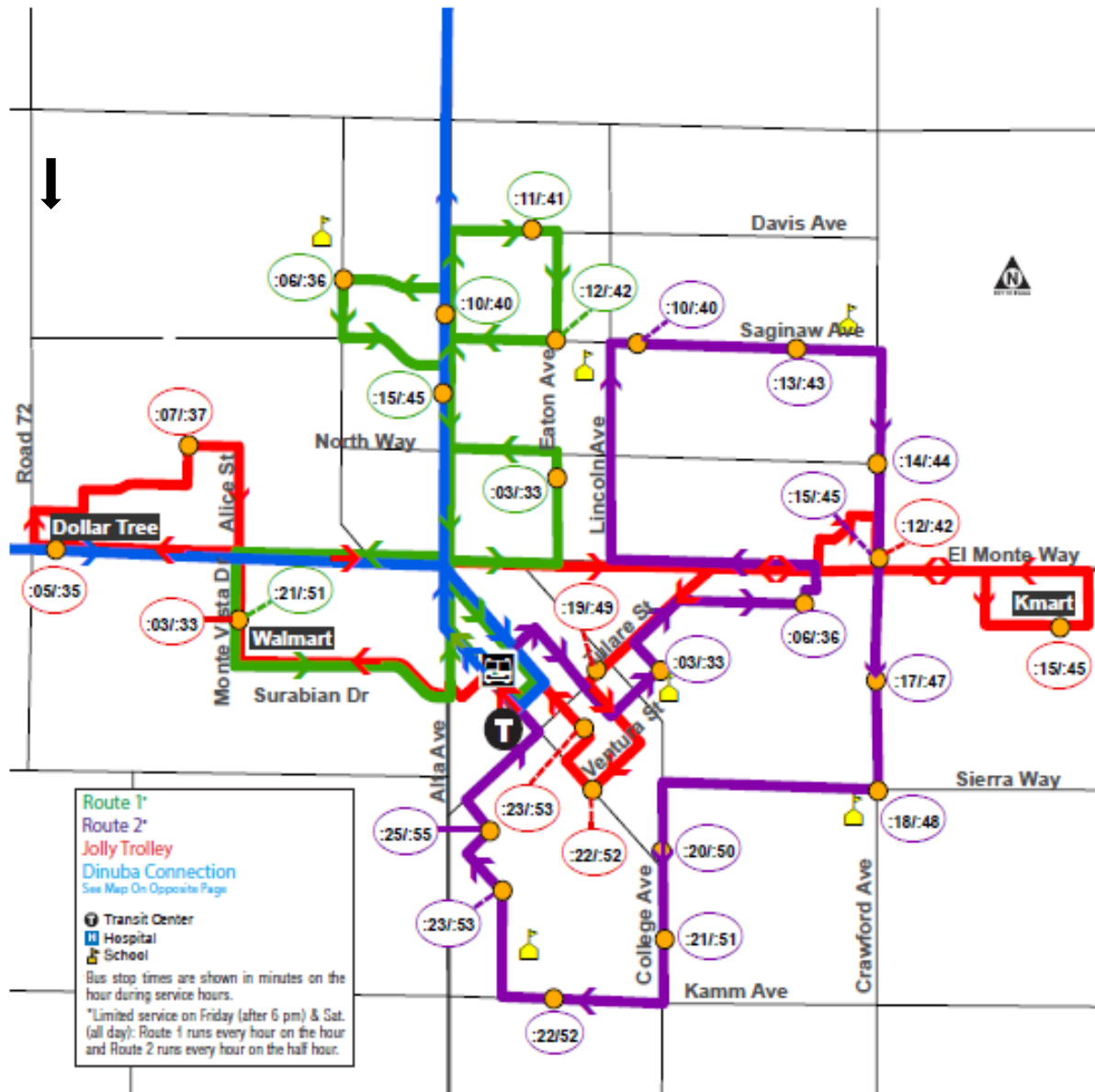
### Transit

Dinuba operates Dinuba Area Regional Transit (DART). DART operates Monday through Saturday, no service provided on Sundays. DART Flexroute is a combined fixed route and dial-a-ride service. Two 30-minute routes take you throughout Dinuba. Routes 1 and 2 operate along a fixed route, but leave the route to pick up dial-a-ride passengers between route stops. Passengers may transfer between DART routes, or to Tulare County Area Transit (TCaT) at the Transit Center in Dinuba. TCaT transfers can be made to connect to Visalia, Tulare, and the remainder of the TCAT public transit system. TCAT vehicles are wheelchair accessible and full size buses include bike racks. DART connects with the Fresno County Rural Transit Agency (FCRTA) at Reedley College. Separate fares are required to board the TCaT and FCRTA systems. El Monte Mobile Village is connected via to Dinuba and its transit center (**see Figure 9**). Bus stop for El Monte Mobile Village is located approximately one (1) mile east along Avenue 416 at the Dollar Tree Store, Dinuba California.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 9 - Dinuba Area Regional Transit (DART)

## DART System Map



## AMTRAK

The Hanford AMTRAK station, located approximately 25 miles to the southwest in Kings County, is the closest station to El Monte Mobile Village providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 20 miles southwest, offers passenger service to Los Angeles. Fresno Yosemite International Airport (FAT), approximately 40 miles north of El Monte Mobile Village, is the principal passenger and airfreight airport in the central San Joaquin Valley.

## State Route 99

State Route (SR) 99 is the major highway between cities within the Central Valley and is the primary route between the City of Fresno to the north and the City of Bakersfield to the south. El Monte Mobile Village is located approximately ten miles east of SR 99.

## Internet Access

Most families in El Monte Mobile Village do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in El Monte Mobile Village.

## PUBLIC SERVICES

### Sheriff

Police protection services are provided in El Monte Mobile Village by the Tulare County Sheriff's Department sub-station, located 40765 Road 128, in Cutler, California, approximately 10 miles southeast of El Monte Mobile Village. The substation provides patrol services 24-hours per day, 365 per year. Additional Sheriff resources are available as needed via dispatch from the main Sheriff's Office in Visalia, California.

### Fire

Fire protection and emergency medical services are provided for El Monte Mobile Village by the Tulare County Fire Department. The community is served by Dinuba Fire Station #3, located at 40404 Road 80 Dinuba, California. Parade 1, Engine 3 and Water Tender 3 are assigned to this location.

There are currently no fire hydrants in El Monte Mobile Village.

### Schools

Schools are an especially important component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The El Monte Mobile Village Area is within the Dinuba Unified School District approximately three (3) miles away). The District provides K-12/adult education at Dinuba Schools.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Library

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>10</sup> The Dinuba Branch Library is located in Dinuba, approximately three (3) miles to the east of El Monte Mobile Village (see Table 5).

Table 5 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Dinuba	Dinuba Branch 150 S. I Street Dinuba, CA 93618	Tuesday - Friday 9:00 am – 1:00 pm 2:00 pm – 6:00 pm

Library hours current as of May 2017

## Parks

There are no County owned/operated public parks in El Monte Mobile Village. In the community of El Monte Mobile Village there is no available open green space for children to play and adults to be physically active. The nearest park is located three (3) miles away in the City of Dinuba.

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

Tulare County’s relationship to the State Route system, nearby counties, cities, and communities

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>11</sup>

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<sup>10</sup> General Plan Background Report, page 7-96

<sup>11</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Traffic

Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation

El Monte Mobile Village is a small agricultural community located adjacent to Avenue 416. The community of El Monte Mobile Village is comprised of a private internal road roads. The major rural collector road is Avenue 416. There are no proposed major streets as part of this Legacy Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the El Monte Mobile Village Legacy Plan prescribes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in the El Monte Mobile Village Legacy Boundary is proposed to reflect a residential land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for El Monte Mobile Village to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan, El Monte Mobile Village Legacy Plan. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

It is important to recognize that County policies reflect the unique attributes of the various locations and geographic areas in the County. As such, there are policies applicable to one area of the County that are not applicable to others based on natural setting, topography, habitat, existing development, or other attributes which are unique within the planning context of the County. The following planning goals, objectives, policies and standards reflect, as applicable, the rural, suburban, or urban contexts of the Legacy planning areas designated by the County.

### **Community Development**

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Locate high density residential uses in close proximity to planned shopping areas.
2. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
3. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
4. The County shall work with the Schools to

provide safe routes to school.

5. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the El Monte Mobile Village Community Plan.
6. The El Monte Mobile Village Legacy Plan should be reviewed every five years to determine if amendments are appropriate.
7. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## **Housing**

### **GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

## Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to initiate home maintenance/repair programs in El Monte Mobile Village.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

## Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.

2. Assure that the housing industry is made aware of residential development potential in El Monte Mobile Village.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

## Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide the services necessary to support new development.



# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## Policies:

1. Encourage the El Monte Mobile Village to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

**Objective:** Provide the necessary safe guards to attract quality development to the community.

## Policies:

1. Assure that developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## ***Environmental Quality and Public Safety***

**GOAL I: Preserve and enhance the quality of life for present and future generation of El Monte Mobile Village citizens.**

**Objective:** Upgrade the level of community health, sanitation and safety.

## Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

**Objective:** Provide sufficient open space for community recreation needs.

## Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.



# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

**Infrastructure** In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

**Use Permits** There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

**Education** Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

**Health Care** Health care is important for economic development as businesses need healthy employees. The nearest medical offices are in the City of Dinuba.

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Legacy Development Boundary, the Land Use Plan (see **Table 6**) and Zoning Districts Plan (see **Table 7**) have been updated.

Table 6 - Proposed Land Use	
Proposed Land Use	Acres
Residential	4.41
Right-of Ways	0.0
El Monte Mobile Village LDB	4.41

Table 7 - Proposed Zoning Districts	
Proposed Zones Districts	Acres
R-3	4.41
Right-of Ways	0.0
El Monte Mobile Village LDB	4.41

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the El Monte Mobile Village Legacy Plan. The following components comprise the El Monte Mobile Village Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone and the Zoning Map Update), Complete Streets/Safe Routes to School

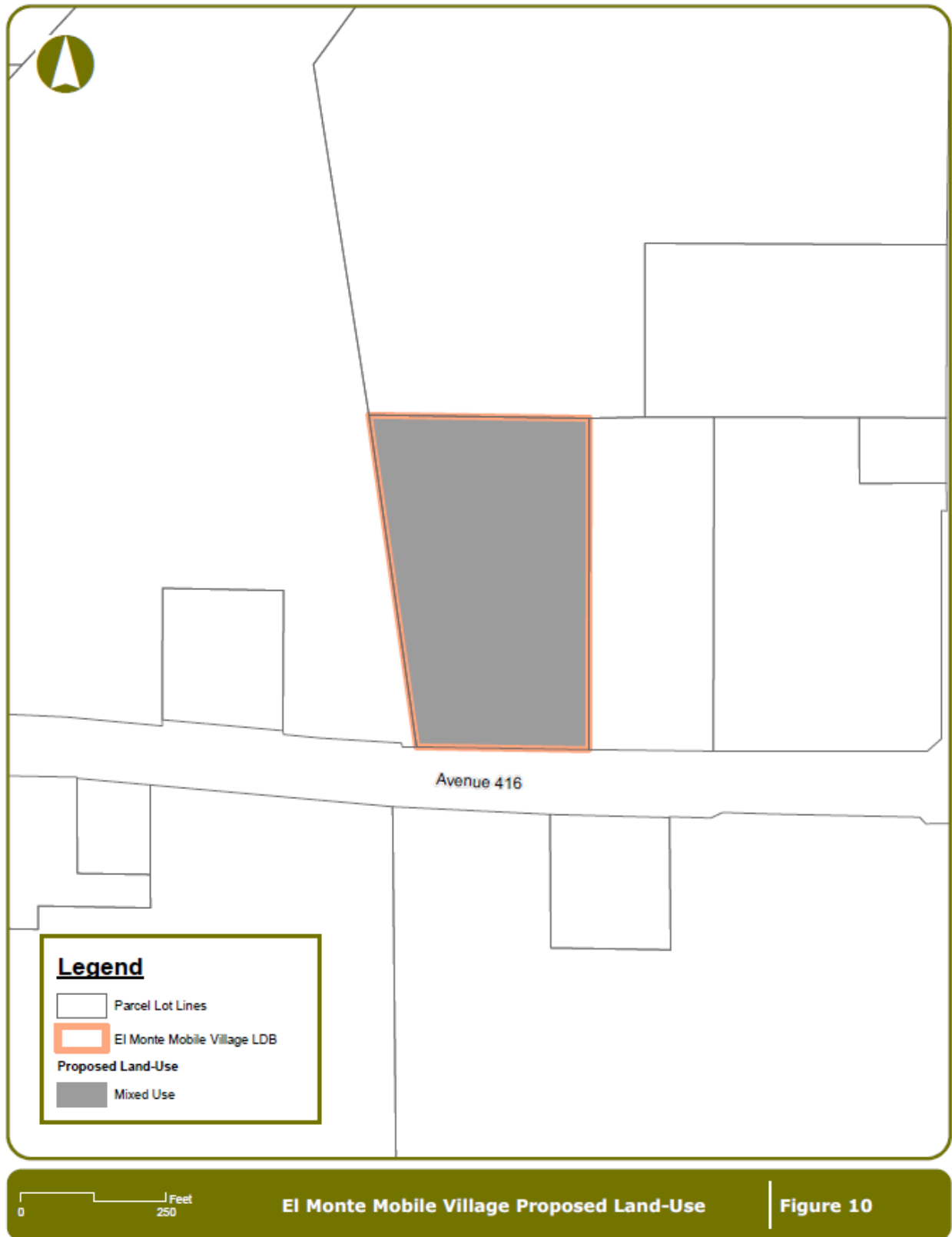
**Zoning District Changes** As part of this Implementation Program for the El Monte Mobile Village Legacy Plan, there are a variety of changes to existing zoning districts. These changes are described below.

**Chapter 16 of the Zoning Code** Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

**Zoning Map Update** The current Zoning Map (**see Figure 11**) for El Monte Mobile Village will be amended to be compatible with the Land Use Map (**see Figure 10**) outlined in the General Plan. There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 12**) El Monte Mobile Village Proposed Zoning.

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 10 - Proposed Land Use Plan



# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 11 - Existing Zoning District



# EL MONTE MOBILE VILLAGE LEGACY PLAN

Figure 12 - Proposed Zoning Districts



# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## **ATTACHMENT**

A-1 – Use Permit Requirement Changes (Zone Change Text)

# EL MONTE MOBILE VILLAGE LEGACY PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### Additional By-Right Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1



# EL MONTE MOBILE VILLAGE LEGACY PLAN

Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1

# EL MONTE MOBILE VILLAGE LEGACY PLAN

Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# EL MONTE MOBILE VILLAGE LEGACY PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9342

General Plan Amendment GPA 17-035-Resolution No. 9343

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9344

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9345

Zoning District Map-Resolution No. 9346

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE LEGACY PLANS 2017 ) RESOLUTION NO. 9342  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, II AND PART III )  
AMENDMENTS (GPA 17-033) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Legacy Plans 2017 Update which consists of General Plan Amendment No. GPA 17-033 amendment to Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities & Services Elements, and Part II Rural Valley Lands Plan, and Part III (El Monte Mobile Village GPA 17-025; Hypericum GPA 17-026; Jovista GPA 17-027; Matheny Tract GPA 17-029 and Tooleville GPA 17-030 amending the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, Change of Zones No. PZC 17-025 El Monte Mobile Village; PZC 17-026 Hypericum; PZC 17-027 Jovista; PZC 17-029 Matheny Tract and Tooleville PZC 17-030, PZC 17-032 for an amendment to Section 16 of Ordinance No. 352, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Development Boundary, and Zone Ordinance Amendment No. PZC 17-031 establishing a Mixed Use Combining Zone in El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville (incorporated by reference herein). The unincorporated Legacy Communities are located within the Mount Diablo Base & Meridian as follows: El Monte Mobile Village, Section 12, Township 16S, Range 23E, Hypericum, Section 24, Township 19S, Range 25E, Jovista, Section 32, Township 24S, Range 26E, Tooleville, Section 12, Township 19S, Range 26E, Matheny Tract Section 22, 23, & 27, Township 20 south, Range 24 East.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element, Transportation & Circulation Element and the Public Facilities & Services Element; (2) add El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Legacy



Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Legacy Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-033 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management and the Transportation and Circulation Element; Change of Zone No. PZC 17-031 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for El Monte Mobile Village, Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista, Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract, Change of Zone No. PCZ 17-029 (Zoning District Map) and Tooleville, Change of Zone No. PZC 17-030 (Zoning District Map) within the respective Legacy Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-032 an amendment to Section 16 to allow additional by-right uses in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Legacy Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Legacy Plans;
  - b. Encourage infill development within Legacy Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Legacy Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The legacy planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on September 15, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Legacy Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Legacy Plans 2017 Update General Plan Amendment 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Change of Zone No. PZC 17-025; Hypericum General Plan Amendment No. GPA 17-026; Change of Zone No. PZC 17-026; Jovista General Plan Amendment No. GPA 17-027; Change of Zone No. PZC 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; Change of Zone No. PZC 17-029; Tooleville General Plan Amendment No. GPA 17-030; Change of Zone No. PZC 17-030; Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-032 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation, Public Facilities & Services, and Part II Rural Valley Lands Plan has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,

4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Legacy Plans 2017 Update. Therefore, the public will benefit from the Legacy Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on October 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano  
NOES: None  
ABSTAIN: None  
ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE ) RESOLUTION NO. 9343  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT AGRICULTURE ELEMENT, SCENIC )  
LANDSCAPES ELEMENT, AIR QUALITY ELEMENT, )  
PUBLIC FACILITIES & SERVICES ELEMENT, )  
INTRODUCTION, COMPONENTS A, B, C, AND D, AND )  
PART II RURAL VALLEY LANDS PLAN OF THE TULARE )  
COUNTY GENERAL PLAN FOR THE LEGACY PLANS )  
2017 UPDATE GPA 17-033 INCLUSIVE OF GPA's 17-025, )  
GPA 17-026, GPA 17-027, GPA 17-29, AND GPA 17-030 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Introduction; Component A-Planning Framework Element; Component B-Agriculture and Land Use Elements, Component C-Scenic Landscapes, Environmental Resources Management and Air Quality Elements; Component D-Transportation & Circulation and Public Facilities & Services Elements; and Part II Rural Valley Lands Plan of the Tulare County General Plan as set forth in Exhibits "A-1 to A-13" and Part III Community Plan as set forth in Exhibits "B-F" for the Legacy Plans 2017 Update General Plan Amendment No. GPA 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Legacy communities as part of the Legacy Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on September 15, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017 and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan for the Legacy Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Legacy Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Legacy Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to establish Legacy Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;

4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the establishment of legacy development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;
5. The Amendment to Part I Introduction, Component A, Component B, Agriculture, Component C, Scenic Landscapes, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and the newly established Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Legacy Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-033 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-033 the Legacy Plans 2017 Update set



forth in attached Exhibits “A-1 to A-13” and Part III Exhibits “B-F” which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

Exhibits “A-1 – A-13” – Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Service Elements, and Part II Rural Valley Lands Plan.

Exhibit “B” – 2017 El Monte Mobile Home Park Legacy Plan (Part III)

Exhibit “C” – 2017 Hypericum Legacy Plan (Part III)

Exhibit “D” – 2017 Jovista Legacy Plan (Part III)

Exhibit “E” – 2017 Matheny Tract Legacy Plan (Part III)

Exhibit “F” – 2017 Tooleville Legacy Plan (Part III)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO            )  
SECTION 18.9 "MU" MIXED USE COMBINING        )  
ZONE OF ORDINANCE NO. 352 CONSISTENT        )        RESOLUTION NO. 9344  
WITH THE ADOPTED LEGACY PLANS 2017            )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE        )  
NO. PZC 17-031                                    )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and

Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9345
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-032	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the communities of El Monte Mobile Village,

Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update, consistent with the Legacy Communities 2017 Update including affirmation that Section 16 H. is applicable in the Matheny Tract.

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	RESOLUTION NO. 9346
IN THE LEGACY DEVELOPMENT BOUNDARIES	)	
EL MONTE MOBILE VILLAGE, HYPERICUM,	)	
JOVISTA, MATHENY TRACT, AND TOOLEVILLE	)	
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS 2017 UPDATE (GPA 17-033), AS	)	
PROPOSED INCHANGE OF ZONE NO. PZC's	)	
17-025, 17-026, 17-027, 17-029, AND 17-030	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" El Monte Mobile Village Rezoning Plan (PZC 17-025), to include the Legacy Community of El Monte Mobile Village, consistent with the adopted El Monte Mobile Village Legacy Plan 2017 Update, Exhibit "E" Hypericum Rezoning Plan (PCZ 17-026), to include the Legacy Community of Hypericum, consistent with the adopted Hypericum Legacy Plan 2017 Update, Exhibit "H" Jovista Rezoning Plan (PCZ 17-027), to include the Legacy Community of Jovista, consistent with the adopted Jovista Legacy Plan 2017 Update, Exhibit "K" Matheny Tract Rezoning Plan (PCZ 17-029), to include the Legacy Community of Matheny Tract, consistent with the adopted Matheny Tract Legacy Plan 2017 Update, Exhibit "N" Tooleville Rezoning Plan, to include the Legacy Community of Tooleville, consistent with the adopted Tooleville Legacy Plan 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and



WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update (GPA 17-033) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities;
3. The proposed changes in zone district designations identified in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the El Monte Mobile Village Change of Zone No. PZC 17-025; Hypericum Change of Zone No. PZC 17-026; Jovista Change of Zone No. PZC 17-027; Matheny Tract Change of Zone No. PZC 17-029; and Tooleville Change of Zone No. PZC 17-030, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" and "N" Rezoning Plans consistent with the adopted El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update (GPA 17-033).

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

El Monte Mobile Village

Exhibit "A" – Proposed Zone Ordinance Map (PZC 17-025)

Exhibit "B" – Proposed Zoning Map

Exhibit "C" – List of Affected Properties/Map

Hypericum

Exhibit "D" – Proposed Zone Ordinance Map (PZC 17-026)

Exhibit "E" – Proposed Zoning Map

Exhibit "F" – List of Affected Properties/Map

Jovista

Exhibit "G" – Proposed Zone Ordinance Map (PZC 17-027)

Exhibit "H" – Proposed Zoning Map

Exhibit "I" – List of Affected Properties/Map

Matheny Tract

Exhibit "J" – Proposed Zone Ordinance Map (PZC 17-029)

Exhibit "K" – Proposed Zoning Map

Exhibit "L" – List of Affected Properties/Map

Tooleville

Exhibit "M" – Proposed Zone Ordinance Map (PZC 17-030)

Exhibit "N" – Proposed Zoning Map

Exhibit "O" – List of Affected Properties/Map

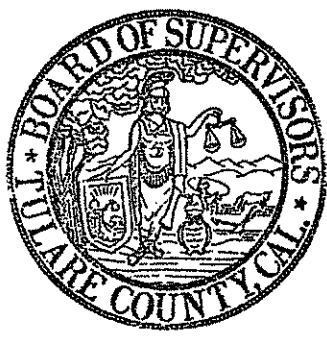
Appendix B – Board of Supervisors  
Resolution No. 2017-0868

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER LEGACY PLANS 2017 )  
UPDATE (GENERAL PLAN AMENDMENT 17- ) Resolution No. 2017-0868  
033) AND AMENDMENTS TO THE TULARE ) Ordinance No. 3508  
COUNTY ZONING ORDINANCE NO. 352 )

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR CROCKER, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD OCTOBER 17, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS  
NOES: NONE  
ABSTAIN: NONE  
ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY: *Michael C. Spata*  
Deputy Clerk

\*\*\*\*\*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Legacy Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Legacy Plans 2017 Update (GPA 17-033); and
3. Adopted one General Plan Amendment No. GPA 17-033, for the Legacy Plans 2017 Update inclusive of the El Monte Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029); Tooleville (GPA 17-030) and Amendments to Tulare County General Plan Part III to establish Legacy Plans and Legacy Development Boundaries; Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D,

Public Facilities and Services Elements, and Part II General Plan Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update; and

4. Waived the final reading and adopted Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Legacy Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-031) to Section 18.9 to adopt a Mixed-Use Combining Zone within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-032) to Section 16 to allow additional by-right uses within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for El Monte Village (PZC 17-025), Hypericum (PZC 17-026), Jovista (PZC 17-027), Matheny Tract (PZC 17-029), and Tooleville (PZC 17-030), consistent with the Legacy Plans 2017 Update; and
5. Adopted the findings of approval set forth in Planning Commission Resolution No. 9342 (Addendum to EIR), Planning Commission Resolution No. 9343 (Legacy Plans 2017 Update), Planning Commission Resolution No. 9344 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9345 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9346 (Zoning District Ordinance Maps); and
6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of El Monte Mobile Village, Hypericum, Matheny Tract, Jovista, and Tooleville to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents; and
10. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

## **EXHIBIT "A"**

### **A. Legacy Plans 2017 Update (General Plan Amendment 17-033)**

WHEREAS, on September 27, 2017 the Planning Commission held a public hearing on the Legacy Plans 2017 Update (GPA 17-033), which included an application for El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030, El Monte Mobile Village Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract Change of Zone No. PZC 17-029 (Zoning District Map); and Tooleville Change of Zone No. PZC 17-030 (Zoning District Map), Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9) and Change of Zone No. PZC 17-032 (By-Right Uses–Section 16), collectively referred to as the Legacy Plans 2017 Update (GPA 17-033); and

WHEREAS, upon conclusion of the public hearing, the Planning Commission recommended the Board of Supervisors certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopt the Mitigation Monitoring and Reporting Program and approve the Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors held a public hearing on October 17, 2017 to consider the proposed Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors finds the public hearing for the Legacy Plans 2017 Update was properly noticed in accordance with state law and the Tulare County Ordinance Code.

WHEREAS, the Board of Supervisors finds the Legacy Plans 2017 Update is compatible, integrated and internally consistent with the existing policies of and with the Tulare County General Plan.

WHEREAS, the Board of Supervisors accepts and incorporates by reference herein, the findings of facts and recommendations adopted by the Planning Commission as follows:

1. Planning Commission Resolution No. 9342, in the matter of the Certification and adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update (PC Package Attachment 1 – CEQA Resolution).

2. Planning Commission Resolution No. 9343, to adopt General Plan Amendment No. GPA 17-033 for the Legacy Plans 2017 Update inclusive of El Monte Mobile Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029) and Tooleville (GPA 17-030) and Amendments to Part III of the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, to the Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities and Services Elements, and Part II Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update (PC Package Attachment 2 – GPA Resolution).
3. Planning Commission Resolution No. 9344, in the matter of an Amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed Use Combining Zone within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 3 – Section 18.9 Resolution).
4. Planning Commission Resolution No. 9345, in the matter of an Amendment to Section 16 of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 4 – Section 16 Resolution).
5. Planning Commission Resolution No. 9346, in the matter of an Amendment to Ordinance No. 352, the Zoning Ordinance, as set forth in the Zoning District Ordinance Maps for El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the Legacy Plans 2017 Update to rezone certain properties within El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville. (PC Package Attachment 5 – Rezoning Plan Resolution).

NOW, THEREFORE, BE IT RESOLVED as follows:

A. The Board of Supervisors hereby certifies and adopts the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopts the Mitigation Monitoring Reporting Programs.

NOW BE IT FURTHER RESOLVED as follows:

B. The Board of Supervisors adopts the entirety of all of the above findings and actions as one General Plan Amendment No. GPA 17-033.

C. The Board of Supervisors hereby waives the final reading of Change of Ordinance 352 and adopts amendments to the Tulare County Zoning Ordinance 352 pertaining to Section 18.9 - Mixed-Use Combining Zone, to Section 16 - Variances and Special Uses, and Zoning District Map Changes.



D. The Board of Supervisors authorizes the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of Legacy Plans 2017 Update to include Mixed Use Combining Zone and additional "By-Right" uses.

E. The Board of Supervisors directs the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq.

F. The Board of Supervisors directs the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file Notices of Determination with the Tulare County Clerk.

G. The Board of Supervisors authorizes the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents.

## Hypericum Legacy Plan

# HYPERICUM LEGACY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development and Planning Branch



# HYPERICUM LEGACY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# HYPERICUM LEGACY PLAN

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## HYPERICUM LEGACY PLAN

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# Hypericum Legacy Plan

Adopted: October 17, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0866

Tulare County Planning Commission  
Recommendations: September 27, 2017  
Resolutions No. 9342, 9343, 9344, 9345, and 9346

General Plan Amendment: GPA 17-033  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-033  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-031

Hypericum Legacy Plan: GPA 17-026  
Zoning District Map: PZC 17-026



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
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# HYPERICUM LEGACY PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)



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## **County Administrative Office**

Michael C. Spata, CAO  
Eric Coyne, Deputy CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director  
Michael Washam, Associate Director  
Aaron Bock, Chief Planner, Project Processing  
Dave Bryant, Chief Planner, Special Projects  
Susan Simon, Planner III  
Velma Quiroz, Planning Tech II  
Johnson Vang, Engineer I

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## INTRODUCTION

An important objective in preparing a Legacy Plan for Hypericum is to develop a plan which can accurately reflect the needs and priorities of this community. Hypericum was designated in the 2030 Tulare County General Plan (2012) through the SB 244 Study under GPA 2015-D. It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [for example wells, water distribution piping, and storage tanks], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

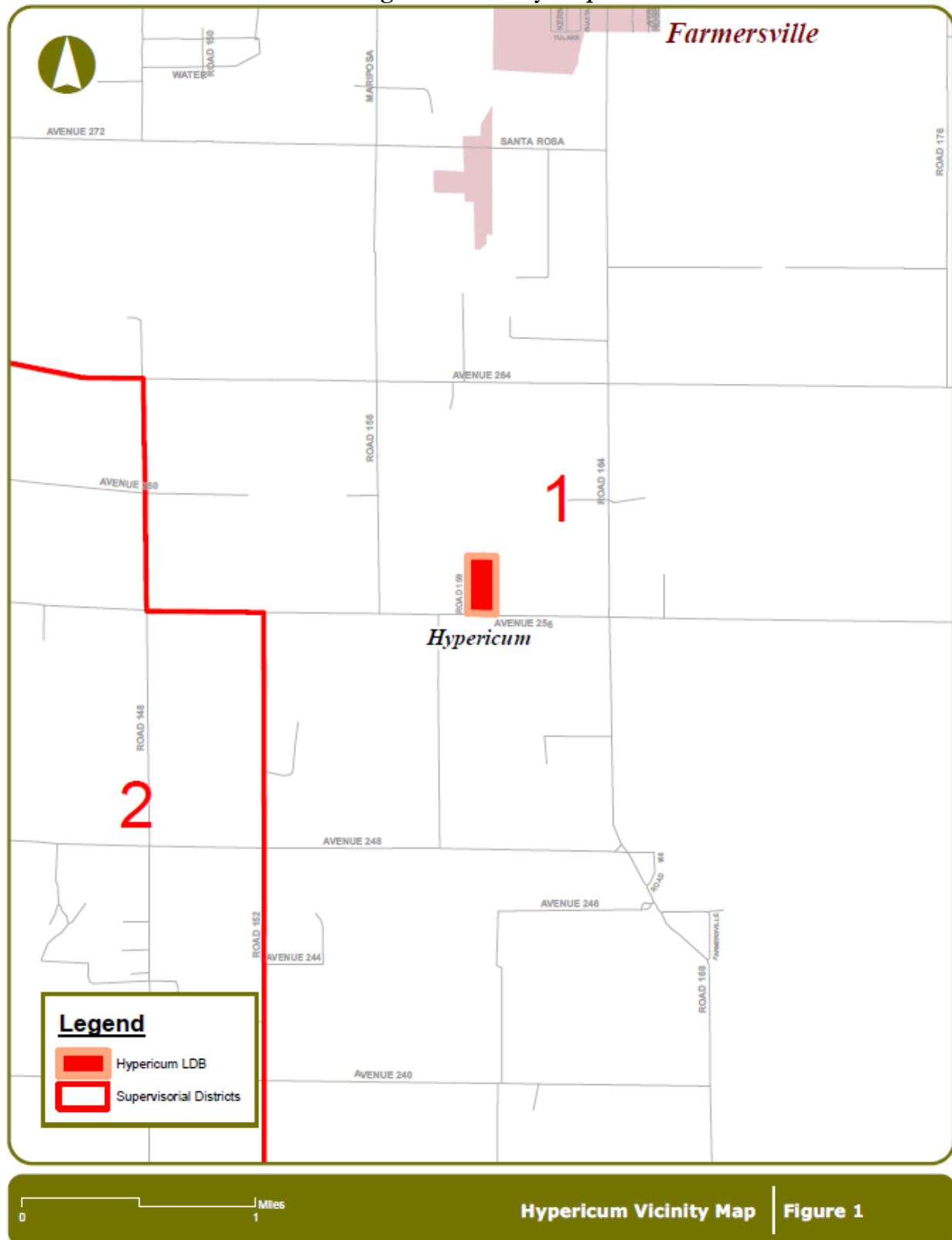
### Location

Hypericum (**see Figure 1**) is located on the western part of Tulare County on the valley floor. Hypericum also known as “Dog-town” is a small community comprised of two residential roads and approximately 40 parcels of land. Hypericum (**see Figure 2**) is located approximately 3 miles south of the City of Farmersville, in an unincorporated area of Tulare County. Hypericum is in Section 24, Township 19 South, Range 25 East, MDB&M and can be found within the Exeter, United States Geological Survey 7.5 minute topographic quadrangle. Hypericum is 340 feet above sea level. Coordinates of Hypericum are: Latitude: 36.256397 and Longitude: -119.216734. Hypericum is located in Census Tract 16.02, Block Group 3. The population is 161 people, Medium Household Income is \$31,156, and is comprised of approximately 43 residences.



# HYPERICUM LEGACY PLAN

Figure 1 - Vicinity Map



# HYPERICUM LEGACY PLAN

Figure 2 - Hypericum Aerial Map



# HYPERICUM LEGACY PLAN

Figure 3 - Hypericum LDB



# HYPERICUM LEGACY PLAN

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## COMMUNITY OUTREACH AND INVITATION PROCESS

The County of Tulare received a grant from the Strategic Growth Council (SGC) to assist in the preparation of this plan. In efforts to invite and make local residents aware of the Strategic Growth Council (SGC) projects, Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the SGC project.

- 1.) SHE staff developed an informational flyer.
- 2.) SHE staff went door to door to distribute flyers to every home on February 13 and February 19, 2016.
- 3.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.
- 4.) SHE partnered with the Outside Creek School District to spread the word about the meeting; meeting flyers were sent home from school with each student.
- 5.) SHE partnered with a community resident who helped to distribute flyers to his neighbors and allowed us to have the community meeting at his home.

### Community Meeting

Self-Help Enterprises conducted a community meeting (with two SHE staff members present) in Hypericum on Sunday, February 21, 2016, at 3:00 pm. The meeting was scheduled on a weekend to make it easier for working residents to attend. The meeting was held at 25649 Hypericum Road, the home of a local resident. This is an ideal meeting location because the community of Hypericum does not have a community meeting space and because most residents feel welcome and comfortable with their neighbors. The purpose of the meeting was to discuss Hypericum improvement needs, gather community data, and report the findings of the meeting in a final report. Nine (9) Hypericum residents attended the meeting. Eight (8) community surveys were collected.





# HYPERICUM LEGACY PLAN

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## Community Survey

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Waste Water, Storm Water Drainage, Complete Streets, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Hypericum SGC community input meeting, residents concluded that the following is a list of Hypericum priority improvement needs:

## Priority Improvements:

- Energy/Natural Gas
- Water System
- Sewer System
- Storm water drainage
- Road Conditions
- Street Lights
- Sidewalks
- Transportation
- Internet
- Community Park

**Natural Gas** - Residents report that they do not have natural gas in their homes and that they use propane as their source of fuel. This is concerning to them because propane is more expensive than natural gas and the cost to heat their homes with propane is more than they can afford. Survey results show that Hypericum residents would like to have natural gas but cannot afford the cost of extending gas lines to their homes. The community is open to any option of converting to natural gas.

**Water** - Hypericum does not have a community water system, as such, residents own and maintain individual wells. Over the last year many wells have gone dry in this community. Through the help of Self Help Enterprises and Tulare County, residents whose wells have gone dry have been connected to temporary water tanks as an interim solution. Residents are receiving an allocated amount of water per month. Resident expressed that they are interested in exploring their options for connecting to a neighboring community water system, they understand that this may include an initial cost and would result in paying a monthly water bill.

# HYPERICUM LEGACY PLAN

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Residents are concerned with their water quality and perceive their water to be unsafe to drink. Most families do not drink the water from their tap. In 2014, SHE tested three wells in Hypericum, test results show that two of the three wells tested positive for total coliform and the Nitrate levels are as follows:

- Well 1 - 47ppm
- Well 2 - 65ppm
- Well 3 - 47ppm

**Sewer System** - The community is dependent on individual septic tank systems for sewage disposal. Many residents would prefer to be part of a community wide sewer system, they understand that this may include an initial cost and would result in paying a monthly sewer bill.

**Storm Water Drainage** - Residents report that large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that street flooding occurs during the rainy season and the stagnant water afterwards attracts insects and breeds mosquitos.

**Road Improvements** - The community of Hypericum is comprised of two main roads, Hypericum Road and Road 159. Road conditions are a major concern to Hypericum residents. It is reported that road conditions are poor and the roads need work, most streets have potholes, cracks and bumps. Drivers feel unsafe and are constantly worried about damage to their vehicles while driving on that road. Residents report that roads are narrow and that two cars barely fit on one road at the same time. According to residents, Hypericum roads are not regularly maintained.

**Street Lights** - Residents report dark streets at night because the streets do not have lighting. People stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the sheriff's department patrols in this area are limited due to the large geographic area of the County. Residents are especially concerned about how this affects the safety of local children. Residents said that their streets do not have light poles.

**Sidewalks** – There are currently no street walks located within Hypericum. Residents report that this is a problem for children and parents who push a stroller through the area. The lack of sidewalks becomes an even bigger problem during the rainy season as residents and children are forced to walk in the mud and through big puddles.

## **Transportation**

Hypericum lacks access to public transportation. Hypericum residents that do not drive find it hard to travel outside of town. Residents often need to travel to Visalia or Farmersville for doctors' appointments and shopping needs. Many residents expressed the need for a bus route that goes out to Hypericum. Bus transportation to and from Hypericum would help many residents with their daily travel needs.

**Internet Access** - Most families in Hypericum do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to

# HYPERICUM LEGACY PLAN

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what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Hypericum.

**Neighborhood Park** - In the community of Hypericum there is no safe open green space for children to play and adults to be physically active. The nearest park is located 4 miles away in the city of Farmersville, survey results show that in Hypericum, children play in the street or do not play outside. Hypericum residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

## DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Legacy's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Legacy plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Legacy allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Legacy. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller Legacy such as Hypericum.

### Population

Hypericum is located within Census Tract 16.02, Block Group 3. The population for Census Tract 16.02 was 2,088 people respectively 161 of the people in Hypericum (**Census data: ACS 2015 5-year**).

### Median Age

Hypericum's median age of 33 is lower than the median age of the State of California (**Census data: ACS 2015 5-year**) (see Table 1).

Table 1 - Median Age	
Geography	Median Age (years)
California	35.6
Tulare County	30.1
Hypericum	33

# HYPERICUM LEGACY PLAN

## ECONOMIC CONDITIONS

### Employment in Hypericum

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production.

### Median Household Income

In 2015, Hypericum's median household income was \$31,156 whereas the State of California's median household income was \$61,818. **(Census data: ACS 2015 5-year) (see Table 2)** Therefore, based upon Hypericum's median household income of \$31,156 (which is about 51% of the State of California's median household income), it is considered a disadvantaged community.

Table 2 - Median Household Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Hypericum	\$31,156		\$16,606

### Severely Disadvantaged Community

Hypericum is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

## HOUSING CHARACTERISTICS

### Housing Conditions

Table 3 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Hypericum	0	0	43	100	0	0	0	0			43

Source: Tulare County 2015 Housing Element



# HYPERICUM LEGACY PLAN

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The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

As indicated by Tulare County Building Department approximately 100% of the housing units in Hypericum are in a minor deteriorated condition. **(see Table 3).**

## Household Size (Overcrowding)

In 2015 the average housing units and occupancy were 3.7 persons per household **(Census data: ACS 2015 5-year).**

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## ENVIRONMENTAL CONDITIONS

### **Natural Conditions**

#### Wildlife<sup>2</sup>

A California Natural Diversity Database (CNDDDB) search conducted on November 1, 2016 **(see Figure 4)** indicates there are special status species within the Exeter Quadrant Species List (which includes the Hypericum Planning Study Area) consisting of one animal species and no plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

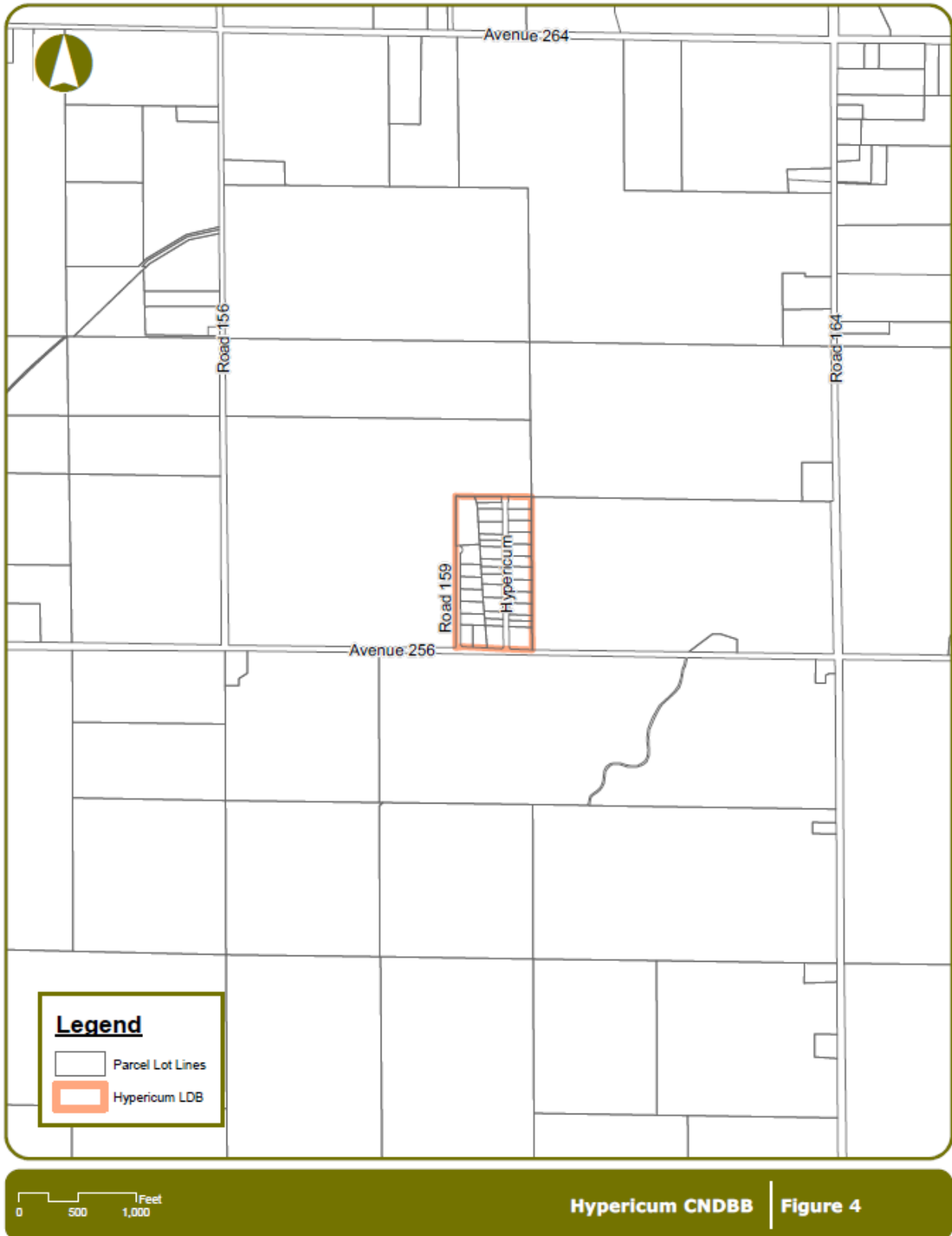
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<sup>1</sup> U.S. Department of Housing and Urban Development, “Measuring Overcrowding in Housing” 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDDB/Maps-and-Data> accessed November 1, 2016

# HYPERICUM LEGACY PLAN

Figure 4 - CNDDDB Map Exeter Quad



# HYPERICUM LEGACY PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Hypericum are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Hypericum] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>4</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Hypericum (**see Figure 5**). Generally, these soils are moderately to very deep, are well drained, and run in a northwest to southeast direction throughout the Legacy Development Boundary (LDB). The following description is provided for the above soil type:

**Nord Fine Sandy Loam, 0 to 2 percent slopes.** Consists of very deep, well drained; negligible to low runoff, moderate permeability. Nord soils are used for such crops as irrigated alfalfa, cotton, corn, milo, barley, wheat, sugar beets, tomatoes, grapes, walnuts, peaches, and other fruit and nut trees.

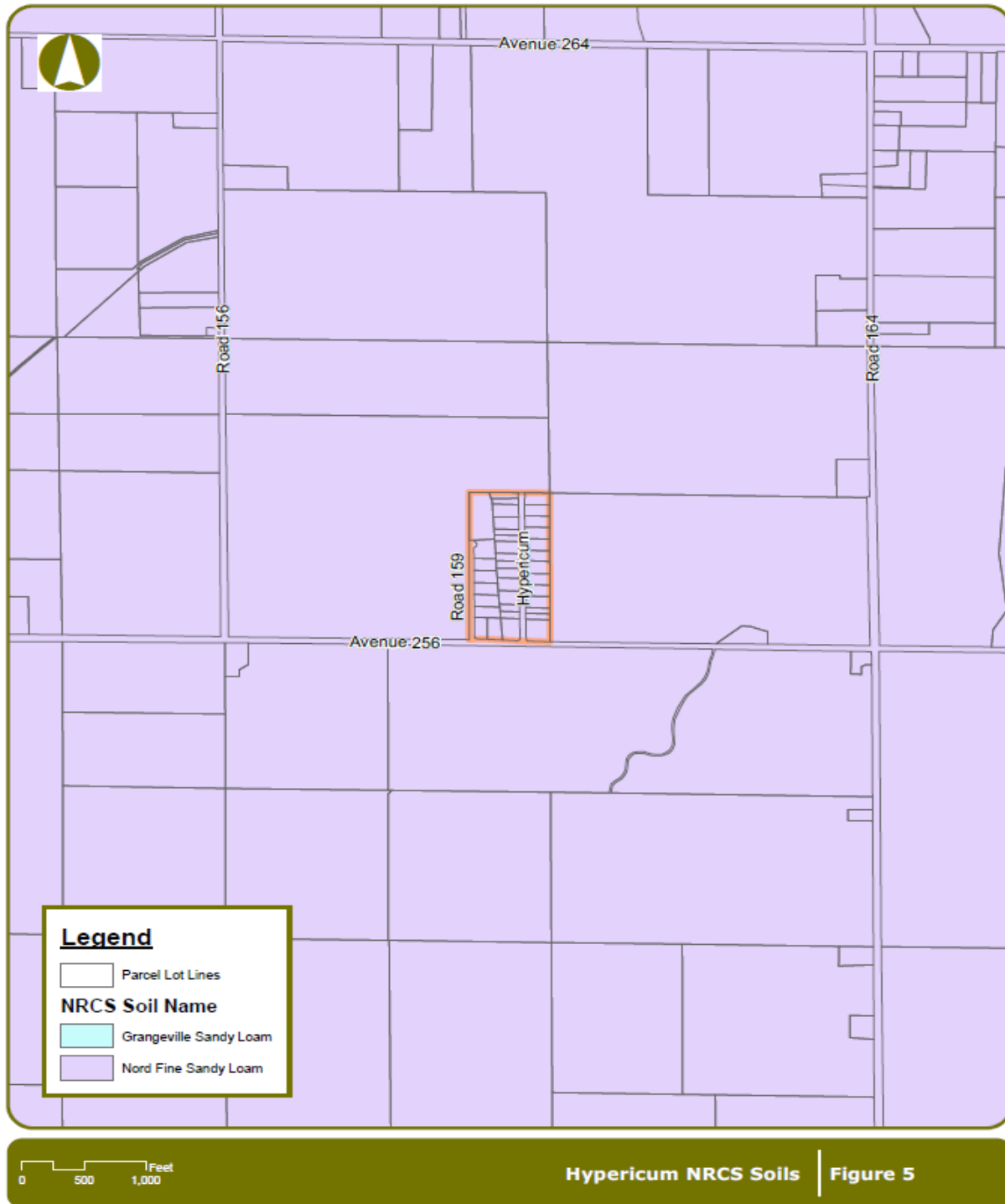
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<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>4</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database August 17, 2006.

# HYPERICUM LEGACY PLAN

Figure 5 - NRCS Soils Map



# HYPERICUM LEGACY PLAN

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## Physical Conditions

### Air Quality

The Hypericum Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Hypericum is located near the central portion of the Valley with prevailing

# HYPERICUM LEGACY PLAN

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winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of Hypericum include Avenue 256 and agricultural activities.

## Flooding<sup>5</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0965E, Hypericum is located within Flood Zone X – Areas of 0.2% annual chance flood; area of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood (see **Figure 6**). Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

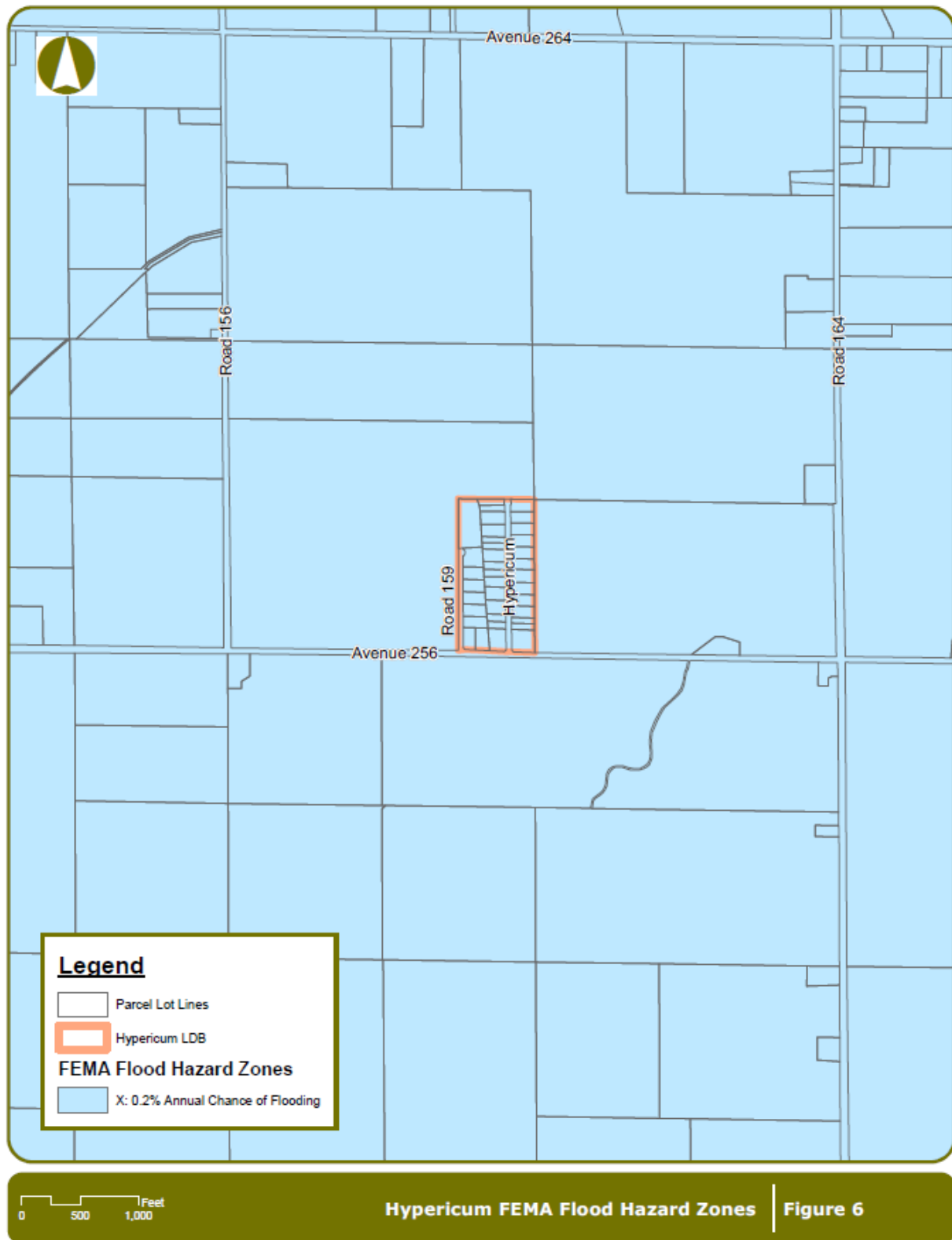
The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), which means the County of Tulare agreed to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available on Tulare County Resource Management Agency website at: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

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<sup>5</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0965E, Panel Number 965 of 2550, June 16, 2009. <https://msc.fema.gov/portal/> accessed January 31, 2017

# HYPERICUM LEGACY PLAN

Figure 6 - FEMA Flood Map



# HYPERICUM LEGACY PLAN

## Noise<sup>6</sup>

Tulare County adopted a new noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. (**Table 4**) summarizes the daily traffic volumes along Avenue 256 from State Route 99 to Road 216. Together, these noise sources place a portion of Hypericum's urbanized areas within the 60 dB Ldn noise contour. The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise.

Table 4 - Noise Levels					
Location	ADT	From Roadway Centerline			
		Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
Avenue 256 from State Route 99 to Road 216	2,210	15	33	70	151

Source: 2010 General Plan Background Report

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Hypericum, this Legacy Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.). The prioritized concerns of the Community include:

- Energy/Nature Gas
- Water System
- Sewer System
- Storm Water Drainage

<sup>6</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006. [https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006. [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)



# HYPERICUM LEGACY PLAN

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- Road Conditions
- Street Lights
- Sidewalks
- Transportation
- Internet
- Community Park

## Energy Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom SCE has arrangements, and rolling blackouts.

## Water System

The drinking water is provided by private individual wells. Data related to any well monitoring in this area is unavailable. Therefore it is difficult to determine the availability of groundwater in the area as well as the water quality and potential deficiencies may exist.

## Sewer System

The community is dependent on individual septic tank systems for sewage disposal.

## Storm Water Drainage

Hypericum does not currently have a storm drainage system.

## Road Conditions

The community of Hypericum is comprised of two roads, Hypericum Road and Road 159.

## Street Lights

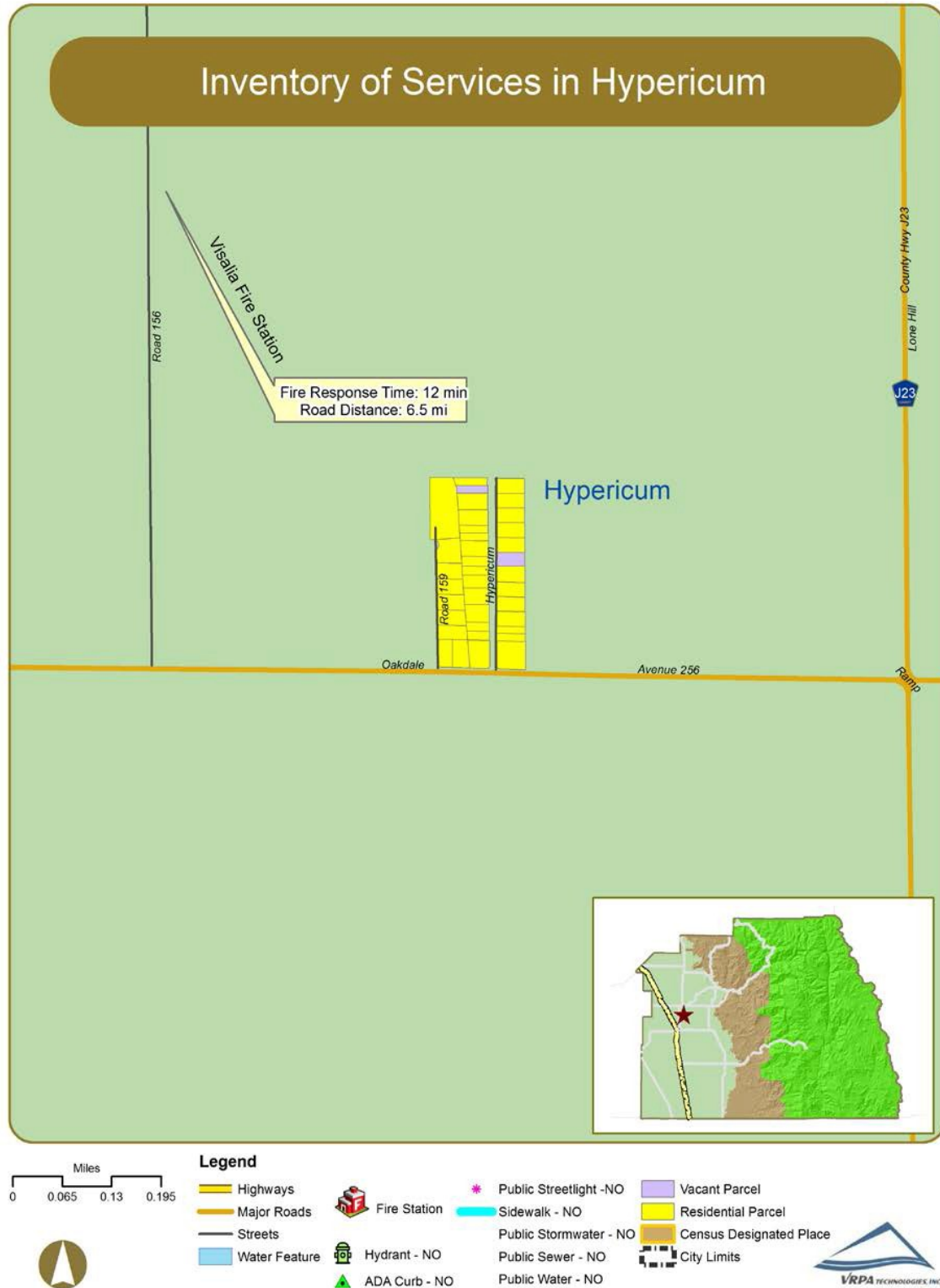
There are currently no street lights located within Hypericum.

## Sidewalks

There are currently no sidewalks located within Hypericum.

# HYPERICUM LEGACY PLAN

Figure 7 - Inventory of Services – Hypericum



# HYPERICUM LEGACY PLAN

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## ADA Curb Ramps

The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

There are currently no ADA compliant curb ramps located in Hypericum.

## **Public Transportation**

### Transit/Bus Stop

Hypericum lacks access to public transportation. Hypericum residents that do not drive find it hard to travel outside of town. Residents often need to travel to Visalia or Farmersville for doctors' appointments and shopping needs. Many residents expressed the need for a bus stop. Bus transportation to and from Hypericum would help many residents with their daily travel needs. The nearest bus stop is 4 miles away in Farmersville.

### AMTRAK

The Hanford AMTRAK station, located approximately 30 miles to the west in Kings County, is the closest station to Hypericum providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

### Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 20 miles west, offers passenger service to Los Angeles. A community general aviation airport is located in Exeter (formerly known as Thunderhawk Field), approximately 10 miles east of Hypericum.

Fresno Yosemite International Airport (FAT), approximately 65 miles northwest of Hypericum, is the principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field, Bakersfield's principal commercial airport, is approximately 75 miles southwest of Hypericum and offers direct flights to several destinations.

### Solid Waste

Solid waste disposal services for the Community of Hypericum is provided by USA Waste, a private company. Solid waste generated in Hypericum can be disposed of at Road 80 Landfill, located at 8614 Avenue 328.

# HYPERICUM LEGACY PLAN

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## Internet Access

Most families in Hypericum do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Hypericum.

## Parks

There are no County owned/operated public parks in Hypericum. The nearest park is located approximately four (4) miles away in the City of Farmersville.

## PUBLIC SERVICES

### Sheriff

Police protection services are provided in Hypericum by the Tulare County Sheriff's Department main Sheriff Office located at 2404 W. Burrel Avenue, in Visalia, California, approximately 20 miles northwest of Hypericum.

### Fire

Tulare County and all of the incorporated cities have a mutual-aid agreement for fire protection services. The closest station to Hypericum (approximately 6.5 miles) is in the City of Visalia, California.

There are currently no fire hydrants are found in Hypericum.

### Schools

Schools are an especially important component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Hypericum Area is within the Exeter Union School District Area 4 (10 miles away). The District provides K-8 grade education at Outside Creek Elementary School. High School students attend high school at the Exeter Union High School.

# HYPERICUM LEGACY PLAN

## Library

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>7</sup> The Exeter Branch Library is located in Exeter 10 miles to the east (see Table 5).

Table 5 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Exeter	Exeter Branch 230 East Chestnut Exeter, CA 93221-1712	Tuesday – Wednesday: 11:00 am – 5:00 pm 6:00 pm – 8:00 pm Thursday - Friday: 9:00 am – 1:00 pm 2:00 pm – 6:00 pm

Library hours current as of May 2017

## CIRCULATION AND TRAFFIC

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

Tulare County’s relationship to the State Route system, nearby counties, cities, and communities:

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>8</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>9</sup>

<sup>7</sup> General Plan Background Report, page 7-96

<sup>8</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>9</sup> Tulare County General Plan, Background Report, page 5-7.

# HYPERICUM LEGACY PLAN

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**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation

Hypericum is a small agricultural community located adjacent to Avenue 256. The community of Hypericum is comprised of two roads, Hypericum Road and Road 159. The major rural collector is Avenue 256. There are no proposed major streets as part of this Legacy Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

# HYPERICUM LEGACY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Hypericum Legacy Plan prescribes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Legacy Boundaries are proposed to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Hypericum to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan, Hypericum Legacy Plan, and input received from Hypericum citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

It is important to recognize that County policies reflect the unique attributes of the various locations and geographic areas in the County. As such, there are policies applicable to one area of the County that are not applicable to others based on natural setting, topography, habitat, existing development, or other attributes which are unique within the planning context of the County. The following planning goals, objectives, policies and standards reflect, as applicable, the rural, suburban, or urban contexts of the Legacy planning areas designated by the County.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned commercial areas.



# HYPERICUM LEGACY PLAN

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4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
5. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
6. The County shall work with the Schools to provide safe routes to school.
7. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Hypericum Legacy Plan.
8. The Hypericum Legacy Plan should be reviewed every five years to determine if amendments are appropriate.
9. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

**Housing**

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to initiate home maintenance/repair programs in Hypericum.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Hypericum.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to construct new housing.



# HYPERICUM LEGACY PLAN

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Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

**GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new commercial development.

Policies:

1. Encourage the Hypericum to give priority to community service development in the areas reserved for commercial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial areas.

Objective: Provide the necessary safe guards to attract quality commercial development to the community.

Policies:

1. Assure that commercial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of commercial areas.

## **Environmental Quality and Public Safety**

**GOAL I: Preserve and enhance the quality of life for present and future generation of Hypericum citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

# HYPERICUM LEGACY PLAN

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Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Hypericum.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Hypericum.

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

Infrastructure In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

Use Permits There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

Education Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Health Care Health care is important for economic development as businesses need healthy employees. The nearest medical offices are in the City of Farmersville and Visalia.

# HYPERICUM LEGACY PLAN

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Legacy Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 6**) and Zoning Districts Plan (**see Table 7**) have been updated.

<b>Table 6 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	17.1
Right-of Ways	2.4
Hypericum LDB	19.5

<b>Table 7 - Proposed Zoning</b>	
Proposed Zones	Acres
C-2-MU	2.5
R-A-43	14.7
Right-of Ways	2.4
Hypericum LDB	19.5

## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the Hypericum Legacy Plan. The following components comprise the Hypericum Plan implementation strategy:

**Limitation of the A-1 Zoning District.** Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

**Zoning District Changes** As part of this Implementation Program for the Hypericum Legacy Plan, there are a variety of changes to existing zoning districts. These changes are described below.

**Chapter 16 of the Zoning Code** Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

**Mixed Use Overlay District** This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Hypericum.

**Zoning Map Update** The current Zoning Map (**see Figure 9**) for Hypericum will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 8**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 10**) Hypericum Proposed Zoning.

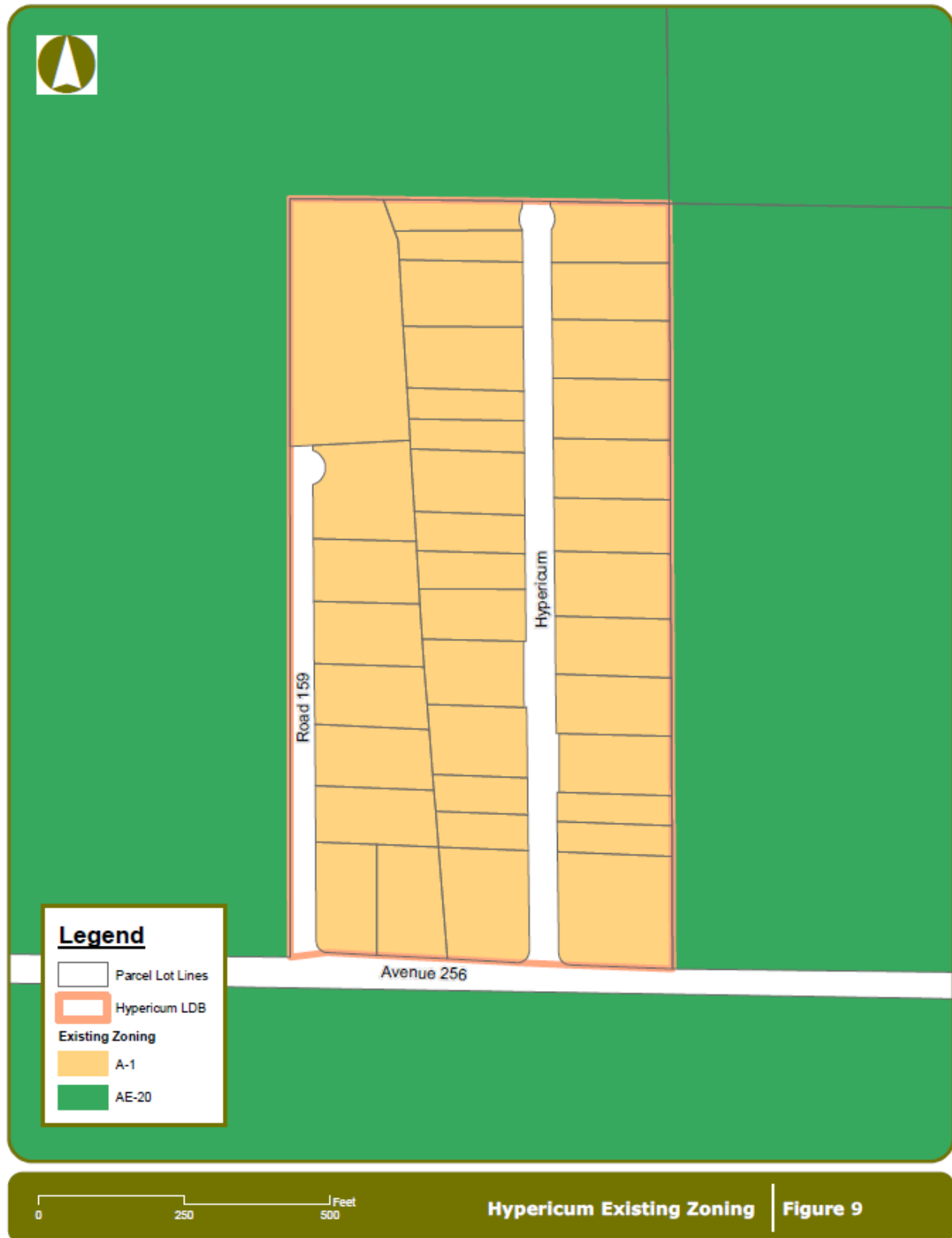
# HYPERICUM LEGACY PLAN

Figure 8 - Proposed Land Use Plan



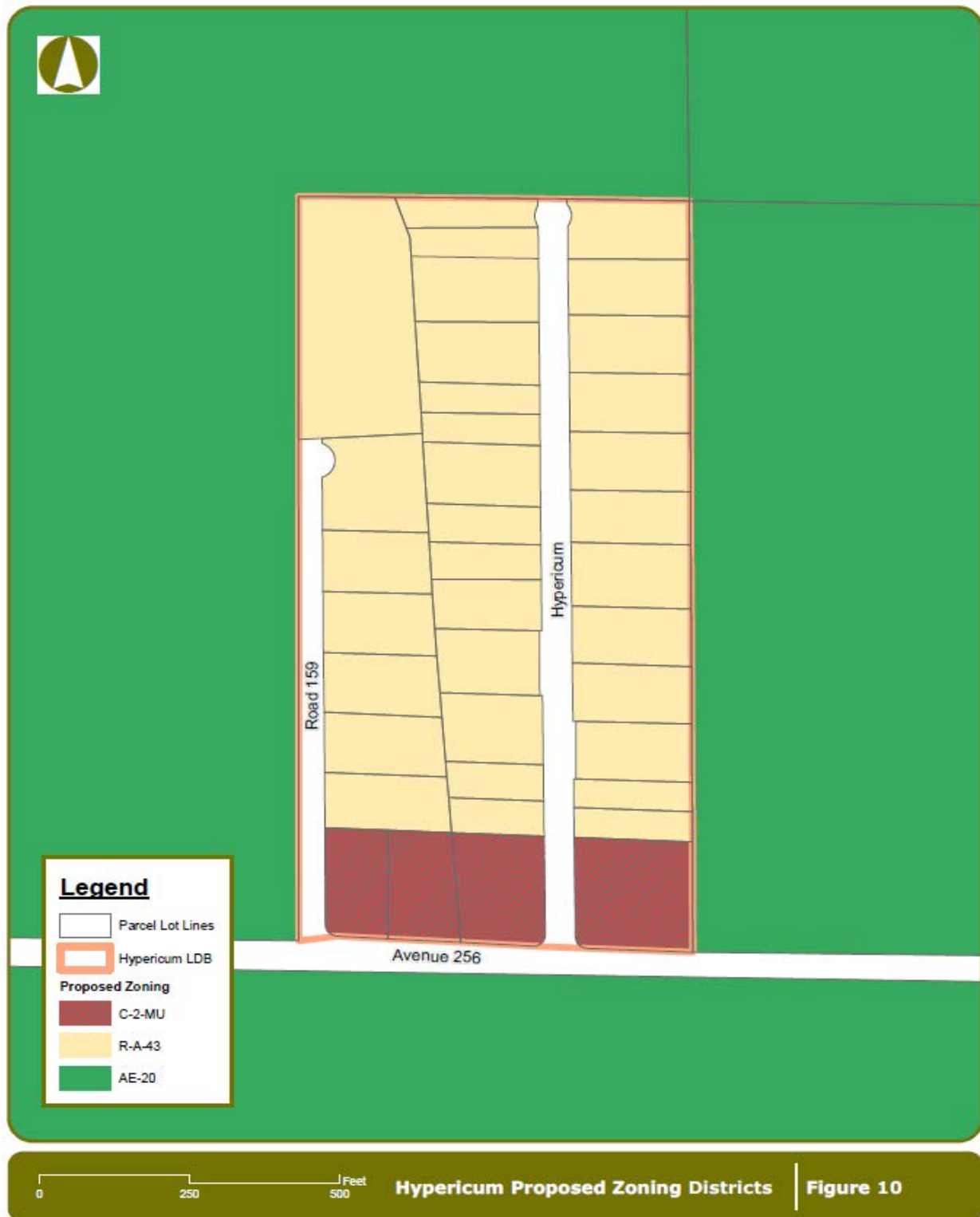
# HYPERICUM LEGACY PLAN

Figure 9 – Existing Zoning Plan



# HYPERICUM LEGACY PLAN

Figure 10 - Proposed Zoning Districts



# HYPERICUM LEGACY PLAN

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## **ATTACHMENTS**

A-1 – Section 16: Use Permit Requirement Changes (Zone Change Text)

A-2 – Section 18: Mixed Use Overlay District (Zone Change Text)

# HYPERICUM LEGACY PLAN

## A-1 Section 16 Use Permit Requirement Changes (Zone Change Text)

### Additional By-Right Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes.	O, CO, C-1, C-2, C-3,	



# HYPERICUM LEGACY PLAN

Structure < 10,000 sq. ft.	M-1, M-2	
Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# HYPERICUM LEGACY PLAN

Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1

# HYPERICUM LEGACY PLAN

Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1 ,C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1 ,C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	

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Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1

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Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone

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Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# HYPERICUM LEGACY PLAN

## A-2 Section 18 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the community of Hypericum, unless otherwise provided in this Ordinance.

### A. PURPOSE

The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

### B. APPLICATION

This overlay zone only applies to the communities of Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Hypericum, Jovista, Matheny Tract and Tooleville..

### C. USE

No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Hypericum.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# HYPERICUM LEGACY PLAN

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The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## D. DEVELOPMENT STANDARDS

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.



# HYPERICUM LEGACY PLAN

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## E. ALL OTHER DEVELOPMENT STANDARDS

All other Development Standards are included below and in the Community Plans for Ducor, Terra Bella, Traver, Strathmore, Pixley, and Tipton, and the Legacy Plans for Hypericum, Jovista, Matheny Tract and Tooleville. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

# HYPERICUM LEGACY PLAN

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A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# HYPERICUM LEGACY PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

# HYPERICUM LEGACY PLAN

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

# HYPERICUM LEGACY PLAN

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LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

# HYPERICUM LEGACY PLAN

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WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.



# HYPERICUM LEGACY PLAN

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SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

# HYPERICUM LEGACY PLAN

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding

buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.



# HYPERICUM LEGACY PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# HYPERICUM LEGACY PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions  
Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9342

General Plan Amendment GPA 17-035-Resolution No. 9343

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9344

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9345

Zoning District Map-Resolution No. 9346

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE LEGACY PLANS 2017 ) RESOLUTION NO. 9342  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, II AND PART III )  
AMENDMENTS (GPA 17-033) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Legacy Plans 2017 Update which consists of General Plan Amendment No. GPA 17-033 amendment to Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities & Services Elements, and Part II Rural Valley Lands Plan, and Part III (El Monte Mobile Village GPA 17-025; Hypericum GPA 17-026; Jovista GPA 17-027; Matheny Tract GPA 17-029 and Tooleville GPA 17-030 amending the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, Change of Zones No. PZC 17-025 El Monte Mobile Village; PZC 17-026 Hypericum; PZC 17-027 Jovista; PZC 17-029 Matheny Tract and Tooleville PZC 17-030, PZC 17-032 for an amendment to Section 16 of Ordinance No. 352, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Development Boundary, and Zone Ordinance Amendment No. PZC 17-031 establishing a Mixed Use Combining Zone in El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville (incorporated by reference herein). The unincorporated Legacy Communities are located within the Mount Diablo Base & Meridian as follows: El Monte Mobile Village, Section 12, Township 16S, Range 23E, Hypericum, Section 24, Township 19S, Range 25E, Jovista, Section 32, Township 24S, Range 26E, Tooleville, Section 12, Township 19S, Range 26E, Matheny Tract Section 22, 23, & 27, Township 20 south, Range 24 East.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element, Transportation & Circulation Element and the Public Facilities & Services Element; (2) add El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Legacy

Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Legacy Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-033 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management and the Transportation and Circulation Element; Change of Zone No. PZC 17-031 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for El Monte Mobile Village, Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista, Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract, Change of Zone No. PCZ 17-029 (Zoning District Map) and Tooleville, Change of Zone No. PZC 17-030 (Zoning District Map) within the respective Legacy Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-032 an amendment to Section 16 to allow additional by-right uses in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Legacy Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Legacy Plans;
  - b. Encourage infill development within Legacy Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Legacy Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The legacy planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on September 15, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Legacy Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Legacy Plans 2017 Update General Plan Amendment 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Change of Zone No. PZC 17-025; Hypericum General Plan Amendment No. GPA 17-026; Change of Zone No. PZC 17-026; Jovista General Plan Amendment No. GPA 17-027; Change of Zone No. PZC 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; Change of Zone No. PZC 17-029; Tooleville General Plan Amendment No. GPA 17-030; Change of Zone No. PZC 17-030; Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-032 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation, Public Facilities & Services, and Part II Rural Valley Lands Plan has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,

4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Legacy Plans 2017 Update. Therefore, the public will benefit from the Legacy Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on October 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano  
NOES: None  
ABSTAIN: None  
ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE ) RESOLUTION NO. 9343  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT AGRICULTURE ELEMENT, SCENIC )  
LANDSCAPES ELEMENT, AIR QUALITY ELEMENT, )  
PUBLIC FACILITIES & SERVICES ELEMENT, )  
INTRODUCTION, COMPONENTS A, B, C, AND D, AND )  
PART II RURAL VALLEY LANDS PLAN OF THE TULARE )  
COUNTY GENERAL PLAN FOR THE LEGACY PLANS )  
2017 UPDATE GPA 17-033 INCLUSIVE OF GPA's 17-025, )  
GPA 17-026, GPA 17-027, GPA 17-29, AND GPA 17-030 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Introduction; Component A-Planning Framework Element; Component B-Agriculture and Land Use Elements, Component C-Scenic Landscapes, Environmental Resources Management and Air Quality Elements; Component D-Transportation & Circulation and Public Facilities & Services Elements; and Part II Rural Valley Lands Plan of the Tulare County General Plan as set forth in Exhibits "A-1 to A-13" and Part III Community Plan as set forth in Exhibits "B-F" for the Legacy Plans 2017 Update General Plan Amendment No. GPA 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Legacy communities as part of the Legacy Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on September 15, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017 and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan for the Legacy Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Legacy Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Legacy Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to establish Legacy Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;

4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the establishment of legacy development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;
5. The Amendment to Part I Introduction, Component A, Component B, Agriculture, Component C, Scenic Landscapes, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and the newly established Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Legacy Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-033 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-033 the Legacy Plans 2017 Update set

forth in attached Exhibits “A-1 to A-13” and Part III Exhibits “B-F” which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

Exhibits “A-1 – A-13” – Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Service Elements, and Part II Rural Valley Lands Plan.

Exhibit “B” – 2017 El Monte Mobile Home Park Legacy Plan (Part III)

Exhibit “C” – 2017 Hypericum Legacy Plan (Part III)

Exhibit “D” – 2017 Jovista Legacy Plan (Part III)

Exhibit “E” – 2017 Matheny Tract Legacy Plan (Part III)

Exhibit “F” – 2017 Tooleville Legacy Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO           )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT     )  
WITH THE ADOPTED LEGACY PLANS 2017       )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE    )  
NO. PZC 17-031                                 )

RESOLUTION NO. 9344

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and

Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9345
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-032	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the communities of El Monte Mobile Village,

Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update, consistent with the Legacy Communities 2017 Update including affirmation that Section 16 H. is applicable in the Matheny Tract.

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	RESOLUTION NO. 9346
IN THE LEGACY DEVELOPMENT BOUNDARIES	)	
EL MONTE MOBILE VILLAGE, HYPERICUM,	)	
JOVISTA, MATHENY TRACT, AND TOOLEVILLE	)	
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS 2017 UPDATE (GPA 17-033), AS	)	
PROPOSED INCHANGE OF ZONE NO. PZC's	)	
17-025, 17-026, 17-027, 17-029, AND 17-030	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" El Monte Mobile Village Rezoning Plan (PZC 17-025), to include the Legacy Community of El Monte Mobile Village, consistent with the adopted El Monte Mobile Village Legacy Plan 2017 Update, Exhibit "E" Hypericum Rezoning Plan (PCZ 17-026), to include the Legacy Community of Hypericum, consistent with the adopted Hypericum Legacy Plan 2017 Update, Exhibit "H" Jovista Rezoning Plan (PCZ 17-027), to include the Legacy Community of Jovista, consistent with the adopted Jovista Legacy Plan 2017 Update, Exhibit "K" Matheny Tract Rezoning Plan (PCZ 17-029), to include the Legacy Community of Matheny Tract, consistent with the adopted Matheny Tract Legacy Plan 2017 Update, Exhibit "N" Tooleville Rezoning Plan, to include the Legacy Community of Tooleville, consistent with the adopted Tooleville Legacy Plan 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update (GPA 17-033) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities;
3. The proposed changes in zone district designations identified in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the El Monte Mobile Village Change of Zone No. PZC 17-025; Hypericum Change of Zone No. PZC 17-026; Jovista Change of Zone No. PZC 17-027; Matheny Tract Change of Zone No. PZC 17-029; and Tooleville Change of Zone No. PZC 17-030, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" and "N" Rezoning Plans consistent with the adopted El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update (GPA 17-033).

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

El Monte Mobile Village

Exhibit "A" – Proposed Zone Ordinance Map (PZC 17-025)

Exhibit "B" – Proposed Zoning Map

Exhibit "C" – List of Affected Properties/Map

Hypericum

Exhibit "D" – Proposed Zone Ordinance Map (PZC 17-026)

Exhibit "E" – Proposed Zoning Map

Exhibit "F" – List of Affected Properties/Map

Jovista

Exhibit "G" – Proposed Zone Ordinance Map (PZC 17-027)

Exhibit "H" – Proposed Zoning Map

Exhibit "I" – List of Affected Properties/Map

Matheny Tract

Exhibit "J" – Proposed Zone Ordinance Map (PZC 17-029)

Exhibit "K" – Proposed Zoning Map

Exhibit "L" – List of Affected Properties/Map

Tooleville

Exhibit "M" – Proposed Zone Ordinance Map (PZC 17-030)

Exhibit "N" – Proposed Zoning Map

Exhibit "O" – List of Affected Properties/Map

Appendix B – Board of Supervisors  
Resolution No. 2017-0868

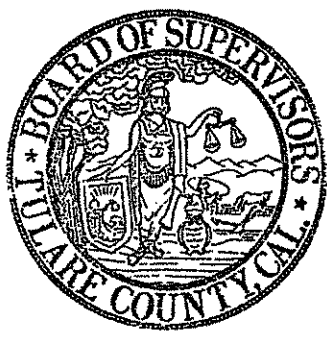


# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER LEGACY PLANS 2017 )  
UPDATE (GENERAL PLAN AMENDMENT 17- ) Resolution No. 2017-0868  
033) AND AMENDMENTS TO THE TULARE ) Ordinance No. 3508  
COUNTY ZONING ORDINANCE NO. 352 )

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR CROCKER, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD OCTOBER 17, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS  
NOES: NONE  
ABSTAIN: NONE  
ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY: *Michael C. Spata*  
Deputy Clerk

\*\*\*\*\*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Legacy Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Legacy Plans 2017 Update (GPA 17-033); and
3. Adopted one General Plan Amendment No. GPA 17-033, for the Legacy Plans 2017 Update inclusive of the El Monte Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029); Tooleville (GPA 17-030) and Amendments to Tulare County General Plan Part III to establish Legacy Plans and Legacy Development Boundaries; Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D,

Public Facilities and Services Elements, and Part II General Plan Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update; and

4. Waived the final reading and adopted Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Legacy Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-031) to Section 18.9 to adopt a Mixed-Use Combining Zone within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-032) to Section 16 to allow additional by-right uses within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for El Monte Village (PZC 17-025), Hypericum (PZC 17-026), Jovista (PZC 17-027), Matheny Tract (PZC 17-029), and Tooleville (PZC 17-030), consistent with the Legacy Plans 2017 Update; and
5. Adopted the findings of approval set forth in Planning Commission Resolution No. 9342 (Addendum to EIR), Planning Commission Resolution No. 9343 (Legacy Plans 2017 Update), Planning Commission Resolution No. 9344 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9345 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9346 (Zoning District Ordinance Maps); and
6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of El Monte Mobile Village, Hypericum, Matheny Tract, Jovista, and Tooleville to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents; and
10. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

## **EXHIBIT "A"**

### **A. Legacy Plans 2017 Update (General Plan Amendment 17-033)**

WHEREAS, on September 27, 2017 the Planning Commission held a public hearing on the Legacy Plans 2017 Update (GPA 17-033), which included an application for El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030, El Monte Mobile Village Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract Change of Zone No. PZC 17-029 (Zoning District Map); and Tooleville Change of Zone No. PZC 17-030 (Zoning District Map), Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9) and Change of Zone No. PZC 17-032 (By-Right Uses–Section 16), collectively referred to as the Legacy Plans 2017 Update (GPA 17-033); and

WHEREAS, upon conclusion of the public hearing, the Planning Commission recommended the Board of Supervisors certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopt the Mitigation Monitoring and Reporting Program and approve the Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors held a public hearing on October 17, 2017 to consider the proposed Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors finds the public hearing for the Legacy Plans 2017 Update was properly noticed in accordance with state law and the Tulare County Ordinance Code.

WHEREAS, the Board of Supervisors finds the Legacy Plans 2017 Update is compatible, integrated and internally consistent with the existing policies of and with the Tulare County General Plan.

WHEREAS, the Board of Supervisors accepts and incorporates by reference herein, the findings of facts and recommendations adopted by the Planning Commission as follows:

1. Planning Commission Resolution No. 9342, in the matter of the Certification and adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update (PC Package Attachment 1 – CEQA Resolution).

2. Planning Commission Resolution No. 9343, to adopt General Plan Amendment No. GPA 17-033 for the Legacy Plans 2017 Update inclusive of El Monte Mobile Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029) and Tooleville (GPA 17-030) and Amendments to Part III of the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, to the Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities and Services Elements, and Part II Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update (PC Package Attachment 2 – GPA Resolution).
3. Planning Commission Resolution No. 9344, in the matter of an Amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed Use Combining Zone within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 3 – Section 18.9 Resolution).
4. Planning Commission Resolution No. 9345, in the matter of an Amendment to Section 16 of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 4 – Section 16 Resolution).
5. Planning Commission Resolution No. 9346, in the matter of an Amendment to Ordinance No. 352, the Zoning Ordinance, as set forth in the Zoning District Ordinance Maps for El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the Legacy Plans 2017 Update to rezone certain properties within El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville. (PC Package Attachment 5 – Rezoning Plan Resolution).

NOW, THEREFORE, BE IT RESOLVED as follows:

A. The Board of Supervisors hereby certifies and adopts the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopts the Mitigation Monitoring Reporting Programs.

NOW BE IT FURTHER RESOLVED as follows:

B. The Board of Supervisors adopts the entirety of all of the above findings and actions as one General Plan Amendment No. GPA 17-033.

C. The Board of Supervisors hereby waives the final reading of Change of Ordinance 352 and adopts amendments to the Tulare County Zoning Ordinance 352 pertaining to Section 18.9 - Mixed-Use Combining Zone, to Section 16 - Variances and Special Uses, and Zoning District Map Changes.

D. The Board of Supervisors authorizes the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of Legacy Plans 2017 Update to include Mixed Use Combining Zone and additional "By-Right" uses.

E. The Board of Supervisors directs the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq.

F. The Board of Supervisors directs the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file Notices of Determination with the Tulare County Clerk.

G. The Board of Supervisors authorizes the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents.

**Jovista Legacy Plan**

# JOVISTA LEGACY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development and Planning Branch



# JOVISTA LEGACY PLAN

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# JOVISTA LEGACY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# JOVISTA LEGACY PLAN

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# JOVISTA LEGACY PLAN

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## Jovista Legacy Plan

Adopted: October 17, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0868

Tulare County Planning Commission  
Recommendations: September 27, 2017  
Resolutions No. 9342, 9343, 9344, 9345, and 9346

General Plan Amendment: GPA 17-033  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-032  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-031

Jovista Legacy Plan: GPA 17-027  
Zoning District Map: PZC 17-027



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# JOVISTA LEGACY PLAN

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# JOVISTA LEGACY PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# JOVISTA LEGACY PLAN

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# JOVISTA LEGACY PLAN

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## **County Administrative Office**

Michael C. Spata, CAO  
Eric Coyne, Deputy CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director  
Michael Washam, Associate Director  
Aaron Bock, Chief Planner, Project Processing  
Dave Bryant, Chief Planner, Special Projects  
Susan Simon, Planner III  
Velma Quiroz, Planning Tech II  
Johnson Vang, Engineer I

# JOVISTA LEGACY PLAN

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# JOVISTA LEGACY PLAN

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# JOVISTA LEGACY PLAN

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# JOVISTA LEGACY PLAN

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## INTRODUCTION

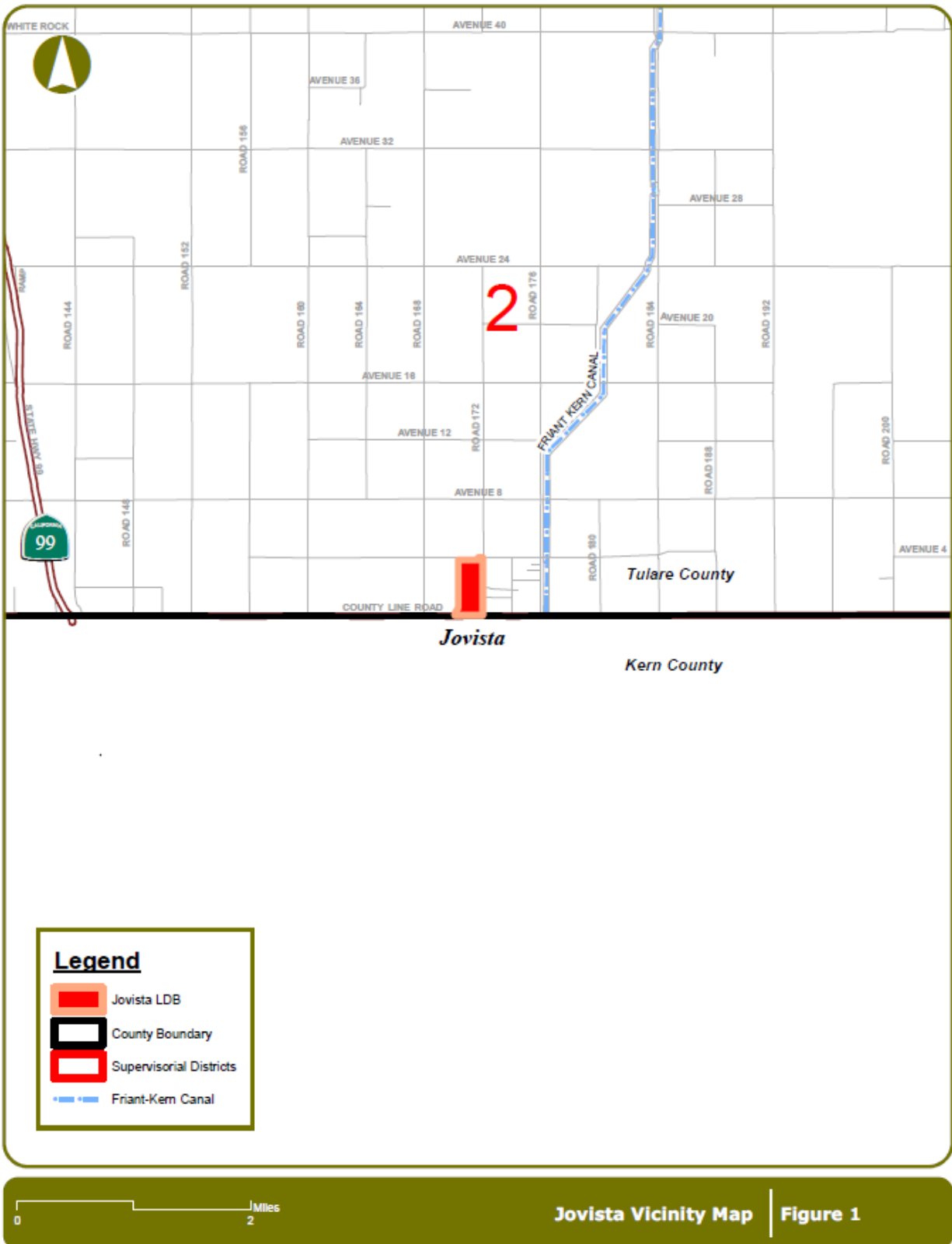
An important objective in preparing a Legacy Plan for Jovista is to develop a plan which can accurately reflect the needs and priorities of this community. Jovista was designated in the 2030 Tulare County General Plan (2012) through the SB 244 Study under GPA 2015-D. It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding (for things such as drinking water system improvements [for example wells, water distribution piping, and storage tanks], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

### Location

Jovista (**see Figure 1**) is located on the eastern side of the San Joaquin Valley, approximately 50 miles north of Bakersfield and 75 miles southeast of Fresno. Jovista is located just north of the Tulare County / Kern County line. Jovista (**see Figure 2**) is a small community located approximately 4 miles west of the Community of Richgrove and 4 miles south of the City of Delano, Kern County. Jovista is in Section 32, Township 24 south, Range 26 East MDB&M, and can be found within the Delano East United States Geological Survey 7.5 minute topographic quadrangle. Jovista is located at an elevation of 411 feet above sea level, National Geodetic Vertical Datum. The coordinates of Jovista are Latitude: 35.796437, Longitude: -119.188131. Sierra Vista Ranch Housing Certificates of Compliance were issued in May of 1984. Jovista is located in Census Tract 43, Block Group 3. The population in Jovista is approximately 68 people, Medium Household Income is \$29,766 comprised of approximately 18 parcels of property containing approximately 17 residential units.

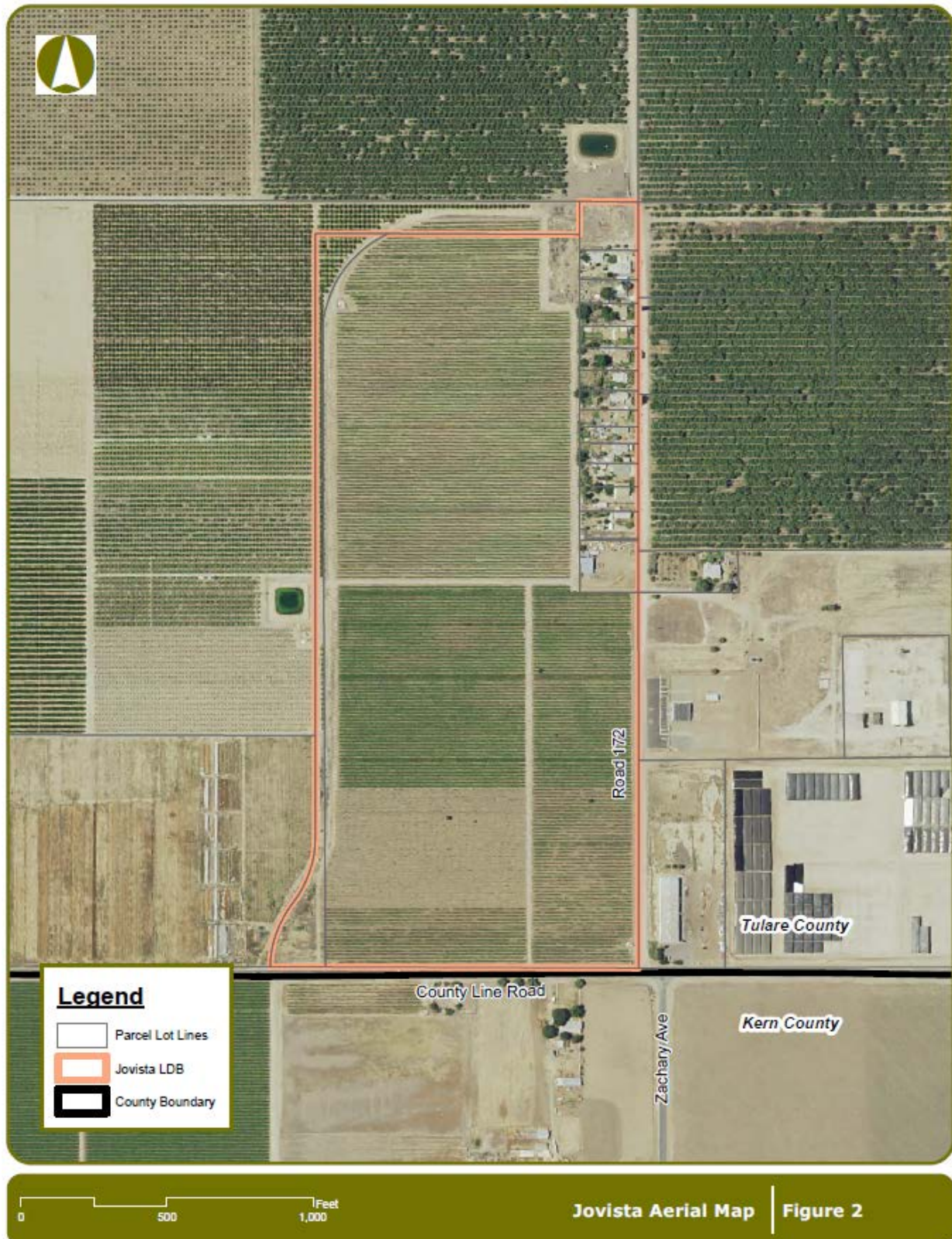
# JOVISTA LEGACY PLAN

Figure 1 - Vicinity Map



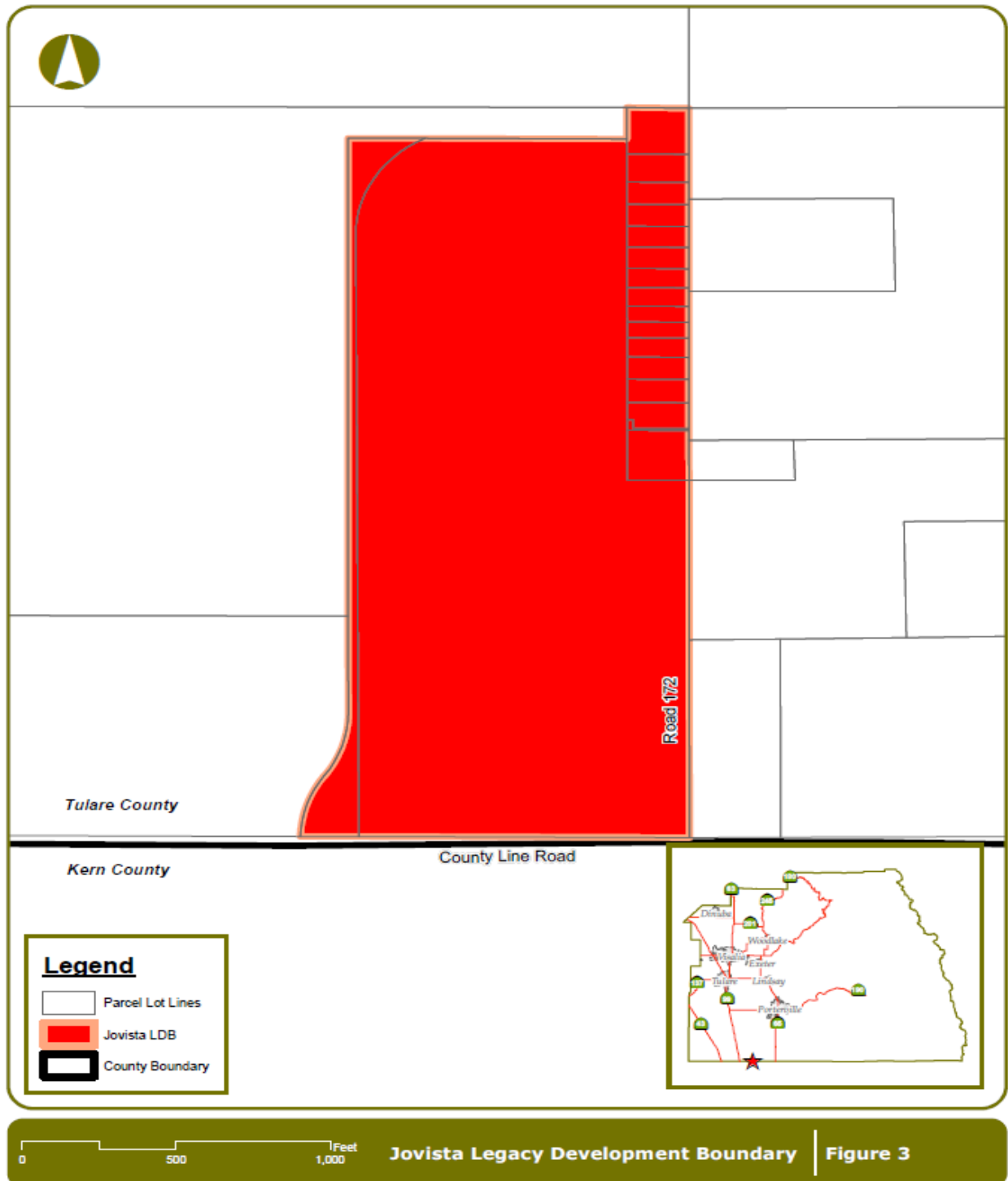
# JOVISTA LEGACY PLAN

Figure 2 - Jovista Aerial



# JOVISTA LEGACY PLAN

Figure 3 - Jovista LDB





# JOVISTA LEGACY PLAN

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## DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Legacy's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Legacy plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Legacy allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Legacy. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller Legacy such as Jovista.

### Population

Jovista is located within Census Tract 43, Block Group 3. The population for Census Tract 43 was 6,901 people, with approximately 68 people living in Jovista (**Census data: ACS 2015 5-year**).

### Median Age

Jovista's median age of 33 is lower than the median age of the State of California (**Census data: ACS 2015 5-year**) (see **Table 1**).

<b>Table 1 - Median Age</b>	
<b>Geography</b>	<b>Median Age (years)</b>
California	35.8
Tulare County	30.3
Jovista	33

## ECONOMIC CONDITIONS

### Employment in Jovista

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production.

### Median Household Income

In 2015, Jovista's median household income was \$29,766 whereas the State of California's median household income was \$61,818 (**Census data: ACS 2015 5-year**) (see **Table 2**). Therefore, based upon Jovista's median household income of \$29,766 (which is about 48% of the State of California's median household income), it is considered a disadvantaged community.

# JOVISTA LEGACY PLAN

**Table 2 - Median Household Income**

Median Household Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Jovista	\$29,766		\$16,606

## Severely Disadvantaged Community

Jovista is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

## HOUSING CHARACTERISTICS

### Housing Conditions

**Table 3 - 2015 Housing Conditions Survey**

Table 3 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Jovista	0	0	17	100							17

Source: Tulare County 2015 Housing Element

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A "sound" housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person's ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

As indicated by Tulare County Building Department approximately 100% of the housing units in Jovista are in a minor deteriorated condition (see Table 3).

# JOVISTA LEGACY PLAN

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## Household Size (Overcrowding)

In 2015 the average housing units and occupancy were 3.7 persons per household (**Census data: ACS 2015 5-year**).

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## ENVIRONMENTAL CONDITIONS

### Natural Conditions

#### Wildlife<sup>2</sup>

A California Natural Diversity Database (CNDDB) search conducted on November 1, 2016 (**see Figure 4**) indicates there are special status species within the Delano East Quadrant Species List (which includes the Jovista Planning Study Area) consisting of no animal species and one (1) plant species: California jewelflower (*Caulanthus californicus*, Federal and State Endangered).

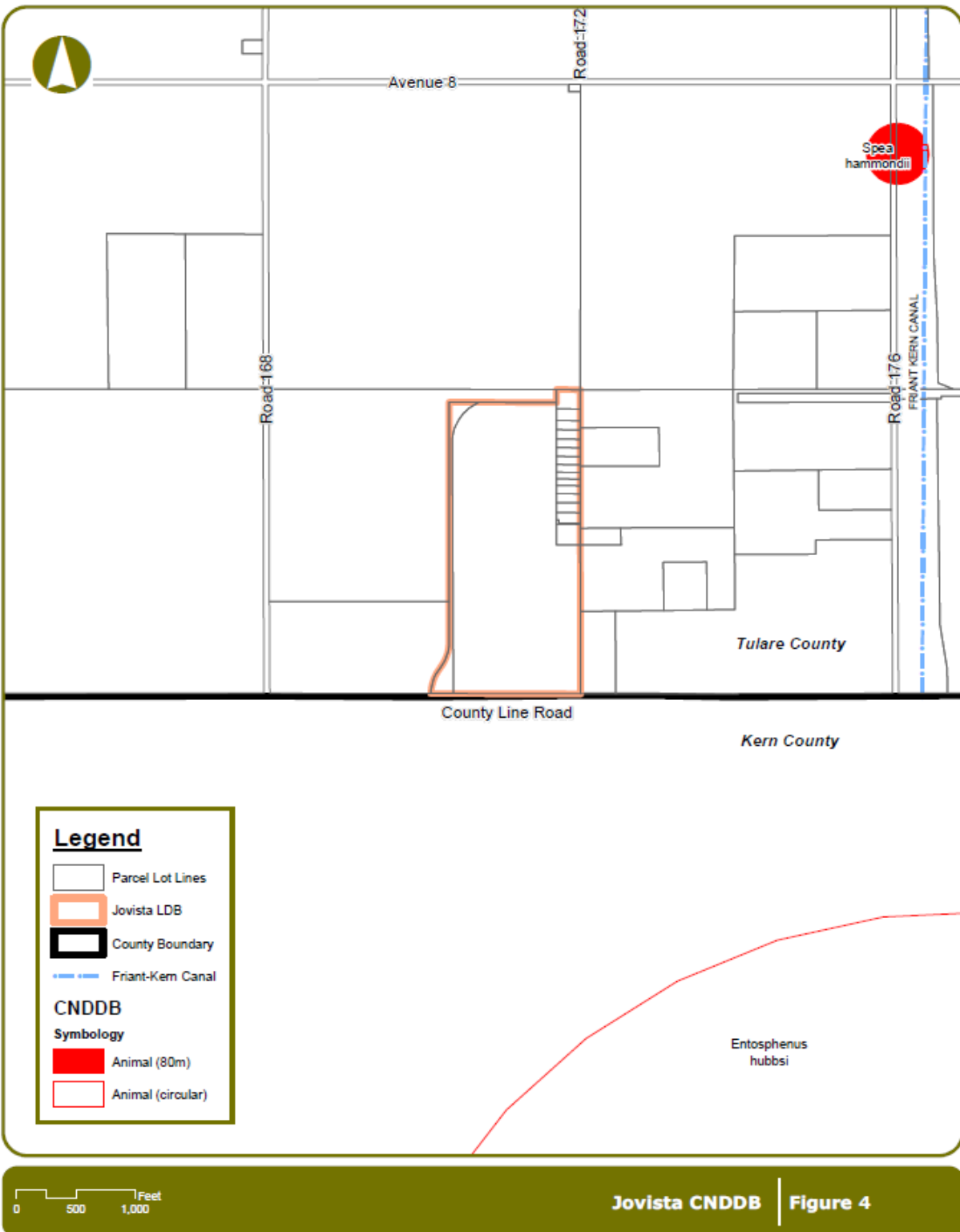
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<sup>1</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDB/Maps-and-Data> accessed November 1, 2016

# JOVISTA LEGACY PLAN

Figure 4 - CNDDDB Map



# JOVISTA LEGACY PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Jovista are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and Figure 10-5 (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Jovista] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils<sup>4</sup>

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Jovista throughout the Legacy Development Boundary (LDB). The following description is provided for the above soil type:

**Hanford sandy loam, 0 to 2 percent slopes.** Consists of very deep, well drained; negligible to low runoff, moderate rapid permeability. Hanford soil is used for such crops as fruits, vegetables, and general farm crops, dairies, and urban development.

**Wasco sandy loam, 0 to 2 percent slopes,** Consists of very deep, well drained; negligible to low runoff, moderate rapid permeability. Wasco soil is used for growing field, forage and row crops.

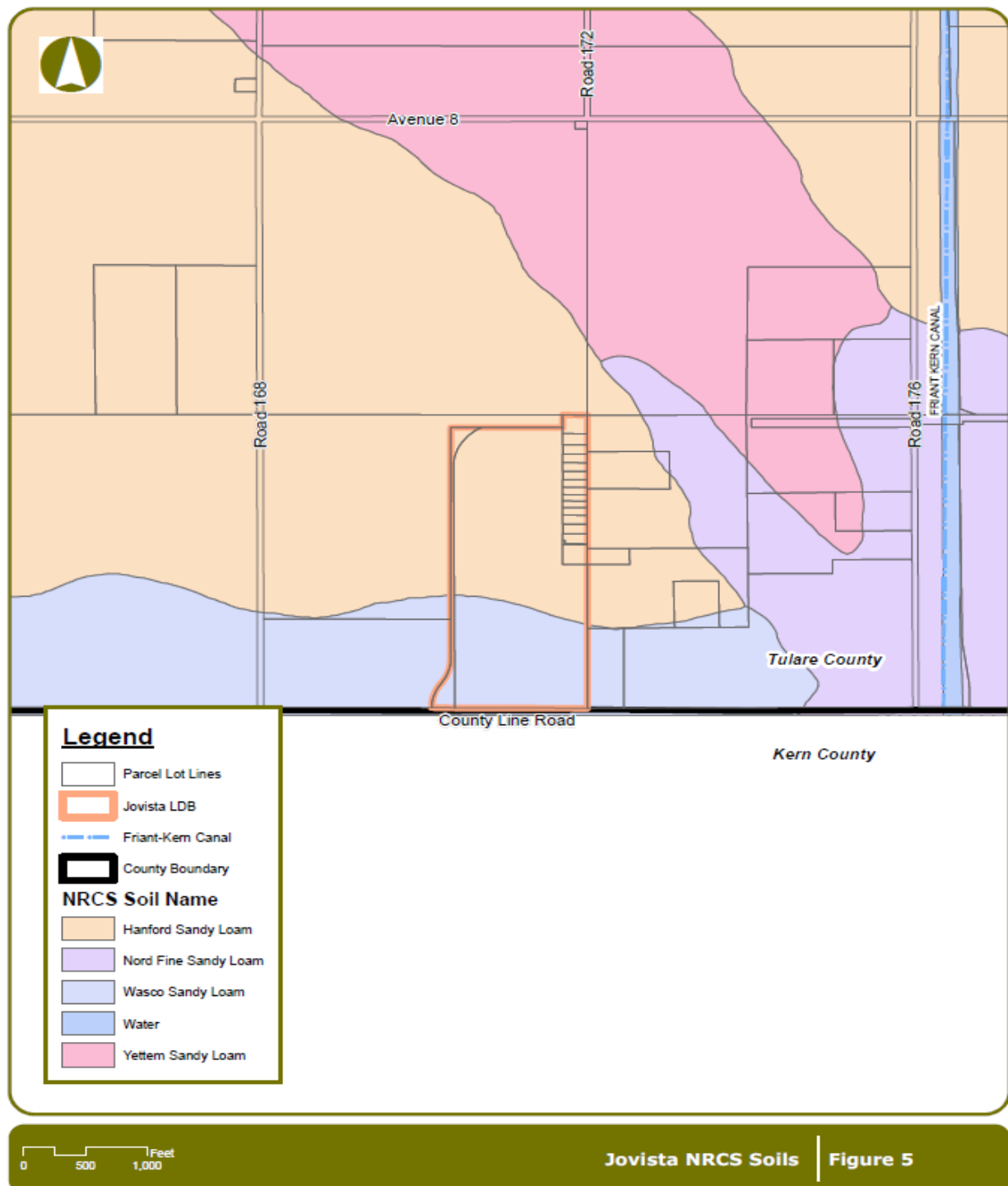
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<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

<sup>4</sup> United States Department of Agriculture, Natural Resources Conservation Service, Soils Map for Central Tulare County, 2009 Database August 17, 2006.

# JOVISTA LEGACY PLAN

Figure 5 - NRCS Soils Map



# JOVISTA LEGACY PLAN

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## Physical Conditions

### Air Quality

The Jovista Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Jovista is located near the central portion of the Valley with prevailing

# JOVISTA LEGACY PLAN

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winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of Jovista include agricultural activities.

## Flooding<sup>5</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C2300E. Jovista is located within Flood Zone X – areas of 0.2% annual chance flood; area of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood (**see Figure 6**). Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), which means the County of Tulare agreed to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available on Tulare County Resource Management Agency website at: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

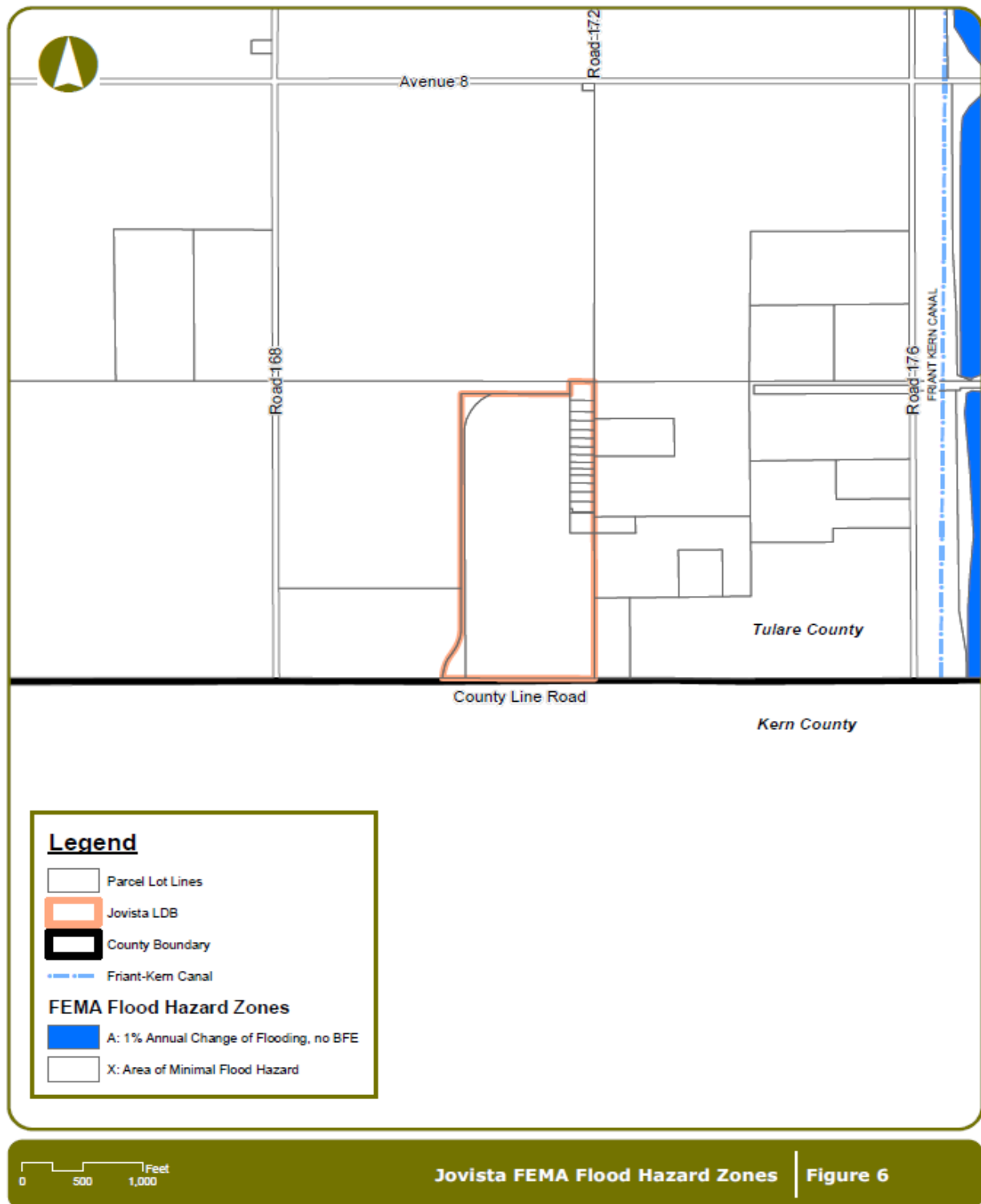
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<sup>5</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C2300E, Panel Number 2300 of 2550, June 16, 2009. <https://msc.fema.gov/portal/> accessed May 23, 2017



# JOVISTA LEGACY PLAN

Figure 6 - FEMA Flood Map



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## Noise<sup>6</sup>

Tulare County adopted a new noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBa) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the general Plan Background Report Jovista lies outside any noise source.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Jovista, this Legacy Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.). The prioritized concerns of the Community include:

1. Energy
2. Water
3. Sewer
4. Storm Water Drainage
5. Solid Waste
6. Roads & Circulation
7. Street Lights
8. Sidewalks
9. ADA Curb Ramps
10. Public Transportation
11. Internet

## Energy Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

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<sup>6</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.  
[https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.  
[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and  
[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

# JOVISTA LEGACY PLAN

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On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom PG&E has arrangements, and rolling blackouts.

Residents of Jovista use propane as their source of fuel.

## Water Supply

The drinking water is provided by private and/or small community wells. Data related to any well monitoring in this area is unavailable. Therefore it is difficult to determine the availability of groundwater in the area as well as the water quality and potential deficiencies may exist.

## Sewer

The community is dependent on individual septic tank systems for sewage disposal.

## Storm Water Drainage

Jovista does not currently have a storm drainage system.

## Solid Waste

Solid waste disposal services for Jovista Legacy Plan is provided by South Tulare Richgrove, a private company. Solid waste generated in Richgrove can be disposed of at Teapot Dome Landfill, located at 21063 Avenue 128, Porterville.

## Roads and Circulation

Jovista is comprised of a private road (Zachary Road/Road 172).

## Street Lights

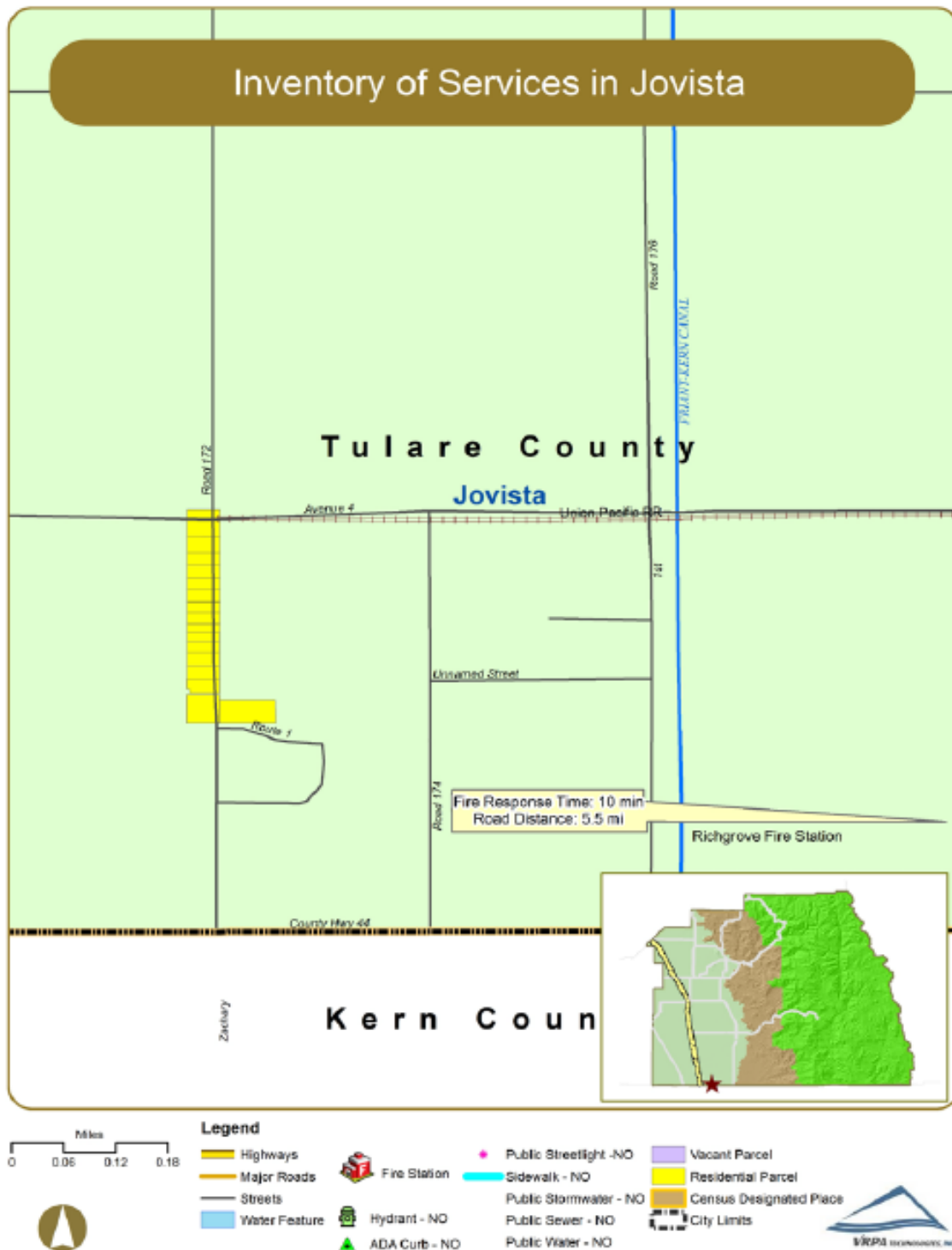
There are currently no street lights located in Jovista.

## Sidewalks

There are currently no sidewalks located in Jovista.

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Figure 7 - Inventory of Services in Jovista



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## ADA Curb Ramps

The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street. Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

There are currently no ADA compliant curb ramps located in Jovista.

## **Public Transportation**

### Transit

Jovista lacks access to public transportation. Jovista residents that do not drive find it hard to travel outside of town. Residents often need to travel to Earlimart or Delano for doctors' appointments and shopping needs. Bus transportation to and from Jovista would help many residents with their daily travel needs. The nearest bus stop is approximately four (4) miles east in Richgrove.

### AMTRAK

The Corcoran AMTRAK station, located 25 miles to the northwest in Kings County, is the closest station to Jovista providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

### Aviation

The nearest operational general aviation airport is Meadows Field, Bakersfield's principal commercial airport, is approximately 30 miles south of Jovista and offers direct flights to several destinations.

Fresno Yosemite International Airport (FAT), approximately 85 miles northwest of Jovista, is the principal passenger and airfreight airport in the central San Joaquin Valley. Visalia Municipal Airport, approximately 40 miles north, offers passenger service to Los Angeles.

### State Route 99

State Route (SR) 99 is the major highway between cities within the Central Valley and the primary route between the City of Fresno to the north and the City of Bakersfield to the south. Jovista is located approximately five (5) miles east of SR 99.

### Internet Access

Most families in Jovista do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in

# JOVISTA LEGACY PLAN

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nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Jovista.

## PUBLIC SERVICES

### Sheriff

The Tulare County Sheriff's Department has a Pixley Substation covering 552 square miles and serving a rural population to include the unincorporated communities of Alpaugh, Allensworth, Teviston, Earlimart, Pixley and Tipton which holds approximately 45,000 people. The Substation runs a four shift operation which includes 16 deputies, five sergeants and one lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. The substation is open for walk-ins from 8:00 am to 5:00 pm. After hours, there is a phone provided outside the substation that calls directly into the dispatch center.

### Fire

Fire protection and emergency medical services are provided for Jovista by the Tulare County Fire Department. The community is served by Tulare County Fire Department Station #10, located at 20892 Grove Drive in Richgrove, California. Engine 10 is assigned to this location. No fire hydrants are found in Jovista.

### Schools

Schools are an especially important component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Jovista Legacy Plan Area is within the Richgrove School District (20890 Grove Drive), a one-school district. The District provides K-8 education at its Richgrove School. High School students attend high school in Delano.

### Libraries

"The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch."<sup>7</sup> The Earlimart Branch Library is located in Earlimart 10 miles to the northwest (see Table 4).

Table 4 - Library Location & Hours		
Branch	Address	Service Hours
Earlimart	780 East Washington St. P.O. Box 12153 Earlimart, CA 93219-	Tuesday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm Wednesday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm Thursday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm Friday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm

Library hours current as of May 2017

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<sup>7</sup> General Plan Background Report, page 7-96

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## Parks

There are no County owned/operated public parks in Jovista. In the community of Jovista there is no available open green space for children to play and adults to be physically active. The nearest park is located approximately four (4) miles east in the community of Richgrove.

## CIRCULATION AND TRAFFIC

“For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

Tulare County’s relationship to the State Route system, nearby counties, cities, and communities

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>8</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

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<sup>8</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014, Page 3-54.

# JOVISTA LEGACY PLAN

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**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation and Transportation Conditions

Jovista is a small agricultural community comprised of a private road (Zachary Road/Road 172). There are no proposed major streets as part of this Legacy Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.



# JOVISTA LEGACY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Jovista Legacy Plan prescribes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Legacy Boundaries are proposed to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Jovista to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan, Jovista Legacy Plan. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

It is important to recognize that County policies reflect the unique attributes of the various locations and geographic areas in the County. As such, there are policies applicable to one area of the County that are not applicable to others based on natural setting, topography, habitat, existing development, or other attributes which are unique within the planning context of the County. The following planning goals, objectives, policies and standards reflect, as applicable, the rural, suburban, or urban contexts of the Legacy planning areas designated by the County.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts**

**through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for industry, commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming commercial and industrial concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned shopping areas.
4. Require public, quasi-public and high density residential uses to locate where direct

# JOVISTA LEGACY PLAN

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access to major streets is available.

5. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.

6. The County shall work with the Schools to provide safe routes to school.

7. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Jovista Community Plan.

8. The Jovista Legacy Plan should be reviewed every five years to determine if amendments are appropriate.

9. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

## Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.

2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

## Policies:

1. Apply the health, safety and welfare

standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.

2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.

3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.

4. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to initiate home maintenance/repair programs in Jovista.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

## Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.

2. Assure that the housing industry is made aware of residential development potentials in Jovista.

3. Coordinate residential zoning with availability of utilities and community services.

4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.

5. Encourage housing advocacy groups, such as Habitat for Human and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

## Policies:

1. Allow for development of mobilehome parks in appropriate locations.

2. Permit mobilehomes to be installed on residentially designated lots within the original

# JOVISTA LEGACY PLAN

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townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.

3. Discourage mobilehomes on individual lots outside the original townsites, except when necessary for caretaker use in conjunction with commercial and industrial activities.

4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.

5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

**GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

### Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.

2. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

### Policies:

1. Encourage the Jovista to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.

2. Place emphasis on development and upgrading of water supply facilities to meet fire

protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

### Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.

2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

## **Environmental Quality and Public Safety**

**GOAL I: Preserve and enhance the quality of life for present and future generation of Jovista citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

### Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.

2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.

3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

### Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.

2. Facilitate innovation in housing and subdivision design so that private recreation

# JOVISTA LEGACY PLAN

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and open space areas can be accommodated.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Jovista.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Jovista.

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

Infrastructure In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

Use Permits There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

Education Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Health Care Health care is important for economic development as businesses need healthy employees. The nearest medical offices are located in the community of Earlimart and the City of Delano, California, Kern County.

# JOVISTA LEGACY PLAN

## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Legacy Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan (**see Table 5**) and Zoning Districts Plan (**see Table 6**) have been updated.

<b>Table 5 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	65.1
Right-of Ways	0.0
Jovista LDB	65.1

<b>Table 6 - Proposed Zoning</b>	
Proposed Zones	Acres
AE-20	58.9
C-2-MU	0.9
R-A-43	5.3
Right-of Ways	0.0
Jovista LDB	65.1

## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the general plan recommendations contained in the Jovista Legacy Plan. The following components comprise the Jovista Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

**Zoning District Changes** As part of this Implementation Program for the Jovista Legacy Plan, there are a variety of changes to existing zoning districts. These changes are described below.

**Chapter 16 of the Zoning Code** **Revise Chapter 16** of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

**Mixed Use Overlay District** This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Jovista. The mixed use land designation would provide for potential commercial activities in addition to the residential development.

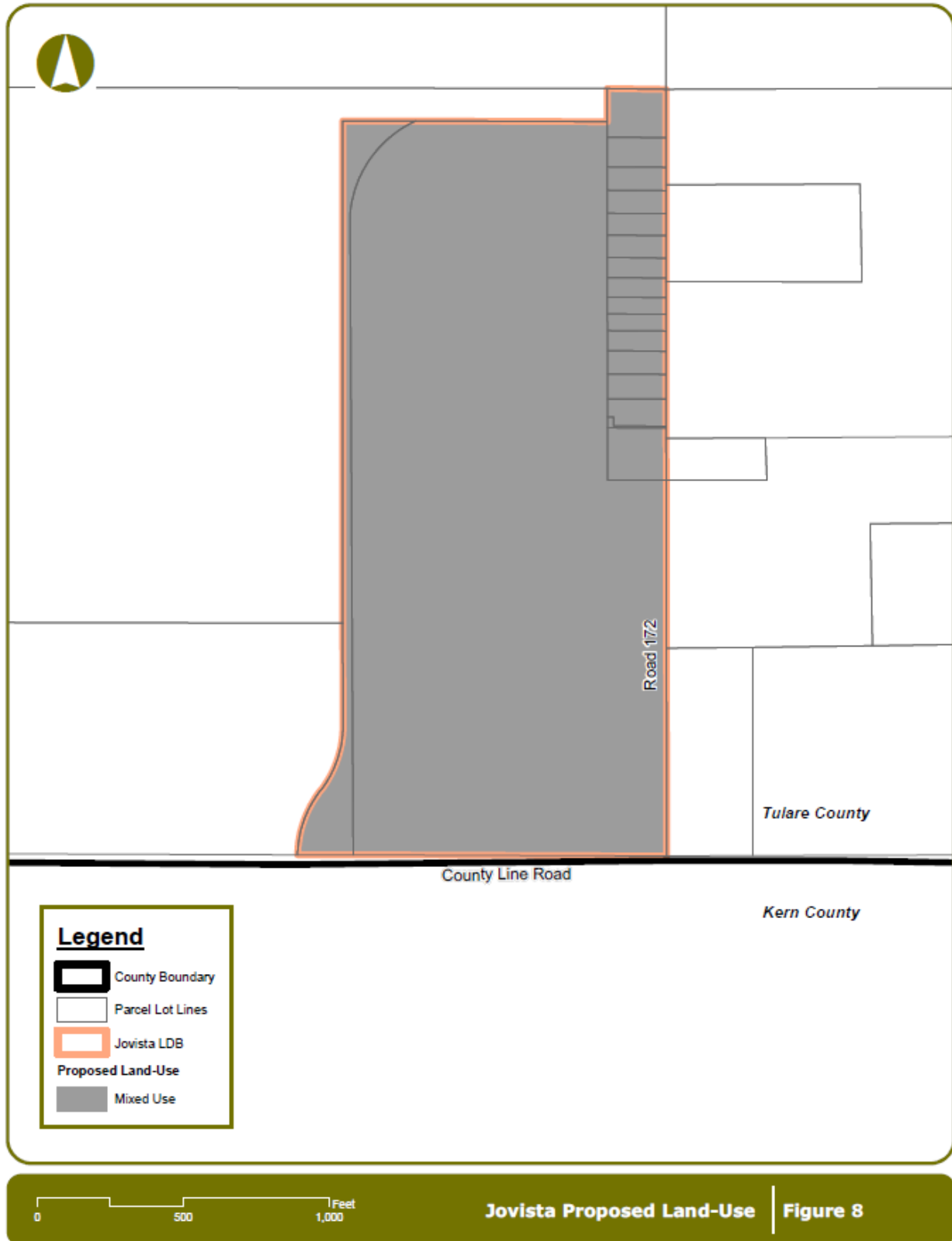
# JOVISTA LEGACY PLAN

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**Zoning Map Update** The current Zoning Map (**see Figure 9**) for Jovista will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 8**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 10** Jovista Proposed Zoning).

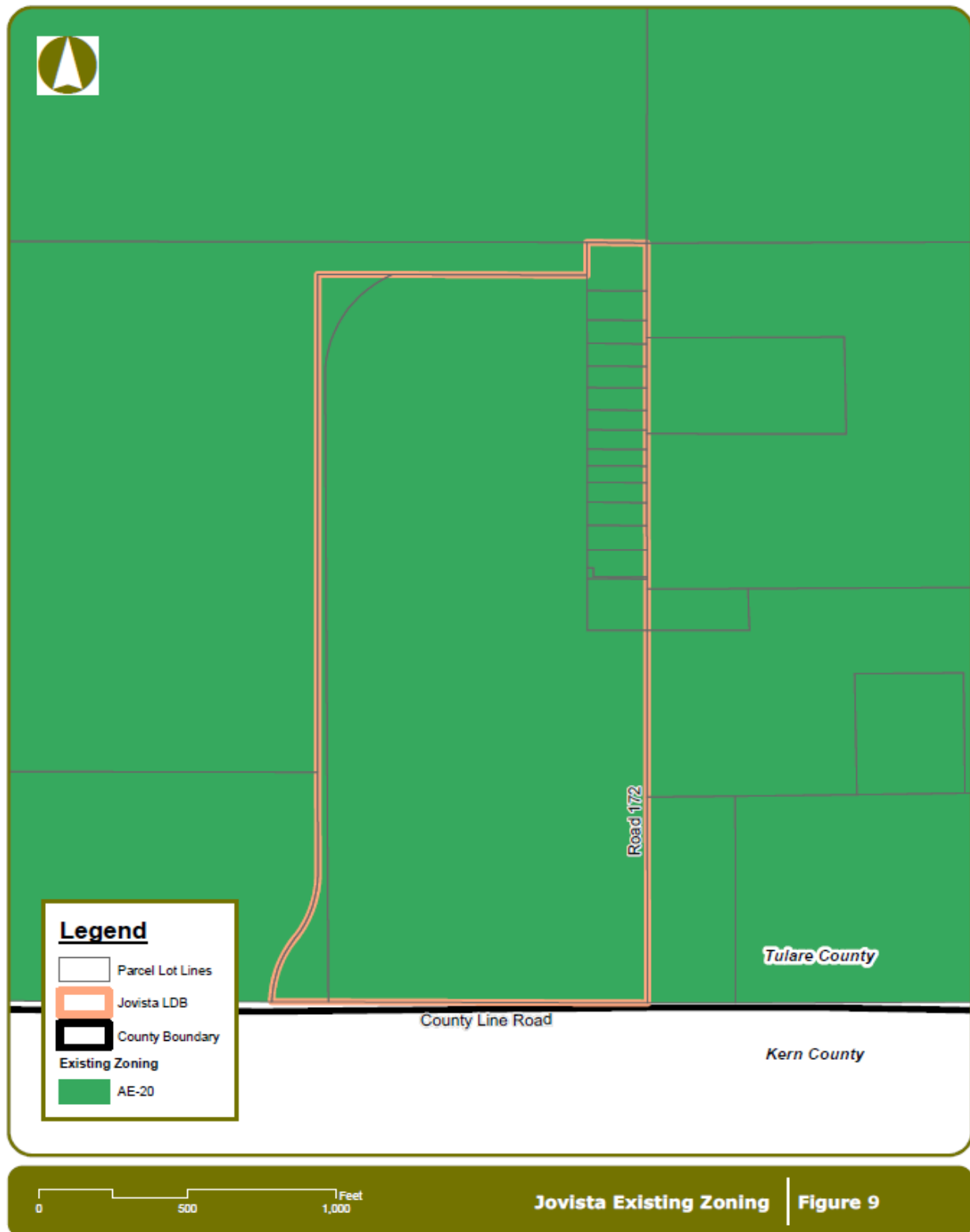
# JOVISTA LEGACY PLAN

Figure 8 - Proposed Land Use Plan



# JOVISTA LEGACY PLAN

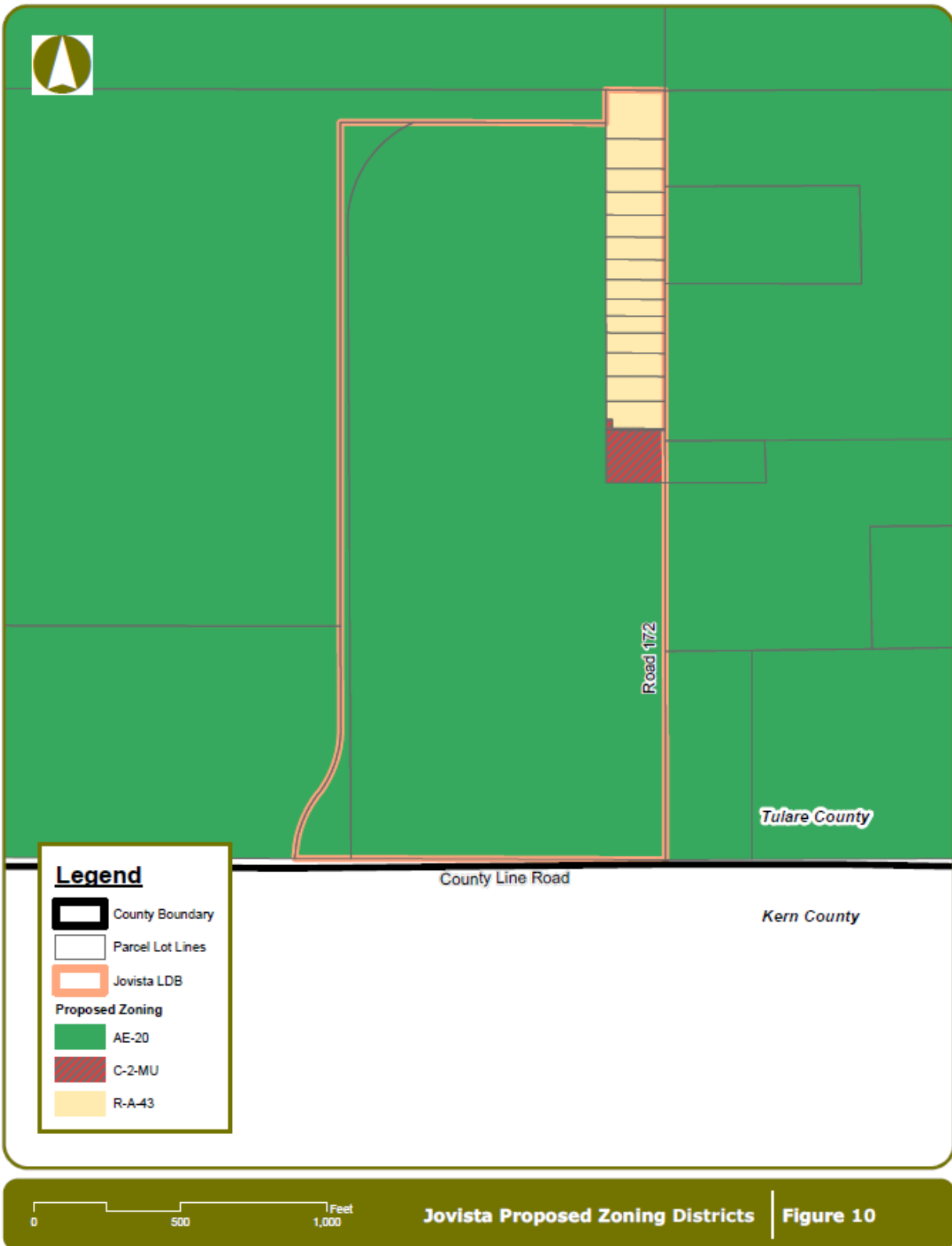
Figure 9 – Existing Zoning Plan





# JOVISTA LEGACY PLAN

Figure 10 - Proposed Zoning Districts



# JOVISTA LEGACY PLAN

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## **ATTACHMENTS**

A-1 – Section 16: Use Permit Requirement Changes (Zone Change Text)

A-2 – Section 18: Mixed Use Overlay District (Zone Change Text)

# JOVISTA LEGACY PLAN

## A-1 Use Permit Requirement Changes (Zone Change Text)

### Additional By-Right Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

# JOVISTA LEGACY PLAN

Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure <	CO, C-1, C-2, C-3, M-1	C-3, M-1

# JOVISTA LEGACY PLAN

10,000 sq. ft.		
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1

# JOVISTA LEGACY PLAN

Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1 ,C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1 ,C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	

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Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1

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Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone



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Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# JOVISTA LEGACY PLAN

## A-2 Section 18 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the community of Jovista, unless otherwise provided in this Ordinance.

### A. PURPOSE

The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

### B. APPLICATION

This overlay zone only applies to the communities of Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Jovista, Jovista, Matheny Tract and Tooleville..

### C. USE

No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Jovista.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# JOVISTA LEGACY PLAN

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The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## D. DEVELOPMENT STANDARDS

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

# JOVISTA LEGACY PLAN

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## E. ALL OTHER DEVELOPMENT STANDARDS

All other Development Standards are included below and in the Community Plans for Ducor, Terra Bella, Traver, Strathmore, Pixley, and Tipton, and the Legacy Plans for Hypericum, Jovista, Matheny Tract and Tooleville. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

# JOVISTA LEGACY PLAN

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A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# JOVISTA LEGACY PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

# JOVISTA LEGACY PLAN

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.



# JOVISTA LEGACY PLAN

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LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.



# JOVISTA LEGACY PLAN

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WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of

# JOVISTA LEGACY PLAN

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the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

# JOVISTA LEGACY PLAN

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SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

# JOVISTA LEGACY PLAN

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## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# JOVISTA LEGACY PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9342

General Plan Amendment GPA 17-035-Resolution No. 9343

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9344

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9345

Zoning District Map-Resolution No. 9346

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE LEGACY PLANS 2017 ) RESOLUTION NO. 9342  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, II AND PART III )  
AMENDMENTS (GPA 17-033) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Legacy Plans 2017 Update which consists of General Plan Amendment No. GPA 17-033 amendment to Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities & Services Elements, and Part II Rural Valley Lands Plan, and Part III (El Monte Mobile Village GPA 17-025; Hypericum GPA 17-026; Jovista GPA 17-027; Matheny Tract GPA 17-029 and Tooleville GPA 17-030 amending the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, Change of Zones No. PZC 17-025 El Monte Mobile Village; PZC 17-026 Hypericum; PZC 17-027 Jovista; PZC 17-029 Matheny Tract and Tooleville PZC 17-030, PZC 17-032 for an amendment to Section 16 of Ordinance No. 352, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Development Boundary, and Zone Ordinance Amendment No. PZC 17-031 establishing a Mixed Use Combining Zone in El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville (incorporated by reference herein). The unincorporated Legacy Communities are located within the Mount Diablo Base & Meridian as follows: El Monte Mobile Village, Section 12, Township 16S, Range 23E, Hypericum, Section 24, Township 19S, Range 25E, Jovista, Section 32, Township 24S, Range 26E, Tooleville, Section 12, Township 19S, Range 26E, Matheny Tract Section 22, 23, & 27, Township 20 south, Range 24 East.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element, Transportation & Circulation Element and the Public Facilities & Services Element; (2) add El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Legacy

Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Legacy Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-033 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management and the Transportation and Circulation Element; Change of Zone No. PZC 17-031 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for El Monte Mobile Village, Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista, Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract, Change of Zone No. PCZ 17-029 (Zoning District Map) and Tooleville, Change of Zone No. PZC 17-030 (Zoning District Map) within the respective Legacy Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-032 an amendment to Section 16 to allow additional by-right uses in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Legacy Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:



1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Legacy Plans;
  - b. Encourage infill development within Legacy Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Legacy Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The legacy planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on September 15, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Legacy Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Legacy Plans 2017 Update General Plan Amendment 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Change of Zone No. PZC 17-025; Hypericum General Plan Amendment No. GPA 17-026; Change of Zone No. PZC 17-026; Jovista General Plan Amendment No. GPA 17-027; Change of Zone No. PZC 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; Change of Zone No. PZC 17-029; Tooleville General Plan Amendment No. GPA 17-030; Change of Zone No. PZC 17-030; Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-032 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation, Public Facilities & Services, and Part II Rural Valley Lands Plan has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,

4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Legacy Plans 2017 Update. Therefore, the public will benefit from the Legacy Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on October 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano  
NOES: None  
ABSTAIN: None  
ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE ) RESOLUTION NO. 9343  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT AGRICULTURE ELEMENT, SCENIC )  
LANDSCAPES ELEMENT, AIR QUALITY ELEMENT, )  
PUBLIC FACILITIES & SERVICES ELEMENT, )  
INTRODUCTION, COMPONENTS A, B, C, AND D, AND )  
PART II RURAL VALLEY LANDS PLAN OF THE TULARE )  
COUNTY GENERAL PLAN FOR THE LEGACY PLANS )  
2017 UPDATE GPA 17-033 INCLUSIVE OF GPA's 17-025, )  
GPA 17-026, GPA 17-027, GPA 17-29, AND GPA 17-030 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Introduction; Component A-Planning Framework Element; Component B-Agriculture and Land Use Elements, Component C-Scenic Landscapes, Environmental Resources Management and Air Quality Elements; Component D-Transportation & Circulation and Public Facilities & Services Elements; and Part II Rural Valley Lands Plan of the Tulare County General Plan as set forth in Exhibits "A-1 to A-13" and Part III Community Plan as set forth in Exhibits "B-F" for the Legacy Plans 2017 Update General Plan Amendment No. GPA 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Legacy communities as part of the Legacy Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on September 15, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017 and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan for the Legacy Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Legacy Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Legacy Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to establish Legacy Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;

4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the establishment of legacy development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;
5. The Amendment to Part I Introduction, Component A, Component B, Agriculture, Component C, Scenic Landscapes, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and the newly established Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Legacy Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-033 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-033 the Legacy Plans 2017 Update set

forth in attached Exhibits “A-1 to A-13” and Part III Exhibits “B-F” which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

Exhibits “A-1 – A-13” – Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Service Elements, and Part II Rural Valley Lands Plan.

Exhibit “B” – 2017 El Monte Mobile Home Park Legacy Plan (Part III)

Exhibit “C” – 2017 Hypericum Legacy Plan (Part III)

Exhibit “D” – 2017 Jovista Legacy Plan (Part III)

Exhibit “E” – 2017 Matheny Tract Legacy Plan (Part III)

Exhibit “F” – 2017 Tooleville Legacy Plan (Part III)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO           )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT     )       RESOLUTION NO. 9344  
WITH THE ADOPTED LEGACY PLANS 2017       )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE    )  
NO. PZC 17-031                                 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is



applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and

Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9345
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-032	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the communities of El Monte Mobile Village,

Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update, consistent with the Legacy Communities 2017 Update including affirmation that Section 16 H. is applicable in the Matheny Tract.

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	RESOLUTION NO. 9346
IN THE LEGACY DEVELOPMENT BOUNDARIES	)	
EL MONTE MOBILE VILLAGE, HYPERICUM,	)	
JOVISTA, MATHENY TRACT, AND TOOLEVILLE	)	
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS 2017 UPDATE (GPA 17-033), AS	)	
PROPOSED INCHANGE OF ZONE NO. PZC's	)	
17-025, 17-026, 17-027, 17-029, AND 17-030	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" El Monte Mobile Village Rezoning Plan (PZC 17-025), to include the Legacy Community of El Monte Mobile Village, consistent with the adopted El Monte Mobile Village Legacy Plan 2017 Update, Exhibit "E" Hypericum Rezoning Plan (PCZ 17-026), to include the Legacy Community of Hypericum, consistent with the adopted Hypericum Legacy Plan 2017 Update, Exhibit "H" Jovista Rezoning Plan (PCZ 17-027), to include the Legacy Community of Jovista, consistent with the adopted Jovista Legacy Plan 2017 Update, Exhibit "K" Matheny Tract Rezoning Plan (PCZ 17-029), to include the Legacy Community of Matheny Tract, consistent with the adopted Matheny Tract Legacy Plan 2017 Update, Exhibit "N" Tooleville Rezoning Plan, to include the Legacy Community of Tooleville, consistent with the adopted Tooleville Legacy Plan 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update (GPA 17-033) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities;
3. The proposed changes in zone district designations identified in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.



AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the El Monte Mobile Village Change of Zone No. PZC 17-025; Hypericum Change of Zone No. PZC 17-026; Jovista Change of Zone No. PZC 17-027; Matheny Tract Change of Zone No. PZC 17-029; and Tooleville Change of Zone No. PZC 17-030, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" and "N" Rezoning Plans consistent with the adopted El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update (GPA 17-033).

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

El Monte Mobile Village

Exhibit "A" – Proposed Zone Ordinance Map (PZC 17-025)

Exhibit "B" – Proposed Zoning Map

Exhibit "C" – List of Affected Properties/Map

Hypericum

Exhibit "D" – Proposed Zone Ordinance Map (PZC 17-026)

Exhibit "E" – Proposed Zoning Map

Exhibit "F" – List of Affected Properties/Map

Jovista

Exhibit "G" – Proposed Zone Ordinance Map (PZC 17-027)

Exhibit "H" – Proposed Zoning Map

Exhibit "I" – List of Affected Properties/Map

Matheny Tract

Exhibit "J" – Proposed Zone Ordinance Map (PZC 17-029)

Exhibit "K" – Proposed Zoning Map

Exhibit "L" – List of Affected Properties/Map

Tooleville

Exhibit "M" – Proposed Zone Ordinance Map (PZC 17-030)

Exhibit "N" – Proposed Zoning Map

Exhibit "O" – List of Affected Properties/Map

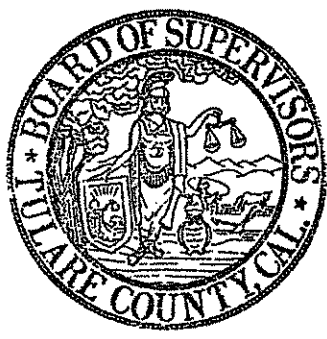
Appendix B – Board of Supervisors  
Resolution No. 2017-0868

# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

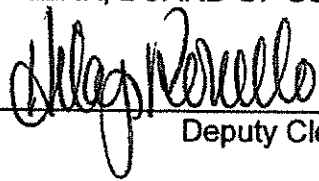
IN THE MATTER LEGACY PLANS 2017 )  
UPDATE (GENERAL PLAN AMENDMENT 17- ) Resolution No. 2017-0868  
033) AND AMENDMENTS TO THE TULARE ) Ordinance No. 3508  
COUNTY ZONING ORDINANCE NO. 352 )

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR CROCKER, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD OCTOBER 17, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS  
NOES: NONE  
ABSTAIN: NONE  
ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY:   
Deputy Clerk

\*\*\*\*\*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Legacy Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Legacy Plans 2017 Update (GPA 17-033); and
3. Adopted one General Plan Amendment No. GPA 17-033, for the Legacy Plans 2017 Update inclusive of the El Monte Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029); Tooleville (GPA 17-030) and Amendments to Tulare County General Plan Part III to establish Legacy Plans and Legacy Development Boundaries; Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D,

Public Facilities and Services Elements, and Part II General Plan Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update; and

4. Waived the final reading and adopted Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Legacy Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-031) to Section 18.9 to adopt a Mixed-Use Combining Zone within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-032) to Section 16 to allow additional by-right uses within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for El Monte Village (PZC 17-025), Hypericum (PZC 17-026), Jovista (PZC 17-027), Matheny Tract (PZC 17-029), and Tooleville (PZC 17-030), consistent with the Legacy Plans 2017 Update; and
5. Adopted the findings of approval set forth in Planning Commission Resolution No. 9342 (Addendum to EIR), Planning Commission Resolution No. 9343 (Legacy Plans 2017 Update), Planning Commission Resolution No. 9344 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9345 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9346 (Zoning District Ordinance Maps); and
6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of El Monte Mobile Village, Hypericum, Matheny Tract, Jovista, and Tooleville to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents; and
10. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

## **EXHIBIT "A"**

### **A. Legacy Plans 2017 Update (General Plan Amendment 17-033)**

WHEREAS, on September 27, 2017 the Planning Commission held a public hearing on the Legacy Plans 2017 Update (GPA 17-033), which included an application for El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030, El Monte Mobile Village Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract Change of Zone No. PZC 17-029 (Zoning District Map); and Tooleville Change of Zone No. PZC 17-030 (Zoning District Map), Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9) and Change of Zone No. PZC 17-032 (By-Right Uses–Section 16), collectively referred to as the Legacy Plans 2017 Update (GPA 17-033); and

WHEREAS, upon conclusion of the public hearing, the Planning Commission recommended the Board of Supervisors certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopt the Mitigation Monitoring and Reporting Program and approve the Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors held a public hearing on October 17, 2017 to consider the proposed Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors finds the public hearing for the Legacy Plans 2017 Update was properly noticed in accordance with state law and the Tulare County Ordinance Code.

WHEREAS, the Board of Supervisors finds the Legacy Plans 2017 Update is compatible, integrated and internally consistent with the existing policies of and with the Tulare County General Plan.

WHEREAS, the Board of Supervisors accepts and incorporates by reference herein, the findings of facts and recommendations adopted by the Planning Commission as follows:

1. Planning Commission Resolution No. 9342, in the matter of the Certification and adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update (PC Package Attachment 1 – CEQA Resolution).

2. Planning Commission Resolution No. 9343, to adopt General Plan Amendment No. GPA 17-033 for the Legacy Plans 2017 Update inclusive of El Monte Mobile Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029) and Tooleville (GPA 17-030) and Amendments to Part III of the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, to the Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities and Services Elements, and Part II Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update (PC Package Attachment 2 – GPA Resolution).
3. Planning Commission Resolution No. 9344, in the matter of an Amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed Use Combining Zone within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 3 – Section 18.9 Resolution).
4. Planning Commission Resolution No. 9345, in the matter of an Amendment to Section 16 of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 4 – Section 16 Resolution).
5. Planning Commission Resolution No. 9346, in the matter of an Amendment to Ordinance No. 352, the Zoning Ordinance, as set forth in the Zoning District Ordinance Maps for El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the Legacy Plans 2017 Update to rezone certain properties within El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville. (PC Package Attachment 5 – Rezoning Plan Resolution).

NOW, THEREFORE, BE IT RESOLVED as follows:

A. The Board of Supervisors hereby certifies and adopts the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopts the Mitigation Monitoring Reporting Programs.

NOW BE IT FURTHER RESOLVED as follows:

B. The Board of Supervisors adopts the entirety of all of the above findings and actions as one General Plan Amendment No. GPA 17-033.

C. The Board of Supervisors hereby waives the final reading of Change of Ordinance 352 and adopts amendments to the Tulare County Zoning Ordinance 352 pertaining to Section 18.9 - Mixed-Use Combining Zone, to Section 16 - Variances and Special Uses, and Zoning District Map Changes.

D. The Board of Supervisors authorizes the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of Legacy Plans 2017 Update to include Mixed Use Combining Zone and additional "By-Right" uses.

E. The Board of Supervisors directs the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq.

F. The Board of Supervisors directs the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file Notices of Determination with the Tulare County Clerk.

G. The Board of Supervisors authorizes the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents.



## **Matheny Tract Legacy Plan**

# MATHENY TRACT LEGACY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development and Planning Branch



# MATHENY TRACT LEGACY PLAN

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# MATHENY TRACT LEGACY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council, for the Sustainable Communities Grant and Incentives Program, under Proposition 84 (2006) in order to integrate infrastructure analysis within rural disadvantaged community's needs (Senate Bill 244). The intent is to reduce greenhouse gas emissions, promote equity, strengthen the economy, protect the environment and promote healthy and safe communities.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# MATHENY TRACT LEGACY PLAN

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# Matheny Tract Legacy Plan

Adopted: October 17, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0868

Tulare County Planning Commission  
Recommendations: September 27, 2017  
Resolutions No. 9342, 9343, 9344, 9345, and 9346

General Plan Amendment: GPA 17-033  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-032  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-031

Matheny Tract Legacy Plan: GPA 17-029  
Zoning District Map: PZC 17-029



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# MATHENY TRACT LEGACY PLAN

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# MATHENY TRACT LEGACY PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)



# MATHENY TRACT LEGACY PLAN

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# MATHENY TRACT LEGACY PLAN

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## **County Administrative Office**

Michael C. Spata, CAO  
Eric Coyne, Deputy CAO

## **Tulare County Resource Management Agency**

Reed Schenke, Director  
Michael Washam, Associate Director  
Aaron Bock, Chief Planner, Project Processing  
Dave Bryant, Chief Planner, Special Projects  
Susan Simon, Planner III  
Velma Quiroz, Planning Tech II  
Johnson Vang, Engineer I

# MATHENY TRACT LEGACY PLAN

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# MATHENY TRACT LEGACY PLAN

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# MATHENY TRACT LEGACY PLAN

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# MATHENY TRACT LEGACY PLAN

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## Introduction

An important objective in preparing a Legacy Plan for Matheny Tract is to develop a plan which can accurately reflect the needs and priorities of the Matheny Tract community. Matheny Tract is not currently designated as a legacy area in the 2030 Tulare County General Plan (2012) Planning Framework Element since legacy areas have not been formally established to date. It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding (for things such as wastewater improvements [for example wastewater distribution piping], curbs, gutters, sidewalks, etc.) and to stimulate economic development within the community.

Matheny Tract is located adjacent to the city of Tulare (**see Figure 1**) generally located south of Avenue 216 (Paige Avenue), east of Road 96 (Pratt Street) and west of I Drive and State Route 99. Matheny Tract is located just west of industrial land uses and a Union Pacific Railroad (formerly Southern Pacific Transportation Company) line running through Tulare County. Physically, the Community of Matheny Tract (**see Figure 2**) is physically divided into a north and south area by agricultural land uses that separate approximately 256 households in North Matheny from 80 households in South Matheny Tract. The Matheny Tract Community is predominantly surrounded by agricultural land.

Along the eastern boundary, running parallel to South “T” Drive is an irrigation ditch, above ground power lines and the Union Pacific Railroad (formerly Southern Pacific Transportation Company) tracks. Also visible to the east is the city of Tulare corporation yard. Above ground power lines run parallel to Pratt Road that acts as the western boundary. The City of Tulare’s Wastewater Treatment and Reclamation facility is located about  $\frac{3}{4}$  miles northwest of Matheny Tract. In addition, an industrial area is located immediately northeast of the Community.

There is a canal within Tulare Irrigation District (TID) service area that bisects North and South Matheny Tract. The Oakland Colony Ditch runs in a north south direction through North Matheny Tract along the Canal Street corridor and extends in an east-west direction between North and South Matheny Tract. There are two east-west crossings of the Oakland Colony Ditch - one along Wade Avenue and the other along Addie Avenue - in North Matheny Tract.

Matheny Tract is an aging unincorporated Tulare County subdivision. As indicated above, there are two parts to Matheny Tract, the southern and smaller portion laid out as Tulare County Tract 53 in 1946 and the northern portion laid out as Tract 104 in 1947 by E.S. Matheny and Grace L. Matheny with a population of approximately 1,043 and 280 households. Matheny Tract is also designated as a disadvantaged community, which is, “a census designated place that has household median incomes that are less than 80% of the statewide household median income.

## Location

The Tulare County community of Matheny Tract is comprised of two groupings of dwellings located south of the City of Tulare just east of Pratt Road and adjacent to the city of Tulare. Matheny Tract is in Section 22, 23, & 27, Township 20 South, Range 24 East, MDB&M, and can be found within the Page, United States Geological Survey 7.5 Minute topographic quadrangle. Matheny Tract is located at an elevation of 269 feet above sea level, National Geodetic Vertical Datum. The coordinates of Matheny Tract are: Latitude: 36.172179 and Longitude: -119.351606.



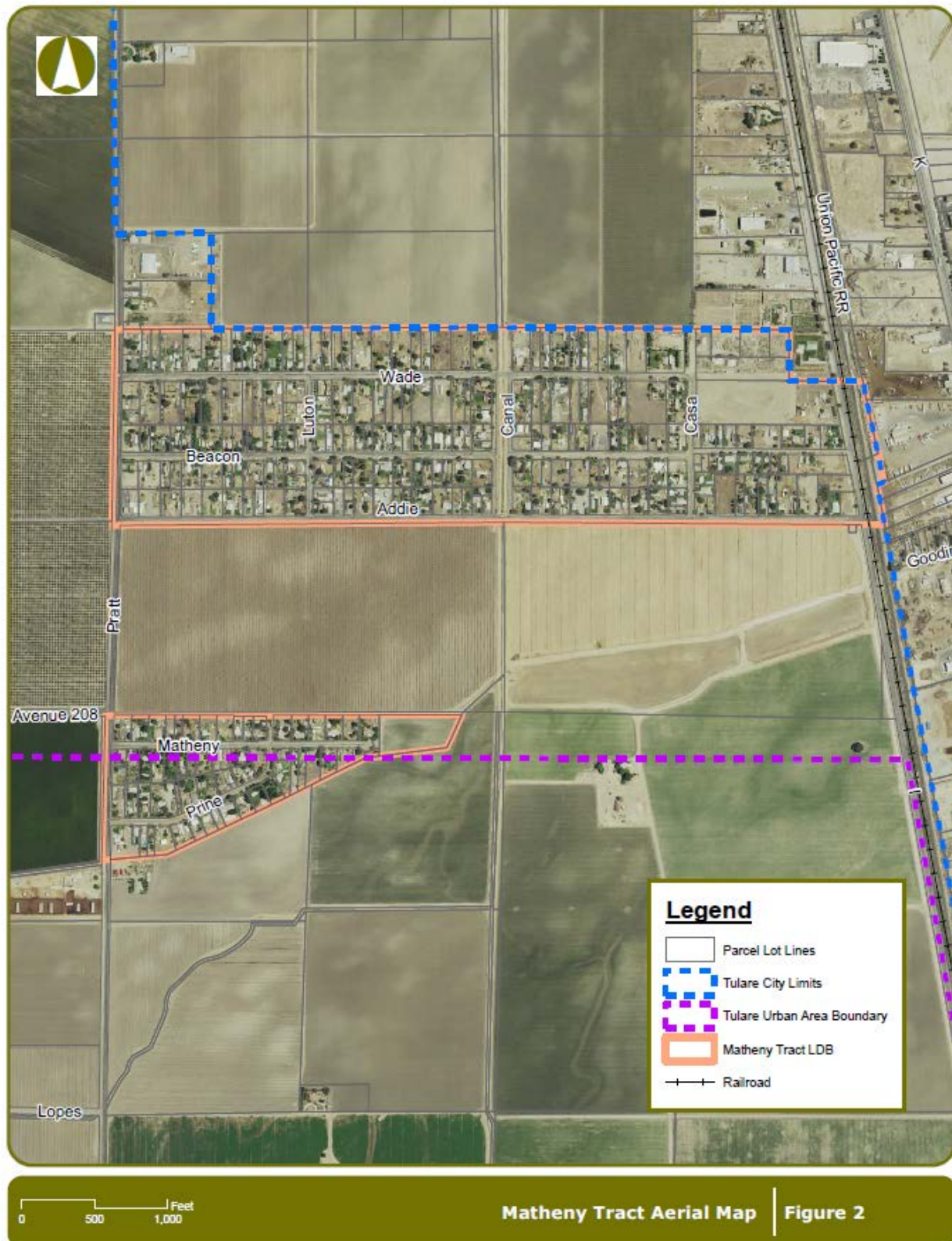
**Legend**

- Matheny Tract LDB
- Tulare Urban Development Boundary
- Tulare Urban Area Boundary
- Supervisorial Districts

**Matheny Tract Vicinity Map** | **Figure 1**

# MATHENY TRACT LEGACY PLAN

Figure 2 - Matheny Tract Aerial



# MATHENY TRACT LEGACY PLAN

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## Planning Area

The Matheny Tract Legacy Development Boundary (HDB) area consists of 187.8 acres (**see Figure 3**). The Land Uses within the HDB are described as follows. Agricultural activities, including orchards and pasture, currently occupy 1.54 percent of the 187.8 acres. Urban development, including urbanized uses such as residential and commercial development occupy 80.77 percent of the 187.8 acres. The remaining 17.5 percent are lands dedicated for Right-of-way (**See Table 1**).

<b>Table 1 - Matheny Tract Land Use</b>	
<b>Land Use</b>	<b>Acres</b>
Residential	145.3
Commercial	2.5
Agricultural	2.9
Non Zoned Parcels	0.2
Z	2.9
Rights-of-way	32.9
<b>Total</b>	<b>187.8</b>

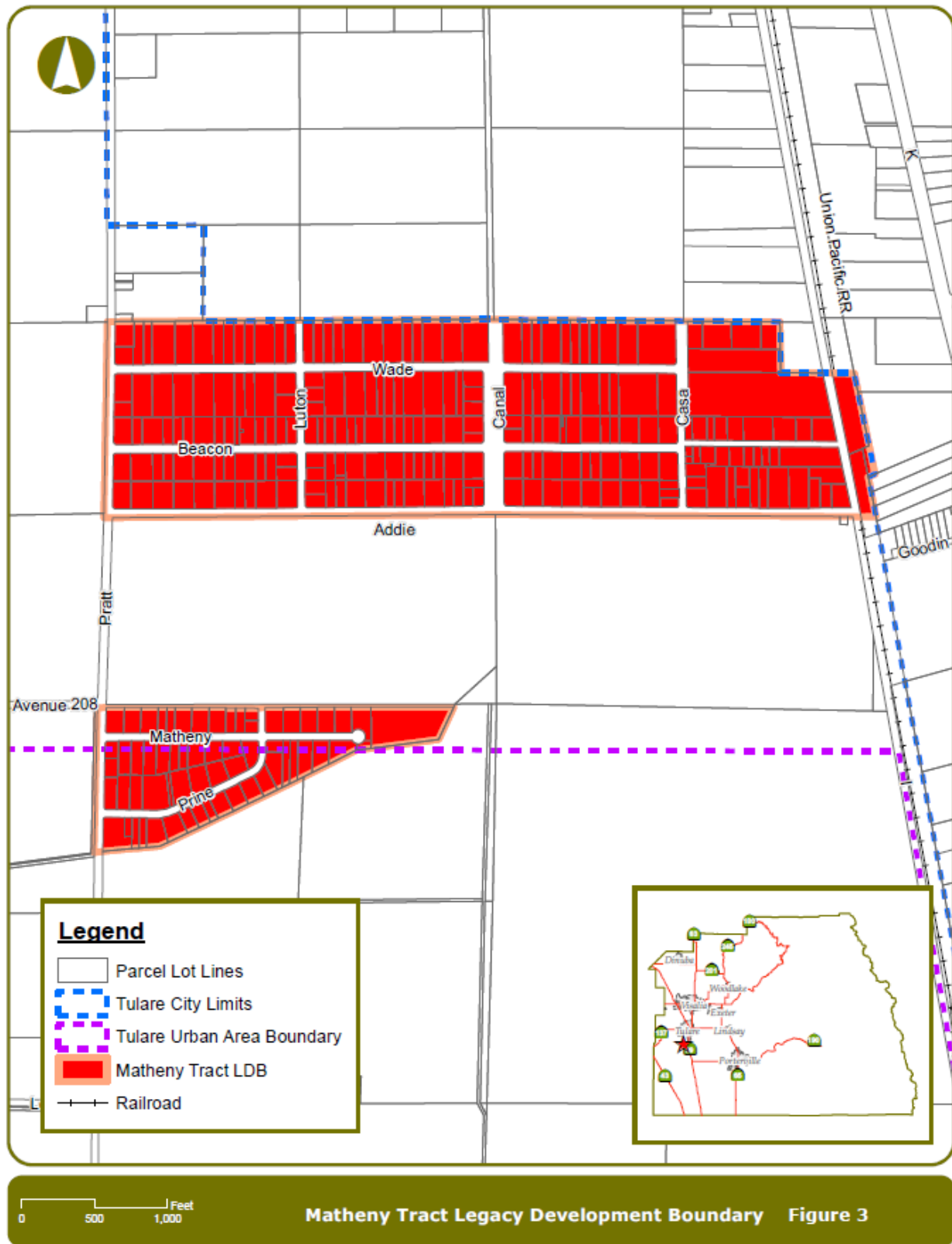
No change is proposed to the Hamlet Development Boundary.

## City of Tulare's Sphere of Influence (SOI)

The community is located adjacent to the Tulare city limits and outside the city's 2035 Urban Development Boundary. Although Matheny Tract is within the city's Sphere of Influence (SOI) (**see Figure 4**), the city's General Plan map does not designate the area. Many residents have lived in Matheny Tract for many years; a 2012 survey found that 51% had lived there longer than ten years. Thirty-four percent had lived in Matheny Tract for more than twenty years. Most children attend Palo Verde School, which is not part of the Tulare City School District, and many of their parents attended Palo Verde as well.

# MATHENY TRACT LEGACY PLAN

Figure 3 - Matheny Tract HDB



**Legend**

- Road Network
- City Limits
- +— Railroad
- Matheny Tract LDB

**Matheny Tract - City of Tulare LAFCO Sphere of Influence** | Figure 4



# MATHENY TRACT LEGACY PLAN

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## SUSTAINABLE COMMUNITIES STRATEGY OUTREACH REPORT

### Community Description

Matheny Tract is a disadvantaged unincorporated community located immediately adjacent to the City of Tulare's southwestern boundary. It is a community that is considered a Census Designated Place (CDP) by the 2010 U.S. Census. This means that it is a place where there is a settled concentration of population that is identifiable by name, but is not legally incorporated under the laws of the state in which it is located and is delineated to provide data.<sup>1</sup>

This report is based on data from the American Community Survey (ACS), Matheny Tract residents' testimony through surveys, and informal residents' testimony collected via community canvassing. The observations made by Leadership Counsel for Justice and Accountability (Leadership Counsel) and the Matheny Tract Committee (MTC) is also included. Leadership Counsel is a 501(c)3 non-profit organization that helps disadvantaged communities with infrastructure needs and MTC is a group of Matheny Tract residents that volunteer their time to work towards improving their community. MTC and Leadership Counsel conducted canvassing and door-to-door outreach in Matheny Tract to create this report.

The ACS estimates that Matheny Tract's total population in 2014 was 1,098 residents with 305 households.<sup>2</sup> The age breakdown is as follows: 38.8% of the population in Matheny Tract is between the ages 0 to 19, 36.3% are between 20 to 44 years of age, 18.1% are between 45 to 59 years of age, and 11.9% are over 60 years of age.<sup>3</sup>

The male and female population makes up 49.2% and 50.8%, respectively.<sup>4</sup> In Matheny Tract, residents of Hispanic or Latino background make up 63.4% of the population. Additionally, of the population that identified as "Not Hispanic," 33.8% identified themselves as White and 2% identified themselves as Black or African American.<sup>5</sup>

Additionally, Matheny Tract has many infrastructure needs. Notably, the residents have stated that their current water servicer, the Pratt Mutual Water Company, has tested the drinking water in Matheny Tract and confirmed that it is contaminated with arsenic, nitrates, and leaching septic tanks. Attaining clean drinking water is a top priority for residents, and for many years, the community has worked towards securing their right to potable water.<sup>6</sup>

Heading this movement is the Matheny Tract Committee. As mentioned, MTC's purpose is to advocate for improved conditions and quality of life in the Matheny Tract. MTC and Leadership Counsel for Justice and Accountability have secured an agreement by the City of Tulare to consolidate its water system with Matheny Tract's new water system constructed with Proposition 84 funds.

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<sup>1</sup> [https://www.census.gov/geo/reference/gtc/gtc\\_place.html](https://www.census.gov/geo/reference/gtc/gtc_place.html)

<sup>2</sup> <http://factfinder.census.gov/faces/tableservices/jsf/pages/productview.xhtml?src=CF>

<sup>3</sup> Id.

<sup>4</sup> Id.

<sup>5</sup> Id.

<sup>6</sup> <http://www.theatlantic.com/politics/archive/2015/10/california-drought-towns-without-drinking-water/409930/>

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In 2011, the City of Tulare signed an agreement with Pratt Mutual Water Company, the water company that serves Matheny Tract, to provide potable water to Matheny Tract and consolidate its water system with the community's water system, which would be constructed with Proposition 84 funding. In June 2015, the City of Tulare initiated a lawsuit to alter the terms of the agreement, and MTC countersued to seek the City's compliance with the agreement. The City, MTC, and Pratt Mutual arrived at an agreement in April 2016, prior to trial and after the State Water Resources Control Board (SWRCB) ordered the City of Tulare to initiate potable water service to Matheny Tract and consolidate its water system with the community's pursuant to the authority granted to it under SB 88. SB 88 grants the SWRCB authority to require local governments such as Tulare, to consolidate their water systems with adjacent disadvantaged communities when those communities have high levels of contamination in their drinking water that exceed public health thresholds.

## Community Outreach and Invitation Process

Leadership Council met with MTC on two occasions to discuss dates for outreach and whether or not we should have a large-scale workshop at Palo Verde Elementary School. The group decided to schedule the first workshop for January 9, 2016, at 10:00 am. The group usually meets outdoors at Reina Palma's house on 256 East Beacon Avenue on Reina's driveway. However, the group informed us that the workshop should not be outdoors during that time of the year, because it was too cold in the evenings, and no one would attend.

Also, because MTC has had large meetings at the Palo Verde Elementary cafeteria in the past, we contacted Mr. Manning, Palo Verde's principal, who agreed to allow us to use the Palo Verde Cafeteria for the workshop location and agreed to have someone there to help us open the gate for a small fee, at first. Later, he decided to sponsor us and did not charge a fee.

For outreach for the workshop, we worked alongside MTC members, Tim Denney, Reina Palma, and Leadership Counsel Employees, Ashley Werner, Merced C. Barrera, and Robert Fuentes. Together we canvassed all of Matheny Tract by dividing up into teams (See "Outreach Dates" listed above and the flyer). On average, we spoke to fifteen to twenty residents per team per day.

During outreach, team members had to contend with threatening and stressful encounters with stray dogs. Tim, Reina, and Merced, for example, were approached by hostile stray dogs but fortunately were not bitten. However, there was one day in which Reina was chased by a dog. Residents of Matheny Tract must deal with the stray dogs that roam the community's streets on a daily basis. The residents stated that this is because animal control does not frequent Matheny Tract.

As described above, MTC and Leadership Counsel conducted extensive outreach for the SGC workshop. Nevertheless, just 13 residents attended the workshop on January 9th, fewer than hoped for and expected. During outreach, we received feedback from various residents that they did not plan to attend the workshop, because after so many years of neglect, broken promises and discriminatory actions and statements by their own representatives and the City of Tulare, the residents didn't believe anything could come of it. However, MTC did not give up. Together with MTC, we planned an additional meeting to get more resident input.

In addition to the workshop, we held a follow-up meeting with MTC where residents that did not attend the workshop completed the surveys. This additional meeting was held at MTC's usual place of convening, Reina Palma's house, on January 21, 2016 at 6 pm. We also did outreach for this meeting

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by calling our list of residents that have been involved in MTC at some point in the past; this is a list of approximately 155 residents. Additionally, MTC leaders informed their neighbors and relatives that live in Matheny Tract of the additional opportunity to provide input. Please see list below for more details:

1. 11/19/15, 6:00 pm:
  - a. Matheny Tract Committee (MTC) Meeting - introduction to SGC Workshops and RMA, dates for workshop suggested
2. 12/13/16:
  - a. Matheny Tract Committee Meeting- setting dates for SGC workshop in Matheny Tract and outreach dates
3. 1/4/16:
  - a. Phone calls to MTC members re outreach for 1/5/16
4. 1/5/16, 2:00 pm- 5:00 pm:
  - a. Door-to-door flyer (300)
5. 1/7/16:
  - a. Phone calls to MTC members re outreach for 1/8/16
6. 1/8/16, 2:00 pm-5:00 pm
  - a. Door-to-door flyer (300)
7. 1/9/16- Workshop at Palo Verde Elementary School Cafeteria
8. 1/14/16-
  - a. Completed survey with residents (Mr. Vance and Mrs. Andrade)
9. 1/20/16-
  - a. Phone calls to MTC members re meeting for 1/21/16
10. 1/21/16- Meeting at MT resident's house (Reina Palma)

Lastly, the educational materials and outreach flyer included Tulare Resource Management's logo, Leadership Counsel's logo, and phone numbers for Leadership Counsel, date, location, time, and purpose of the workshop. The educational materials were copies of the survey, a PowerPoint printout, and easel boards. We used a survey to review with the residents and for discussion groups. All materials and flyers were in both, English and Spanish.

## Community Meeting

The workshop was held as planned on January 9th and began at 10am in Palo Verde Elementary new cafeteria. As mentioned above, 13 residents attended the workshop. We gave a brief presentation in which we explained the types of infrastructure and services relevant to the workshop, the purpose of collecting surveys to determine residents' priorities for improvements in the community, and the role of the County of Tulare in the planning process and project application process. We also emphasized the importance of public participation in the planning process, and why their engagement in these types of infrastructure needs assessment workshops for Matheny Tract is one way to contribute to the improvement of their community.

We provided the residents with a printout of the presentation (see Attachment A). The presentation was in English and Spanish because there were five English-speaking residents and eight Spanish-speaking residents in attendance. Childcare was not provided, and none of the participants brought children to the workshop.



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After the presentation, we broke out into two groups, a Spanish-speakers group and an English-speakers group, to complete the surveys. Ashley Werner facilitated the English group and Merced C. Barrera facilitated the Spanish group. Once the groups completed the surveys, they numbered their improvement needs priorities on page 6 of the survey. Their priorities are tallied on the attached Excel spreadsheet, which shows the group's order of priorities (see Attachment B).

Please note that three residents forgot to sign the sign-in sheet, but as you can see in our workshop picture and the attached surveys, they attended the workshop and completed the survey (see Attachment C).



*Pictures of Matheny Tract community meeting and outreach.*

## Community Survey

A survey was developed as a tool to gather a variety of information from the community about multiple topics (see Attachment D). It was 6 pages in length and was provided in both English and Spanish. On average, it took about 30 to 40 minutes for residents to complete. The survey asks about the following community-related topics: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Transportation Options, Walkability, Roads, Street Safety, Flooding, Fire, Safety, Infrastructure, Water Quality & Quantity, Wastewater, Storm Water Drainage, Multimodal Opportunities, and the priority of various improvement needs.

Residents were encouraged to add information and comments to the survey. There were only two translations that were confusing to the Spanish-speaking residents. The first was under the topic of “Gas Stations:” the residents did not understand the difference between “estacion de servicio” and “gasolinera.” Second, under “Fire and Safety,” residents needed some explanation about the meaning of “hidrantes de bomberos.” We discussed residents’ questions to ensure that they fully understood and could complete the survey.

After careful discussion at the Matheny Tract SGC community input meeting, residents concluded that the following list represents Matheny Tract priority improvement needs.

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## Priority Improvements

1. Water Quality/ Water Quantity
2. Sewer System
3. Storm Water Drainage/ Street Flooding
4. Road Improvements
5. Street lights
6. Sidewalks
7. Trash Service
8. Community Safety
9. Fresh food availability/Market
10. Community Park

## **Water Quantity**

Matheny is comprised of approximately 305 residential units and a local store.<sup>7</sup> Currently, Pratt Mutual Water Company and septic tanks serve all of the residential lots. Pratt Mutual Water Company serves the local store as well. The residents also mentioned that the water pressure is very low sometimes.

## **Water Quality**

Pratt Mutual's wells were drilled several decades ago. The residents report that all of the wells are contaminated with Arsenic. Residents strongly desire access to potable drinking water, consistent with Pratt Mutual's agreement with the City of Tulare.

During outreach calls to residents for the workshop, one resident stated that they had been denied building permits to construct a single-family resident on a vacant lot that they owned due to inability to demonstrate a source of potable water.

Also, while canvassing, one resident asked whether she could boil her water to eliminate arsenic from her tap water; we informed the residents that boiling water would not address arsenic contamination.

## **Sewer System**

Matheny Tract is presently unsewered, with wastewater disposal provided via individual septic systems that date to the 1960's. Based on visual inspection there are approximately 305 dwellings within the community on 290 residential lots; approximately one-third of the dwellings are mobile homes. Some residents report that the septic systems are failing due to age, lack of maintenance and insufficient percolation capacity of the underlying soils.

In the recent "Wastewater System Project Feasibility" survey, all of the residents voted in favor of the Alternative # 2 presented by the County's report, which is application for a state grant to construct a waste water system in Matheny Tract and consolidation of that system with the City of Tulare's system. This seems especially relevant given that the wastewater treatment plant is less than a mile away from Matheny Tract and continues to impact the community with foul odors despite years of complaints by residents.

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<sup>7</sup> <http://factfinder.census.gov/faces/tableservices/jsf/pages/productview.xhtml?src=CF>

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## **Storm Water Drainage**

The lack of storm water drainage was the second highest concern for the residents. At the workshop, 2 out of 13 participants identified drainage and sewer as their top priority and 4 out of 13 residents named this their second priority on the survey.

This is of great concern for the community because Matheny Tract experiences heavy flooding during the rainy season. This flooding greatly reduces walkability in the community, as residents must walk through muddy, flooded streets that lack sidewalks. These conditions pose special risks to children and the elderly who are more vulnerable to the wetness and cold. During the outreach conducted in December and January, one of the issues that MTC and Leadership Counsel encountered while canvassing was that there were many large puddles of dirty water that blocked driveways that led to the front doors of homes.

## **Road Improvements**

There are many potholes in Matheny Tract, and some of the residents stated that the pavement cracks. Also, the roads do not have a lane-dividing line or a boundary line for the residential sides of the streets. There are no bike lanes. Most of the residents stated that vehicles that pass through the community, especially on Addie Avenue, do so at extremely fast and dangerous speeds.

## **Street Lights**

There are very few streetlights in Matheny Tract, and several of the streetlights in the community do not work well or at all. One streetlight is located at the intersection of West Beacon Avenue and Luton Street. The residents stated that having more street lights are imperative for the safety in their community, as it is now, without much lighting, none of the survey participant felt safe walking or being outdoors in their neighborhood at night.

In 2012, MTC secured a community-benefits agreement with Colony Energy LLC -- which plans to build an anaerobic co-digester facility upwind of the community by the City of Tulare's Waste Water Treatment Plant -- which provides that Colony Energy will work with Southern California Edison to install approximately 17 street lights in Matheny Tract and pay for their operation for ten years. Colony Energy's duty to do so is triggered when it secures financial close. Colony Energy representatives report that it has not yet secured financial close.

## **Speed Limit Signs**

The community needs traffic calming signs throughout and enforcement by patrol officers.

## **Public Transportation**

TCAT's South County Route #20 is scheduled to stop at the corner of Pratt Street and Addie Avenue approximately five times a day. However, many of the survey participants said that they have only seen the bus pass by once a day.

## **Fire Hydrants**

The majority of the residents stated that they did not know how many fire hydrants are in Matheny Tract. One resident said there weren't any fire hydrants, two others said there were several, and three residents respectively said there were more than fifty, two, and two. Also, while canvassing, we only saw one fire hydrant on Wade Street, but we were not sure whether it was operating. The point here

# MATHENY TRACT LEGACY PLAN

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is that there seems to be a lot of confusion as to whether residents are safe in case of a fire in their community.

## **Flooding**

Almost all of the participants answered yes to this question. Two participants did not answer this question and two others said that their particular driveway did not flood, but that others on their street did. However, the majority of the participants said that flooding is an issue on all of the streets of Matheny Tract. Some of the streets that were identified by the participants were Prine Drive, Beacon Ave, and Addie Ave.

Some explained that this is due to lack of concern and maintenance by the county. Another pointed out that none of the streets have curbs. Also, some residents blame the flooding on the lack of storm water drains, as noted above. One other participant said that this was due to the many potholes on the streets as well.

## **Zoning**

Most residents reported that they did not have limitations building a home or starting a business in Matheny Tract or stated that this issue was not applicable. One resident reported that it was a problem, because she did not have money to start a business. We recommend that this question be amended in the future to account for resident's financial ability to build on their lots or start a business to better reflect how zoning can affect a resident under these circumstances. Residents may have been concerned about exposing themselves to zoning violation citations if he or she already made changes to their homes without consulting the proper permitting agencies as a result of answering the question.

## **Community Based Officer**

Additionally, some residents expressed concern about security in their neighborhoods. They would like to see the Tulare County Sheriff do more surveillance rounds, so that they can "catch" a group of motorcyclist that speed on their extremely loud motorcycles at late hours of the night, which is disturbs the many residents who work or go to school early in the morning. Some residents have seen or heard these motorcyclists as early as 4 a.m.

While canvassing for outreach in the afternoon, we also witnessed at least four motorcycles and four-wheelers speeding past us on the streets.

## **Internet**

Lastly, some residents stated that they have Comcast as their Internet and cable provider, but also stated that this service is very costly.

## **Public Services**

The nearest library for Matheny Tract residents is the City of Tulare's library, which is located four miles away. There is no direct bus route to the library. In fact, only one route services Matheny Tract, TCAT's South County Route #20, which stops at the corner of Pratt Street and Addie Avenue.

Most Matheny Tract residents reported that they have access to an electricity provider for their homes.



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## Local Market

The local market is a small gas and liquor store. It does not offer fresh produce for Matheny Tract residents. Most of the residents travel to R&N Market or Vallarta Supermarket in Tulare, which are five to eight miles from Matheny Tract.

## Medical Clinic

Matheny Tract does not have a medical clinic. Some residents travel three miles to Hillman Clinic or Family Healthcare Network. Others travel six miles to the Community Health Clinic on the North side of Tulare.

## Follow Up Meeting

County staff and Self Help Enterprises had a follow up meeting on January 9<sup>th</sup>, 2017 to discuss the results of the outreach from Leadership and the Surveys taken at the previous meeting. The citizens remained concerned about sewer for the community. They were pleased to hear about roadway improvements along Pratt Street.



# MATHENY TRACT LEGACY PLAN

## DEMOGRAPHICS

An important part of planning is having information that describes the characteristics of a Legacy's population. Collectively, these characteristics are known as "demographics" which is data typically consisting of the age, gender (i.e., male or female), income, race, employment, and other characteristics of a community. This data, and looking at historical trends of this data, allows a reasonable way to project what may occur in the future and thereby provides a guide to which issues need to be addressed in a Legacy plan. For example, knowing the age and percentage of a population allows proper planning for school needs for school-age children; knowing how many people may eventually live in a Legacy allows for proper planning to meet housing needs and the amount of land needed to provide housing for a growing population. If a population can be estimated, it is possible to project how much water and/or sewer service may be needed for a Legacy. The following information provides a summary of some of the more important demographic data needed to craft a plan that can realistically address the needs of smaller Legacy such as Matheny Tract.

The 2015 United States Census reported that Matheny Tract had a population of 1,043. The racial makeup of Matheny Tract was (33.8%) White, (2.0%) African American, (0.0%) Native American, (0.0%) Asian, (0.5%) some other race, and (0.3%) from two or more races. Hispanic or Latino of any race were 345 persons (88.5%). The average renting household size was 4.35. There were 295 housing units, of which (43.6%) were owner-occupied, and (56.4%) were occupied by renters.

### Population

In 2015, Matheny Tract's population was 1,043, (see Table 2).

Table 2 - Population						
	California	%	Tulare County	%	Matheny Tract	%
Male	18,911,519	49.68%	225,881	50.07%	449	43.0%
Female	19,155,401	50.32%	225,227	49.93%	594	57.0%
Total	38,066,920		451,108		1,043	

2011-2015 American Community Survey 5-Year Estimates

### Median Age

Matheny Tract's population is younger than the median age throughout all of Tulare County. Matheny Tract's median age of 27 is lower than the median age of the State of California (see Table 3).

Table 3 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Matheny Tract	27

2011-2015 American Community Survey 5-Year Estimates

# MATHENY TRACT LEGACY PLAN

## ETHNICITY AND RACE

In 2015, approximately 33.8% of Matheny Tract's population was White, 0.0% was African American, 0.0% was Native American, 0.0% was Asian, 0.5% some other race alone, and 0.3% was two races or more (see Table 4). Approximately 63.4% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 4 - Race & Ethnicity						
	California	%	Tulare County	%	Matheny Tract	%
Total	38,421,464		454,033		1,043	
Hispanic or Latino (of any race)	14,750,686	38.4 %	283,533	62.4 %	696	63.4%
White (Not Hispanic)	14,879,258	38.7 %	139,581	30.7 %	371	33.8%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	6	2.0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5 %	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	6	0.5%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	3	0.3%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Matheny Tract

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see Table 4).

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Matheny Tract was about 9.1% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 9.1% includes only the employable labor force (that is, not every person of the population) results in about 67 unemployed persons of Matheny Tract's person labor force of 390.

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Table 5 - Employment Status			
Employment Status	California	Tulare County	Matheny Tract
Population 16 years & over	30,312,429	325,404	733
In labor force	19,269,449	194,420	390
Civilian labor force	19,137,441	194,102	390
Employed	17,246,360	170,780	323
Unemployed	1,891,081	23,322	67
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	343

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

In 2015, Matheny Tract's median household income was \$30,565 whereas the State of California's median household income was \$61,818. Therefore, based upon Matheny Tract's median household income of \$30,565 (which is about 49% of the State of California's median household income), it is considered a severely disadvantaged community (see Table 6).

Table 6 - 2011-2015 American Community Survey: Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Matheny Tract CDP	\$30,565	\$30,450	\$10,660

## Severely Disadvantaged Community:

Matheny Tract is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial opportunities, allow expansion of the size of existing housing (for example, increase a unit by an additional bedroom),

Table 7 - Housing Tenure			
	California	Tulare County	Matheny Tract
Occupied housing units	12,717,801	133,570	280
Owner-occupied	6,909,176	75,685	122
Renter-occupied	5,808,625	57,885	158

2011-2015 American Community Survey 5-Year Estimates



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and allow various housing types (such as mobile homes). According to the California Department of Finance, the 2011-2015 American

Table 8 - Housing Tenure (%)			
	California	Tulare County	Matheny Tract
Owner-occupied	54.8%	57.2%	43.6%
Renter-occupied	45.2%	42.8%	56.4%

Community Survey data indicated 43.6% of the housing units in Matheny Tract was owner-occupied (**see Table 8**). Similarly, 57.2% of housing units were owner occupied in Tulare County and 54.8% of the housing units were owner occupied in the State of California.

## Housing Conditions

Table 9 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Matheny Tract	27	14	23	12	58	29	28	14	63	32	199

Source: Tulare County 2015 Housing Element

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Matheny Tract indicated that approximately 14% of the housing units were sound. Approximately 55% of Matheny Tract's housing units were deteriorated and 32% were dilapidated (**see Table 9**).

## Age of Structures

Most of the homes in Tulare County were built between 1940 and 1989 (**see Table 10**). The total number of housing units in Matheny Tract is 295.

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Table 10 - Year Structure Built			
Year Structure Built	California	Tulare County	Matheny Tract
Total housing units	13,845,790	144,792	295
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	11
Built 2000 to 2009	1,646,490	25,997	6
Built 1990 to 1999	1,495,571	21,767	14
Built 1980 to 1989	2,117,819	22,733	49
Built 1970 to 1979	2,503,688	27,111	69
Built 1960 to 1969	1,871,029	15,500	42
Built 1950 to 1959	1,907,512	13,694	42
Built 1940 to 1949	865,607	7,494	43
Built 1939 or earlier	1,298,438	8,186	19

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2014 the average renter occupied household size in Matheny Tract was 4.10 persons per household (see Table 11). The average owner occupied household size is 3.24.

Table 11 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	2.99	2.01
Tulare County	3.23	3.52
Matheny Tract	3.24	4.10

2010-2014 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>8</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. Typically, a vacancy rate of about 5-6% is considered normal; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Matheny Tract was 0.0%, the rental vacancy rate for Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate for Matheny Tract was 5.4%, the homeowner vacancy rate for Tulare County at 1.4% and the State of California at 1.4%, (see Table 12).

Table 12 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Matheny Tract	5.4%	0.0%

2011-2015 American Community Survey 5-Year Estimates

<sup>8</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# MATHENY TRACT LEGACY PLAN

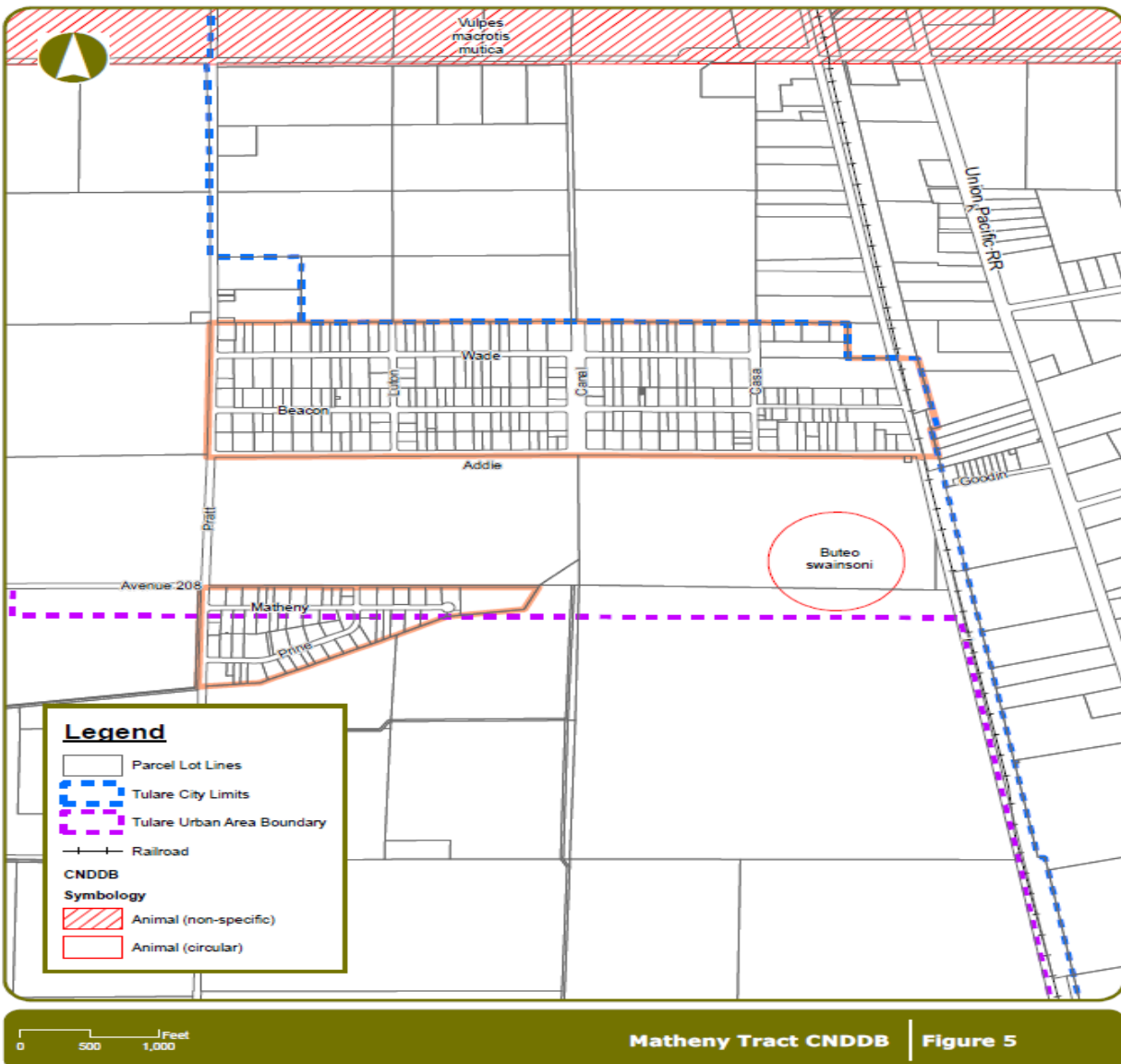
## ENVIRONMENTAL CONDITIONS

### Natural Conditions

#### Wildlife

A California Natural Diversity Database (CNDDB) search conducted on November 1, 2016 (see **Figure 5**), indicates there are special status species within the Tulare Quadrant Species List (which includes the Matheny Tract Planning Study Area) consisting of two animal species and two plant species: San Joaquin kit fox (*Vulpes macrotis mulica*, federal endangered); Swainson's hawk (*Buteo Swainsoni*, state threatened); San Joaquin adobe starburst (*Pseudobahia peirsonii*, federal threatened); and California jewelflower (*Caulanthus californicus*, federal and state endangered). The California Department of Fish and Wildlife (CDFW) Occurrence Reports indicate a last sighting of kit fox in 1975.

**Figure 5 – CNDDB Map – Tulare Quad**



# MATHENY TRACT LEGACY PLAN

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## Geology & Seismic Hazards

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Matheny Tract are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Matheny Tract] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

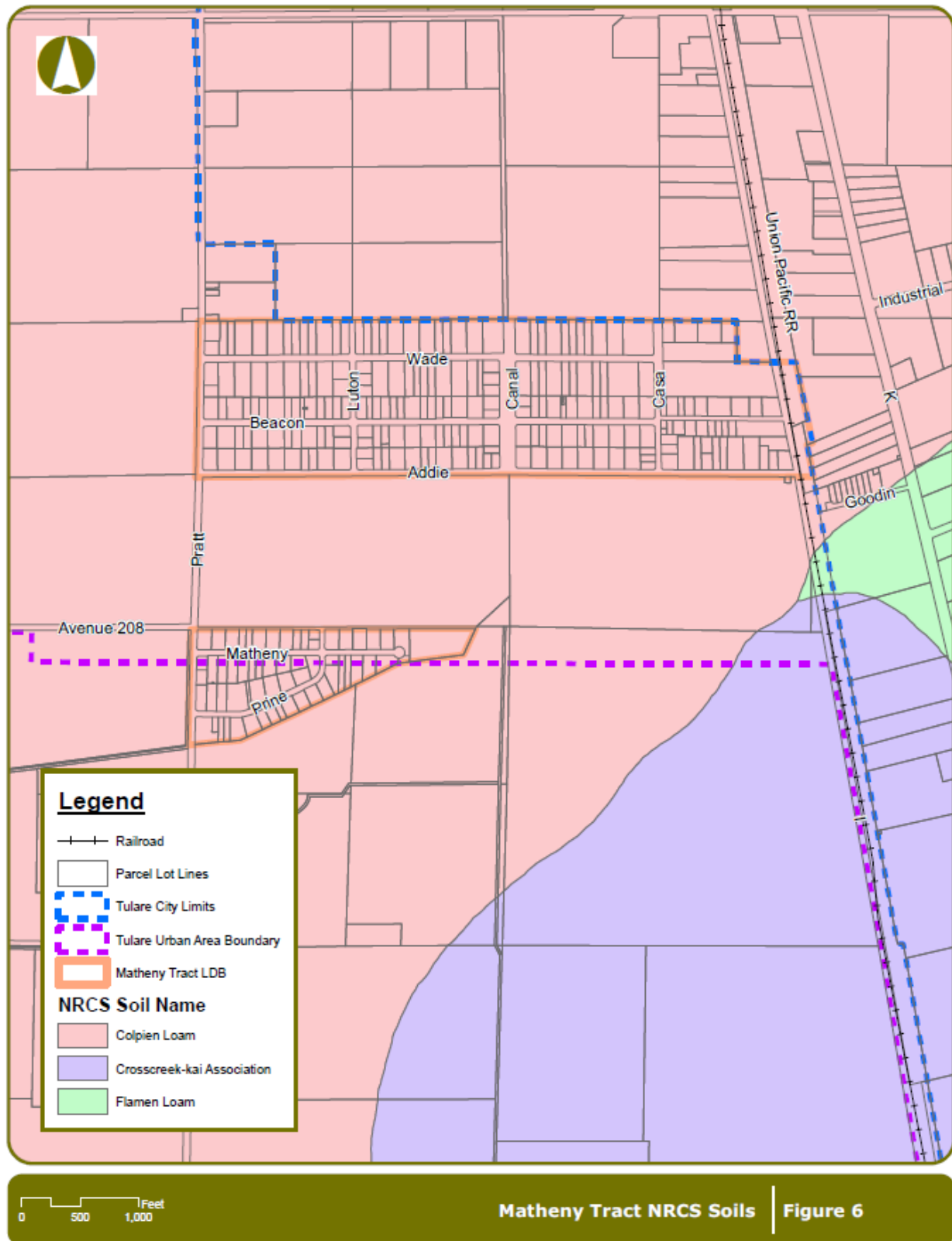
## Soils

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County (**see Figure 6**), the following soil types are located in Matheny Tract. The following descriptions are provided for the above soil types:

**Copien loam soil 0 to 2 percent slopes**, consists of very deep, moderately well drained soil on terraces that formed in alluvium derived mainly from granitic rocks.

# MATHENY TRACT LEGACY PLAN

Figure 6 – NRCS Soils Map



# MATHENY TRACT LEGACY PLAN

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## Physical Conditions

### Air Quality

The Matheny Tract Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State O<sub>3</sub> 1-hour standard, non-attainment for the State O<sub>3</sub> 8-hour standard, non-attainment for the State PM<sub>10</sub> standard, non-attainment for the federal and State PM<sub>2.5</sub> standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural

# MATHENY TRACT LEGACY PLAN

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qualities of the air. Because Matheny Tract is located near the central portion of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

Local air pollution sources within the general vicinity of Matheny Tract include agricultural activities.

## Flooding<sup>9</sup>

“According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C1275E (**see Figure 7**). Matheny Tract is located within Flood Zone X – areas of 0.2% annual chance flood; area of 1% annual chance flood with average depths of less than 1 square mile; and areas protected by levees from 1% annual chance flood. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), which means the County of Tulare agreed to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Further flood information is available on Tulare County Resource Management Agency website at: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>

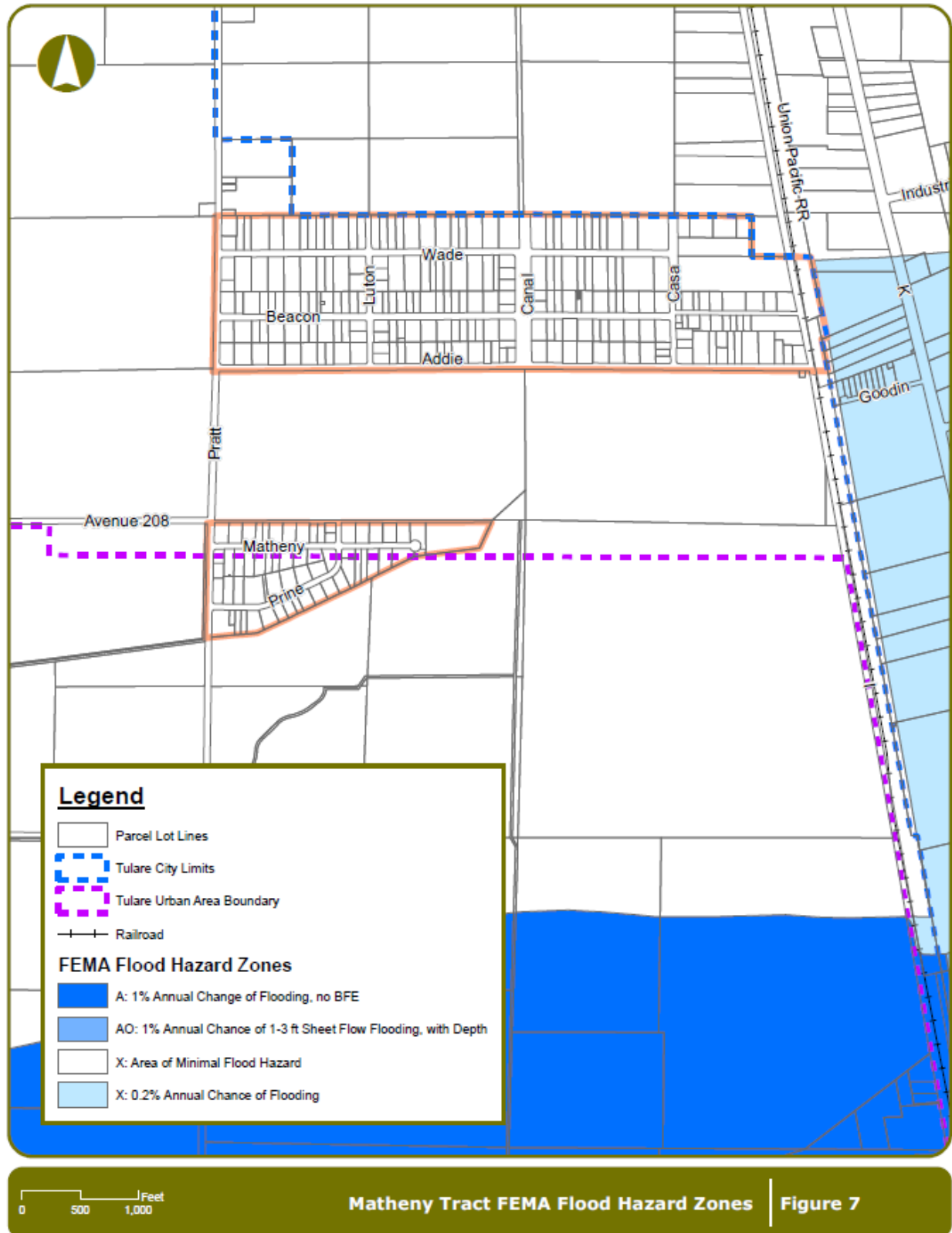
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<sup>9</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C1275E, Panel Number 1275 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>



# MATHENY TRACT LEGACY PLAN

Figure 7 - FEMA Flood Map





# MATHENY TRACT LEGACY PLAN

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## Noise

Tulare County adopted a new noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan Background Report, the predicted Ldn noise level at the distance from the SR 99 centerline to the 60 dB-Ldn contour between Airport Avenue and Paige Avenue is 1,545 feet. The Matheny Tract lies outside of the 60 dB-Ldn contour line.

Operations along the Union Pacific Railroad line are another dominant source of noise in Matheny Tract. According to the Tulare County General Plan EIR, there are more than twenty (20) freight train operations per day along the Union Pacific Railroad line in Tulare County and may occur at any time of day or night. According to the Wyle methodology, the type and frequency of rail operation results in noise exposures of 65 and 60 dB Ldn at approximately 335 and 660 feet, respectively, from the center of the tracks for present operations, and at approximately 440 and 800 feet, respectively, from the center of the tracks for estimated future operations. There are areas in the east side of the Matheny Tract LDB that are located within 335 feet of the Union Pacific Railroad line. Noise levels are higher at grade crossings due to the warning horn. The Paige Avenue (Avenue 216) and Avenue 200 grade crossings are over one (1) mile from the Community of Matheny Tract.

## Airport Safety Tulare County Comprehensive Airport Land Use Plan (CALUP)

The purpose of the CALUP is to promote the safety and well-being of the public by ensuring adoption of land use regulations which minimize exposure of persons to hazards associated with the operation of these airports including aircraft accidents and aircraft noise. The Matheny Tract is located within safety zones 4 (Outer Approach/Departure Zone) and 6 (Traffic Pattern Zone) of the Tulare Municipal Airport (Mefford Field) which is classified as a “general transport” facility. Future development is regulated by land use and airport compatibility requirements are determined by the CALUP and regulations found in General Plan, Improvement Standards and the *Tulare County* Zoning Ordinance. Residents of Matheny Tract may experience some noise related to aircraft operations from this airport, although it is not considered to be extreme as the area is located outside of the 60 dB CNEL airport noise contour. (Please see extensive discussion regarding Mefford Field beginning on page 44).

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Matheny Tract, this Legacy Plan is intended to address deficiencies and the need for improvements to the wastewater system improvements, (for example wastewater distribution piping), curbs, gutters, streets, sidewalks, etc.). The prioritized concerns of the Community include:

# MATHENY TRACT LEGACY PLAN

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1. Water Supply
2. Sewer System
3. Storm Water Drainage
4. Road Improvements
5. Street Lights
6. Sidewalks
7. Solid Waste
8. Community Safety
9. Community Park
10. Fresh food availability/Market

## Water Supply

In 2016, the State Water Board exercised its power and directed the city of Tulare to accept Matheny Tract as water customers. Water supply for Matheny Tract is provided by City of Tulare's Water Division. As per the city of Tulare's MSR update, August 2013 The Water Division is responsible for providing water that is of safe and sanitary quality for the citizens of Tulare and an adequate water supply for fire protection. "Tulare's water supply source consists of a 30 domestic wells that are scattered throughout the city, extracting water from the city's underground aquifer. Newer wells drilled by the city over the past thirty-five years are gravel packed and have been drilled to approximately 700 feet. The older wells, and wells purchased by the city are generally around 350 feet deep. The city has one elevated water storage tank with a capacity of 150,000 gallons, and several hydro-pneumatic pressure tanks that are used for storage."<sup>10</sup>

"The Water Division operates under the direction of the Board of Public Utilities. The quality of water is maintained through monitoring and proper maintenance of the system. State regulations require the city test the entire system, from wells to service connections, and that data is then reported to the State."<sup>11</sup>

## Sewer System

There is no community wide sewer system in Matheny Tract. The community depends on individual on-site septic tank systems for wastewater disposal. In wet years, the combination of a perched water table and tight soils creates problems for effective leaching of septic tank effluent. SGC survey results indicate that the Matheny Tract residents are in favor of creating a community wide sewer system. No public sewer facilities are available in Matheny Tract. Each land owner relies on septic systems for wastewater.

The City of Tulare and the County of Tulare are in the process of identifying/discussing specifics to allow connection to the city of Tulare's Wastewater Treatment System.

## Storm Water Drainage

"A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters,

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<sup>10</sup> City of Tulare Final Municipal Service Review August 2013, page 34

<sup>11</sup> ID

# MATHENY TRACT LEGACY PLAN

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manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Matheny Tract currently has a limited storm water system, which is served by Tulare Irrigation District (TID). The purpose of TID is to obtain and deliver surface water supplies for the purpose of agricultural irrigation in the District and for groundwater recharge efforts within the basin underlying the District. The District must also operate and maintain 330 miles of canal and approximately 30 miles of pipeline along with 1,110 acres of groundwater recharge/regulation basins.

In Matheny Tract (northern portion), the Oakland Colony Ditch bisects the Community running in a north- south direction between Ruth Street and Canal Street. **Figure 8** identifies Matheny Tract, the Oakland Colony ditch, a groundwater recharge basin, TID service area and the City of Tulare limits. According to TID, the Oakland Colony Ditch is used primarily for irrigation and flood control purposes. An existing pump station is located the northeast corner of Addie Avenue/Canal Street that pumps surface water into the ditch.

Excessive runoff from the Oakland Colony Ditch is directed to a groundwater recharge basin located south of North Matheny Tract. An additional basin is located to the east of the current basin for large flood events.

Tulare County has completed initial base mapping for Matheny Tract. Files obtained from RMA include surveying data with bench mark locations and top of curb or pavement elevations in North Matheny Tract. In addition, existing concrete pipes and directional flow arrows are mapped as shown in **Figure 8**.<sup>12</sup>

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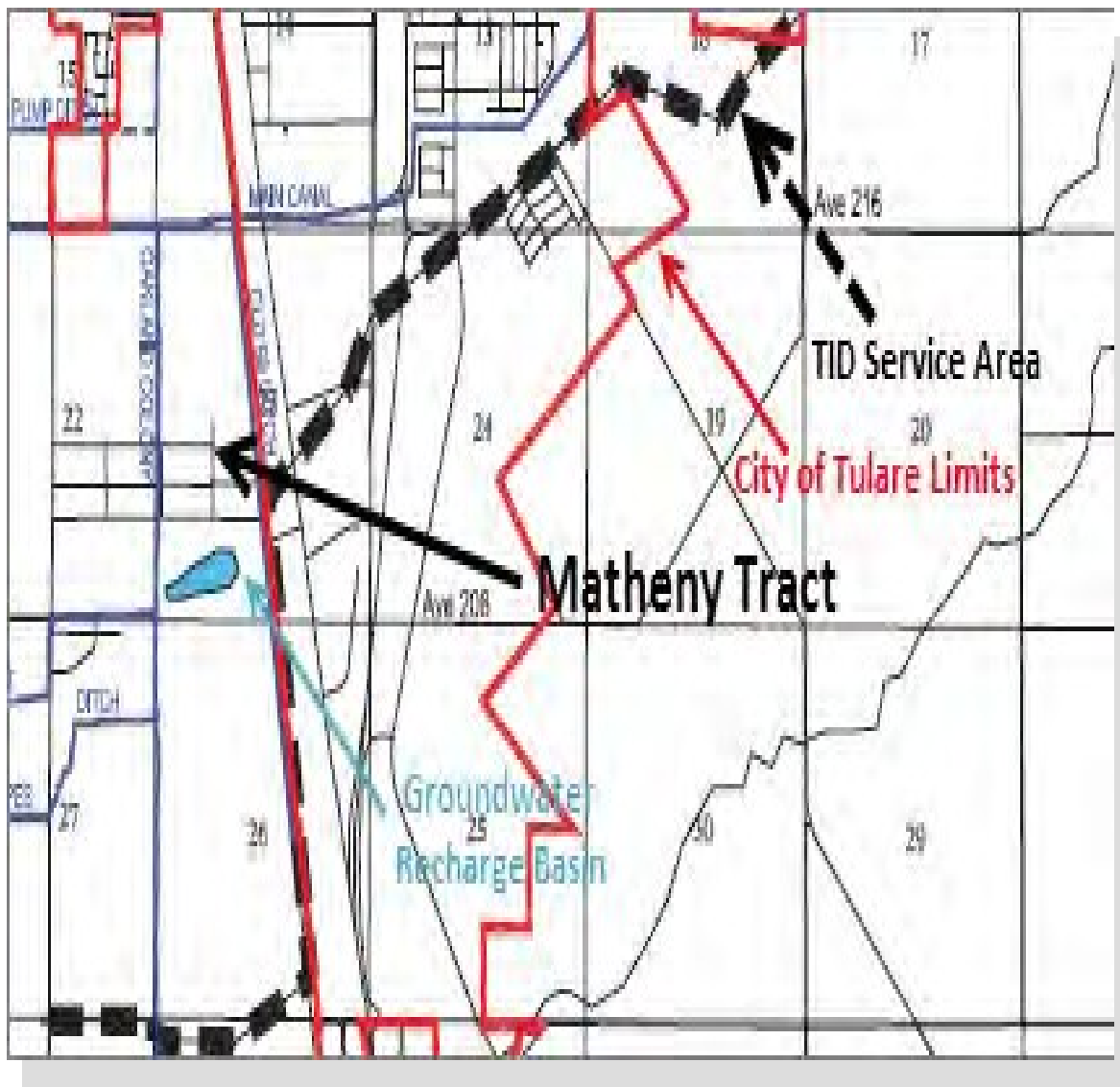
<sup>12</sup> Action Program 9, Tulare County 2015 Housing Element page 33-5

# MATHENY TRACT LEGACY PLAN

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It was also indicated by TID that trash accumulation in and around Oakland Colony Ditch in the Matheny Tract Community is a serious concern. Any future efforts to underground the ditch with a grate to exclude trash and other debris would be a maintenance issue to prevent obstruction of water flow. Additional costs would have to be offset with a maintenance agreement between the property owners and the beneficiaries of such improvements to safety and aesthetics, if desired.

**Figure 8 - Tulare Irrigation District Map**

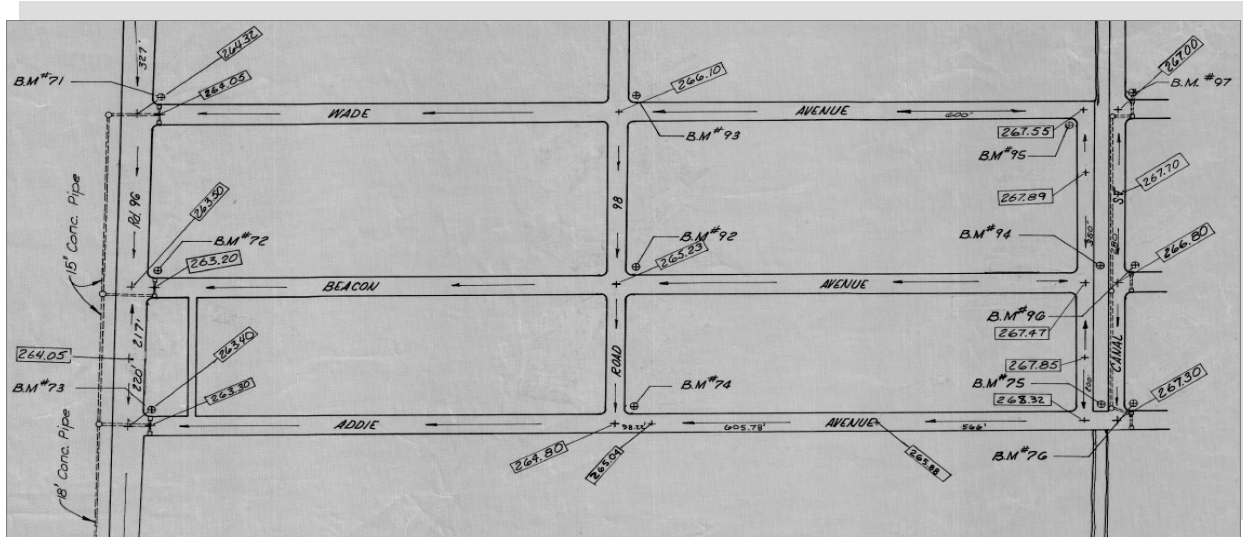


# MATHENY TRACT LEGACY PLAN

## Road Conditions

“Tulare County also has road classifications that are used to determine how the road is constructed, i.e., typical cross section (median, travel lanes, curb, gutter, sidewalk, shoulder, etc.), pavement structure, design speeds, grades, super elevation, sight distance, horizontal alignments, intersections, etc.. Tulare County has four road classifications as summarized below:

**Figure 9 - Matheny Tract Transportation and Infrastructure Plan**



(Source: Matheny Tract Transportation and Infrastructure Plan, Omni Means September 2014)

**Class 1 Roads:** a cul-de-sac or minor residential street so designed that it cannot serve more than 50 lots, the primary function of which is to provide access to abutting property.

**Class 2 Roads:** a minor residential street so designed that it cannot serve more than 120 lots, the primary function of which is to provide access to abutting property.

**Class 3 Roads:** a minor residential collector street that has one or is expected to have the dual purpose of providing access to abutting property and of carrying traffic from Class 1 and Class 2 Roads to roads in the County Select System.

**Select System Roads:** All State Highways, Federal Aid Secondary Routes, arterials and collector roads existing or unconstructed, that are designated for inclusion in the Select System by the Board of Supervisors with the approval of the State Department of Transportation.

Currently in Matheny Tract, there are only roads that are built within a two-lane right-of-way for Class 1, 2 and 3 Roads.

Based upon field reviews, the roads in Matheny Tract are generally in poor to fair condition, lack continuous curbs, gutters and sidewalks, are poorly lit at night, lack crosswalks, are susceptible to flooding i.e., lack drainage and provide limited opportunity for walking and bicycling beyond the vehicle travel surface.

# MATHENY TRACT LEGACY PLAN

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There are several roadways in Matheny Tract that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads.”<sup>13</sup>

## Street Lights

“Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

The County typically provides street lighting at major road intersections in the communities but does not provide mid-block lighting as is typical within cities. Matheny Tract has street lighting at 10 locations. Overhead utility poles are prevalent in Matheny Tract. These utility poles provide electricity and telephone service to residences and businesses in Matheny Tract and are located within the County right- of- way. **Figures 10, 11, and 12** display Existing Utilities Poles, Fire Hydrants and Street Lights in three sub-areas in Matheny Tract (Northwest Area, Northeast Area and Southwest Area). *[Note: New fire hydrants were being installed at various locations in 2014 that are not shown in these Figures].”<sup>14</sup>*

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<sup>13</sup> Ibid. 33-7

<sup>14</sup> Op. Cit.



# MATHENY TRACT LEGACY PLAN

Figure 10 - Existing Street Lights, Utility Poles and Fire Hydrants (Northwest Area)

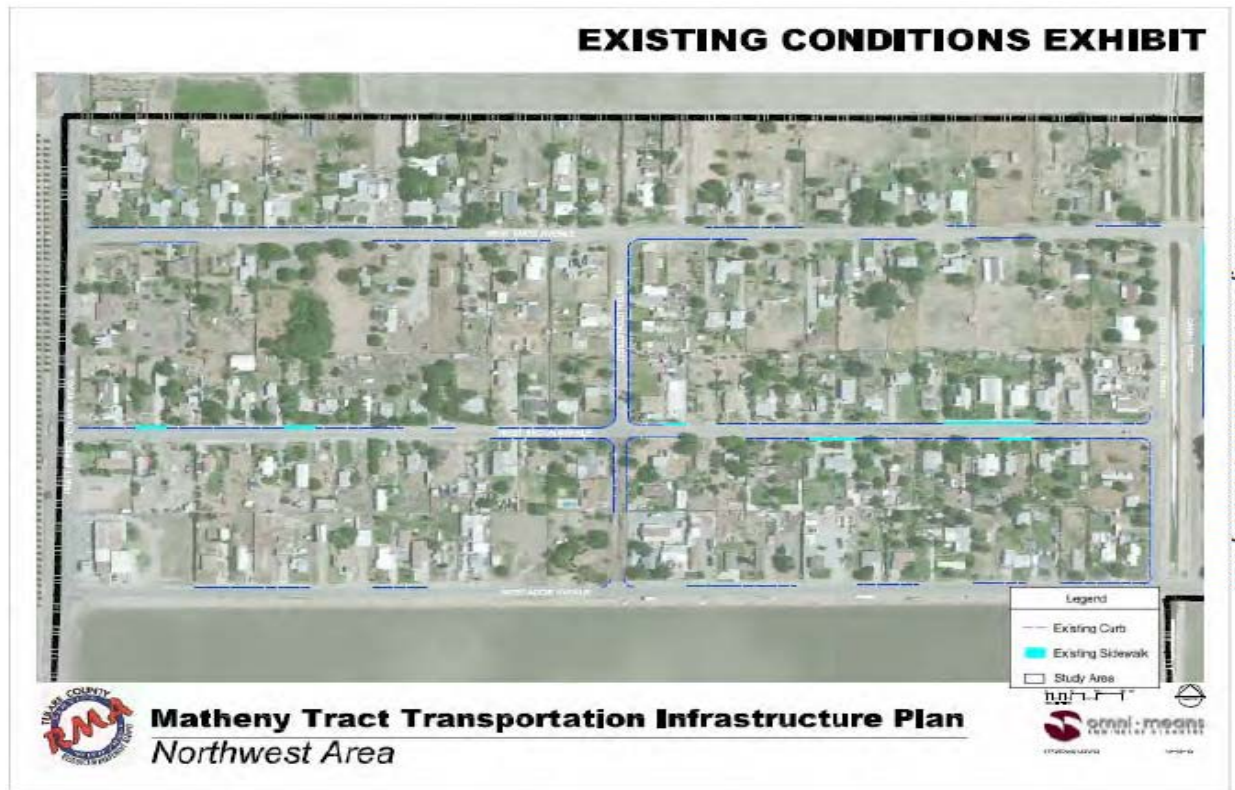
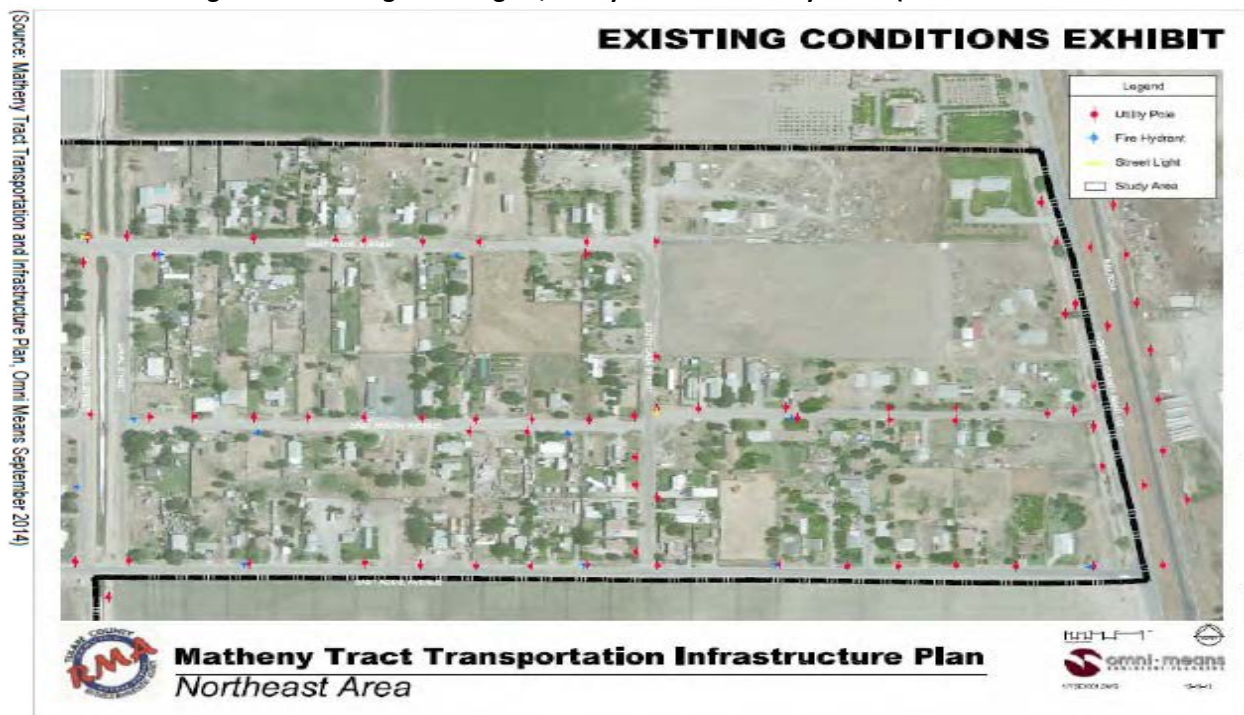


Figure 11 - Existing Street Lights, Utility Poles and Fire Hydrants (Northeast Area)



# MATHENY TRACT LEGACY PLAN

Figure 12 - Existing Street Lights, Utility Poles and Fire Hydrants of Matheny Tract (Southwest Area)



## Sidewalks

“Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in clear width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers, or other existing conditions.

The presence of curbs, gutters and sidewalks (CG&S) varies significantly between the communities in Tulare County. Some street segments within the Matheny Tract have curbs and fewer segments have sidewalks; however, several segments have no curbs, gutters or sidewalks. **Figures 13, 14, and 15** display existing curbs and sidewalks in three sub-areas in Matheny Tract (Northwest Area, Northeast Area and Southwest Area). As indicated in the Figures, many gaps and non-contiguous sections for both curbs and sidewalks exist.



# MATHENY TRACT LEGACY PLAN

Matheny Tract currently consists of existing sidewalks within the Community; however, many of these existing sidewalks are fragmented or are in relatively poor condition and need to be replaced entirely because they have deteriorated past the point where spot repairs are feasible or cost effective. The photograph to the left shows an existing street with driveways and a partial sidewalk. Many of the existing non-contiguous sidewalks are proposed to be replaced entirely in order to have uniformity and to be in compliance with current County Standards.”<sup>15</sup>

## ADA Curb Ramps

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are no ADA compliant curb ramps located within Matheny Tract.”<sup>16</sup>

**Figure 13 - Existing Curbs and Sidewalks of Matheny Tract (Northwest Area)**



<sup>15</sup> Op. Cit.

<sup>16</sup> Op. Cit.

# MATHENY TRACT LEGACY PLAN

Figure 14 - Existing Curbs and Sidewalks of Matheny Tract (Northeast Area)



Figure 15 - Existing Curbs and Sidewalks of Matheny Tract (Southwest Area)





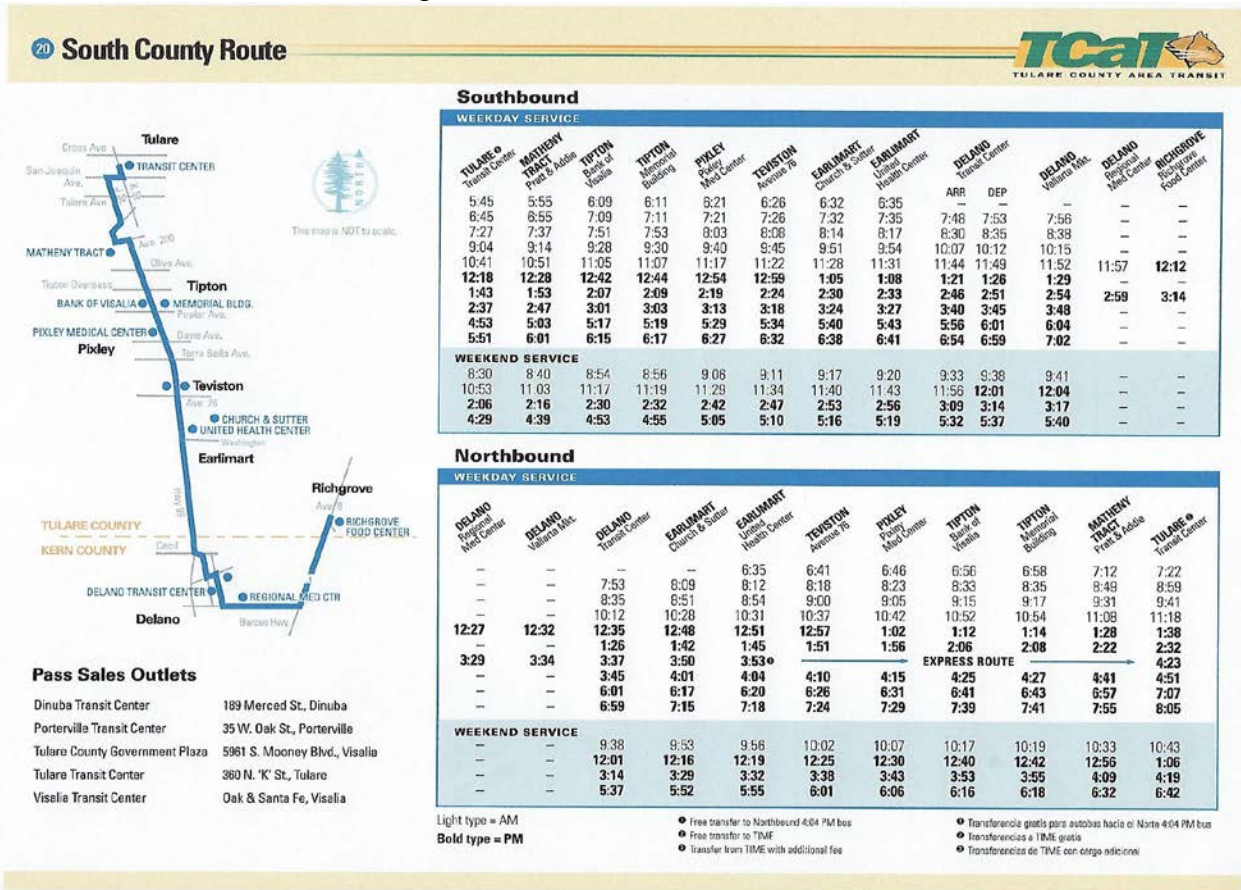
# MATHENY TRACT LEGACY PLAN

## Transit and Bus Stops

The Tulare County Transit Agency (TCaT) operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Matheny Tract is connected via TCAT Route 20 Southbound to Tulare (see Figure 16) and its transit center. TCaT also connects Delano (Kern County) in the south. Route 20 has ten northbound and southbound buses serving Matheny Tract on weekdays and four buses in each direction on Saturdays and Sundays. Stops are currently located at Pratt and Addie. In Tulare, transfers can be made to connect to Visalia (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>). TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Matheny Tract, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

Beginning in August 2017, TCaT Bus Stops for Southbound Route 20 at Matheny Tract have been realigned so that Passengers getting on or off will not be required to cross Pratt Street or Addie Avenue.

Figure 16 - TCaT Route 20 Southbound



# MATHENY TRACT LEGACY PLAN

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## State Route 99

State Route (SR) 99 is the major highway between cities within the Central Valley and is the primary route between the City of Fresno to the north and the City of Bakersfield to the south. Matheny Tract is located approximately two (2) miles west of SR 99.

## AMTRAK

The Hanford AMTRAK station, located approximately 25 miles to the northwest in Kings County, is the closest station to Matheny Tract providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

## Mefford Field Airport

“The Tulare County General Plan Part I – Goals and Policies Report pertaining to Aviation is intended to guide the long-term development of the airport, including the compatibility of adjacent land uses. The principle concerns of the Tulare County Comprehensive Airport Land Use Plan (CALUP) fall into five categories

- *Height restrictions:* to enhance aircraft safety by protecting the navigable airspace around airports;
- *Safety of persons on the ground:* to reduce risks to the population from aircraft operations and accidents;
- *Noise compatibility:* to minimize the effects of aircraft noise on communities adjacent to airports;
- *Overflight:* to balance land development within traffic patterns of public use airports; and
- Any proposed public, private or charter school site, or community college site, within two miles of the airport runway.”<sup>17</sup>

Currently, the airport is primarily used for general aviation operations, including local and itinerant services. Other Airport activities include airtaxi services and government operations.

“The ALUC is responsible for airport land use planning that balances the safety risks inherent to an airport against unnecessarily restricting a property owner’s ability to develop or use his or her land. Issues such as bird strikes, pilot error, mechanical failure, and obstructions in navigable airspace, as well as large concentrations of people on the ground need to be considered in achieving this balance. Large land areas around airports are exposed to the possibility of aircraft accidents even with well-maintained aircraft and highly trained pilots. Despite stringent laws governing aircraft and airport maintenance and pilot training, history demonstrates that aircraft accidents are going to occur. Although the risk to persons on the ground being killed or injured in an aircraft accident is small, such an accident is a high-consequence event, and particularly so if the accident location coincides with a large concentration of persons on the ground. For this reason airport safety zones are needed to define the nature of the risk and to minimize the number of persons who may be exposed to air crash hazards.”<sup>18</sup>

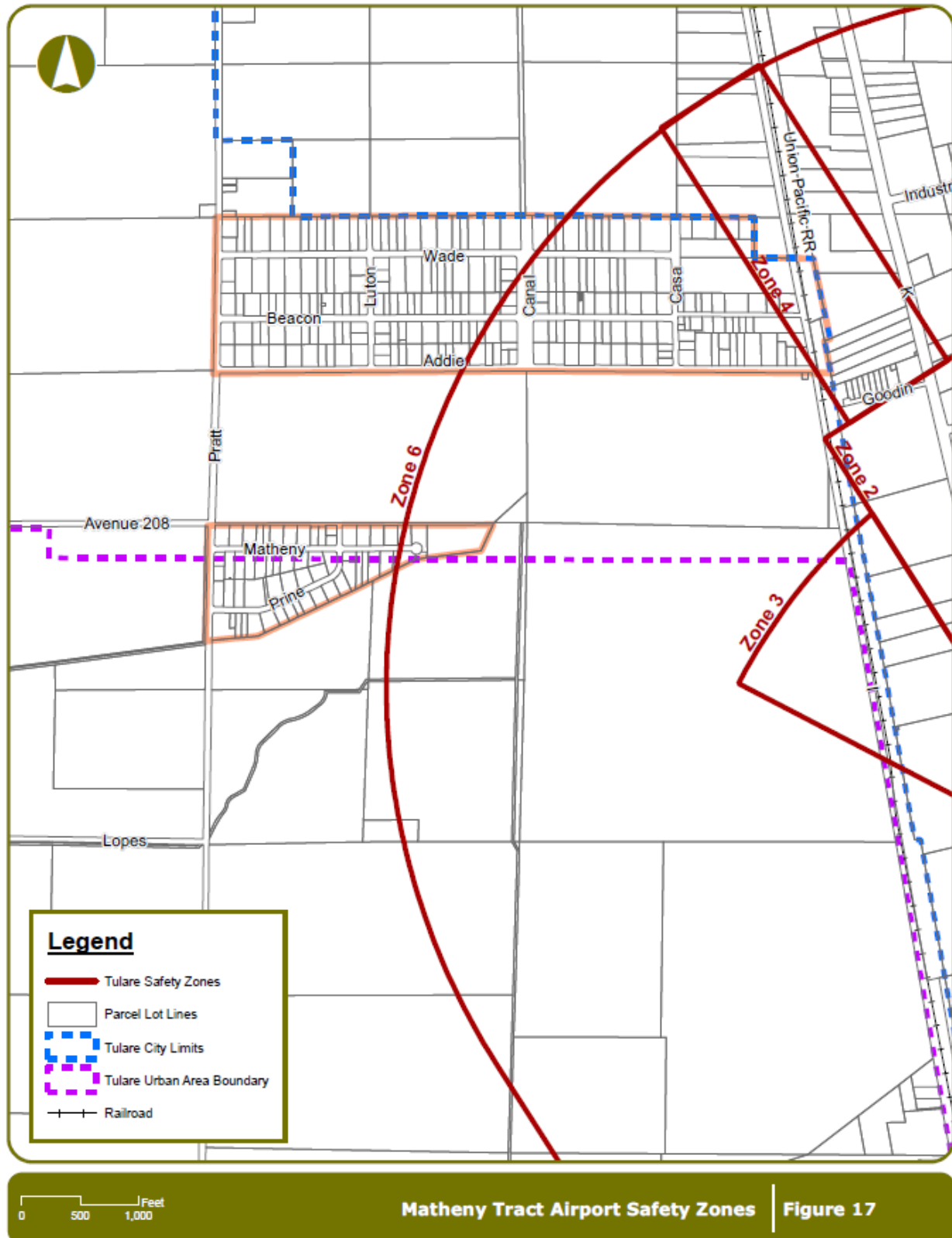
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<sup>17</sup> Tulare County Comprehensive Airport Land Use Plan, page 2-2

<sup>18</sup> Ibid. 2-8

# MATHENY TRACT LEGACY PLAN

Figure 17 - Mefford Field Airport Safety Zones



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## Airport Safety Zones

Six safety zones (see **Figure 17**) were identified to represent the relative safety risks. These safety zones and their respective risk factors are summarized below:

- ***“Safety Zone 1, Runway Protection Zone*** – The Runway Protection Zone (RPZ) is a trapezoidal area located immediately off each end of a runway. This area is defined by FAA who recommends it be a part of the airport property due to its very high risk factors. Aircraft over fly this area at altitudes below 200 feet. Caltrans research indicates that 20 to 21 percent of near-runway accidents occur in this zone.
- ***Safety Zone 2, Inner Approach/Departure Zone*** – The Inner Approach/ Departure Zone is a rectangular area located along the extended runway centerline immediately beyond the RPZ. Aircraft over fly this area at altitudes between 200 and 400 feet above the runway elevation. Caltrans research indicates that 8 to 22 percent of near-runway accidents occur in this zone.
- ***Safety Zone 3, Inner Turning Zone*** - The Inner Turning Zone is a cone shaped zone which lies on either side of the Inner Approach/Departure Zone. The sides of this zone are defined by a 20 or 30 degree angle, depending upon runway length, measured from the runway centerline. The apex of the cone is located on the runway at a distance from the runway end that is also dependent upon runway length. This zone encompasses locations where arriving aircraft are typically turning from the base to final approach legs of the standard traffic pattern and are descending from traffic pattern altitude or where departing aircraft normally complete the transition from takeoff power and flap settings to a climb mode and has begun to turn to their en route heading. Aircraft are less than 500 feet above the runway elevation. Caltrans research indicates that 4 to 8 percent of near-runway accidents occur in this zone.
- ***Safety Zone 4, Outer Approach/Departure Zone*** – is a rectangular are, which lies immediately beyond the Inner Approach/Departure Zones along the extended runway centerline. Particularly applicable for runways with straight-in instrument approach procedures, and other runways where straight-in or straight-out flight paths are common. Approaching and departing aircraft are usually at less than 1,000 feet above the runway elevation. Caltrans research indicates that 2 to 6 percent of near-runway accidents occur in this zone.
- ***Safety Zone 5, Sideline Zone*** - The Sideline Zone encompasses close-in areas lateral to the runway. These areas are typically within the airport property. The area is not normally over flown and the primary risk is from twin-engine aircraft losing directional control on takeoff. Caltrans research indicates that 3 to 5 percent of near-runway accidents occur in this zone.
- ***Safety Zone 6, Traffic Pattern Zone*** - The Traffic Pattern Zone is an oval shaped area centered on the extended runway centerline. This zone encompasses all other portions of the regular traffic patterns and pattern entry routes. This area generally has a low likelihood of accident occurrence at most airports, except where high concentrations of people present the potential for severe consequences. Caltrans

# MATHENY TRACT LEGACY PLAN

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research indicates that 18 to 29 percent of near- runway accidents occur in this zone, but that these numbers are misleading due to the large size of this zone.”<sup>19</sup>

Matheny Tract is located in portions of Safety Zones 4 and 6.

## Aircraft Noise

The most common public complaint regarding airports is the noise generated by aircraft operations. Most individuals can tolerate low levels of aircraft noise, but as the overall noise level rises and begins to interfere with conversation, sleep, business and other activities, the frequency of complaints increases. Complaints can also result from a single event in which the perception is held that an aircraft is too low or too noisy. Eventually, excess noise levels become detrimental to the public health, safety and welfare and, therefore, contrary to the public interest.

Noise restriction policies at Tulare County public-use airports are proposed to limit the number of people exposed to frequent and/or high levels of airport noise or to frequent and/or high cumulative noise levels of which airport noise is one component. The basic strategy for achieving noise compatibility is to limit the development of land uses that are particularly sensitive to noise and to obtain avigation easements for aircraft noise within all aircraft safety areas (defined in Section 2.4) and overflight areas (defined in Section 2.6). The following policies were established:

### **HS-3.1 Airport Land Use Compatibility Plan**

The County shall require that development around airports is consistent with the safety policies and land use compatibility guidelines contained in the adopted Tulare County Comprehensive Airport Land Use Plan (CALUP)

### **HS-3.2 Compliance with Federal Aviation Administration (FAA) Regulations**

The County shall ensure that development within the airport approach and departure zones is in compliance with Part 77 of the FAA Regulations (*FAA regulations that address objects affecting navigable airspace*).

### **HS-8.4 Airport Noise Contours**

The county shall ensure new noise sensitive land uses are located outside the 60 CNEL contour of all public use airports.

Matheny Tract is located outside of the 55 dB CNEL noise contour of the Tulare Municipal Airport (Mefford Field).

## Land Use Compatibility Matrix

“The Tulare County Airport Land Use Compatibility Matrix consists of two tables. Table 12 addresses land use compatibility in the safety and height restriction zones by land use type. Table 13 addresses the maximum residential densities in the safety and height restriction zones. Table 13 also addresses the maximum non-residential intensity in the safety and height restriction zones. Footnotes document the application of ALUC policy for specific categories of land use. It should be noted that land uses are subject to jurisdictional requirements and restrictions found in General and Specific Plans and

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<sup>19</sup> Op. Cit. 2-9

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zoning ordinances adopted by the various cities and Tulare County, some of which may be more restrictive than those presented in **Table 13.**<sup>20</sup>

<b>Table 13 - Tulare County Airport Land Use Compatibility</b>							
<b>Land Use Category</b>	<b>Safety Zone 1</b>	<b>Safety Zone 2</b>	<b>Safety Zone 3</b>	<b>Safety Zone 4</b>	<b>Safety Zone 5</b>	<b>Safety Zone 6</b>	<b>Remainder Areas within Airport Influence Area<sup>3</sup></b>

## **Agriculture & Animal Keeping**

Crop production including dry and irrigated farming	C <sup>8</sup>	C <sup>8</sup>	C	C	C <sup>8</sup>	C	C
Truck Farming, Specialty Crops, Orchards, Vineyards, Landscape Nurseries, Greenhouses	P	C	C	C	P	C	C
Crop Processing and Packaging, Wineries	P	C	C	C	C <sup>8</sup>	C	C
Pasture and Rangeland Grazing	P	C	C	C	P	C	C
Hogs, Dairies, Bee Keeping	P	C	C	C	P	C	C
Commercial Poultry	P	P	P	P	P	P	C
Fish Farms, Game Preserves	P	C <sup>8,9</sup>	C <sup>8,9</sup>	C <sup>8,9</sup>	P	C	C
Feed Lots, Stockyards, Sales Yards	P	C <sup>8,9</sup>	C <sup>8,9</sup>	C <sup>8,9</sup>	P	C	C
Animal Hospital, Veterinary Clinic, Kennels, Pet Boarding, Equestrian Facilities, Exotic Animals	P	C <sup>8,9</sup>	C <sup>8,9</sup>	C <sup>8,9</sup>	P	C	C
Roadside Stands, Farmers Markets	P	C	C	C	P	C	C

## **Residential**

Single Family Residential	P	P	P	P	P	C <sup>11</sup>	C <sup>11</sup>
Multi-Family Residential, Mobile Home Parks	P	P	P	P	P	P	C <sup>11</sup>
Group Homes, Convalescent Facilities, Nursing Homes	P	P	P	P	P	P	C <sup>11</sup>
Granny Flat (1,200 s.f. or less)	P	P	P	P	P	P	C <sup>11</sup>
Caretaker Residence (1,200 s.f. or less)	P	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>

<sup>20</sup> Op. Cit. 3-1



# MATHENY TRACT LEGACY PLAN

(Table 13 continued) - Tulare County Airport Land Use Compatibility

Land Use Category	Safety Zone 1	Safety Zone 2	Safety Zone 3	Safety Zone 4	Safety Zone 5	Safety Zone 6	Remainder Areas within Airport Influence Area <sup>3</sup>
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## Institutional, Public and Quasi-Public

Schools and Hospitals	P <sup>18</sup>	P <sup>18</sup>	P <sup>18</sup>	P <sup>18</sup>	P <sup>18</sup>	P <sup>18</sup>	C <sup>18</sup>
Libraries, Day Care Centers, Social Clubs/Lodges, Churches	P	P	P	P	P	P	C
Parks, Playgrounds, Picnic Areas	P	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>
Athletic Fields	P	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>	C <sup>17</sup>
Cemeteries - People or Pets	P	C	C	C	C	C	C
Public Utility Facilities (except Electric Plants)	P	C <sup>8</sup>	C <sup>8</sup>	C <sup>8</sup>	C	C	C
Electric Power Plants (including wind turbines and solar) and overhead transmission lines	P	P	P	P	P	C	C
Correctional Facilities	P	P	P	P	P	C	C

## Communications

Broadcast Studios	P	C	C	C	P	C	C
Transmission Stations, Towers, Antennas	P	P	P	P	P	C <sup>16</sup>	C

## Resource Extraction

Mining – Sand, Gravel, Fill Dirt	P	P	C	C	P	C	C
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## Commercial Recreational

Mining – Sand, Gravel, Fill Dirt	P	C	C <sup>17</sup>	C <sup>17</sup>	C	C <sup>17</sup>	C
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# MATHENY TRACT LEGACY PLAN

(Table 13 continued) Tulare County Airport Land Use Compatibility

Land Use Category	Safety Zone 1	Safety Zone 2	Safety Zone 3	Safety Zone 4	Safety Zone 5	Safety Zone 6	Remainder Areas within Airport Influence Area <sup>3</sup>
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## Commercial Recreation (continued)

Outdoor Theaters, Amusement Parks, Carnivals, Fairs	P	P	C <sup>17</sup>	C <sup>17</sup>	P	C <sup>17</sup>	C
Golf Courses, Tennis Courts	P	C	C	C	C	C	
Multi-Use Stadium/Motor Speedway	P	P	P	P	P	C	
Swimming Pools, Water Slides	P	P	C <sup>17</sup>	P	P	C	

## Retail Commercial

Aircraft Fuel, Aircraft Sales, Aircraft Repairs and Aircraft Flying Schools	P	P	P	P	C	P <sup>19</sup>	C
Vehicles and Parts Sales, Building Materials, Food and Beverage Sales	P	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>	P	C <sup>11</sup>	C
Shopping Centers	P	P	P	P	P	C <sup>11</sup>	C
Banks	P	P	P	P	P	C <sup>11</sup>	C
Small Retail Commercial Center	P	P	C <sup>11</sup>	C <sup>11</sup>	P	C <sup>11</sup>	C
Gasoline Service Stations	P	P	C	C	P	C	C
Restaurant and Food Take-Out, General Retail Stores, Tasting Rooms	P	P	C <sup>11</sup>	C <sup>11</sup>	P	C <sup>11</sup>	C
Convention and Conference Centers	P	P	C <sup>11</sup>	P	P	C <sup>11</sup>	C
Fuel Dealers, Fuel Storage	P	C <sup>13</sup>	C <sup>13</sup>	C <sup>13</sup>	P	C <sup>13</sup>	C

## Service Commercial

Office Buildings, Public Buildings, Research Laboratories	P	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>	C	C
Appliance and Equipment Repair, Car Wash	P	C	C	C	P	C	C

# MATHENY TRACT LEGACY PLAN

(Table 13 continued) Tulare County Airport Land Use Compatibility							
Land Use Category	Safety Zone 1	Safety Zone 2	Safety Zone 3	Safety Zone 4	Safety Zone 5	Safety Zone 6	Remainder Areas within Airport Influence Area <sup>3</sup>

## Service Commercial (continued)

Personal Services, Health Clinics	P	C <sup>11</sup>	C <sup>11</sup>	C <sup>11</sup>	P	C <sup>11</sup>	C
Recycling	P	C <sup>8,13</sup>	C <sup>8,13</sup>	C <sup>8,13</sup>	P	C	C

## Transient Lodgings

Hotels and Motels, Bed and Breakfast	P	P	C <sup>10</sup>	C <sup>10</sup>	C <sup>10</sup>	C <sup>10</sup>	C
RV Parks	P	P	C <sup>10</sup>	C <sup>10</sup>	C <sup>10</sup>	C <sup>10</sup>	C

## Wholesale & Storage

Mini-Storage	P	P	P	P	C	C	C
Ammonium Nitrates	P	P	P	P	P	P	P
Warehouse, Wholesale and Distributing	P	C <sup>7</sup>	C	C <sup>7</sup>	C <sup>15</sup>	C	C
Landfills	P	P	P	P	P	P	P
Petroleum and Chemical Products – Bulk Storage	P	P	C <sup>13</sup>	C <sup>13</sup>	C	C	C

## Manufacturing & Processing

Indoor Processes	P	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C
Industrial Manufacturing	P	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C
Warehousing & Distribution	P	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C <sup>14</sup>	C

## Transportation

Vehicle Storage and Parking	C <sup>7</sup>	C	C <sup>7</sup>	C	C	C	C
Taxi Stands, Bus Stations/Terminals	P	C <sup>12</sup>	C <sup>12</sup>	C <sup>12</sup>	C <sup>12</sup>	C	C
Truck Terminals	P	C	C	C	C <sup>15</sup>	C	C

## Notes

- Land uses are identified as being “C” – compatible, or “P” – prohibited based upon the following interpretations:

*Compatible* - Compatible land uses are designated by the symbol “C”. This designation means associated land use groups are at a level of intensity or density, or location, which does not present a significant risk to the safety of persons on the ground or to persons in aircraft over-flying the proposed use, nor is the land use type sensitive to anticipated aircraft noise or frequent aircraft over-flights.

*Prohibited* - Prohibited land uses are designated by the symbol “P”. The associated land use groups are at a level of intensity or density, or location, which presents a significant risk to the

# MATHENY TRACT LEGACY PLAN

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safety of persons on the ground or to persons in aircraft over-flying the proposed use, or the land use groups are sensitive to anticipated aircraft noise or frequent aircraft over-flights.

2. The land use categories illustrated provide a representative sample of land uses found in Tulare County for the purpose of identifying any associated noise, safety, height, or overflight issues within the various zones of the Airport Influence Area. Other land use types that exhibit functional characteristics similar to the uses listed are likely to receive a similar compatibility rating. When it is not clear how a particular land use type might be rated for compatibility the referring agency, landowner or developer should contact ALUC Staff.
3. Safety Zones 1 through 5 represent areas of greatest risk with respect to aircraft accidents. All uses that constitute a hazard to flight, including physical objects in the navigable airspace, activities that create a glare or visual interference to a pilot, or electronic interference with aircraft operations are specifically excluded from these zones regardless of whether they meet other qualifying criteria, unless such prohibition is precluded by applicable state statutes. Land use development that may cause the attraction of birds is also prohibited. In locations under portions of established instrument approach or departure routes, object heights may be restricted to less than that indicated by FAR Part 77 imaginary surfaces. An FAA aeronautical study may be required. All new development within Safety Zones 1 through 6 must dedicate an avigation easement to the airport sponsor.
4. Safety Zone 6 includes considerable overflight activity and although safety concerns are diminished, aircraft noise and objects within the navigable airspace are of primary concern.
5. The Airport Influence Area is defined by the outer edge of the conical surface as described in FAR Part 77, plus aircraft noise areas outside the conical surface that exceed 60 dB CNEL.
6. The Remainder areas include portions of the FAR Part 77 horizontal surface not included within the safety zones, together with the conical surface and any 60 dB CNEL noise zones that project beyond the conical surface.
7. Allowed as a temporary use of Airport lands provided the activity does not attract birds or interfere with Airport operations.
8. No structures, congregations of equipment or vehicles, or public venues shall be located within 500 feet of runway centerline.
9. Land uses that incorporate the use of any weapons or implements that would launch a projectile into the air other than animal tranquilizers are prohibited.
10. As a general policy, new residential development is an undesirable land use within Safety Zones 1 to 5. It is the intent of the ALUC to prohibit further residential subdivision of land within these Safety Zones, or to allow changes to land use or zoning in a manner that would accommodate additional dwelling units. Dwelling units already approved in accordance with current General Plans or Zoning and property owners allowed development of a single family house by right are not affected.
11. In areas where aircraft noise is expected to exceed 60dB CNEL; inhabited residential structures must meet California Noise Standards and be designed to achieve an interior noise level of 45 dB CNEL or less. Non-residential structures such as offices, restaurants and retail stores must meet an interior noise level of 50 dB CNEL or less.
12. Allowed only to the extent that such uses support the flow of passengers to and from the Airport.
13. For otherwise acceptable land uses, the limit for aboveground storage of hazardous materials is 2,000 gallons.

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14. Allowed if dust, fumes, and other aspects of the process are carried out in a controlled environment.
15. A compatible use only when the activity is an integral part of an acceptable on-Airport use.
16. Subject to location and height limits.
17. Any activities located in Safety Zones 1 through 6 must meet nonresidential intensity standards – See Table 3-2 of this matrix.
18. No local schools (K-12) or hospitals are permitted in Safety Zones 1 to 6. School locations must meet California Education Code standards.
19. Retail Commercial (Aircraft Fuel, Aircraft Sales, Aircraft Repairs and Aircraft Flying Schools) are a compatible use on airport property within Safety Zone 6.

**Table 14 - Maximum Allowed Densities**

Current Setting	Safety Zone 1	Safety Zone 2	Safety Zone 3	Safety Zone 4	Safety Zone 5	Safety Zone 6	Remainder Areas within Airport Influence Area <sup>3</sup>

**Maximum Residential Densities (average number of dwelling units per gross acre)**

Rural	0	Note A	Note A	Note A	Note A	No Limit Note B	No Limit Note B
Suburban	0	1 per 10-20 ac	1 per 2-5 ac	1 per 2-5 ac	1 per 1-2 ac	No Limit Note B	No Limit Note B
Urban	0	0	Note C	Note C	Note C	No Limit Note B	No Limit Note B
Dense Urban	0	0	Note C	Note C	Note C	No Limit Note B	No Limit Note B

**Maximum Nonresidential Intensities (average number of people per gross acre)**

Rural	0 Note D	10-40	50-70	70-100	50-70	150-200	No Limit
Suburban	0 Note D	40-60	70-100	100-150	70-100	200-300	No Limit
Urban	0 Note D	60-80	100-150	150-200	100-150	No Limit Note E	No Limit
Dense Urban	0 Note D	Note F	Note F	Note F	Note F	No Limit Note E	No Limit

**Maximum Single Gross Acre Intensity (numbers of people)**

Rural	0	50-80 Note G	150-210 Note H	210-300 Note H	150-210 Note H	600-800 Note I	No Limit
Suburban	0	80-120 Note G	210-300 Note H	300-450 Note H	210-300 Note H	800-1200 Note I	No Limit
Urban	0	120-160 Note G	300-450 Note H	450-600 Note H	300-450 Note H	No Limit Note E	No Limit
Dense Urban	0	Note F	Note F	Note F	Note F	No Limit Note E	No Limit

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- Notes:**
- A Maintain current zoning if less than density criteria for suburban setting. Noise and overflight should be considered. Affected jurisdictions may impose greater density restrictions through their general plan and/or zoning.
  - B Allow infill at up to the average density of surrounding residential area.
  - C Exceptions can be permitted for agricultural activities, roads and automobile parking provided that FAA criteria are satisfied.
  - D Large stadiums and similar uses should be prohibited.
  - E Allow infill at up to the average intensity of comparable surrounding uses.
  - F Based on 2x the Maximum Nonresidential Density.
  - H Based on 3x the Maximum Nonresidential Density.
  - I Based on 4x the Maximum Nonresidential Density.

**Source:** Derived from Figures 4B through 4G, *California Airport Land Use Planning Handbook*, State of California, Department of Transportation, and Division of Aeronautics, October 2011.

## Aircraft Overflight Areas

Matheny Tract is located within the Airport Influence Area of Mefford Field. “Current California Law requires residential property owners to disclose to prospective buyers that the property is in the “vicinity” of an airport.”<sup>21</sup> “To ensure that potential and prospective land users within an Airport Influence Area are provided with sufficient information regarding the presence of, and activities at, the nearby airport the following overflight policies are adopted:

- a. Avigation easements shall be obtained and recorded for all properties to be developed within Safety Zones 2 to 6 (See Policy 2.4.3.g. earlier), and in those portions of Safety Zone 1 that are not owned by the Airport Sponsor.
- b. Deed notices describing the potential for airport impacts shall be required as a condition of development in those areas of the Airport Influence Area outside designated Safety Zones.
- c. All real estate transfers within the Airport Influence Area shall include the following Notice of Airport in Vicinity in the Real Estate Transfer Disclosure Statement:

### ***Notice of Airport in Vicinity***

*This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.*<sup>22</sup>

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<sup>21</sup> Op. Cit. 2-18

<sup>22</sup> Op. Cit. 2-19

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Visalia Municipal Airport, is located approximately 15 miles northeast of Matheny Tract, offers passenger service to Los Angeles. Fresno Yosemite International Airport (FAT), approximately 70 miles northeast of Matheny Tract, is the principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field, Bakersfield's principal commercial airport, is approximately 70 miles southeast of Matheny Tract and offers direct flights to several destinations.

## Union Pacific Railroad

Matheny Tract is bound on the east side by the Union Pacific Railroad (formerly Southern Pacific Transportation Company) line running through Tulare County. "According to the Trainmaster's office in Fresno, there are more than 20 freight train operations per day in the Tulare County Area. Passenger trains presently do not operate on Union Pacific tracks in Tulare County"<sup>23</sup> Train speeds on the mainline are generally 45-65 mph and train movements may occur at any time during the night or day.

According to the Wyle methodology, the type and frequency of rail operation results in noise exposures of 65 and 60 dB Ldn at approximately 335 and 660 feet, respectively, from the center of the tracks for present operations, and at approximately 440 and 800 feet, respectively, from the center of the tracks for estimated future operations. There are areas in the east side of the Matheny Tract LDB that are located within 335 feet of the Union Pacific Railroad line. Noise levels are higher at grade crossings due to the warning horn. The Paige Avenue (Avenue 216) and Avenue 200 grade crossings are over one (1) mile from the Community of Matheny Tract.

The Union Pacific Railroad tracks are elevated approximately 10-feet above natural ground surface; these tracks serve as a physical boundary between the commercial and industrial areas located in the City of Tulare to the east and the Matheny Tract.

## Energy: Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom PG&E has arrangements, and rolling blackouts.

The Southern California Gas Company provides gas service to Matheny Tract.

## Solid Waste

Solid waste disposal services for Matheny Tract is provided by Waste Management, a private company. Solid waste generated in Matheny Tract can be disposed of at Visalia Landfill, located at 8614 Avenue 328.

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<sup>23</sup> Tulare County General Plan Background Report, page 8-61

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## Internet Access

Most families in Matheny Tract do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Matheny Tract.

## PUBLIC SERVICES

### Sheriff

Police protection services are provided in Matheny Tract by the Tulare County Sheriff's Department main Sheriff Office located at 2404 W. Burrel Avenue, in Visalia, approximately 20 miles northeast of Matheny Tract.

### Fire

Tulare County and all of the incorporated cities have a mutual-aid agreement for fire protection services. The proximity of the nearest City or County fire station varies significantly between the unincorporated communities. There are three City fire stations and one County fire station in the Tulare area. County Fire Station #25 is within Tulare City limits. County Fire Station #25, located at Foster Drive/Turner Drive, is the closest station to Matheny Tract (two miles) and is shown in Figure 33-6.

Eleven fire hydrants are found within Matheny Tract. These fire hydrants are located within the County right-of-way. Emergency Services are located in the City of Tulare (**see Figure 18**).

### Parks:

There are no County owned/operated public parks in Matheny Tract. In the community of Matheny Tract there is no safe open green space for children to play and adults to be physically active. The nearest park is located approximately two (2) miles away in the City of Tulare.

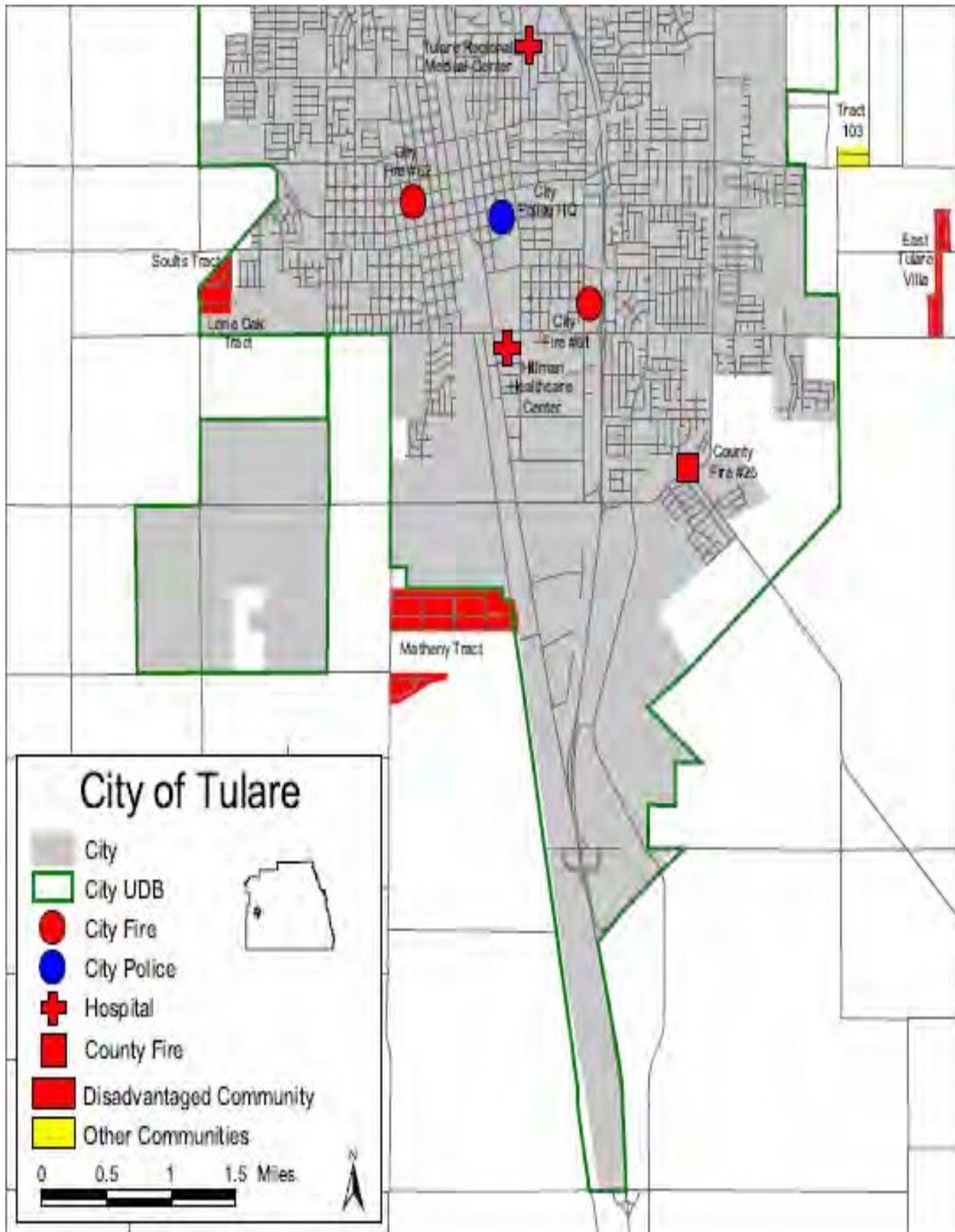
### Schools

Schools are an especially important component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. Most children attend Palo Verde Union School District (not part of the Tulare City School District). The District provides Preschool -8<sup>th</sup> grade education. High School students attend high school in the City of Tulare, approximately two (2) miles away.



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Figure 18 - Emergency Services - City of Tulare



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## Library

“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>24</sup> The Visalia Branch Library

is located in the City of Visalia approximately 20 mile to the northeast. The Tulare Public Library is located approximately five miles northeast in the City of Tulare (see Table 15).

Table 15 - Library Location & Hours		
	Address	Service Hours (2017)
Tulare Public Library	Main Branch 200 W. Oak Ave Visalia, CA 93291	Tuesday – Friday: 10:00 am – 7:00 pm Saturday 10:00 am – 5:00 pm

Library hours current as of August 2017

## CIRCULATION AND TRAFFIC

“For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

Tulare County’s relationship to the State Route system, nearby counties, cities, and communities

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>25</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and

<sup>24</sup> General Plan Background Report, page 7-96

<sup>25</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

# MATHENY TRACT LEGACY PLAN

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a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>26</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation

Matheny Tract is a small agricultural community located adjacent to Pratt Street (Road 90). The internal traffic circulation system for the Matheny Tract Legacy Plan is comprised of Matheny Avenue, Wade Avenue, Beacon Avenue, Addie Avenue, Pratt Street, Lutton Street, Canal Street, Casa Street, and Prine Drive. There are no proposed major streets as part of this Legacy Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

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<sup>26</sup> Tulare County General Plan Background Report, page 5-7.

# MATHENY TRACT LEGACY PLAN

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## GOALS, OBJECTIVES AND POLICIES

This Section of the Matheny Tract Legacy Plan prescribes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Legacy Boundaries were updated in 2012 to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Matheny Tract to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan,

Matheny Tract Legacy Plan, and input received from Matheny Tract citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

### Community Development

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

#### Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

#### Policies:

1. Establish areas zoned exclusively for commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned commercial areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
5. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
6. The County shall work with the Schools to provide safe routes to school.

# MATHENY TRACT LEGACY PLAN

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7. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Matheny Tract Legacy Plan.
8. The Matheny Tract Legacy Plan should be reviewed every five years to determine if amendments are appropriate.
9. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.

3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to initiate home maintenance/repair programs in Matheny Tract.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Matheny Tract.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.



# MATHENY TRACT LEGACY PLAN

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3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### **Policies:**

1. Promote a concentration of commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new commercial development.

#### **Policies:**

1. Encourage the Matheny Tract to give priority to community service development in the areas reserved for commercial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial areas.

Objective: Provide the necessary safe guards to attract quality commercial development to the community.

#### **Policies:**

1. Assure that commercial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of commercial areas.

## **Environmental Quality and Public Safety**

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Matheny Tract citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### **Policies:**

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

Objective: Provide sufficient open space for community recreation needs.

#### **Policies:**

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

# MATHENY TRACT LEGACY PLAN

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Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Matheny Tract.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Matheny Tract.

## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

Infrastructure: In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

Use Permits: There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

Education: Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Health Care: Health care is important for economic development as businesses need healthy employees. The nearest medical offices are located in the City of Tulare.

# MATHENY TRACT LEGACY PLAN

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## LAND USE AND ZONING ACREAGES

### Land Use and Zoning District Updates

As suggested earlier, and based on the forecasted growth and the recommended Legacy Development Boundary, the Land Use Plan (see **Table 15**) and Zoning Districts Plan (see **Table 16**) has been updated.

<b>Table 16 - Proposed Land Use Plan</b>	
Proposed Land Use	Acres
Mixed Use	154.9
Right-of Ways	32.9
Matheny Tract HDB	187.8

<b>Table 17 - Proposed Zoning District</b>	
Proposed Zones Districts	Acres
AE-20	2.9
C-2-MU	16.3
M-1	3.1
R-2	1.0
R-A	131.6
Unclassified (Right-of Ways)	32.9
Matheny Tract HDB	187.8

## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the General Plan recommendations contained in the Matheny Tract Legacy Plan. The following components comprise the Matheny Tract Community Plan implementation strategy:

Limitation of the A-1 Zoning District. Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

**Zoning District Changes:** As part of this Implementation Program for the Matheny Tract Legacy Plan, there are a variety of changes to existing zoning districts. These changes are described below.

**Chapter 16 of the Zoning Code:** Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

**Mixed Use Overlay District:** This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Matheny Tract.



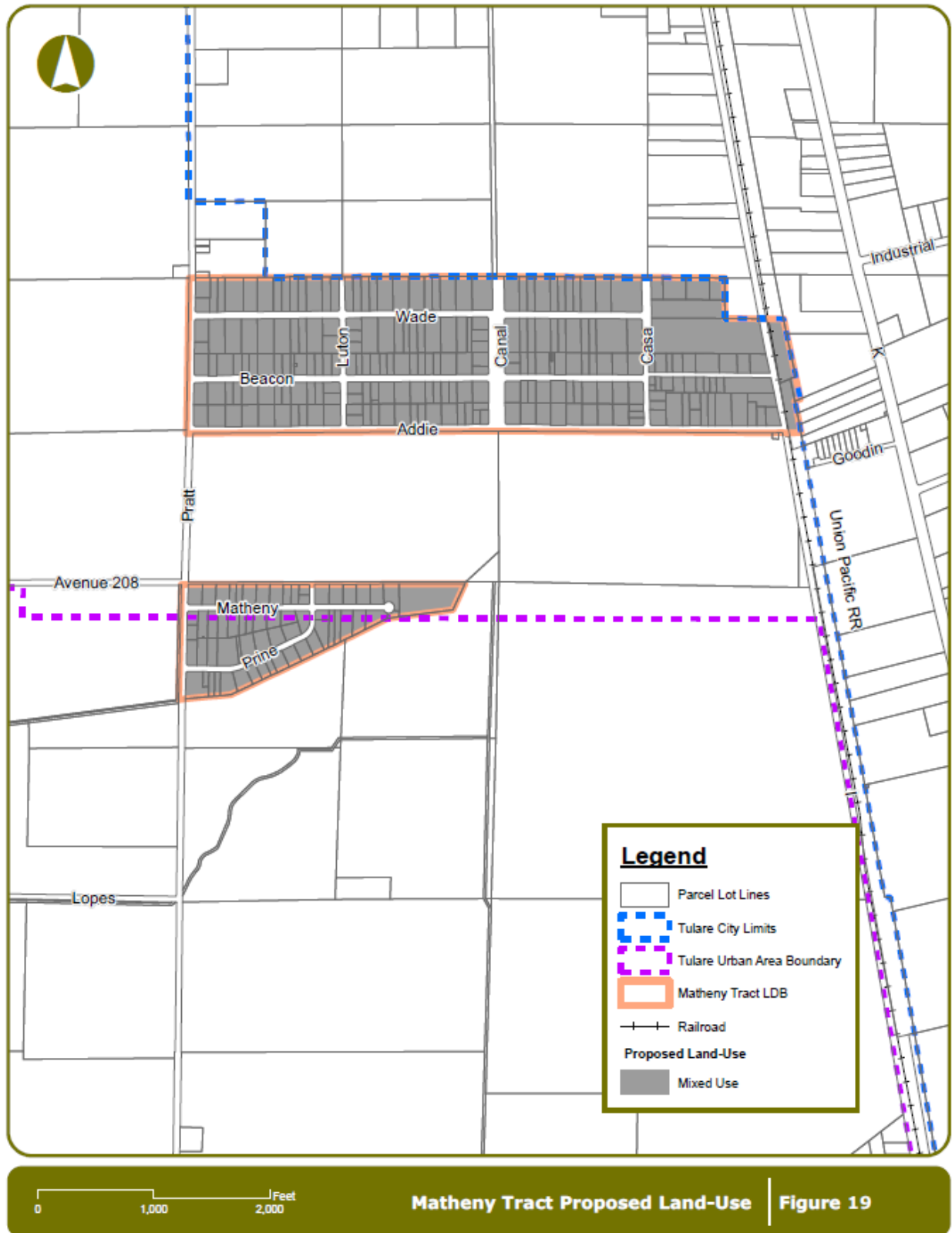
# MATHENY TRACT LEGACY PLAN

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**Zoning Map Update:** The current Zoning Map for Matheny Tract (**see Figure 20**) will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 19**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 21**) Matheny Tract Proposed Zoning).

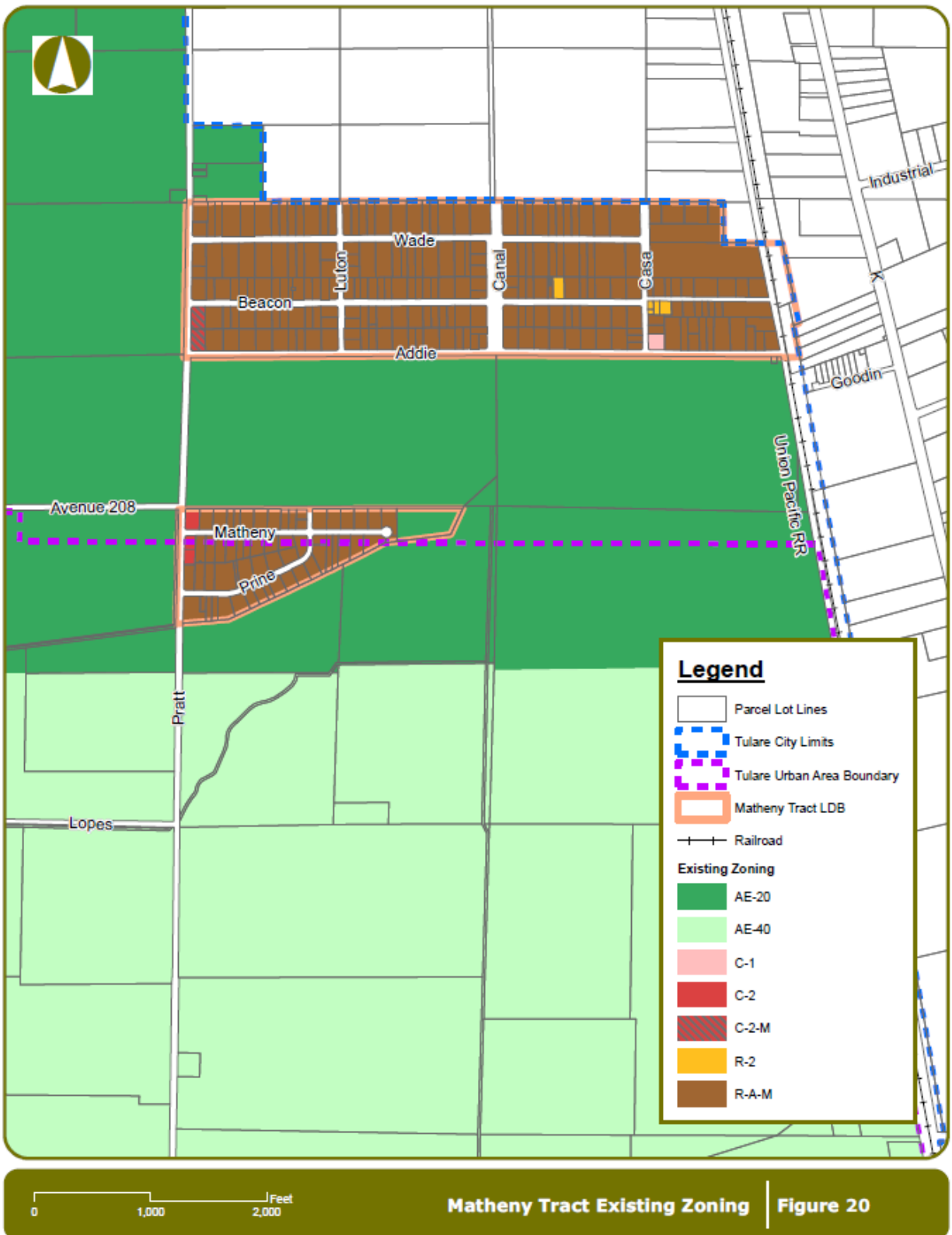
# MATHENY TRACT LEGACY PLAN

Figure 19 - Proposed Land Use Plan



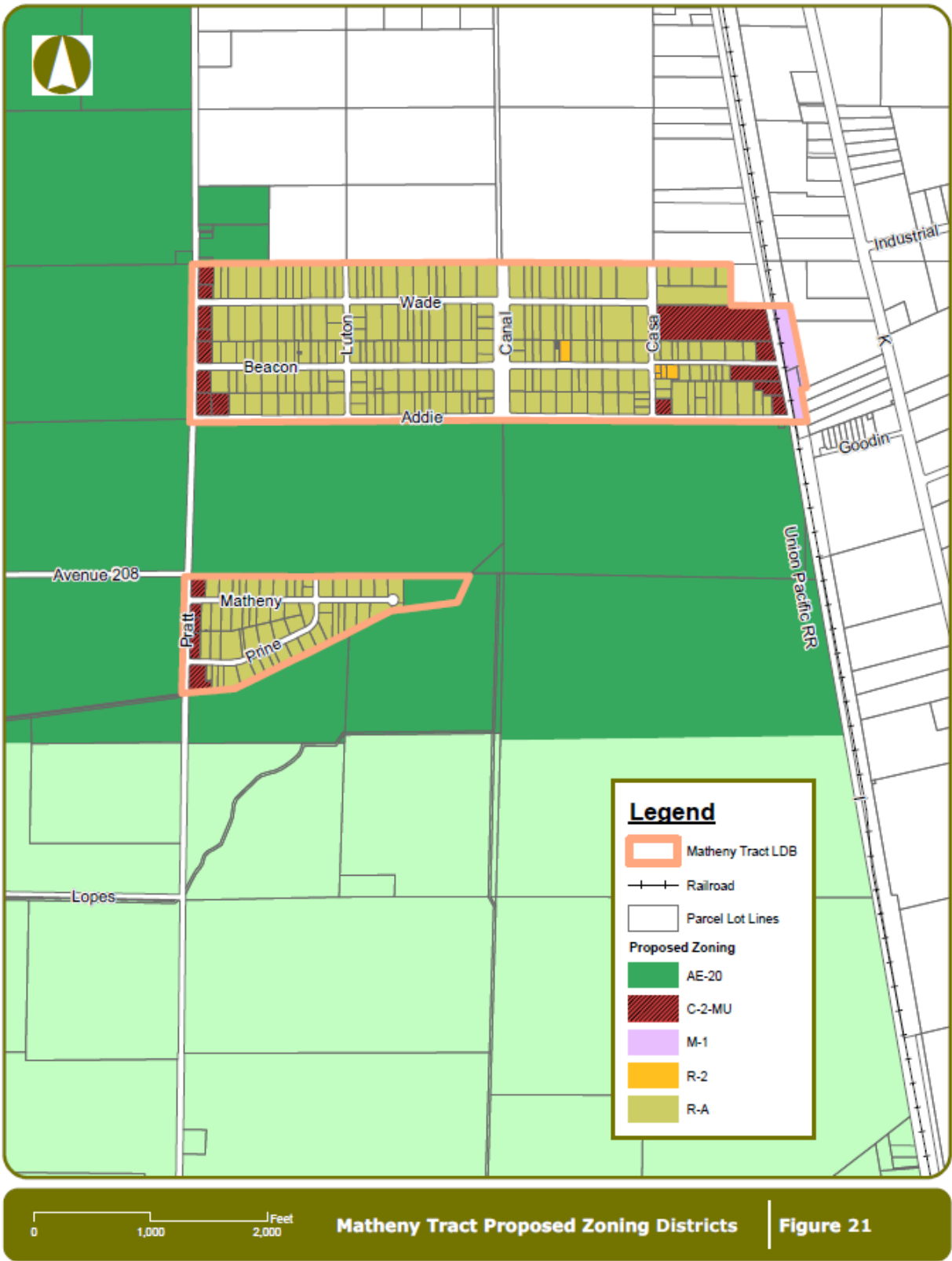
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Figure 20 - Existing Zoning Districts



# MATHENY TRACT LEGACY PLAN

Figure 21 - Proposed Zoning Districts



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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

A-3 – Matheny Tract Community Outreach Flyers

# MATHENY TRACT LEGACY PLAN

## *A-1 Use Permit Requirement Changes (Zone Change Text)*

### **Additional By-Right Uses**

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1

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Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1



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Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1 ,C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1 ,C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	

# MATHENY TRACT LEGACY PLAN

Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1

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Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone

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Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# MATHENY TRACT LEGACY PLAN

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## ***A-2 Mixed Use Overlay District (Zone Change Text)***

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The following regulations shall apply in the community of Matheny Tract, unless otherwise provided in this Ordinance.

### **A. PURPOSE**

The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

### **B. APPLICATION**

This overlay zone only applies to the communities of Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Matheny Tract, Jovista, Matheny Tract and Tooleville..

### **C. USE**

No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Matheny Tract.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

<b>Uses/Combination of Uses reviewed by Planning Commission</b>
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# MATHENY TRACT LEGACY PLAN

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The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## D. DEVELOPMENT STANDARDS

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

# MATHENY TRACT LEGACY PLAN

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## E. ALL OTHER DEVELOPMENT STANDARDS

All other Development Standards are included below and in the Community Plans for Ducor, Terra Bella, Traver, Strathmore, Pixley, and Tipton, and the Legacy Plans for Matheny Tract, Jovista, Matheny Tract and Tooleville. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

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A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.



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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to

screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior

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fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

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WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

# MATHENY TRACT LEGACY PLAN

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SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

# MATHENY TRACT LEGACY PLAN

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SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateways signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.



# MATHENY TRACT LEGACY PLAN

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AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# MATHENY TRACT LEGACY PLAN

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## A-3 – Community Outreach Flyers

**Leadership Counsel for Justice & Accountability and TULARE COUNTY**

invite you to participate in a

### **COMMUNITY MEETING**

to

### **DISCUSS IMPROVEMENT NEEDS**

in

### **Matheny Tract**

*The Sustainable Communities Strategy Project (SCS) seeks to document the following needs: Sewer, Water, Water Quality & Quantity, Storm Water Drainage, Fire, Access to Transportation, Housing, Schools, Libraries, Parks, Access to Healthy Foods and Shopping Opportunities, Access to Medical Facilities, Internet Access and more.*

The information that you provide at this meeting will help Tulare County create a plan and identify resources for future infrastructure improvements, land use changes, and zoning changes where possible.

**When:** January 9<sup>th</sup>, 2016

**Location:** Palo Verde School, Old Cafeteria  
10369 Avenue 196, Tulare, CA  
93274

**Time:** 10am- 12pm



For more information please call: Merced at 310-499-8034 or Ashley at 415-686-1368

\*Refreshments and snacks will be provided.





# MATHENY TRACT LEGACY PLAN

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El Consejo de Liderazgo y el CONDADO DE TULARE

les invita a una

## **JUNTA COMMUNITARIA**

para

### **Hablar de las necesidades de infraestructura en Matheny Tract**

*El Proyecto de Estrategia de Comunidades Sostenibles (SCS) esta documentando las siguientes necesidades: alcantarillado, agua, calidad y cantidad del agua, drenaje pluvial, agua, fuego, el acceso al transporte, vivienda, escuelas, bibliotecas, parques, acceso a alimentos y tiendas, acceso a servicios médicos, acceso al internet y mucho más!*

La información que usted proporcione en esta junta ayudará al condado crear un plan e identificar recursos para mejorar la infraestructura de La Villa donde sea posible.

También ayudará a planificar el uso de la tierra y los cambios de zonificación.



**When: January 9<sup>th</sup>, 2016**

**Location: Palo Verde School, la Cafeteria pequeña  
10369 Avenue 196, Tulare, CA 93274**

**Time: 10am- 12pm**

Para más información llame a: Merced al 310-499-8034 o Ashley al 415-686-1368

*\*Refrescos y aperitivos serán proveídos.*



# MATHENY TRACT LEGACY PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9342

General Plan Amendment GPA 17-035-Resolution No. 9343

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9344

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9345

Zoning District Map-Resolution No. 9346

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE LEGACY PLANS 2017 ) RESOLUTION NO. 9342  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, II AND PART III )  
AMENDMENTS (GPA 17-033) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Legacy Plans 2017 Update which consists of General Plan Amendment No. GPA 17-033 amendment to Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities & Services Elements, and Part II Rural Valley Lands Plan, and Part III (El Monte Mobile Village GPA 17-025; Hypericum GPA 17-026; Jovista GPA 17-027; Matheny Tract GPA 17-029 and Tooleville GPA 17-030 amending the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, Change of Zones No. PZC 17-025 El Monte Mobile Village; PZC 17-026 Hypericum; PZC 17-027 Jovista; PZC 17-029 Matheny Tract and Tooleville PZC 17-030, PZC 17-032 for an amendment to Section 16 of Ordinance No. 352, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Development Boundary, and Zone Ordinance Amendment No. PZC 17-031 establishing a Mixed Use Combining Zone in El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville (incorporated by reference herein). The unincorporated Legacy Communities are located within the Mount Diablo Base & Meridian as follows: El Monte Mobile Village, Section 12, Township 16S, Range 23E, Hypericum, Section 24, Township 19S, Range 25E, Jovista, Section 32, Township 24S, Range 26E, Tooleville, Section 12, Township 19S, Range 26E, Matheny Tract Section 22, 23, & 27, Township 20 south, Range 24 East.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element, Transportation & Circulation Element and the Public Facilities & Services Element; (2) add El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Legacy

Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Legacy Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-033 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management and the Transportation and Circulation Element; Change of Zone No. PZC 17-031 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for El Monte Mobile Village, Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista, Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract, Change of Zone No. PCZ 17-029 (Zoning District Map) and Tooleville, Change of Zone No. PZC 17-030 (Zoning District Map) within the respective Legacy Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-032 an amendment to Section 16 to allow additional by-right uses in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Legacy Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Legacy Plans;
  - b. Encourage infill development within Legacy Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Legacy Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The legacy planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on September 15, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Legacy Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Legacy Plans 2017 Update General Plan Amendment 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Change of Zone No. PZC 17-025; Hypericum General Plan Amendment No. GPA 17-026; Change of Zone No. PZC 17-026; Jovista General Plan Amendment No. GPA 17-027; Change of Zone No. PZC 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; Change of Zone No. PZC 17-029; Tooleville General Plan Amendment No. GPA 17-030; Change of Zone No. PZC 17-030; Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-032 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation, Public Facilities & Services, and Part II Rural Valley Lands Plan has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,

4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Legacy Plans 2017 Update. Therefore, the public will benefit from the Legacy Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on October 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano  
NOES: None  
ABSTAIN: None  
ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE ) RESOLUTION NO. 9343  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT AGRICULTURE ELEMENT, SCENIC )  
LANDSCAPES ELEMENT, AIR QUALITY ELEMENT, )  
PUBLIC FACILITIES & SERVICES ELEMENT, )  
INTRODUCTION, COMPONENTS A, B, C, AND D, AND )  
PART II RURAL VALLEY LANDS PLAN OF THE TULARE )  
COUNTY GENERAL PLAN FOR THE LEGACY PLANS )  
2017 UPDATE GPA 17-033 INCLUSIVE OF GPA's 17-025, )  
GPA 17-026, GPA 17-027, GPA 17-29, AND GPA 17-030 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Introduction; Component A-Planning Framework Element; Component B-Agriculture and Land Use Elements, Component C-Scenic Landscapes, Environmental Resources Management and Air Quality Elements; Component D-Transportation & Circulation and Public Facilities & Services Elements; and Part II Rural Valley Lands Plan of the Tulare County General Plan as set forth in Exhibits "A-1 to A-13" and Part III Community Plan as set forth in Exhibits "B-F" for the Legacy Plans 2017 Update General Plan Amendment No. GPA 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Legacy communities as part of the Legacy Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on September 15, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017 and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan for the Legacy Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Legacy Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Legacy Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to establish Legacy Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;

4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the establishment of legacy development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;
5. The Amendment to Part I Introduction, Component A, Component B, Agriculture, Component C, Scenic Landscapes, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and the newly established Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Legacy Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-033 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-033 the Legacy Plans 2017 Update set

forth in attached Exhibits “A-1 to A-13” and Part III Exhibits “B-F” which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

Exhibits “A-1 – A-13” – Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Service Elements, and Part II Rural Valley Lands Plan.

Exhibit “B” – 2017 El Monte Mobile Home Park Legacy Plan (Part III)

Exhibit “C” – 2017 Hypericum Legacy Plan (Part III)

Exhibit “D” – 2017 Jovista Legacy Plan (Part III)

Exhibit “E” – 2017 Matheny Tract Legacy Plan (Part III)

Exhibit “F” – 2017 Tooleville Legacy Plan (Part III)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO            )  
SECTION 18.9 "MU" MIXED USE COMBINING       )  
ZONE OF ORDINANCE NO. 352 CONSISTENT        )  
WITH THE ADOPTED LEGACY PLANS 2017           )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE       )  
NO. PZC 17-031                                    )

RESOLUTION NO. 9344

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and

Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9345
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-032	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the communities of El Monte Mobile Village,

Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update, consistent with the Legacy Communities 2017 Update including affirmation that Section 16 H. is applicable in the Matheny Tract.

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	RESOLUTION NO. 9346
IN THE LEGACY DEVELOPMENT BOUNDARIES	)	
EL MONTE MOBILE VILLAGE, HYPERICUM,	)	
JOVISTA, MATHENY TRACT, AND TOOLEVILLE	)	
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS 2017 UPDATE (GPA 17-033), AS	)	
PROPOSED INCHANGE OF ZONE NO. PZC's	)	
17-025, 17-026, 17-027, 17-029, AND 17-030	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" El Monte Mobile Village Rezoning Plan (PZC 17-025), to include the Legacy Community of El Monte Mobile Village, consistent with the adopted El Monte Mobile Village Legacy Plan 2017 Update, Exhibit "E" Hypericum Rezoning Plan (PCZ 17-026), to include the Legacy Community of Hypericum, consistent with the adopted Hypericum Legacy Plan 2017 Update, Exhibit "H" Jovista Rezoning Plan (PCZ 17-027), to include the Legacy Community of Jovista, consistent with the adopted Jovista Legacy Plan 2017 Update, Exhibit "K" Matheny Tract Rezoning Plan (PCZ 17-029), to include the Legacy Community of Matheny Tract, consistent with the adopted Matheny Tract Legacy Plan 2017 Update, Exhibit "N" Tooleville Rezoning Plan, to include the Legacy Community of Tooleville, consistent with the adopted Tooleville Legacy Plan 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update (GPA 17-033) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities;
3. The proposed changes in zone district designations identified in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the El Monte Mobile Village Change of Zone No. PZC 17-025; Hypericum Change of Zone No. PZC 17-026; Jovista Change of Zone No. PZC 17-027; Matheny Tract Change of Zone No. PZC 17-029; and Tooleville Change of Zone No. PZC 17-030, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" and "N" Rezoning Plans consistent with the adopted El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update (GPA 17-033).

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

El Monte Mobile Village

Exhibit "A" – Proposed Zone Ordinance Map (PZC 17-025)

Exhibit "B" – Proposed Zoning Map

Exhibit "C" – List of Affected Properties/Map

Hypericum

Exhibit "D" – Proposed Zone Ordinance Map (PZC 17-026)

Exhibit "E" – Proposed Zoning Map

Exhibit "F" – List of Affected Properties/Map

Jovista

Exhibit "G" – Proposed Zone Ordinance Map (PZC 17-027)

Exhibit "H" – Proposed Zoning Map

Exhibit "I" – List of Affected Properties/Map

Matheny Tract

Exhibit "J" – Proposed Zone Ordinance Map (PZC 17-029)

Exhibit "K" – Proposed Zoning Map

Exhibit "L" – List of Affected Properties/Map

Tooleville

Exhibit "M" – Proposed Zone Ordinance Map (PZC 17-030)

Exhibit "N" – Proposed Zoning Map

Exhibit "O" – List of Affected Properties/Map

Appendix B – Board of Supervisors  
Resolution No. 2017-0868



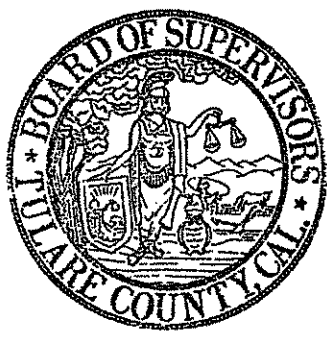
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# BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER LEGACY PLANS 2017 )  
UPDATE (GENERAL PLAN AMENDMENT 17- ) Resolution No. 2017-0868  
033) AND AMENDMENTS TO THE TULARE ) Ordinance No. 3508  
COUNTY ZONING ORDINANCE NO. 352 )

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR CROCKER, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD OCTOBER 17, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS  
NOES: NONE  
ABSTAIN: NONE  
ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY: *Michael C. Spata*  
Deputy Clerk

\*\*\*\*\*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Legacy Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Legacy Plans 2017 Update (GPA 17-033); and
3. Adopted one General Plan Amendment No. GPA 17-033, for the Legacy Plans 2017 Update inclusive of the El Monte Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029); Tooleville (GPA 17-030) and Amendments to Tulare County General Plan Part III to establish Legacy Plans and Legacy Development Boundaries; Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D,

Public Facilities and Services Elements, and Part II General Plan Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update; and

4. Waived the final reading and adopted Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Legacy Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-031) to Section 18.9 to adopt a Mixed-Use Combining Zone within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-032) to Section 16 to allow additional by-right uses within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for El Monte Village (PZC 17-025), Hypericum (PZC 17-026), Jovista (PZC 17-027), Matheny Tract (PZC 17-029), and Tooleville (PZC 17-030), consistent with the Legacy Plans 2017 Update; and
5. Adopted the findings of approval set forth in Planning Commission Resolution No. 9342 (Addendum to EIR), Planning Commission Resolution No. 9343 (Legacy Plans 2017 Update), Planning Commission Resolution No. 9344 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9345 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9346 (Zoning District Ordinance Maps); and
6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of El Monte Mobile Village, Hypericum, Matheny Tract, Jovista, and Tooleville to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents; and
10. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

## **EXHIBIT "A"**

### **A. Legacy Plans 2017 Update (General Plan Amendment 17-033)**

WHEREAS, on September 27, 2017 the Planning Commission held a public hearing on the Legacy Plans 2017 Update (GPA 17-033), which included an application for El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030, El Monte Mobile Village Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract Change of Zone No. PZC 17-029 (Zoning District Map); and Tooleville Change of Zone No. PZC 17-030 (Zoning District Map), Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9) and Change of Zone No. PZC 17-032 (By-Right Uses–Section 16), collectively referred to as the Legacy Plans 2017 Update (GPA 17-033); and

WHEREAS, upon conclusion of the public hearing, the Planning Commission recommended the Board of Supervisors certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopt the Mitigation Monitoring and Reporting Program and approve the Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors held a public hearing on October 17, 2017 to consider the proposed Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors finds the public hearing for the Legacy Plans 2017 Update was properly noticed in accordance with state law and the Tulare County Ordinance Code.

WHEREAS, the Board of Supervisors finds the Legacy Plans 2017 Update is compatible, integrated and internally consistent with the existing policies of and with the Tulare County General Plan.

WHEREAS, the Board of Supervisors accepts and incorporates by reference herein, the findings of facts and recommendations adopted by the Planning Commission as follows:

1. Planning Commission Resolution No. 9342, in the matter of the Certification and adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update (PC Package Attachment 1 – CEQA Resolution).

2. Planning Commission Resolution No. 9343, to adopt General Plan Amendment No. GPA 17-033 for the Legacy Plans 2017 Update inclusive of El Monte Mobile Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029) and Tooleville (GPA 17-030) and Amendments to Part III of the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, to the Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities and Services Elements, and Part II Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update (PC Package Attachment 2 – GPA Resolution).
3. Planning Commission Resolution No. 9344, in the matter of an Amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed Use Combining Zone within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 3 – Section 18.9 Resolution).
4. Planning Commission Resolution No. 9345, in the matter of an Amendment to Section 16 of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 4 – Section 16 Resolution).
5. Planning Commission Resolution No. 9346, in the matter of an Amendment to Ordinance No. 352, the Zoning Ordinance, as set forth in the Zoning District Ordinance Maps for El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the Legacy Plans 2017 Update to rezone certain properties within El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville. (PC Package Attachment 5 – Rezoning Plan Resolution).

NOW, THEREFORE, BE IT RESOLVED as follows:

A. The Board of Supervisors hereby certifies and adopts the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopts the Mitigation Monitoring Reporting Programs.

NOW BE IT FURTHER RESOLVED as follows:

B. The Board of Supervisors adopts the entirety of all of the above findings and actions as one General Plan Amendment No. GPA 17-033.

C. The Board of Supervisors hereby waives the final reading of Change of Ordinance 352 and adopts amendments to the Tulare County Zoning Ordinance 352 pertaining to Section 18.9 - Mixed-Use Combining Zone, to Section 16 - Variances and Special Uses, and Zoning District Map Changes.

D. The Board of Supervisors authorizes the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of Legacy Plans 2017 Update to include Mixed Use Combining Zone and additional "By-Right" uses.

E. The Board of Supervisors directs the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq.

F. The Board of Supervisors directs the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file Notices of Determination with the Tulare County Clerk.

G. The Board of Supervisors authorizes the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents.

## Tooleville Legacy Plan

# TOOLEVILLE LEGACY PLAN 2017

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Funded by Strategic Growth Council



Tulare County Resource Management Agency  
Economic Development and Planning Branch



# TOOLEVILLE LEGACY PLAN

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# TOOLEVILLE LEGACY PLAN

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The work upon which this publication is based was funded in whole or in part through a grant awarded by the Strategic Growth Council.

## **Disclaimer**

The statements and conclusions of this report are those of Tulare County and not necessarily those of the Strategic Growth Council or the Department of Conservation, or its employees. The Strategic Growth Council and the Department of Conservation make no warranties, express or implied, and assume no liability for the information contained in the succeeding text.

# TOOLEVILLE LEGACY PLAN

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# TOOLEVILLE LEGACY PLAN

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## Tooleville Legacy Plan

Adopted: October 17, 2017

Tulare County Board of Supervisors  
Resolution No. 2017-0868

Tulare County Planning Commission  
Recommendations: September 27, 2017  
Resolutions No. 9342, 9343, 9344, 9345, and 9346

General Plan Amendment: GPA 17-033  
Section 16 Zoning Ordinance (Mixed-Use): PZC 17-032  
Section 18.9 Zoning Ordinance (By-Right Uses): PZC 17-031

Tooleville Legacy Plan: GPA 17-030  
Zoning District Map: PZC 17-030



Tulare County Resource Management Agency  
Economic Development and Planning Branch  
5961 S Mooney Boulevard  
Visalia, CA 93277-9394  
(559) 624-7000

# TOOLEVILLE LEGACY PLAN

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# TOOLEVILLE LEGACY PLAN

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## **Tulare County Board of Supervisors**

Kuyler Crocker – District 1  
Pete Vander Poel – District 2 (Chairman)  
Amy Shuklian – District 3  
Steve Worthley – District 4 (Vice Chairman)  
Mike Ennis – District 5

## **Tulare County Planning Commission**

John F. Elliott – District 1 (Chair)  
Nancy Pitigliano – District 2  
Bill Whitlatch – District 3  
Melvin K. Gong – District 4  
Vacant – District 5  
Wayne O. Millies – At Large  
Ed Dias – At Large  
Gil Aguilar – District 2 (Alternate) (Vice Chair)

# TOOLEVILLE LEGACY PLAN

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# TOOLEVILLE LEGACY PLAN

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## **Tulare County Resource Management Agency**

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Susan Simon, Planner III  
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Johnson Vang, Engineer I

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# TOOLEVILLE LEGACY PLAN

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## INTRODUCTION

Tooleville is a small rural community located on the east side of Spruce Road (Road 204) roughly a mile and a half east of the city of Exeter in Tulare County. Homes in Tooleville are located along Alfred Avenue on the north and Morgan Avenue on the south, with a few homes fronting Spruce Road. The Friant Kern Canal borders the east side of the community.

Tooleville (named for the Toole family) was a farmworker settlement established by Dust Bowl migrants in the 1940's. It remains largely a farmworker town, with a population this is currently about 88.5% Hispanic and 11.5% White.

### Location

The Tooleville Census Designated Place (CDP) is located in the central part of Tulare County on the valley floor. Tooleville is a small rural community located on the east side of Spruce Road (Road 204) roughly a mile and a half east of the city of Exeter in Tulare County. Tooleville is in Section 12. Township 19 South, Range 26 East, MDB&M, and can be found within Rocky Hill United States Geological Survey 7.5 minute topographic quadrangle. Tooleville is located at an elevation of 396 feet National Geodetic Vertical Datum. The coordinates of Tooleville are: Latitude: 36.287450, Longitude: -119.424884. Homes in Tooleville are located along Alfred Avenue on the north and Morgan Avenue on the south, with a few homes fronting Spruce Road. The Friant Kern Canal borders the east side of the community. Tooleville is divided into two Census Blocks (4000 and 4001). Its population was 339 at the 2010 Census. According to Census data, there were 78 households and the median age was 22.4 years. The nearest city is Exeter, directly to the west (**see Figure 1**).

**Legend**

- Tooleville LDB
- Supervisorial Districts
- Exeter Urban Development Boundary
- Exeter Urban Area Boundary

**Tooleville Vicinity Map | Figure 1**



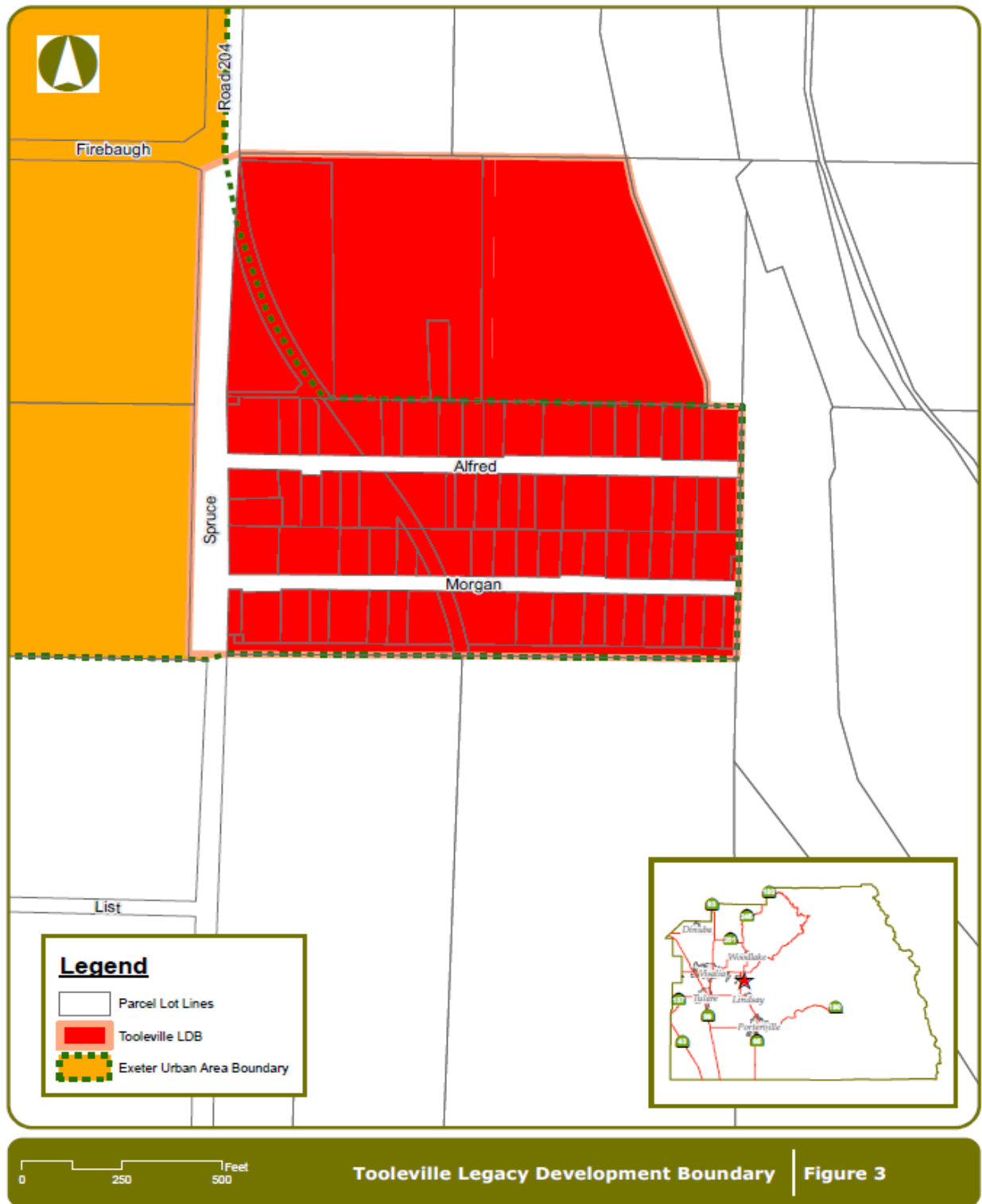
# TOOLEVILLE LEGACY PLAN

Figure 2 - Tooleville Aerial Map



# TOOLEVILLE LEGACY PLAN

Figure 3 - Tooleville LDB





# TOOLEVILLE LEGACY PLAN

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## **Community Outreach and Invitation Process**

The County of Tulare was successful in obtaining a grant from the Strategic Growth Council (SGC) to assist in the preparation of this Legacy Plan. In efforts to invite and make local residents aware of the Strategic Growth Council (SGC) project, Self Help Enterprises (SHE) went door to door throughout the community of Tooleville to distribute flyers at every home and local business. During this process SHE talked with residents about the importance of participating in this process. Meeting invitations were sent to the members of the Tooleville Mutual Water Company. Flyers were posted at the local taco stand.

Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the (SGC) project.

- 1.) SHE staff developed an informational flyer.
- 2.) SHE staff partnered with Tooleville Mutual Water Company to post flyers and spread the word about the meeting. The Tooleville Water Mutual Company Board Members were invited to attend the meeting.
- 3.) SHE staff did door to door outreach on Tuesday October 20 and Thursday October 22, 2015.
- 4.) During the community outreach process, SHE staff explained the goals of the project and talked with residents about the importance and the community benefits of participating in the SGC community meeting.

## **Community Meeting:**

The Strategic Growth Council held a community input meeting in Tooleville on Tuesday October 27, 2015 at 6:00 pm. The meeting was scheduled in the evening to allow all residents to attend, including those that work during the day. The meeting was held at the office of the Tooleville Water Mutual Company. This is an ideal location because most residents know where the office is located.



# TOOLEVILLE LEGACY PLAN

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## Community Survey

A detailed survey was developed in efforts to gather specific community information about different areas of concern. The survey asked about the following: Schools, Libraries, Housing, Zoning, Parks, Shopping Opportunities, and Access to Gas Stations, Access to Medical Facilities, Natural Gas, Internet Access, Flooding, Fire, Safety, Water Quality & Quantity, Waste Water, Storm Water Drainage, Complete Streets, and priority improvements. During the Strategic Growth Council Community meeting, residents listed the following as their priority needs/concerns:

## Priority Concerns

- Water Supply
- Storm Water Drainage
- Street lights
- Stop Signs
- Internet
- Police Patrol
- Clinic

**Water** - Tooleville Mutual Non-Profit Water Association is a small mutual water company ran by a five-member board. Tooleville has two undependable water wells and is planning to drill a new well once the location has been determined. They are actively searching for potential well sites in Tooleville and neighboring Exeter. Tooleville is exploring the different ways that could potentially partner with Exeter. TMNPWA is currently reviewing three options: Water wheeling, Master meter or full consolidation with the City of Exeter.

**Storm Water Drainage** – Tooleville residents report that the community does not have adequate storm water drainage. Large puddles form when it rains and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season. This becomes a bigger problem when the stagnant water attracts insects and mosquitos.

**Street Lights** - Residents report dark streets at night due to little or no street lighting. Most people stay indoors at night because they do not feel safe walking on a dark street. At night, one cannot see down the street; this is a major safety concern for them, especially since the Sheriff's Department seldom patrols Tooleville. Residents are especially concerned about how this affects the safety of local children. Residents said that some streets have light poles but the light bulb is out and other streets have no poles at all. Residents report being told that there is a cost to them to install a light pole on their street.

**Stop Signs on Alfred and Morgan** - Tooleville is a small community comprised of two residential roads. Residents are concerned with road safety on the corners of Alfred and Spruce as well as Morgan and Spruce. Drivers state they cannot see oncoming traffic and that this has led to car crashes at both corners. Residents are asking for stop signs or flashing lights to slow traffic on Spruce Road.

# TOOLEVILLE LEGACY PLAN

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**Internet** - Most families in Tooleville do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Tooleville.

**Police Patrol** - Residents report that the TCSD response time is long and that there is too little Sheriff Patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go un-responded to. People feel that the limited presence of law enforcement contributes to the local crime rate and makes criminals feel unstoppable in that area of Tooleville. Tooleville residents are asking for increased Sheriff presence in their community.

**Clinic** - Tooleville residents do not have access to medical care within the community. The nearest medical clinic is approximately 2.5 miles away in Exeter. Most residents report traveling to Visalia, 13 miles for medical care. This is concerning to residents especially during medical emergencies. Families without vehicles and one car families are especially affected by this. Families that do not have a car are forced to find transportation to access medical care in a nearby community by paying for a ride or using public transportation to travel there. The nearest hospital is in Visalia, 13 miles away, and it takes approximately 20 minutes for the nearest ambulance to get to Tooleville from Visalia.

## DEMOGRAPHICS

The 2015 United States Census reported that Tooleville had a population of 391. The racial makeup of Tooleville was 45 (11.5%) White, (0.0%) African American, (0.0%) Native American, (0.0%) Asian, (0.0%) some other races, and (0.0%) from two or more races. Hispanic or Latino of any race were 346 persons (88.5%). The average renting household size was 4.51. There were 124 housing units, of which 83 (66.9%) were owner-occupied, and 41 (33.1%) were occupied by renters.

### Population

In 2015, the population of Tooleville was 391 (see **Table 1**).

Table 1 - Population						
	California	%	Tulare County	%	Tooleville	%
Male	18,911,519	49.68%	225,881	50.07%	222	56.8%
Female	19,155,401	50.32%	225,227	49.93%	169	43.2%
Total	38,066,920		451,108		391	

2011-2015 American Community Survey 5-Year Estimates

# TOOLEVILLE LEGACY PLAN

## Median Age

Tooleville's population is younger than the median age throughout all of Tulare County. Tooleville's median age of 35.1 is lower than the median age of the State of California (see Table 2).

Table 2 - Median Age	
Geography	Median Age (years)
California	35.8
Tulare County	30.3
Tooleville	35.1

2011-2015 American Community Survey 5-Year Estimates

## ETHNICITY AND RACE

In 2015, approximately 11.5% of Tooleville's population was White, 0.0% was African American, 0.0% was Native American, 0.0% was Asian, and 0.0% was two races or more (see Table 3). Approximately 88.5% was Hispanic (of any race). This statistic is important because persons of Hispanic origin may speak a language other than English (in this case, likely to be Spanish) and the needs of the Spanish-speaking community should be considered in order to reach out to persons on issues which may affect their community.

Table 3 - Race & Ethnicity						
	California	%	Tulare County	%	Tooleville	%
Total	38,421,464		454,033		391	
Hispanic or Latino (of any race)	14,750,686	38.4 %	283,533	62.4 %	346	88.5%
White (Not Hispanic)	14,879,258	38.7 %	139,581	30.7 %	45	11.5%
Black or African American (Not Hispanic)	2,160,795	5.7%	6,448	1.3%	0	0%
American Indian and Alaska Native (Not Hispanic)	142,191	0.4%	3,069	0.7%	0	0%
Asian (Not Hispanic)	5,192,548	13.5 %	14,546	3.2%	0	0%
Some other race (Not Hispanic)	84,477	0.2%	288	0.1%	0	0%
Two or more races (Not Hispanic)	1,072,500	2.8%	6,304	1.4%	0	0%

2011-2015 American Community Survey 5-Year Estimates

## ECONOMIC CONDITIONS

### Employment in Tooleville

According to the Tulare County 2030 General Plan Update, the County's economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Despite this, the Tulare County unemployment rate has remained consistently higher than the State average, which can be largely attributed to the seasonal nature of agricultural production (see Table 4).

# TOOLEVILLE LEGACY PLAN

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that the unemployment rate for Tooleville was about 5.2% while the rate for Tulare County was 7.2%. The unemployment rate for the State of California was 6.2%. Keeping in mind that the 5.24% includes only the employable labor force (that is, not every person of the population) results in about 19 unemployed persons of Tooleville's person labor force of 251.

Table 4 - Employment Status			
Employment Status	California	Tulare County	Tooleville
Population 16 years & over	30,312,429	325,404	364
In labor force	19,269,449	194,420	251
Civilian labor force	19,137,441	194,102	251
Employed	17,246,360	170,780	232
Unemployed	1,891,081	23,322	19
Armed Forces	132,008	318	0
Not in labor force	11,042,980	130,984	113

2011-2015 American Community Survey 5-Year Estimates

## Median Household Income

Tooleville's median household income was \$29,455, compared to \$42,031 for Tulare County and \$61,818 for the State of California (see Table 5).

Table 5 - 2011-2015 American Community Survey: Income			
Geography	Median household income (dollars)	Median family income (dollars)	Per capita income (dollars)
California	\$61,818	\$70,720	\$30,318
Tulare County	\$42,031	\$44,814	\$17,876
Tooleville CDP	\$29,455	\$27,644	\$13,599

2011-2015 American Community Survey 5-Year Estimates

## Severely Disadvantaged Community

Tooleville is a severely disadvantaged community based upon household income. As defined by the State of California Public Resources Code 75005. (g), a "[d]isadvantaged community" means a community with a median household income less than 80% of the statewide average. "Severely disadvantaged community" means a community with a median household income less than 60% of the statewide average."

# TOOLEVILLE LEGACY PLAN

## HOUSING CHARACTERISTICS

### Tenure

As the community grows, it will be important to provide new housing and commercial

opportunities, allow expansion of the size of existing housing

(for example, increase a unit by an additional bedroom), and allow various

housing types (such as mobile homes). According to the California

Department of Finance, the 2011-2015 American Community Survey data indicated 66.9% of the housing units in Tooleville were owner-occupied (**see Table 7**). Similarly, 57.2% of housing units were owner occupied in Tulare County and 54.8% of the housing units were owner occupied in the State of California.

Table 6 - Housing Tenure			
	California	Tulare County	Tooleville
Occupied housing units	12,717,801	133,570	124
Owner-occupied	6,909,176	75,685	83
Renter-occupied	5,808,625	57,885	41

2011-2015 American Community Survey 5-Year Estimates

Table 7 - Housing Tenure (%)			
	California	Tulare County	Tooleville
Owner-occupied	54.8%	57.2%	66.9%
Renter-occupied	45.2%	42.8%	33.1%

2011-2015 American Community Survey 5-Year Estimates

### Housing Conditions

Table 8 - 2015 Housing Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units
			Minor		Moderate		Substantial				
	Units	%	Units	%	Units	%	Units	%	Units	%	
Tooleville	6	18	4	12	4	12	3	9	17	50	34

Source: Tulare County 2015 Housing Element

The condition of housing is typically categorized as sound, deteriorated, or dilapidated. A “sound” housing unit (house) is one that is in good-to-excellent condition requiring no structural or other improvements. Deteriorated housing means a housing unit may need some level of maintenance/repair to improve the condition of the housing. Deterioration is classified as minor, moderate, or substantial. Dilapidated means a unit is not considered suitable for living. There may be structural, electrical, plumbing or other conditions that could endanger the well-being and safety of persons living in dilapidated housing. Housing condition is an indicator of a person’s ability to afford maintenance/repair of the house they live in. Without income to pay for maintenance/repair, it is likely that a house, as it ages, will continue to deteriorate over time.

According to the 2015 Tulare County Housing Element, a sampling of housing conditions in Tooleville indicated that approximately 18% of the housing units were sound. Approximately 32% of Tooleville's housing units were deteriorated and 50% were dilapidated (**see Table 8**).



# TOOLEVILLE LEGACY PLAN

## Age of Structures

Most of the homes in Tulare County were built between 1970 and 1979 (see Table 9). The total number of housing units in Tooleville is 124.

Table 9 - Year Structure Built			
Year Structure Built	California	Tulare County	Tooleville
Total housing units	13,845,790	144,792	124
Built 2014 or later	10,183	196	0
Built 2010 to 2013	129,453	2,114	0
Built 2000 to 2009	1,646,490	25,997	19
Built 1990 to 1999	1,495,571	21,767	0
Built 1980 to 1989	2,117,819	22,733	0
Built 1970 to 1979	2,503,688	27,111	87
Built 1960 to 1969	1,871,029	15,500	0
Built 1950 to 1959	1,907,512	13,694	18
Built 1940 to 1949	865,607	7,494	0
Built 1939 or earlier	1,298,438	8,186	0

2011-2015 American Community Survey 5-Year Estimates

## Household Size (Overcrowding)

In 2015 the average renter occupied household size in Tooleville was 4.51 persons per household (see Table 10). The average owner occupied household size is 2.48.

Table 10 - Average Household Size		
Geography	Average Household size (Owner Occupied)	Average Household size (Renter Occupied)
California	2.99	2.01
Tulare County	3.23	3.52
Tooleville	2.48	4.51

2011-2015 American Community Survey 5-Year Estimates

By definition, the most common measure of overcrowding is persons per room in a dwelling unit.<sup>1</sup> More than one person for each room of a dwelling unit is considered overcrowding. It is important to note that the measure is based on all rooms of a dwelling unit, not just the number of bedrooms. It is not uncommon for persons to share a bedroom, for example siblings or adults.

## Vacancy Rate

Vacancy rate is important as it provides an indicator of unoccupied housing units. Vacancies may occur as people move away from a housing unit and it remains vacant until the next person/family moves in. Vacancy allows persons to decide whether to choose owning or renting housing based on their need and income. Typically, a vacancy rate of about 5-6% is considered normal; however, vacancy rates above 6% can be a reflection of affordability or housing condition. In 2015, the rental vacancy rate in Tooleville was not determined however, the rental vacancy rate for Tulare County at 3.7% and the State of California at 4.1%. The homeowner vacancy rate for Tooleville also was not determined however, the homeowner vacancy rate for Tulare County at 1.7% and the State of California at 1.4%, (see Table 11).

Table 11 - Vacancy Rate		
Geography	Homeowner vacancy rate	Rental vacancy rate
California	1.4%	4.1%
Tulare County	1.7%	3.7%
Tooleville	N/A	N/A

2011-2015 American Community Survey 5-Year Estimates

<sup>1</sup> U.S. Department of Housing and Urban Development, "Measuring Overcrowding in Housing" 2007. Page 2 See: [http://www.huduser.org/publications/pdf/Measuring\\_Overcrowding\\_in\\_Hsg.pdf](http://www.huduser.org/publications/pdf/Measuring_Overcrowding_in_Hsg.pdf)

# TOOLEVILLE LEGACY PLAN

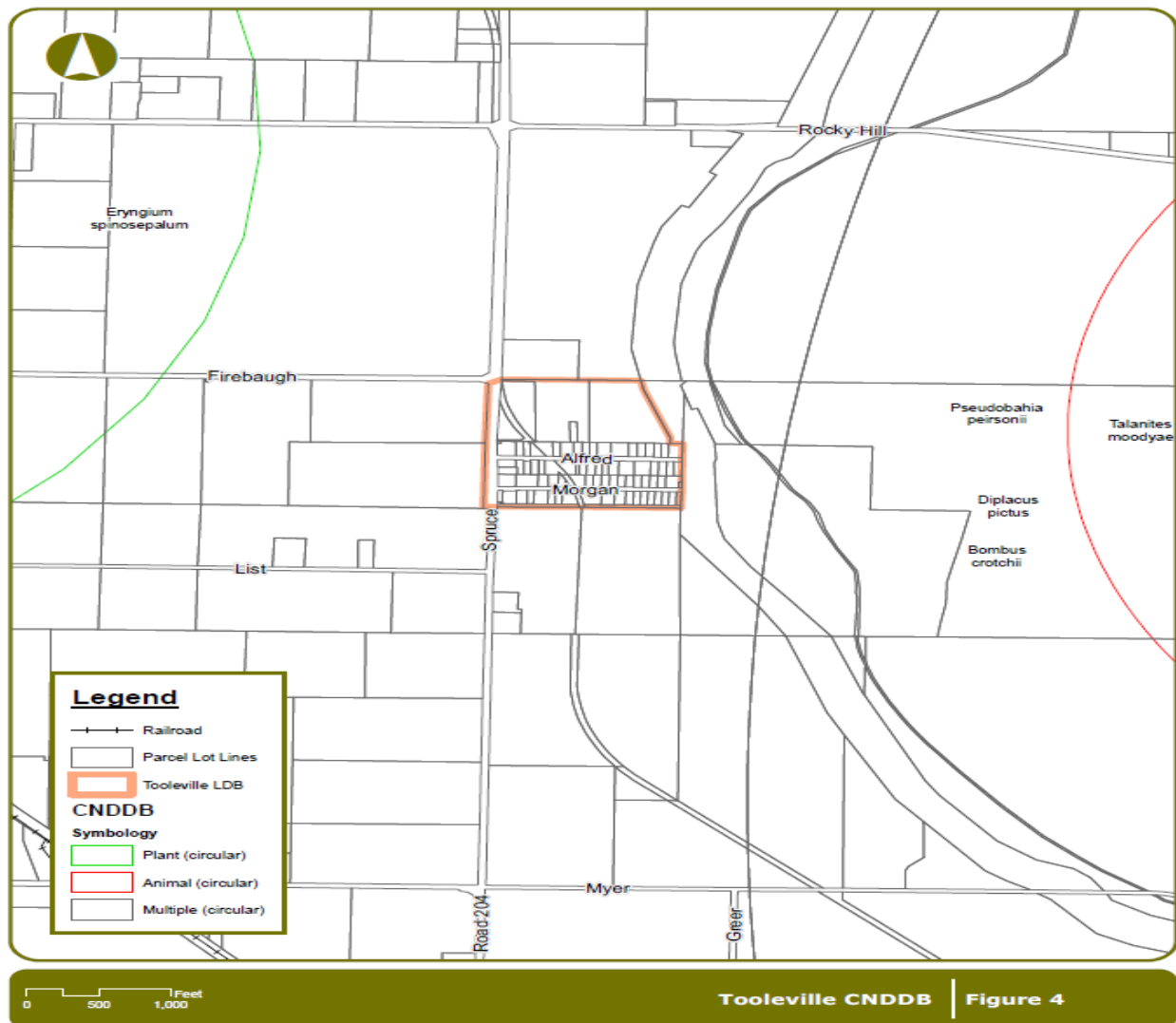
## ENVIRONMENTAL CONDITIONS

### Natural Conditions

#### Wildlife<sup>2</sup>

A California Natural Diversity Database (CNDDB) search conducted on May 24, 2017, (see Figure 4) indicates there are special status species within the Rocky Hill Quadrant Species List (which includes the Tooleville Planning Study Area) consisting of one (1) animal species and no plant species: Vernal pool fairy shrimp (*Branchinecta lynchi*, federal threatened). The planning area has been heavily disturbed with urban uses.

Figure 4 - CNDDB Map – Rocky Hill Quad



<sup>2</sup> California Department of Fish and Wildlife. Biogeographic Information & Observation System (BIOS). <https://www.wildlife.ca.gov/Data/CNDDB/Maps-and-Data>; California Department of Fish and Wildlife. California Natural Diversity Database. CNDDB Maps & Data. <https://www.wildlife.ca.gov/Data/CNDDB/Maps-and-Data> accessed November 1, 2016



# TOOLEVILLE LEGACY PLAN

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## Geology & Seismic Hazards<sup>3</sup>

“The Official Maps of Earthquake Fault Zones delineated by the California Geological Survey (CGS), State of California Department of Conservation, through December 2010, under the Alquist-Priolo Earthquake Fault Zoning Act, indicate that there are no substantial faults known to occur in Tulare County. The nearest known faults likely to affect Tooleville are the San Andreas Fault (approximately 75 miles to the west), the Owens Valley Fault (approximately 65 miles to the northeast), and the Pond Fault (approximately 25 miles southwest). According to the Five County Seismic Safety Element (FCSSE) and (Seismic/Geologic Hazards and Microzone) of the Tulare County 2030 General Plan Health and Safety Element (GPHSE), the Project area, [Tooleville] is located in the V-1 zone, characterized as a moderately thick section of marine and continental sedimentary deposits overlying the granitic basement complex. The FCSSE further states that, “Amplification of shaking that would affect low to medium-rise structures is relatively high, but the distance to either of the faults that are expected sources of the shaking [the San Andreas and Owens Valley Faults] is sufficiently great that the effects should be minimal. The requirements of Zone II of the Uniform Building Code should be adequate for normal facilities.”

## Soils

According to the United States Department of Agriculture (USDA), Natural Resources Conservation Service (NRCS), and the Soil Survey of Tulare County, the following soil types are located in Tooleville (see Figure 5). The following descriptions are provided for the above soil types:

**Exeter loam, 0 to 2 percent slopes**, is moderately deep to a duripan, moderately well drained, very slow to medium runoff, moderately slow permeability above the duripan. The soils that formed in alluvium mainly from granitic sources. Exeter soils are on alluvial fans and stream terraces. The soil is used for irrigated cropland growing oranges, olives and deciduous orchards, vineyards and row crops.

**Porterville clay, 2 to 9 percent slopes**, is moderately deep well drained soils that formed in fine textured alluvial material from basic and metabasic igneous rock. Porterville soils are on fans and foothills, used mostly for range pasture. Vegetation is annual grasses, burclover, herbs and widely spaced shrubs. Most cultivated areas are irrigated and planted to oranges, lemons, olive, figs, and some grapes.

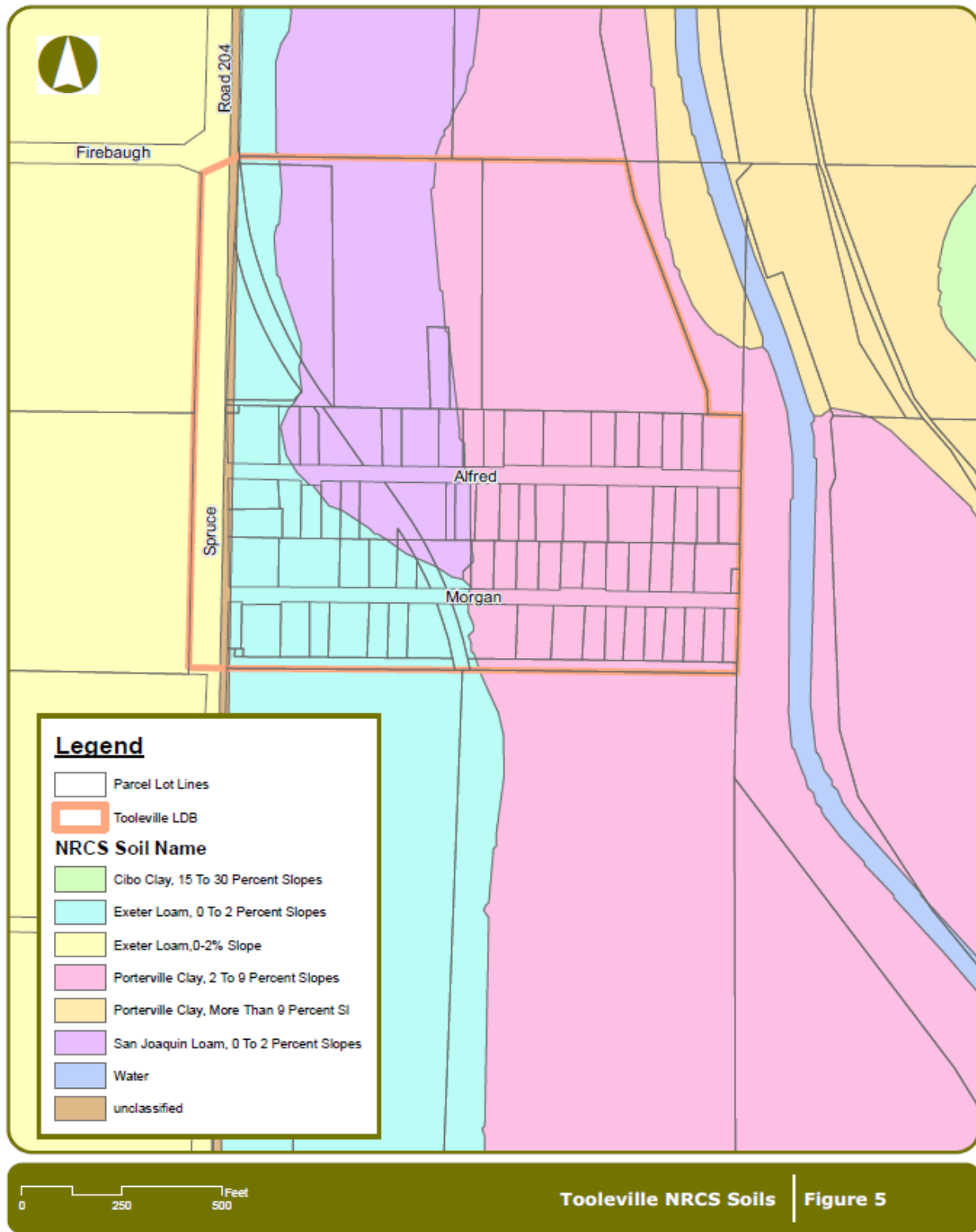
**San Joaquin loam, 0 to 2 percent slopes**, is moderately deep to a hardpan, well drained and nearly level. This soil is suitable for orchards, vineyards, and cultivated crops but is somewhat limited by the presence of hardpan which restricts root growth. This problem can be alleviated by ripping and shattering the hardpan. The soil is poorly suited to urban uses because of a high clay content, very slow permeability and a cemented hardpan. Septic tank filter fields are severely limited for these reasons.

---

<sup>3</sup> California Department of Conservation, California Geological Survey: Earthquake Shaking Potential for California, 2008. [http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48\\_revised.pdf](http://www.conservation.ca.gov/cgs/information/publications/ms/Documents/MS48_revised.pdf); California Department of Conservation, Official Maps of Earthquake Fault Zones delineated by the California Geological Survey through December 2010 under the Alquist-Priolo Earthquake Fault Zoning Act. [http://www.quake.ca.gov/gmaps/ap/ap\\_maps.htm](http://www.quake.ca.gov/gmaps/ap/ap_maps.htm)

# TOOLEVILLE LEGACY PLAN

Figure 5 – NRCS Soils Map - Tooleville



# TOOLEVILLE LEGACY PLAN

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## Physical Conditions

### Air Quality

The Tooleville Plan Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State  $O_3$  1-hour standard, non-attainment for the State  $O_3$  8-hour standard, non-attainment for the State  $PM_{10}$  standard, non-attainment for the federal and State  $PM_{2.5}$  standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The CO and NOX emissions are typically generated by motor vehicles (mobile sources). The ROG emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32) directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural

# TOOLEVILLE LEGACY PLAN

qualities of the air. Because Tooleville is located near the central portion of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants.

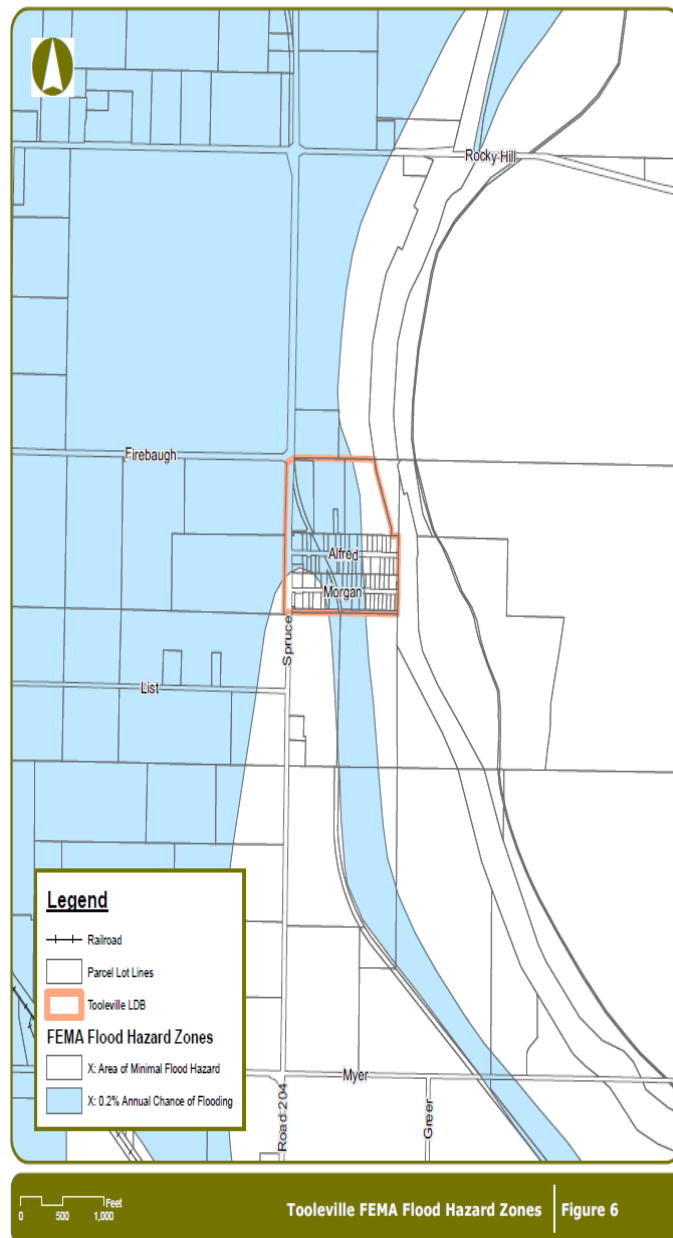
Local air pollution sources within the general vicinity of Tooleville include agricultural activities.

## Flooding<sup>4</sup>

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Number 06107C0990E (see **Figure 6**). Tooleville is within Flood Zone X, areas of 0.2% chance flood; areas of 1 % annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance of flood. Structures located in a shaded X zone are recommended to be elevated one foot above natural ground. Elevation certificates not required unless the property owner wishes to use them for insurance rating purposes; for example, a preferred risk policy.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), which means the County of Tulare agreed to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: [www.fema.gov](http://www.fema.gov). As part of the county's participation in the NFIP, individuals are eligible to obtain flood insurance.

**Figure 6 - FEMA Flood Map**



<sup>4</sup> Federal Emergency Management Agency, 2009. National Flood Insurance Rate Map, Map Number 06107C0990E, Panel Number 990 of 2550, June 16, 2009. <https://msc.fema.gov/portal/>

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Further flood information is available on Tulare County Resource Management Agency website at: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

## Noise<sup>5</sup>

Tulare County adopted a new noise element as part of the Health and Safety Chapter of the 2030 General Plan Update. The noise contours were prepared in terms of day-night average decibel level (Ldn), which are descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. Roadways and traffic noise are the dominant source of ambient noise in the County. According to summarized daily traffic volumes in the General Plan Background Report, the predicted Ldn noise level at the distance from the Road 204 centerline to the 60 dB-Ldn contour between SR 198 and SR 137 is 187 feet. The distance from the Road 204 centerline to the 65 dB-Ldn contour between SR 198 and SR 137 is 87 feet. As a result, the parcels generally located adjacent to Road 204 in Tooleville are exposed to 65 dB-Ldn noise levels. New construction proposed in the 65 dB-Ldn noise contour is conditionally acceptable after a detailed analysis of the noise reduction requirements is made and needed noise insulation features are included in the design.

## INFRASTRUCTURE

Infrastructure is defined as “the basic physical and organizational structures needed for the operation of a society or enterprise or the services and facilities.” In regards to Tooleville, this Legacy Plan is intended to address deficiencies and the need for improvements to the drinking water system improvements, (for example wells, water distribution piping, and storage tanks), curbs, gutters, streets, sidewalks, etc.). The prioritized concerns of the Community include:

- Water Supply
- Storm Water Drainage
- Street lights
- Stop Signs
- Internet
- Police Patrol
- Clinic

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<sup>5</sup> Tulare County General Plan 2030 Update, August 2012; United States Department of Transportation, Federal Transit Administration. Transit Noise and Vibration Impact Assessment, FTA-VA-90-1003-06; May, 2006.

[https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA\\_Noise\\_and\\_Vibration\\_Manual.pdf](https://www.transit.dot.gov/sites/fta.dot.gov/files/docs/FTA_Noise_and_Vibration_Manual.pdf); United States Department of Transportation, Federal Transit Administration. Construction Noise Handbook, 2006.

[http://www.fhwa.dot.gov/environment/noise/construction\\_noise/handbook/handbook09.cfm](http://www.fhwa.dot.gov/environment/noise/construction_noise/handbook/handbook09.cfm) and [http://www.fhwa.dot.gov/environment/noise/construction\\_noise/special\\_report/hcn04.cfm#sou](http://www.fhwa.dot.gov/environment/noise/construction_noise/special_report/hcn04.cfm#sou)

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## Water and Sewer Supply

The Mutual Water System, *The Tooleville Mutual Nonprofit Water Association, Inc.* is governed by a 5-member Board of Directors. Like many mutual. The board pays a monthly stipend to directors who are present at each month's meeting. The stipend is \$40. The board currently meets every other month. There are a couple of families, the Enloes and the Baileys, who have lived in Tooleville since time immemorial and have long-term knowledge of the water system. Both families remain involved in running the water system, or at least staying in touch with what is happening. This is a valuable resource. Again, like many Mutuals, it is rare to get a majority of shareholders at annual meetings, so the bylaws were amended to require only a 20% presence to achieve a quorum. The water system has two professional support personnel: a bookkeeper who is actually an employee of the company, and a D3 operator who is a contractor. The operator calls out a well or pipe repair company as needed for major repairs.

**Table 12 - Existing Infrastructure**

### Existing Infrastructure in DUCs

Disadvantaged Unincorporated Community (DUC)	Public Sewer Available	Public Water Available	Public Stormwater Available	Existing Streetlights	Existing Sidewalks	Existing ADA Curb Ramps
Tooleville, CDP	Yes	Yes	Yes	Yes	No	No

The board makes the decisions at its semi-monthly meetings. The bookkeeper receives the mail, so she makes sure that state filings happen, that taxes are filed, bills paid and revenue deposited, etc. The operator visits weekly or as needed to check the system, flush lines, pull samples, etc. Tulare County takes bacteriological samples. As a Mutual, the system is not regulated by the PUC.

Since the Tooleville Mutual Nonprofit Water Association, Inc. has less than 200 connections, the system is monitored by the Tulare County Health & Human Services Agency, Tulare County Public Health Environmental Health Division.

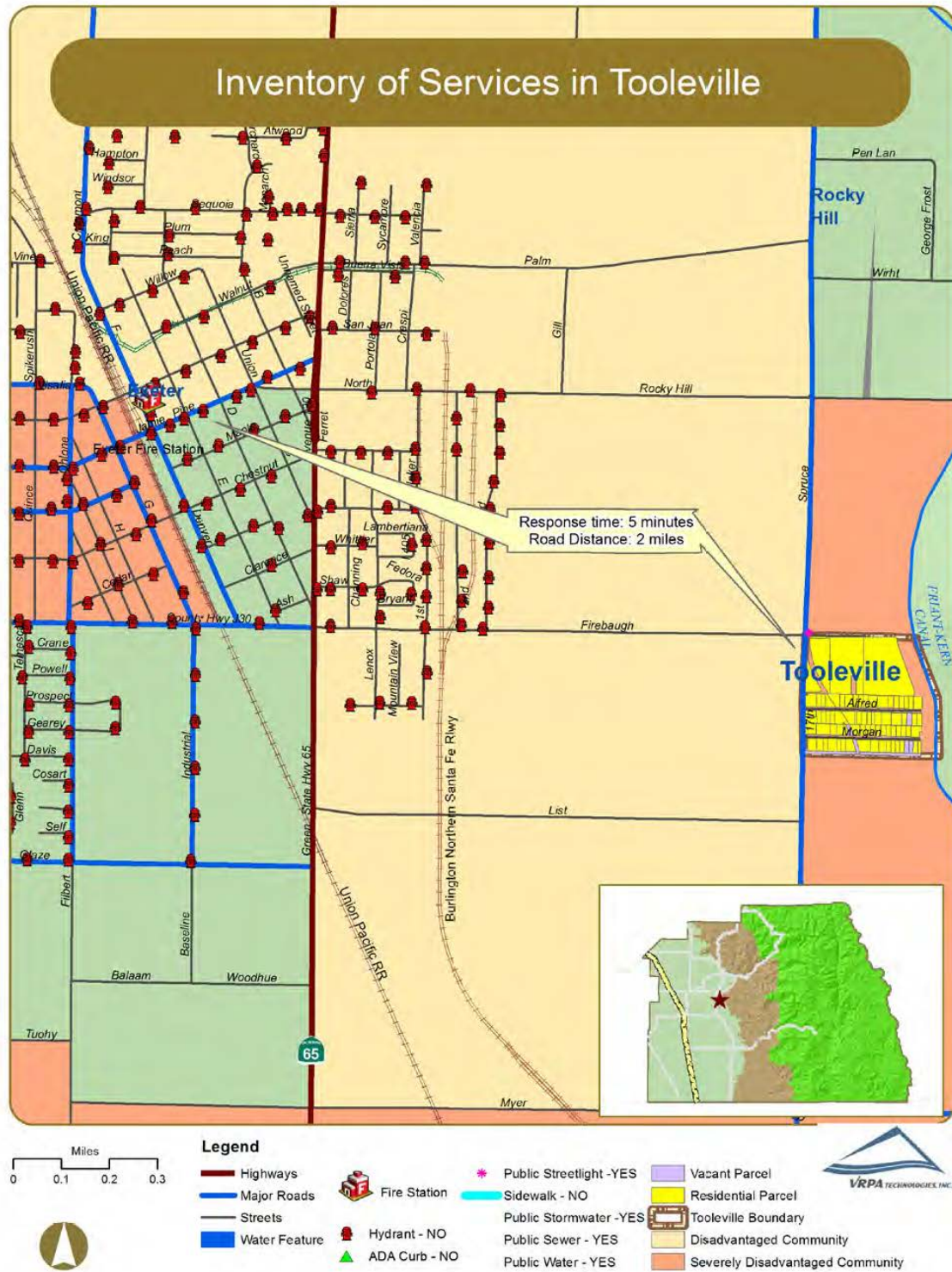
They have successfully applied for CDBG and USDA funding that was used to drill a water test well and replace the water distribution system. The Tooleville Mutual Nonprofit Water Association, Inc. has also successfully applied for and received a commitment for a Proposition 84 planning grant from CDPH to design a source of water by drilling a new well on the west side of Exeter (where higher-quality water is known to exist), wheeling that water through that city to a connection point where a transmission line would transport water to the Tooleville water distribution system.

The Tulare County Service Area #1 Tooleville Zone of benefit provides sewer service to the community. The Tooleville Mutual Nonprofit Water Association, Inc. has 76 connections servicing 77 residences. The system has two water wells that supply the community. The Alfred Avenue well was drilled to a depth of 310 feet with an annular seal to a depth of 90 feet. This is an open bottom well with no perforations and no gravel pack. It is equipped with a 7.5 hp submersible pump and 3,500 gallon hydro pneumatic tank. The Morgan Avenue well was drilled to a depth of 320 feet with an annular seal to a depth of 99 feet. This is an open bottom well with no perforations and no gravel pack. It is equipped with a 7.5 hp submersible pump and 1,200 gallon hydro pneumatic tank.



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Figure 7 - Inventory of Services - Tooleville



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Interestingly, for about the past three years, the nitrate levels in both wells have dropped below 45 ppm. Water pumped from the wells has intermittently exceeded the nitrate Maximum Contaminant Level set by EPA and CDPH. The chronic on again and off again problem with Tooleville's water quality has been the nitrate levels of water produced from the community's two wells. Attached is a table listing nitrate levels from both wells from 1978 through October 2012. This table shows that both wells have produced water exceeding the nitrate MCL 9 times over this period (**see Table 13**).

The distribution system was replaced entirely in 2009 with a USDA grant. Unfortunately, total coliform bacteria has plagued the system since that time. It is unknown whether the system was not sufficiently flushed at installation, or whether there is a cross-contamination problem (at least two old wells are known to exist and the water company has been unable to prove that they are connected to household plumbing that could be creating a cross-contamination situation). Routine flushing at hydrants on a monthly basis seems to be keeping the problem in check, but on average there are about two bacteriological violations per year.

The Tooleville Mutual Nonprofit Water Association, Inc. water system has had sporadic nitrate problems and uncertainty since the late 1990's. Low system pressure was frequently a problem until the distribution system was replaced, and that seems to have helped.

Consolidation with the City of Exeter could be a good way to resolve the water quality problems of residents served by the Tooleville Mutual Nonprofit Water Association, Inc. Unfortunately, however, both communities are more or less opposed to full consolidation. Exeter maintains that it does not have the resources to own or manage Tooleville's system. It also remains concerned about the charter city prevailing wage exemption that Exeter fears could be lost to them if they serve an outlying community. This problem was theoretically resolved by SB2X9 in 2009 but it has not yet been tested in court, so Exeter prefers to err on the side of caution in this case. For its part, Tooleville's board of directors is not interested in being a charity case, and would prefer to continue owning and operating their own system. A partial consolidation has also been considered (buying water in bulk through a master meter) but is not currently favored, due to risk on the part of the water company (which would have to cover every month's bill, regardless of collection problems or other revenue issues) and Exeter's unwillingness to help with billing and maintenance. As a result, the only option for a long term water source (that would not intermittently be contaminated with nitrate levels over the MCL) appears to be the drilling of a water well west of Exeter and wheeling potable water through the city's distribution system and then on to Tooleville.



# TOOLEVILLE LEGACY PLAN

**Table 13 - Tooleville Water System**

<b>Tooleville Water System</b> <b>Nitrate Levels in Active Wells</b> Nitrate MCL = 45 ppm		
Date	Alfred Well (ppm)	Morgan Well (ppm)
8/3/1978	43.0	39.0
6/28/1982	43.0	44.0
7/27/1982		40.0
8/26/1982	40.0	42.0
10/17/1984		5.5*
9/30/1988	28.0	40.0
2/27/1992	42.0	40.0
1/5/1995	42.0	42.0
1/3/1997		70.0
2/13/1997		85.0
6/2/1997		87.0
2/25/1998		85.0
2/15/2000	45	
4/19/2000	47.9	
8/30/2001	54	71.7
9/17/2001	43	
2/14/2002	45	
5/9/2002		67.1
7/23/2003	55.4	
1/20/2005		46.2
5/24/2006	46.2	43.1
7/27/2006	68	47
9/28/2006	55.7	
11/29/2006	34.1	65
1/25/2007	40.3	
4/26/2007	51.9	
5/23/2007		41.7
12/26/2008	40.5	30.2
6/5/2009	47	42
6/23/2010	18	39
9/28/2010	40	42
11/22/2010	38	37
2/25/2011	39	
6/24/2011	45	37
9/2/2011	42	41
10/7/2011	38	41
2/9/2012	40	38
5/10/2012	44	39
7/13/2012	45	38
10/23/2012	39	42
<b>Times Exceeding MCL</b>	<b>9</b>	<b>9</b>

\* Questionable Test result

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## Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.
- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.

Tooleville does not currently have a storm drainage system.

## Solid Waste

Solid waste disposal services for the Community of Tooleville is provided by USA Waste, a private company. Solid waste generated in Tooleville can be disposed of at Road 80 Landfill, located at 8614 Avenue 328, Visalia, California.

## Roads

Road conditions are of the highest concern to the residents of Tooleville. It is reported that road conditions are poor and most roads need work, roads have potholes, cracks and bumps. This makes residents concerned with both driving conditions and walkability. Road conditions are described as poor and most of the roads in town need work. Tooleville roads are not regularly maintained and the few times that roads have been serviced, the repair work was poor because the roads were patched and not repaired.

The following roads are a priority to the residents:

- Alfred
- Morgan

There are several roadways in Tooleville that are in need of repair. Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.

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If the road is still structurally sound, a bituminous surface treatment, such as a chip seal or surface dressing can prolong the life of the road at low cost. Such repairs are considered medium if the maintenance strategy consists of:

- ✓ Chip seal - surface treatment in which the pavement is sprayed with asphalt and then immediately covered with aggregate and rolled. Chip seals are used primarily to seal the surface of a pavement with cracks not associated with heavy loads.

Some roadways require more extensive repairs such as resurfacing, grinding, remix and or reconstruction. These repairs are considered major if the maintenance strategy consists of:

- ✓ Grind and remix - process by which construction materials are recycled and reused to add structure to roadways.
- ✓ Overlay resurfacing operation - consists of grinding off selected areas of old asphalt, patching any potholes, placing a fabric (in some cases), placing and compacting hot mix asphalt pavement, and adjusting any street hardware.
- ✓ Asphalt reconstruction - consists of excavating the entire roadway, placing and compacting rock beneath the roadway, and placing and compacting hot mix asphalt.
- ✓ Cold mix reconstruction - similar to asphalt reconstruction except cold mix asphalt is used. It is commonly used as patching material and on lower volume service roads

**Table 14** lists the roadways, the limits, and type of maintenance, and the date the roadways last repaired.

Table 14 - Road Maintenance Strategies				
No.	Roadway	Limits	Repair Code	Date Repaired
1	Alfred Avenue	Spruce Road - End	CHIP	2011
2	Morgan	Spruce Road - End	CHIP	2011

OLAY = overlay resurfacing operation

CHIP = chip seal

GRX = grind and remix

ACST = asphalt reconstruction

RCST = cold mix reconstruction

## Street Lights

The County typically provides street lighting at major road intersections in the communities but does not provide mid-block lighting as is typical within cities. Tooleville has street lighting at one (1) location. Overhead utility poles are prevalent in Tooleville. These utility poles provide electricity and telephone service to residences and businesses in Tooleville and are located within the County right-of-way (see **Table 15**).

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**Table 15 - Streetlight Site Inventory - Tooleville**

Streetlight Site Inventory					
Streetlight Locations					
Community	Main Road	Crossroad	Side	Corner	Location Detail
Tooleville	E Firebaugh Ave (Ave 276)	Spruce Ave (14th Ave E)		N/E	

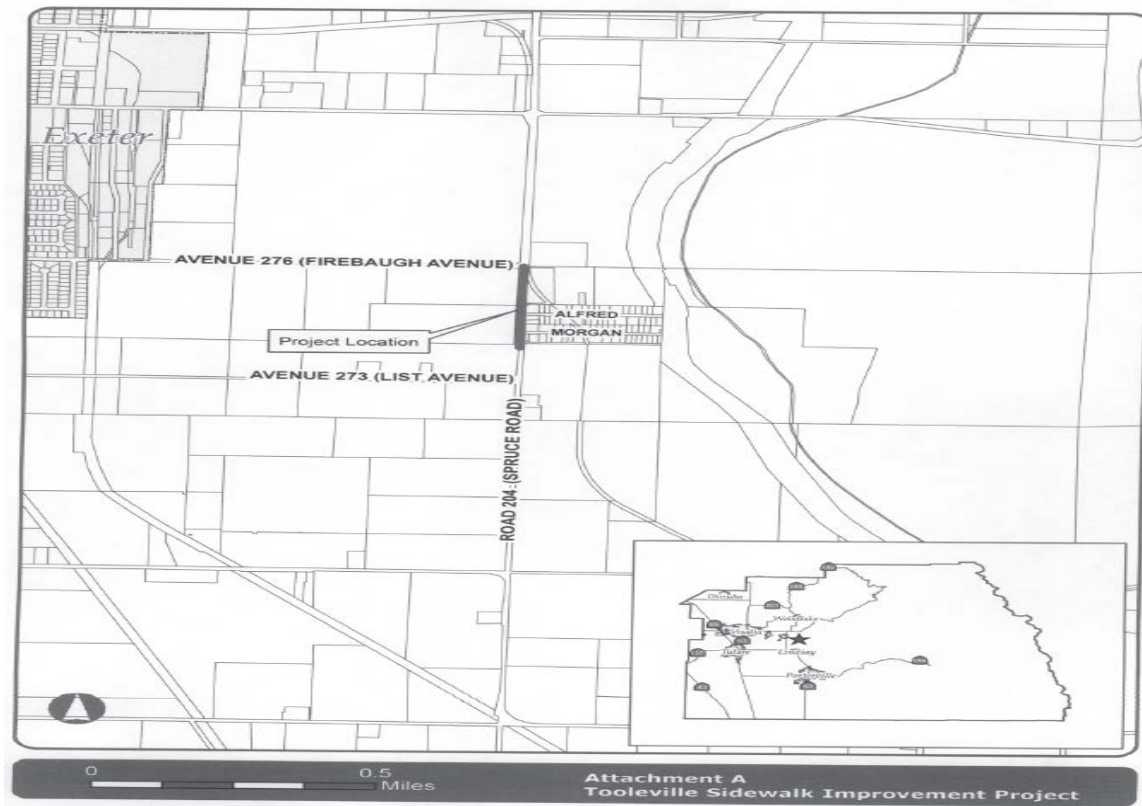
## Sidewalks

There are currently no sidewalks located within the interior streets in Tooleville. A sidewalk project was completed in 2016 along the frontage of Road 204 in Tooleville.

## Tooleville Sidewalk Improvement Project (2016)

The project consisted of construction of approximately 765 linear feet of curb, gutter, sidewalk, and associated drainage improvements along Road 204 (Spruce Road) from Morgan Avenue to Alfred (see **Figure 8**). The project included the construction of a bus pullout that accommodates two school busses. All improvements were constructed to meet current Americans with disability Act (ADA) requirements. The Construction of the Tooleville Sidewalk Improvement Project was completed in August of 2016.

**Figure 8 - Sidewalk Improvement Project**



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## ADA Curbs

“The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

## Transit and Bus Stops

Recognizing the need to provide transportation services to those who are disadvantaged, the County of Tulare provides public transportation services to the elderly, handicapped, low-income, and residents without access to transportation. Public transit is only available in Tooleville through the Dial-A-Ride Service. Public transit is only available in Tooleville through the Dial-A-Ride Service.

The Tulare County Transit Agency (TCAT) operates fixed-route services that link communities with each other and with Visalia and Tulare’s urban transit systems. Tulare County Transit Agency (TCAT) operates Dial-A-Ride Service Monday through Friday. Reservations must be placed the day before planned trip for a curb to curb trip on Dial-A-Ride. (see TCAT website at: <http://www.tularecounty.ca.gov/rma/index.cfm/public-works/tulare-county-area-transit-tcat/>).

TCAT vehicles are wheelchair accessible and all full size buses include bike racks. As such, public transit is likely to remain a limited option due to fiscal constraints and the high cost of providing services to a community of less than one thousand residents. The low level of auto congestion in Tooleville, now and as forecasted into the future, suggests that driving will continue to be more convenient in rural communities than the use of transit for those with access to a private car.

## AMTRAK

The Hanford AMTRAK station, located approximately 35 miles to the west in Kings County, is the closest station to Tooleville providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway) bus service.

## Aviation

The nearest operational general aviation airport is Visalia Municipal Airport, approximately 25 miles west, offers passenger service to Los Angeles. A community general aviation airport is located in Exeter (formerly known as Thunderhawk Field), approximately 5 miles southwest of Tooleville.

Fresno Yosemite International Airport (FAT), approximately 70 miles northwest of Tooleville, is the principal passenger and airfreight airport in the central San Joaquin Valley. Meadows Field, Bakersfield’s principal commercial airport, is approximately 80 miles southwest of Tooleville and offers direct flights to several destinations.

## Energy Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural

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gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained fairly constant. As a result, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom SCE has arrangements, and rolling blackouts.

## Internet Access

Most families in Tooleville do not have internet access at their homes. Any available internet service is unreliable and expensive. Residents are told by internet providers that there is not a tower in their area and that is why internet service is either unreliable or very expensive compared to what it costs in nearby cities. This is a major problem to residents, without access to the internet, they are unable to do very basic things such as online job hunting, applying for resources, and providing homework help to their kids. Internet access ranked as a high priority improvement need in Tooleville.

## PUBLIC SERVICES

### Sheriff

The Tulare County Sheriff Department (TCSD) Main Office located at 2404 W. Burrel Avenue, in Visalia, approximately 20 miles west of Tooleville is responsible for providing police protection and for patrolling the community of Tooleville. Residents report that the TCSD response time is long and that there is too little Sheriff Patrol within the community. Residents are worried about the rate of unreported crimes and crimes that go un-responded to. People feel that the limited presence of law enforcement contributes to the local crime rate and makes criminals feel unstoppable in that area of Tooleville. Tooleville residents are asking for increased Sheriff Presence in their community.

### Fire

Tulare County and all of the incorporated cities have a mutual-aid agreement for fire protection services. The proximity of the nearest City or County fire station varies significantly between the unincorporated communities. There is one (1) County fire station in the Exeter area. County Fire Station #11 is located at 137 North “F” Street, is the closest station to Tooleville (two miles) and is shown in Figure 33-6.

There are currently no Fire Hydrants in Tooleville.

### Schools

Schools are an especially important component of a community. The quality of residential development is often measured by whether schools exist in close proximity to the neighborhood. As a result, schools are often a gravitational factor in attracting residential development to certain areas of a community. The Tooleville Area is within the Exeter Unified School District located approximately five (5) miles to the northwest.

### Library

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“The Tulare County Public Library System is comprised of interdependent branches, grouped by services, geography and usage patterns to provide efficient and economical services to the residents of the county. At present, there are 14 regional libraries and one main branch.”<sup>6</sup> The Exeter Branch Library is located in Exeter approximately five (5) miles to the northwest (see Table 16).

Table 16 - Library Location & Hours		
Branch	Address	Service Hours (2017)
Exeter	Exeter Branch Library 230 East Chestnut Exeter, CA 93221-1712	Tuesday – Wednesday: 11:00 am – 5:00 pm 6:00 pm – 8:00 pm Thursday - Friday: 9:00 am – 1:00 pm 2:00 pm – 6:00 pm

Library hours current as of May 2017

## Parks

There are no County owned/operated public parks in Tooleville. In the community of Tooleville there is no available open green space area for children to play and adults to be physically active. The nearest park is located approximately four (4) miles away in the city of Exeter, survey results show that in Tooleville, children play in the street or do not play outside at all. Tooleville residents are concerned about the health and safety of their children, they are concerned that the safety of their kids is at risk when they play on the street and for the health of their children from lack of physical activity that comes from not having a safe place to be physically active.

## CIRCULATION AND TRAFFIC

### Circulation

For a land use plan to operate effectively, it is imperative that a comprehensive circulation plan be prepared. Existing roadways must have the capacity to expand as traffic counts increase, and new roads must be planned so that one part of the community can be connected to another.

Tulare County’s relationship to the State Route system, nearby counties, cities, and communities

“The purpose of the highway, streets and roads section is to identify the existing regional circulation system and determine both feasible short-term and long-range improvements. Tulare County's planned circulation system consists of an extensive network of regional streets and roads, local streets and State Highways. The system is designed to provide an adequate [Level of Service] LOS that satisfies the transportation needs of County residents. However, Tulare County has experienced a large increase in population and is beginning to outgrow portions of the circulation system. The need for major improvements to the State Highways, streets and roads network is an important issue.

The existing State Highway system was completed in the 1950's and 60's. The average design life of a State Highway is approximately 20 years and many Tulare County's highways were constructed 50 years ago. The Agricultural and commercial industry continue to utilize the circulation system to get products to market. With industry intensification and other development, many facilities are beginning

<sup>6</sup> General Plan Background Report, page 7-96



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to show structural fatigue (e.g., surface cracks, potholes, and broken pavement).”<sup>7</sup>

## Traffic

“Tulare County is linked to Fresno County and Kern County principally by State Route 99. This route provides the only continuous north-south route through the County and is heavily used for regional travel. The entire length of State Route 99 in Tulare County and State Route 198 through Visalia and a portion of State Route 65 in Porterville are constructed to freeway standards.”<sup>8</sup>

**Freeways** provide for the ability to carry large traffic volumes at high speeds for long distances. Access points are fully controlled. Freeways connect points within the County and link the County to other parts of the State.

**Arterials** provide for mobility within the County and its cities, carrying through traffic on continuous routes and joining major traffic generators, freeways, and other arterials. Access to abutting private property and intersecting local streets shall generally be restricted.

**Collectors** provide for internal traffic movement within communities, and connect local roads to arterials. Direct access to abutting private property shall generally be permitted.

**Local Roads** provide direct access to abutting property and connect with other local roads, collectors, and arterials. Local roads are typically developed as two-lane undivided roadways. Access to abutting private property and intersecting streets shall be permitted.

## Existing Circulation and Transportation Conditions

Tooleville is a small agricultural community located adjacent to Road 204 (Spruce). The community of Tooleville is comprised of two roads, Morgan Avenue and Alfred Avenue. The major rural collector road is Avenue 204. There are no proposed major streets as part of this Legacy Plan.

## Patterns of Blocks and Streets

The prominent grid pattern of County roads provides efficient and direct collector routes that provide ease of travel from place to place.

## Complete Streets

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” The Complete Street roadway typically includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

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<sup>7</sup> 2014-2040 Regional Transportation Plan & Sustainable Communities Strategy, Tulare County Association of Governments (TCAG), June 2014. Page 3-54.

<sup>8</sup> Tulare County General Plan Background Report, page 5-7.



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## GOALS, OBJECTIVES AND POLICIES

This Section of the Tooleville Legacy Plan prescribes the land use policy framework which will govern the development of the community through the year 2030. It includes text, which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place. The land uses in all the Legacy Boundaries are proposed to reflect a “mixed use land use designation.” The following land use designations and descriptions, with densities and intensities are recommended for Tooleville to address land demand needs through the 2030 planning horizon year.

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives. The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan, Tooleville Legacy Plan, and input received from Tooleville citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

It is important to recognize that County policies reflect the unique attributes of the various locations and geographic areas in the County. As such, there are policies applicable to one area of the County that are not applicable to others based on natural setting, topography, habitat, existing development, or other attributes which are unique within the planning context of the County. The following planning goals, objectives, policies and standards reflect, as applicable, the rural, suburban, or urban contexts of the Legacy planning areas designated by the County.

### **Community Development**

#### **GOAL I: Foster a cohesive community with easy access to necessary services and support facilities**

Objective: Prevent premature urban-type development on agriculturally productive lands.

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.

#### **GOAL II: Avoid land use conflicts through planning separation of uses.**

Objective: Promote concentrations of similar or compatible uses.

Policies:

1. Establish areas zoned exclusively for commerce and residences consistent with the policies in this plan.
2. Phase-out existing nonconforming concerns within planned residential areas through appropriate zoning amortization procedures.
3. Locate high density residential uses in close proximity to planned commercial areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.

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5. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
6. The County shall work with the Schools to provide safe routes to school.
7. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Tooleville Legacy Plan.
8. The Tooleville Legacy Plan should be reviewed every five years to determine if amendments are appropriate.
9. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

Policies:

1. Require adequate setbacks, side and rear yards, landscaping and screening between living and working areas.
2. Utilize roadways, railroad right of ways and other physical features to separate planned living and working areas.

## Housing

**GOAL I: Provide safer and adequate housing for all citizens within the community.**

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of

vacant substandard housing units.

2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.
4. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to initiate home maintenance/repair programs in Tooleville.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Tooleville.
3. Coordinate residential zoning with availability of utilities and community services.

4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.
5. Encourage housing advocacy groups, such as Habitat for Humanity and Self-Help Enterprises, to construct new housing.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

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1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.
3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

## **Economic Base**

### **GOAL I: Develop a strong and diversified economy.**

Objective: Provide sufficient land for commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

#### **Policies:**

1. Promote a concentration of commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new commercial development.

#### **Policies:**

1. Encourage the Tooleville to give priority to community service development in the areas reserved for commercial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial areas.

Objective: Provide the necessary safe guards to attract quality commercial development to the community.

#### **Policies:**

1. Assure that commercial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of commercial areas.

## **Environmental Quality and Public Safety**

### **GOAL I: Preserve and enhance the quality of life for present and future generation of Tooleville citizens.**

Objective: Upgrade the level of community health, sanitation and safety.

#### **Policies:**

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

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Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Tooleville.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.
3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Tooleville.

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## ECONOMIC DEVELOPMENT

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants. In rural areas, elimination of all barriers to economic development is the foundation for growth. This Plan addresses the following four potential barriers to Economic Development.

**Infrastructure** In order for more development to occur, service levels for water to be expanded. Grant funding is needed to increase service levels.

**Use Permits** There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

**Education** Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California- Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

**Health Care** The nearest hospital is in Visalia, approximately 13 miles away, and it takes approximately 20 minutes for the nearest ambulance to get to Tooleville from Visalia.

## LEGACY DEVELOPMENT BOUNDARY

The initial Legacy Development Boundary planning area contained approximately 21.3 acres (**see Table 17**). At the request of the community, an area to the north of the existing developed community was added to the initial planning area of the project. The northern addition of approximately 17.4 acres increases Tooleville's proposed Legacy Boundary to approximately 38.7 acres.

<b>Table 17 - Proposed Legacy Development Boundary</b>	
Proposed LDB	Acres
Proposed Tooleville Legacy Development Boundary	21.3
LDB Area Added in the North (As Requested by the Community )	17.4
Proposed Tooleville LDB	38.7

# TOOLEVILLE LEGACY PLAN

## LAND USE CHANGES

### General Plan Land Use and Zoning District Updates

As part of this Legacy Plan for Tooleville, there are a number of land use changes. These changes are described below:

As suggested earlier, and based on the forecasted growth and the recommended Legacy Development Boundary in the Opportunities and Constraints Analysis, the Land Use Plan map (see **Table 18**) and Zoning Plan (see **Table 19**) have been updated.

<b>Table 19 - Proposed Land Use</b>	
Proposed Land Use	Acres
Mixed Use	33.6
Right-of Ways	5.1
Tooleville LDB	38.7

<b>Table 18 - Proposed Zoning</b>	
Proposed Zones	Acres
C-2-MU	5.4
R-A	28.2
Right-of Ways	5.1
Tooleville LDB	38.7

## IMPLEMENTATION STRATEGY

The purpose of this section is to prescribe a proposed approach to implement the General Plan recommendations contained in the Tooleville Legacy Plan. The following components comprise the Tooleville Community Plan implementation strategy:

**Limitation of the A-1 Zoning District.** Zoning Code Changes (changes to Use Permit issuance, eliminating the SR Combining Zone, Mixed-Use Overlay District, Zoning Map Update), Complete Streets/Safe Routes to School

**Zoning District Changes** As part of this Implementation Program for the Tooleville Legacy Plan, there are a variety of changes to existing zoning districts. These changes are described below.

**Chapter 16 of the Zoning Code** Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit. As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

**Mixed Use Overlay District** This alternative involves the creation of a Mixed Use Zoning Designation for the Community of Tooleville.

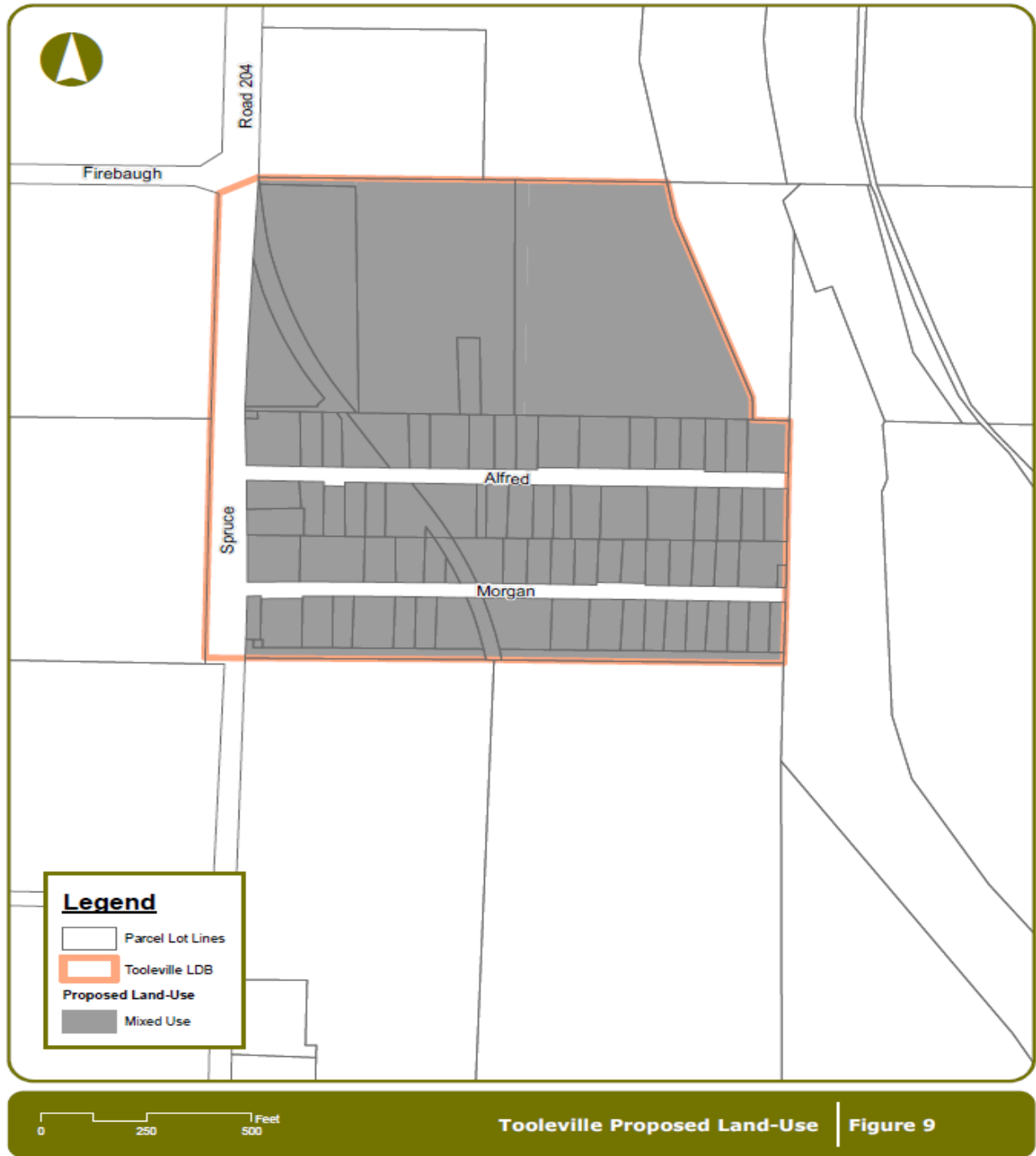
# TOOLEVILLE LEGACY PLAN

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**Zoning Map Update** The current Zoning Map (**see Figure 10**) for Tooleville will be amended to be compatible with the Land Use Map outlined in the General Plan (**see Figure 9**). There are a couple of zoning district changes that are proposed to allow the General Plan and Zoning Ordinance to be in conformity with each other (**see Figure 11**) Tooleville Proposed Zoning).

# TOOLEVILLE LEGACY PLAN

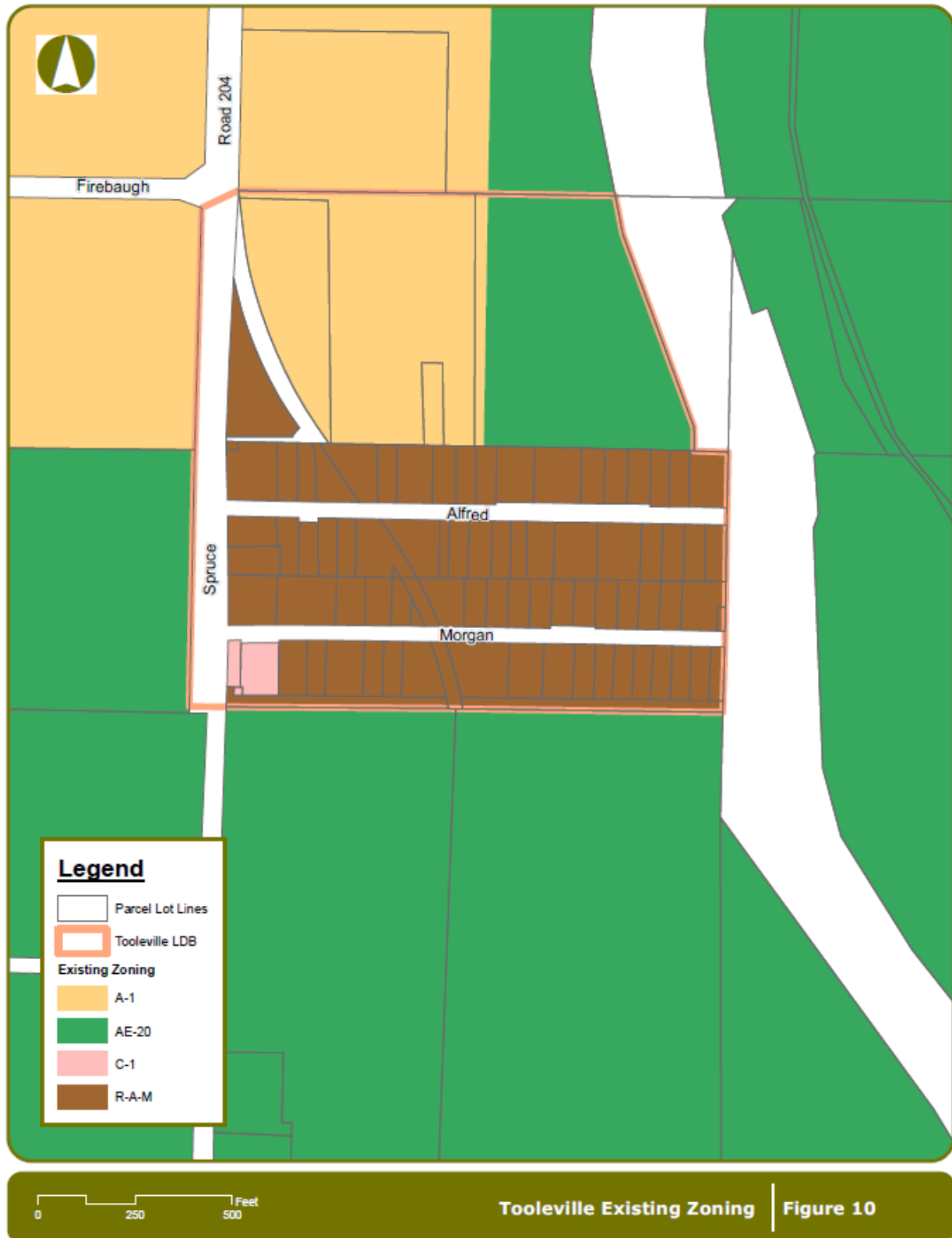
Figure 9 - Proposed Land Use Plan





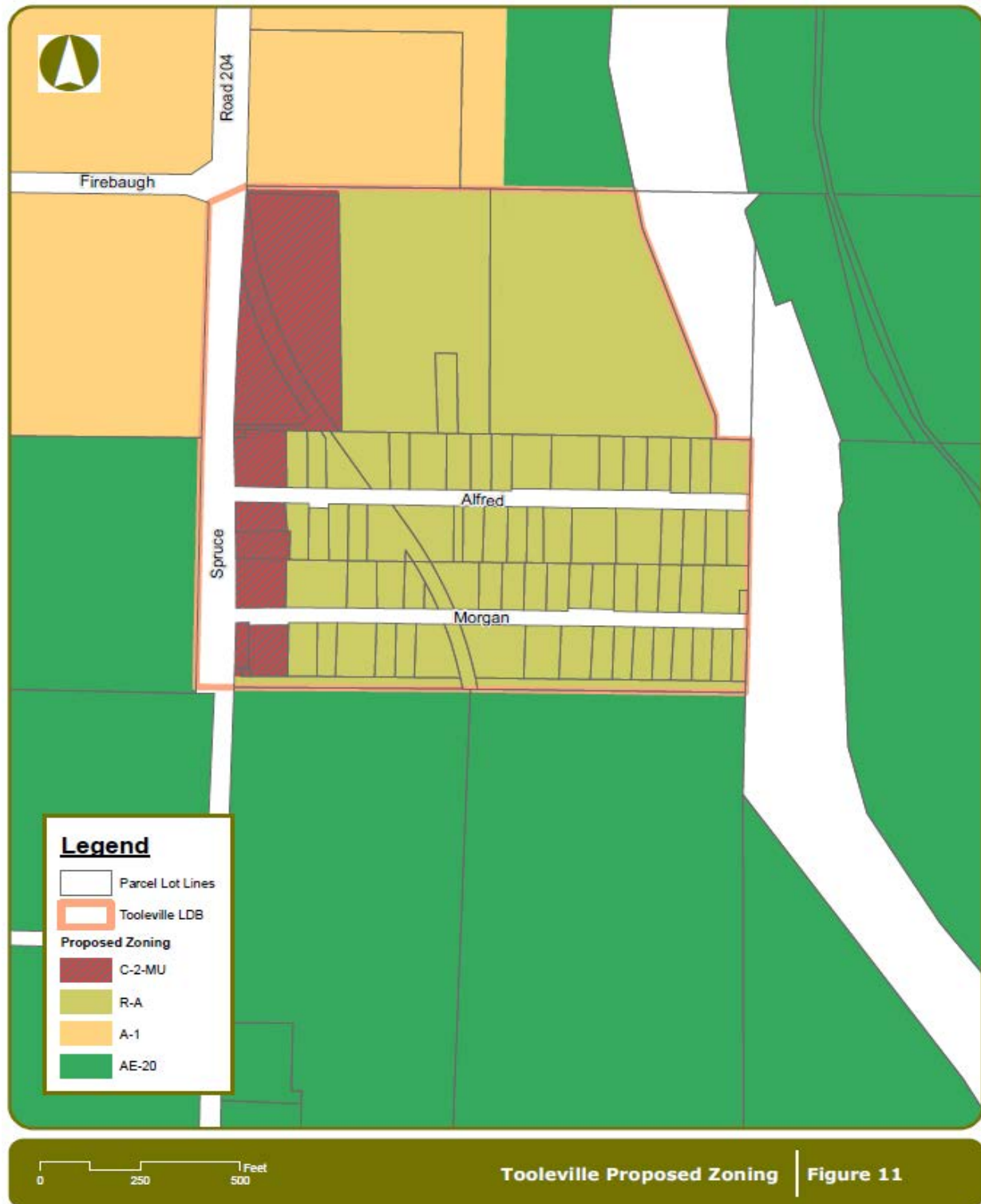
# TOOLEVILLE LEGACY PLAN

Figure 10 - Existing Zoning Districts



# TOOLEVILLE LEGACY PLAN

Figure 11 - Proposed Zoning Districts



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## **ATTACHMENTS**

A-1 – Use Permit Requirement Changes (Zone Change Text)

A-2 – Mixed Use Overlay District (Zone Change Text)

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## A-1 Use Permit Requirement Changes (Zone Change Text)

### Additional By-Right Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1, C-2, C-3, M-1, R-2, R-3	C-1, C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	C-2, C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bakery goods store.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Banks and financial institutions.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Barber shop or beauty parlor.	C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1, C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1, C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Business, professional and trade schools and colleges. Structure <	CO, C-1, C-2, C-3, M-1	C-3, M-1

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10,000 sq. ft.		
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1, C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1, C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1, C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1, C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, RA	C-1, C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1

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Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1 ,C-2, M-1, R-2, R-3, RA	C-2, C-3, M-1
Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1 ,C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	

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Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3,*C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1
Mobilehome for use by caretaker or night watchman.	CO, C-1, C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	R-3, C-1, C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, RA, R-3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1, C-2, M-1, R-1, R-2, R-3, PO	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O, CO, C-1, C-2, C-3, M-1, M-2	
Post Office.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1, C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1



# TOOLEVILLE LEGACY PLAN

Public library.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1, C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1, C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	
Public utility structure.	CO, C-1, C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1, C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1, C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1, C-2, C-3, M-1	
School, private.	CO, C-1, C-2, C-3, M-1, PO	
School, public.	CO, C-1, C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone

# TOOLEVILLE LEGACY PLAN

Shoe store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1, C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1

# TOOLEVILLE LEGACY PLAN

## A-2 Section 18 Mixed Use Overlay District (Zone Change Text)

The following regulations shall apply in the community of Tooleville, unless otherwise provided in this Ordinance.

### A. PURPOSE

The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

### B. APPLICATION

This overlay zone only applies to the communities of Ducor, Terra Bella, Traver, Strathmore, Pixley, Tipton, Tooleville, Jovista, Matheny Tract and Tooleville..

### C. USE

No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Tooleville.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses: All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

Uses/Combination of Uses reviewed by Planning Commission
Auto wrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

# TOOLEVILLE LEGACY PLAN

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The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

## **D. DEVELOPMENT STANDARDS**

1. Height: No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
2. Front Yard: 0 Feet
3. Side Yard: Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
4. Rear Yard: Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
5. Lot Area: The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
6. Floor Area Ratio: The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
7. Distance between structures: The minimum distance between structures is 10 feet.
8. Parking: Off-street parking and loading shall be required in conformance with Section 15.
9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any "R" Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

# TOOLEVILLE LEGACY PLAN

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## E. ALL OTHER DEVELOPMENT STANDARDS

All other Development Standards are included below and in the Community Plans for Ducor, Terra Bella, Traver, Strathmore, Pixley, and Tipton, and the Legacy Plans for Tooleville, Jovista, Matheny Tract and Tooleville. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

### ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

# TOOLEVILLE LEGACY PLAN

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A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

# TOOLEVILLE LEGACY PLAN

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## **ROOFS AND AWNINGS**

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

## **SITE PLANNING**

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.



# TOOLEVILLE LEGACY PLAN

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SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

## **LANDSCAPING**

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery

(Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.



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LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

## **REFUSE AND STORAGE AREAS**

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

## **LIGHTING**

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

## **WALLS AND FENCES**

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

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WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

## **STREETSCAPE**

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

## **SIGNAGE**

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

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SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or "lollipop signs."
- Moving signs and flashing signs.

# TOOLEVILLE LEGACY PLAN

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SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway. These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

## **SERVICE STATIONS AND CAR WASHES**

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

# TOOLEVILLE LEGACY PLAN

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## **AUTO REPAIR SERVICES**

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

## **CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS**

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

## **CONSUMER STORAGE FACILITIES**

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

## **SPECIAL CONDITIONS**

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

# TOOLEVILLE LEGACY PLAN

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## **APPENDICES**

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

**Appendix A – Planning Commission Resolutions**

Addendum Environmental Impact Report and MMRP-Resolution No. 9342

General Plan Amendment GPA 17-035-Resolution No. 9343

Section 18.9 Zoning Ordinance (Mixed Use)-Resolution No. 9344

Section 16 Zoning Ordinance (By Right Uses)-Resolution No. 9345

Zoning District Map-Resolution No. 9346

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE ADDENDUM EIR )  
AND MITIGATION MONITORING AND REPORTING )  
PROGRAM FOR THE LEGACY PLANS 2017 ) RESOLUTION NO. 9342  
UPDATE AND PROPOSED CHANGES TO THE )  
LAND USE MAPS AND ZONING CHANGES )  
THAT ARE NECESSARY FOR CONSISTENCY WITH )  
THE GENERAL PLAN PARTS I, II AND PART III )  
AMENDMENTS (GPA 17-033) )

Resolution of the Tulare County Planning Commission recommending to the Board of Supervisors adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update set forth in attached Exhibit "A" and Mitigation Monitoring and Reporting Program (MMRP) set forth in Exhibit "B" for the entire Legacy Plans 2017 Update which consists of General Plan Amendment No. GPA 17-033 amendment to Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities & Services Elements, and Part II Rural Valley Lands Plan, and Part III (El Monte Mobile Village GPA 17-025; Hypericum GPA 17-026; Jovista GPA 17-027; Matheny Tract GPA 17-029 and Tooleville GPA 17-030 amending the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, Change of Zones No. PZC 17-025 El Monte Mobile Village; PZC 17-026 Hypericum; PZC 17-027 Jovista; PZC 17-029 Matheny Tract and Tooleville PZC 17-030, PZC 17-032 for an amendment to Section 16 of Ordinance No. 352, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Development Boundary, and Zone Ordinance Amendment No. PZC 17-031 establishing a Mixed Use Combining Zone in El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville (incorporated by reference herein). The unincorporated Legacy Communities are located within the Mount Diablo Base & Meridian as follows: El Monte Mobile Village, Section 12, Township 16S, Range 23E, Hypericum, Section 24, Township 19S, Range 25E, Jovista, Section 32, Township 24S, Range 26E, Tooleville, Section 12, Township 19S, Range 26E, Matheny Tract Section 22, 23, & 27, Township 20 south, Range 24 East.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California; and,

WHEREAS, the General Plan Amendment will accomplish the following: (1) update the Planning Framework Element, Land Use Element, Environmental Resources Management Element, Transportation & Circulation Element and the Public Facilities & Services Element; (2) add El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville to the Mixed-Use Combining Zone; (3) allow additional by-right uses within the aforementioned Legacy



Communities; and (4) rezoning of properties consistent with the Land Use Element, as amended; and,

WHEREAS, The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,

WHEREAS, The Board of Supervisors has given notice of the proposed amendment to the General Plan as provided in Sections 65355, 65090, and 65091 of the Government Code of the State of California. A public notice was printed in the Dinuba Sentinel on August 23, 2012 the Visalia Times Delta, Porterville Recorder, and the Tulare Advance Register on August 17, 2012 at least ten days prior to the public hearing before the Board of Supervisors on August 28, 2012.; and,

WHEREAS, this Addendum EIR and MMRP is for the Legacy Plans 2017 Update inclusive of: General Plan Amendment No. GPA 17-033 an amendment to the Planning Framework Element, Land Use Element, Environmental Resources Management and the Transportation and Circulation Element; Change of Zone No. PZC 17-031 an amendment to Section 18.9 the "MU" Mixed-Use Combining Zone; A Change of Zone for El Monte Mobile Village, Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista, Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract, Change of Zone No. PCZ 17-029 (Zoning District Map) and Tooleville, Change of Zone No. PZC 17-030 (Zoning District Map) within the respective Legacy Development Boundaries in conformance with the updated Land Use Element; Change of Zone No. PZC 17-032 an amendment to Section 16 to allow additional by-right uses in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities; and

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the Addendum EIR, staff found, and the Planning Commission agrees that a summary of impacts and potential mitigation measures is included in Table ES-4 of the RDEIR and contains: (1) environmental impacts; (2) mitigation measures; (3) significance before mitigation; and (4) significance after mitigation, and required additional mitigating policies and implementation measures are recommended for the following: (1) Land Use and Aesthetics; (2) Traffic and Circulation; (3) Energy and Global Climate Change; (4) Noise; (5) Geology, Soils, Seismicity, and Mineral Resources; (6) Hazardous Materials and Public Safety; (7) Public Services, Recreation Resource and Utilities; (8) Agricultural Resources; (9) Biological Resources; and (10) Cultural Resources; and; therefore, the Addendum EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, the proposed Legacy Plans 2017 Update is consistent with the Tulare County General Plan and includes the following primary goals and objectives:

1. Land Use and Environmental Planning - Promote Economic Development within planning areas in order to implement the following General Plan goals:
  - a. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals through an amendment of the Land Use Element to incorporate use designations contained in the proposed Legacy Plans;
  - b. Encourage infill development within Legacy Development Boundaries, thereby discouraging leapfrog development within Tulare County;
  - c. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
  - d. Reduce vehicle miles travelled throughout the County, thereby reducing greenhouse gas emissions and positively affecting air quality;
  - e. Amend the Transportation and Circulation Element and incorporate circulation plan designations in the Legacy Plans. This will improve the community's circulation, transit and pedestrian transportation system by enabling the construction of key projects including Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths; and
  - f. Promote Economic Development by reducing entitlement requirements and providing flexible land uses in a mixed use overlay zone, which will enable the County to adapt to current market conditions.
2. Improvements for a "disadvantaged community" - The legacy planning areas will be improved because of the following:
  - a. Faster project processing resulting in increased employment opportunities by the private sector, as proposed projects can be reviewed and approved expeditiously;
  - b. Increased housing grant awards that are consistent with the policies of the recently adopted General Plan Update and Housing Element; and
  - c. Enhanced infrastructure grant awards providing access to funding to upgrade road, water, wastewater, and storm water facilities.
3. Strengthening Relationship with TCAG - An important benefit of this expedited community plan process is the opportunity for RMA to strengthen the County's relationship with the Tulare County Association of Governments (TCAG), as this Community Plan will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a collaborative process, there is greater probability of getting projects in the ground faster, thereby making the community safer and healthier by providing a more efficient transportation network.

WHEREAS, on September 15, 2017 a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

WHEREAS, the Planning Commission is the advisory body to the Board of Supervisors with respect to the Addendum EIR, MMRP and the Legacy Plans 2017 Update; and

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends adoption based on the substantial evidence in the record that the analysis presented in the Addendum EIR and Mitigation Monitoring Reporting Program (MMRP) for the Legacy Plans 2017 Update General Plan Amendment 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Change of Zone No. PZC 17-025; Hypericum General Plan Amendment No. GPA 17-026; Change of Zone No. PZC 17-026; Jovista General Plan Amendment No. GPA 17-027; Change of Zone No. PZC 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; Change of Zone No. PZC 17-029; Tooleville General Plan Amendment No. GPA 17-030; Change of Zone No. PZC 17-030; Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9); Change of Zone No. PZC 17-032 (By-Right Uses–Section 16); and Amendments to General Plan Elements Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation, Public Facilities & Services, and Part II Rural Valley Lands Plan has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970; and,
2. The Commission further finds that The County prepared a Final EIR for the 2010 draft Tulare County General Plan 2030 Update (SCH #2006041162) and released the Final EIR for public review on or about August 30, 2011. The FEIR for the General Plan 2030 Update was prepared in compliance with the CEQA (Public Resources Code Section 21000 et seq. and the CEQA Guidelines (California Code of Regulations, Title 14). The Final EIR was distributed on CD to the State Clearinghouse, and all agencies who commented on the RDEIR. Individual notices of the FEIR availability were sent to agencies, organizations, and individuals who commented on the RDEIR. The Final EIR was available in all Tulare County Libraries, at the Tulare County Resource Management Agency and the following website, <http://generalplan.co.tulare.ca.us>; and,
3. The findings made in regards to this Addendum EIR and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency; and,

4. Consistent with Public Resource Code Section 21081 and Guidelines Sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines Section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project Addendum. Based on substantial evidence in the IS/MND and Pursuant to the discussion in each section of the Addendum EIR, and the Public Record of Proceedings, the Commission finds and declares that mitigation measures are required and that the Project will not cause a significant impact to the environment with adoption of these mitigation measures.

AND, BE IT FURTHER RESOLVED as follows:

A. The Planning Commission finds that Tulare County is required to undertake Mitigation Measures set forth in attached Exhibit "B" that are restrictive and applied only to the Legacy Plans 2017 Update. Therefore, the public will benefit from the Legacy Plans as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on October 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano  
NOES: None  
ABSTAIN: None  
ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Addendum EIR

B – Mitigation Monitoring and Reporting Plan (MMRP)

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AMENDMENTS TO THE PART I )  
PLANNING FRAMEWORK ELEMENT, ENVIRONMENTAL )  
RESOURCES MANAGEMENT ELEMENT, LAND USE ) RESOLUTION NO. 9343  
ELEMENT, TRANSPORTATION & CIRCULATION )  
ELEMENT AGRICULTURE ELEMENT, SCENIC )  
LANDSCAPES ELEMENT, AIR QUALITY ELEMENT, )  
PUBLIC FACILITIES & SERVICES ELEMENT, )  
INTRODUCTION, COMPONENTS A, B, C, AND D, AND )  
PART II RURAL VALLEY LANDS PLAN OF THE TULARE )  
COUNTY GENERAL PLAN FOR THE LEGACY PLANS )  
2017 UPDATE GPA 17-033 INCLUSIVE OF GPA's 17-025, )  
GPA 17-026, GPA 17-027, GPA 17-29, AND GPA 17-030 )

Resolution of the Tulare County Planning Commission recommending the adoption of the proposed amendments to Part I Introduction; Component A-Planning Framework Element; Component B-Agriculture and Land Use Elements, Component C-Scenic Landscapes, Environmental Resources Management and Air Quality Elements; Component D-Transportation & Circulation and Public Facilities & Services Elements; and Part II Rural Valley Lands Plan of the Tulare County General Plan as set forth in Exhibits "A-1 to A-13" and Part III Community Plan as set forth in Exhibits "B-F" for the Legacy Plans 2017 Update General Plan Amendment No. GPA 17-033; inclusive of El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030 by the Board of Supervisors.

WHEREAS, the Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) which initiated the action to amend the Tulare County General Plan pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California, and

WHEREAS, the Planning Commission has given notice of the proposed amendment to the General Plan as provided in Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has made such investigation of fact bearing upon the proposed amendments to assure the action is consistent with the procedures and purposes set forth in the California Government Code, the State General Plan Guidelines, and other elements of the Tulare County General Plan, and

WHEREAS, staff has conducted multiple public outreach meetings in the Legacy communities as part of the Legacy Plans 2017 Update to receive public participation in shaping the Update, and

WHEREAS, on September 15, 2017 a notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, to consider the Legacy Plans 2017 Update, and

WHEREAS, a Public Hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017 and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded.

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA, prior to taking action on the proposed amendments to the Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan for the Legacy Plans 2017 Update.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings to be relevant in evaluating the proposed General Plan Amendment:

1. The amendment of the Land Use Element to incorporate the land use designations contained in the Legacy Plans 2017 Update is consistent with the Land Use Element of the Tulare County General Plan;
2. The amendment of the Transportation & Circulation Element to incorporate circulation plan designations contained in the Legacy Plans 2017 Update is consistent with the Transportation & Circulation Element of the Tulare County General Plan;
3. The Amendment of the Planning Framework Element to establish Legacy Development Boundaries as set forth in the Tulare County General Plan, Planning Framework Element, adopted by the Tulare County Board of Supervisors in August 2012;

4. The Amendment to the Environmental Resources Management Element to modify the urban expansion area in the open space element reflective of the establishment of legacy development boundaries consistent with the Planning Framework Element of the Tulare County General Plan;
5. The Amendment to Part I Introduction, Component A, Component B, Agriculture, Component C, Scenic Landscapes, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan is necessary to maintain consistency and uniformity regarding the application of policies and procedures relative to Communities, Hamlets, and the newly established Legacy Communities.
6. Amendments to the Zoning Ordinance to implement the Legacy Plans 2017 Update will reflect and remain consistent with the Land Use Plan for the community;
7. The proposed amendments are internally consistent with the Tulare County General Plan. Specifically, Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Services Elements, and Part II Rural Valley Lands Plan of the Tulare County General Plan;
8. The Environmental Assessment Officer approved the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) prepared for the project;
9. Based on substantial evidence, the analysis presented in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for GPA 17-033 have been completed in compliance with the California Environmental Quality Act and the State Guidelines for the implementation of the California Environmental Quality Act of 1970.

AND, BE IT FURTHER RESOLVED as follows:

C. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH #2006041162).

D. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt General Plan Amendment No. GPA 17-033 the Legacy Plans 2017 Update set



forth in attached Exhibits “A-1 to A-13” and Part III Exhibits “B-F” which is incorporated by reference herein.

The foregoing was adopted upon motion of Commissioner Pitigliano, seconded by Commissioner Whitlatch, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

Exhibits “A-1 – A-13” – Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Transportation & Circulation and Public Facilities & Service Elements, and Part II Rural Valley Lands Plan.

Exhibit “B” – 2017 El Monte Mobile Home Park Legacy Plan (Part III)

Exhibit “C” – 2017 Hypericum Legacy Plan (Part III)

Exhibit “D” – 2017 Jovista Legacy Plan (Part III)

Exhibit “E” – 2017 Matheny Tract Legacy Plan (Part III)

Exhibit “F” – 2017 Tooleville Legacy Plan (Part III)



BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO           )  
SECTION 18.9 "MU" MIXED USE COMBINING    )  
ZONE OF ORDINANCE NO. 352 CONSISTENT     )       RESOLUTION NO. 9344  
WITH THE ADOPTED LEGACY PLANS 2017       )  
UPDATE, AS PROPOSED IN CHANGE OF ZONE    )  
NO. PZC 17-031                                 )

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 18.9 "MU" Combining Zone of Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is

applicable for the proposed amendment to Section 18.9 “MU” Mixed Use Combining Zone of Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through mixed land uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of this Mixed Use Combining Zoning District is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicle miles traveled by locating residential uses within proximity of employment areas;
4. This Mixed Use Combining Zone update currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added;
5. Within the Mixed Use Combining Zone, all uses outlined in the M-1 (Light Manufacturing), C-3 (Service Commercial), C-2 (General Commercial), C-1 (Neighborhood Commercial), R-1 (Single Family Residential), R-2 (Two-Family Residential), and R-3 (Multiple Family Residential) uses are allowed. Uses and activities that are found by the Planning Director to be similar to, and compatible with, those specific zoning districts are also allowed. Uses and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed;
6. All conditional uses allowed in the M-1, C-3, C-2, C-1, R-1, R-2, and R-3 zoning districts shall also be allowed by right with the exception of the following uses and combination of uses: Auto Wrecking and Residential, Battery Manufacture and Residential or Commercial, Biomass Fuel Production and Residential, Flammable Liquids over 10,000 gallons, Hazardous Waste Facility, Planing Mills and

Residential or Commercial, Sand blasting, Slaughterhouse and Residential, Solid Waste Recycling and Residential, Super service stations and Residential, Airport, and Heliport;

7. All uses shall be non-detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the community, or to the general welfare of the County. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards. All allowed uses are subject to the determination of appropriateness by the Director of Planning; and
8. The Planning Director has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 18.9 "MU" Mixed Use Combining Zone of Ordinance No. 352 will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update is applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZ 17-032, an amendment to Ordinance 352, Section 18.9 "MU" Mixed Use Combining Zone to include the communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

A – Section 18.9 Mixed-Use Combining Zone

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT	)	
TO SECTION 16.H OF ORDINANCE NO. 352	)	
TO ALLOW ADDITIONAL BY-RIGHT USES	)	RESOLUTION NO. 9345
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS, AS PROPOSED IN CHANGE	)	
OF ZONE NO. PZC 17-032	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Section 16.H of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses set forth in attached Exhibit "A," to include the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Zoning Ordinance Amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 and of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report, made a part hereof, and recommended approval of the Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and

WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed amendment to Section 16.H of Ordinance No. 352, it has reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed Change of Zone.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update and to promote sustainability through the allowance of Additional By-Right Uses while promoting economic development and prosperity in the Communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville;
3. The purpose of adding Additional By-Right Uses is to provide design flexibility, streamline the approval process, promote economic development, and reduce vehicles miles traveled by locating residential uses within proximity of employment areas; and
4. The allowance of Additional By-Right Uses currently only applies to the communities of Traver, Strathmore, Pixley, Tipton, Terra Bella and Ducor. The communities of El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville will be added.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the amendment to Section 16.H of Ordinance No. 352 to allow Additional By-Right Uses will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # State Clearinghouse No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve Change of Zone No. PZC 17-032 an amendment to Ordinance No. 352, Section 16.H to allow Additional By-Right Uses to include the communities of El Monte Mobile Village,

Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the adopted Legacy Plans 2017 Update, consistent with the Legacy Communities 2017 Update including affirmation that Section 16 H. is applicable in the Matheny Tract.

The foregoing was adopted upon motion of Commissioner Dias, seconded by Commissioner Pitigliano, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
Michael Washam, Secretary

Exhibits:

A – Section 16.H Additional By-Right Uses

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF AN AMENDMENT TO	)	
ORDINANCE NO. 352, ZONING ORDINANCE	)	
DISTRICT MAPS TO REZONE PROPERTIES	)	RESOLUTION NO. 9346
IN THE LEGACY DEVELOPMENT BOUNDARIES	)	
EL MONTE MOBILE VILLAGE, HYPERICUM,	)	
JOVISTA, MATHENY TRACT, AND TOOLEVILLE	)	
CONSISTENT WITH THE ADOPTED LEGACY	)	
PLANS 2017 UPDATE (GPA 17-033), AS	)	
PROPOSED INCHANGE OF ZONE NO. PZC's	)	
17-025, 17-026, 17-027, 17-029, AND 17-030	)	

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors amend Ordinance No. 352, the Zoning Ordinance as set forth in attached Exhibit "B" El Monte Mobile Village Rezoning Plan (PZC 17-025), to include the Legacy Community of El Monte Mobile Village, consistent with the adopted El Monte Mobile Village Legacy Plan 2017 Update, Exhibit "E" Hypericum Rezoning Plan (PCZ 17-026), to include the Legacy Community of Hypericum, consistent with the adopted Hypericum Legacy Plan 2017 Update, Exhibit "H" Jovista Rezoning Plan (PCZ 17-027), to include the Legacy Community of Jovista, consistent with the adopted Jovista Legacy Plan 2017 Update, Exhibit "K" Matheny Tract Rezoning Plan (PCZ 17-029), to include the Legacy Community of Matheny Tract, consistent with the adopted Matheny Tract Legacy Plan 2017 Update, Exhibit "N" Tooleville Rezoning Plan, to include the Legacy Community of Tooleville, consistent with the adopted Tooleville Legacy Plan 2017 Update.

WHEREAS, a petition has been filed pursuant to the regulations contained in Section 17 of the Tulare County Zoning Ordinance No. 352, and

WHEREAS, the Planning Commission has given notice of the Ordinance amendment as provided in Section 18 of Ordinance No. 352 and Section 65854 of the Government Code of the State of California, and

WHEREAS, staff has performed necessary investigations, prepared a written report made a part hereof, and recommended approval of this Ordinance amendment, and

WHEREAS, on September 15, 2017, a Notice of Public Hearing by the Tulare County Planning Commission was duly published in the Visalia Times-Delta, a newspaper of general circulation in Tulare County, and

WHEREAS, a public hearing was held and an opportunity for public testimony was provided at a regular meeting of the Planning Commission on September 27, 2017, and



WHEREAS, at Tulare County Planning Commission meeting, public testimony was presented by Mel Meleka, Principal Planner with the City of Tulare, who confirmed the City's position regarding wastewater treatment which was received and recorded, and

NOW, THEREFORE, BE IT RESOLVED as follows:

A. This Planning Commission hereby certifies that prior to taking action on the proposed Change of Zone, it reviewed and considered the information contained in the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) for compliance with the with the California Environmental Quality Act (CEQA), and the State Guidelines for the implementation of CEQA that were prepared for the Legacy Plans 2017 Update and is applicable for the proposed amendment to Ordinance No. 352.

B. This Planning Commission, after considering all of the evidence presented, including all comments received during the public review process, hereby determines the following findings were relevant in evaluating the proposed Change of Zone:

1. The Tulare County Board of Supervisors, on April 4, 2017, approved General Plan Initiation (GPI 17-001) to prepare a General Plan Amendment for Unincorporated Community Plans, Hamlet Plans and Legacy Plans pursuant to Title 7, Chapter 3, Articles 5 and 6 of the Government Code of the State of California;
2. The purpose of this proposal is to obtain the appropriate zoning consistent with the adopted Legacy Plans 2017 Update (GPA 17-033) and to promote sustainability through mixed land uses while promoting economic development and prosperity in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Communities;
3. The proposed changes in zone district designations identified in the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans are consistent with Tulare County General Plan policy.

C. This Planning Commission, after considering all of the evidence presented, hereby finds the proposed Ordinance amendment to be consistent with the purpose of Ordinance No. 352 and further finds the petition is in conformance with the adopted General Plan for the County of Tulare.

AND, BE IT FURTHER RESOLVED as follows:

D. This Planning Commission hereby recommends that the Board of Supervisors find there is no substantial evidence that the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Rezoning Plans will have a significant effect on the environment. And that the Addendum EIR and Mitigation Monitoring and Reporting Program (MMRP) that were prepared for the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update are applicable for the proposed amendment to Ordinance No. 352 and reflects the independent judgment of the County.

E. This Planning Commission hereby recommends that the Tulare County Board of Supervisors adopt the Addendum EIR and Mitigation Monitoring Reporting Program (SCH # No. 2006041162).

F. This Planning Commission hereby recommends that the Tulare County Board of Supervisors approve the El Monte Mobile Village Change of Zone No. PZC 17-025; Hypericum Change of Zone No. PZC 17-026; Jovista Change of Zone No. PZC 17-027; Matheny Tract Change of Zone No. PZC 17-029; and Tooleville Change of Zone No. PZC 17-030, an amendment to Ordinance No. 352, the Zoning Ordinance set forth in attached Exhibits "B" "E" "H" "K" and "N" Rezoning Plans consistent with the adopted El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville Legacy Plans 2017 Update (GPA 17-033).

The foregoing was adopted upon motion of Commissioner Whitlatch, seconded by Commissioner Dias, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano

NOES: None

ABSTAIN: None

ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION

  
\_\_\_\_\_  
Michael Washam, Secretary

Exhibits:

El Monte Mobile Village

Exhibit "A" – Proposed Zone Ordinance Map (PZC 17-025)

Exhibit "B" – Proposed Zoning Map

Exhibit "C" – List of Affected Properties/Map

Hypericum

Exhibit "D" – Proposed Zone Ordinance Map (PZC 17-026)

Exhibit "E" – Proposed Zoning Map

Exhibit "F" – List of Affected Properties/Map

Jovista

Exhibit "G" – Proposed Zone Ordinance Map (PZC 17-027)

Exhibit "H" – Proposed Zoning Map

Exhibit "I" – List of Affected Properties/Map

Matheny Tract

Exhibit "J" – Proposed Zone Ordinance Map (PZC 17-029)

Exhibit "K" – Proposed Zoning Map

Exhibit "L" – List of Affected Properties/Map

Tooleville

Exhibit "M" – Proposed Zone Ordinance Map (PZC 17-030)

Exhibit "N" – Proposed Zoning Map

Exhibit "O" – List of Affected Properties/Map

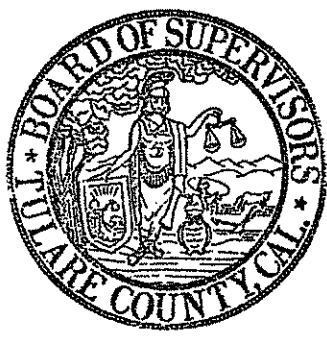
Appendix B – Board of Supervisors  
Resolution No. 2017-0868

**BEFORE THE BOARD OF SUPERVISORS  
COUNTY OF TULARE, STATE OF CALIFORNIA**

IN THE MATTER LEGACY PLANS 2017 )  
UPDATE (GENERAL PLAN AMENDMENT 17- ) Resolution No. 2017-0868  
033) AND AMENDMENTS TO THE TULARE ) Ordinance No. 3508  
COUNTY ZONING ORDINANCE NO. 352 )

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY  
SUPERVISOR CROCKER, THE FOLLOWING WAS ADOPTED BY THE BOARD OF  
SUPERVISORS, AT AN OFFICIAL MEETING HELD OCTOBER 17, 2017, BY THE  
FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY,  
AND ENNIS  
NOES: NONE  
ABSTAIN: NONE  
ABSENT: NONE



ATTEST: MICHAEL C. SPATA  
COUNTY ADMINISTRATIVE OFFICER/  
CLERK, BOARD OF SUPERVISORS

BY: *Michael C. Spata*  
Deputy Clerk

\*\*\*\*\*

That the Board of Supervisors:

1. Held Public Hearing at 9:30 A.M. or shortly thereafter; and
2. Certified that the Board of Supervisors has reviewed and considered the information contained in the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update is applicable to the General Plan Amendment and associated Zoning Ordinance Amendments for the Legacy Plans 2017 Update as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and the Mitigation Monitoring Reporting Program for the Legacy Plans 2017 Update (GPA 17-033); and
3. Adopted one General Plan Amendment No. GPA 17-033, for the Legacy Plans 2017 Update inclusive of the El Monte Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029); Tooleville (GPA 17-030) and Amendments to Tulare County General Plan Part III to establish Legacy Plans and Legacy Development Boundaries; Part I Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D,

Public Facilities and Services Elements, and Part II General Plan Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update; and

4. Waived the final reading and adopted Amendments to Section 18.9 and Section 16 of Tulare County Ordinance 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Legacy Plans as follows:
  - A. Zone Ordinance Amendment (PZC 17-031) to Section 18.9 to adopt a Mixed-Use Combining Zone within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - B. Zone Ordinance Amendment (PZC 17-032) to Section 16 to allow additional by-right uses within the El Monte Village, Hypericum, Jovista, Matheny Tract, and Tooleville Development Boundaries.
  - C. Zone Ordinance Amendments as set forth in the Zoning District Ordinance Maps for El Monte Village (PZC 17-025), Hypericum (PZC 17-026), Jovista (PZC 17-027), Matheny Tract (PZC 17-029), and Tooleville (PZC 17-030), consistent with the Legacy Plans 2017 Update; and
5. Adopted the findings of approval set forth in Planning Commission Resolution No. 9342 (Addendum to EIR), Planning Commission Resolution No. 9343 (Legacy Plans 2017 Update), Planning Commission Resolution No. 9344 (Section 18.9 Mixed Use), Planning Commission Resolution No. 9345 (Section 16 By-Right Uses), and Planning Commission Resolution No. 9346 (Zoning District Ordinance Maps); and
6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of El Monte Mobile Village, Hypericum, Matheny Tract, Jovista, and Tooleville to include the Mixed Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file a Notice of Determination with the Tulare County Clerk; and
9. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents; and
10. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein.

## **EXHIBIT "A"**

### **A. Legacy Plans 2017 Update (General Plan Amendment 17-033)**

WHEREAS, on September 27, 2017 the Planning Commission held a public hearing on the Legacy Plans 2017 Update (GPA 17-033), which included an application for El Monte Mobile Village General Plan Amendment No. GPA 17-025; Hypericum General Plan Amendment No. GPA 17-026; Jovista General Plan Amendment No. GPA 17-027; Matheny Tract General Plan Amendment No. GPA 17-029; and Tooleville General Plan Amendment No. GPA 17-030, El Monte Mobile Village Change of Zone No. PZC 17-025 (Zoning District Map); Hypericum Change of Zone No. PZC 17-026 (Zoning District Map); Jovista Change of Zone No. PZC 17-027 (Zoning District Map); Matheny Tract Change of Zone No. PZC 17-029 (Zoning District Map); and Tooleville Change of Zone No. PZC 17-030 (Zoning District Map), Change of Zone No. PZC 17-031 (Mixed-Use Combining Zone–Section 18.9) and Change of Zone No. PZC 17-032 (By-Right Uses–Section 16), collectively referred to as the Legacy Plans 2017 Update (GPA 17-033); and

WHEREAS, upon conclusion of the public hearing, the Planning Commission recommended the Board of Supervisors certify and adopt the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopt the Mitigation Monitoring and Reporting Program and approve the Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors held a public hearing on October 17, 2017 to consider the proposed Legacy Plans 2017 Update; and

WHEREAS, the Board of Supervisors finds the public hearing for the Legacy Plans 2017 Update was properly noticed in accordance with state law and the Tulare County Ordinance Code.

WHEREAS, the Board of Supervisors finds the Legacy Plans 2017 Update is compatible, integrated and internally consistent with the existing policies of and with the Tulare County General Plan.

WHEREAS, the Board of Supervisors accepts and incorporates by reference herein, the findings of facts and recommendations adopted by the Planning Commission as follows:

1. Planning Commission Resolution No. 9342, in the matter of the Certification and adoption of the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update (PC Package Attachment 1 – CEQA Resolution).

2. Planning Commission Resolution No. 9343, to adopt General Plan Amendment No. GPA 17-033 for the Legacy Plans 2017 Update inclusive of El Monte Mobile Village (GPA 17-025); Hypericum (GPA 17-026); Jovista (GPA 17-027); Matheny Tract (GPA 17-029) and Tooleville (GPA 17-030) and Amendments to Part III of the Tulare County General Plan to establish Legacy Plans and Legacy Development Boundaries, to the Part I General Plan Amendment to the Introduction, Component A, Planning Framework, Component B, Agriculture, Land Use, Component C, Scenic Landscapes, Environmental Resources Management, Air Quality, Component D, Public Facilities and Services Elements, and Part II Rural Valley Lands Plan, consistent with the Legacy Plans 2017 Update (PC Package Attachment 2 – GPA Resolution).
3. Planning Commission Resolution No. 9344, in the matter of an Amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed Use Combining Zone within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 3 – Section 18.9 Resolution).
4. Planning Commission Resolution No. 9345, in the matter of an Amendment to Section 16 of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses within the El Monte Mobile Village, Hypericum, Jovista, Matheny Tract, and Tooleville Legacy Development Boundaries (PC Package Attachment 4 – Section 16 Resolution).
5. Planning Commission Resolution No. 9346, in the matter of an Amendment to Ordinance No. 352, the Zoning Ordinance, as set forth in the Zoning District Ordinance Maps for El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville, consistent with the Legacy Plans 2017 Update to rezone certain properties within El Monte Mobile Village, Hypericum, Jovista, Matheny Tract and Tooleville. (PC Package Attachment 5 – Rezoning Plan Resolution).

NOW, THEREFORE, BE IT RESOLVED as follows:

A. The Board of Supervisors hereby certifies and adopts the Addendum to the (2012) Tulare County 2030 General Plan Final Environmental Impact Report for the Legacy Plans 2017 Update, as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopts the Mitigation Monitoring Reporting Programs.

NOW BE IT FURTHER RESOLVED as follows:

B. The Board of Supervisors adopts the entirety of all of the above findings and actions as one General Plan Amendment No. GPA 17-033.

C. The Board of Supervisors hereby waives the final reading of Change of Ordinance 352 and adopts amendments to the Tulare County Zoning Ordinance 352 pertaining to Section 18.9 - Mixed-Use Combining Zone, to Section 16 - Variances and Special Uses, and Zoning District Map Changes.



D. The Board of Supervisors authorizes the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of Legacy Plans 2017 Update to include Mixed Use Combining Zone and additional "By-Right" uses.

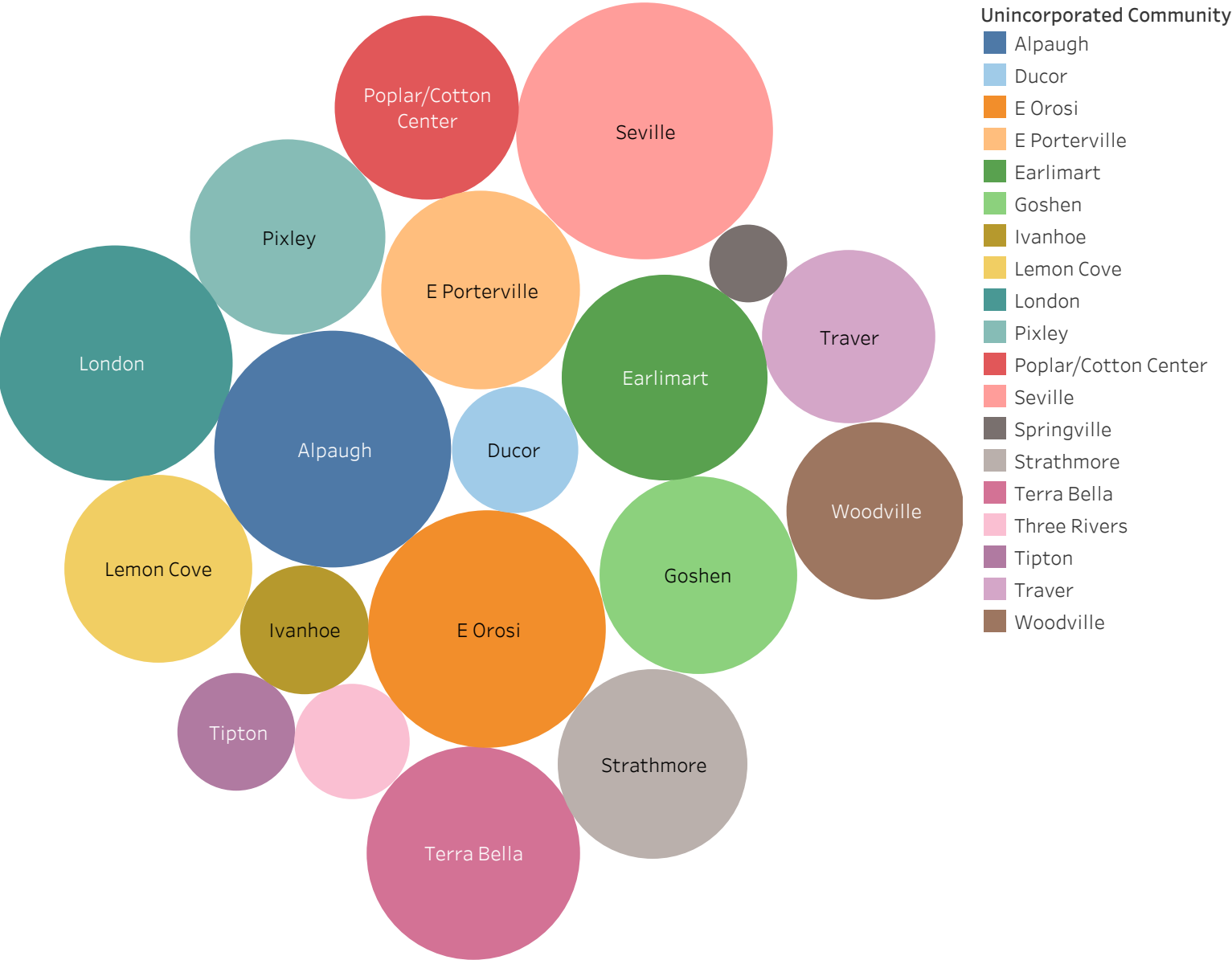
E. The Board of Supervisors directs the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq.

F. The Board of Supervisors directs the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file Notices of Determination with the Tulare County Clerk.

G. The Board of Supervisors authorizes the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Legacy Plans, and associated project documents.

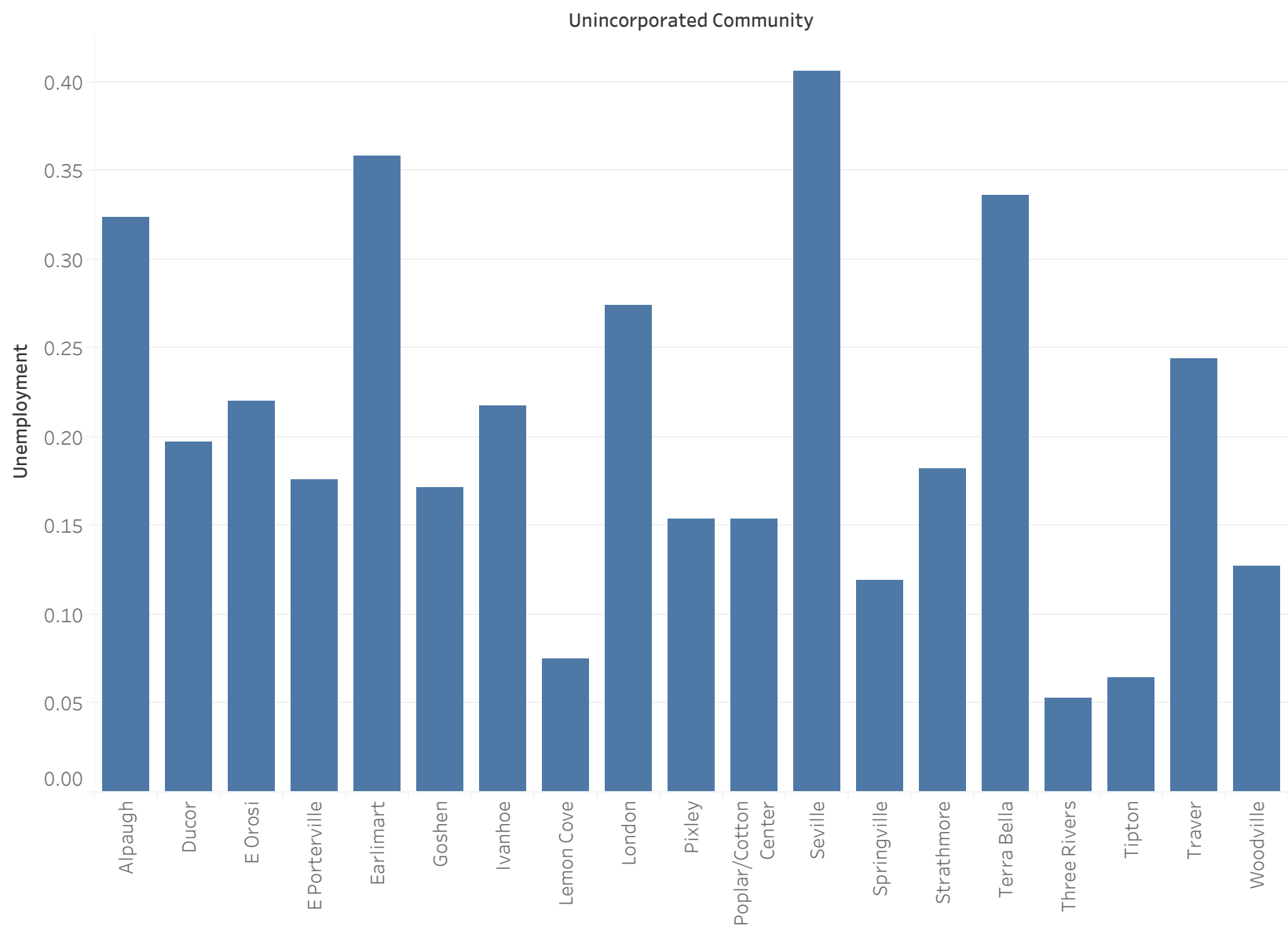
## Appendix E – Housing Condition Study

Poverty Percentage



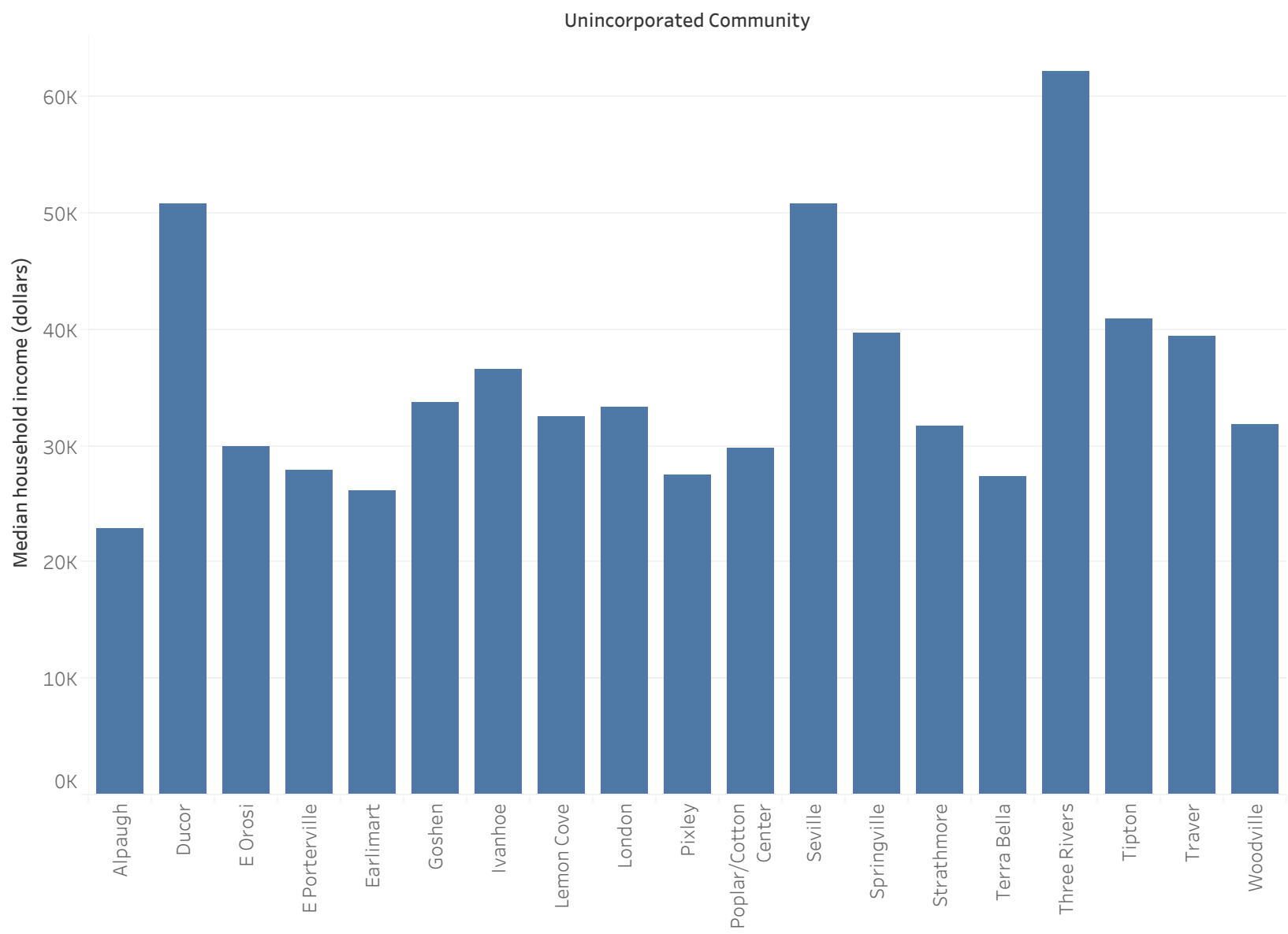
Unincorporated Community. Color shows details about Unincorporated Community. Size shows sum of Poverty Percent. The marks are labeled by Unincorporated Community.

# Unemployment



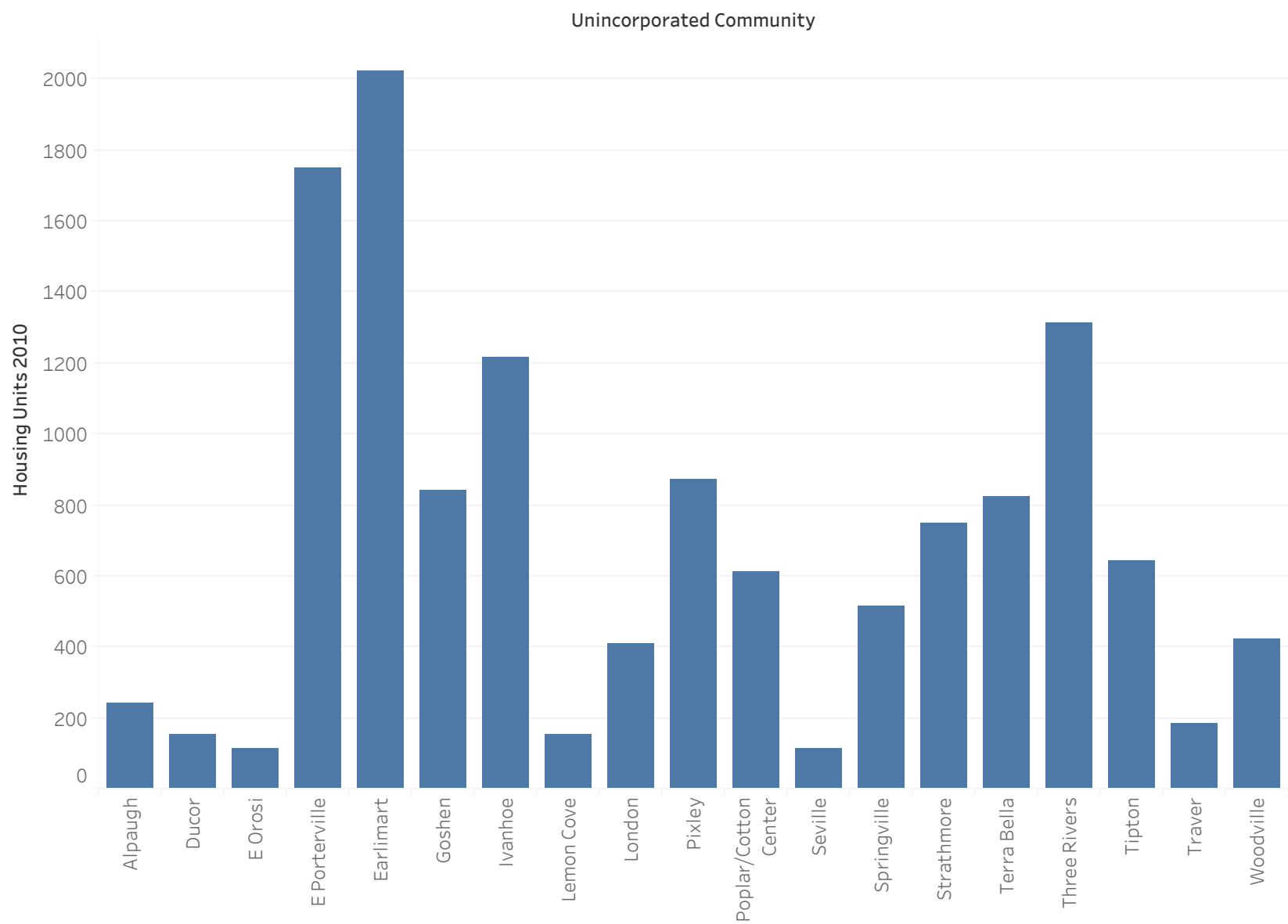
Sum of Unemployment for each Unincorporated Community.

# Median Income



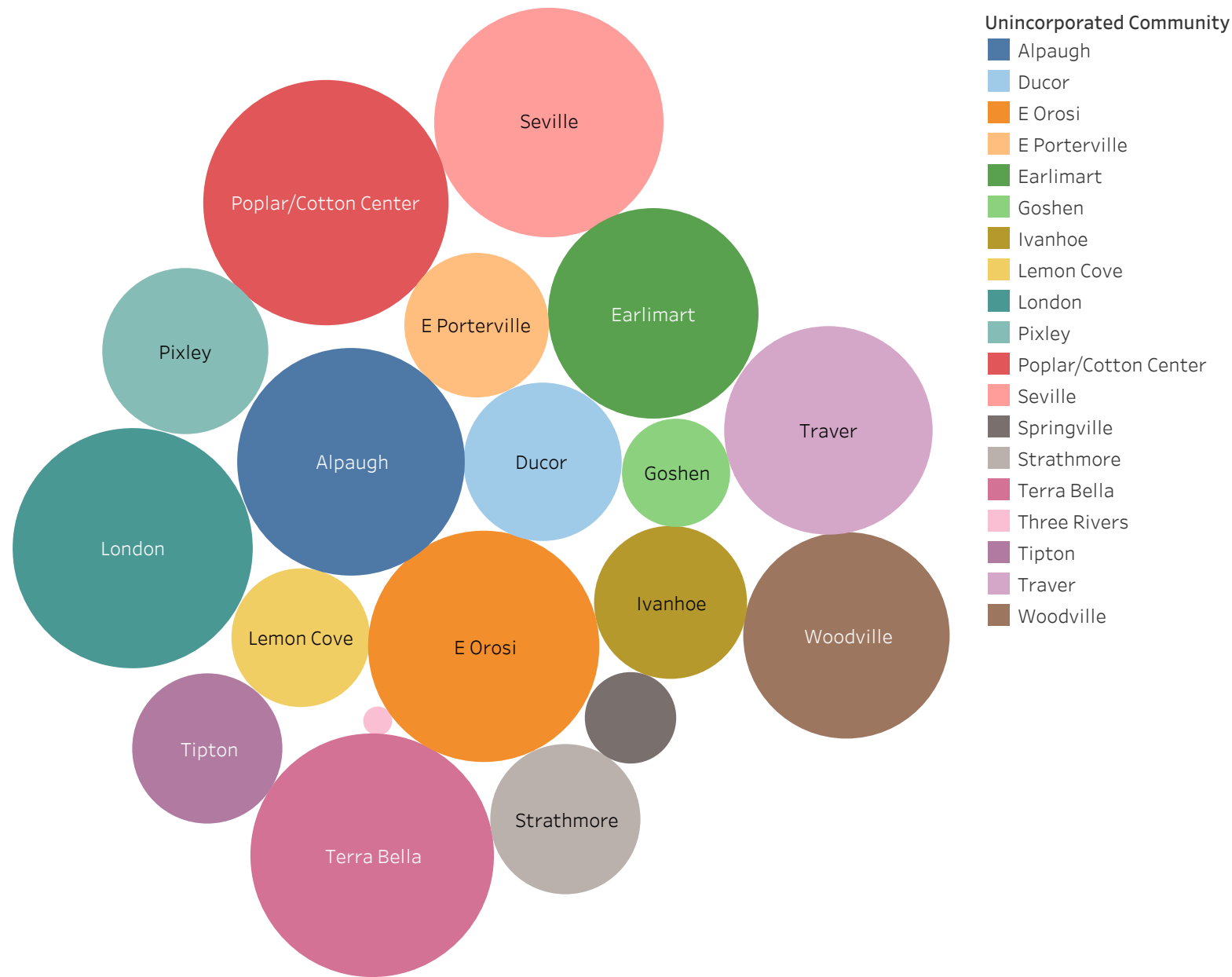
Sum of Median household income (dollars) for each Unincorporated Community.

# Housing Units



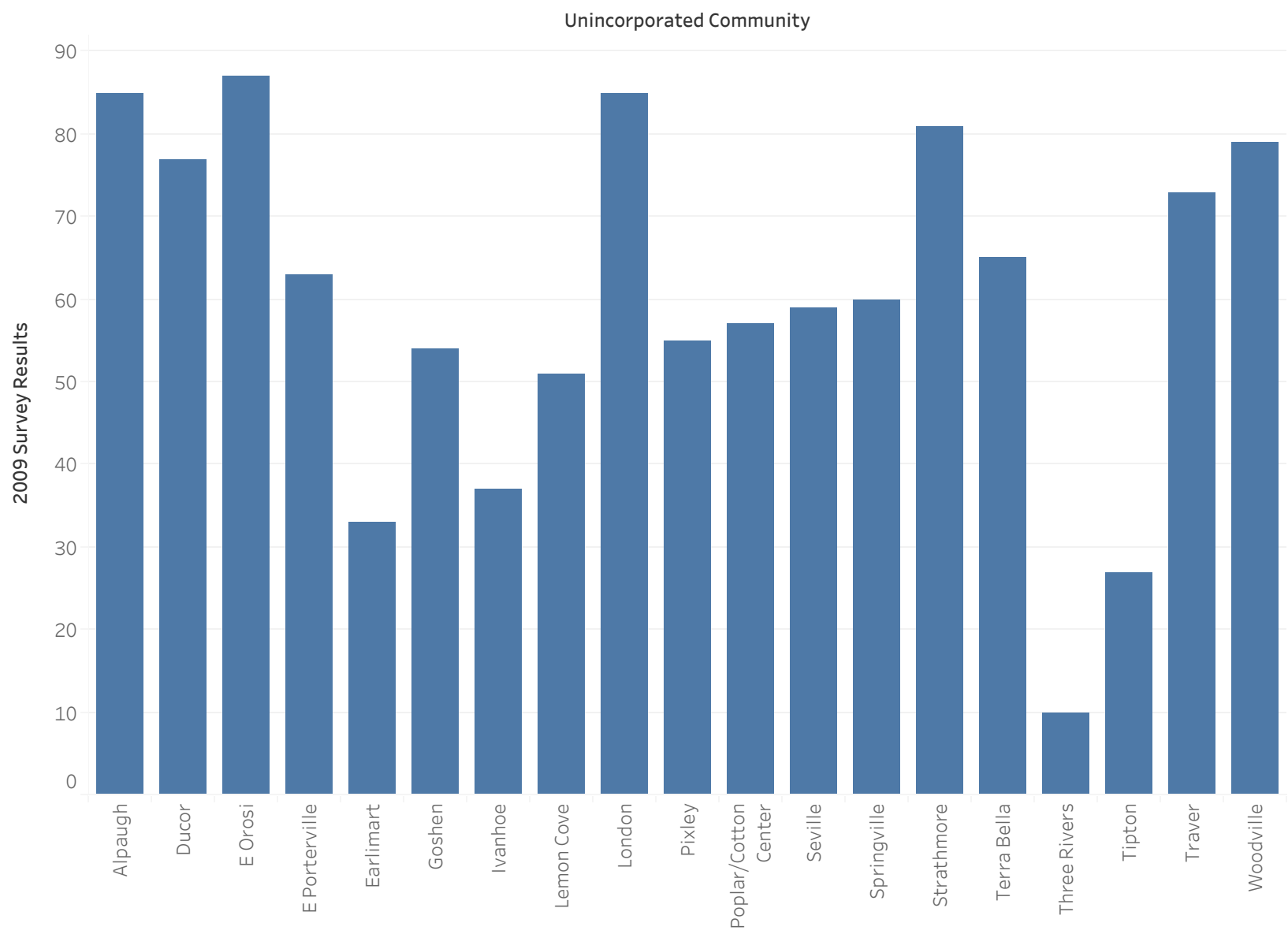
Sum of Housing Units 2010 for each Unincorporated Community.

Substd 1992



Unincorporated Community. Color shows details about Unincorporated Community. Size shows % of Total 1992 Survey Results. The marks are labeled by Unincorporated Community.

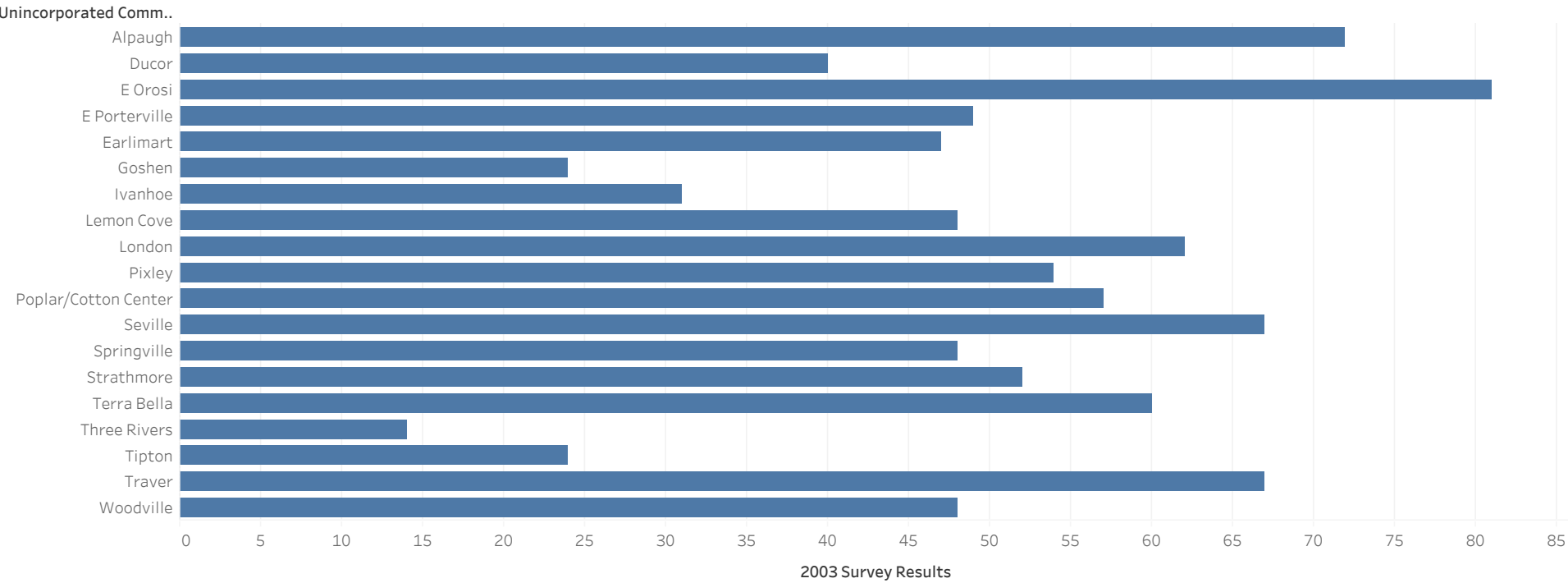
Subsd 2009



Sum of 2009 Survey Results for each Unincorporated Community.

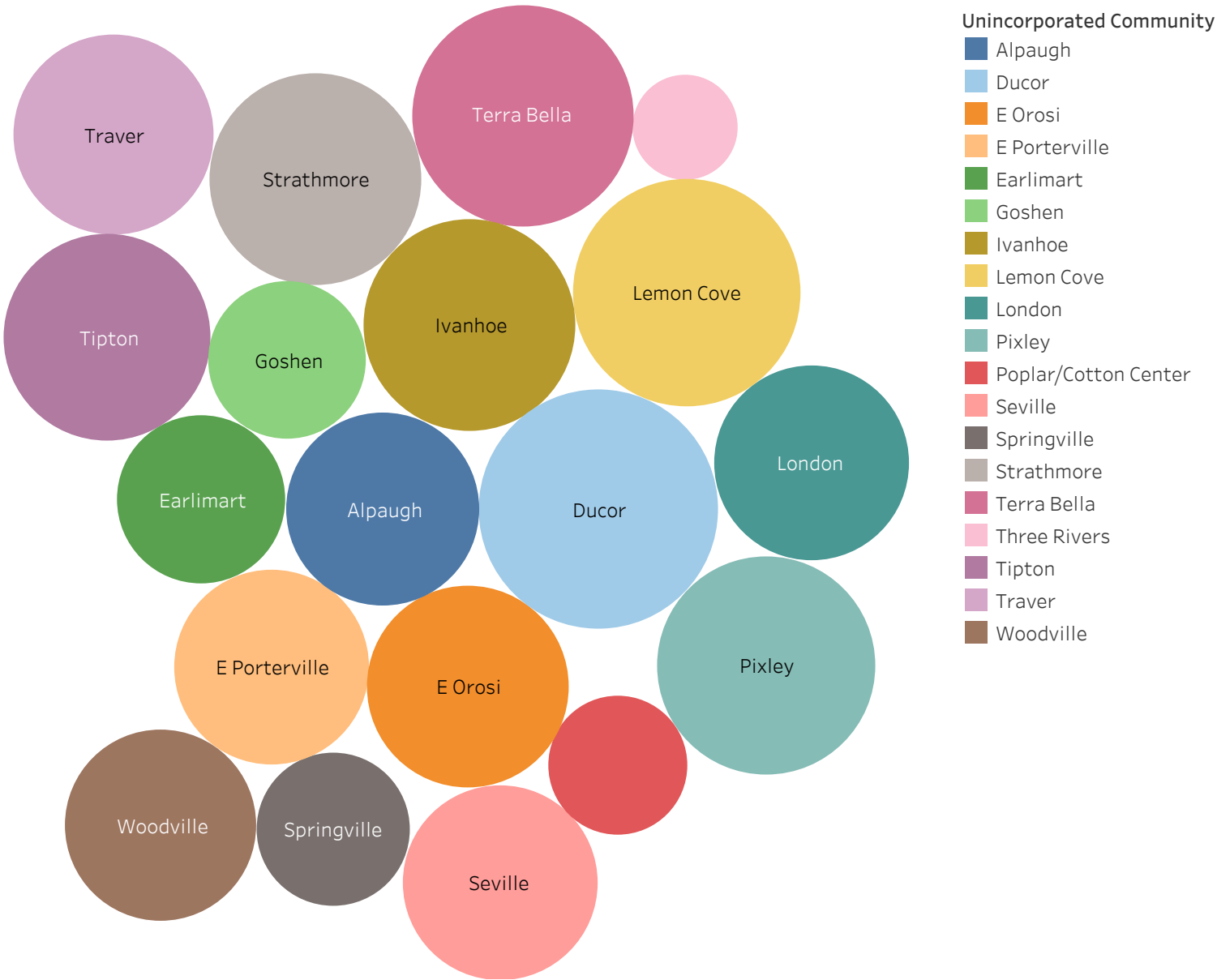


Substd 2003



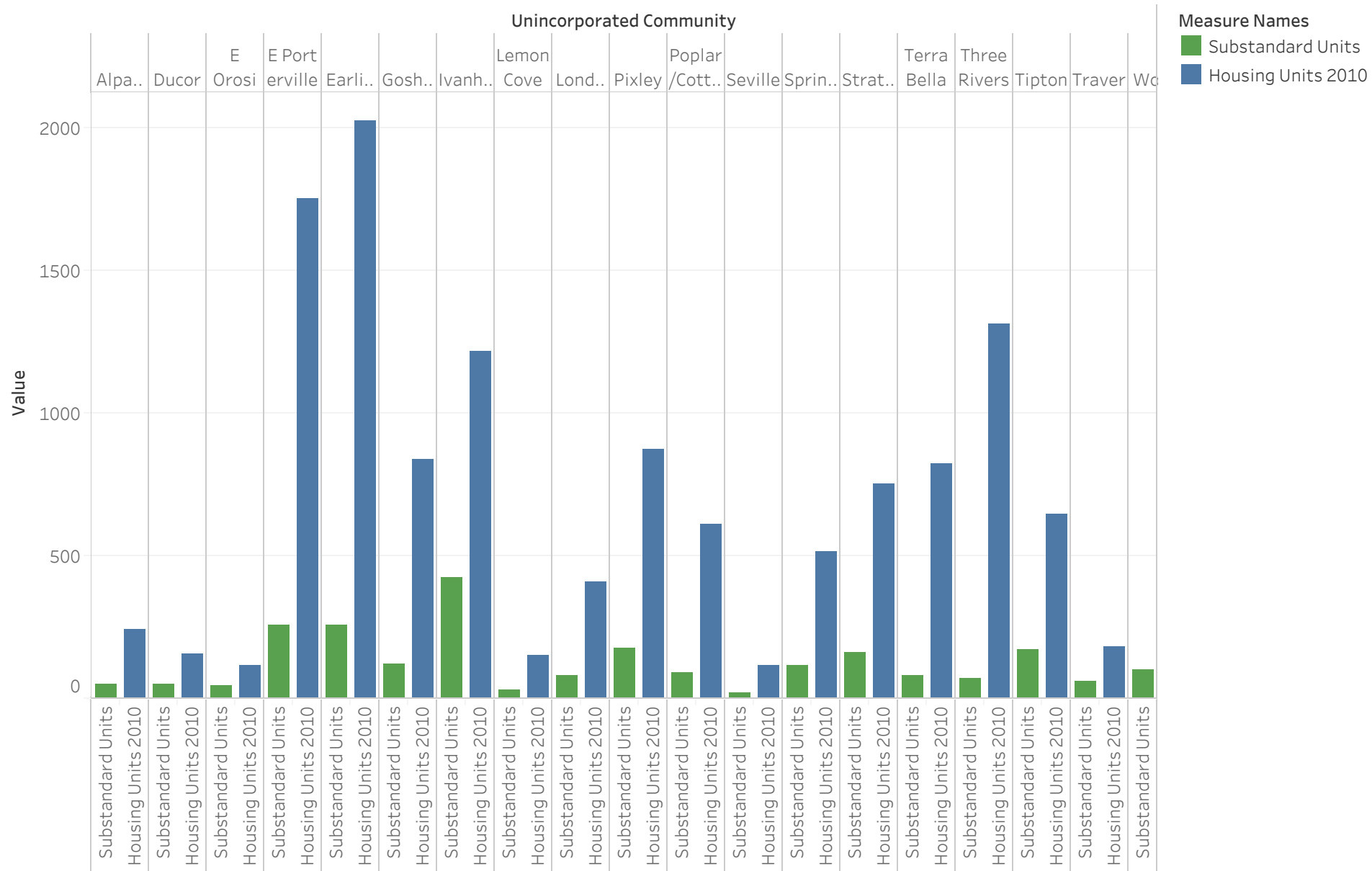
Sum of 2003 Survey Results for each Unincorporated Community.

Substandard 2015



Unincorporated Community. Color shows details about Unincorporated Community. Size shows sum of 2015 Survey Results. The marks are labeled by Unincorporated Community.

# Substandard VS. Total Housing Units



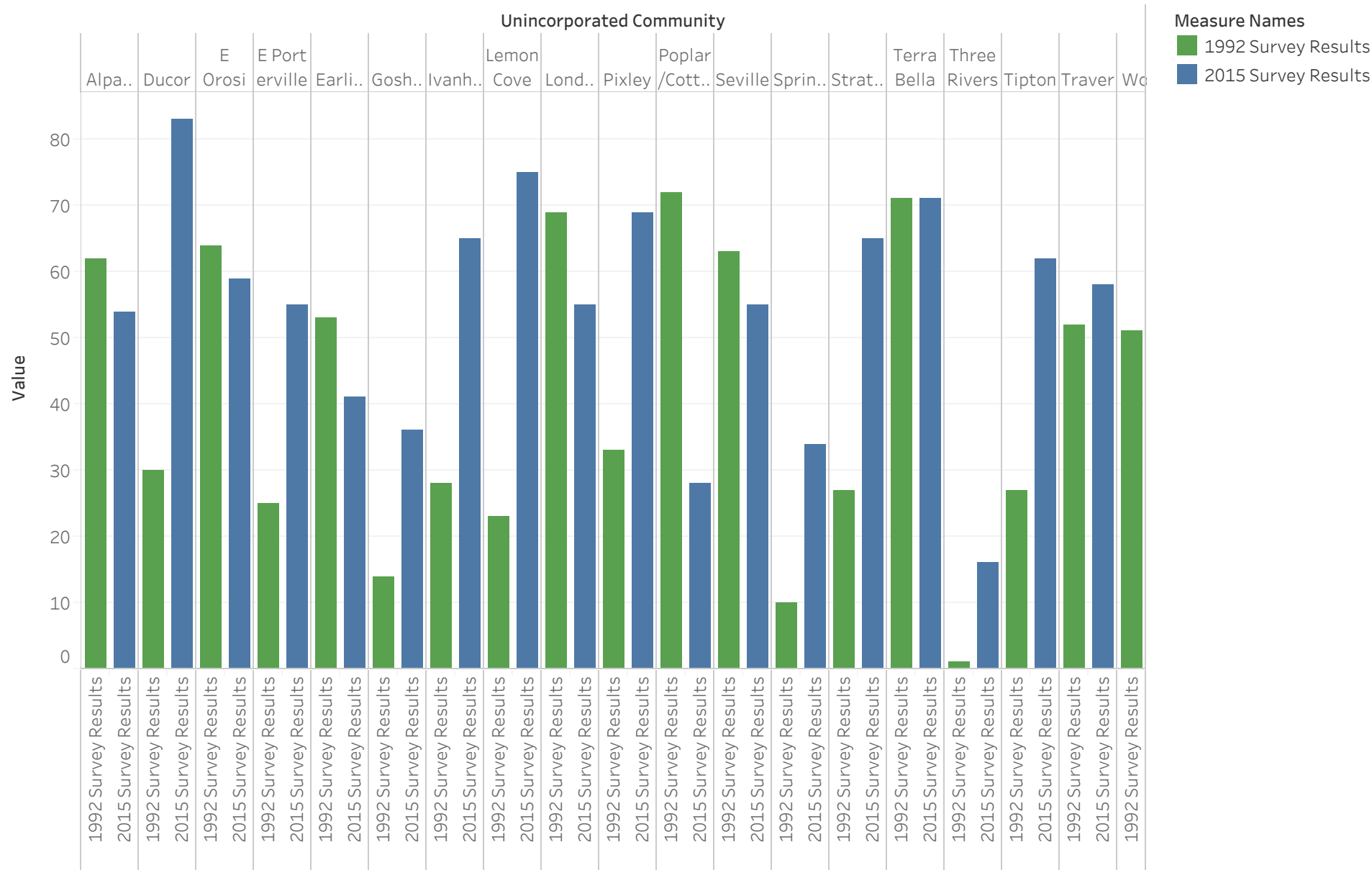
Substandard Units and Housing Units 2010 for each Unincorporated Community. Color shows details about Substandard Units and Housing Units 2010.

# Substandard VS. Total Housing Units



Substandard Units and Housing Units 2010 for each Unincorporated Community. Color shows details about Substandard Units and Housing Units 2010.

# Substandard Housing Survey 1992 vs. 2015



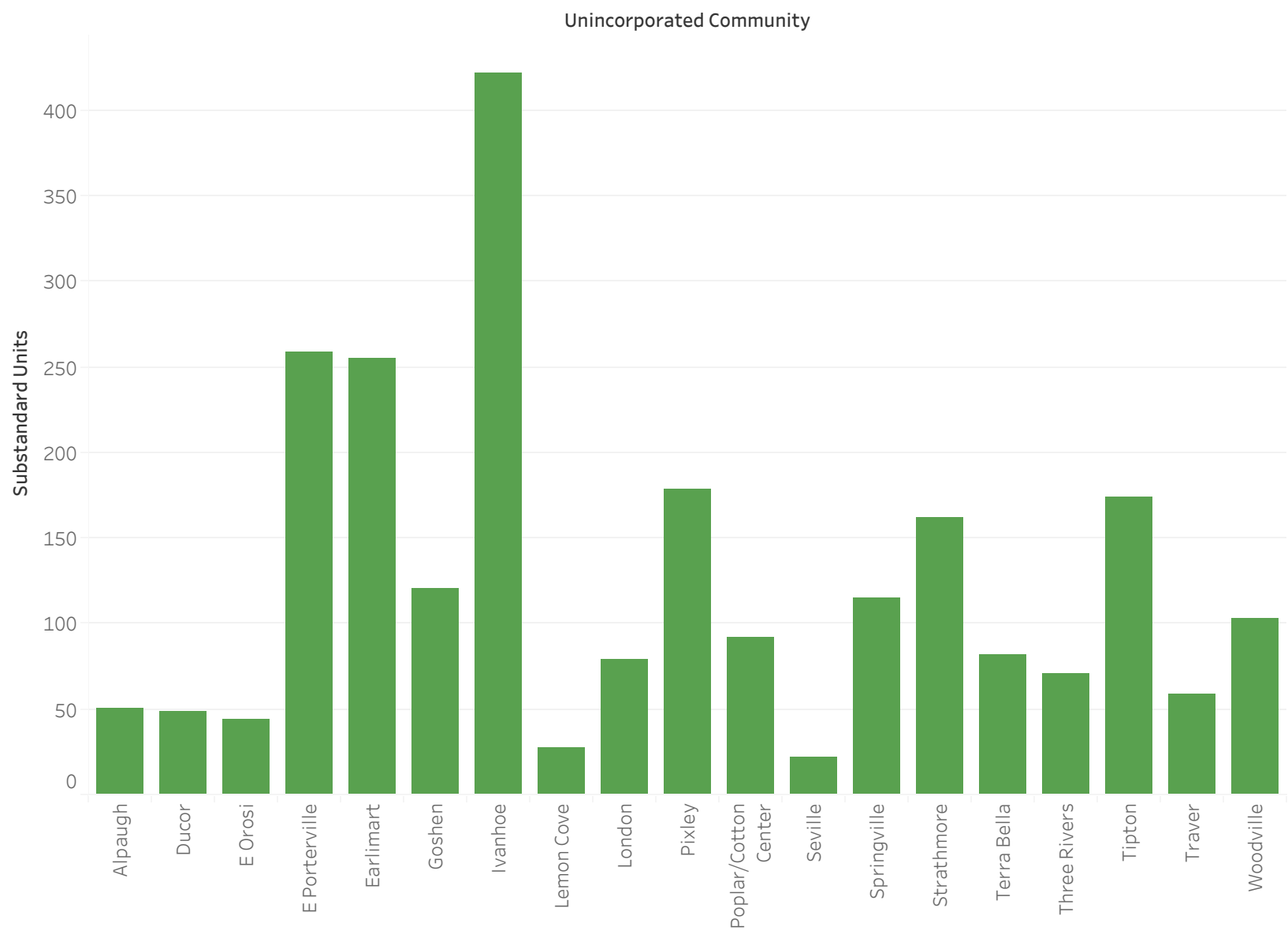
1992 Survey Results and 2015 Survey Results for each Unincorporated Community. Color shows details about 1992 Survey Results and 2015 Survey Results.

# Substandard Housing Survey 1992 vs. 2015



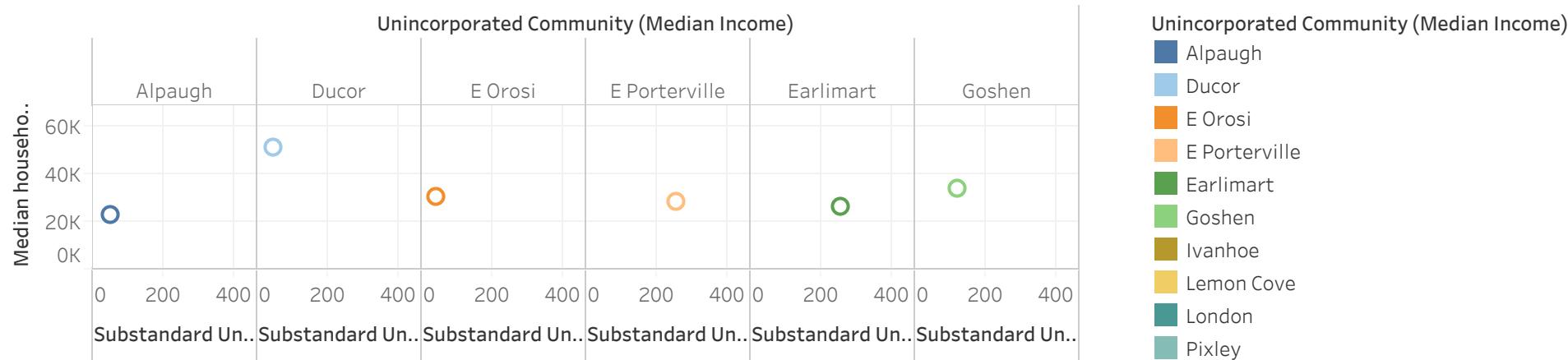
1992 Survey Results and 2015 Survey Results for each Unincorporated Community. Color shows details about 1992 Survey Results and 2015 Survey Results.

# Substandard Units by Community



Sum of Substandard Units for each Unincorporated Community.

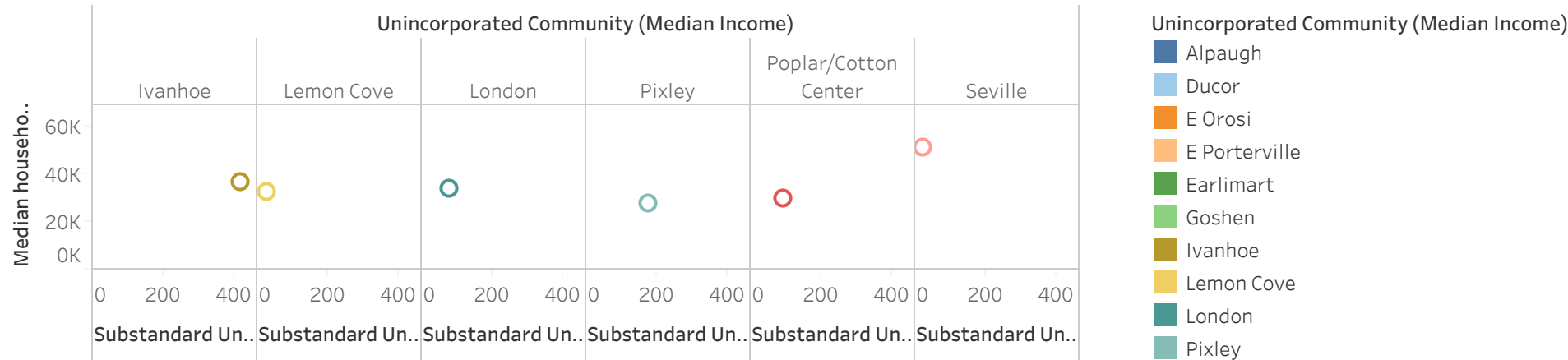
# Substandard Units and Median Income



Sum of Substandard Units vs. sum of Median household income (dollars) broken down by Unincorporated Community (Median Income). Color shows details about Unincorporated Community (Median Income).

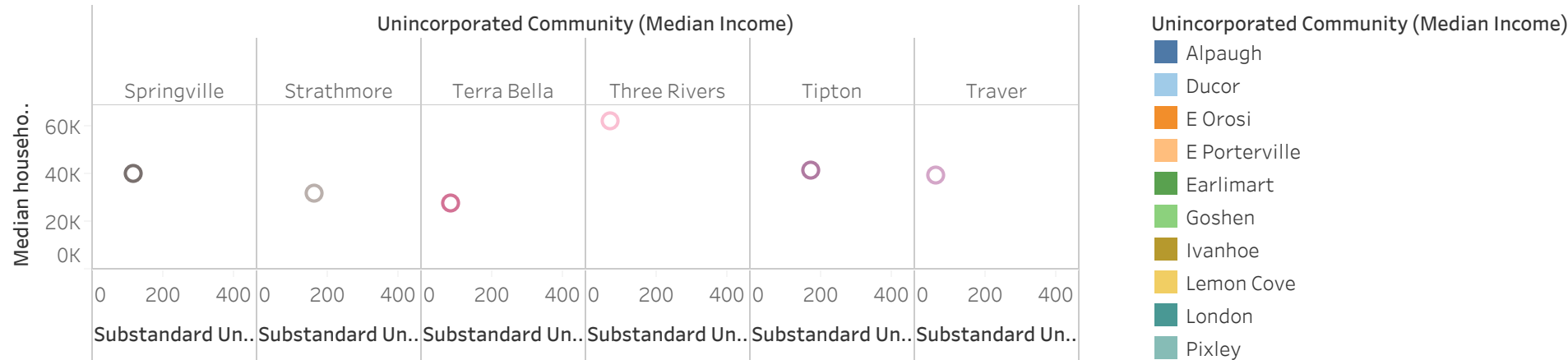


# Substandard Units and Median Income



Sum of Substandard Units vs. sum of Median household income (dollars) broken down by Unincorporated Community (Median Income). Color shows details about Unincorporated Community (Median Income).

# Substandard Units and Median Income



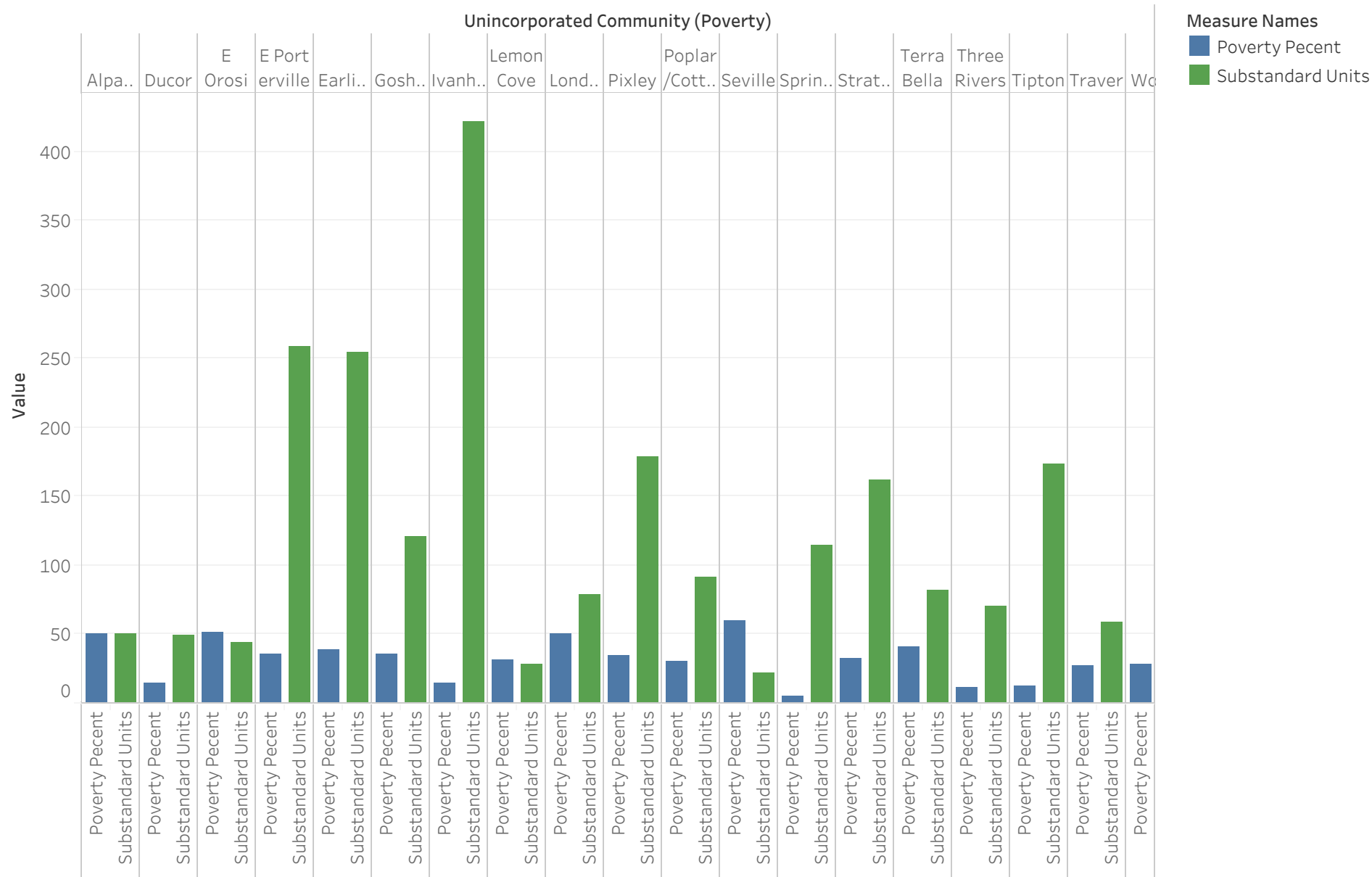
Sum of Substandard Units vs. sum of Median household income (dollars) broken down by Unincorporated Community (Median Income). Color shows details about Unincorporated Community (Median Income).

# Substandard Units and Median Income



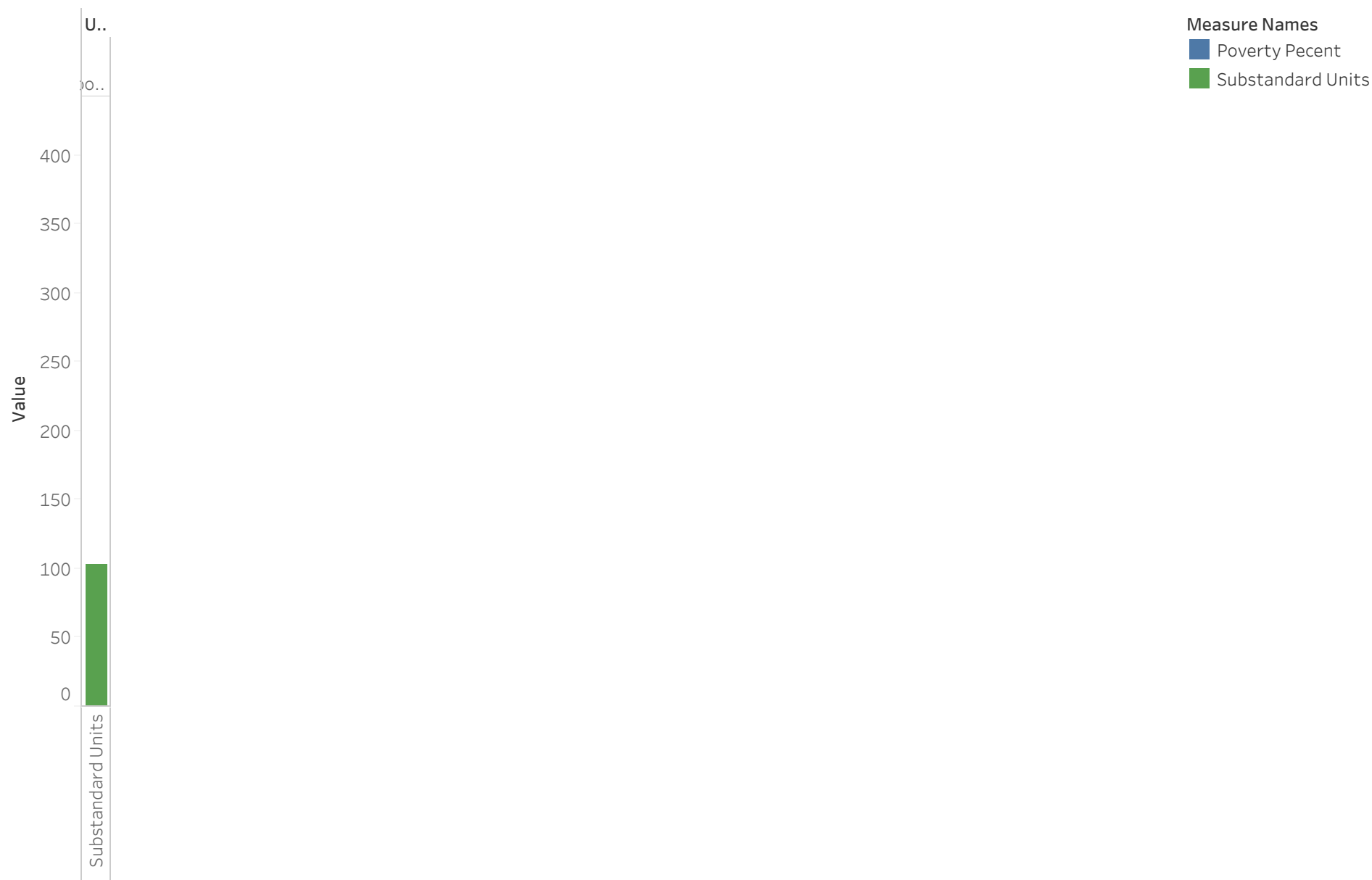
Sum of Substandard Units vs. sum of Median household income (dollars) broken down by Unincorporated Community (Median Income). Color shows details about Unincorporated Community (Median Income).

# Substandard Units and Poverty



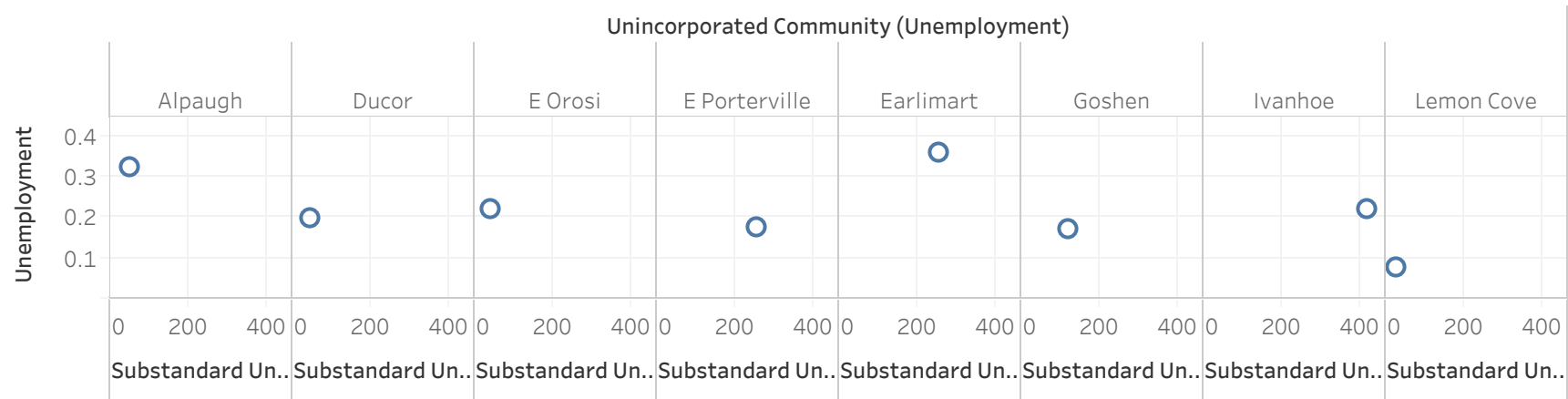
Poverty Pcent and Substandard Units for each Unincorporated Community (Poverty). Color shows details about Poverty Pcent and Substandard Units.

# Substandard Units and Poverty



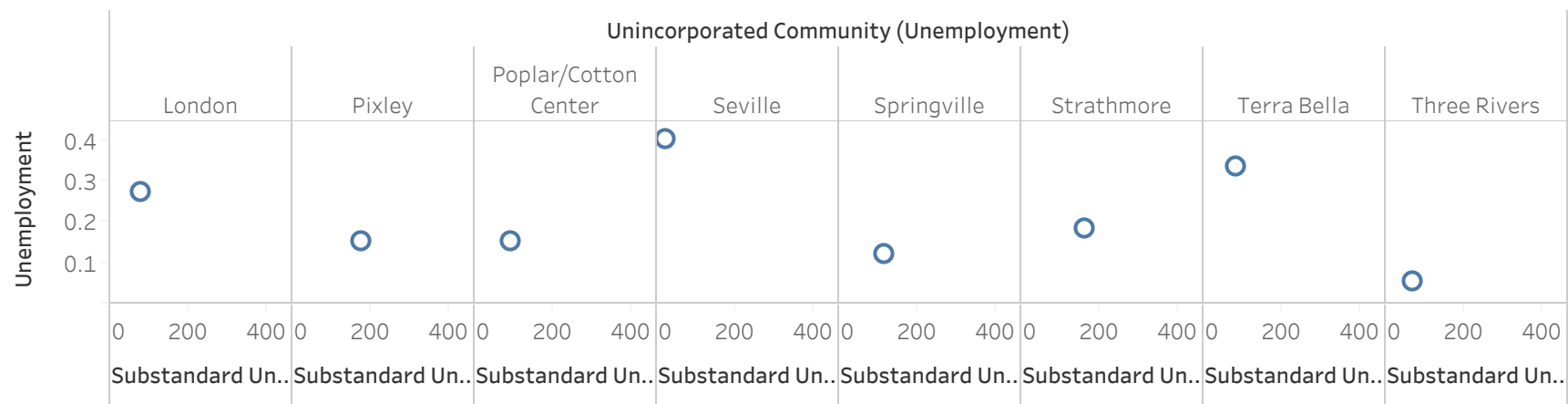
Poverty Percent and Substandard Units for each Unincorporated Community (Poverty). Color shows details about Poverty Percent and Substandard Units.

# Substandard Units and Unemployment



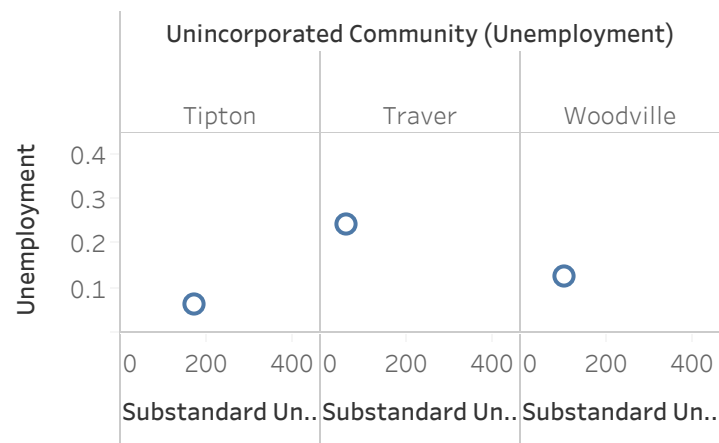
Sum of Substandard Units vs. sum of Unemployment broken down by Unincorporated Community (Unemployment).

# Substandard Units and Unemployment



Sum of Substandard Units vs. sum of Unemployment broken down by Unincorporated Community (Unemployment).

# Substandard Units and Unemployment



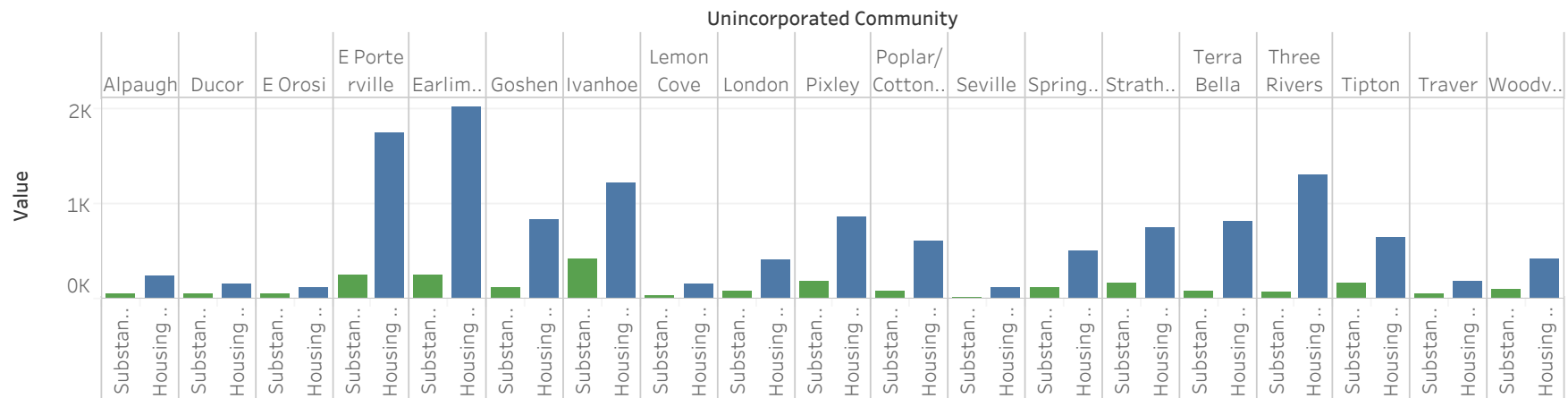
Sum of Substandard Units vs. sum of Unemployment broken down by Unincorporated Community (Unemployment).



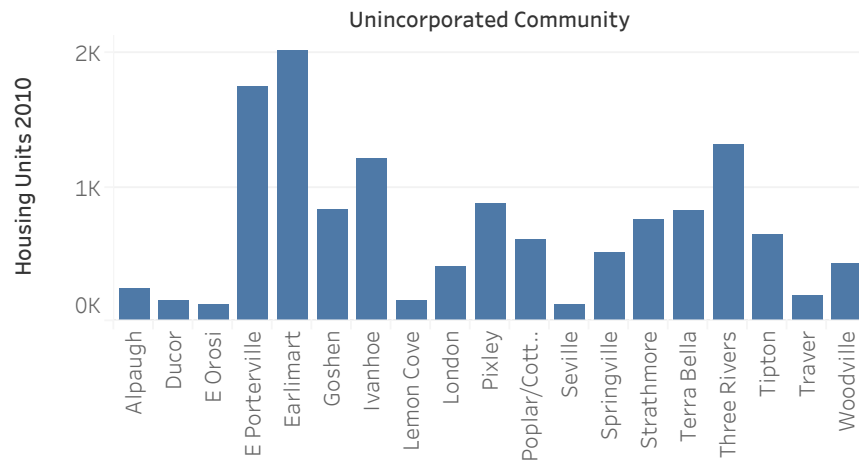
# Tulare County Substandard Units

These tables look at the number of substandard units by community compared to the total amount of units within that community.

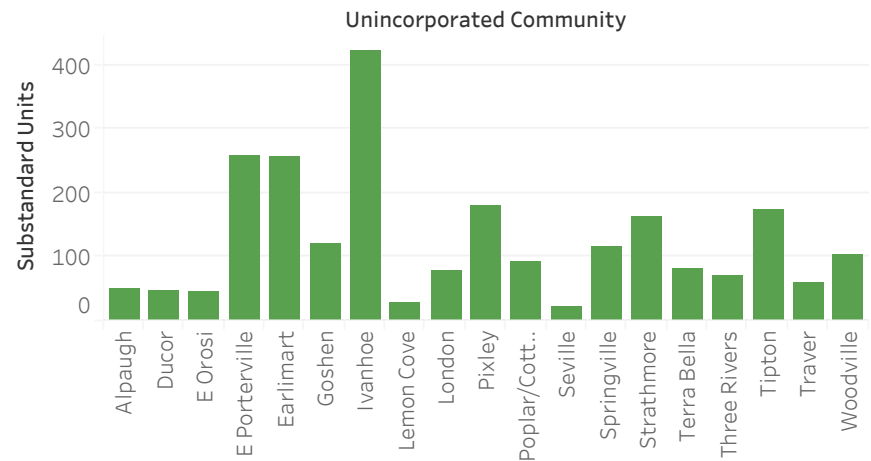
## Substandard VS. Total Housing Units



## Housing Units



## Substandard Units by Community



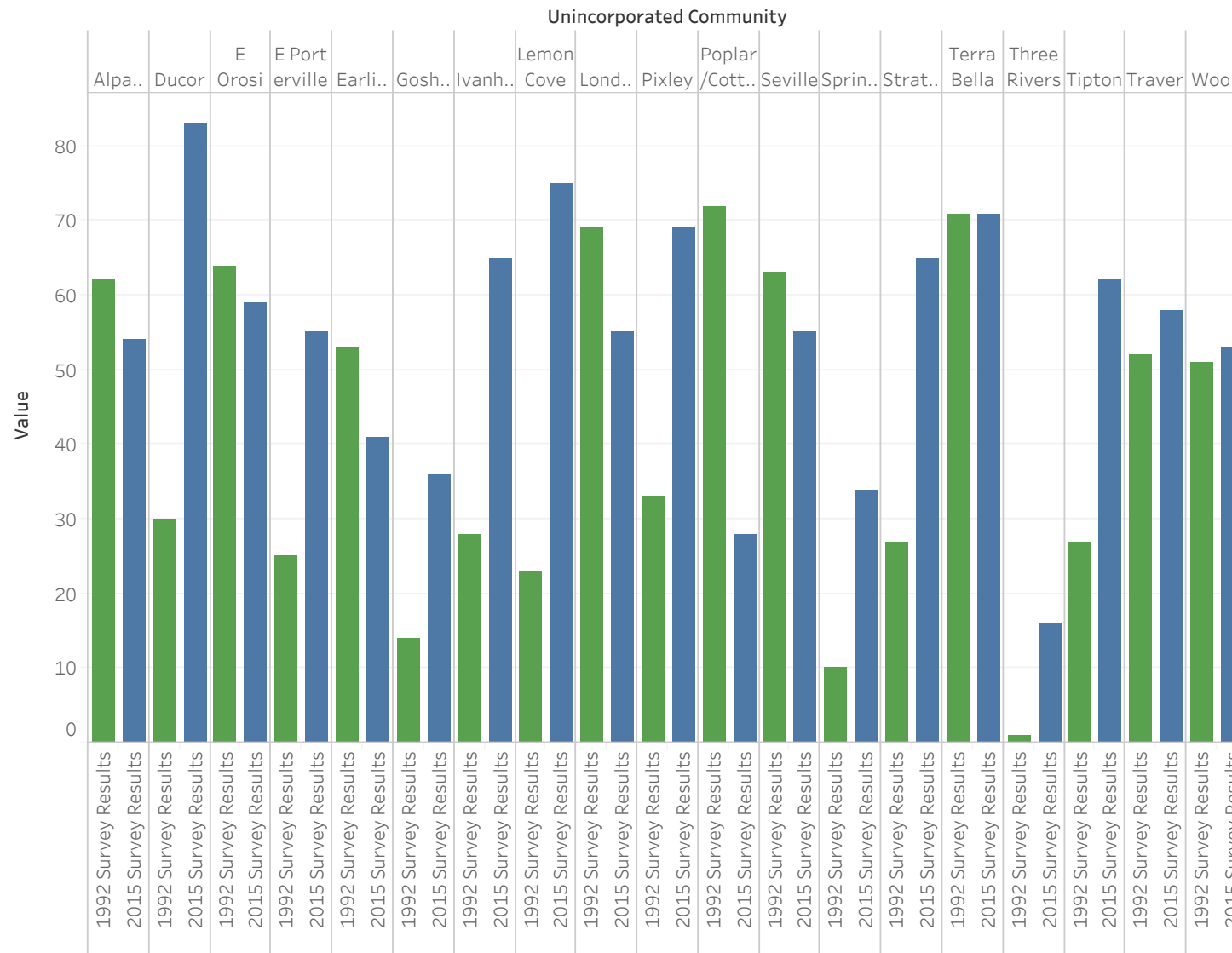
# Substandard Housing History 1992 Vs. 2015

This table compares Tulare County's 1992 Housing Conditions Survey with the 2015 Housing Condition Survey

## Measure Names

- 1992 Survey Results
- 2015 Survey Results

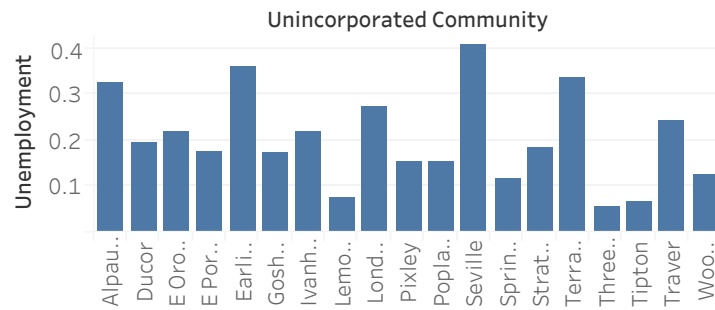
## Substandard Housing Survey 1992 vs. 2015



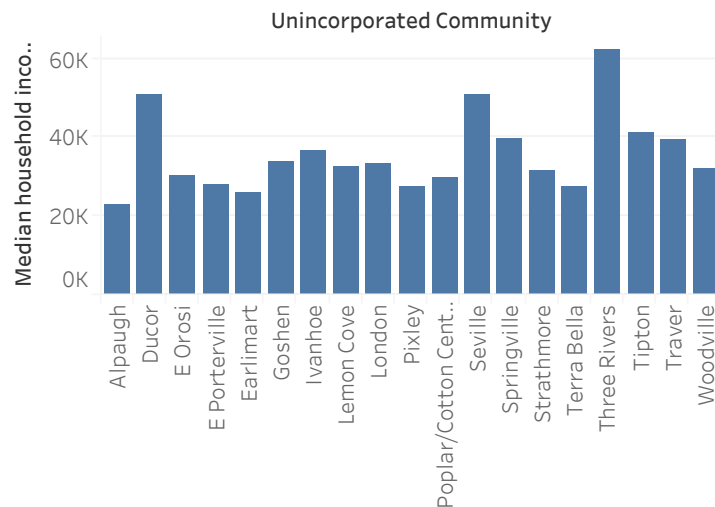
# Factors Affecting Substandard Units

These graphs look at Issues that may affect substandard housing within the County. Is there a link between a high poverty rate of poverty (DOF 2016), high unemployment and low median income affect the percentage of substandard housing within Tulare County Communities.

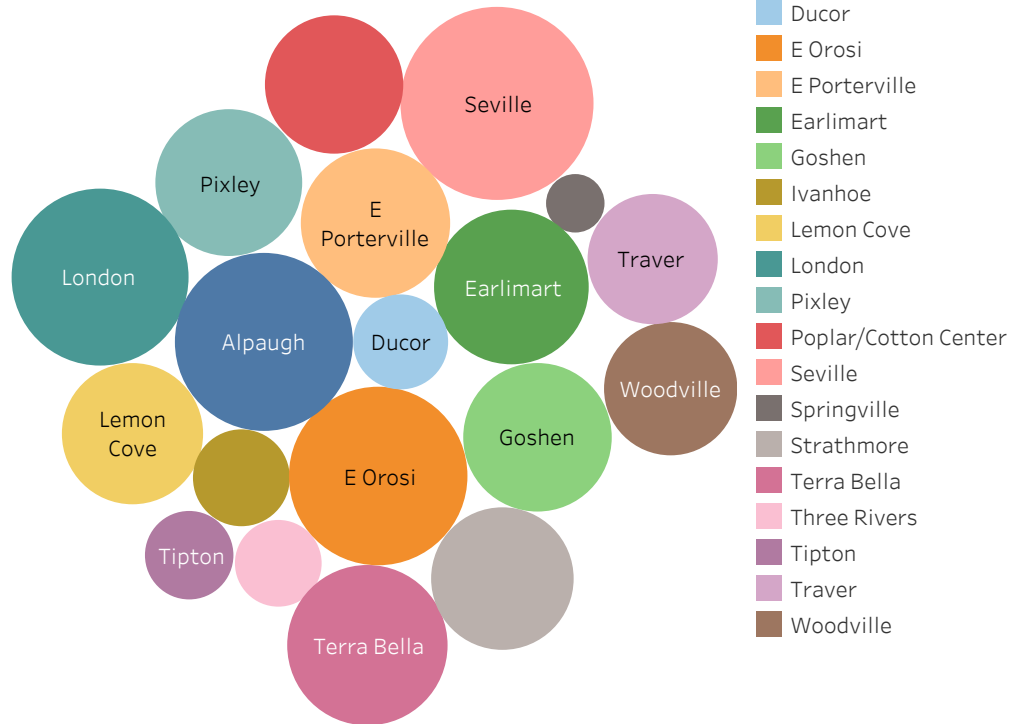
## Unemployment



## Median Income



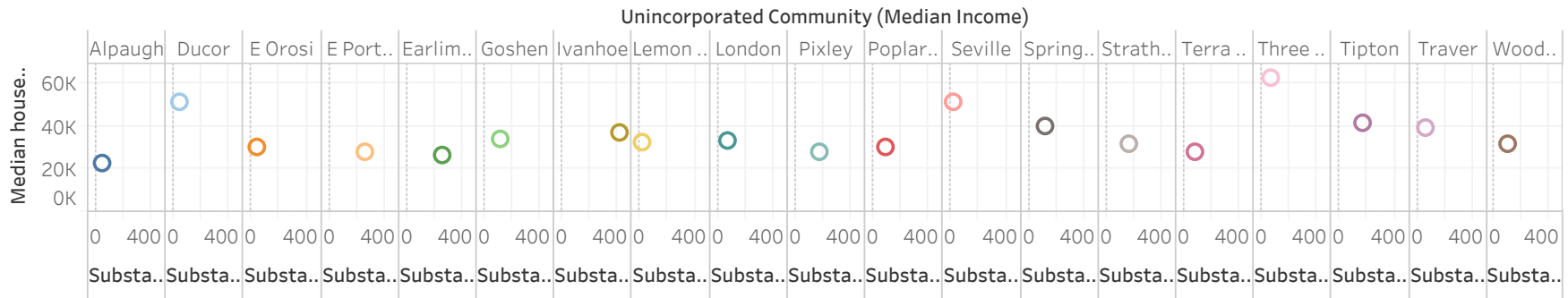
## Poverty Percentage



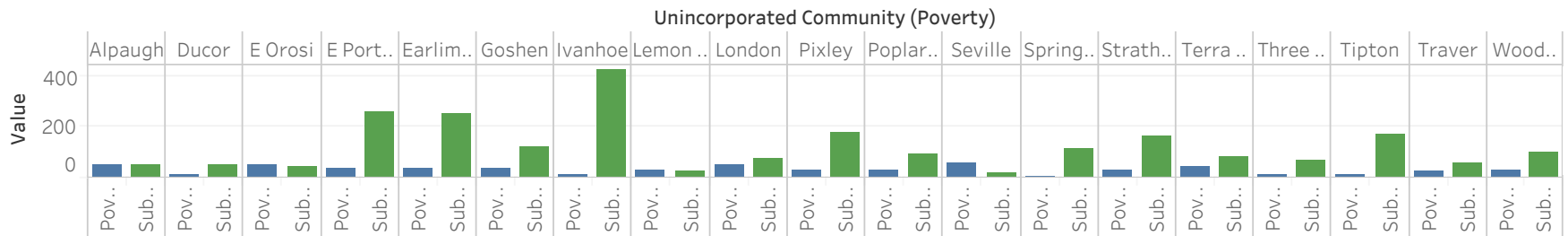
# Issues Affecting Substandard Units

These graphs show any correlation between poverty, unemployment and median income on the number of substandard units.

## Substandard Units and Median Income



## Substandard Units and Poverty



## Substandard Units and Unemployment

